



Metro Inner Development Assessment Panel Agenda

Meeting Date and Time: Tuesday, 3 December 2024; 9:30am
Meeting Number: MIDAP/49
Meeting Venue: 140 William Street, Perth

A live stream will be available at the time of the meeting, via the following link:
[MIDAP/49 - 3 December 2024 - City of Fremantle - Town of Bassendean](#)

PART A – INTRODUCTION

1. Opening of Meeting, Welcome and Acknowledgement
2. Apologies
3. Members on Leave of Absence
4. Noting of Minutes

PART B – CITY OF FREMANTLE

1. Declarations of Due Consideration
2. Disclosure of Interests
3. Form 1 DAP Applications
 - 3.1 Lot 8 (No.19) Essex Street, Fremantle - Four Storey Tourist Development – DAP/24/02724
4. Form 2 DAP Applications
5. Section 31 SAT Reconsiderations

PART C – TOWN OF BASSENDEAN

1. Declarations of Due Consideration
2. Disclosure of Interests
3. Form 1 DAP Applications
 - 3.1 Lot 85 (No.94) West Road, Bassendean - Childcare Premises – DAP/24/02721
4. Form 2 DAP Applications
5. Section 31 SAT Reconsiderations

PART D – OTHER BUSINESS

1. State Administrative Tribunal Applications and Supreme Court Appeals
2. Meeting Closure

Please note, presentations for each item will be invited prior to the items noted on the agenda and the presentation details will be contained within the related information documentation



ATTENDANCE

DAP Members

Tony Arias (Presiding Member)
Lee O'Donohue (Deputy Presiding Member)
Karen Hyde (Specialist Member)

Part B – City of Fremantle

Cr Ingrid Van Dorssen (Local Government DAP Member, City of Fremantle)
Cr Andrew Sullivan (Local Government DAP Member, City of Fremantle)

Part C – Town of Bassendean

Cr Jennifer Carter (Local Government DAP Member, Town of Bassendean)
Cr Tallan Ames (Local Government DAP Member, Town of Bassendean)

Minute Secretary

Laura Simmons (DAP Secretariat)

Officers in Attendance

Zoe Hendry (DAP Secretariat)



PART A – INTRODUCTION

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PART B – CITY OF FREMANTLE

1. Declarations of Due Consideration

2. Disclosure of Interests

3. Form 1 DAP Applications

- 3.1 Lot 8 (No.19) Essex Street, Fremantle - Four Storey Tourist Development
– DAP/24/02724

4. Form 2 DAP Applications

Nil.

5. Section 31 SAT Reconsiderations

Nil.

**Part D – Item 3.1 - LOT 8 (NO. 19) ESSEX STREET FREMANTLE – FOUR
STOREY TOURIST DEVELOPMENT**

**Form 1 – Responsible Authority Report
(Regulation 12)**

ADDENDUM TO RAR

At its meeting on 1 October 2024, the Metro Inner Development Assessment Panel (DAP) resolved to defer the application. The application was deferred by the DAP for the following reasons:

To enable the applicant to liaise further with the City to address potential height and visual impacts of the proposed addition on neighbouring residences and management of the proposed waste disposal system.

The reasons provided were as follows:

Whilst the majority of panel members considered the application had merit, there was concern regarding the visual amenity impact of the proposed rear four storey addition on surrounding residences.

Deferral was proposed and supported so that the applicant can reconsider the height and visual impact of the proposed addition on adjoining single and two storey neighbouring residences to the south and east.

The proposed waste disposal system may have a detrimental impact on Norfolk Lane which has limited area for placement of bins for collection. Review of this element is therefore sought.

This Responsible Authority Report (RAR) addendum relates to the specific deferral reasons outlined by the DAP and the revised proposal subsequently submitted by the Applicant. The RAR that follows is as previously submitted as the proposal remains (in substance) the same, unless specifically mentioned in this addendum.

The applicant submitted amended plans to the City of Fremantle on 22 October 2024 (included as Attachment 1 to this RAR, with previous plans included in the additional information) which included the following changes:

- Reduced overall building height from 13.8m to 13.32m.
- Additional opening to east aspect of bin storage room.
- Allocation of a bin holding zone pending collection.
- Additional detail on lift overrun capping.
- Additional colour and material detail.

Additional justification was also provided in relation to privacy, neighbouring amenity car parking and scheme requirements. This report is included in the additional information.

Height and visual impacts

<i>Previous building height</i>	<i>Amended building height</i>
4 storeys	4 storeys
13.8m external wall height	13.32m external wall height

The City's previous RAR details the provisions of Local Planning Scheme No. 4 (LPS4), and the reasons why, the previous scheme meets the detailed assessment criteria. In response to the reasons for deferral, the applicant has reduced the building height by 480mm, which further supports this position.

In addition to the reduction in building height, the lift overrun has also been shown on the plans. This component sits below 14m and is not considered to result in an unreasonable amenity impact.

No other changes have been made to setbacks to boundary or design of the new addition, however a further detailed design report has been provided by the applicant. This is included in the additional information.

Management of waste disposal

In addition to the above, a further amended Waste Management Plan (WMP) was submitted on 23 October 2024 which is included as Attachment 2. This plan provides additional detail and clarification on the waste collection process. Specifically, it is clearly identified that that waste collection vehicle will reverse into the right of way, with bins being ferried directly to and from the vehicle at the time of collection. There will be no presentation of bins to Norfolk Lane. The City is satisfied that this approach is capable of being delivered.

Condition 17 in the Officers Recommendation, has been updated to reflect the updated WMP date.

In summary, the proposed changes to the development proposal, while subtle, do reduce the height of the development. Further, the updates to the waste management strategy are a suitable solution to reduce the impact on the streetscape.

The Officers Recommendation remains as conditional approval.

DAP Name:	Metro Inner
Local Government Area:	City of Fremantle
Applicant:	Developed Property
Owner:	Kvarken Pty Limited
Value of Development:	\$8 million
Responsible Authority:	City of Fremantle
Authorising Officer:	Manager Development Approvals
LG Reference:	DAP002/24
DAP File No:	DAP/24/02724
Application Received Date:	14 June 2024
Report Due Date:	17 January 2025
Application Statutory Process Timeframe:	90 Days
Attachment(s):	1. Amended Development Plans - Dated 22 October 2024 2. Amended Waste Management Plan - Dated 23 October 2024 3. Acoustic Report 4. Applicant's Planning Report 5. Architectural Report

	6. Heritage Impact Statement 7. City of Fremantle Heritage Assessment 8. Heritage Council Response 9. Transport Impact Statement 10. DAC Meeting Minutes 11. Schedule of Submissions and Applicant Response. 12 Sustainability Report 13. Site Photos 14. Additional Applicant Justification 15. Previous Development Plans
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Responsible Authority Recommendation

That the Metro Inner DAP resolves to:

1. **Accept** that the DAP Application reference DAP/24/02724 is appropriate for consideration as a Tourist Development land use and compatible with the objectives of the zoning table in accordance with the City of Fremantle Local Planning Scheme No. 4.
2. **Refuse** DAP Application reference DAP/24/02724 and accompanying plans (DA00.00, DA00.01, DA01.01, DA01.02, DA01.03, DA01.04, DA01.05, DA01.06, DA01.07, DA02.01, DA02.02, DA02.03, DA02.04, DA02.05, DA02.06, DA03.01, DA03.02, DA03.03, DA03.04, DA04.01) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of the City of Fremantle Local Planning Scheme No. 4, for the following reasons:

Reasons

1. The proposal is inconsistent with clauses 67(a) of the Deemed Provisions as the building height does not meet the requirements of Schedule 7 clause 1.2b and c of Local Planning Scheme No. 4 due to the bulk and scale of the rear addition resulting in a detrimental impact to the amenity and heritage significance of neighbouring properties and the surrounding area.
2. The proposal is inconsistent with clauses 67(m) and 67(n) of the Deemed Provisions as the proposal will result in a detrimental impact to the amenity of neighbouring properties by virtue of the height, scale and setbacks of the rear addition.

Reasons for Responsible Authority Recommendation

The proposal is considered to have an unreasonable impact on heritage properties in the area due to the bulk and scale of the new addition at the rear. It is not considered that the applicant has sufficiently dealt with the reasons for deferral by the DAP, with the reduction in height doing little to change the built form and reducing the amenity of the internal units.

Details: outline of development application

Region Scheme	Metropolitan Region Scheme (MRS)
Region Scheme - Zone	Central City
Local Planning Scheme	Local Planning Scheme No. 4 (LPS4)
Local Planning Scheme - Zone	City Centre
Use Class and permissibility:	Tourist Development - D
Lot Size:	1614m ²
Existing Land Use:	Office, Restaurant, Small Bar
State Heritage Register	No
Local Heritage	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> Heritage List <input checked="" type="checkbox"/> Heritage Area
Design Review	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other
Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

The application seeks approval for a four storey Tourist development at No. 19 (Lot 8) Essex Street, Fremantle (subject site). The proposed development includes internal alterations to the existing building as well as the addition of a four storey building to the rear of the site. The proposed land use is for a short stay accommodation premises comprising 80 rooms with 247 beds and associated amenities (communal dining, kitchen, laundry and bathroom facilities etc.) and a small bar.

The key components of the proposed development can be seen at Attachment 1 and are summarised as follows:

Proposed Land Use	Tourist development
Proposed Net Lettable Area	2600m ²
Proposed No. Storeys	Two (2) - Four (4)
Proposed No. Dwellings	N/A

The works component of the development includes:

- Demolition of internal ground floor partitions within the south building (ground floor of the north building will be left as existing).
- Demolition of internal upper floor partitions, stairwell and wet areas in both the north and south building
- Removal of sections of roof sheeting to be replaced with translucent sheeting.
- Internal fit out of the ground floor of the existing south building for a small bar, guest communal dining, kitchen and laundry facilities.
- Internal fit out of the upper floor for bedrooms and bathrooms in both the north and south building

- Construction of a four storey addition at the rear of the site consisting of waste, laundry, parking, bathroom and bike store facilities on the ground floor and bedrooms on the first, second and third floors.
- Remediation works to heritage façade of the north building.

The applicant submitted amended plans on 23 August 2024 including the following changes:

- Addition of three (3) 'bike loops' on the Essex Street verge and the indication of an existing bike store containing 10 bicycle hoops at the rear of the building.
- Removal of the 'LyLo' signage on the exterior of the rear addition.
- Indication of air conditioner units on the roof of the existing buildings (to be contained within the valley of the north and south buildings of the subject site).
- Diagram illustrating the visibility of the four storey addition from Essex and Norfolk Street.

Background:

Site Context

The subject site has a total land area of 1614m² and is zoned City Centre under Local Planning Scheme No. 4 (LPS4). The site is located within the Central Fremantle Heritage Area and abuts a State heritage listed building to the west and City of Fremantle heritage listed buildings to the south and east (a right of way runs along a portion of the south rear boundary). The surrounding area features a mix of residential and commercial land uses.

The subject site contains two buildings joined by a party wall down the middle of the site. The existing north building was constructed circa 1907 and is part of the Former Mills and Co. Building. In 2012 the buildings located on the southern half of the site, constructed circa 1940, were demolished and a new building to match the retained north building was constructed. Various alterations and changes in use have occurred on the site throughout recent decades with the building currently being used for offices, an educational establishment, a small bar and a restaurant.

Refer to the figures below for additional site context.



Figure 1 – Planning Context Map



Figure 2 – Site Aerial Image



Figure 3 – Existing Street View

Site History

The relevant development history evident under the property file is listed below:

- DA0065/23 – Internal alterations to existing building and change of use to public amusement (existing 'Escape Room' venue in tenancy 1)
- DA0120/22 – Signage and internal alterations to existing building (signage and fit-out for the exiting small bar 'The Flaming Galah' in tenancy 2 – note: Small bar land use is exempt under Local Planning Policy 1.7).
- DA0525/20 – Change of use from office to education establishment, signage and internal fit-out (existing 'Centacare' educational establishment in tenancy 3 – to be replaced by proposed land use)
- DA0322/19 – Four storey tourist accommodation building and additions and alterations to existing building (never acted on)
- DA0364/14 - Change of use to Fast Food Outlet and addition of signage (existing 'Tutti Frutti' fast food outlet in tenancy 3 – to be replaced by proposed land use)
- DA0472/10 - Partial demolition of existing buildings on the south of the site, partial change of use to office and signage additions

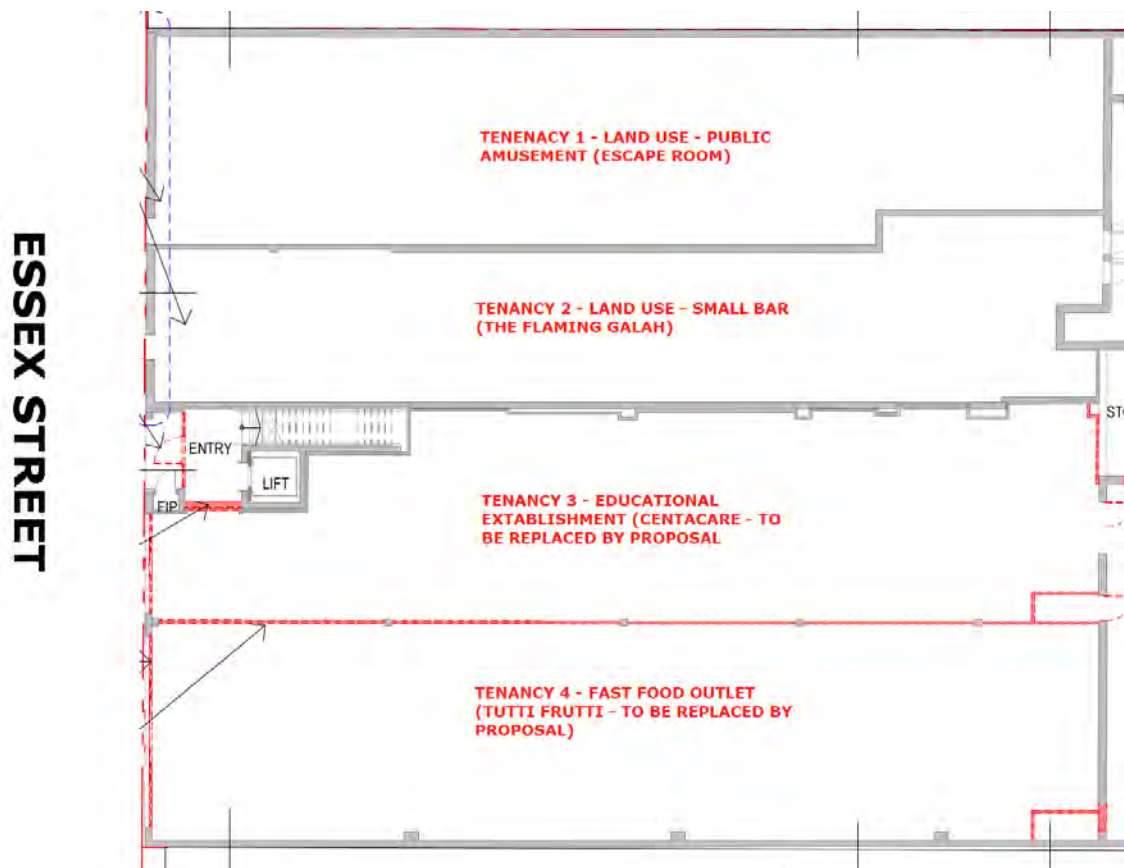


Figure 4 – Existing Land Uses

Legislation and Policy:

Legislation

- *Planning and Development Act 2005*
- *Planning and Development (Local Planning Schemes) Regulations 2015 (WA)*
- *Planning and Development (Development Assessment Panels) Regulations 2011*
- Metropolitan Region Scheme
- City of Fremantle Local Planning Scheme No. 4

State Government Policies

- State Planning Policy 3.5 - Historic Heritage Conservation
- State Planning Policy 7.0 - Design of the Built Environment

Local Policies

- Local Planning Policy 1.3 – Community Consultation on Planning Proposals
- Local Planning Policy 1.6 – Heritage Assessment and Protection
- Local Planning Policy 1.9 – Design Advisory Committee and Principles of Design
- Local Planning Policy 1.10 – Construction Sites
- Local Planning Policy 2.3 – Fremantle Port Buffer Area Development Guidelines

- Local Planning Policy 2.13 – Sustainable Buildings Design Requirements
- Local Planning Policy 2.19 – Contributions for Public Art and/or Heritage Works
- Local Planning Policy 2.24 – Waste Management Plans for New Development
- Local Planning Policy 3.6 – Heritage Areas

The development assessment has been undertaken in accordance with the abovementioned legislation and policies.

Consultation:

Public Consultation

In accordance with LPP1.3 (Community Consultation on Planning Proposals), the application was advertised to the public from the 8 July 2024 until 6 August 2024 by means of letters to owners/occupiers of properties within a 200m radius of the site, a sign on site, notices in the press and inclusion on the City's MySay webpage.

In response, the City received a total of 25 submissions. 22 submissions were received that objected to the proposal and 3 submissions in support of the proposal. A schedule of the public submissions which includes the full details on each submission as well as the applicant's response to these submissions are included in the additional information.

A summary of the key points raised in the submission in support of the proposal are detailed below:

- The development is an opportunity to draw more visitors into the city centre.
- More accommodation in the city centre will be a positive change.
- The development does not appear to significantly change the appearance of the streetscape.
- Fremantle is in need of higher density tourist accommodation.

A summary of the key points raised in objection to the proposal are included in Table 1 below:

Table 1 – Summarised Objections and Officer Comments

Issue Raised	Officer comments
Parking/ Traffic <ul style="list-style-type: none"> • Insufficient car parking provided • Insufficient facilities for bicycle storage • Increased traffic from service vehicles, deliveries and guest arrivals causing traffic congestions. • No designated area for guests arriving via taxi/uber etc. • There is currently difficulty in finding parking on Essex Street. 	Parking and bicycle facilities have been assessed in the following sections. A requirement for an Operational Management Plan to manage guest arrivals and service deliveries is included as a recommended condition.
Building height <ul style="list-style-type: none"> • Detrimental impact on the character of Norfolk/Essex Street 	Building height in excess of 11m (to a height of four storeys/ 14m) can be considered subject to satisfying the

<p>due to the visibility of the rear addition.</p> <ul style="list-style-type: none"> The 11m maximum building height must not be exceeded. 	<p>requirements of LPS4. The building height has been assessed in the following sections below.</p>
<p>Design of the rear building</p> <ul style="list-style-type: none"> The rear addition will not blend in with the surrounding area and is not aesthetically pleasing. A four storey square shaped building does not fit with the character of buildings in the surrounding area. The branding on the exterior of the building is excessive. 	<p>The proposal has been carefully considered against the context and character of the Central Fremantle Heritage Area. In addition, the proposal was referred to the Design Advisory Committee (DAC) and Heritage Council who were generally supportive of the proposal. The DAC and Heritage Council comments and built form assessment are included in the following sections below.</p>
<p>Overshadowing</p> <ul style="list-style-type: none"> The development exceeding 11m in height will result in significant overshadowing of outdoor living areas/major openings on adjacent properties. Insufficient overshadowing plans/models provided. 	<p>The applicant provided shadow diagrams which detail the shadow cast throughout various times of the day. It is considered that the shadow diagrams are an accurate representation of how the shadow will move throughout the day. It is considered that the shadow will not be significantly concentrated on a single site for a substantial period of the day.</p>
<p>Visual privacy</p> <ul style="list-style-type: none"> There will be a loss of privacy to major openings and outdoor living areas on adjoining properties. Direct overlooking from windows over adjoining properties. 	<p>The applicant has indicated screening to be installed on the upper floor windows of the rear addition to reduce the impact on neighbouring properties. A condition is recommended for a detailed drawing of this screening to be provided prior to the lodgement of building permit application.</p>
<p>Impact to adjacent heritage buildings</p> <ul style="list-style-type: none"> Detrimental impact to the level 2 heritage listed building on the subject site. Detrimental impact to the adjacent heritage listed buildings during construction. Impact to the small portion of limestone boundary wall adjoining a State Heritage Registered place. 	<p>The proposal was referred to the Heritage Council to consider heritage implications on adjacent heritage listed buildings (including the limestone boundary wall at the rear of the site). The Heritage Council was generally supportive of the proposal subject to conditions pertaining to a dilapidation survey of adjacent heritage listed buildings as well as a program of monitoring any structural movements of these buildings. The advice from the Heritage Council is included in the following sections below. This advice will form recommended conditions.</p>
<p>Visual impact to the streetscape/ character of the area / surrounding properties</p> <ul style="list-style-type: none"> There's no representation of how the four-storey development at the 	<p>The proposal has been carefully considered against the context and character of the surrounding streetscapes. The applicant provided 3D mapping data to allow input into the</p>

<p>rear will impact the Norfolk Street streetscape.</p> <ul style="list-style-type: none"> Visual impact of external plant and fixtures 	<p>City's mapping software. 3D Visual representations are included in the building height assessment below. A condition will be recommended to ensure external plant and fixtures are in a suitable location to minimise visual impact on the surrounding area.</p>
<p>Development density/ number of occupants/rooms</p> <ul style="list-style-type: none"> Excessive number of rooms/beds. The density will result in excessive activity in an area where people reside. 	<p>The density of the development is considered appropriate for the region-serving role of the City Centre.</p>
<p>Noise</p> <ul style="list-style-type: none"> Excessive noise from plant and machinery (air con etc.) Excessive noise from patrons of the small bar as well as guests staying at the accommodation. Impact from the 24/7 operation of the accommodation. 	<p>The applicant has provided an acoustic report which is included as Attachment 3. It is considered that the plant and equipment for the development, as indicated in the acoustic report, features sufficient separation/screening to effectively reduce the impact of noise on surrounding noise sensitive premises. Regardless of this, any noise generated from the proposed development must comply with the <i>Environmental Protection (Noise) Regulations 1997</i>.</p>
<p>Waste management</p> <ul style="list-style-type: none"> Detrimental impact on the amenity of the area from odour and noise from disposing of waste. Insufficient detail to illustrate how the amount of waste generate from 300 people will be managed. Placement of bins blocking right of ways and access along Norfolk Lane. 	<p>A waste management plan (WMP) has been provided and reviewed by internal departments at the City. A contractor will be utilised to manage the waste output from the proposed development. The waste management of the development has been further addressed in the applicable section below.</p>
<p>Construction impacts</p> <ul style="list-style-type: none"> Impact on adjoining businesses (nearby bed and breakfast etc.) during construction. 	<p>A condition is recommended for a construction management plan to be provided prior to the lodgement of a building permit application to ensure impact to the surrounding area is mitigated during the construction phase.</p>
<p>Other</p> <ul style="list-style-type: none"> The accommodation is an unappealing place to stay. Hotels are not full during winter time; Another hotel in the City Centre is not needed. Backpackers are already catered for in the City Centre, more accommodation for them is not required. 	<p>Assessment of the proposed land use and the impact on the amenity of the area is discussed in the report below. A requirement for an Operational Management Plan to mitigate any potential detrimental impact from anti-social behaviour is included as a recommended condition.</p>

<ul style="list-style-type: none"> • There are other more appropriate locations for the proposed development. • Anti-social behaviour from guests • The proposal has little to offer the Fremantle City Centre and will not offer anything to the community. • There is a misrepresentation of how the rear addition will appear from the streetscape. • The proposed development submission fails to identify 15-17 as a residential property. • The type of cliental the Small Bar is targeting via the provision of cheaper alcohol will increase anti-social behavior. 	
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Referrals/consultation with Government/Service Agencies

Heritage Council

As the subject site is adjacent to a State Heritage listed building and proposes significant additions and alterations, it was required to be referred to the Heritage Council.

Heritage Council have advised that the proposal will not affect the cultural heritage significance of the adjacent properties (Port Flour Mill and Fremantle Technical College Annexe) and minimal adverse impact on 26-28 Norfolk Street. This is subject to conditions as follows:

- 1. A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken.*
- 2. A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer.*

Conditions of approval have been recommended in accordance with the above. A copy of the Heritage Council advice is included in the additional information.

City of Fremantle Heritage Comment

19 Essex Street contains three elements of cultural heritage significance:

- 19B façade
- 19B interior of ground floor tenancies
- Limestone wall to rear boundary

19A Essex Street, the southern half of the building is a two-storey addition constructed in 2012 to replace an earlier building from 1941. This part of the building

has no heritage significance but is sympathetic with the character of streetscape. The proposed changes will have no impact on the heritage significance of 19 Essex Street or the heritage character of Essex Street.

The works proposed in this application will have only a minor impact on the heritage significance of 19B Essex Street. The façade and the shopfronts will undergo some remediation and repainting which not adversely affect heritage fabric or heritage significance. The interior of the ground floor tenancies will undergo some minor changes but will retain original finishes and remain as two tenancies.

The proposed four storey accommodation block is located at the rear of the site and will not directly affect any significant heritage fabric at 19 Essex Street or adjoining heritage properties. It will have little visual impact on the presentation of 19 Essex Street and its contribution to the streetscape as it is a considerable distance from the street and will be largely concealed by the existing two storey building at the front of the site.

The limestone wall on the rear boundary of the site will not be affected by this proposal. However, the wall needs to be protected during construction as it will not have a modern structurally designed footing only a thickening of the wall. Any works to this wall must match the existing original stonework and lime pointing and cement mortars and sealers must not be used.

The works proposed in this application are acceptable as they will have only a minor impact on the heritage values of 26-28 Norfolk Street, 19 Essex Street and the Central Fremantle Heritage Area. However, precautions need to be undertaken during construction to ensure that the adjacent State Heritage listed buildings 26-28 Norfolk Street and Port Flour Mill are not adversely affected.

Recommended conditions:

1. Refer to HCWA advice dated 29 July 2024.
2. Methodology for stonework repair to be provided as part of the Building Licence documentation. Repairs to match original stone, stonework and pointing in stone type, coursing pattern and mortar composition and colour. Cement mortar or sealers are not to be used.

The full heritage assessment is included in the additional information.

Fremantle Port Authority

The Fremantle Port Authority requested that the relevant requirements of LPP2.3 (Fremantle Port Buffer Area Development Guidelines), be addressed. As the site sits in the 'Buffer Zone 2', these requirements will be secured by a recommended condition.

Other Advice

The application was referred to the relevant internal departments at the City of Fremantle for formal comment as part of the assessment process. All departments were generally satisfied with the proposed development. Any comments received will be addressed via conditions and advice notes. The comments received related to ensuring standard requirements are followed, specifically:

1. Building permit required (advice notes)

2. Removal of asbestos (advice note)
3. Noise from construction works (advice note)
4. Stormwater management (condition)
5. Construction management plan (condition)
6. Waste management (condition)

Design Review Panel Advice

The applicant engaged with the City's Design Advisory Committee (DAC) prior to lodgement of the application on 13th May 2024.

The DAC provided comment and recommendations on the proposal against the design generally as well as against each of the 10 Principles of Design within LPP1.9 (Design Advisory Committee & Principles of Design) and State Planning Policy 7 (Design of the Built Environment):

The DAC meeting minutes are provided as follows:

Strengths of the Proposal

- *A new tourism development proposal in the Fremantle City Centre for visitors who are seeking affordable hotel accommodation.*
- *The LyLo model appears to be successful, with hotels in some of the major cities of New Zealand and in Brisbane; the proposal for Fremantle is the first in WA.*
- *The Hotel model has the clear core values of "Authentic, Home and Local".*
- *The Hotel is an 'intense' development in terms of the high density of visitors accommodated (potentially 248 guests based on the room configurations). This new visitor population should contribute to the life and vibrancy of the city.*
- *A sustainable design approach through the adaptive re-use of the existing two buildings on the site (including a Local Heritage Listed building at 19B).*
- *New 4-storey addition is located at the eastern rear of the site with minimal negative visual and amenity impacts on the streetscape and surrounding existing properties.*
- *Provision of an extensive area of different types of communal facilities for guests.*
- *Activation opportunities of the building interface with Essex Street at both floor levels by way of al fresco dining areas.*
- *Inclusion of accessibility rooms for people with disabilities.*

Principle 1 Context and character

- a) *The Proponent should ensure the early consideration of the integration of heritage with the new proposal. The Proponent should demonstrate an understanding of what is of heritage significance and what is not, and how much significant fabric is to be retained or demolished and its impact assessment on the heritage values of the place.*
- b) *Provide a Heritage Impact Statement (HIS) prior to the submission of the Development Application. The impact assessment should address, avoid and propose a mitigation strategy for the demolition of significant fabric and aesthetical streetscape impact. In addition to the Local Heritage Listed*

structures, the HIS also should address the State Registered Properties near to and adjacent to the site.

The DAC suggested providing a streetscape perspective of the proposal looking north along Essex Street and to include the Fremantle Technical School.

- c) 19 Essex Street is essentially the amalgamation of two buildings on two lots (19A and 19B). The Proponent should maintain the character of Essex Street by respecting the existing fine grain and rhythm of the lot pattern and buildings.*

The DAC recommends a review of the proposed new awning (19A) and its exact horizontal alignment with the existing awning of the Local Heritage Listed building (19B). The awning is proposed to extend across the entire width of the building frontage as a consistent horizontal line; unfortunately, in doing so, the impression is conveyed of one large building.

Consider the rich DNA of the Central Fremantle Heritage Area and how local character, culture and history (indigenous and post-colonial), rather than an 'international' approach, should be woven into the development. Consider a multi-layered approach to 'storytelling' that can be educational and enjoyable for local and overseas visitors, such as the integration of public art, wayfinding, and signage throughout the publicly visible and accessible external and internal areas of the proposal.

Recommendations

- 1. Demonstrate the integration of heritage with the new proposal.*
- 2. Provide a Heritage Impact Statement (HIS) prior to the submission of the Development Application.*
- 3. Maintain the character of Essex Street by respecting the existing fine grain and rhythm of the lot pattern and buildings; in particular, review the awning design.*
- 4. Consider the rich DNA of the Central Fremantle Heritage Area and how local character, culture and history should be woven into the development.*

Principle 2 Landscape quality

- a) Consider the opportunity to provide more landscape and natural amenity (daylight, ventilation) into the central communal "Main Street" on the first floor and the large communal areas at ground level.*

Recommendations

- 1. Consider opportunities to provide more landscape and natural amenity into the communal areas.*

Principle 3 Built form and scale

- a) The DAC supports the bulk, height and scale of the new 4-storey accommodation block.*

Principle 4 Functionality and build quality

- a) *Consider the technical aspects of design at this early stage, including safety and escape for visitors in the event of a fire.*

Recommendations

1. *Consider the technical aspects of design at this early stage.*

Principle 5 Sustainability

- a) *The DAC strongly supports the adaptive re-use of the two buildings for this new proposed hotel, however, consider long term sustainability and the ability to modify the proposal for other different uses in the future (noting, however, comments in Principle 1 about impact of changes on the integrity of the heritage building).*

Recommendations

1. *Consider long term sustainability and the ability to modify the proposal for other different uses in the future.*

Principle 6 Amenity

- a) *The DAC supports the clustering of the hotel rooms into legible “Houses” accessed off the central “Main Street”.*
- b) *In view of the number of visitors accommodated in the main part of the hotel, consider the adequacy of the communal restroom ‘block’ and if re-distribution of the block’s showers and toilets into smaller clusters (co-located with the “Houses”) could improve accessibility.*
- c) *Generally, consider improvements to the access of natural amenity into the rooms, particularly where between 3 to 8 visitors are accommodated together. Good levels of natural ventilation and daylight are critical for visitors to be comfortable and if staying in the hotel for longer than a few days.*
- d) *Consider improving the 4-storey block’s rear staircase for the access of natural daylight and views out.*
- e) *Consider opportunities to improve the current narrow areas of external communal open space (at both levels) at the building’s interface with the street.*
- f) *Consider improving the constrained front lobby area in view of the potential conflicts between visitors entering the front door, waiting outside lift 1, using stair 1, and squeezing through the narrow gap to the Bar.*
- g) *Consider relocating the bike store from the Back of House area to a more prominent and accessible part of the Hotel. Bike use is part of the Fremantle character and should be visible and celebrated.*

Recommendations

1. *Consider the adequacy of the communal restroom ‘block’ and possible co-location of facilities with the “Houses”.*
2. *Consider improvements to the access of natural amenity into the rooms.*

3. *Consider improving the 4-storey block's rear staircase for the access of natural daylight and views out.*
4. *Consider opportunities to improve the current narrow areas of external communal open space at both levels.*
5. *Consider improving the constrained front lobby area.*
6. *Consider relocating the bike store from the Back of House area to a more prominent and accessible part of the Hotel.*

Principle 7 Legibility

- a) *The layout of different spaces and circulation throughout the building is generally well considered and legible.*

Principle 8 Safety

- a) *The proposal is for 24-hour use and includes passive surveillance opportunities from openings to help enhance the safety of Essex Street.*

Principle 9 Community

- a) *The DAC supports the tourism use and potential for hotel guests to socialise with the community in the Fremantle City Centre.*

Principle 10 Aesthetics

- a) *Provide further detail on the materiality of the new 4-storey addition.*

Concluding Remarks

While the DAC provides its initial support for this generally well-considered tourism development proposal in the Fremantle City Centre, the DAC did identify several areas for further consideration:

- *Heritage: Demonstrate an understanding of the integration of heritage with the proposal; provide a comprehensive Heritage Impact Statement with key street views; and review current design elements that are affecting the integrity of the Essex Street character.*
- *Integrate local Fremantle character, culture and history into the development for a strong sense of place.*
- *Provide more landscape and natural amenity into the communal areas.*
- *Review the technical aspects of design at this early stage.*
- *Consider long term sustainability and the ability to modify the proposal for other different uses in the future (noting the importance of maintaining the integrity of the heritage building).*
- *Amenity: Suggested improvements to the provision and location of restroom facilities; more natural amenity into guest rooms and the rear staircase; design of the constrained external communal open spaces at both levels and the front lobby; and a more prominent and accessible location for the bike store.*
- *Provide further detail on the materiality of the new 4-storey addition.*

As commented above, the DAC supports in principle the four storey addition to the subject site as well as the internal fit out of the existing heritage building. It is considered that the proposal is of a quality design and suitably addresses the design principles of SPP7.0 (Design of the Built Environment) subject to additional recommendations. These recommendations are addressed below.

- The applicant has provided a comprehensive Heritage Impact Statement which is included in the additional information.
- The proposed development will maintain the original heritage fabric of the original north building. The heritage facade of the north building will be retained and conserved with only minor 'make good' works proposed.
- The building will remain largely in its current form with the proposal involving the adaptive reuse of the envelope of both buildings on the site.
- The publicly accessible uses will continue on the ground floor of both buildings which will conserve the building's community associations, sense of place and social significance in the Fremantle context.
- Additional clarity on the materiality of the rear building has been included on the development plans. A condition is recommended that final details of the external materials, colours and finishes are to be submitted and approved by the City.
- Minor alterations and reconfiguration to the ground floor lobby/kitchen/dining areas have been made however, the proposed layout of the development remains largely unchanged. A condition of approval is recommended regarding the materiality of the development to be reviewed by DAC.

Planning Assessment:

The proposal has been assessed against all the relevant legislative requirements of the Scheme, State and Local Planning Policies, and outlined in the Legislation and Policy section of this report. The following matters have been identified as key considerations for the determination of this application:

- Land use
- Parking
- Building height
- Heritage
- Built form

Land Use

Table 2 – Land Use Permissibility

Provision	Proposal	Zone	Permissibility
Local Planning Scheme 4 clause 3.3 - Zoning Table	Tourist Development	City Centre	D
As above	Small Bar	City Centre	Exempt A use (refer note 1)

Note 1: Local Planning Policy 1.7 exempts a Small Bar land use in the City Centre zone where the tenancy is not located on Market Street or High Street west of Walyalup Koort. The Small Bar is considered exempt and is not subject to further assessment.

A Tourist Development under Schedule 1 of LPS4 means –

- a building, or a group of buildings forming a complex, other than a bed or breakfast, a caravan park or short stay dwelling, used to provide –*
- (a) short term accommodation for guests; and*
- (b) onsite facilities for the use of guests; and*
- (c) facilities for the management of the development;*

A Tourist Development is a 'D' use in the City Centre zone which means that the use is not permitted unless the Council has exercised its discretion by granting planning approval. In considering a 'D' land use, the Council will have regard to the matters set out in clause 67 of the Regulations, Schedule 2. In this regard the following matters have been considered:

- (a) the aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area;*
- (c) any approved State planning policy*
- (g) any local planning policy for the Scheme area;*
- (k) the built heritage conservation of any place that is of cultural significance;*
- (l) the effect of the proposal on the cultural heritage significance of the area in which the development is located;*
- (m) the compatibility of the development with its setting, including –*
 - (i) the compatibility of the development with the desired future character of its setting; and*
 - (ii) the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development;*
- (n) the amenity of the locality including the following –*
 - (i) environmental impacts of the development;*
 - (ii) the character of the locality;*
 - (iii) social impacts of the development;'*
- (s) the adequacy of –*
 - (i) the proposed means of access to and egress from the site; and*
 - (ii) arrangements for the loading, unloading, manoeuvring and parking of vehicles;*
- (t) the amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system in the locality and the probable effect on traffic flow and safety;*
- (w) the history of the site where the development is to be located;*
- (y) any submissions received on the application;*

For the purpose of assessing matter (a) above, the objectives of the City Centre zone are as follows:

Development within the city centre zone shall –

- (i) provide for a full range of shopping, office, administrative, social, recreation, entertainment and community services, consistent with the region-serving role of the centre and including residential uses, and*
- (ii) comply with the objectives of local planning area 1 of schedule 7,*

- (iii) *conserve places of heritage significance the subject of or affected by development.*

The proposed development is considered to be consistent with the Regulations and zone objectives for the following reasons:

- The proposal is for a mixed use development which will maintain the existing small bar and public amusement land uses while incorporating a new short stay accommodation land use and additional small bar into the subject site.
- The built heritage of the site has been carefully considered throughout the assessment of the application which has been supported by the Heritage Council. The alterations to the heritage listed building will allow for it's continued use and vitality into the future.
- The rear addition satisfies the building height requirements of Local Planning Area 1 of Schedule 7. The rear addition is setback sufficiently from Essex street such that it will not be visible resulting in minimal impact to the character and amenity of the streetscape. The rear addition is setback behind existing residential and commercial tenancies located on Norfolk Street which will effectively minimise its visibility and appearance of bulk and/or scale.
- There is considered to be sufficient separation from adjoining residential and commercial tenancies to reduce any detrimental impact on the amenity of these properties.
- Any impact from traffic, waste disposal and service vehicles is considered to be suitably mitigated via the provided Traffic Impact Statement and Waste Management Plan.

Parking

The car parking ratio applied by LPS4 for the proposed Tourist development land use is as follows:

Table 3 – Parking Requirement

Provision	Requirement	Proposal	Assessment
Car parking	Tourist development 1: 4 units or* 1: 4 bedrooms	1	19 bay shortfall
	Required: 20 bays		
	Small bar	Nil	N/A – Refer Note 1
	Existing small bar	1	N/A – Refer Note 1
	Existing public amusement 1: 10 seats or 1: 10m ² of floor area dedicated to amusement facilities whichever is the greater 131m ² floor area Required: 13 bays	1 (consistent with development approval DA0065/23)	12 bay shortfall – Refer Note 2

Delivery bays	2 bays (1 each for tourist development and public amusement land uses)	1 bay shared	1 bay shortfall
Total	35 bays	4 bays	32 bay shortfall

Note 1: A Small Bar land use is exempt from requiring development approval under Local Planning Policy 1.7 'Development Exempt from Approval Under Local Planning Scheme No. 4' and is therefore not required to comply with minimum on-site parking requirements.

Note 2: One (1) car bay has been provided for the public amusement land use which is consistent with the parking provided (one (1) bay) under development approval DA0065/23.

Clause 4.7.3.1 of LPS4 states that Council may –

- (a) *Subject to the requirements of Schedule 7*, waive or reduce the standard parking requirement specified in Table 2 subject to the applicant satisfactorily justifying a reduction due to one or more of the following —*
- (i) *the availability of car parking in the locality including street parking,*
 - (ii) *the availability of public transport in the locality,*
 - (iii) *any reduction in car parking demand due to the sharing of car spaces by multiple uses, either because of variation of car parking demand over time or because of the efficiencies gained from the consolidation of shared car parking spaces,*
 - (iv) *any car parking deficiency or surplus associated with the existing use of the land,*
 - (v) *legal arrangements have been made in accordance with clause 4.7.5 for the parking or shared use of parking areas which are in the opinion of the Council satisfactory,*
 - (vi) *any credit which should be allowed for a car parking demand deemed to have been provided in associated with a use that existed before the change of parking requirement,*
 - (vii) *the proposal involves the restoration of a heritage building or retention of a tree or trees worthy of preservation,*
 - (viii) *any other relevant considerations.*

*Note: *In some sub areas identified in Schedule 7 reduction of parking bays is not permitted. The requirements of Schedule 7 prevail over this clause.*

The reduction in on-site car parking is considered to meet the requirements of Clause 4.7.3.1 of the Local Planning Scheme No.4 for the following reasons:

- The subject site is located within a 250-metre buffer of a high frequency bus route (bus stops located within 100m of the site) and is within an 800-metre buffer of the Fremantle Train Station entrance.
- There are public car parking lots with unrestricted time limits located on Marine Terrace and at the Fishing Boat Harbour, within 250m and 400m respectively, of the subject site. Additionally, there are a number of private car parking facilities that offer all day parking in the area, which may be suitable for a short term accommodation user if they have a vehicle.
- The City's Integrated Transport Strategy identifies the future approach for parking within the City of Fremantle. The aim is to provide new parking

facilities located on the periphery of the central city. This approach will minimise the impact of vehicles on the pedestrian priority city centre. The proposed development is consistent with this strategy as it will not encourage the use of vehicles to drive into the central city area.

The bicycle parking required by LPS4 for the proposed Tourist development land use is as follows:

Table 4 – Bicycle parking requirement

Provision	Requirement	Proposal	Assessment
Bicycle parking	Tourist development Class 2: 1 per 4 units Required: 20	21 (plus 3 in Essex Street verge)	Complies

Building Height

Schedule 7 of LPS4 provides the building height requirements for the subject site within Local Planning Area 1, sub-area 1.3.1 which states as follows:

Despite the general height requirements outlined in 1.1 above, building height shall be limited to a maximum height of three storeys (maximum external wall height of 11 metres as measured from ground level with a maximum roof plain pitch of 33 degrees).*

Council may consent to an additional storey subject to —

- (a) Where a site meets any of the requirements of Clause 1A(a)-(e) of the deemed provisions, the upper level being sufficiently setback from the street so as to not be visible from the street(s) adjoining the subject site.*
- (b) maximum external wall height of 14* metres, and*
- (c) compliance with clause 1.2. above.*

**Inclusive of roof parapet and spacing between floors.*

In granting consent to the maximum height prescribed, Council shall be satisfied in regard to all of the following—

- (a) that the proposal is consistent with predominant, height patterns of adjoining properties and the locality generally,*
- (b) the proposal would not be detrimental to the amenity of adjoining properties or the locality,*
- (c) the proposal would be consistent, if applicable, with conservation objectives for the site and locality generally, and*
- (d) any other relevant matter outlined in Council's local planning policies.*

Council may impose a lesser height in the event that the proposal does not satisfy any of the above requirements.

The proposed development features a maximum of four (4) storeys with a total building height of 13.8m as detailed in Table 4 below.

Table 4 – Building Height

Provision	Permitted – General Height	Permitted - Additional Height	Proposal	Assessment
Building height	3 storeys 11m external wall height	4 storeys/ 14m external wall height/ upper level not visible from the street	4 storeys 13.8m external wall height Not visible from Essex Street	Compliant

The height of the proposed building on the subject site is considered acceptable and meets the permitted additional height provisions under Schedule 7 of LPS4, for the following reasons:

- The proposed rear addition is setback approximately 40m from the Essex Street lot boundary. The setback of this addition, behind the existing building, will minimise its visibility and effectively reduce any potential detrimental impact on the streetscape as illustrated by Figure 4 below.
- The rear addition will be separated from the residential dwellings of 24 and 26 Norfolk Street by a 3m right of way as illustrated by Figure 5 below. Regardless of this, the addition will oppose the parking area/carport of 24 Norfolk Street only and will not immediately oppose 26 Norfolk Street.
- 22 Norfolk Street is utilised for commercial purposes (restaurant). Regardless of this, the rear addition will not directly oppose any customer seating area or other areas considered important to providing customer amenity (there is a blank boundary wall abutting the rear boundary of the subject site)
- The rear addition will feature a 3m setback from the lot boundary of 15-17 Essex Street as illustrated by Figure 5 below. In addition, 15-17 Essex Street will not immediately oppose the rear addition (the adjoining lot is located mostly adjacent the existing building to be retained) and is not located immediately to the south the rear addition (the adjoining site will therefore not experience any significant overshadowing).
- As illustrated by figures 6-12 below, the rear addition is suitably located to minimise impact on the streetscapes of the surrounding area.

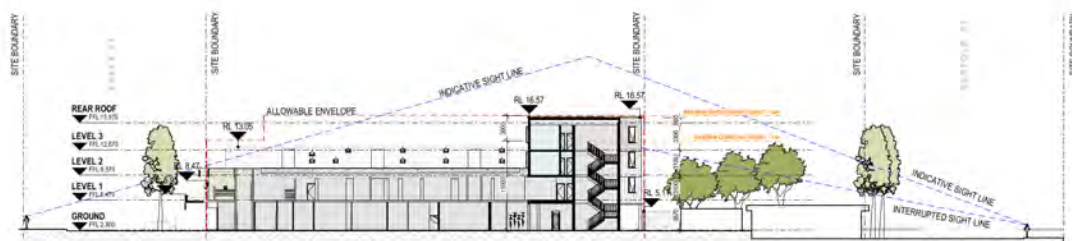


Figure 4 – Line of Sight diagram from Essex and Norfolk Street.



Figure 5 – Location of the four storey addition highlighted in yellow.

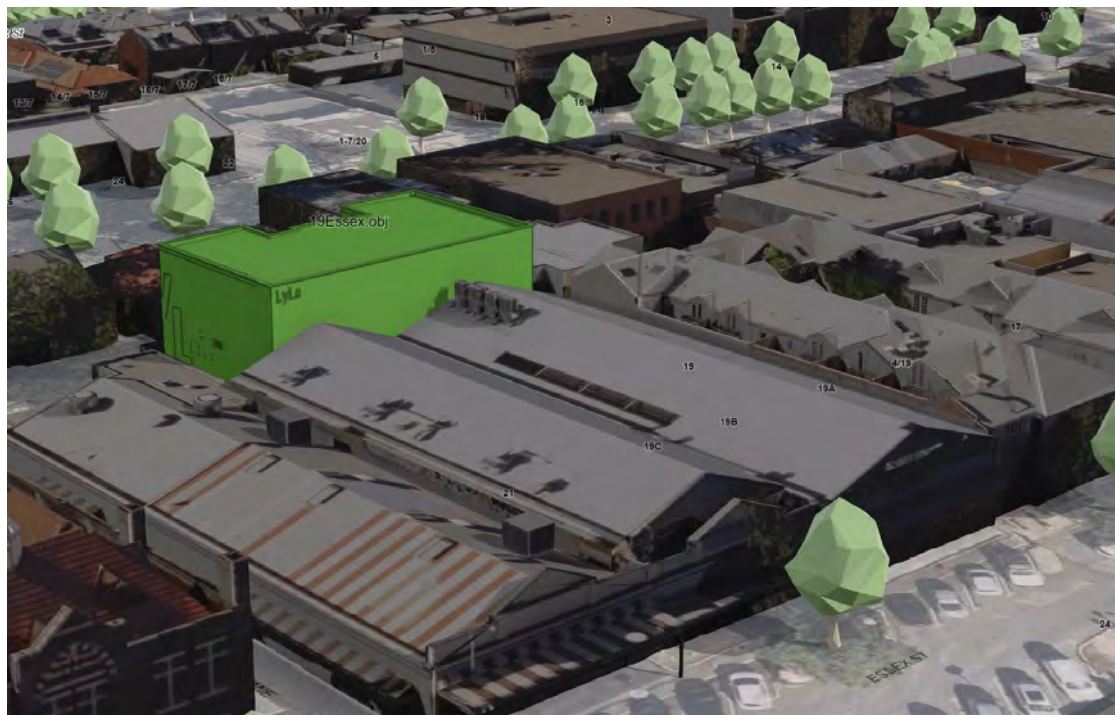


Figure 6 – 3D representation of the four storey addition looking south.

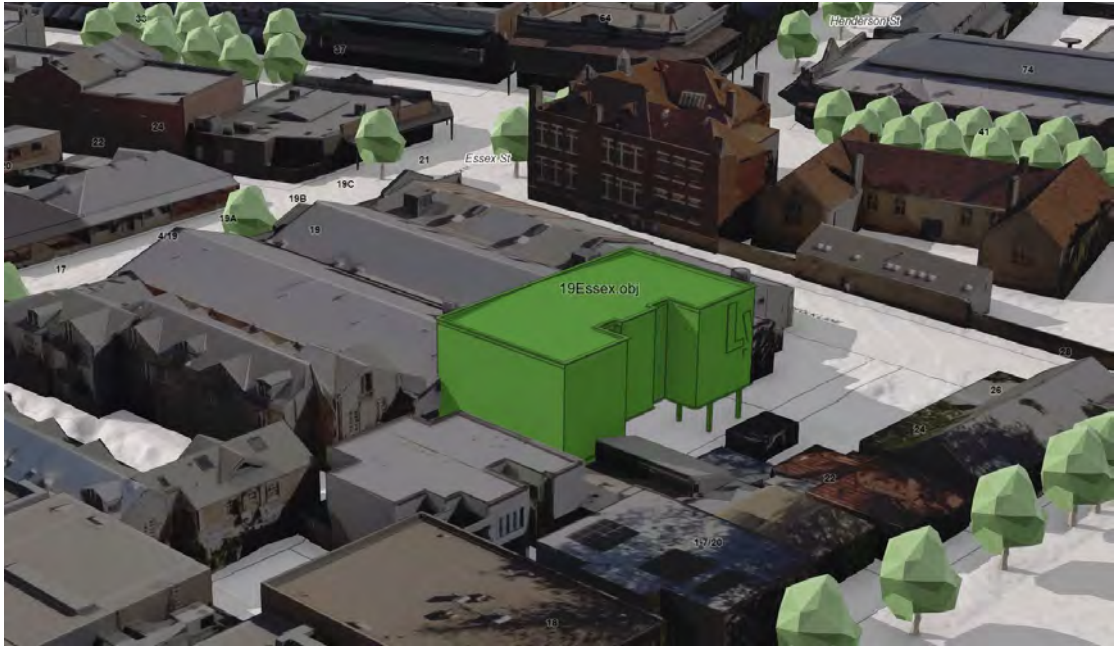


Figure 7 – 3D representation of the four storey addition looking north.

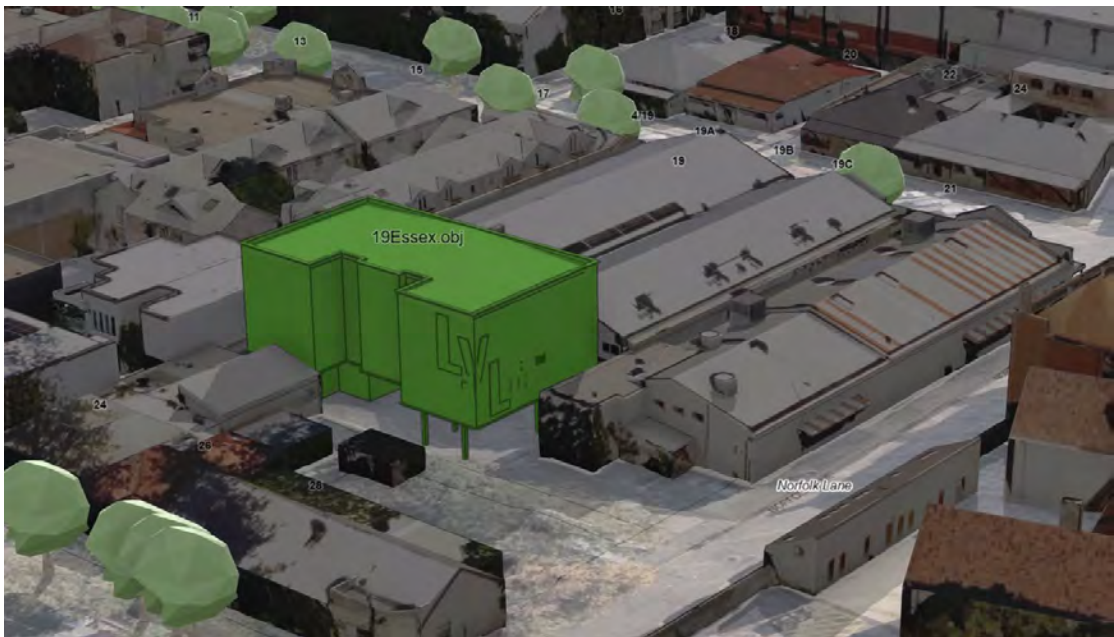


Figure 8 – 3D representation of the four storey addition looking west.



Figure 9 – 3D representation of the four storey addition looking east.



Figure 10 – 3D representation of the street frontage of 19 Essex Street. Note that the four storey addition will not be visible from the streetscape.

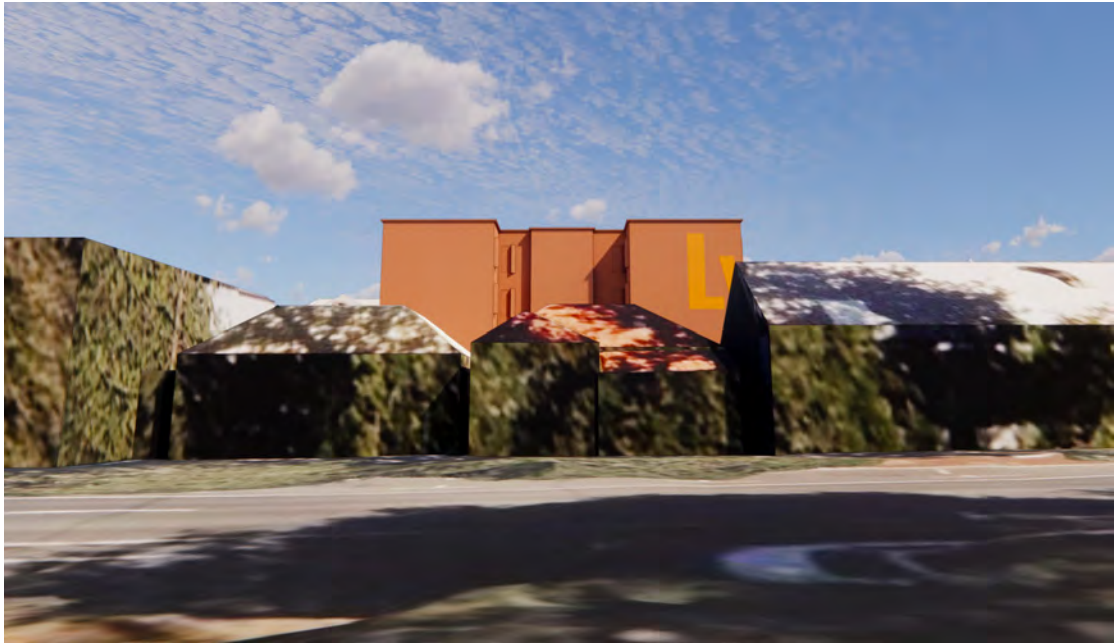


Figure 11 – 3D representation of the four storey addition situated behind the dwellings, restaurant and offices located on Norfolk Street.



Figure 12 – 3D representation of the four storey addition situated behind the dwellings, restaurant and offices located on Norfolk Street.

Local Planning Policy

Local Planning Policy 1.6 – Heritage Assessment and Protection

LPP1.6 requires a Heritage Impact Statement to be prepared by a qualified heritage professional for any proposal that may impact any heritage place and/or area listed on the State heritage register or the City's Municipal Heritage Inventory. The purpose of a Heritage Impact Statement is to consider the impact of a specific proposal (e.g. development) on the cultural heritage significance of a heritage place or area. Where a proposal is accompanied by a proponent-commissioned Heritage Assessment or

Heritage Impact Assessment, the City will undertake its own assessment but may draw on information submitted.

In accordance with the above, the applicant has provided a Heritage Impact Statement which is included in the additional information. The City conducted its own assessment which has been summarised in the City of Fremantle Heritage Comment section above.

Local Planning Policy 2.19 - Contributions For Public Art and/or Heritage Works

As per LPP2.19, the subject site falls within the 'City Centre and Surrounds' contribution area which requires the contribution of a monetary amount equal in value to one per cent of the estimated total development cost, as indicated on the Form of Application for Planning Approval, for the development of public art works and/or heritage works to enhance the public realm. The proposed development is not exempt from the contribution requirement as it involves a development greater than 1000m² of gross lettable area. The contribution requirement is recommended as a condition of approval.

Local Planning Policy 3.6 – Heritage Areas

The following table outlines the proposal against the relevant provisions of LPP 3.6 Heritage Areas.

3.1 Conservation	
3.1.1 Intent	Officer Comment
<p>Conservation is the process of managing change to a place in such a way that its cultural heritage significance is retained. This involves routine care and maintenance but may also require more significant adaptation at some stages of a building's life. Conservation means all the processes of looking after a place to retain its cultural significance and can include maintenance, preservation, restoration, reconstruction and adaptation. It is recognised that adaptation of heritage buildings for appropriate new uses is an important way of ensuring their continued viability and safeguarding their long-term future. However, the need for change should always respect the underlying requirement to conserve heritage significance.</p>	<p>The north half of the existing building is part of Former Mills and Co Building which was constructed c. 1907. The proposed development has been designed to minimise impact to this original north building. Most of the works (internal fit out, removal of internal walls etc.) will occur to the more recently constructed south building.</p> <p>The adaptation of the building to facilitate the Tourist development will allow for the continued viability of the building and safeguard its long-term future.</p>
3.1.2 Design Guidance	Officer Comment
<p>i. Conservation of a place should identify and take into consideration all aspects of cultural and natural significance without unwarranted emphasis on any one value at the expense of others. Conservation work should ensure that the heritage fabric and other values are well maintained.</p> <p>ii. Traditional techniques and materials are preferred for the conservation of significant fabric. In some circumstances modern techniques and materials which</p>	<p>The south building is a two-storey addition constructed in 2012 to replace an earlier building from 1941. This part of the building has no heritage significance but is sympathetic with the character of streetscape.</p>

<p>offer substantial conservation benefits may be appropriate on the advice of the City.</p> <p>iii. Preservation is appropriate where the existing fabric or its condition constitutes evidence of cultural heritage significance, or where insufficient evidence or investigation has been carried out to allow an informed approach to restoration and reconstruction.</p> <p>iv. Restoration is appropriate only if there is sufficient evidence of an earlier state of the fabric.</p> <p>v. Reconstruction is appropriate only where a place is incomplete through damage or alteration, and only where there is sufficient evidence to reproduce an earlier state of the fabric. It should be identifiable on close inspection or through additional interpretation.</p> <p>vi. Whilst new work should be readily identifiable, it should also:</p> <ul style="list-style-type: none"> • Not adversely affect the setting of the place. • Have minimal impact on the cultural significance of the place. • Not distort or obscure the cultural significance of the place, or detract from its interpretation and appreciation. • Respect and have minimal impact on the cultural significance of the place. <p>vii. Brick and limestone walls constructed prior to 1950 should not be painted with acrylic paints or rendered with a cement render because this can damage the fabric and contribute to rising damp.</p> <p>viii. Building maintenance should seek to avoid the removal of, or damage to, the existing fabric of the building or the use of new materials.</p> <p>ix. Repairs, including replacing missing or deteriorated fabric with 'like for like' fabric should not involve damage to the significant fabric of the building.</p> <p>x. Replacement of utility services should use existing routes or voids that do not involve the removal of, or damage to, the fabric of the building.</p> <p>xi. Excavation for the purpose of exposing, inspecting, maintaining or replacing utility services should not affect archaeological remains.</p> <p>xii. Removal of significant landscaping should be avoided where feasible.</p>	<p>The works proposed to the north building will have only a minor impact its heritage significance.</p> <p>The façade and the shopfronts of both buildings will undergo some remediation and repainting which will not adversely affect any significant heritage fabric.</p> <p>There is no proposed reconstruction of original heritage elements.</p> <p>A condition is recommended which requires methodology for stonework repair to be provided as part of the building permit application. Repairs will be required to match the original stonework, pointing, coursing pattern and mortar composition and colour. Cement mortar or sealers will not be permitted to be used.</p>
3.3 Change of Use	
3.3.1 Intent	Officer Comment
<p>The use of a heritage place, or a group of places within a heritage area is often intrinsically linked to its historical evolution and heritage significance. Generally, the retention of original or long-associated uses is encouraged, however it is acknowledged that,</p>	<p>The change of use to a Tourist development is considered appropriate to ensure the ongoing viability of the building and site. The alterations and additions required to</p>

<p>as places evolve over time, the use of heritage places may also need to change. An empty, disused heritage place is more likely to deteriorate at a quicker rate, be subject to vandalism and lose its relevance to the local community. As such, one of the most effective ways to retain the heritage significance of a place or a heritage area is to ensure it has an ongoing, compatible and viable use. Introducing new building services as part of a use change can potentially disturb large portions of significant fabric and it is important that these are carefully considered when adapting places of heritage significance for new purposes.</p>	<p>facilitate this change of use have been appropriately designed to minimise the impact to original heritage fabric of the building. The investment into the building and the remediation and repair work will safeguard the buildings future and ensure its ongoing viability.</p>
3.3.2 Design Guidance	Officer Comment
<p>i. Where the use of the place is of cultural heritage significance (and consistent with the zoning of the land and other planning considerations), the continuation or reinstatement of this use is encouraged, and is the preferred form of conservation.</p> <p>ii. Where continuation of a culturally significant use is not feasible, a compatible use should be sought that minimises alteration to the place, setting and streetscape.</p> <p>iii. Interpretation of earlier significant uses may be required if those uses were significant to the heritage area.</p> <p>iv. Where a place has historically been accessible to the public, any change of use should consider the continuation of public accessibility in some form or publicly accessible interpretation.</p> <p>v. Should new services be required for a change of use for a contributory place, conceptual details of new services should be submitted at development application stage. New building services should be designed and integrated to minimise any impact on the significant fabric.</p>	<p>The reinstatement of the original use of the north building as a factory is not considered to be appropriate for the central city location. The proposed change of use is considered compatible with the city centre. The building was not historically open for public use and it is not considered that the various uses which have occurred on the site are significant to warrant ongoing interpretation.</p> <p>The alterations required to facilitate the change in use have been appropriately designed to minimise impact to the heritage listed buildings.</p>
3.4 Demolition	
3.4.1 Intent	Officer Comments
<p>Demolition is a permanent change that cannot be reversed: even removal of places with lower levels of individual heritage significance can cumulatively undermine the significance of a heritage area. Demolition of any building or structure on any site located in a Heritage Area requires development approval under the Local Planning Scheme. In considering a proposal for demolition on any site in a Heritage Area where the mapping of Contributory Places has not been completed and/or is incomplete, the Council shall determine whether any fabric located thereon qualifies as a Contributory Place and applications will be assessed against Clause 4.14 of</p>	<p>The proposed development involves removal of internal building fabric of the buildings. These works have been largely contained to the recently constructed south building. The heritage significant north building will experience minor internal works required to facilitate the proposed accommodation.</p>

Local Planning Scheme No. 4 with the following providing additional guidance.	
3.6.2 Design Guidance	Officer Comments
<p>i. Demolition of a contributory place or removal of significant fabric within a heritage area is contrary to the objectives of this policy, and will generally not be supported.</p> <ul style="list-style-type: none"> • Demolition approval will not be considered on the grounds of economic or other perceived gain for the redevelopment of the land. • Condition of the place is not necessarily an acceptable reason for demolition. <p>ii. When considering partial demolition, the original / early portion of the building should be retained and conserved: demolition of elements of cultural heritage significance will generally not be supported.</p> <p>iii. The removal and replacement of asbestos or other hazardous materials from a place located within a heritage area is supported if the removal involves the immediate replacement with a matching (but non-hazardous) material (e.g. flat asbestos wall sheeting with joint cover battens is replaced with flat fibre cement sheeting with joint cover battens in the same configuration – a replacement with timber weatherboards or fibre cement weatherboards would generally not be supported).</p> <p>iv. Where demolition of a heritage protected place is approved, an archival record prepared in a format approved by the City may be required as a condition of planning approval.</p>	<p>The proposed development does not involve any complete demolition of buildings on the site however, there will be internal removal of walls and partitions. The significant alterations have been confined to the recently constructed south building with only minimal impact to the heritage significant north building. The internal works will have no significant impact on the heritage significance of the Central Fremantle Heritage Area.</p> <p>Regardless, a condition is recommended that the works shall be undertaken in a manner which does not irreparably damage any original or significant fabric of the building which is not subject to the development hereby approved.</p>
3.6 Infill development (new buildings)	
3.6.1 Intent	Officer Comments
<p>New buildings within a heritage area should respect and complement the heritage significance of the area. A respectful design approach gives special consideration to the siting, scale, architectural style and form, materials and finishes of the proposed development in relation to its neighbours, without copying historic detailing or decoration. New infill buildings should respond sympathetically to the heritage values of the heritage area as a whole, and also to that part of the heritage area in the vicinity of the proposed development. Imaginative, well designed and harmonious construction is encouraged. Professional architectural services can be of great assistance in formulating appropriate designs.</p>	<p>The proposed four storey addition is located a significant distance (approximately 40m) from Essex Street. It's visibility from Essex Street will be minimal and will have no impact on the heritage significance of the area. The rear addition will be located approximately 25m from Norfolk Street and is set behind the existing buildings on the adjoining lots to the south east.</p> <p>The rear additions will have no impact on the pattern of development which defines the surrounding area.</p> <p>The proposed rear addition is not attempting to mimic or copy any historic detailing or decoration.</p>

	A contemporary building located at the rear of a site behind existing buildings is therefore considered to be appropriate in the context of the area.
3.6.2 Design guidance	Officer Comments
<p><i>Siting and Scale</i></p> <p>i. New infill development within a heritage area should:</p> <p>a) Maintain a setting that is consistent with the original streetscape, including front and side setback patterns.</p> <p>b) Have a consistent bulk and scale in relation to the original street pattern. E.g. If the original street pattern is single storey then the new infill development should also be (or present as) single storey (at least to the front section of the lot).</p> <p>c) Have a plate height consistent with the original street pattern. New developments often propose a lower plate height than the earlier and original buildings. To ensure a consistency of scale the plate height is an important element to ensure it is consistent with the original street pattern.</p> <p>ii. New Infill development to secondary streets will be assessed on individual circumstances and merit. Issues to consider include:</p> <p>a) Prevailing streetscape and setbacks of the side street</p> <p>b) Avoiding a continuous wall and providing articulation of walls to a secondary street.</p> <p>c) Avoiding a two-storey height wall to the side street, unless the prevailing streetscape is predominantly two-storey.</p> <p>iii. Street setbacks deemed to comply with the above are specified in Schedule 1 for some areas.</p>	<p>The four storey rear addition is setback sufficiently from the streetscape such that it will have minimal impact on the setting of the street.</p> <p>The appearance of the front section of the lot will remain as per the existing buildings on the site. It is considered that the siting and scale of the rear addition is appropriate and will have only a minimal impact on the prevailing streetscape.</p>
<p><i>Building Form</i></p> <p>The form of the building is its overall shape, size and the general arrangement of its main parts.</p> <p>i. New infill building within a heritage area should respect and harmonise with and be sympathetic to the predominant form of the prevailing streetscape without mimicking heritage detailing.</p> <p>ii. Where a building form is highly repetitive, significant departures in form will appear at variance to the streetscape and should not be introduced.</p> <p>iii. The treatment of new infill buildings in terms of the roof form, proportions, materials, number, size and orientation of openings, ratio of window to wall etc. should relate to that of its neighbours.</p>	<p>The four storey rear addition is not considered to be attempting to mimic or recreate any original heritage detail. The rear addition is setback behind the existing heritage listed buildings such that there will be minimal impact on the building form of the heritage area. The appearance of the front section of the lot will remain as per the existing buildings on the site. The contemporary rear addition is consistent with the scale of existing nearby buildings.</p>

<p>iv. Symmetry or asymmetry of facades in the prevailing streetscape is an element of form to be kept consistent.</p> <p>v. Contemporary building designs should respond to, and interpret, the scale, articulation and detail of the existing nearby buildings in a modern, innovative and sympathetic way.</p>	
<p><i>Materials, Colours and Detailing</i></p> <p>i. Materials and level of detailing should reflect / interpret the predominant materials and detailing of the original prevailing streetscape and not visually dominate the streetscape or adjacent heritage buildings.</p> <p>ii. Whilst the basic form, scale and structure of new development should be consistent with the character of the area, new buildings should not seek to emulate heritage detailing to any great extent: 'Faux' or 'mock' heritage detracts from an understanding and appreciation of the original building and will not be supported. New development should blend in with the streetscape but be discernible as new when looked at more closely.</p> <p>iii. Use of original or traditional colours is encouraged. Glossy materials or finishes should be avoided unless a historical precedent for their use can be demonstrated.</p>	<p>The four storey rear addition has been designed to blend in with the materiality and colour palettes of the surrounding area. The addition is not attempting to emulate surrounding heritage buildings rather, it is proposing a contemporary building that is clearly discernible from the original buildings on the site. It is considered that the rear addition is consistent in bulk and scale with various buildings in the surrounding area and blends in with the varying bulk and scale of character the Central Fremantle Heritage Area.</p>
<p><i>Other Elements</i></p> <p><i>Roofs</i></p> <p>i. Traditionally roof lines are a predominant element of the streetscape. All new infill development shall respond to and reinforce the existing characteristics of the prevailing streetscape regarding plate and wall heights, roof form, ridge lines, parapet lines, roof slopes and eaves overhangs.</p> <p>ii. Roof forms that interpret the predominant roof forms of the prevailing streetscape may be considered.</p> <p><i>Verandahs / Porches / Awnings</i></p> <p>i. Verandahs, porches and awnings were often an important element of streetscapes. Inclusion of verandahs, porches and awnings appropriate to the streetscape are encouraged without too precisely mimicking the style of the original character-building elements or heritage detailing.</p> <p><i>Doors and Windows</i></p> <p>i. All windows and door openings visible from the street should have a vertical emphasis, which means they should be taller and narrower in appearance unless there is a predominance in the prevailing streetscape of larger, interwar and later windows.</p>	<p>The traditional roof lines of the heritage listed buildings at the front of the site will be maintained. The rear addition is setback sufficiently behind existing buildings such that it will have no significant impact on the roof form character of the area.</p> <p>The awnings of the heritage listed buildings will be maintained.</p> <p>The windows of the rear addition, while only minimally visible from the street, will have a vertical emphasis.</p>

<p>ii. Front doors should generally address the street and should be centrally located in the front façade of the new infill building unless there is a different original pattern in the prevailing streetscape.</p>	
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State Planning Policy

State Planning Policy 3.5 - Historic Heritage Conservation

Clause 6.6 Development Control Principles of State Planning Policy 3.5 – Historic Heritage Conservation (SPP3.5) includes provisions for development within heritage areas. The proposed alterations of the existing building and the construction of a four storey rear addition is consistent with clause 6.6 of SPP3.5 for the following reasons:

- The internal alterations have been designed to minimise the impact to the original north building. Any significant removal of internal fabric will be confined to the south building which was constructed in 2012.
- The proposed rear addition is situated appropriately at the rear of the site such that it will not result in any significant detracting from the character and amenity of the established streetscape.
- There are no significant alterations to the external façade of the buildings. Restoration works will be conducted on the external façade of the original north building to ensure its ongoing vitality.
- The architectural design of the proposed rear addition is not attempting to mimic any historical heritage features of the surrounding area. It is considered to be a quality design and will not detract from the heritage character of the area.

State Planning Policy 7.0 - Design of the Built Environment

An assessment has been conducted against the design principles of SPP7.0 which are Context and character, Landscape quality, built form and scale, Functionality and build quality Sustainability, Amenity, Legibility, Safety, Community, and Aesthetics. Refer comments below:

- The proposal has been carefully considered against the distinctive characteristics of the area. It is considered that the design is a quality architectural design and will not detract from the heritage significant or the character and amenity of the surrounding area.
- The proposed addition does not attempt to imitate the existing building, rather it offers a distinctive change between the heritage listed building and the new addition.
- It is considered that the existing heritage listed building will remain as the key feature on the subject site when viewed from street level, with the rear addition being subservient to this existing building.

- The subject site features limited opportunity for landscaping due to the existing building (to be retained) covering the majority of the site area.
- The addition will not be immediately visible and will maintain the views along the streetscape.
- The addition is suitably setback behind the existing building such that any impact from bulk and scale will be minimal.
- The mix of land uses on the site (short stay accommodation, small bar) are considered compatible and will assist in contributing to the vitality of the building and area.
- The proposed development will provide opportunity for additional people to stay in the city centre.
- The overall development is considered logical and appropriate and is of a quality architectural design.

Conclusion:

Approval is sought for a Tourist development at No. 19 (Lot 8) Essex Street, Fremantle which includes alterations to the existing building as well as a four storey addition of the existing buildings. The Mills & Co Building is a City of Fremantle heritage listed building and is adjacent State heritage listed sites. Conditions have been recommended to ensure that any potential impact to the adjoining listed sites are suitably addressed. The heritage aspect of this proposal has been a key consideration in the assessment of the application. After completing a comprehensive assessment on the various aspects of the proposal as well as receiving comments and advice from the Heritage Council and Design Advisory Committee, the proposal is recommended for conditional approval.

Officer Recommendation

That the Metro Inner DAP resolves to:

1. **Accept** that the DAP Application reference DAP/24/02724 is appropriate for consideration as a Tourist Development land use and compatible with the objectives of the zoning table in accordance with the City of Fremantle Local Planning Scheme No. 4;
2. **Approve** DAP Application reference DAP/24/02724 and accompanying plans (DA00.00, DA00.01, DA01.01, DA01.02, DA01.03, DA01.04, DA01.05, DA01.06, DA01.07, DA02.01, DA02.02, DA02.03, DA02.04, DA02.05, DA02.06, DA03.01, DA03.02, DA03.03, DA03.04, DA04.01 *dated 22 October 2024*) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of the City of Fremantle Local Planning Scheme No. 4, subject to the following conditions:

Conditions

1. Pursuant to clause 26 of the Metropolitan Region Scheme, this approval is deemed to be an approval under clause 24(1) of the Metropolitan Region Scheme.
2. This decision constitutes planning approval only and is valid for a period of 4 years from the date of approval. If the subject development is not substantially

commenced within the specified period, the approval shall lapse and be of no further effect.

3. All storm water discharge shall be contained and disposed of on-site unless otherwise approved by the City of Fremantle.
4. Prior to commencement of works, a program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented to the satisfaction of the City of Fremantle, on advice from the Heritage Council. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer.
5. Prior to lodgement of a Building Permit application for the development hereby approved, a dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional to the satisfaction of the City of Fremantle, on advice from the Heritage Council.
6. Prior to lodgement of a Building Permit application for the development hereby approved, a detailed Mortar Mix Methodology shall be submitted and approved to the satisfaction of the City of Fremantle. Only lime mortars and no cement or impervious materials are to be used. The works shall be undertaken using the approved mortar mix.
7. Prior to lodgement of a Building Permit application for the development hereby approved, final details of the external materials, colours and finishes of the proposed development, including a physical sample board or materials is to be submitted and approved to the satisfaction of the City of Fremantle, on the advice of the City's Design Advisory Committee.
8. Prior to lodgement of a Building Permit application for the development hereby approved, design plans for the location, materials and construction for screening of any proposed external building plant must be submitted to and approved by the City of Fremantle. All piped, ducted and wired services, air conditioners, hot water systems, water storage tanks, service meters, other plant and bin storage areas must be integrated into the design of the building and located to minimise any visual and/or noise impact on the occupants of nearby properties and screened from view from the street.
9. Prior to lodgement of a Building Permit or Demolition Permit application for the development hereby approved, a Construction/Demolition Management Plan shall be submitted and approved, to the satisfaction of the City of Fremantle addressing, but not limited to, the following matters:
 - a) Use of City car parking bays for construction related activities;
 - b) Protection of infrastructure and street trees within the road reserve;
 - c) Security fencing around construction sites;
 - d) Gantries;
 - e) Access to site by construction vehicles;
 - f) Contact details;
 - g) Site offices;
 - h) Noise - Construction work and deliveries;
 - i) Sand drift and dust management;

- j) Waste management;
 - k) Dewatering management plan;
 - l) Traffic management; and
 - m) Works affecting pedestrian areas.
10. Prior to lodgement of a Building Permit application for the development hereby approved, the applicant is to submit, and have approved to the satisfaction of the City of Fremantle, a detailed parking plan design which complies with the Australian Standard AS/NZS 2890 and AS/NZS 1428 including parking bay/s (comprising visitor bays, loading bays, disabled bays, motorcycle / scooter bays), aisle widths, circulation areas, driveway/s and points of ingress and egress, and demonstrates the safety of building users accessing the basement bike store and carparking.
 11. Prior to lodgement of a Building Permit application for the development hereby approved, the design and materials of the development shall adhere to the requirements set out within City of Fremantle policy L.P.P2.3 - Fremantle Port Buffer Area Development Guidelines for properties contained within Area 2. Specifically, the development shall provide the following:
 - a) Glazing to windows and other openings shall be laminated safety glass of minimum thickness of 6mm or "double glazed" utilising laminated or toughened safety glass of a minimum thickness of 3mm.
 - b) Air conditioners shall provide internal centrally located 'shut down' points and associated procedures for emergency use.
 - c) Roof insulation in accordance with the requirements of the Building Codes of Australia.
 12. Prior to lodgement of a Building Permit application for the development hereby approved, details of how the recommendations contained in the Acoustic Report (Ref: 33221-1-24296), prepared by Herring Storer Acoustics, dated 21 August 2024 are to be implemented are to be submitted to the satisfaction of the City of Fremantle.
 13. Prior to lodgement of a Building Permit application for the development hereby approved, a detailed drawing showing how the windows located on the east, south and west elevation of the four storey addition, are to be screened to address overlooking to the adjoining residential properties. Prior to occupation of the development hereby approved, the approved screening method shall be installed and maintained to the satisfaction of the City of Fremantle.
 14. Prior to the lodgement of a Building Permit application for the development hereby approved, the applicant is to submit, and have approved to the satisfaction of the City of Fremantle, an Operational Management Plan addressing the following:
 - Emergency contact details
 - Management and reporting of antisocial behaviour
 - Complaints management procedure
 - Security and access to rooms
 - Guest arrival / check in/out
 - Maximum length of stay
 - Deliveries and servicing

- Maximum occupancy

The Operational Management Plan must be implemented at all times to the satisfaction of the City of Fremantle for the life of the development.

15. Prior to occupation of the development hereby approved, all car parking, bicycle parking, motorcycle/scooter parking and vehicle access and circulation areas shall be installed, maintained and available for car parking/loading, and vehicle access and circulation on an ongoing basis to the satisfaction of the City of Fremantle.
16. Prior to occupation of the development hereby approved, the owner shall contribute a monetary amount equal in value to one percent of the estimated development cost or otherwise approved by the City in accordance with the policy, as indicated on the Form of Application for Planning Approval, to the City of Fremantle for development of public art works and/or heritage works to enhance the public realm in accordance with LPP 2.19: Contributions for Public Art and/or Heritage Works and to the satisfaction of the City of Fremantle. Based on the estimated cost of the development being \$8 million the contribution to be made is \$80,000.
17. The waste management plan, prepared by Talis Consultants, dated 23 October 2024 (WMP24011) must be implemented at all times to the satisfaction of the City of Fremantle.
18. All works indicated on the approved plans, including any footings, shall be wholly located within the cadastral boundaries of the subject site.
19. The works hereby approved shall be undertaken in a manner which does not irreparably damage any original or significant fabric of the building which is not subject to the development hereby approved. Any damage shall be rectified to the satisfaction of City of Fremantle.
20. Where any of the preceding conditions has a time limitation for compliance, if any condition is not met by the time requirement within that condition, then the obligation to comply with the requirements of any such condition (other than the time limitation for compliance specified in that condition), continues whilst the approved development continues.

Advice Notes

1. A Building permit is required for the proposed Building Works. A certified BA1 application form must be submitted and a Certificate of Design Compliance (issued by a Registered Building Surveyor Contractor in the private sector) must be submitted with the BA1.
2. In relation to the Heritage conditions above, should there be any further clarification regarding the requirements of these conditions please contact the City's Heritage Department on 9432 9999 or alternatively planning@fremantle.wa.gov.au, or where relevant the Heritage Council of Western Australia. In addition to this, there are a number of technical advice sheets for conservation of privately-owned heritage buildings on the City's website.

3. Any removal of asbestos is to comply with the following –

Under ten (10) square metres of bonded (non-friable) asbestos can be removed without a license and in accordance with the *Health (Asbestos) Regulations 1992* and the *Environmental Protection (Controlled Waste) Regulations 2001*. Over 10 square metres must be removed by a licensed person or business for asbestos removal. All asbestos removal is to be carried out in accordance with the *Occupational Safety and Health Act 1984* and accompanying regulations and the requirements of the Code of Practice for the Safe Removal of Asbestos 2nd Edition [NOHSC: 2002 (2005)];

Note: Removal of any amount of friable asbestos must be done by a licensed person or business and an application submitted to WorkSafe, Department of Commerce. <http://www.docep.wa.gov.au>

4. If construction works involve the emission of noise above the assigned levels in the *Environmental Protection (Noise) Regulations 1997*, they should only occur on Monday to Saturday between 7.00 am and 7.00 pm (excluding public holidays). In instances where such construction work needs to be performed outside these hours, an Application for Approval of a Noise Management Plan must be submitted to the City of Fremantle Environmental Health Services for approval at least 7 days before construction can commence.

Note: Construction work includes, but is not limited to, Hammering, Bricklaying, Roofing, use of Power Tools and radios etc.

5. All noise from the proposed development must comply with the requirements of the *Environmental Protection (Noise) Regulations 1997* (as amended), such as:
- a. mechanical service systems like air-conditioners, exhaust outlets, motors, compressors and pool filters;
 - b. vehicles;
 - c. amplified acoustic systems; and
 - d. patron noise.

It is advised to seek the services of an acoustic consultant to assist the applicant to address the potential noise impacts on noise sensitive receivers.

6. Any works within the adjacent thoroughfare, i.e. road, kerbs, footpath, verge, crossover or right of way, requires a separate approval from the City of Fremantle's Infrastructure Business Services department who can be contacted via info@fremantle.wa.gov.au or 9432 9999.
7. The proponent must make application during the Building Permit application stage to Environmental Health Services via Schedule 3 – Application for registration of a lodging house as a requirement of the City of Fremantle's Health Local Laws 1997. For further information and a copy of the application form contact Environmental Health Services on 9432 9999 or via health@fremantle.wa.gov.au.
8. The premises must comply with the *Food Act 2008*, regulations and the Food Safety Standards incorporating AS 4674-2004 Design, construction and fit-out of food premises. Detailed architectural plans and elevations must be submitted to Environmental Health Services for approval prior to construction via an

application to notify/register a food business. The food business is required to be registered under the *Food Act 2008*. For further information contact Environmental Health Services on 9432 9999 or via health@fremantle.wa.gov.au.

9. In regard to the condition requiring a Construction Management Plan, Local Planning Policy 1.10 Construction Sites can be found on the City's web site via <http://www.fremantle.wa.gov.au/development/policies>.

A copy of the City's Construction and Demolition Management Plan Proforma which needs to be submitted with building and demolition permits can be accessed via:

<https://www.fremantle.wa.gov.au/sites/default/files/Construction%20and%20Demolition%20Management%20Plan%20Proforma.pdf>

The Infrastructure Business Services department can be contacted via info@fremantle.wa.gov.au or 9432 9999.

10. In relation to the condition relating to the public art contribution, the applicant is advised that Council may waive the requirement for the public art/heritage work contribution in accordance with clause 6 of LPP 2.19 where the development incorporates public art in the development to the same value as that specified in the relevant condition that is located in a position clearly visible to the general public on the site of the development. Should artwork be incorporated in the development a public art strategy for the site must be submitted to and approved prior to the issue of a Building Permit, to the satisfaction of the City of Fremantle. Please contact the City's Public Arts Coordinator on 9432 9999 for further information on this process. Prior to occupation of the development, the approved artwork must be installed and thereafter maintained to the satisfaction of the City of Fremantle. In determining the appropriateness and artistic merit of the public art, council shall seek relevant professional advice.

DRAWING LIST			
NUMBER	NAME	REV.	REVISION DESCRIPTION
DA00.00	Cover Sheet	P13	DA ISSUE 2 R2.1
DA00.01	Proposed Site Plan	P19	DA ISSUE 2 R2.1
DA01.01	Demolition Ground Floor Plan	P11	DA ISSUE 2 R2.1
DA01.02	Demolition First Floor Plan	P11	DA ISSUE 2 R2.1
DA01.03	Demolition Roof Plan	P11	DA ISSUE 2 R2.1
DA01.04	Ground Floor Plan	P13	DA ISSUE 2 R2.1
DA01.05	First Floor Plan	P11	DA ISSUE 2 R2.1
DA01.06	L2-L3 Plan	P11	DA ISSUE 2 R2.1
DA01.07	Proposed Roof Plan	P13	DA ISSUE 2 R2.1
DA02.01	Existing & Demolition Elevations	P10	DA ISSUE 2 R2.1
DA02.02	Existing & Demolition Elevations	P10	DA ISSUE 2 R2.1
DA02.03	Existing & Demolition Elevations	P8	DA ISSUE 2 R2.1
DA02.04	Proposed Elevations	P12	DA ISSUE 2 R2.1
DA02.05	Proposed Elevations	P12	DA ISSUE 2 R2.1
DA02.06	Proposed Elevations	P9	DA ISSUE 2 R2.1
DA03.01	Sections	P11	DA ISSUE 2 R2.1
DA03.02	Sections	P12	DA ISSUE 2 R2.1
DA03.03	Sections	P12	DA ISSUE 2 R2.1
DA03.04	Wide Format Section	P4	DA ISSUE 2 R2.1
DA04.01	Overshadowing Study	P8	DA ISSUE 2 R2.1



DEVELOPMENT SUMMARY										
LEVEL	EXISTING BUILDING					REAR BUILDING				TOTAL ROOMS
	4 BED (A)	4 BED (B)	8 BED (C)	8 BED (C2)	8 BED (C3)	QUEEN ACC. ENS. (D)	QUEEN ENS. (D2)	QUEEN (D3)	QUEEN ACC. (D4)	
GROUND	0	0	0	0	0	0	0	0	0	0
LEVEL 1	3	23	2	1	8	1	10	2	0	50
LEVEL 2	2	0	0	0	0	1	10	1	1	15
LEVEL 3	2	0	0	0	0	1	10	1	1	15
Grand total	7	23	2	1	8	3	30	4	2	80

PROPOSED CAR PARKING		
LEVEL	CARPARKS	BICYCLE PARKS
	COMMERCIAL	
GROUND	4	27

REFUSE - BINS	
BIN TYPE	TOTAL
BIN 1100L	5



CITY OF FREMANTLE
These Revised Plans Form Part of

DAP002/24

22 Oct 2024

Revisions /
P7 29.04.2024 DAC ISSUE
P8 04.06.2024 DA ISSUE
P9 05.06.2024 DA ISSUE 1
P10 19.08.2024 DA ISSUE RFI 1
P11 18.10.2024 DA ISSUE 2
P12 21.10.2024 DA ISSUE 2 R2.0
P13 22.10.2024 DA ISSUE 2 R2.1

Project /
LyLo Fremantle
19 ESSEX STREET, FREMANTLE, WA

Drawing /
Cover Sheet

Project No /
223252

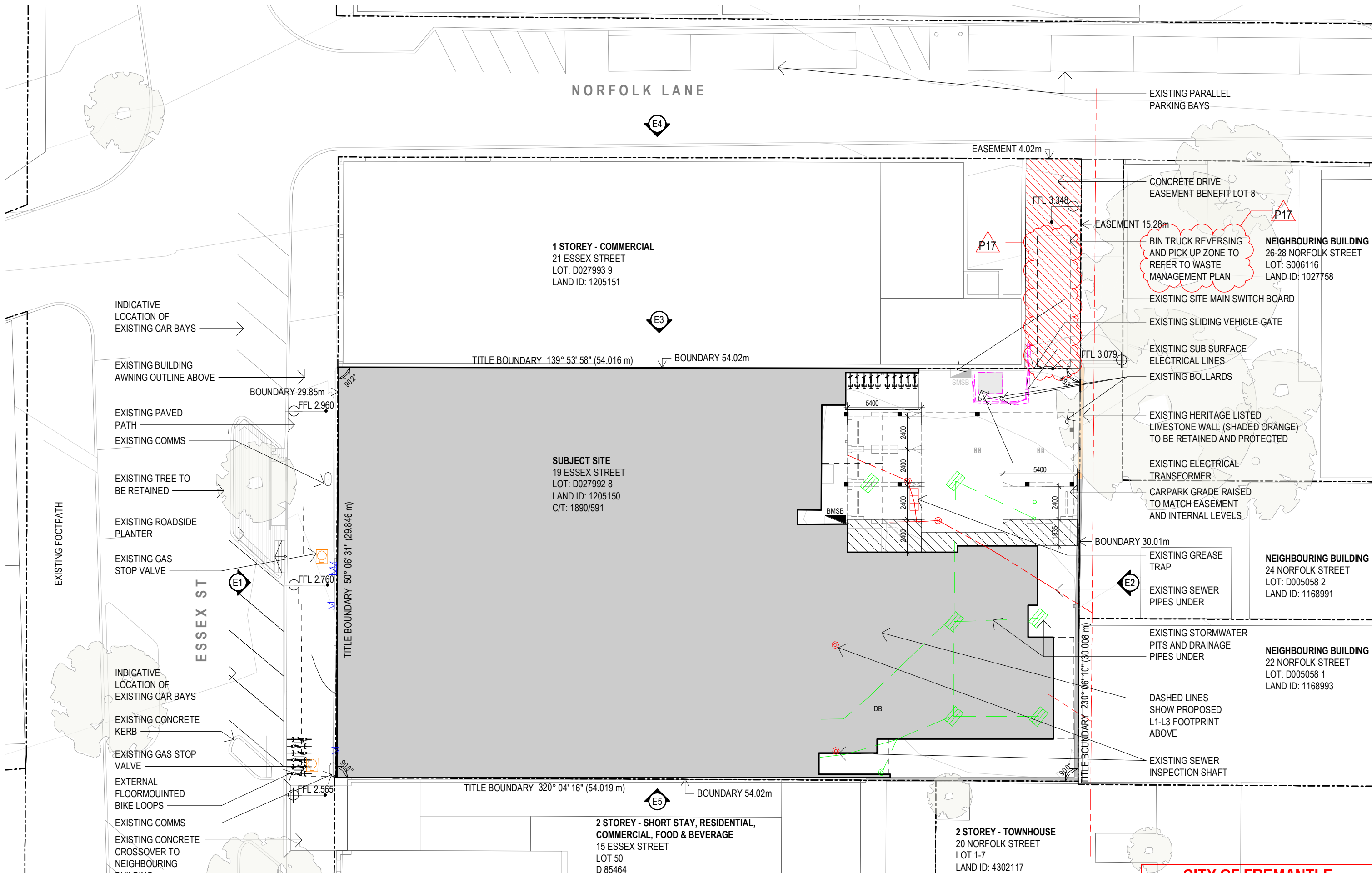
Author /
RL

Scale: @ A3 /
1 : 200

Drawing No. /
DA00.00

P13
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Revisions	Project
P13 15.05.2024	ISSUE FOR INFORMATION
P14 04.06.2024	DA ISSUE
P15 05.06.2024	DA ISSUE 1
P16 19.08.2024	DA ISSUE RFI 1
P17 18.10.2024	DA ISSUE 2
P18 21.10.2024	DA ISSUE 2 R2.0
P19 22.10.2024	DA ISSUE 2 R2.1

LyLo Fremantle

19 ESSEX STREET, FREMANTLE, WA

Proposed Site Plan

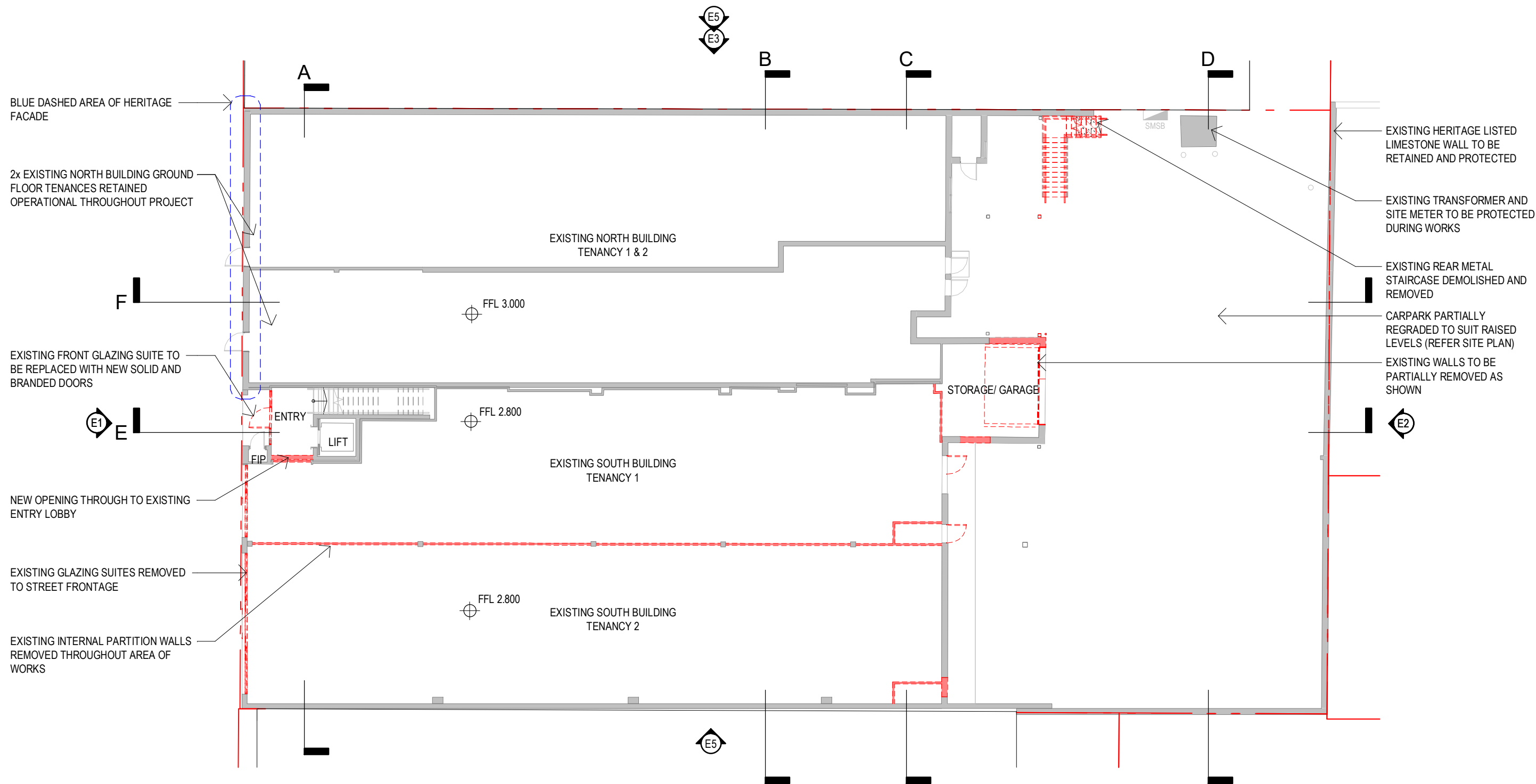
Project No 223252 Author RL

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Drawing No. DA00.01

P19

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE DEMOLISHED
- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED

CITY OF FREMANTLE
These Revised Plans Form Part of
DAP002/24
22 Oct 2024

Revisions	P5	22.04.2024	ISSUE FOR INFORMATION
	P6	24.04.2024	DAC DRAFT PACK
	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1
	P10	21.10.2024	DA ISSUE 2 R2.0
	P11	22.10.2024	DA ISSUE 2 R2.1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **Demolition Ground Floor Plan**

Project No **223252** Author **AL**

Scale: @ A3 **1 : 200**

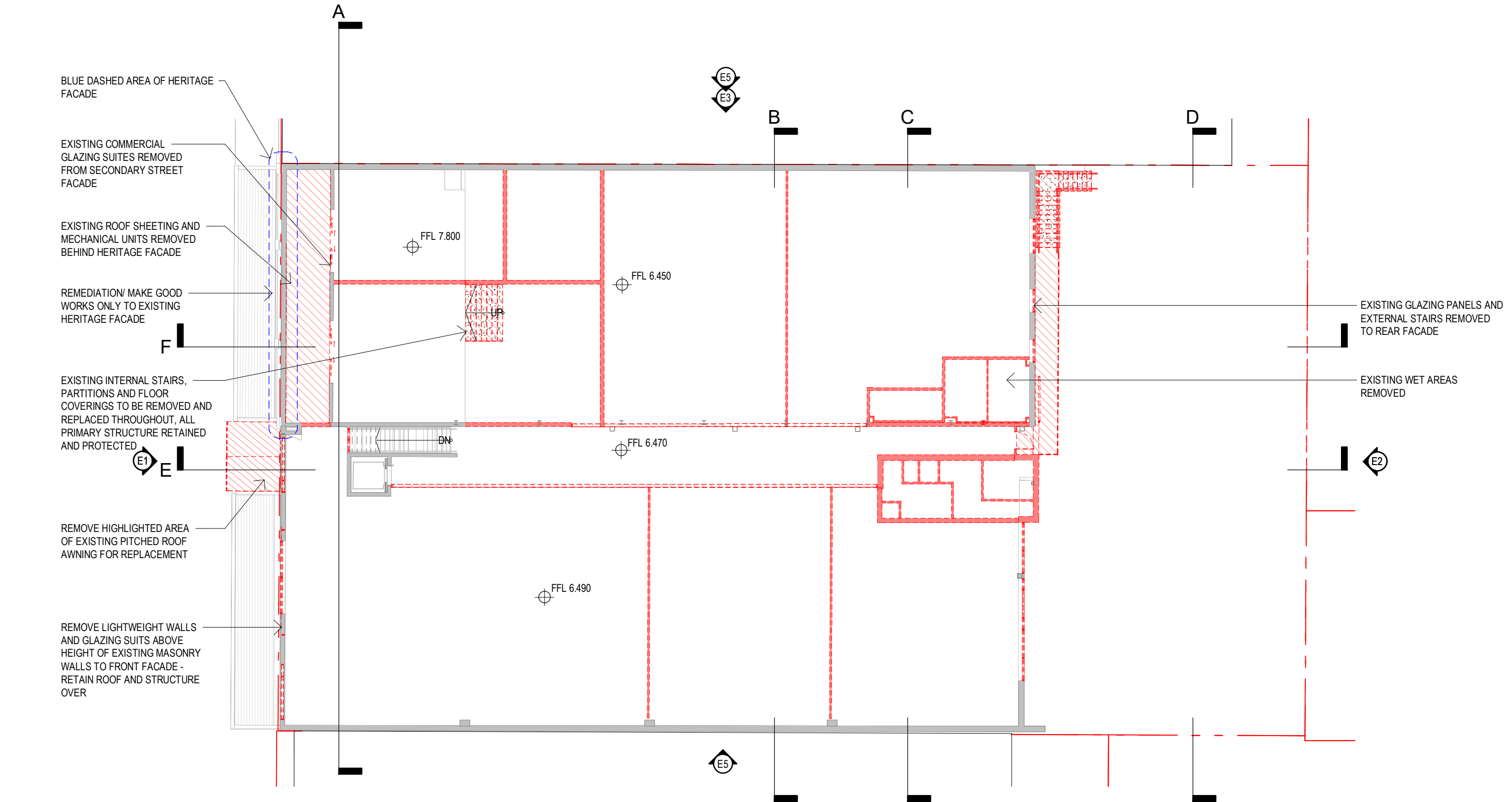
Drawing No. **DA01.01**

P11 rothelowman

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE DEMOLISHED
- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED

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These Revised Plans Form Part of
DAP002/24
22 Oct 2024

Revisions	P5	22.04.2024	ISSUE FOR INFORMATION
	P6	24.04.2024	DAC DRAFT PACK
	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1
	P10	21.10.2024	DA ISSUE 2 R2.0
	P11	22.10.2024	DA ISSUE 2 R2.1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **Demolition First Floor Plan**

Project No **223252** Author **AL**

Scale: @ A3 **1 : 200**

Drawing No. **DA01.02**

P11 rothelowman

22/10/2024 2:26:16 PM

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BLUE DASHED AREA OF HERITAGE
FACADE

DEMOLITION EXTENTS FOR NEW
TRANSLUCENT ROOF SHEETING

CUT LOCATIONS TO EXISTING
FOR NEW SKYLIGHTS

EXISTING EXTENT OF OPEN
ROOF SPACE INCREASED FOR
NEW GLAZED ROOF

EXISTING ROOF SHEETING,
GUTTER AND PURLINS BELOW
TO BE PARTIALLY DEMOLISHED
TO PROVIDE SUFFICIENT HEAD
HEIGHT TO NEW LANDING
BELOW

RL 10.29

RL 13.05

RL 10.24

RL 12.90

RL 9.98

EXISTING ROOF SHEETING
OVERHANG AND FASCIA TO BE
REMOVED FOR NEW REAR
BUILDING

EXISTING MECHANICAL DECK
TO BE REMOVED TO ALLOW
FOR NEW GLAZED ROOF

EXISTING MECHANICAL DECK
TO BE RETAINED AND
PROTECTED

EXISTING ROOF SHEETING
OVERHANG AND FASCIA TO BE
REMOVED FOR NEW REAR
BUILDING

WALL TYPE LEGEND

EXISTING WALLS, FLOORS AND STRUCTURES TO BE DEMOLISHED

EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED

CITY OF FREMANTLE
These Revised Plans Form Part of

DAP002/24

22 Oct 2024

Revisions	P5	22.04.2024	ISSUE FOR INFORMATION
	P6	24.04.2024	DAC DRAFT PACK
	P7	29.04.2024	DAC ISSUE
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	P9	05.06.2024	DA ISSUE 1
	P10	21.10.2024	DA ISSUE 2 R2.0
	P11	22.10.2024	DA ISSUE 2 R2.1

Project **LyLo Fremantle**

19 ESSEX STREET, FREMANTLE, WA

Drawing **Demolition Roof Plan**

Project No **223252** Author **AL**

Scale: @ A3 **1 : 200**

Drawing No. **DA01.03**

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EXISTING SEPARATION FROM
HERITAGE FACADE TO PRIMARY
WALL

BLUE DASHED AREA OF HERITAGE
FACADE

GREY DASHED LINES
INDICATIVE LOCATION OF
EXISTING TRUSSES OVERHEAD

ORANGE DASHED LINES OF
GROUND FLOOR WALL BELOW

NEW WALL TO EXTEND ABOVE
EXISTING MASONRY WALL -
ALIGN DATUM WITH NORTHERN
TENANCY HERITAGE FACADE
WALL

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

Revisions	P5	22.04.2024	ISSUE FOR INFORMATION
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	P9	05.06.2024	DA ISSUE 1
	P10	21.10.2024	DA ISSUE 2 R2.0
	P11	22.10.2024	DA ISSUE 2 R2.1

Project / **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing / **First Floor Plan**

Project No / **223252** Author / **RL**

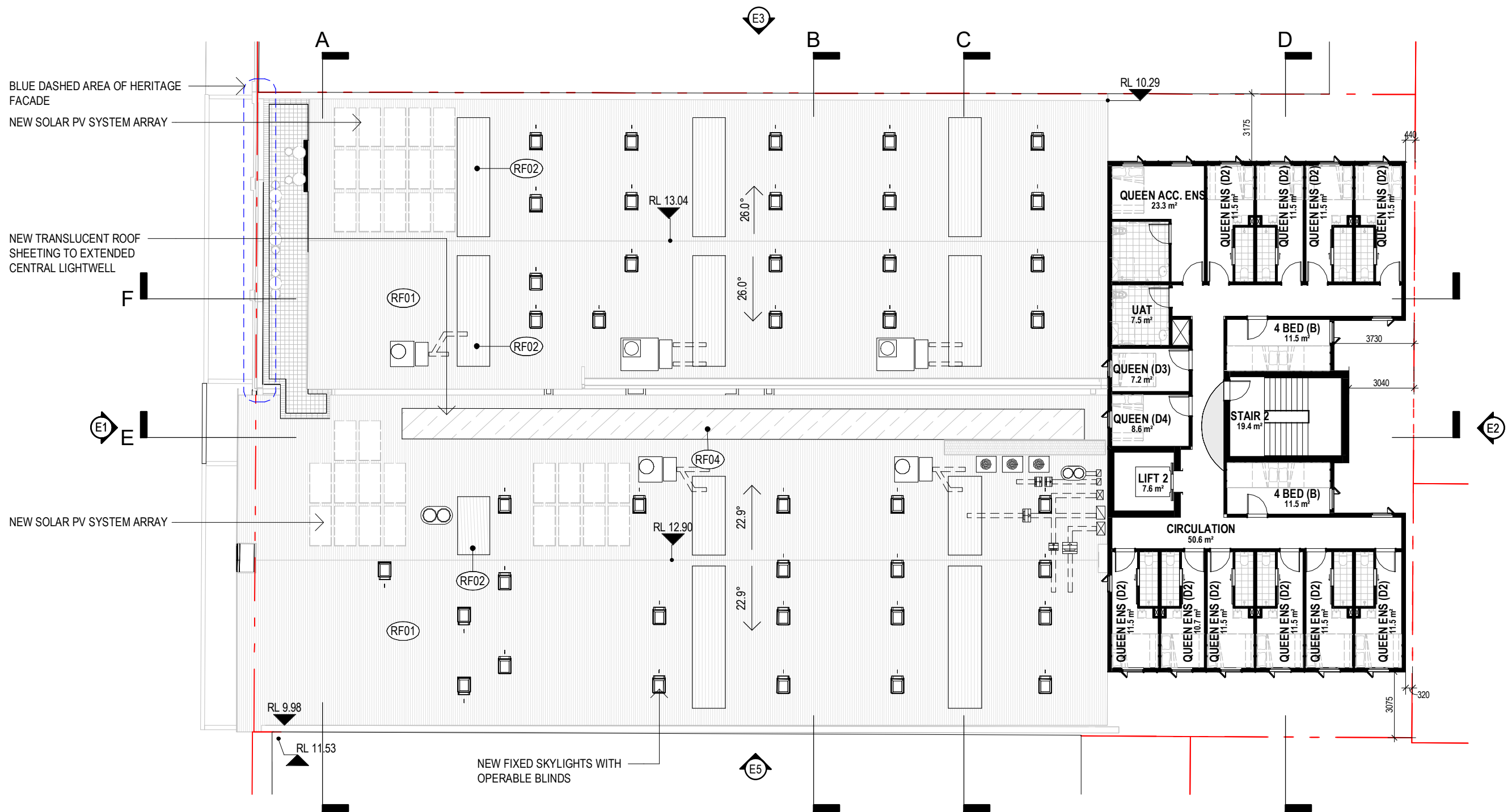
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Drawing No. / **DA01.05**

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

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	P10	21.10.2024	DA ISSUE 2 R2.0
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Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **L2-L3 Plan**

Project No **223252** Author **AL** Scale: @ A3 **1 : 200**

Drawing No **DA01.06**

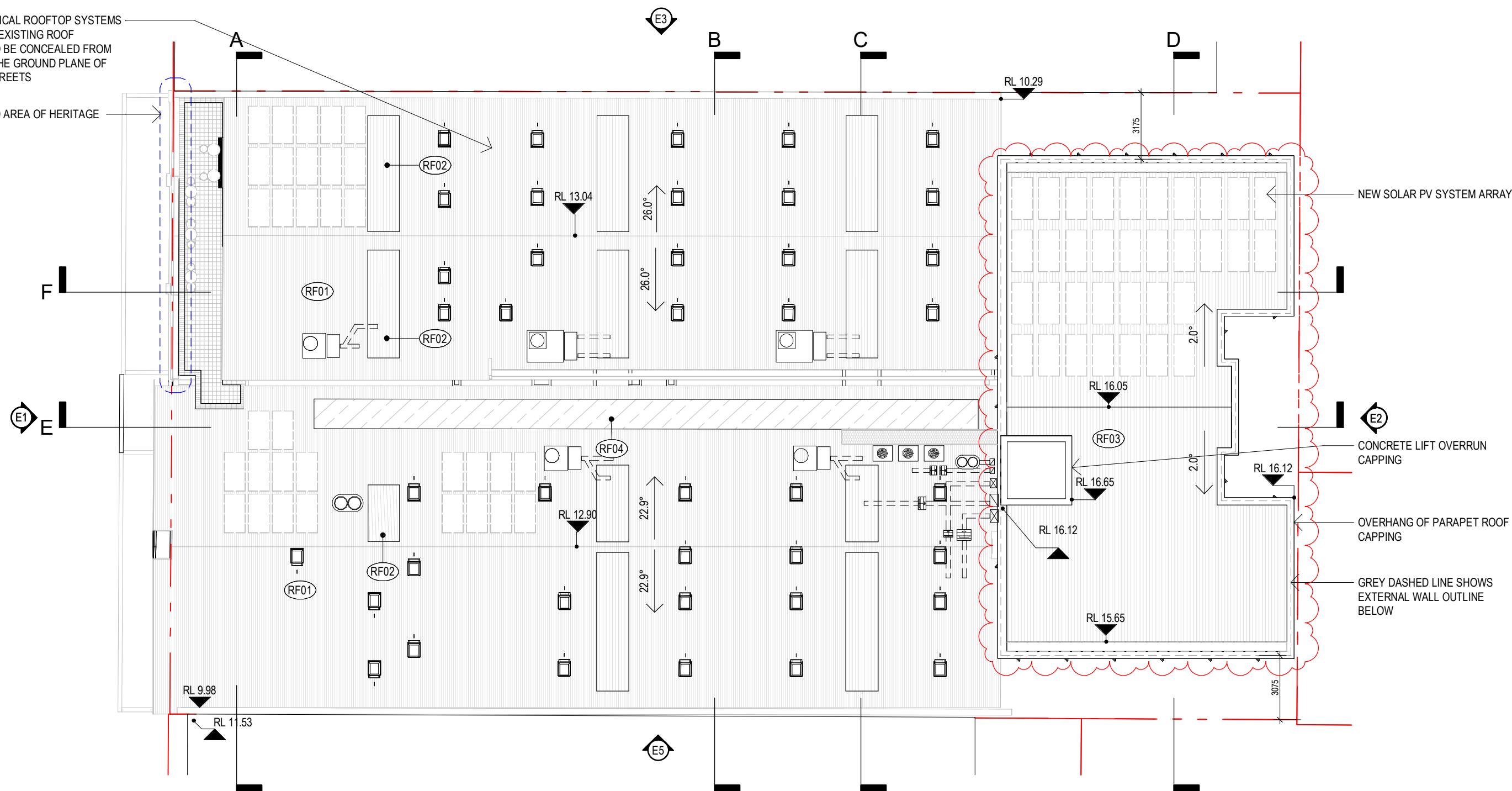
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NEW MECHANICAL ROOFTOP SYSTEMS
LOCATED ON EXISTING ROOF
SHEETING, TO BE CONCEALED FROM
VIEW FROM THE GROUND PLANE OF
ADJACENT STREETS

BLUE DASHED AREA OF HERITAGE
FACADE



WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

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	P11	18.10.2024	DA ISSUE 2
	P12	21.10.2024	DA ISSUE 2 R2.0
	P13	22.10.2024	DA ISSUE 2 R2.1

Project / **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing / **Proposed Roof Plan**

Project No / **223252** Author / **AL**

Scale: @ A3 / **1 : 200**

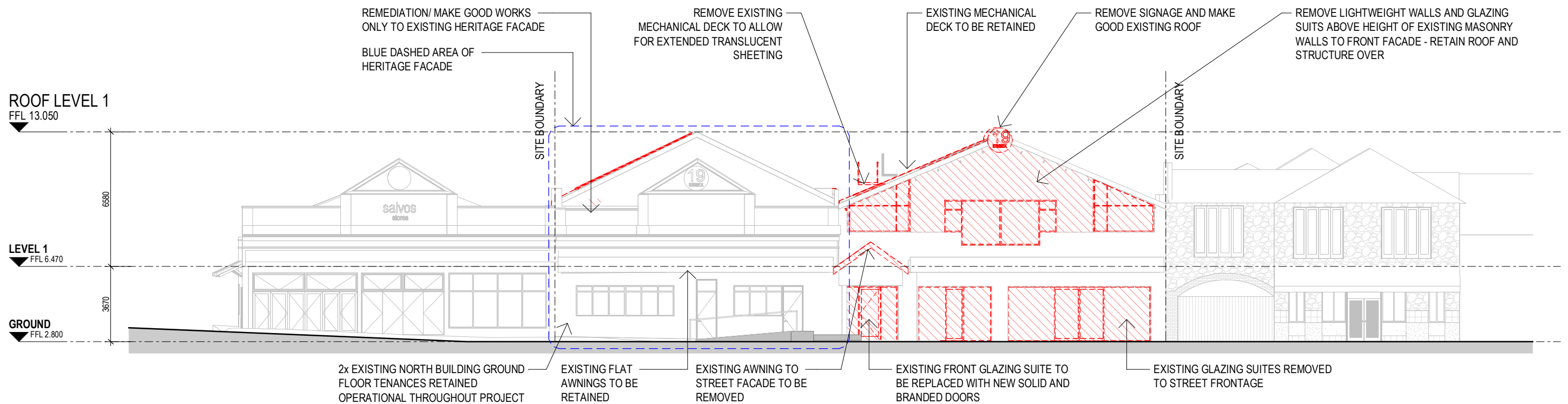
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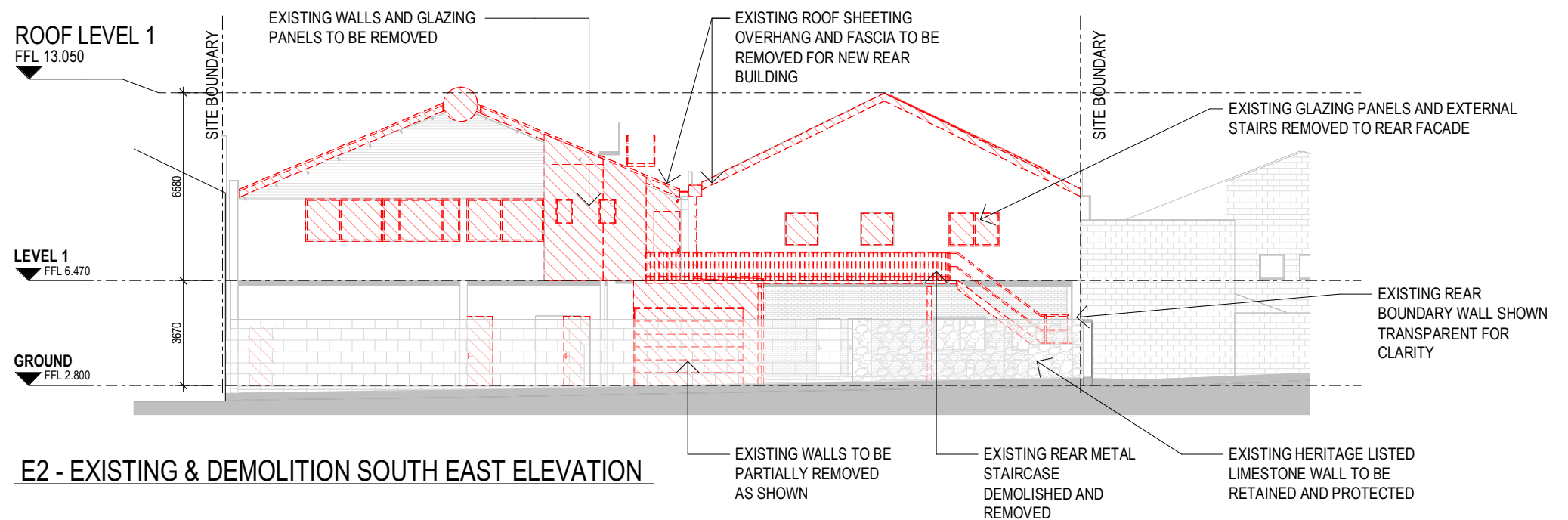
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E1 - EXISTING & DEMOLITION NORTH WEST ELEVATION



E2 - EXISTING & DEMOLITION SOUTH EAST ELEVATION

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE DEMOLISHED
- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED

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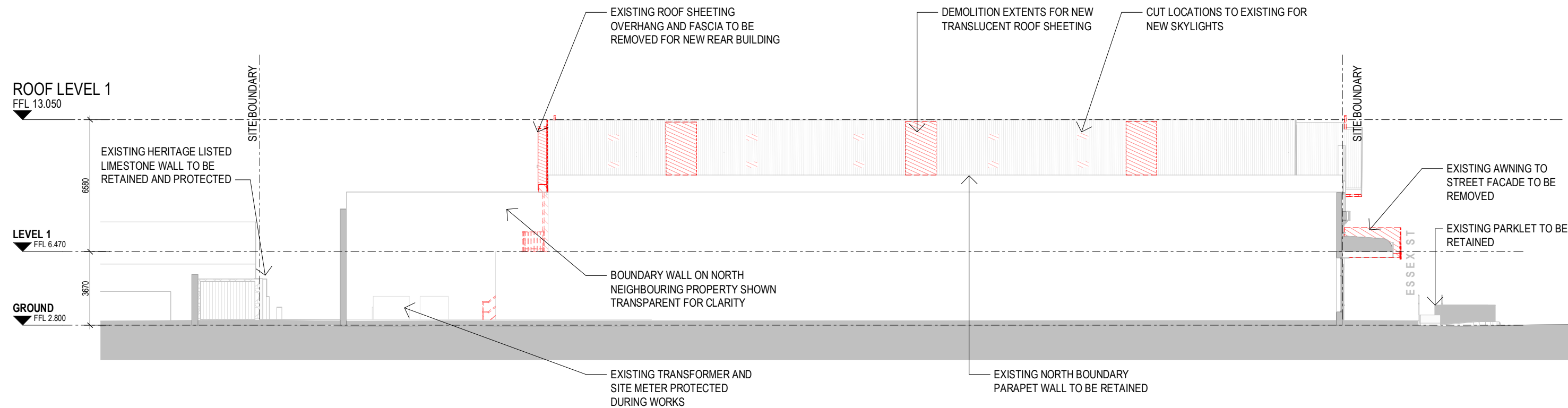
Project
LyLo Fremantle
19 ESSEX STREET, FREMANTLE, WA

Drawing
Existing & Demolition Elevations

Project No
223252
Author
AL
Scale: @ A3
1 : 200

Drawing No.
DA02.01

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E3 - EXISTING & DEMOLITION NORTH EAST ELEVATION



E4 - EXISTING & DEMOLITION NORTH EAST ELEVATION
(NORFOLK LANE)

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE DEMOLISHED
- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED

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	P10	22.10.2024	DA ISSUE 2 R2.1

Project
LyLo Fremantle
19 ESSEX STREET, FREMANTLE, WA

Drawing
Existing & Demolition Elevations

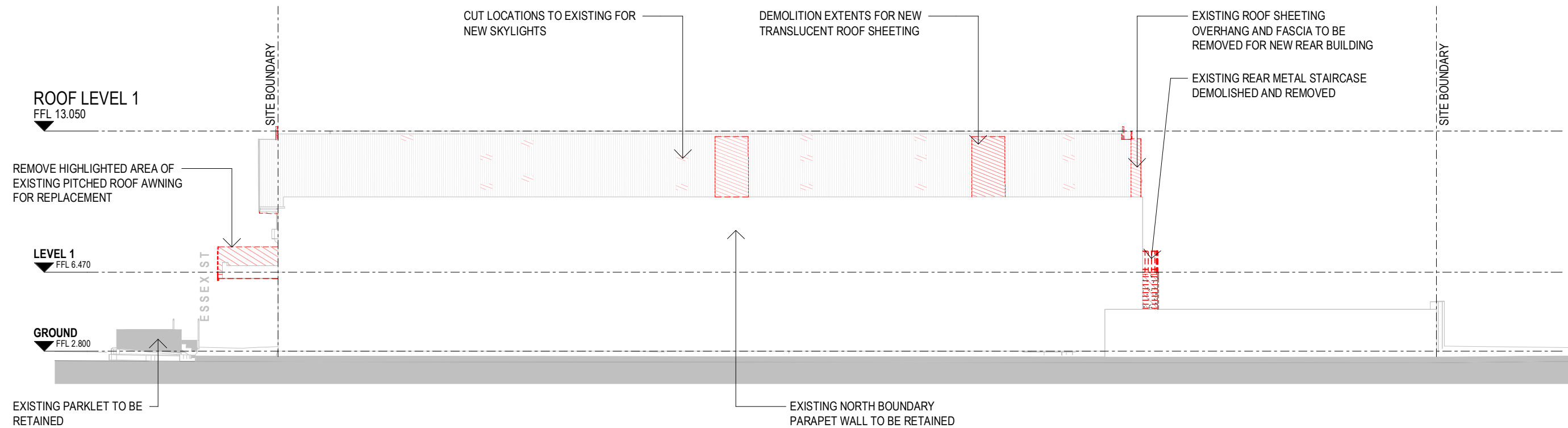
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Author
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E5 - EXISTING & DEMOLITION SOUTH WEST ELEVATION

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Project
LyLo Fremantle

19 ESSEX STREET, FREMANTLE, WA

Drawing
Existing & Demolition Elevations

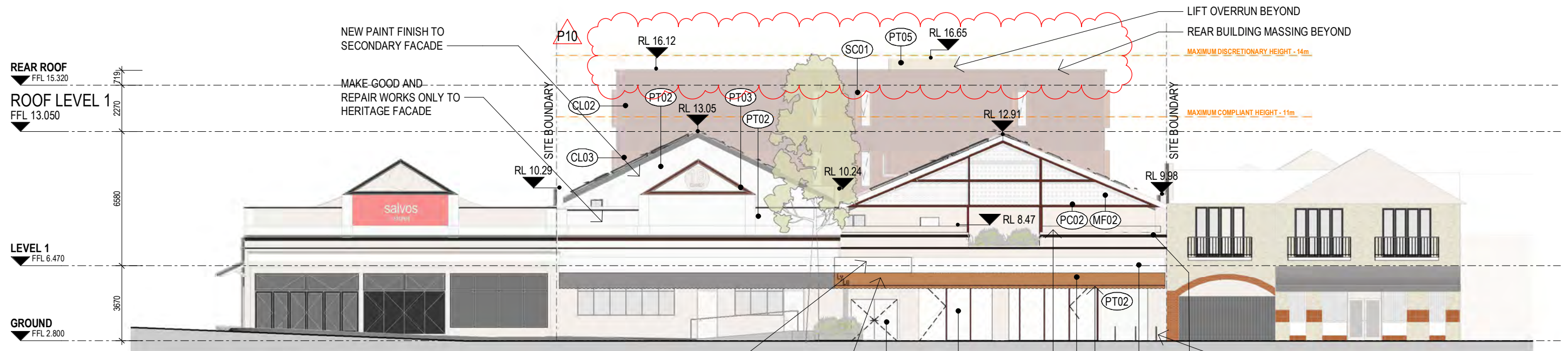
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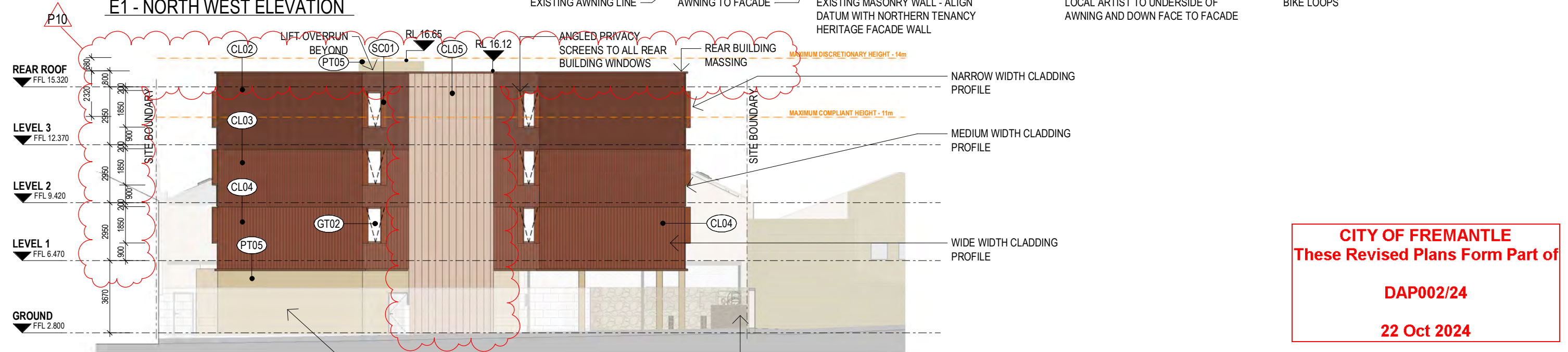
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E1 - NORTH WEST ELEVATION



E2 - SOUTH EAST ELEVATION

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

MATERIAL AND FINISHES LEGEND

CL01 - CLADDING EXISTING	CL05 - CLADDING LONGLINE 305 (155mm) COLOUR: LIGHT BEIGE	PT05 - PAINT FINISH NEW PAINT FINISH COLOUR: BALSA STONE	FB01 - FABRIC FINISH EXISTING	RF01 - ROOF CLADDING EXISTING	GT01 - GLAZING EXISTING GLAZING
CL02 - CLADDING MINI ORB (35mm) COLOUR: MEDIUM TERRACOTTA	PT01 - PAINT FINISH EXISTING	MF01 - METAL FINISH EXISTING	FB02 - ROOF CLADDING NEW FABRIC AWNING COLOUR: CINNAMON SAND	RF02 - ROOF CLADDING NEW TRANSLUCENT ROOF CLADDING COLOUR: SHALE GREY	GT02 - GLAZING NEW GLAZING TRANSPARENCY: CLEAR
CL03 - CLADDING CUSTOM ORB (85mm) COLOUR: MEDIUM TERRACOTTA	PT02 - PAINT FINISH NEW PAINT FINISH COLOUR: LEXICON QUARTER	MF02 - METAL FINISH NEW PERFORATED METAL FINISH COLOUR: CHAIN PEARL	PC01 - POWDERCOAT FINISH EXISTING	RF03 - ROOF CLADDING NEW CORRUGATED ROOF CLADDING COLOUR: SHALE GREY	GT03 - GLAZING NEW FIRE RATED GLAZING TRANSPARENCY: CLEAR
CL04 - CLADDING LONGLINE 305 (155mm) COLOUR: MEDIUM TERRACOTTA	PT03 - PAINT FINISH NEW PAINT FINISH COLOUR: RUSTED CRIMSON	SC01 - SCREEN NEW SCREEN COLOUR: LIGHT TERRACOTTA	PC02 - POWDERCOAT FINISH NEW METAL FINISH COLOUR: RUSTED CRIMSON	RF04 - ROOF GLAZING NEW GLAZED PANELS COLOUR: CLEAR	

Revisions			
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P10	18.10.2024	DA ISSUE 2	
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P12	22.10.2024	DA ISSUE 2 R2.1	

Project **LyLo Fremantle**

19 ESSEX STREET, FREMANTLE, WA

Drawing **Proposed Elevations**

Project No **223252**

Author **AL**

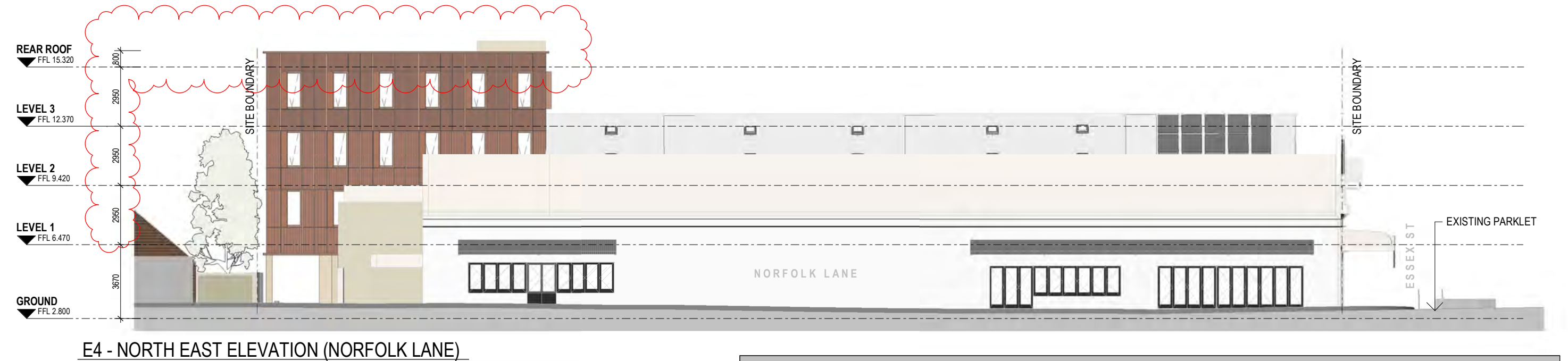
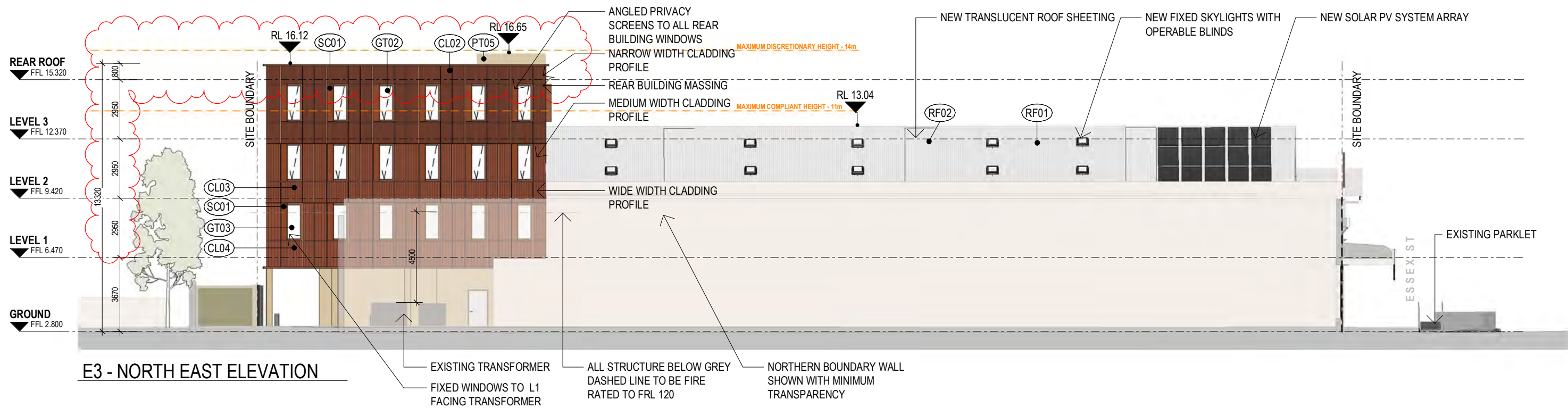
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MATERIAL AND FINISHES LEGEND					
CL01 - CLADDING EXISTING	CL05 - CLADDING LONGLINE 305 (155mm) COLOUR: LIGHT BEIGE	PT05 - PAINT FINISH NEW PAINT FINISH COLOUR: BALSA STONE	FB01 - FABRIC FINISH EXISTING	RF01 - ROOF CLADDING EXISTING	GT01 - GLAZING EXISTING GLAZING
CL02 - CLADDING MINI ORB (35mm) COLOUR: MEDIUM TERRACOTTA	PT01 - PAINT FINISH EXISTING	MF01 - METAL FINISH EXISTING	FB02 - ROOF CLADDING NEW FABRIC AWNING COLOUR: CINNAMON SAND	RF02 - ROOF CLADDING NEW TRANSLUCENT ROOF CLADDING COLOUR: SHALE GREY	GT02 - GLAZING NEW GLAZING TRANSPARENCY: CLEAR
CL03 - CLADDING CUSTOM ORB (85mm) COLOUR: MEDIUM TERRACOTTA	PT02 - PAINT FINISH NEW PAINT FINISH COLOUR: LEXICON QUARTER	MF02 - METAL FINISH NEW PERFORATED METAL FINISH COLOUR: CHAIN PEARL	PC01 - POWDERCOAT FINISH EXISTING	RF03 - ROOF CLADDING NEW CORRUGATED ROOF CLADDING COLOUR: SHALE GREY	GT03 - GLAZING NEW FIRE RATED GLAZING TRANSPARENCY: CLEAR
CL04 - CLADDING LONGLINE 305 (155mm) COLOUR: MEDIUM TERRACOTTA	PT03 - PAINT FINISH NEW PAINT FINISH COLOUR: RUSTED CRIMSON	SC01 - SCREEN NEW SCREEN COLOUR: LIGHT TERRACOTTA	PC02 - POWDERCOAT FINISH NEW METAL FINISH COLOUR: RUSTED CRIMSON	RF04 - ROOF GLAZING NEW GLAZED PANELS COLOUR: CLEAR	

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

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P10	18.10.2024	DA ISSUE 2	
P11	21.10.2024	DA ISSUE 2 R2.0	
P12	22.10.2024	DA ISSUE 2 R2.1	

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Project
LyLo Fremantle
19 ESSEX STREET, FREMANTLE, WA

Drawing
Proposed Elevations

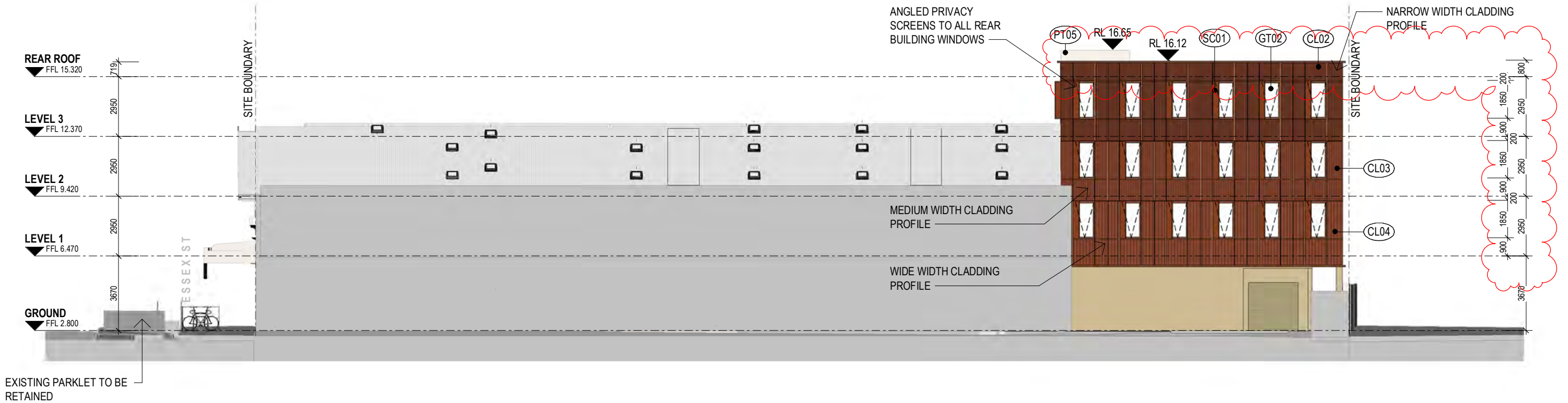
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Author
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E5 - SOUTH WEST ELEVATION

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MATERIAL AND FINISHES LEGEND					
CL01 - CLADDING EXISTING	CL05 - CLADDING LONGLINE 305 (155mm) COLOUR: LIGHT BEIGE	PT05 - PAINT FINISH NEW PAINT FINISH COLOUR: BALSA STONE	FB01 - FABRIC FINISH EXISTING	RF01 - ROOF CLADDING EXISTING	GT01 - GLAZING EXISTING GLAZING
CL02 - CLADDING MINI ORB (35mm) COLOUR: MEDIUM TERRACOTTA	PT01 - PAINT FINISH EXISTING	MF01 - METAL FINISH EXISTING	FB02 - ROOF CLADDING NEW FABRIC AWNING COLOUR: CINNAMON SAND	RF02 - ROOF CLADDING NEW TRANSLUCENT ROOF CLADDING COLOUR: SHALE GREY	GT02 - GLAZING NEW GLAZING TRANSPARENCY: CLEAR
CL03 - CLADDING CUSTOM ORB (85mm) COLOUR: MEDIUM TERRACOTTA	PT02 - PAINT FINISH NEW PAINT FINISH COLOUR: LEXICON QUARTER	MF02 - METAL FINISH NEW PERFORATED METAL FINISH COLOUR: CHAIN PEARL	PC01 - POWDERCOAT FINISH EXISTING	RF03 - ROOF CLADDING NEW CORRUGATED ROOF CLADDING COLOUR: SHALE GREY	GT03 - GLAZING NEW FIRE RATED GLAZING TRANSPARENCY: CLEAR
CL04 - CLADDING LONGLINE 305 (155mm) COLOUR: MEDIUM TERRACOTTA	PT03 - PAINT FINISH NEW PAINT FINISH COLOUR: RUSTED CRIMSON	SC01 - SCREEN NEW SCREEN COLOUR: LIGHT TERRACOTTA	PC02 - POWDERCOAT FINISH NEW METAL FINISH COLOUR: RUSTED CRIMSON	RF04 - ROOF GLAZING NEW GLAZED PANELS COLOUR: CLEAR	

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

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	P8	21.10.2024	DA ISSUE 2 R2.0
	P9	22.10.2024	DA ISSUE 2 R2.1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **Proposed Elevations**

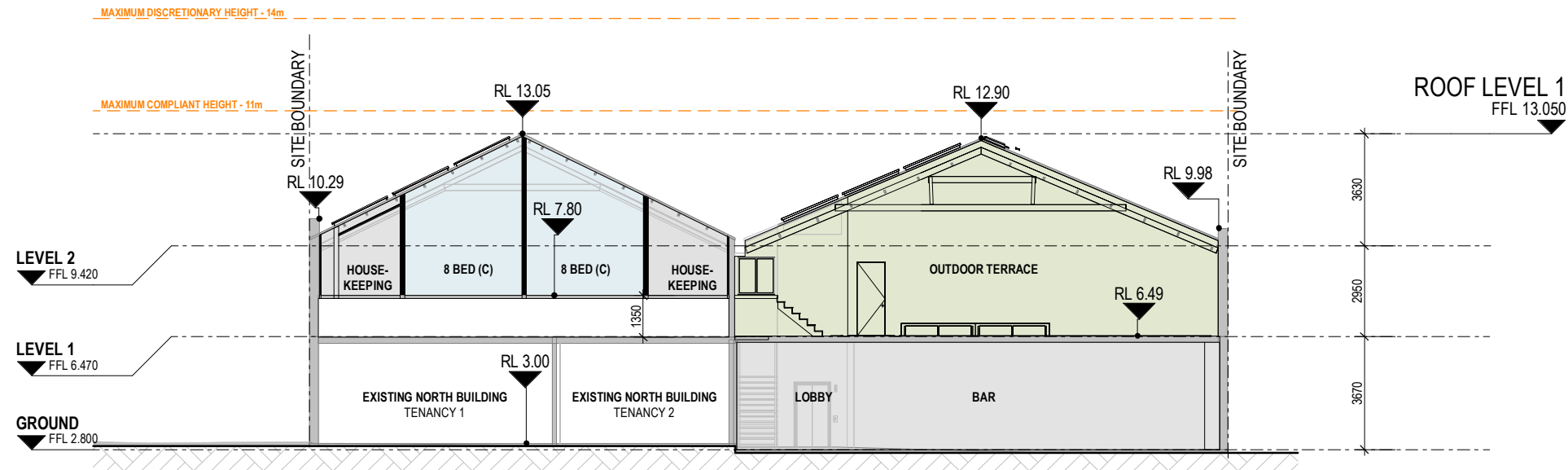
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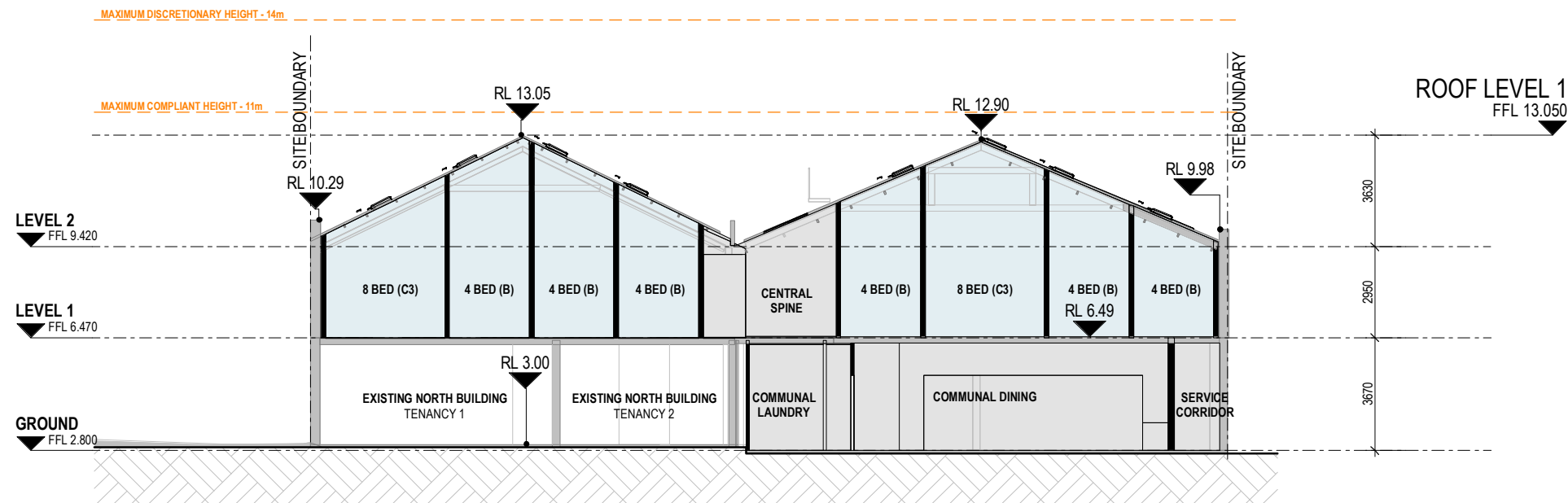
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Section AA



Section BB

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

ROOM LEGEND

- ROOMS
- OTHERS (NON-HABITABLE ROOMS)
- EXTERNAL SPACE

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	P9	19.08.2024	DA ISSUE RFI 1
	P10	21.10.2024	DA ISSUE 2 R2.0
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Project / **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing / **Sections**

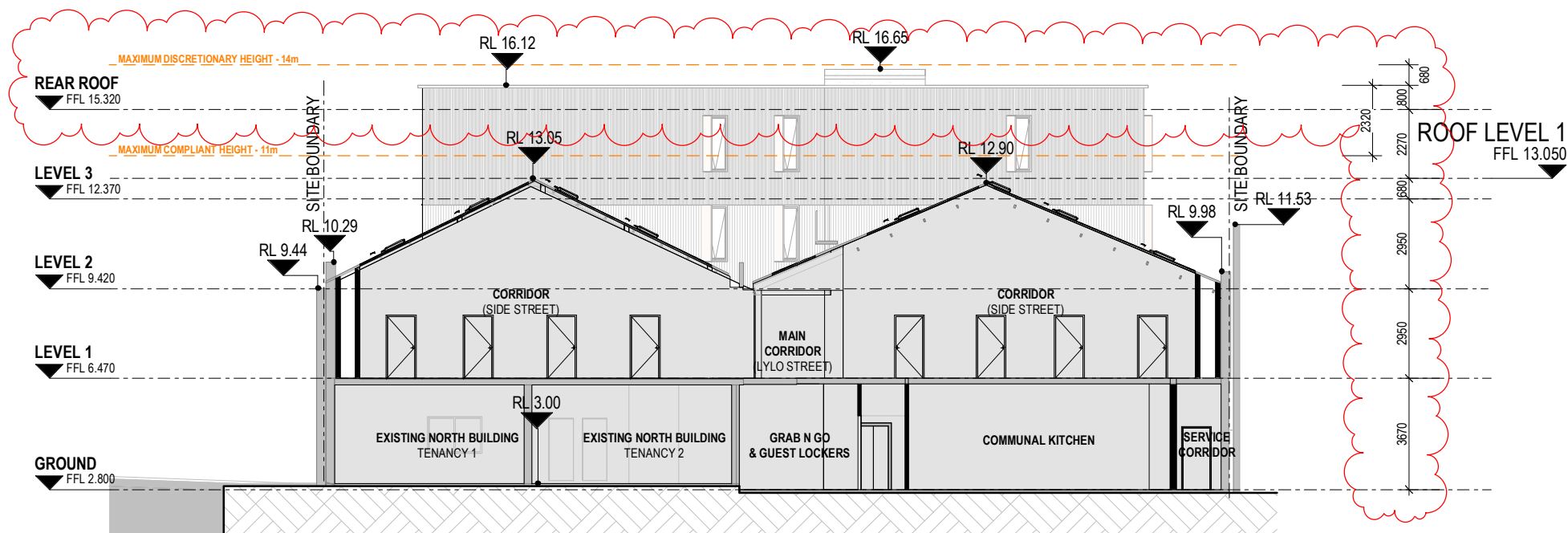
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Drawing No. / **DA03.01**

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Section CC



Section DD

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

ROOM LEGEND

- ROOMS
- OTHERS (NON-HABITABLE ROOMS)
- EXTERNAL SPACE

CITY OF FREMANTLE
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Project / **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing / **Sections**

Project No / **223252** Author / **AL** Scale: @ A3 / **1 : 200**

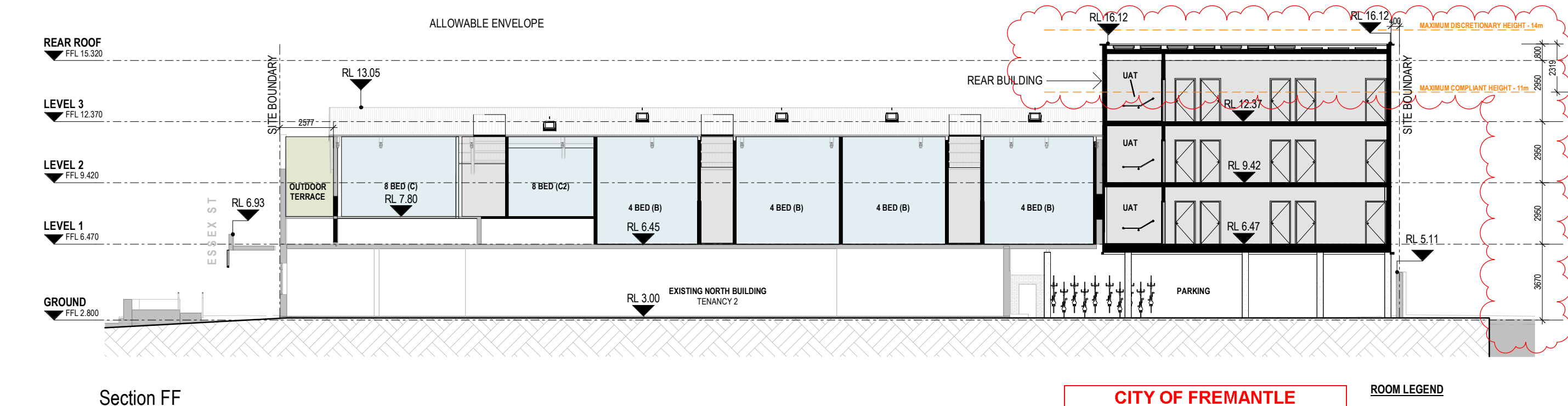
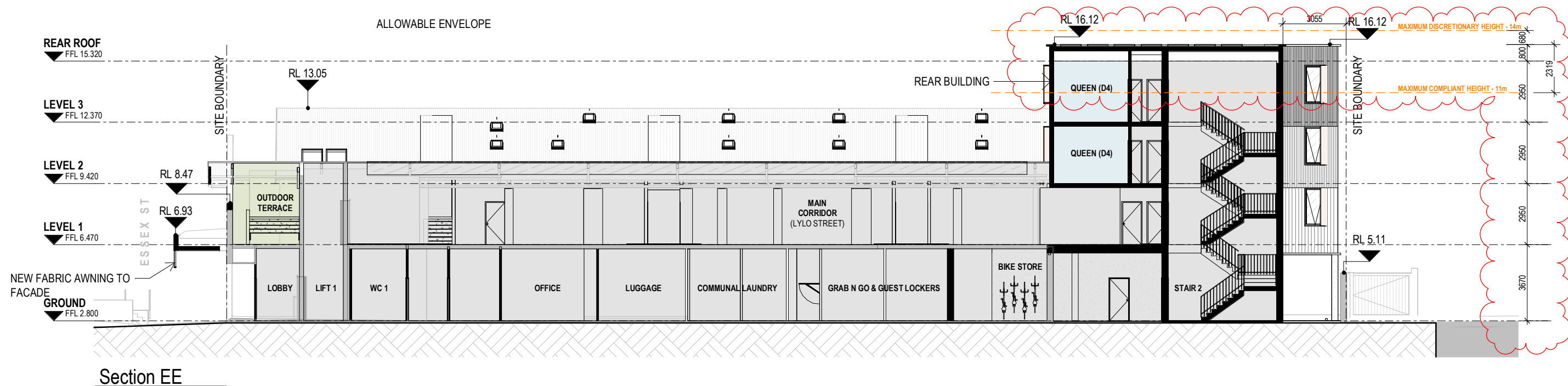
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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

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ROOM LEGEND

- ROOMS
- OTHERS (NON-HABITABLE ROOMS)
- EXTERNAL SPACE

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P7	04.06.2024	DA ISSUE	
P8	05.06.2024	DA ISSUE 1	
P9	19.08.2024	DA ISSUE RFI 1	
P10	18.10.2024	DA ISSUE 2	
P11	21.10.2024	DA ISSUE 2 R2.0	
P12	22.10.2024	DA ISSUE 2 R2.1	

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Project **LyLo Fremantle**

19 ESSEX STREET, FREMANTLE, WA

Drawing **Sections**

Project No **223252** Author **AL**

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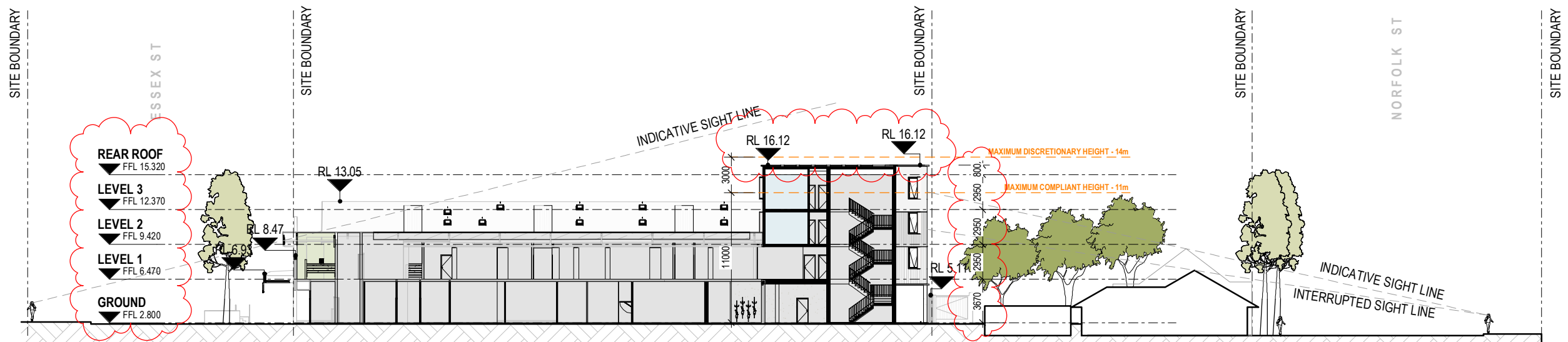
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P12

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LOCAL PLANNING SCHEME NO. 4

- SCHEDULE 7 — LOCAL PLANNING AREAS (DEVELOPMENT REQUIREMENTS)**
 - 1.3.1 SPECIFIC DEVELOPMENT CONTROLS FOR SUB AREAS**

Despite the general height requirements outlined in 1.1 above, building height shall be limited to a maximum height of three storeys (maximum external wall 69/143 height of 11* metres as measured from ground level with a maximum roof plain pitch of 33 degrees).

Council may consent to an additional storey subject to —

- Where a site meets any of the requirements of Clause 1A(a)-(e) of the deemed provisions, the upper level being sufficiently setback from the street so as to not be visible from the street(s) adjoining the subject site.
- maximum external wall height of 14* metres, and
- compliance with clause 1.2. above.

*Inclusive of roof parapet and spacing between floors.

CITY OF FREMANTLE
These Revised Plans Form Part of

DAP002/24

22 Oct 2024

Revisions /
P1 19.08.2024 DA ISSUE RF1 1
P2 18.10.2024 DA ISSUE 2
P3 21.10.2024 DA ISSUE 2 R2.0
P4 22.10.2024 DA ISSUE 2 R2.1

Project /
LyLo Fremantle
19 ESSEX STREET, FREMANTLE, WA

Drawing /
Wide Format Section

Project No /
223252

Author /
RL

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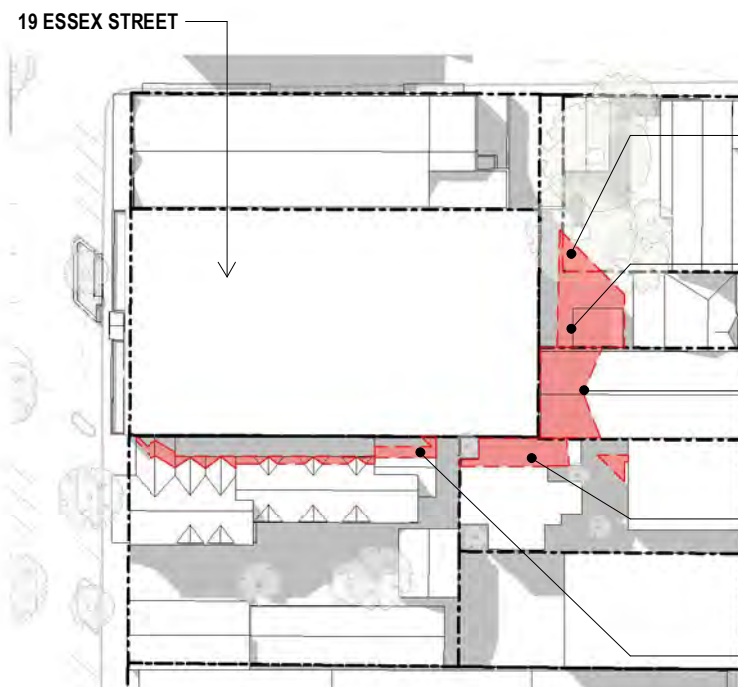
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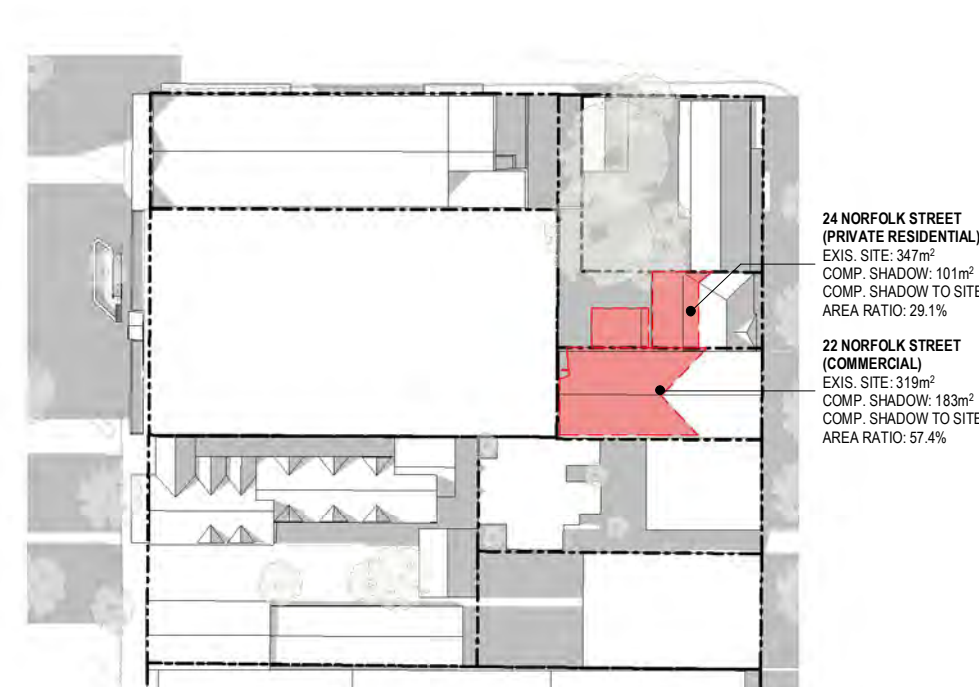
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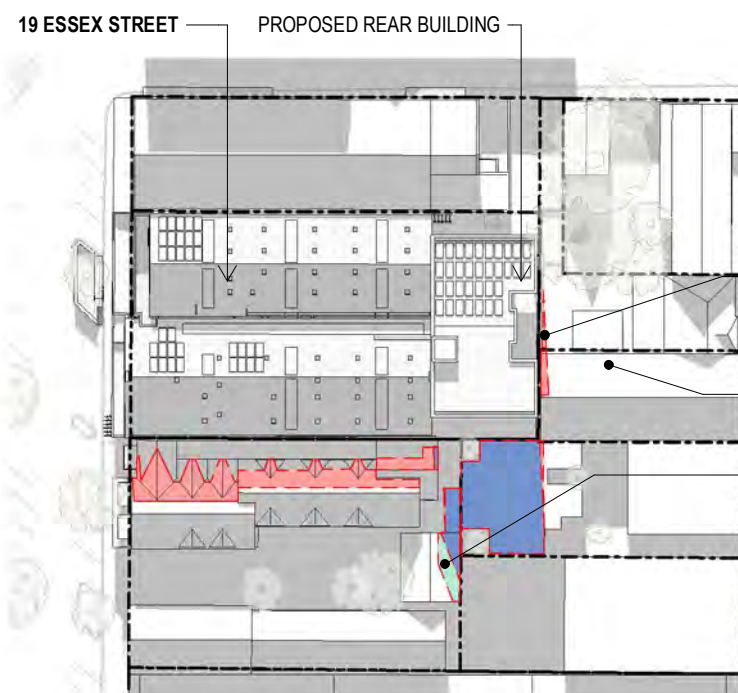
Compliant Mass Plan View - Winter Solstice 9am



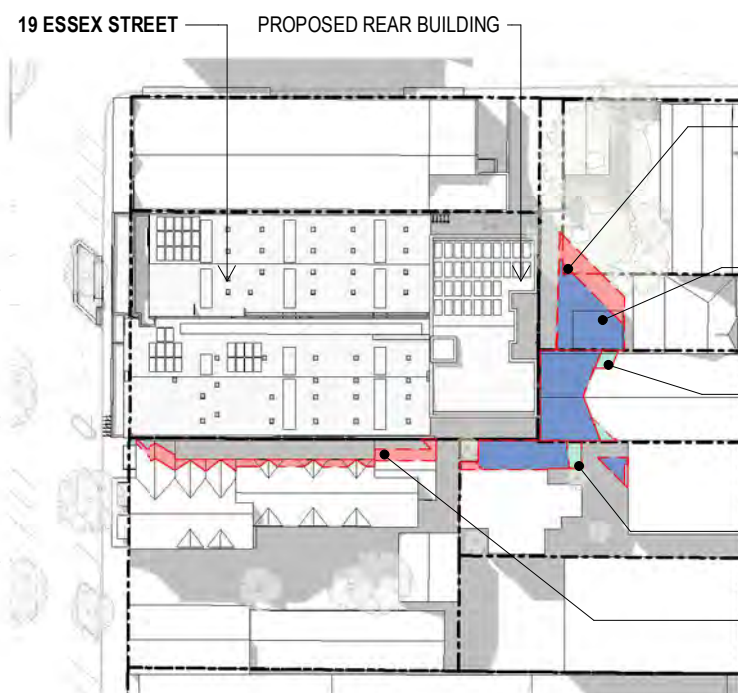
Compliant Mass Plan View - Winter Solstice 12pm



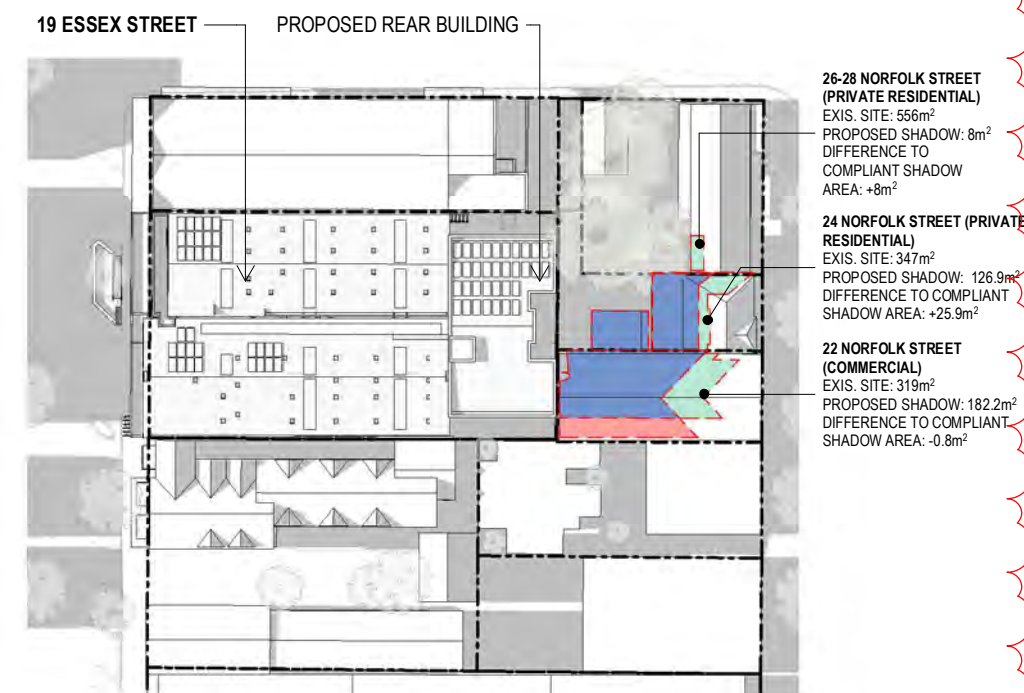
Compliant Mass Plan View - Winter Solstice 3pm



Proposed Works Plan View - Winter Solstice 9am



Proposed Works Plan View - Winter Solstice 12pm



Proposed Works Plan View - Winter Solstice 3pm

LEGEND

- COMPLIANT ENVELOPE SHADOW
- SHADOW CAST BY PROPOSAL
- ADDITIONAL SHADOW CAST BY PROPOSAL

CITY OF FREMANTLE
 These Revised Plans Form Part of
DAP002/24
 22 Oct 2024

Revisions			
P2	29.04.2024	DAC ISSUE	
P3	04.06.2024	DA ISSUE	
P4	05.06.2024	DA ISSUE 1	
P5	19.08.2024	DA ISSUE RFI 1	
P6	18.10.2024	DA ISSUE 2	
P7	21.10.2024	DA ISSUE 2 R2.0	
P8	22.10.2024	DA ISSUE 2 R2.1	

Project / **LyLo Fremantle**
 19 ESSEX STREET, FREMANTLE, WA

Drawing / **Overshadowing Study**

Project No / **223252** Author / **RL**

Scale: @ A3 / **1 : 1000**

Drawing No. / **DA04.01**

P8

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Waste Management Plan

19 Essex Street, Fremantle

Prepared for EVT and Belingbak

23 October 2024

Project Number: WMP24011

DOCUMENT CONTROL

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2.0	Second Approved Release	6/06/2024	AB	DP	DP
3.0	Third Approved Release	13/08/2024	AB	DP	DP
4.0	Fourth Approved Release	23/10/2024	DP	AB	DP

Approval for Release

Name	Position	File Reference
Dilan Patel	Project Manager – Senior Waste Strategy Consultant	WMP24011-01_Waste Management Plan_4.0
Signature		

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Executive Summary

EVT is seeking development approval for the proposed accommodation development located at 19 Essex Street, Fremantle (the Proposal).

To satisfy the conditions of the development application the City of Fremantle (the City) requires the submission of a Waste Management Plan (WMP) that will identify how waste is to be stored and collected from the Proposal. Talis Consultants has been engaged to prepare this WMP to satisfy the City's requirements.

A summary of the bin size, numbers, collection frequency and collection method is provided in the below table.

Proposed Waste Collection Summary

Waste Type	Generation (L/week)	Bin Size (L)	Number of Bins	Collection Frequency	Collection
Refuse	11,905	1,100	Four	Three times each week	Private Contractor
Recycling	2,520	1,100	One	Three times each week	Private Contractor

A private contractor will service the Proposal from the Right of Way. The private contractor will transfer bins to and from the Bin Holding Area and the waste collection vehicle for servicing.

Building management will oversee the relevant aspects of waste management at the Proposal.

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Figure 1: Locality Plan

1 Introduction

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To satisfy the conditions of the development application the City of Fremantle (the City) requires the submission of a Waste Management Plan (WMP) that will identify how waste is to be stored and collected from the Proposal. Talis Consultants has been engaged to prepare this WMP to satisfy the City's requirements.

The Proposal is bordered by Essex Street to the north and commercial properties to the east, south and west, as shown in Figure 1.

1.1 Objectives and Scope

The objective of this WMP is to outline the equipment and procedures that will be adopted to manage waste (refuse and recyclables) at the Proposal. Specifically, the WMP demonstrates that the Proposal is designed to:

- Adequately cater for the anticipated volume of waste to be generated;
- Provide an adequately sized Bin Storage Area, including appropriate bins; and
- Allow for efficient collection of bins by appropriate waste collection vehicles.

To achieve the objective, the scope of the WMP comprises:

- Section 2: Waste Generation;
- Section 3: Waste Storage;
- Section 4: Waste Collection;
- Section 5: Waste Management; and
- Section 6: Conclusion.

2 Waste Generation

The following section shows the waste generation rates used and the estimated waste volumes to be generated at the Proposal.

2.1 Proposed Tenancies

The anticipated volume of refuse and recyclables is based on the number of beds and the floor area (m²) of the tenancies at the Proposal. The Proposal consists of the following:

- Beds – 247 Beds;
- Outdoor Dining/Bar – 133m²;
- Check In – 50m²;
- Kitchen – 42m²;
- Communal Dining/Kitchen – 128m²; and
- Office – 15m².

2.2 Waste Generation Rates

In order to achieve an accurate projection of waste volumes for the Proposal, consideration was given to the City of Perth's *Waste Guidelines for New Developments* (Revision 5, effective from June 2019).

Table 2-1 shows the waste generation rates which have been applied to the Proposal.

Table 2-1: Waste Generation Rates

Tenancy Use Type	City of Perth Guideline Reference	Refuse Generation Rate	Recycling Generation Rate
Beds	Hotel/Motel	5L/bed/day	1L/bed/day
Outdoor Dining/Bar	Hotel/Motel - Bar	50L/100m ² /day	50L/100m ² /day
Check In	Offices	10L/100m ² /day	10L/100m ² /day
Kitchen	Hotel/Motel - Dining Area	667L/100m ² /day	50L/100m ² /day
Communal Dining/Kitchen	Hotel/Motel - Dining Area	667L/100m ² /day	50L/100m ² /day
Office	Offices	10L/100m ² /day	10L/100m ² /day

2.3 Waste Generation Volumes

Waste generation is estimated by volume in litres (L) as this is generally the influencing factor when considering bin size, numbers and storage space required.

Waste generation volumes in litres per week (L/week) adopted for this waste assessment is shown Table 2-2. It is estimated that the Proposal will generate 11,905L of refuse and 2,520L of recyclables each week.

Table 2-2: Estimated Waste Generation

Tenancy Use Type	Number of Beds/Area (m ²)	Waste Generation Rate	Waste Generation (L/week)
Refuse			
Beds	247 Beds	5L/bed/day	8,645
Outdoor Dining/Bar	133m ²	50L/100m ² /day	465
Check In	50m ²	10L/100m ² /day	35
Kitchen	42m ²	667L/100m ² /day	1,956
Communal Dining/Kitchen	15m ²	667L/100m ² /day	714
Office	128m ²	10L/100m ² /day	90
Total			11,905
Recyclables			
Beds	247 Beds	1L/bed/day	1,729
Outdoor Dining/Bar	133m ²	50L/100m ² /day	465
Check In	50m ²	10L/100m ² /day	35
Kitchen	42m ²	50L/100m ² /day	147
Communal Dining/Kitchen	15m ²	50L/100m ² /day	54
Office	128m ²	10L/100m ² /day	90
Total			2,520

3 Waste Storage

Waste materials generated within the Proposal will be collected in the bins located in the Bin Storage Area, as shown in Diagram 1, and discussed in the following sub-sections.

3.1 Internal Transfer of Waste

To promote positive recycling behaviour and maximise diversion from landfill, internal bins will be available throughout the Proposal for the source separation of refuse and recycling.

These internal bins will be collected by the staff/cleaners and transferred to the Bin Storage Area for consolidation into the appropriate bins, as required. This internal servicing method may be conducted outside of main operational hours to mitigate disturbances to staff/visitors.

All bins will be colour coded and labelled in accordance with Australian Standards (AS 4123.7) to assist visitors, staff and cleaners to dispose of their separate waste materials in the correct bins.

3.2 Bin Sizes

Table 3-1 gives the typical dimensions of standard bins sizes that may be utilised at the Proposal. It should be noted that these bin dimensions are approximate and can vary slightly between suppliers.

Table 3-1: Typical Bin Dimensions

Dimensions (m)	Bin Sizes		
	240L	660L	1,100L
Depth	0.730	0.780	1.070
Width	0.585	1.260	1.240
Height	1.060	1.200	1.330

Reference: SULO Bin Specification Data Sheets

3.3 Bin Storage Area Size

To ensure sufficient area is available for storage of the bins, the amount of bins required for the Bin Storage Area was modelled utilising the estimated waste generation in Table 2-2, bin sizes in Table 3-1 and based on collection of refuse and recyclables three times each week.

Based on the results shown in Table 3-2 the Bin Storage Area has been sized to accommodate:

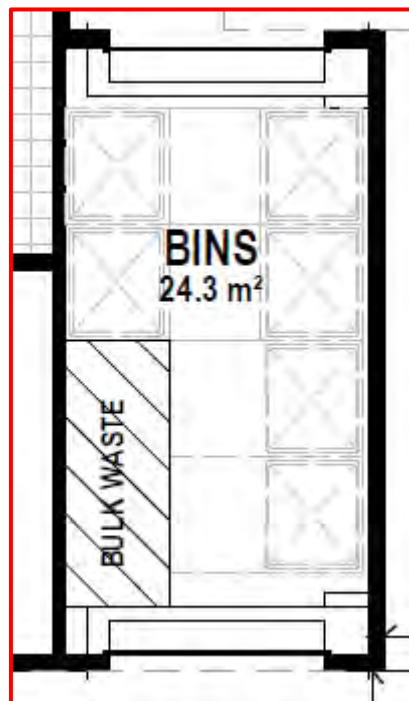
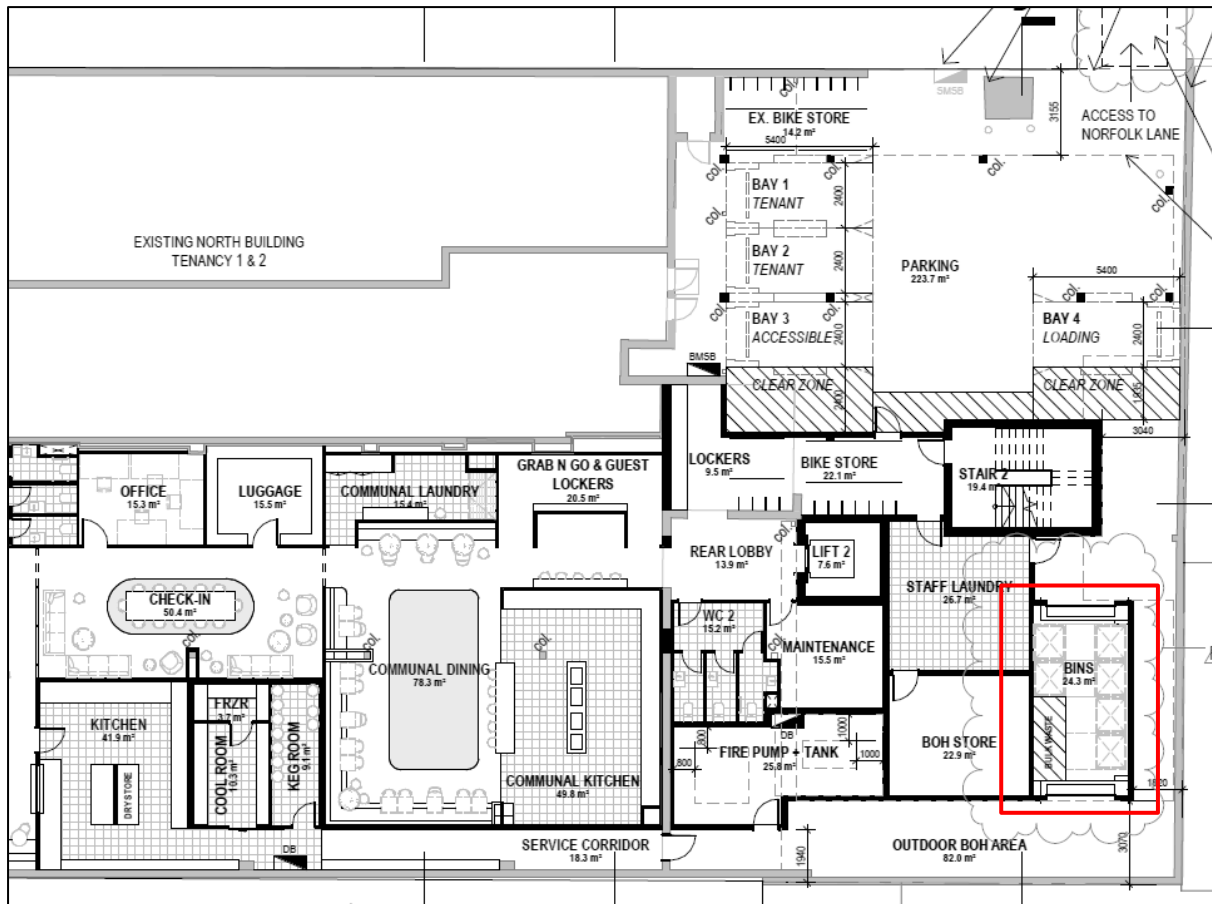
- Four 1,100L refuse bins; and
- One 1,100L recycling bin.

Table 3-2: Bin Requirements for Bin Storage Area

Waste Stream	Waste Generation (L/week)	Number of Bins Required		
		240L	660L	1,100L
Refuse	11,905	17	7	4
Recycling	2,520	4	2	1

The configuration of these bins within the Bin Storage Area is shown Diagram 1. It is worth noting that the number of bins and corresponding placement of bins shown in Diagram 1 represents the maximum requirements assuming three collections each week of refuse and recyclables.

Diagram 1: Bin Storage Area



3.4 Bin Storage Area Design

The design of the Bin Storage Area will take into consideration:

- Smooth impervious floor sloped to a drain connected to the sewer system;
- Taps for washing of bins and Bin Storage Area;
- Adequate aisle width for easy manoeuvring of bins;
- No double stacking of bins;
- Doors to the Bin Storage Area self-closing and vermin proof;
- Doors to the Bin Storage Area wide enough to fit bins through;
- Ventilated to a suitable standard;
- Appropriate signage;
- Undercover where possible and be designed to not permit stormwater to enter the drain;
- Located behind the building setback line;
- Bins not to be visible from the property boundary or areas trafficable by the public; and
- Bins are reasonably secured from theft and vandalism.

Bin numbers and storage space within the Bin Storage Area will be monitored by building management during the operation of the Proposal to ensure that the number of bins and collection frequency is sufficient.

4 Waste Collection

A private waste collection contractor will service the Proposal and provide four 1,100L bins for refuse and one 1,100L bin for recyclables.

The private contractor will collect refuse and recyclables three times each week utilising a rear loader waste collection vehicle.

The private contractor's rear loader waste collection vehicle will service the bins from the Right of Way off the Norfolk Lane. The private contractor's rear loader waste collection vehicle will travel with left hand lane traffic flow on Norfolk Lane and reverse into the Right of Way for servicing.

Servicing may be conducted outside of normal operating hours to mitigate impacts on local traffic movements during peak traffic hours.

Building management will ferry bins to and from the Bin Holding Area and the Bin Storage Area on collection days to accommodate servicing by the private contractor, refer Diagram 2.

Private contractor's staff will ferry bins to and from the Bin Holding Area and the rear loader waste collection vehicle on collection days, refer Diagram 3.

Once servicing is complete the private contractor's rear loader waste collection vehicle will exit the Right of Way in forward gear and continue in a forward motion along Norfolk Lane moving with traffic flow.

The above servicing method will preserve the amenity of the area by removing the requirement for bins to be presented to the street on collection days. In addition, servicing of bins onsite will reduce the noise generated in the area during collection. Noise from waste vehicles must comply with the Environmental Protection (Noise) Regulations and such vehicles should not service the site before 7.00am or after 7.00pm Monday to Saturday, or before 9.00am or after 7.00pm on Sundays and Public Holidays.

Diagram 2: Bin Transfer Path from Bin Storage Area to Bin Holding Area

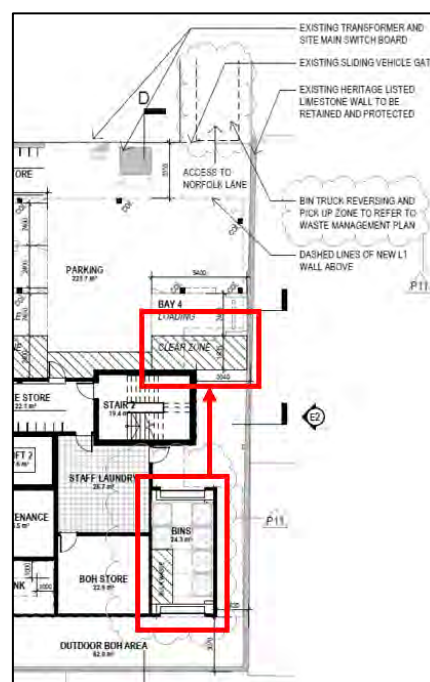
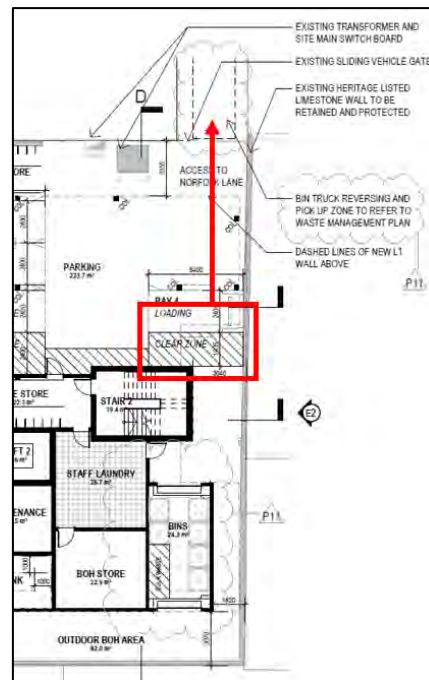


Diagram 3: Bin Transfer Path from Bin Holding Area to Right of Way



4.1 Bulk and Speciality Waste

Adequate space has been allocated within the Bin Storage Area for collection and storage of bulk and specialty wastes that are unable to be disposed of within the bins in the Bin Storage Area, such as:

- Refurbishment wastes from fit outs;
- Mattresses;
- Batteries and E-wastes;
- White goods/appliances;
- Used cooking oil;
- Cleaning chemicals; and
- Commercial light globes.

These materials will be removed from the Proposal once sufficient volumes have been accumulated to warrant disposal, as monitored by building management who will organise their transport to the appropriate waste facility, as required.

5 Waste Management

Building management will be engaged to complete the following tasks:

- Monitoring and maintenance of bins and the Bin Storage Area;
- Cleaning of bins and Bin Storage Area, when required;
- Ferrying bins to and from the Bin Storage Area and the Bin Holding Area;
- Ensure all staff/cleaners at the Proposal are made aware of this WMP and their responsibilities thereunder;
- Monitor staff/cleaner behaviour and identify requirements for further education and/or signage;
- Monitor bulk and speciality waste accumulation and assist with its removal, as required;
- Regularly engage with staff/cleaners to develop opportunities to reduce waste volumes and increase resource recovery; and
- Regularly engage with the private contractor to ensure efficient and effective waste service is maintained.

6 Conclusion

As demonstrated within this WMP, the Proposal provides a sufficiently sized Bin Storage Area for storage of refuse and recyclables based on the estimated waste generation volumes and suitable configuration of bins. This indicates that an adequately designed Bin Storage Area has been provided, and collection of refuse and recyclables can be completed from the Proposal.

The above is achieved using:

- Four 1,100L refuse bins, collected three times each week; and
- One 1,100L recycling bin, collected three times each week.

A private contractor will service the Proposal from the Right of Way. The private contractor will transfer bins to and from the Bin Holding Area and the waste collection vehicle for servicing.

Building management will oversee the relevant aspects of waste management at the Proposal.

Figures

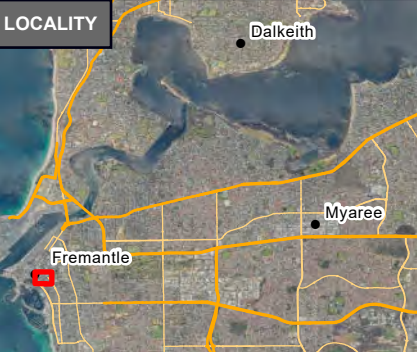
Figure 1: Locality Plan



LEGEND

- Site Boundary
- Cadastre**
- Crown Allotment
- Freehold
- Road
- Strata Plan or Lot
- Easement

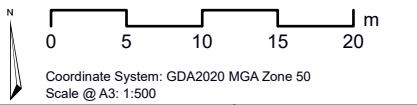
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LOCALITY

19 Essex Street
Fremantle WA 6160

Kvarken Pty Limited



Prepared:	E Jackson	Date:	31/07/2024
Reviewed:	D Patel	Revision:	A
Project:	WMP24011		



Figure 01

Data source: Roads, Cadastre - Landgate, 2024, Imagery: Nearmap, 2024



Assets | Engineering | Environment | Noise | Spatial | Waste

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BELINGBAK

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FREMANTLE**

**DEVELOPMENT APPLICATION
ACOUSTIC REPORT**

AUGUST 2024

OUR REFERENCE: 33221-1-24296

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LYLO
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APPENDICIES

A	DA Plans
B	Preliminary Mechanical Services Drawings

1.0 INTRODUCTION

Herring Storer Acoustics was commissioned by Belingbak, to conduct a preliminary review of the proposed Lylo Hotel at 19 Essex Street, Fremantle.

The proposed development consists of short stay accommodation comprising 80 rooms with 247 beds and associated amenities.

The existing heritage building is proposed to be retained, with partial internal demolition and extended.

This report has been based on the Development Application drawings provided.

2.0 CRITERIA

2.1 BCA PROVISIONS

For Class 2 or 3 buildings, Part F5 of the National Construction Code (NCC), outlines the minimum acoustic isolation of apartments, and in this instance hotel rooms.

The following summarises the acoustic criteria:

2.1.1 Walls

Wet to wet	$R_W + C_{tr}$ not less than 50 dB.
Living to living	$R_W + C_{tr}$ not less than 50 dB.
Wet to living	$R_W + C_{tr}$ not less than 50 dB plus discontinuous construction.
Kitchens to living	$R_W + C_{tr}$ not less than 50 dB plus discontinuous construction.

Note: Where kitchens are part of an open living area, we consider the kitchen to be part of the living area and in these cases a discontinuous construction is required. This also includes cases where kitchens are back-to-back, however, discontinuous construction is only required on one side.

2.1.2 Floors

Floors	$R_W + C_{tr}$ not less than 50 dB.
Impact Isolation	$L_{n,w}$ not more than 55 dB is recommended.

Note: The impact isolation criteria under the BCA is an $L_{n,w}$ of not more than 62 dB. However, as a member firm of the Association of Australasian Acoustic Consultants, (AAAC) we recommend a criteria of an $L_{n,w}$ of not more than 55 dB be adopted for a development of this type.

2.1.3 Service Risers

to Habitable Rooms	$R_W + C_{tr}$ not less than 40 dB.
to Non-Habitable Rooms	$R_W + C_{tr}$ not less than 25 dB.

2.1.4 Hydraulics

The above requirements also apply to storm water down pipes.

2.1.5 Doors

Door (Connecting) R_w not less than 30 dB.

2.2 ENVIRONMENTAL PROTECTION (NOISE) REGULATIONS 1997

The *Environmental Protection (Noise) Regulations 1997* stipulate the allowable noise levels at any noise sensitive premises from other premises. The allowable or assigned noise levels for noise sensitive premises are determined by the calculation of an influencing factor, which is added to the baseline criteria set out in Table 1 of the Regulations. The baseline assigned noise levels are listed in Table 3.1. For commercial premises, the allowable or assigned noise levels are the same for all hours of the day. Table 3.1 also lists the assigned noise levels for commercial premises.

TABLE 3.1 – ASSIGNED NOISE LEVELS

Premises Noise	Receiving	Time of Day	Assigned Level (dB)		
			L_{A10}	L_{A1}	L_{Amax}
Noise sensitive premises within 15 metres of a dwelling		0700 - 1900 hours Monday to Saturday	45 + IF	55 + IF	65 + IF
		0900 - 1900 hours Sunday and Public Holidays	40 + IF	50 + IF	65 + IF
		1900 - 2200 hours all days	40 + IF	50 + IF	55 + IF
		2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and Public Holidays	35 + IF	45 + IF	55 + IF
Commercial Premises		All hours	60	75	80

Note: The L_{A10} noise level is the noise that is exceeded for 10% of the time.
The L_{A1} noise level is the noise that is exceeded for 1% of the time.
The L_{Amax} noise level is the maximum noise level recorded.

It is a requirement that noise from the site be free of annoying characteristics (tonality, modulation and impulsiveness) at other premises, defined below as per Regulation 9.

“impulsiveness” means a variation in the emission of a noise where the difference between L_{Apeak} and $L_{Amax Slow}$ is more than 15dB when determined for a single representative event;

“modulation” means a variation in the emission of noise that –

- (a) is more than 3dB $L_{A Fast}$ or is more than 3dB $L_{A Fast}$ in any one-third octave band;
- (b) is present for more at least 10% of the representative assessment period; and
- (c) is regular, cyclic and audible;

“tonality”

means the presence in the noise emission of tonal characteristics where the difference between –

- (a) the A-weighted sound pressure level in any one-third octave band; and
- (b) the arithmetic average of the A-weighted sound pressure levels in the 2 adjacent one-third octave bands,

is greater than 3 dB when the sound pressure levels are determined as $L_{Aeq,T}$ levels where the time period T is greater than 10% of the representative assessment period, or greater than 8 dB at any time when the sound pressure levels are determined as $L_{A\ slow}$ levels.

Where the above characteristics are present and cannot be practicably removed, the following adjustments are made to the measured or predicted level at other premises.

TABLE 3.2 – ADJUSTMENTS FOR ANNOYING CHARACTERISTICS

Where tonality is present	Where modulation is present	Where impulsiveness is present
+ 5 dB	+ 5 dB	+ 10 dB

From a review of the development, the influencing factor for this development would be +7 dB, based on the following :

Secondary Roads within inner circle;

South Terrace	+ 2 dB
Norfolk Street	+ 2 dB

Commercial Premises within inner circle;

60%	+ 3 dB
-----	--------

Commercial Premises within outer circle;

20%	+ 1 dB
-----	--------

Total IF

+ 8 dB

Hence the influencing factor would be + 8 dB and the assigned noise levels would be as listed in Table 3.3.

TABLE 3.3 - ASSIGNED OUTDOOR NOISE LEVEL

Premises Noise	Receiving	Time of Day	Assigned Level (dB)		
			L_{A10}	L_{A1}	L_{Amax}
Noise sensitive premises within 15 metres of a dwelling		0700 - 1900 hours Monday to Saturday	53	63	73
		0900 - 1900 hours Sunday and Public Holidays	48	58	73
		1900 - 2200 hours all days	48	58	63
		2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and Public Holidays	43	53	63

Note: L_{A10} is the noise level exceeded for 10% of the time.
 L_{A1} is the noise level exceeded for 1% of the time.
 L_{Amax} is the maximum noise level.

We note that noise emissions from the premises need to comply with the requirements of the *Environmental Protection (Noise) Regulations 1997*. This includes noise associated with mechanical services (ie air conditioning and ventilation systems).

2.3 NOISE INGRESS

There are several noise ingress policies that may be applicable to this development, consisting of :

Fremantle Sound Attenuation Policy
Fremantle Port Buffer Area Development Guidelines.

Both the Fremantle Sound Attenuation Policy and the WAPC State Planning Policy result in the same criteria, as listed below :

- L_{eq} 35 dB(A) in sleeping areas (bedrooms); and
- L_{eq} 40 dB(A) in living/work areas and other habitable rooms.

The Fremantle Port Policy dictates minimum construction requirements based on the proximity to the Fremantle Port. The development is located in "Area 3" in accordance with the Port Buffer Area Development Guidelines. "Area 3" does not dictate any increase in built form for development.

3.0 BCA REQUIREMENTS

The proposed development will be constructed to comply with the requirements of Part F5 of the NCC.

During the design development phase of the project, confirmation from a NCC/BCA certifier as to which areas of the development classify as individual sole-occupancy units will need to be provided, as this will dictate the construction requirements between spaces. This is not seen as problematic, and typical for such a development.

4.0 NOISE INGRESS

During the design development phase of the project, ambient noise levels in the vicinity will be recorded, with the resulting noise levels informing the façade constructions required to ameliorate noise levels internally such that the applicable criteria are met.

The façade considerations are likely to be primarily glazing considerations – including the roof skylights that are proposed.

5.0 NOISE FROM DEVELOPMENT

The main source of noise from the proposed development will be from mechanical services consisting of air-conditioning plant. Noise received at neighbouring premises from these items need to comply with the assigned noise levels as determined under the *Environmental Protection (Noise) Regulations 1997*.

5.1 MECHANICAL SERVICES

The main source of noise from the proposed development will be from mechanical services consisting of air-conditioning plant. Noise received at neighbours from these items need to comply with the assigned noise levels as determined under the *Environmental Protection (Noise) Regulations 1997*.

As the mechanical services could operate during the night, noise emissions from the development needs to comply with the assigned L_{A10} night period noise level of 43 dB(A) at residential premises. Potentially, noise emissions from mechanical services could be tonal, in which case an +5 dB(A) penalty for a tonal component could be applied to the resultant noise levels. Therefore, the design level at the neighbouring residential premises would be 37 L_{A10} dB.

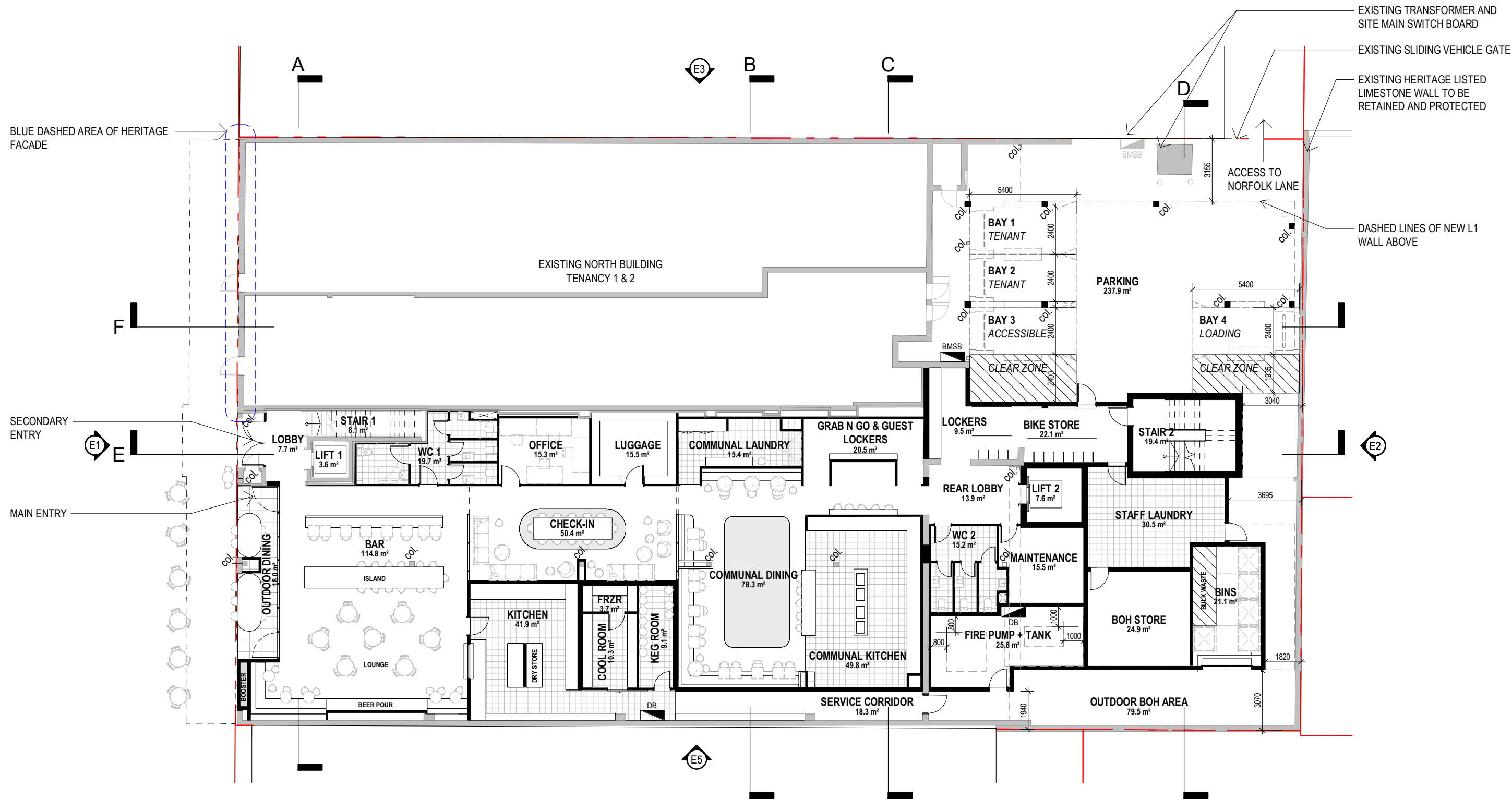
The preliminary mechanical design has been provided and included in Appendix B.

Given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.

A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.

APPENDIX A

DA Plans



WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

Revisions	P4	04.09.2024	CONCEPT COORDINATION MEETING
	P5	22.04.2024	ISSUE FOR INFORMATION
	P6	24.04.2024	DAC DRAFT PACK
	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1

Project / **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing / **Ground Floor Plan**

Project No / **223252** Author / **RL** Scale: @ A3 / **1 : 200**

Drawing No. / **DA01.04**

P9

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5/06/2024 1:27:37 PM

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EXISTING SEPARATION FROM
HERITAGE FACADE TO PRIMARY
WALL

BLUE DASHED AREA OF HERITAGE
FACADE

GREY DASHED LINES
INDICATIVE LOCATION OF
EXISTING TRUSSES OVERHEAD

NEW WALL TO EXTEND ABOVE
EXISTING MASONRY WALL -
ALIGN DATUM WITH NORTHERN
TENANCY HERITAGE FACADE
WALL

ORANGE DASHED LINES OF
GROUND FLOOR WALL BELOW



WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

Revisions	P4	04.09.2024	CONCEPT COORDINATION MEETING
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	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **First Floor Plan**

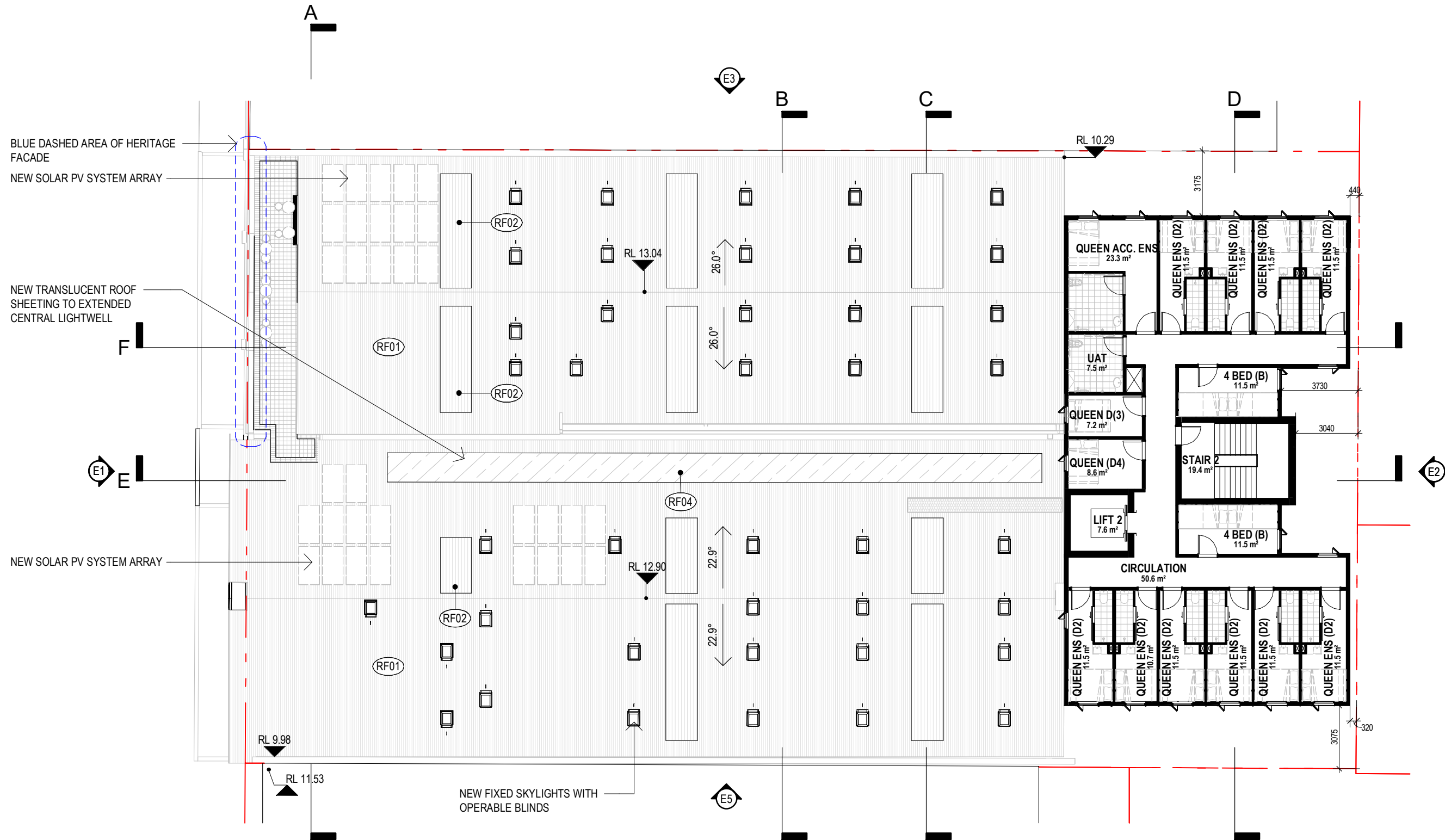
Project No **223252** Author **RL** Scale: @ A3 **1 : 200**

Drawing No **DA01.05**

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

Revisions	P3	04.09.2024	CONCEPT COORDINATION MEETING
	P4	22.04.2024	ISSUE FOR INFORMATION
	P5	24.04.2024	DAC DRAFT PACK
	P6	29.04.2024	DAC ISSUE
	P7	04.06.2024	DA ISSUE
	P8	05.06.2024	DA ISSUE 1

Project / **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing / **L2-L3 Plan**

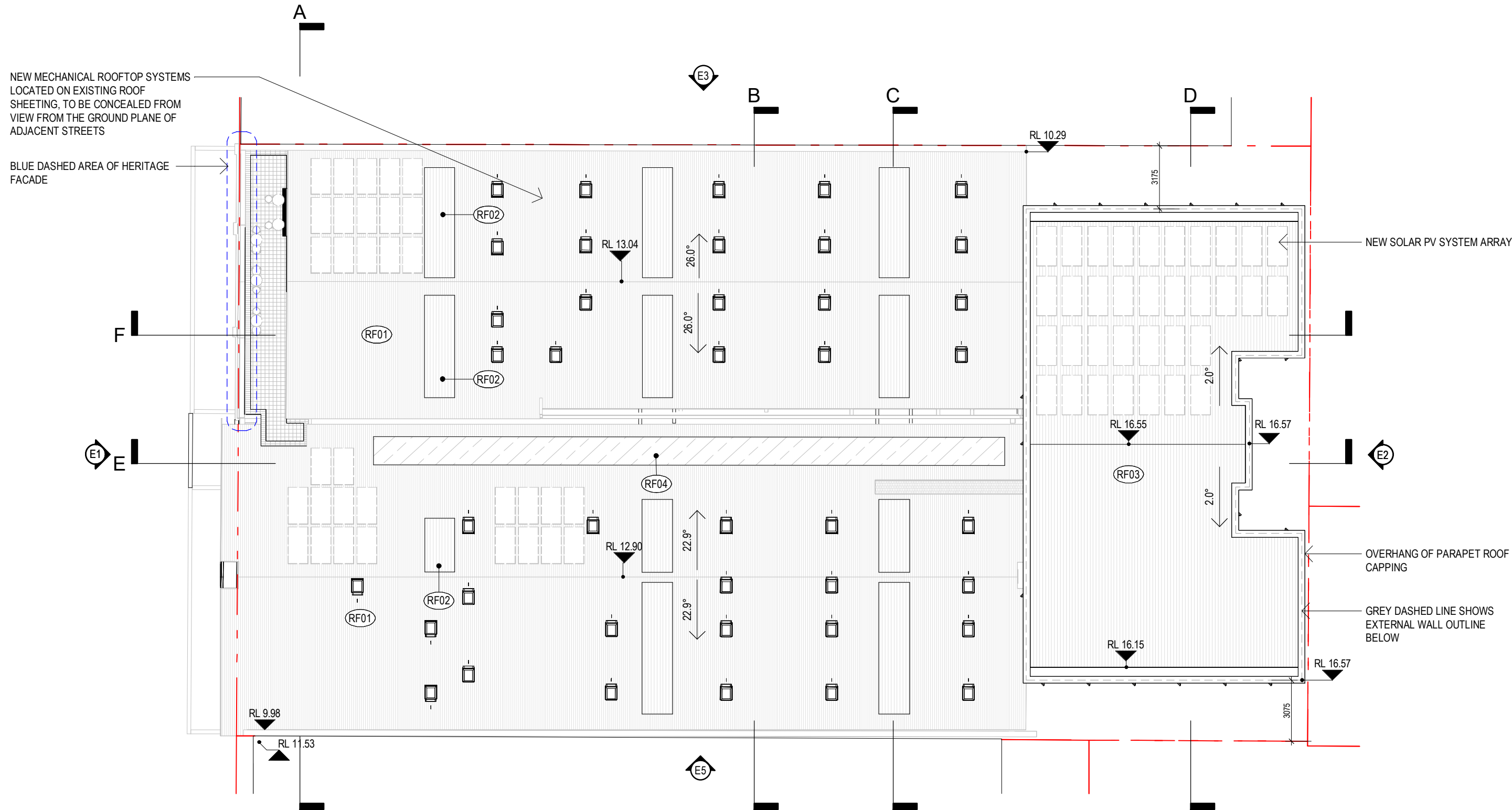
Project No / **223252** Author / **AL** Scale: @ A3 / **1 : 200**

Drawing No. / **DA01.06**

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

Revisions	P4	04.09.2024	CONCEPT COORDINATION MEETING
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	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1

Project / **LyLo Fremantle**

19 ESSEX STREET, FREMANTLE, WA

Drawing / **Proposed Roof Plan**

Project No / **223252**

Author / **AL**

Scale: @ A3 / **1 : 200**

Drawing No. / **DA01.07**

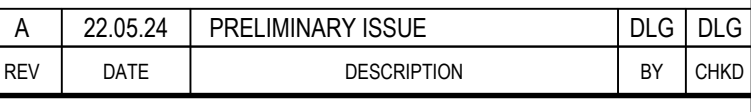
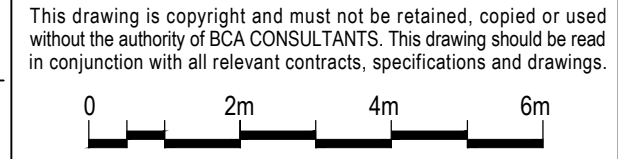
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APPENDIX B

Preliminary Mechanical Services Drawings



LIENT

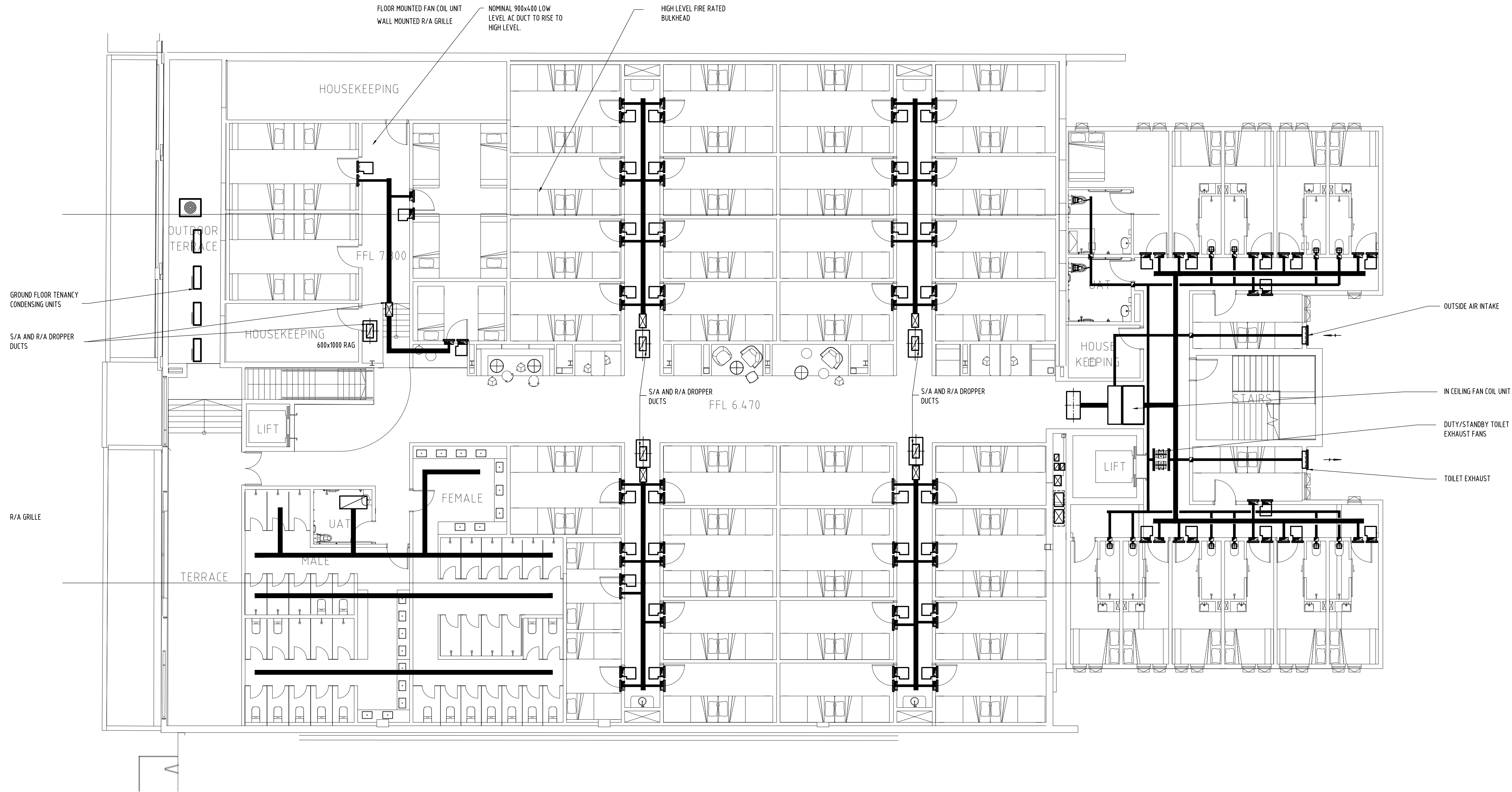
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www.bcagroup.com.au

PROJECT
LYLO FREMANTLE
19 ESSEX ST, FREMANTLE WA

RAWN DLG	DESIGNED DLG	REVIEWED .	APPROVED .	DATE MAY 2024	SCALE 1:100 @ A1
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CA PROJECT No.	DRAWING No.	REV
230341	M01	A



A	22.05.24	PRELIMINARY ISSUE	DLG	DLG
REV	DATE	DESCRIPTION	BY	CHKD

PRELIMINARY ISSUE

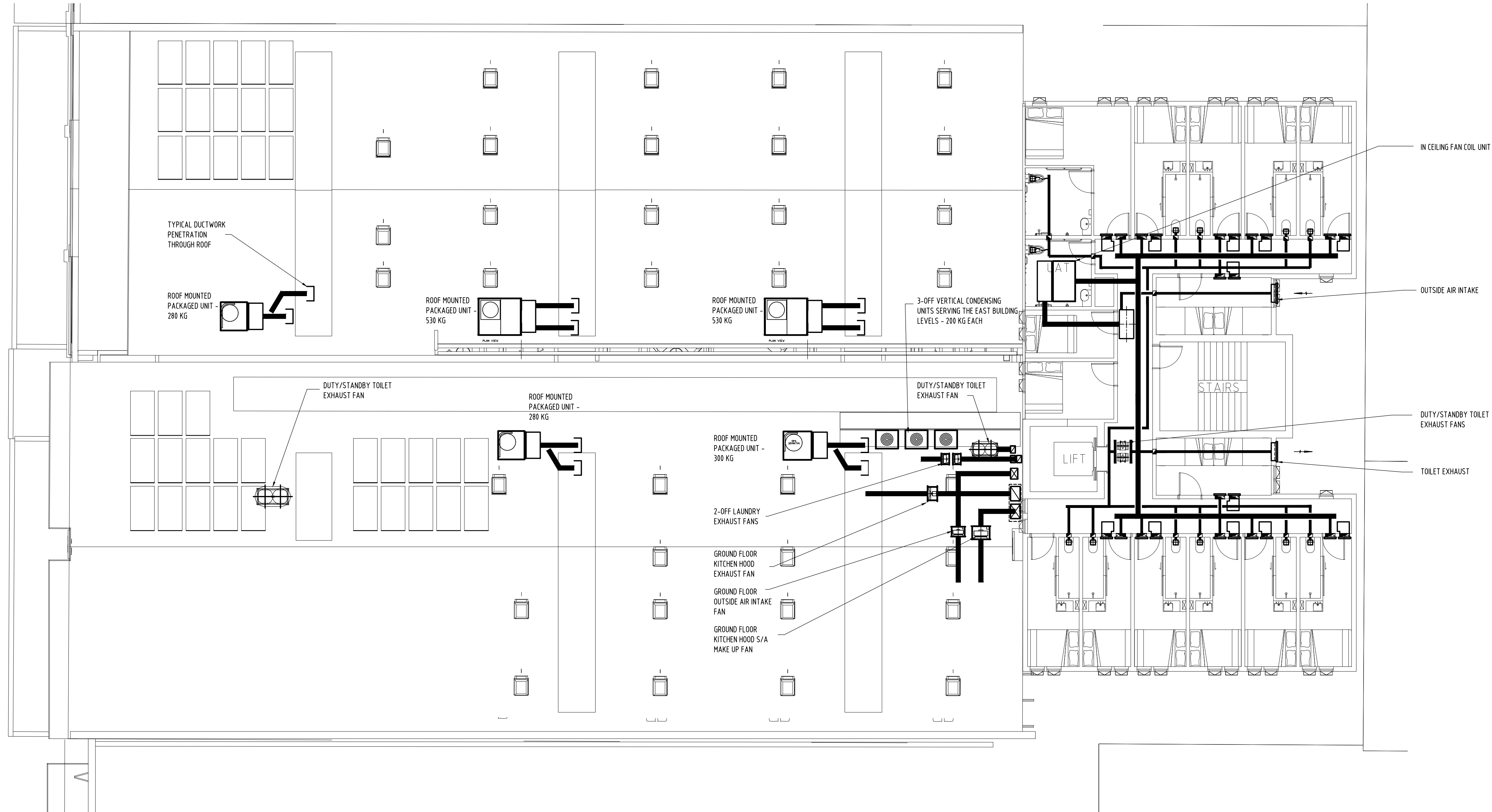


DISCIPLINE MECHANICAL SERVICES

PROJECT
LYLO FREMANTLE
19 ESSEX ST, FREMANTLE WA

DRAWING
FIRST FLOOR AIR
CONDITIONING AND
VENTILATION LAYOUT

DRAWN DLG	DESIGNED DLG	REVIEWED DLG	APPROVED DLG	DATE MAY 2024	SCALE 1:100 @ A1
BCA PROJECT No. 230341			DRAWING No. M02		REV A



A	22.05.24	PRELIMINARY ISSUE	DLG	DLG
REV	DATE	DESCRIPTION	BY	CHKD

PRELIMINARY ISSUE

CLIENT

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DISCIPLINE MECHANICAL SERVICES

PROJECT
LYLO FREMANTLE
19 ESSEX ST, FREMANTLE WA

DRAWING
ROOF PLAN

DRAWN	DESIGNED	REVIEWED	APPROVED	DATE	SCALE
DLG	DLG	.	.	MAY 2024	1:100 @ A1
BCA PROJECT No.		DRAWING No.		REV	
230341		M03		A	

DEVELOPMENT APPLICATION

Proposed Tourist Development

19 Essex Street, Fremantle

Planning Report

June 2024



Developed acknowledges Aboriginal and Torres Strait Islander people as the traditional custodians of the land on which we deliver our services. We pay our respects to elders and leaders past, present and emerging.

Project Team



Owner and Operator

rothelowman

Architect

BELINGBAK

Development Manager

Developed

Town Planning

Developed – Planning | Spatial | Delivery

Developed

Spatial & Survey

Developed – Planning | Spatial | Delivery



Traffic Consultant



Sustainability



Heritage Consultant



Waste Management

Contents

1.0	Introduction	4
2.0	Development Overview	5
3.0	State Planning Framework	11
4.0	Local Planning Framework	12
5.0	Design Performance	19
6.0	Technical Considerations	28
7.0	Conclusion	30

Attachments

- 1. Development Plans**
- 2. Design Report**
- 3. Heritage Impact Statement**
- 4. Sustainability Report**
- 5. Traffic Impact Statement**
- 6. Waste Management Plan**

Issue	Date	Status	Prepared By	Approved By	Circulated
V1	05/06		MC MC	CC	Internal
Final	10/06			DP	Client

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- a third party's use of, or reliance upon, this report;
- use of, or reliance upon, this report in relation to any land other than the subject site; or
- the Client's implementation, or application, of the strategies recommended in this report.

1.0 Introduction

This report has been prepared by Developed – Planning | Spatial | Delivery on behalf of the landowners and development proponents for 19 Essex Street, Fremantle for a proposed Tourist Development – providing short-stay accommodation comprising 80 rooms with 247 beds and associated amenities.

The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development proposal is entirely consistent with the planning aims for this zone, and is highly compatible with the surrounding context of the local area.

The unique concept, by LyLo, will bring innovative, affordable, short stay accommodation to the Fremantle city centre, adding to the vibrancy of the city and attracting more tourists. Short-stay guests will have a range of amenities to enjoy both within and surrounding the development.

The development has been thoughtfully designed by acclaimed architects, RotheLowman, to carefully address the site's locational attributes, maximising the cultural and heritage significance of Fremantle. The design has been supported by a comprehensive landscape strategy by RotheLowman and expert heritage advice from Urbis. Design Advisory Committee input has also been sought through the design development / pre-DA-lodgement stage.

Sustainability will be delivered through an environmentally sensitive design and operation of the building, as well as adaptive heritage building re-use and facilitating alternative transport options. The development is ideally located within walking distance of a wide range of tourist amenities, as well as easily accessed by public transport, significantly reducing the need for on-site car parking bays.

Expert technical input has been obtained on planning, design and operational matters, including input from heritage, sustainability, transport and waste management consultants. Supporting technical reports are provided with the development application.

This planning report provides a comprehensive planning assessment against the state and local government planning frameworks, and details performance against applicable objectives and requirements. This will demonstrate that the proposal is an appropriate built form, a quality design, has strong sustainability benefits, and is a suitable land use in this location; resulting in an overall development proposal that is appropriate for development approval.



2.0 Development Overview

2.1 Development Site

Property Address	19 Essex Street, Fremantle
Title Details	Lot 8, Certificate of Title 1890/591
Lot Size	1,616m ²
Current Use	Commercial
Relevant Encumbrances	Not Applicable
Owner	Kvarken Pty Ltd
Local Government	City of Fremantle

The subject development site is located at 19 Essex Street, Fremantle within the City of Fremantle local government area.

The development site is located within the City of Fremantle central city area and is approximately 650m from the Fremantle Train Station. The site sits between major tourist attractions, including South Terrace, the Fremantle Markets, and Fishing Boat Harbour; making the site an ideal location for tourist accommodation.

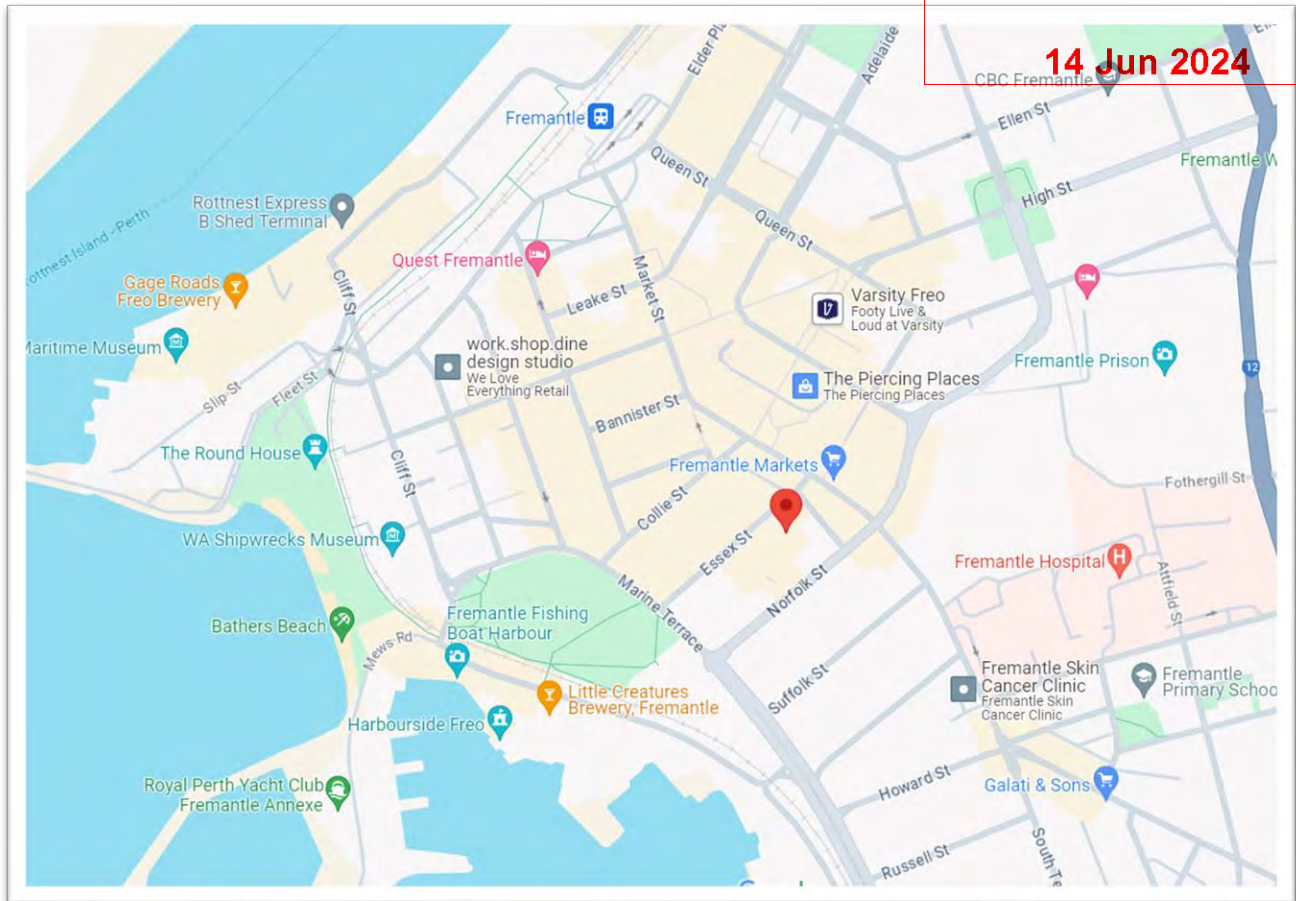
The 1,616m² development site is located on the south-western side of Essex Street. The site is currently developed with a two (2) storey multi-tenancy commercial building. The building includes a bar, offices and a shop. The site also has rear access via a service laneway.

The site is listed on the local government statutory Heritage List; however, the original building has been significantly modified over time. The development application proposes that the building will be retained, adapted with partial internal demolition, and extended.

The development site is surrounded by commercial development, which includes short stay accommodation, restaurants and offices. Surrounding buildings are predominately one to two storeys, with a number of heritage and character buildings.



14 Jun 2024



Site Context Plan



Site Location Aerial View





Site Street View



Service Laneway



2.2 Development Proposal

Property Address	19 Essex Street, Fremantle
Proposed Development	Tourist Development – short stay accommodation comprising 80 rooms with 247 beds, with associated amenities, and ancillary small bar.
Land Use	Tourist Development
Building Height	Four Storeys
Parking	3 car bays, 1 loading bay, 13 bicycle spaces
Cost of Development	\$8.0 million

The application proposes development of a four-storey tourist development, including the following elements:

- Adaptive re-use of the existing two-storey heritage listed commercial building, including internal and external modifications and partial demolition.
- Four storey rear addition to the retained building.
- Ground floor: public bar/café, guest check-in, guest amenities, and back of house.
- Levels 1 to 3: 80 rooms with 247 beds, and shared bathrooms.
- External rear service area for parking and deliveries.
- Three (3) car bays, one loading bay, thirteen (13) bicycle spaces, and end of trip facilities.
- Extensive landscaping, with soft and hardscape areas to internal communal spaces, the Level 1 terrace, and the public bar / café.
- Sustainability features, including adaptive building re-use, natural ventilation, solar PVs, and reduced car parking.

14 Jun 2024



Essex Street Adaptive Re-Use

(Source: RotheLowman)



New Rear Addition

(Source: RotheLowman)



2.3 Planning Framework

Property Address	19 Essex Street, Fremantle
Region Scheme	Metropolitan Region Scheme
Region Scheme - Zone	Central City Area
Local Planning Scheme	City of Fremantle Local Planning Scheme No.4
Local Scheme - Zone	City Centre
Structure Plan / LPD	N/A
Special Control Area	LPA 1 – Fremantle
Bushfire Prone Area	No
Heritage Listed Site	Yes
State Planning Policies	SPP 7.0 - Design of the Built Environment
Local Planning Policies	LPP 1.6 - Heritage Assessment and Protection LPP 2.3 - Fremantle Port Buffer Area Development Guidelines LPP 3.6 - Heritage Areas

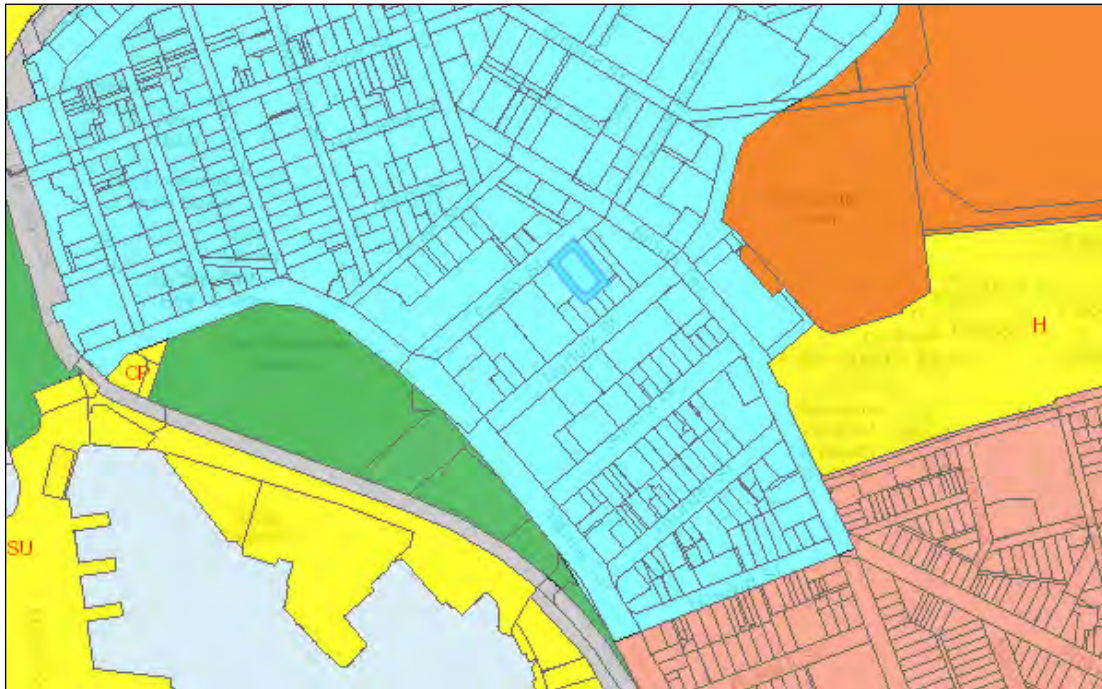


Extract of Local Planning Scheme Map

3.0 State Planning Framework

3.1 Region Scheme

The development application is consistent with the Central City Area zoning of the development site under the Metropolitan Region Scheme.



Extract of Region Scheme Map

3.2 State Planning Policies

State Planning Policy 7.0 - Design of the Built Environment (SPP 7.0)

The development proposal has been designed by highly experienced architects, RotheLowman, with input from heritage, sustainability, and other technical consultants, to produce a high quality design; which is visually attractive, responds to the local context and character, and has high levels of amenity. The development application is highly consistent with the 10 principles of good design, as set out in SPP 7.0.

In addition, the draft plans were submitted to the City of Fremantle Design Advisory Committee for feedback and recommended improvements, prior to finalisation of the development application plans.

Please refer to Section 5 – Design Performance of this report and **Attachment 2 – Design Report** for a detailed response to the ten design principles of SPP 7.0.

4.0 Local Planning Framework

4.1 Local Planning Scheme

Scheme Aims

The development application is entirely consistent with the Scheme Aims, as set out in clause 1.6 of the City of Fremantle Local Planning Scheme No. 4 (LPS4). The proposed development does not conflict with any of the Scheme Aims, and helps to achieve the following specific Aims:

- *To accommodate a diverse mix of people, cultures and lifestyles.*
- *To ensure development promotes a sense of community and encourages participation in community life.*
- *To protect and conserve Fremantle's unique cultural heritage.*
- *To ensure all development complements and contributes to the community's desired identity and character for Fremantle.*
- *To develop a diverse and attractive city centre that functions as a town centre and a regional centre.*
- *To encourage the development of business and employment generating activities in appropriate locations.*
- *To ensure urban form and development contribute to sustainability (environmental, social/cultural and economic).*
- *To integrate planning for land use and transport to achieve sustainable urban development.*
- *To reduce reliance on, and the impact of, private motor vehicles.*
- *reduce the demand for, and balance the provision of parking, to ensure convenient access while promoting economic, environmental and social sustainability.*
- *encourage the use of public transport and promote Fremantle as a major public transport node.*
- *promote and enhance the pedestrian and cycling transport modes.*

Land Use

The development proposes to undertake alterations and additions to an existing building at 19 Essex Street, Fremantle and to operate a 'Tourist Development' land use. This is a 'D' Discretionary land use under the City Centre zoning of the development site.

A 'Tourist Development' land use is entirely consistent with the zone objectives set out in clause 3.2.1(b) of LPS4 for the City Centre zone, as follows:

Provides for a full range of shopping, office, administrative, social, recreation, entertainment and community services, consistent with the region-serving role of the centre and including residential uses.

- The proposed development provides short stay residential accommodation with recreation and entertainment amenities, which is entirely appropriate for a city centre. The hotel guests are also anticipated to support surrounding businesses such as restaurants and retail outlets, which will continue to support the viability and activation of the city centre.

Comply with the objectives of local planning area 1 of schedule 7

- The proposed development is consistent with the building height requirements for area 1.3.1 of schedule 7.

Conserve places of heritage significance the subject of or affected by development

- The development proposal has carefully considered the heritage significance of the existing building and the heritage context of the local area. This has included advice from an expert heritage consultant and an architectural team with extensive heritage experience. The Heritage Impact Statement demonstrates that the proposed works have prioritised the retention of elements that are of primary significance, such as the Essex Street masonry façade, to ensure these features are conserved and improved as part of the adaptive re-use process, and will continue to contribute to the cultural heritage and character of the streetscape.

Please refer to **Attachment 3 – Heritage Impact Statement** for further details.

Small Bar

In addition to the Tourist Development land use proposed, the development proposes an additional ancillary 'Small Bar' land use. The Small Bar is essentially part of the Tourist Development, as it will be predominantly for guests of the property. However, similar to 'Hotel' accommodation developments, the small bar would be open to the public – subject to approval of an appropriate liquor licence and relevant conditions on that licence to ensure appropriate operation and management of the bar.

Operation of the Land Use

Hours

LyLo Fremantle will be accessible 24hr a day for guests staying in the property. Guests will be required to use their room keys to access the building outside of normal operating hours (0600-2200). The check in/out desk is manned 24hrs per day.

Security

Access to the accommodation floor and communal area is restricted to staying guests who are required to use their room key to gain access.

Guest arrival

Check in is proposed to be from 3pm using the self-service kiosks on the reception desk. LyLo staff are onsite 24hrs a day to assist guests with checking into and out of the property.

Guest amenities

Guests have access to communal areas, including a self-service kitchen, co-working spaces, and wellness areas across the property.

Public Bar/Café

The bar area is designed to be open to members of the public and LyLo in-house guests 7 days a week. We are yet to confirm the operating hours - which can be negotiated with the City as a condition of approval.

Vehicle Parking

The applicable vehicle parking standards under Table 2 of LPS 4 are set out below:

Use Class	Car Parking Bays	Delivery Bays	Bicycle Racks
Tourism	1:4 rooms	1: Reception	1:4 rooms
Development	= 20 car bays		= 20 bike spaces
Proposed	3 car bays	1 delivery bay	13 bike spaces

Pursuant to Clause 4.7.3 of LPS4, a reduction in the parking requirements is proposed, due to the following reasons and justification:

- The proposed development is targeted at young / young at heart, interstate and international travellers, who are budget and environmentally conscious, who are very unlikely to be travelling to the Perth Metropolitan Region via car, and are unlikely to be hiring a car for the duration of their stay within Fremantle.
- The site has been specifically selected by Lylo - the developer and operator due to its excellent central location, where hotel guests can walk to a wide variety of amenities, dining, entertainment, and leisure opportunities, and do not need a car.
- The site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:
 - Fremantle Train Station (650m)
 - Several bus services from South Terrace (60m)
 - Taxi Rank South Terrace (75m)
 - Ride share options, such as Uber and Didi
 - One-off care hire options, which are parked off-site, such as Car Share.
- The site includes sufficient bicycle parking facilities, and it is expected that the operators will provide loan bicycles to hotel guests as they would be unlikely to arrive with their own.
- The adaptive reuse of the building limits the capacity for additional parking to be included as part of this development application.

- Reduction in on-site parking reduces traffic impacts to the congested city centre.
- There is a considerable amount of public parking available close to the development site.

Please refer to **Attachment 5 Transport Impact Statement** for further information.

Matters for Due Regard

The following table sets out an assessment of the development application against clause 67 Matters for Due Regard of the Local Planning Scheme 'Deemed Provisions' - which apply through LPS4:

Matters of Due Regard	Comment	
(a) the aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area;	Meets the Scheme Aims, as noted above.	✓
(b) the requirements of orderly and proper planning;	The development proposal, including the land uses, built form, design detail, landscape, and technical considerations are consistent with the principles of orderly and proper planning.	✓
(c) any approved State planning policy;	The proposal is generally consistent with relevant SPPs, particularly SPP 7.0, as noted below.	✓
(d) any environmental protection policy approved under the Environmental Protection Act 1986 section 31(d);	N/A.	
(e) any policy of the Commission;	The proposal is generally consistent with relevant SPPs.	✓
(f) any policy of the State;	N/A	
(fa) any local planning strategy for this Scheme endorsed by the Commission;	The proposal is generally consistent with the City's Local Planning Strategy.	✓
(g) any local planning policy for the Scheme area;	The proposal is generally consistent with relevant LPPs, as noted below.	✓
(h) any structure plan or local development plan that relates to the development;	N/A	N/A
(i) any report of the review of the local planning scheme;	N/A	N/A
(j) in the case of land reserved under this Scheme, the objectives for the reserve and the additional and permitted uses identified in this Scheme for the reserve;	N/A	N/A
(k) the built heritage conservation of any place that is of cultural significance;	The proposal is consistent with heritage objectives and considerations, as per the attached Heritage Impact Statement.	✓

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14 Jun 2024

(l) the effect of the proposal on the cultural heritage significance of the area in which the development is located;	The proposal is consistent with the cultural heritage objectives and considerations, as in the attached Heritage Impact Statement.	✓
(m) the compatibility of the development with its setting, including — (i) the compatibility of the development with the desired future character of its setting; and (ii) the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development;	The proposal is compatible with its settings, including the objectives of the City Centre zone, and the local cultural heritage character of the streetscape.	✓
(n) the amenity of the locality including the following — (i) environmental impacts of the development; (ii) the character of the locality; (iii) social impacts of the development;	The proposal is compatible with the current and intended amenity of the locality as a vibrant and inviting city centre.	✓
(o) the likely effect of the development on the natural environment or water resources and any means that are proposed to protect or to mitigate impacts on the natural environment or the water resource;	N/A	N/A
(p) whether adequate provision has been made for the landscaping of the land to which the application relates and whether any trees or other vegetation on the land should be preserved;	The proposal includes appropriate hard and soft landscaping, as noted below and in the attached development plans.	✓
(q) the suitability of the land for the development taking into account the possible risk of flooding, tidal inundation, subsidence, landslip, bush fire, soil erosion, land degradation or any other risk;	N/A	N/A
(r) the suitability of the land for the development taking into account the possible risk to human health or safety;	N/A	N/A
(s) the adequacy of — (i) the proposed means of access to and egress from the site; and (ii) arrangements for the loading, unloading, manoeuvring and parking of vehicles;	The proposed development generally maintains the existing access, egress, loading and waste collection arrangements of the site – designed in consultation with local government technical staff.	✓
(t) the amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system	The proposed development is considered likely to generate an appropriate level of traffic for the City	✓

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in the locality and the probable effect on traffic flow and safety;	Centre zone, as noted in the attached Transport Impact Statement. 14 Jun 2024	
(u) the availability and adequacy for the development of the following — (i) public transport services; (ii) public utility services; (iii) storage, management and collection of waste; (iv) access for pedestrians and cyclists (including end of trip storage, toilet and shower facilities); (v) access by older people and people with disability;	The proposed development has appropriate transport choices, access and servicing for the nature of the development – as noted in the attached Transport Impact Statement and Waste Management Plan.	✓
(v) the potential loss of any community service or benefit resulting from the development other than potential loss that may result from economic competition between new and existing businesses;	There is no loss to community services arising from this proposal.	✓
(x) the impact of the development on the community as a whole notwithstanding the impact of the development on particular individuals;	The proposed land use will contribute to the community as hotel guests are likely to frequent local businesses such as cafes, restaurants, bars and retail outlets.	✓
(y) any submissions received on the application;	To be advised after local government assessment.	TBC
(za) the comments or submissions received from any authority consulted under clause 66;	To be advised after local government assessment.	TBC
(zb) any other planning consideration the local government considers appropriate.	To be advised after local government assessment.	TBC

4.2 Local Planning Policies

Local Planning Policy Heritage Assessment and Protection (LPP1.6)

The development application is consistent with the following relevant considerations of LPP1.6:

- The Development Application includes a Heritage Impact Statement by an experienced heritage professional, to demonstrate that the proposed development and land use will not unreasonably impact upon the heritage significance of the building and the streetscape.

Local Planning Policy Fremantle Port Buffer Area Development Guidelines (LPP2.3)

The development application is consistent with the following relevant considerations of Area 2 within LPP 2.3:

- The proposed built form is considered to meet all the requirements relating to potential risk and amenity considerations from any event from the port. The specific construction features will be finalised as part of the building permit process.

Local Planning Policy Heritage Areas (LPP3.6)

The development is proposed to be undertaken in a sympathetic manner that does not cause the loss of heritage significance, which is consistent with the intent of LPP 3.6, noting the following:

- The architectural design and detailing of the proposed development responds to the heritage materials, colours, and character of Essex Street and surrounds.
- 19 Essex Street is not intact and has undergone substantial alteration in the late 20th century.
- The proposed works are consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade.
- The proposed four storey addition will be located at the rear of the site and will be largely concealed from view from Essex Street.



5.0 Design Performance

5.1 Principles of Good Design

The development application plans and supporting documentation have been designed and prepared in accordance with the ten principles of good design as set out in State Planning Policy 7.0 Design of the Built Environment (SPP 7.0). A summary of the development application against the ten principles is provided below.

The draft development plans were submitted to the City of Fremantle Design Advisory Committee (DAC) for consideration against the 10 design principles. It is expected that the development application will again be considered by the DAC, with written advice and recommendations to be provided to the City and the Development Assessment Panel (DAP) to assist in determination.

The development application is highly consistent with the DAC pre-lodgement advice and the design principles of SPP 7.0. Therefore, positive feedback is to be expected from the second DRP review, which will enable the City and DAP to be confident of the development's strong performance under SPP 7.0.

Please refer to **Attachment 1 - Development Plans** and **Attachment 2 - Design Report** for full details of the design of the proposed design.

SPP 7.0 Design Principles	Development Application Proposal
Context & Character	The development plans have been carefully designed with consideration for the site context and local character, to develop a proposal that is consistent with the existing intent of the City Centre zoning and the culturally rich area.
Landscape Quality	<p>The design team has worked through the concept development phase, pre-lodgement DAC review, and preparation of the development application to prepare an integrated architectural and landscape design, that appropriately services the intended functions and amenity of the Tourist Accommodation land use and the future occupants of the development.</p> <p>The development site and the existing building footprint limit landscaping areas, however it has integrated landscaping where possible. This includes indoor planting within common areas, balconies and wherever possible.</p> <p>In addition, the existing large tree within the road reserve is to be retained and protected.</p>



Built Form & Scale	<p>The built form and scale of the development is consistent with the objectives and requirements of the City Centre zoning and the local planning area.</p> <p>The built form is softened and articulated with the use of varied building elements, balconies, material detailing, and landscaping, which also serve to provide development character and visual interest.</p>
Functionality & Build Quality	<p>The development has been designed with careful consideration of the functional needs of all future occupants and visitors to the different land uses, as well as achieving an efficient and quality construction.</p> <p>The intended build quality is evident in the proposed architectural, with quality and visually interesting material selection, as well sustainability aspects such as solar PVs and appropriate material selection to ensure the long-term durability of the building.</p>
Sustainability	<p>The development plans have incorporated sustainability practices throughout. The key sustainability features of the proposed development include:</p> <ul style="list-style-type: none"> ○ High-performance fabric and insulation are proposed. ○ Mitigating external noise with strategic layouts and noise absorptive finishes. ○ Certain lighting will be LED, low energy lighting with integrated Smart Technology. ○ Certain appliances installed will be energy and water efficient. ○ Facilitation of alternative transport and access, including development within close proximity to bus services, bicycle parking and showers. ○ Providing accommodation that is walkable to local workplaces and services. ○ A waste management plan has been included in the development application package, including waste recycling. ○ Solar passive design and natural ventilation, reducing heating and cooling requirements and enhancing occupant amenity. ○ Rooftop solar PVs. ○ Soft landscaping across multiple levels.
Amenity	<p>The visual and occupant amenity of the development is evident throughout the architectural and landscape plans.</p> <p>Key amenity aspects include:</p> <ul style="list-style-type: none"> ○ Passive solar design and winter sun access. ○ Natural light and natural ventilation. ○ Consideration for the cultural significance of the site. ○ Landscaped areas provided.

Legibility	<p>The development design strategy has considered access and wayfinding for the different transport modes and for the future occupants and visitors.</p> <p>Clearly defined pedestrian access is provide, including ramped / universal access. This will be enhanced with building signage.</p>
Safety	<p>The safety and security of occupants, visitors, and adjacent development has been considered in the development design.</p> <p>The bar will attract visitors and staff during opening hours, whilst residents will likely be onsite from day to evening.</p> <p>A large balcony/terrace that is proposed to front Essex Street on the first floor will provide activation and passive surveillance.</p>
Community	<p>A key aspect of the development proposal is that it provides positive outcomes for the local community, including the attraction of tourists to Fremantle, the additional spending in local businesses and the influx of culturally diverse people into Fremantle.</p> <p>Social inclusion within the development will also be enabled through universal access design, such as accessible rooms and lifts.</p>
Aesthetics	<p>The architectural plans provide details of the materials, colours and finishes of the proposed development, which demonstrate a well-considered, visual interesting, and quality materials palette.</p>

5.2 Design Review

In accordance with best practice planning and design for major development proposals, draft development plans were submitted to the City of Fremantle Design Advisory Committee for pre-DA lodgement review and recommendations. The City's DAC Advice Note confirmed the following positive aspects of the draft proposal:

- *Early engagement with the DAC.*
- *A new tourism development proposal in Fremantle City Centre for visitors who are seeking affordable hotel accommodation.*
- *The LyLo model appears to be successful, with hotels in some of the major cities of New Zealand and in Brisbane; the proposal for Fremantle is the first in WA.*
- *The Hotel model has the clear core values of 'Authentic, Home and Local'.*
- *The Hotel is an 'intense' development in terms of the high density of visitors accommodated (potentially 248 guests based on the room configuration). This new visitor population should contribute to the life and vibrancy of the city.*
- *A sustainable design approach through the adaptive re-use of the existing two buildings on the site (including a Local Heritage Listed Building and 19B).*
- *New 4 storey addition is located at the eastern rear of the site with minimal negative visual and amenity impacts on the streetscape and surrounding existing facilities for guests.*
- *Activation opportunities of the building interface with Essex Street at both floor levels by way of al fresco dining areas.*
- *Inclusion of accessibility rooms for people with disabilities.*

The DAC Advice Note also provided a number of recommendations to be considered in preparation of the development application plans, which have been considered by the project team and included where possible.

The following key recommendations of the pre-DA lodgement DRP review have been incorporated into the development application package:

- A Heritage Impact Statement has been completed and is included in the lodged documentation.
- The intent for the awning design has been updated and clarified to highlight the retention of the existing and the division of the two building lots.
- Indicative artwork areas have been defined to form part of a future local artist engagement.
- Minor additional loose landscaping opportunities identified on the plans
- Ongoing minor improvements made to ensure technical compliance can be achieved in respect to escape distances and paths.
- Additional information provided on sustainability to clarify intent for future adaptive reuse.
- Locations of all operable windows are shown and additional operable windows introduced.

- Additional clarity included to better define the materiality of the rear building.
- Example imagery provided to demonstrate intent for bicycle parking arrangement.

A response to each specific DAC recommendation is provided below.

Principle 1 – Context and Character	
Recommendations	Response
Demonstrate the integration of heritage with the new proposal.	The Heritage Impact Statement which outlines the integration approach has been provided along with a new render image, further demonstrating the streetscape relationship to Essex St and the Technical School.
Provide a Heritage Impact Statement (HIS) prior to the submission of the Development Application.	The Heritage Impact Statement has been provided.
Maintain the character of Essex Street by respecting the existing fine grain and rhythm of the lot pattern and buildings; in particular, review the awning design.	The awning design is consistent with both the wider and location specific Fremantle character. A number of street elevation studies have been provided demonstrating this relationship, and only minor works are proposed to the existing awnings to remove the inconsistent portion. This extent has been more clearly outlined in the architectural documentation.
Consider the rich DNA of the Central Fremantle Heritage Area and how local character, culture and history should be woven into the development.	An indicative zone has been shown in the documentation package to highlight the applicant's intent to engage with a local artist as the project progresses for an integrated and substantial public art approach which will be focused on local character, culture and history.
Principle 2 - Landscape Quality	
Recommendations	Response
Consider opportunities to provide more landscape and natural amenity into the communal areas.	Additional planter areas have been identified in the development plans, within common areas.
Principle 3 – Built Form and Scale	
Recommendations	Response
No recommendations.	N/A

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Principle 4 - Functionality and Build Quality	
Recommendations	Response
Consider the technical aspects of design at this early stage.	Ongoing minor adjustments have been made to ensure NCC and Australian Standard compliance requirements will be achieved.
Principle 5 - Sustainability	
Recommendations	Response
Consider long term sustainability and the ability to modify the proposal for other different uses in the future.	Additional clarity has been added into the architectural report on this item. In general, the pod rooms are fitted out with lightweight cabinetry style installations allowing potential conversion to a more traditional hotel arrangement in the future should it be required. Similarly, all new non-structural walls and columns are being built from lightweight materials, allowing a simple approach to any more substantial future adaptive re-use.
Principle 6 - Amenity	
Recommendations	Response
Consider the adequacy of the communal restroom 'block' and possible co-location of facilities with the "Houses".	The communal rest room numbers are compliant to NCC requirements and also meet the operational needs of the operator. Dividing the rest rooms while useful, adds additional costs and complications both upfront and ongoing. There is also no plumbing being located within the 'north' building to avoid interruptions of the ground floor tenancies below being retained during the construction works.
Consider improvements to the access of natural amenity into the rooms	Operable windows have been more clearly defined to all external glazing areas except where limited by fire requirements.
Consider improving the 4-storey block's rear staircase for the access of natural daylight and views out.	The functional and compliance requirements of the staircase unfortunately do not enable it to bring access daylight and views.
Consider opportunities to improve the current narrow areas of external communal open space at both levels.	The line of the external street facing level 1 terraces are defined on the 'north' building by the line of the existing secondary façade and the heritage façade. The space being used is the historical terrace area from prior iterations of the building.

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Consider improving the constrained front lobby area.	The existing lobby space in front of the existing stair and lift is being retained as a secondary entry and fire escape. More detail has been added to the render images & documentation to define the intent for this as a semi-concealed doorway. The main entry is defined by signage and wayfinding into the street facing bar space which will also be operated by the hotel, directing new visitors to the check in zone.
Consider relocating the bike store from the Back of House area to a more prominent and accessible part of the Hotel.	Once checked in, guests will be able to use the rear carpark to enter and exit via bicycle enabling effective connection to the street from the rear. The width of the site requires other more critical elements to be closer to the front of property towards Essex St. Additional imagery has been included to better present the intent of the bike storage area as a space to be celebrated, regardless of location.
Principle 7 - Legibility	
Recommendations	Response
No recommendations.	N/A
Principle 8 – Safety	
Recommendations	Response
No recommendations.	N/A
Principle 9 – Community	
Recommendations	Response
No recommendations.	N/A
Principle 10 – Aesthetics	
Recommendations	Response
No recommendations.	N/A



14 Jun 2024



External Aesthetics

(Source: Rothe Lowman)

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Internal Amenity

(Source: Rothe Lowman)

6.0 Technical Considerations

6.1 Sustainability

The development has been designed in accordance with the following sustainability frameworks:

- State Planning Policy 2.0 Environment and Natural Resources Policy.
- State Planning Policy 7.0 Design of the Built Environment.
- City of Fremantle Local Planning Policy 2.13 Sustainable Buildings Design Requirements.
- Section J energy efficiency provisions of the National Construction Code 2022.

Sustainable design, construction, and operation are fundamental aspects of this development proposal. Key sustainability elements of the development include:

- Whole life carbon emissions reduction.
- Energy efficiency, prioritising passive design followed by active design.
- Water efficiency.
- Designing for nature.
- Active transport.
- Circular economy design.

In addition to the above, a detailed Sustainability Report has been prepared by an experienced sustainability consultant – ADP Consulting.

Please refer to **Attachment 4 - Sustainability Report**.

6.2 Transport and Access

The development proposes a balanced and sustainable transport strategy, which seeks to reduce private car use and support alternative transport options. As part of this strategy, there is a proposed reduction to the number of individual car parking bays and promotion of alternative transport, as noted previously in this report - under the Local Planning Scheme parking assessment.

A transport assessment has been undertaken by a suitably qualified consultant, to assess the traffic impact of the proposed development, as well as site access considerations, and alternative transport options. The assessment found that the proposed development would not have a significant impact on local traffic and that there is sufficient proposed car parking within the development as well as a very good level of alternative transport options available to guests.

Please refer to **Attachment 5 - Transport Impact Statement**

6.3 Waste Management

A waste management plan has been prepared by a suitably qualified consultant.

It is considered that the waste generation from the proposed land use collection will require the following:

- Four x 1,100 litre refuse bins, collected three times each week, and
- One x 1,100 litre recycling bins, collected three times each week.

It is proposed that a private contractor will service the proposed land use from the verge off the Norfolk Lane. Bins will be wheeled out to the verge on collection days to ensure the waste collection vehicle is not required to enter the site.

Please refer to **Attachment 6 - Waste Management Plan**.

7.0 Conclusion

This development application seeks approval for a proposed Tourist Development, at No.19 Essex Street, Fremantle; comprising adaptive re-use of a heritage listed building and a four storey rear extension, to create an 80 room short stay building, with associated amenities.

The development application is highly consistent with the objectives and planning intent of the state and local government statutory planning frameworks that applies to the subject site and the proposed land use, including the following planning documents:

- City of Fremantle Local Planning Scheme No.4
- State Planning Policy 7.0 Design of the Built Environment
- City of Fremantle Local Planning Policies

The development has been thoughtfully designed by acclaimed architects, RotheLowman, to carefully address the site's locational attributes and respect the cultural heritage and character of the locality. In addition, expert technical input has been obtained on planning, design and operational matters, including input from heritage, sustainability, transport, and waste management consultants.

The unique LyLo concept is perfectly suited to Fremantle and will bring innovative, affordable, short stay accommodation to the Fremantle city centre, adding to the vibrancy of the city and attracting more tourists to the area.

Having regard to the detailed planning assessment and justification set out in this report, along with the high quality architectural plans, as well as the supporting technical reports provided, we respectfully request that the City of Fremantle recommends to the Development Assessment Panel that the development application be approved.

Attachments

- Attachment 1 Development Plans
- Attachment 2 Design Report
- Attachment 3 Heritage Impact Statement
- Attachment 4 Sustainability Report
- Attachment 5 Traffic Impact Statement
- Attachment 6 Waste Management Plan

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LyLO

Fremantle



Acknowledgement of Country

Rothelowman acknowledges the Traditional Owners of Country throughout Australia and recognises the continuing connection to lands, waters and communities. We pay our respect to Aboriginal and Torres Strait Islander cultures; and to Elders past and present.

We respectfully acknowledge the Traditional Owners of the land on which the site is located, the Whadjuk Nyoongar people of Western Australia.

This is LyLo

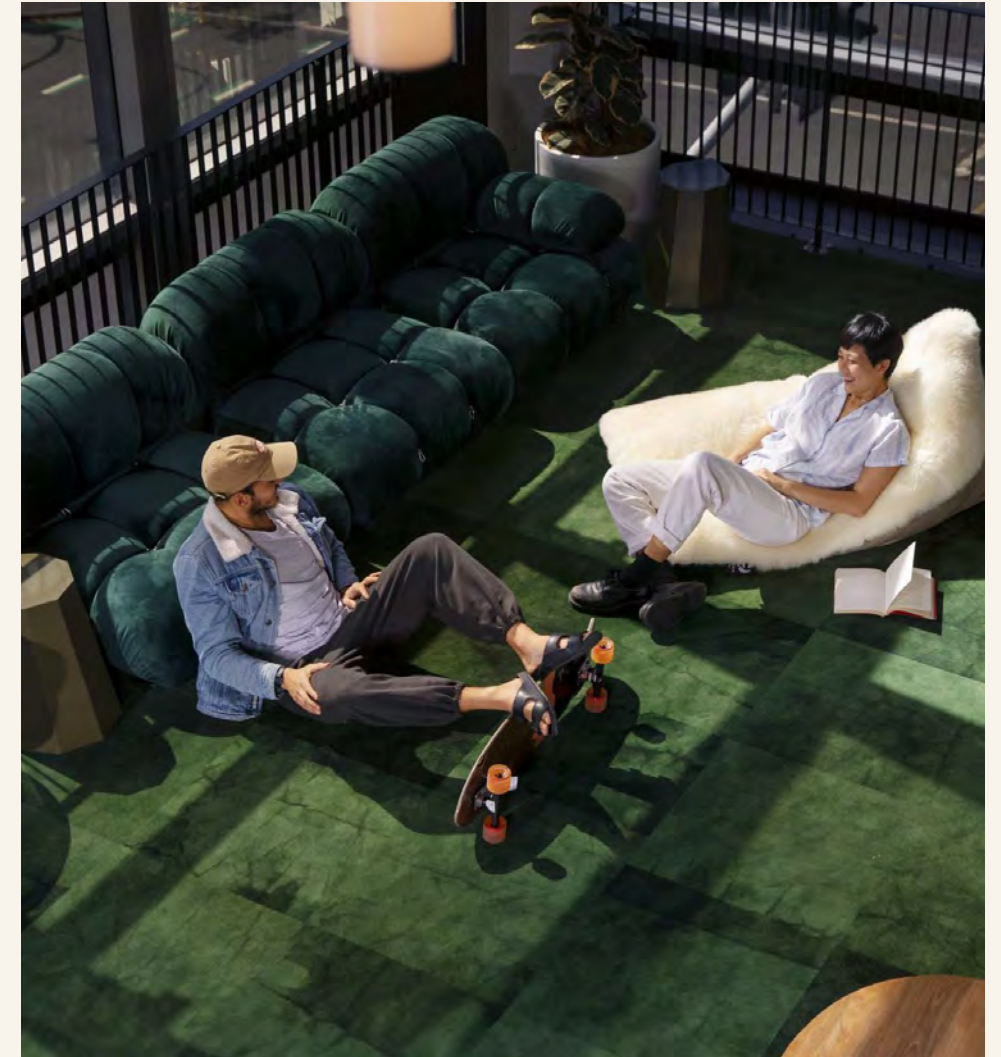
LyLo – Core Values



“ Authentic ”



“ Home ”

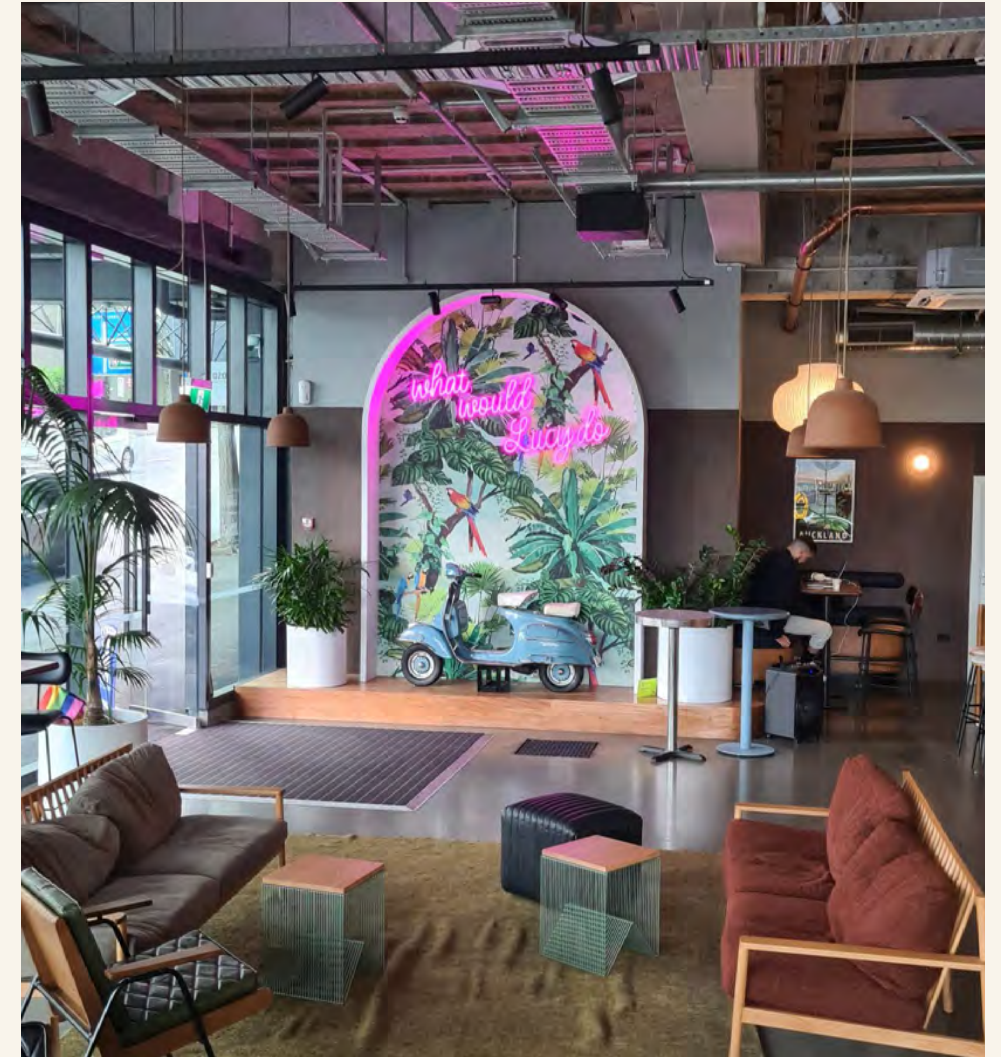


“ Local ”

LyLo – Existing Locations

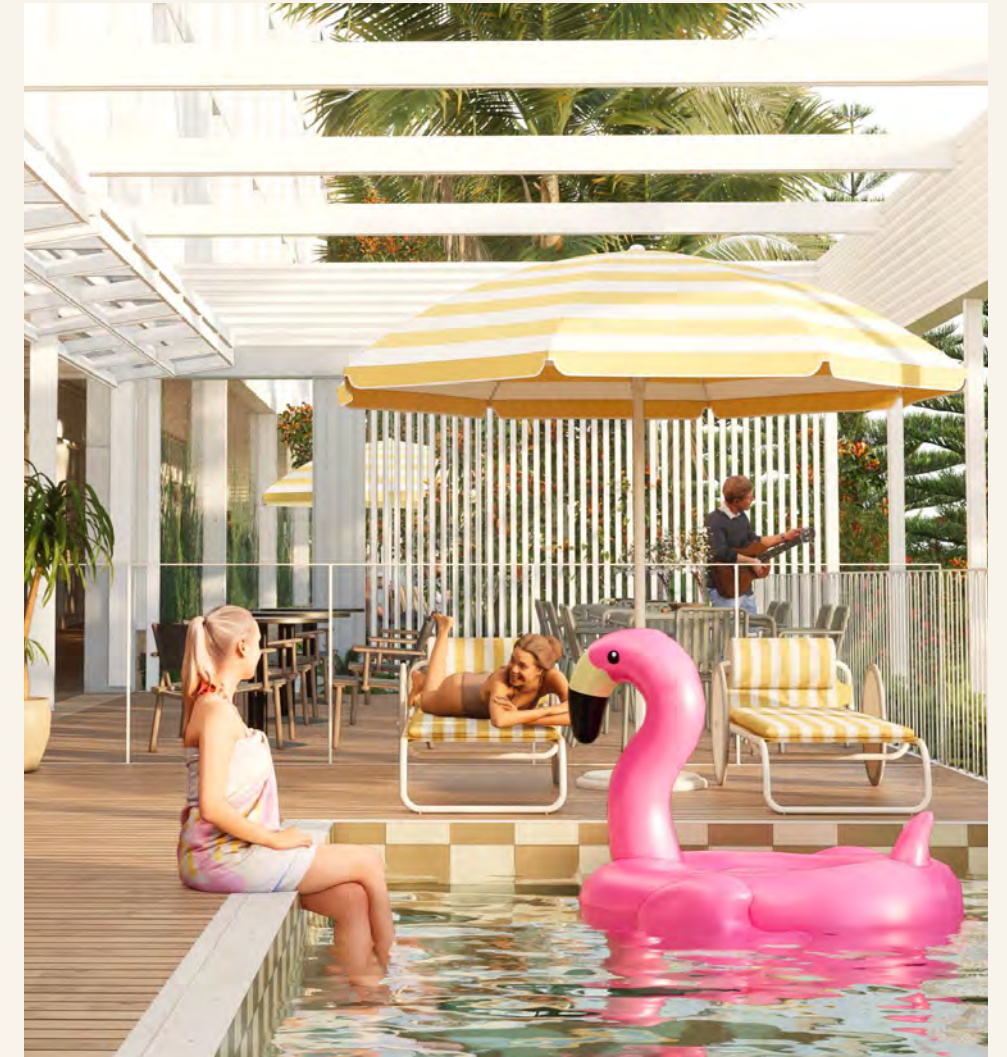


LyLo – Existing Location (Auckland)



“ The new way to stay ! ”

LyLo – Proposed Location (Gold Coast)



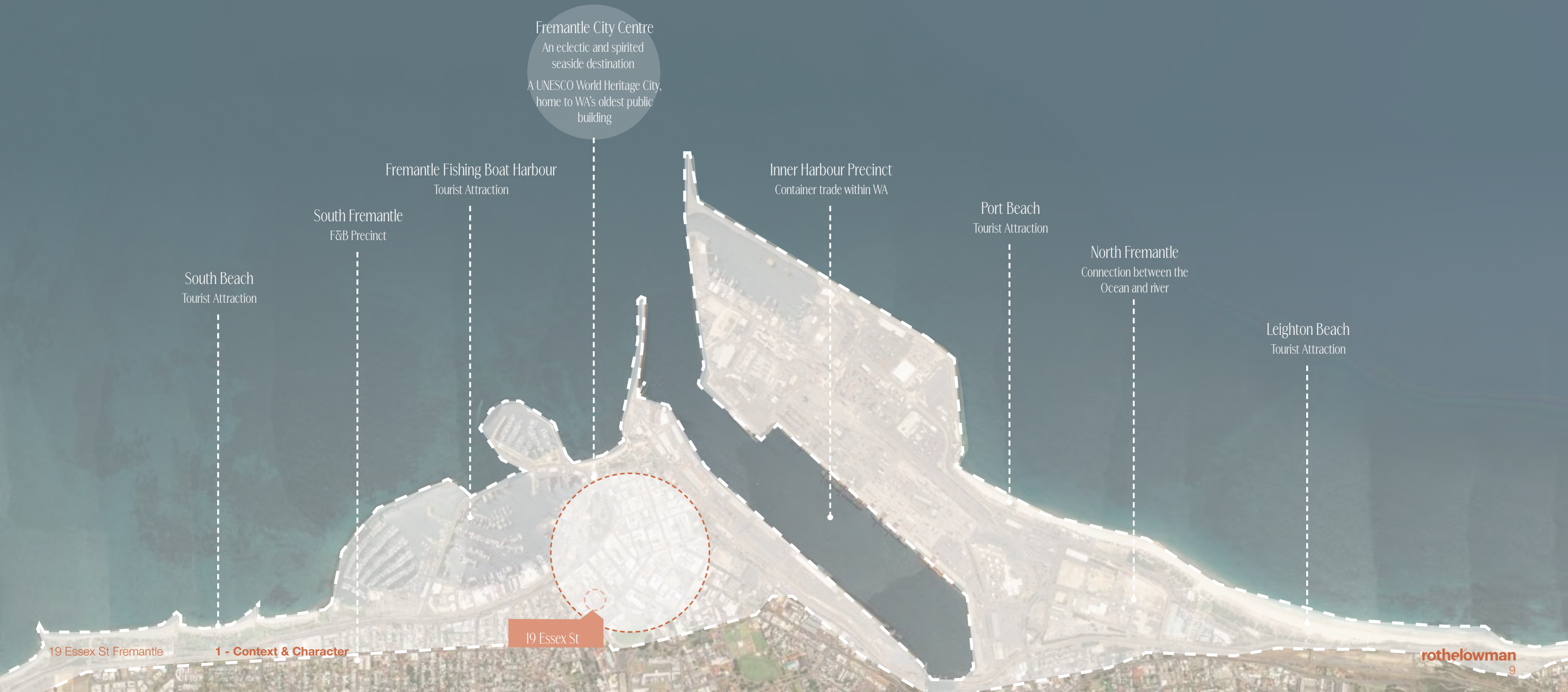
‘An unapologetically iconic place to holiday!’

A photograph of a two-story building with a stone base and a wooden upper floor, featuring a balcony and a central door, with a large tree on the right. The image is overlaid with a semi-transparent orange filter.

Context & Site Analysis

Site Location

Fremantle, an active port city...



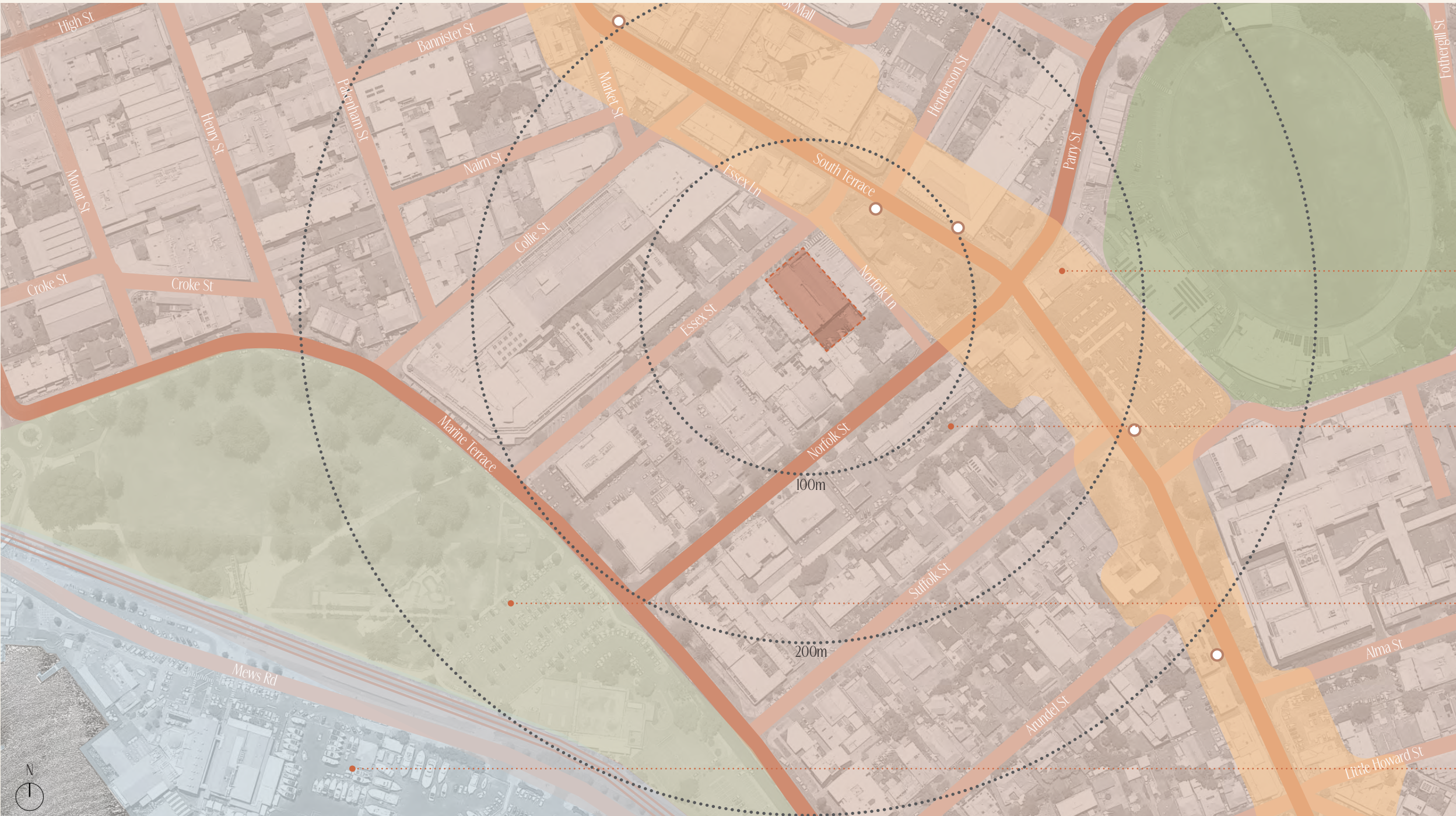
19 Essex St Fremantle

1 - Context & Character

19 Essex St

rothelowman

Contextual Zoning & Analysis



Legend

- Subject Site (Zoning: City Centre)
- Major Road
- Minor Road
- Public Transport Hubs
- The Park
- The Waterfront
- The Side Streets
- The Main Drag



The Main Drag



The Side Streets



The Park



The Waterfront

Neighbouring Amenity

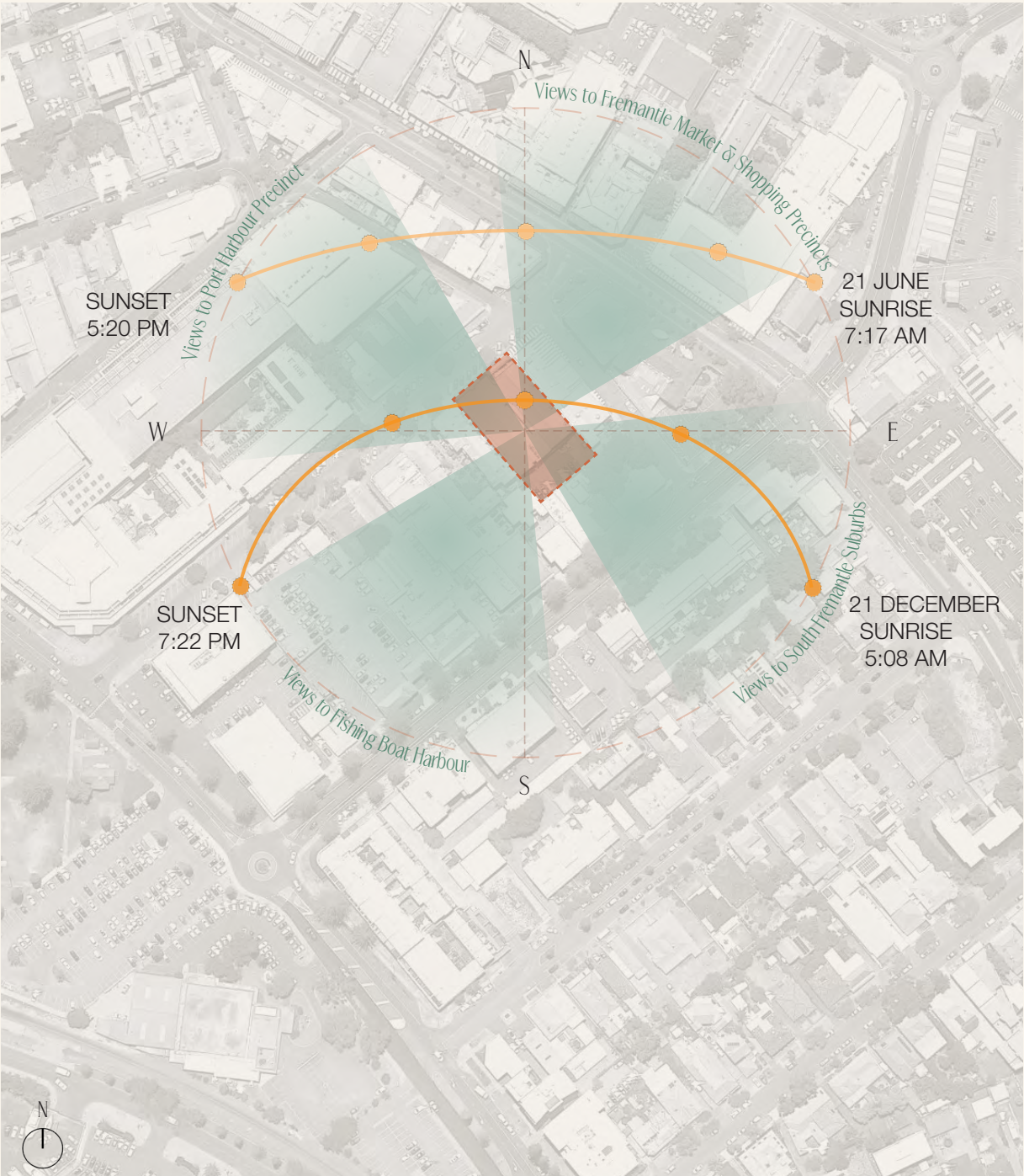


Freo Hotspots	
1	Bread in Common
2	Calamity's Rod
3	Flaming Galah
4	Ghetto Blaster
5	Little Creatures
6	Nieuw Ruin
7	Patio Bar
8	Sail and Anchor
9	Strange Company
10	The Old Synagogue
11	Whisper Wine Bar

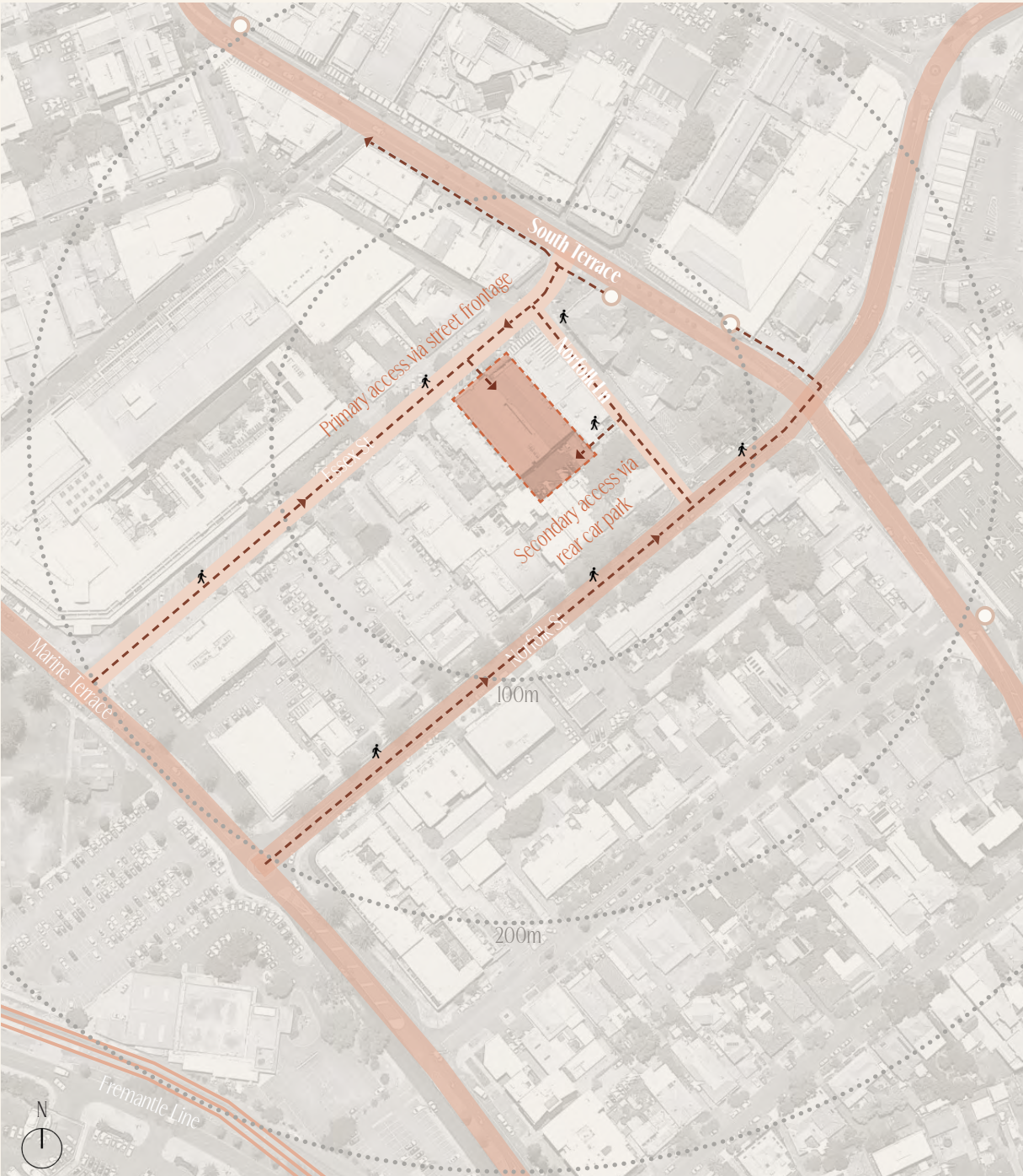
Landmarks	
1	Esplanade Park
2	Fremantle Oval
3	Fremantle Markets
4	Fremantle Fish and Boat Harbour
5	The University of Notre Dame Australia

Legend	
	Subject Site
	Major Road
	Minor Road
	Public Transport Hubs
	Green Space

View & Aspect



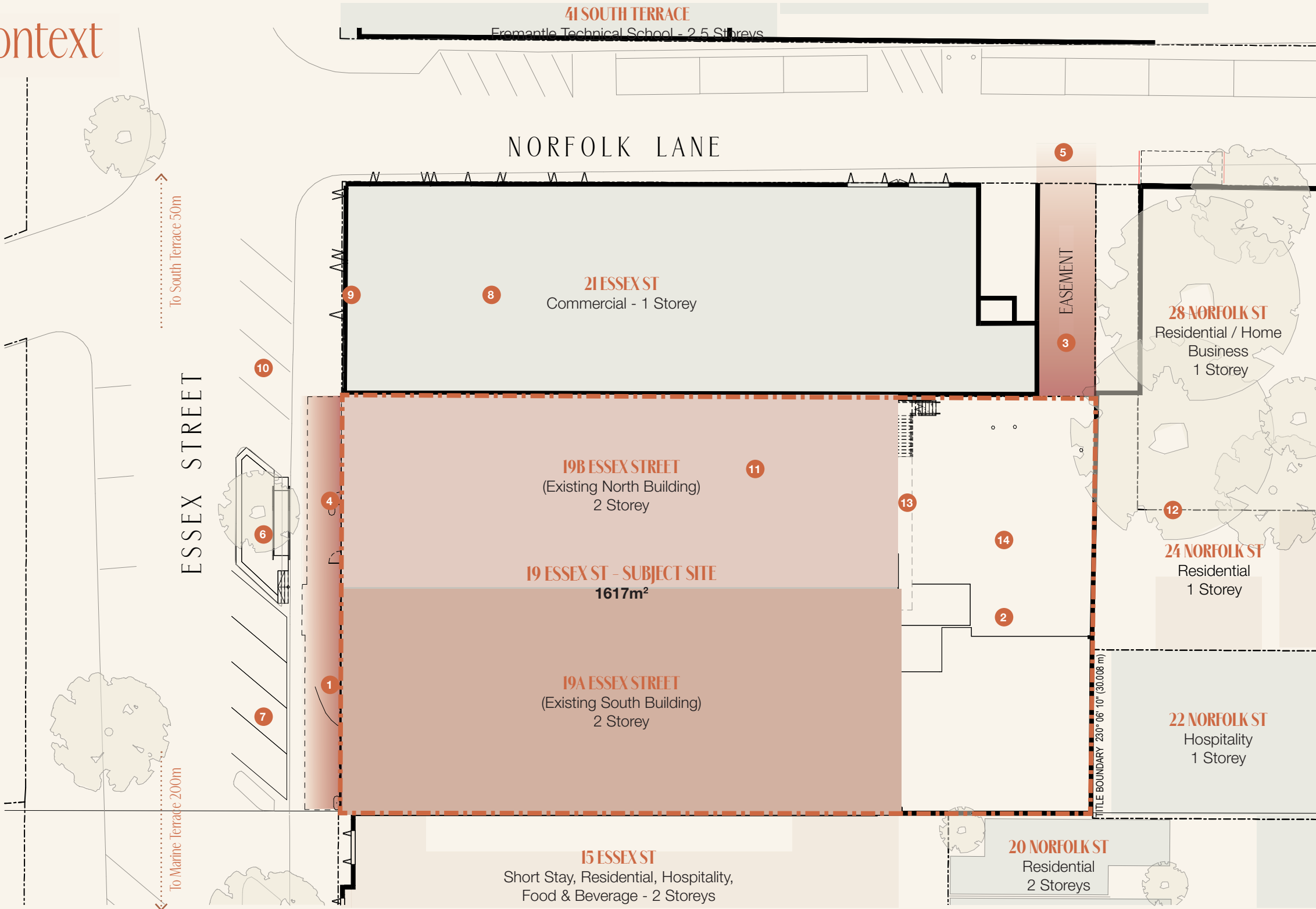
People Movement



Legend

- Subject Site
- Winter Sun Path
- Summer Sun Path
- View & Aspect
- Accessibility to Site
- Public Transport Hubs

Site Context



Constraints & Opportunities	
1	Engage with and repair the
2	Development potential of open
3	Rear loading of services and
4	Heritage facade of existing
5	New rear building visible from
6	Existing F&B and street
7	Existing parking bays
8	Character building
9	Roof form consistent with
10	Significant street parking
11	Roof to be retained over new
12	Overshadowing of 24-28
13	Retention of ground floor North
14	4 storey height limit,
no setbacks	

Legend	
	Subject Site
	Site Frontages
	Key Items
	Site Adjacent Significant Trees
	Adjacent Commercial /
	Adjacent Short Stay /

Site History



View 1 - 1899
View of partially vacant subject site



View 3 - 1970
North building awning has been replaced, & new south building constructed



View 2 - 1913
Original north building has been constructed

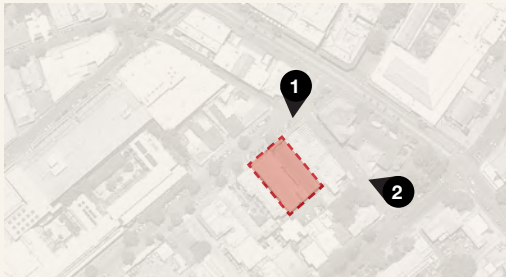


View 4 - 2017
North building roof has been raised, & south building redeveloped

Legend

Subject Site

Existing Site Photographs



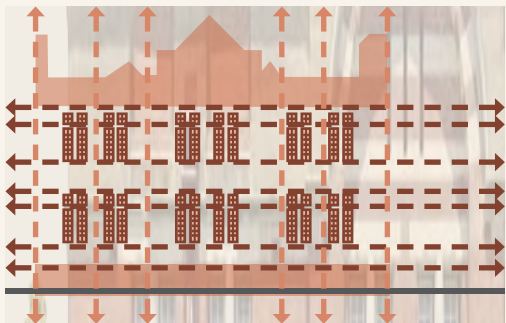
View 1 - Corner of Essex Street & Norfolk Lane



View 2 - Corner of Norfolk Street & Norfolk Lane

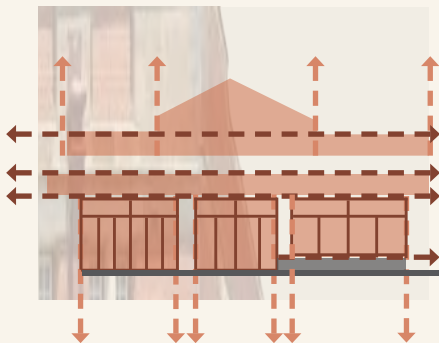
Streetscape

Essex Street Streetscape Studies



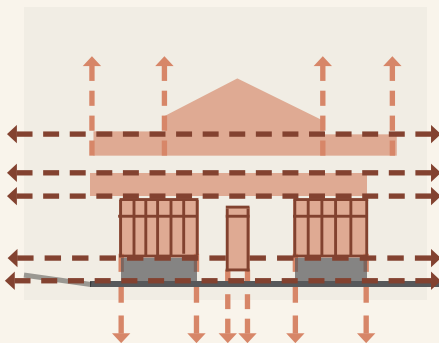
Datum Lines
Glazing Composition

41 South Terrace
Historic Educational Building
2 Storeys



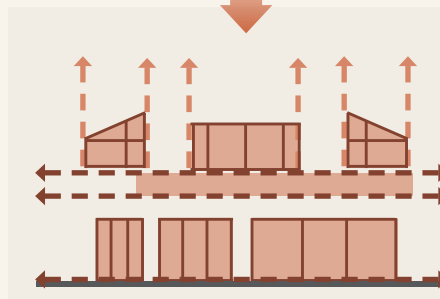
Datum Lines
Facade Composition
Awning
Glazing Composition

21 Essex Street
Commercial
1 Storey



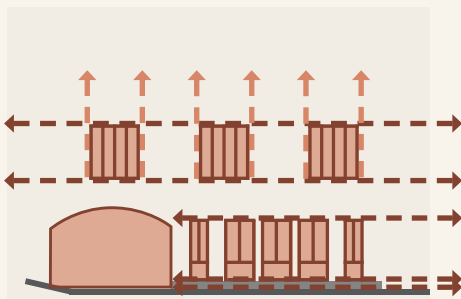
Datum Lines
Facade Composition
Awning

19B Essex Street
F&B Venue
1 Storey



Datum Lines
Facade Composition
Awning
Glazing Composition

19A Essex Street
F&B Venue / Commercial
1 Storey



17 Essex Street
Hotel /
Commercial / F&B
2 Storeys

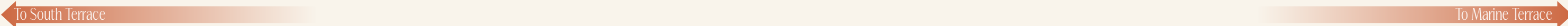
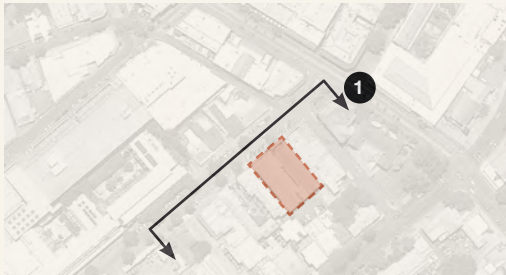


1/15 Essex Street
Residential / F&B Venue
2 Storeys

15 Essex Street
For Lease
1 Storey



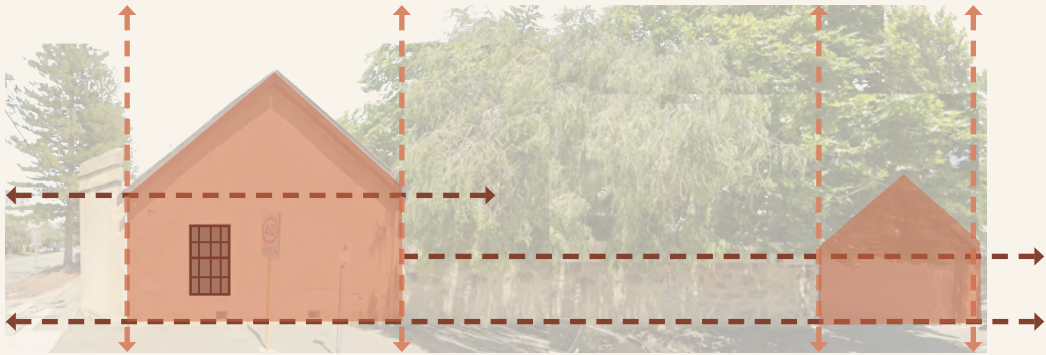
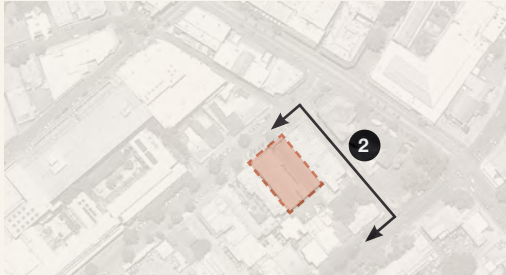
Pre 2009



View I
View Along Essex St - Facing Site

Streetscape

Norfolk Lane Streetscape Studies



Datum Lines
Facade Composition
Glazing Composition

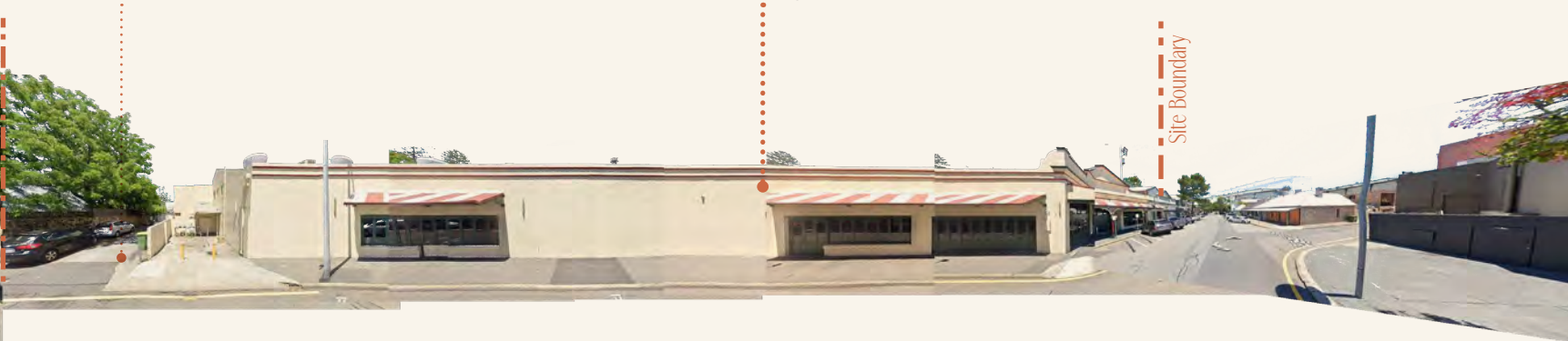
26 Norfolk Street
Residential building
1 Storey



Datum Lines
Facade Composition
Glazing Composition

21 Essex Street
Commercial
1 Storey

Entry towards rear car park



← To Norfolk Street

View I
View Along Norfolk Lane - Facing Site

To Essex St →

Heritage Context

Neighbouring Heritage Elements



Expanding Fremantle's Street Culture



‘ The streets of Fremantle create activation, moments and promote social interactions. ’

Design Proposal



The Port



The Historical Fabric



The Hipster

The Vision

Great Neighbours

LyLo Fremantle aims to deliver high quality engagement with Essex Street & Norfolk Lane, integrating into the local urban context of both the historic and the new. LyLo is an operation that is focused on the Authentic, the Home and the Local, making it a perfect fit for the needs of Fremantle.

Respecting the Historic

The architecture is inspired by its neighbours through proportionality and materiality, activating the ground and upper plane through increased activation and openings. The goal to “repair the street” is paramount in both the conceptual and the functional outcomes. The historic facade & parapet is respected and maintained, while the new facade steps back to give relief to the street. Vignette’s of living appear through openings to the terrace spaces, while the ground plane invites in and offers to the shelter both passersby and those staying on site with it’s generous dining reveal.

Framed Views

The partially hidden rear built form takes on it’s own distinct image, reflective of the wider Fremantle context, creating a clear distinction from the existing and historic. Crafted solar shades provide both site specific protection from the hottest days of the year, while also providing privacy to neighbouring properties.



LyLo view from Essex St



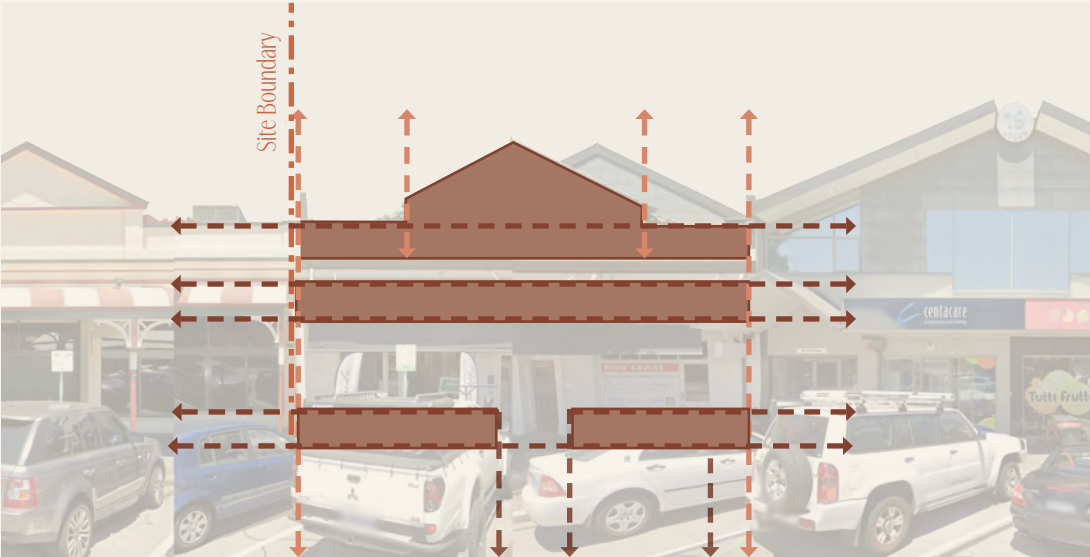
LyLo view from Norfolk Lane, set behind 21 Essex St

Repairing the Street

Essex Street

Street & Neighbourhood Language

Street Context & Analysis

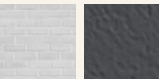


19B Essex Street

F&B Venue

- Datum Lines
- Facade Composition
- Awning

Materiality



Colour Palette

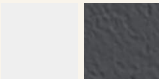


46-54 Marine Terrace

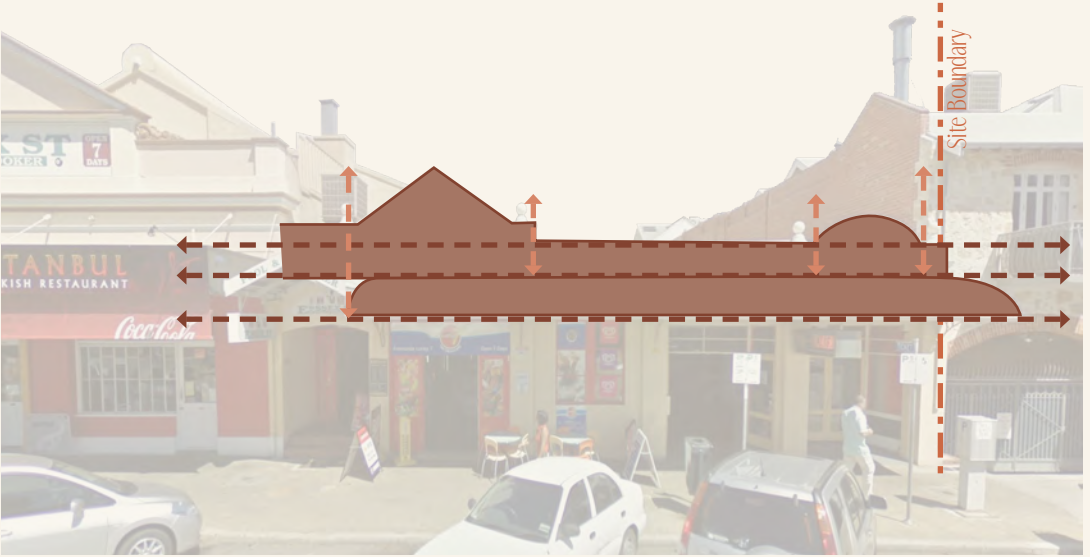
Hotel

- Datum Lines
- Facade Composition
- Awning

Materiality



Colour Palette

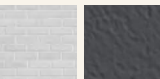


19A Essex Street (Pre-2009)

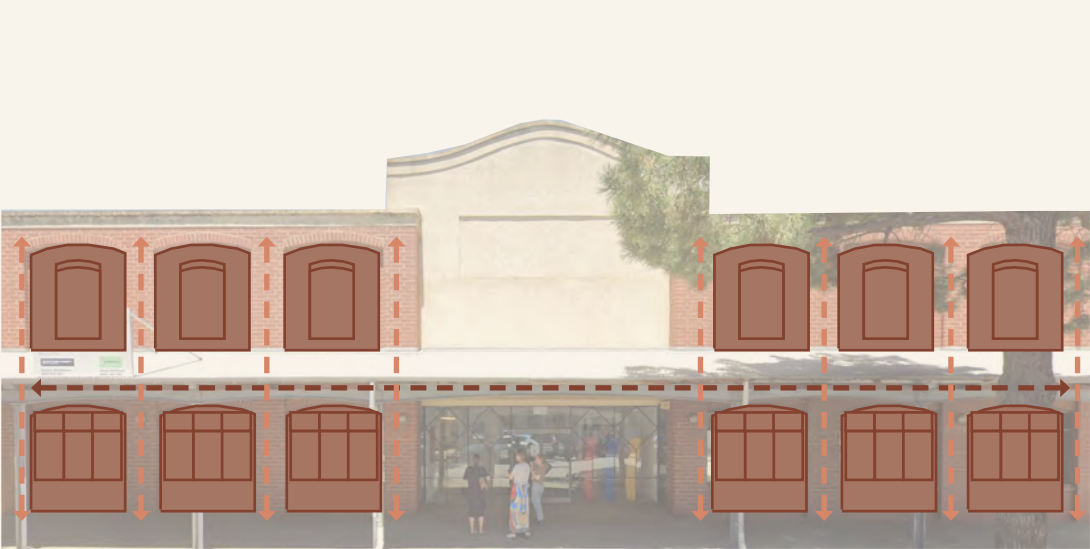
Newsagency

- Datum Lines
- Facade Composition
- Awning

Materiality



Colour Palette



7 Essex St

Commercial

- Datum Lines
- Facade Composition
- Awning

Materiality

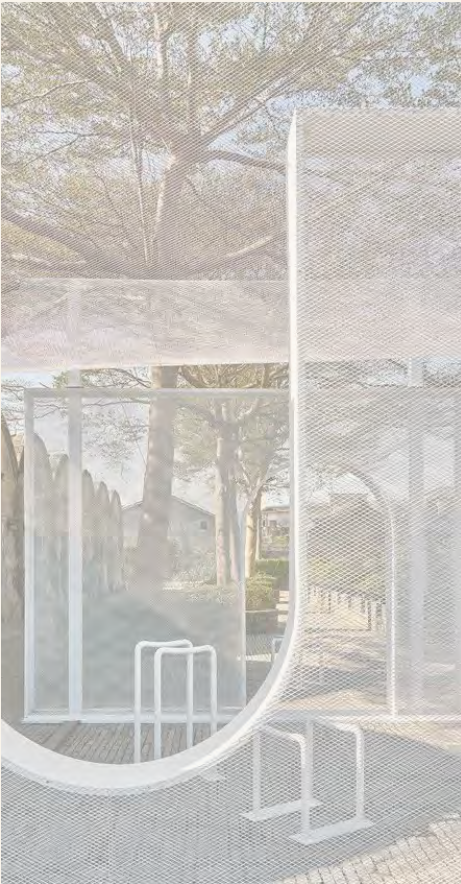


Colour Palette

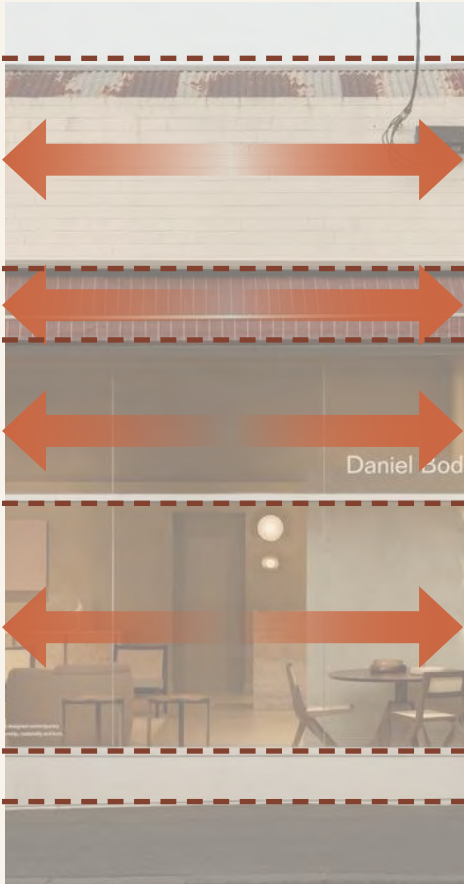


Street Language

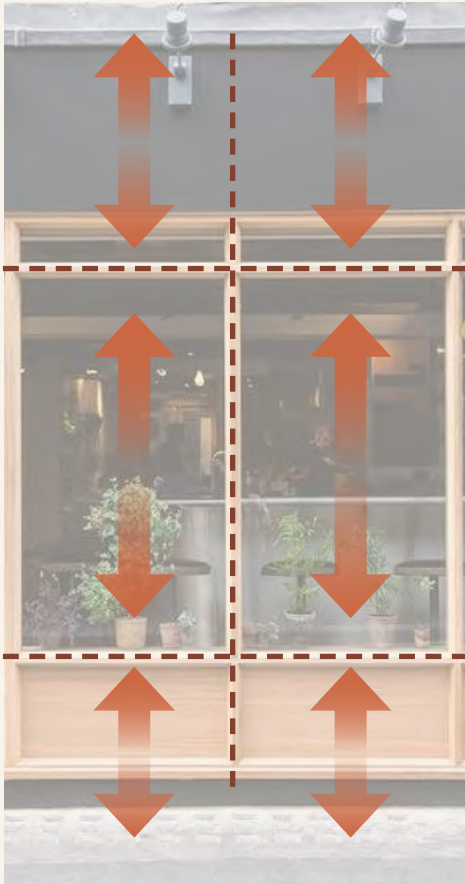
Materiality, Texture & Break up of Facade



Materiality
Providing different materials to add depth to the built form



Horizontal Datums
Referencing the horizontal forms in Fremantle through distinct bands through the built form



Vertical Datums
Pairing units to craft distinct connections



Establishing Street Presence
Addressing the street and creating presence



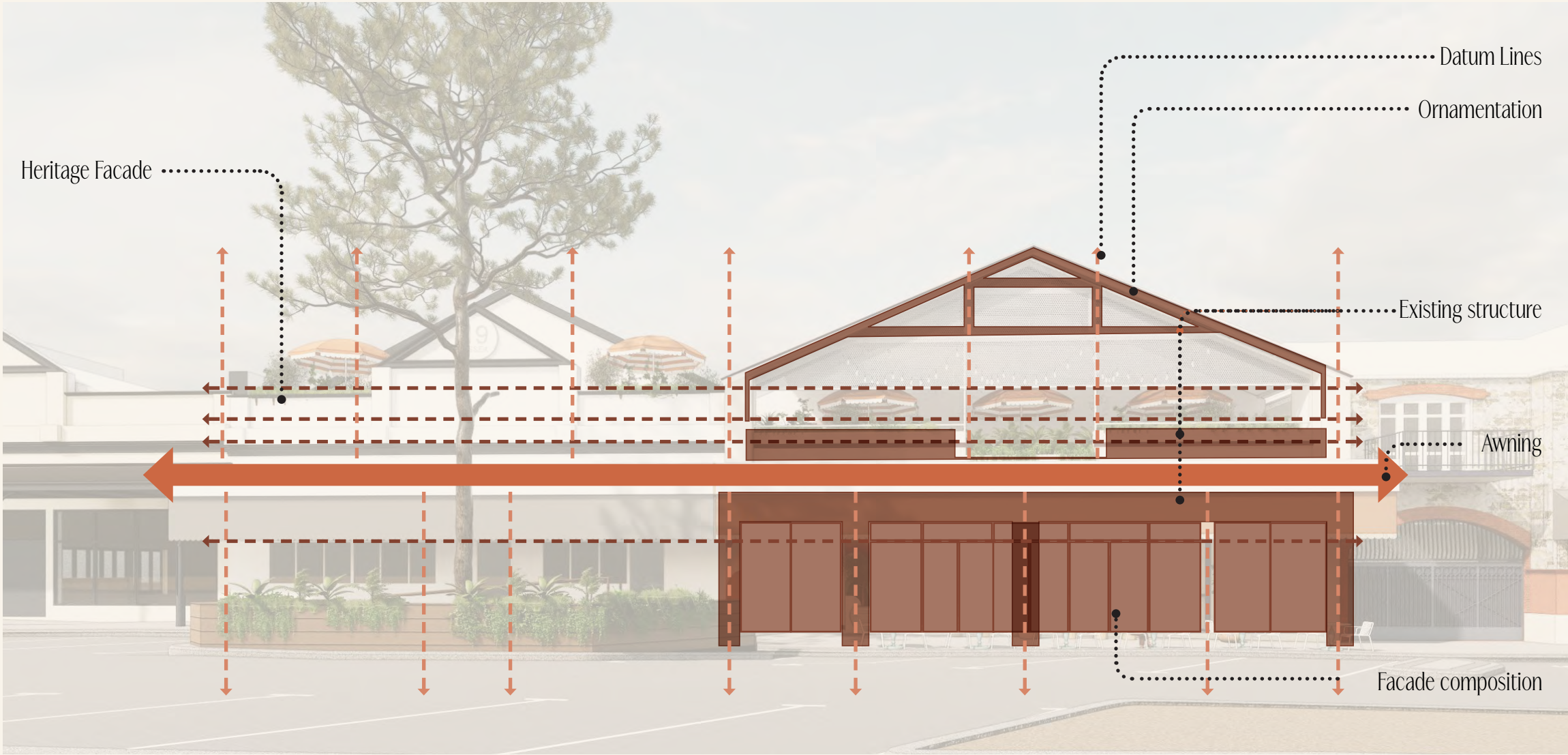
Layering the Thresholds
Distinguishing and layering forms to craft thresholds through the built form



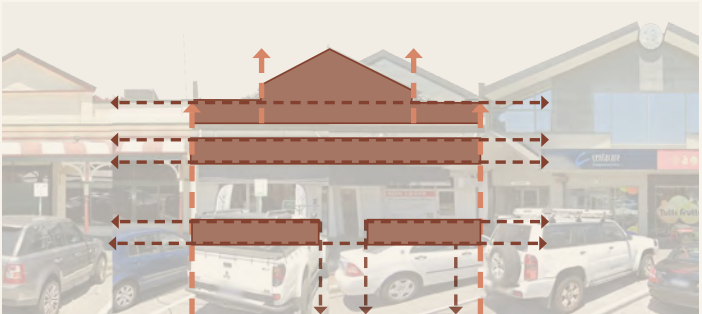
Ornamentation
Providing ornamentation through crafted datums

Essex Street Facade Composition

Expressing & celebrating the existing structural members



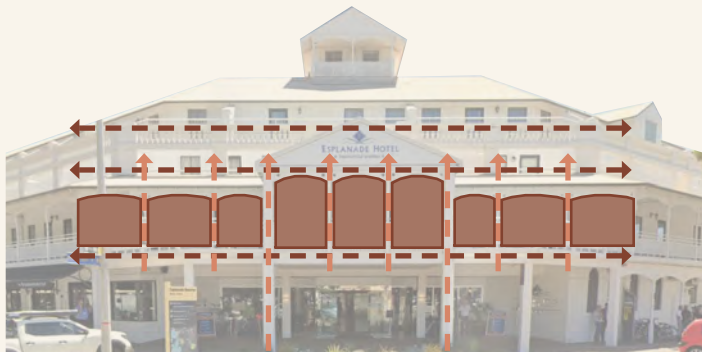
Local Precedence



19B Essex Street

F&B Venue

- Datum Lines
- Facade Composition
- Awning



46-54 Marine Terrace

Hotel

- Datum Lines
- Facade Composition
- Awning

Materiality Context

Fremantle Textures



Masonry (Fremantle Technical School)



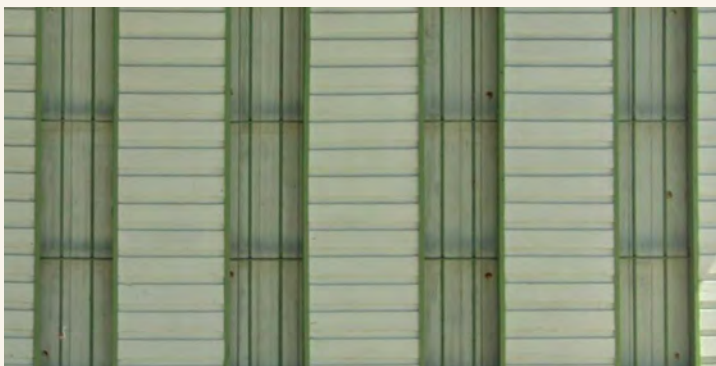
Limestone Building (Warders Hotel)



Industrial Framing



Steel & Wire



Weatherboard

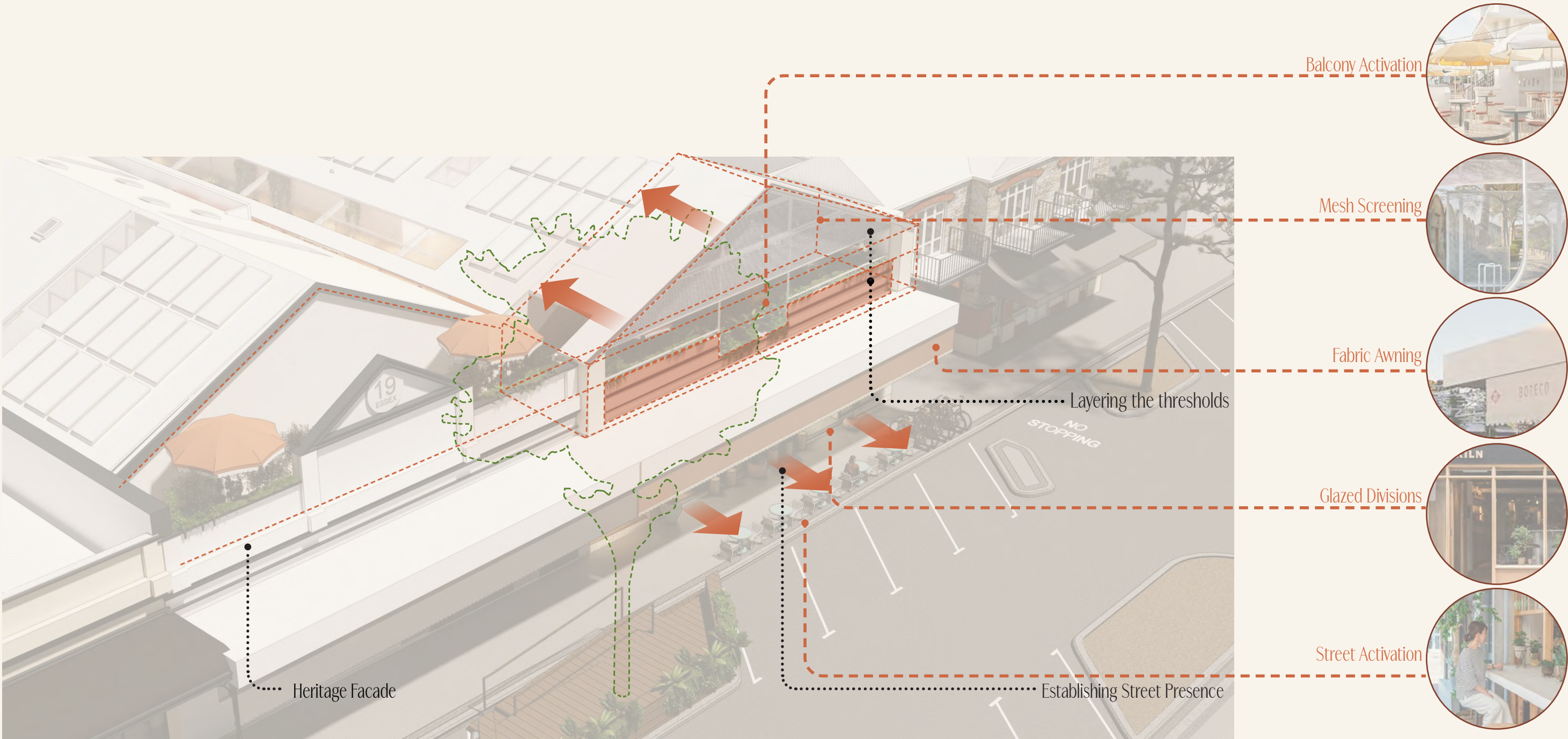


Timber



Greenery

Essex Street Roof Form & Materiality





Existing awning and fabric to Northern Building to be retained

Mural art to wall and underside of awning (Art style and extent is indicative only, pending engagement with local artist)

Infill to area within dashed lines to continue existing awning line

Existing flat awning to Southern Building to be retained

New fabric awning to match existing adjacent

New floor mounted bicycle racks





Fremantle Living

Local Streetscape Precedents

Street Context & Analysis



1 Cliff Street

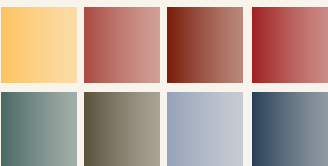
Fremantle Ports

- Texture
- Datum Lines
- Materiality

Materiality



Colour Palette

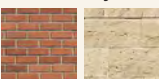


41 South Terrace

Fremantle Technical School

- Window Proportions
- Horizontal Banding
- Datum Lines

Materiality



Colour Palette

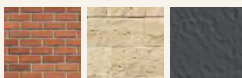


80 High Street

Sundancer Backpackers Hotel

- Window Proportions
- Horizontal Banding
- Datum Lines
- Super Graphics

Materiality



Colour Palette

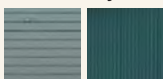


Peter Hughes Drive

E-Shed Markets

- Supergraphics
- Texture
- Materiality breaks
- Vertical Glazing

Materiality

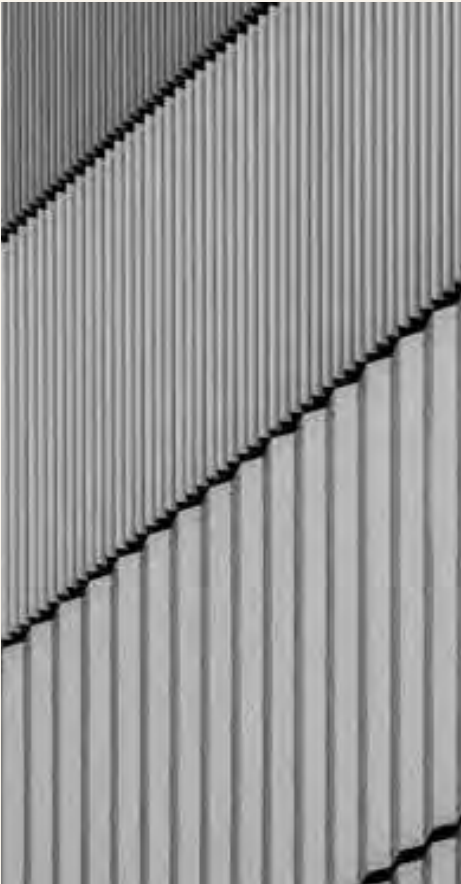


Colour Palette

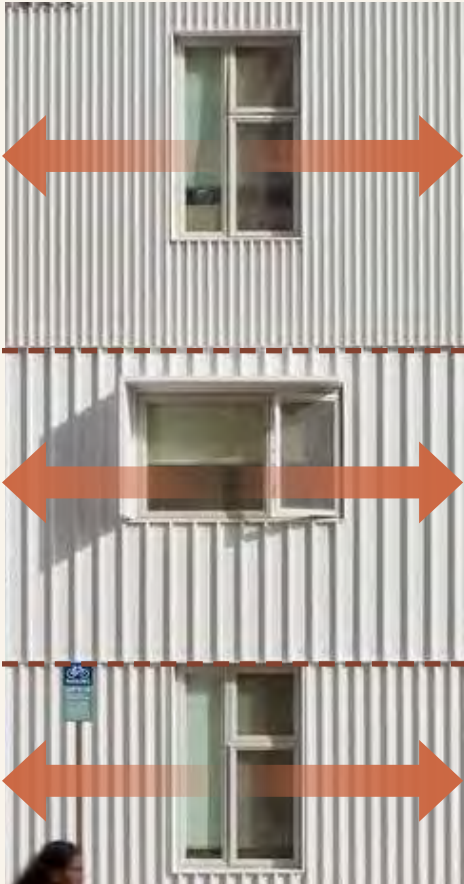


Facade Precedents

Materiality, Texture & Break up of Facade



Texture
Providing different textures to add depth to the built form



Horizontal Bands
Referencing the horizontal forms in Fremantle through distinct bands in the built form



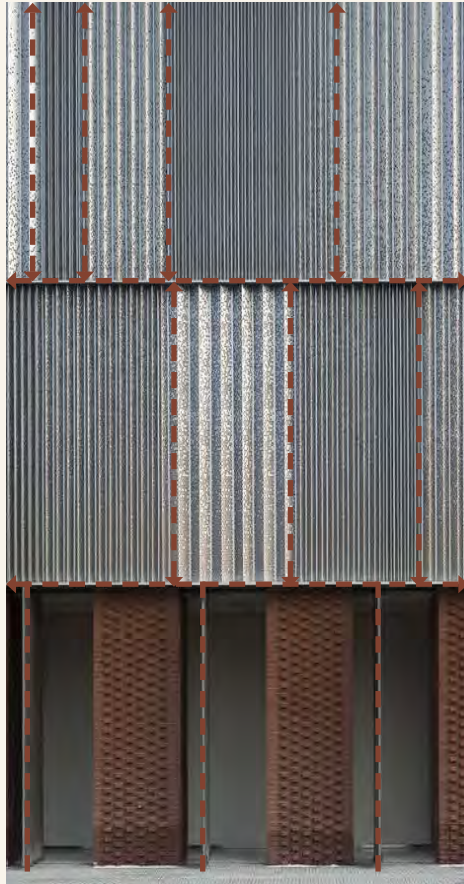
Vertical Connections
Pairing units to craft distinct connections



Landing the Building
Distinguishing the built form on the ground plane

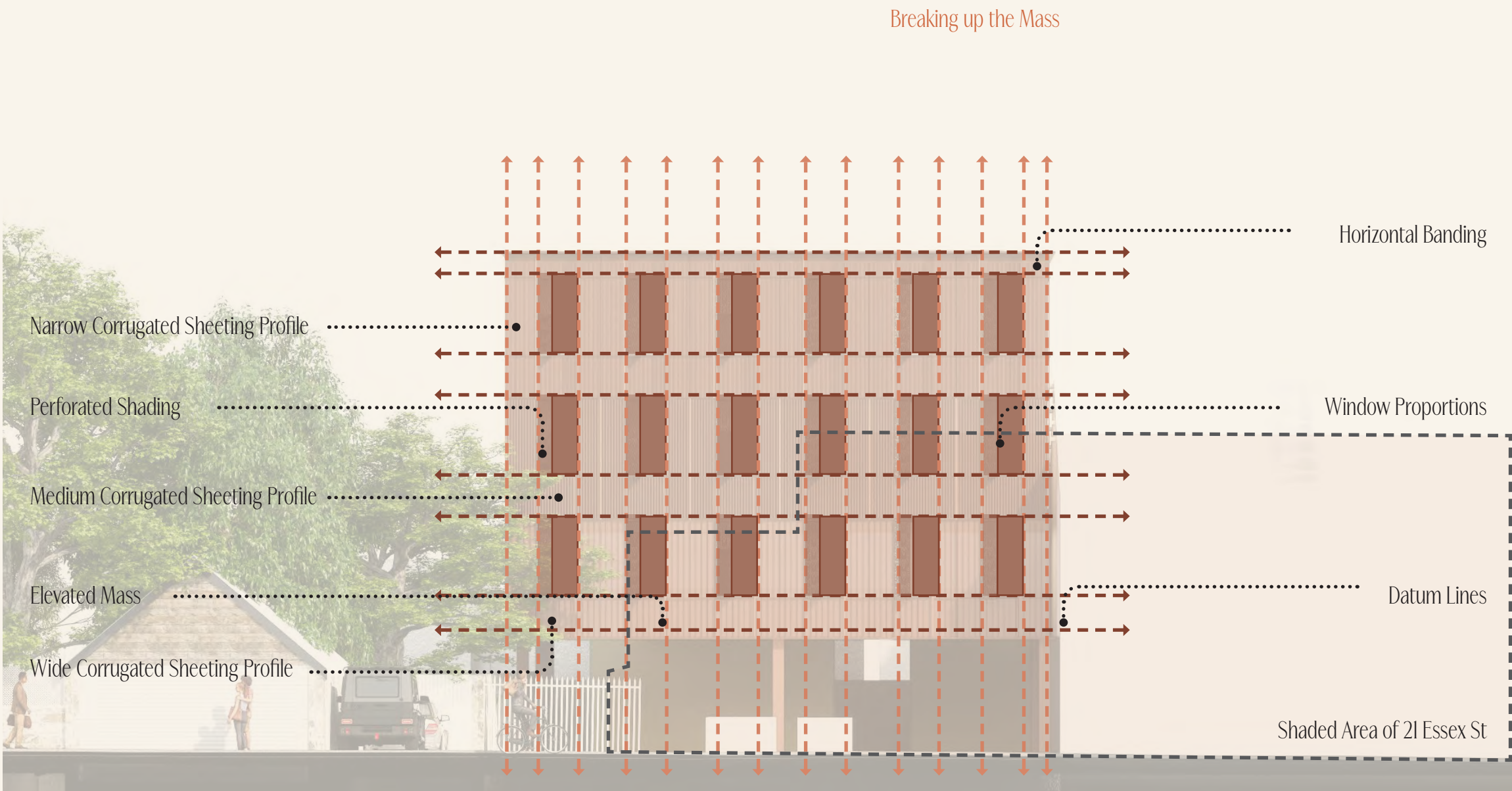


Articulation through Shading
Playing with shadows to promote a unique facade offering.



Ornamentation
Providing ornamentation through crafted datums

Norfolk Lane Facade Composition



Local Precedence



41 South Terrace
Fremantle Technical School

- Window Proportions
- Horizontal Banding
- Datum Lines

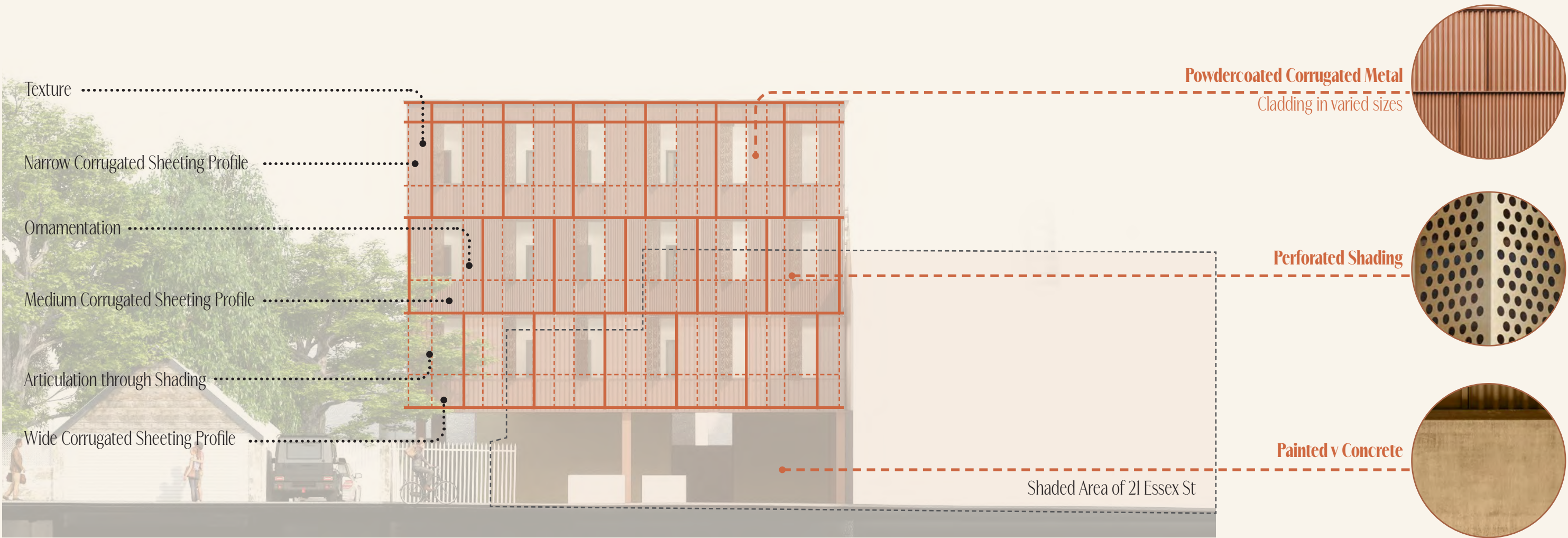


Peter Hughes Drive
E-Shed Markets

- Supergraphics
- Texture
- Materiality breaks
- Vertical Glazing

Norfolk Lane Facade Materiality

A contemporary palette inspired by existing Freo textures



Norfolk Lane Facade

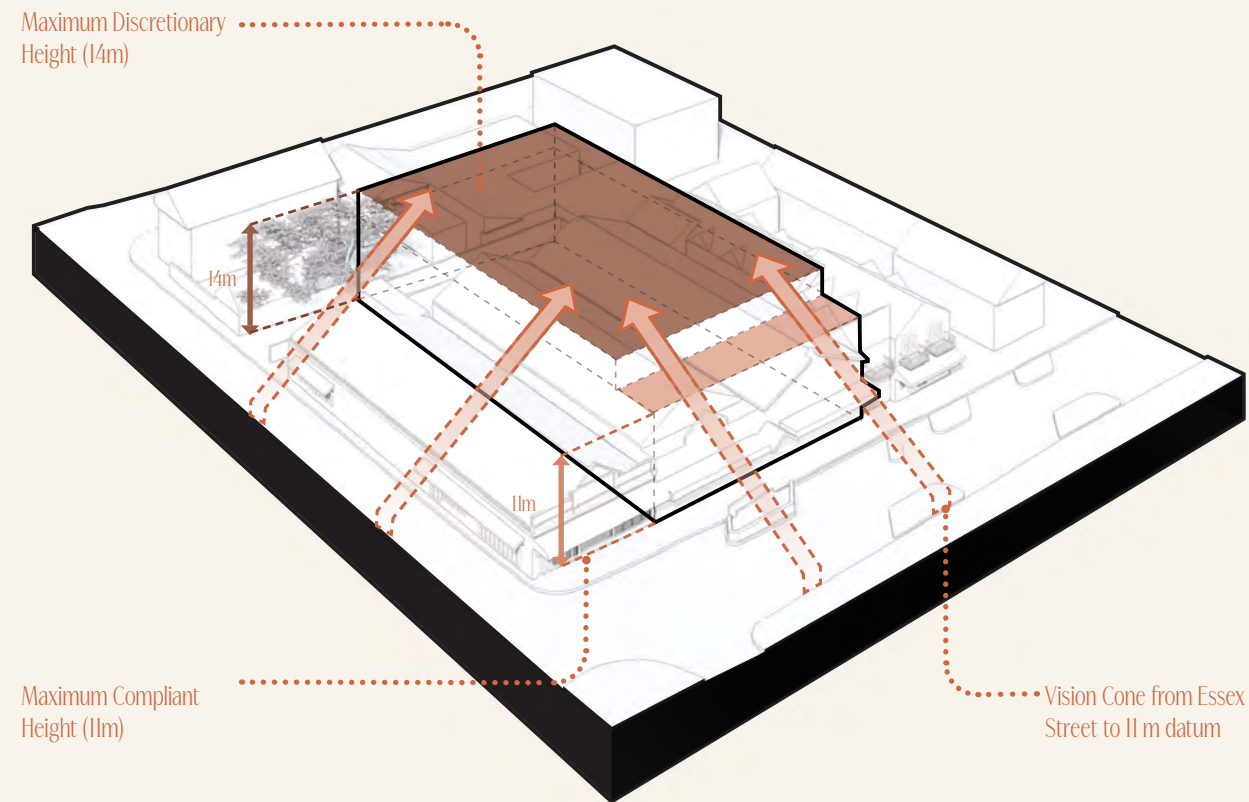
Contemporary Facade inspired by existing Freo textures viewed from Norfolk Lane Entry





Massing

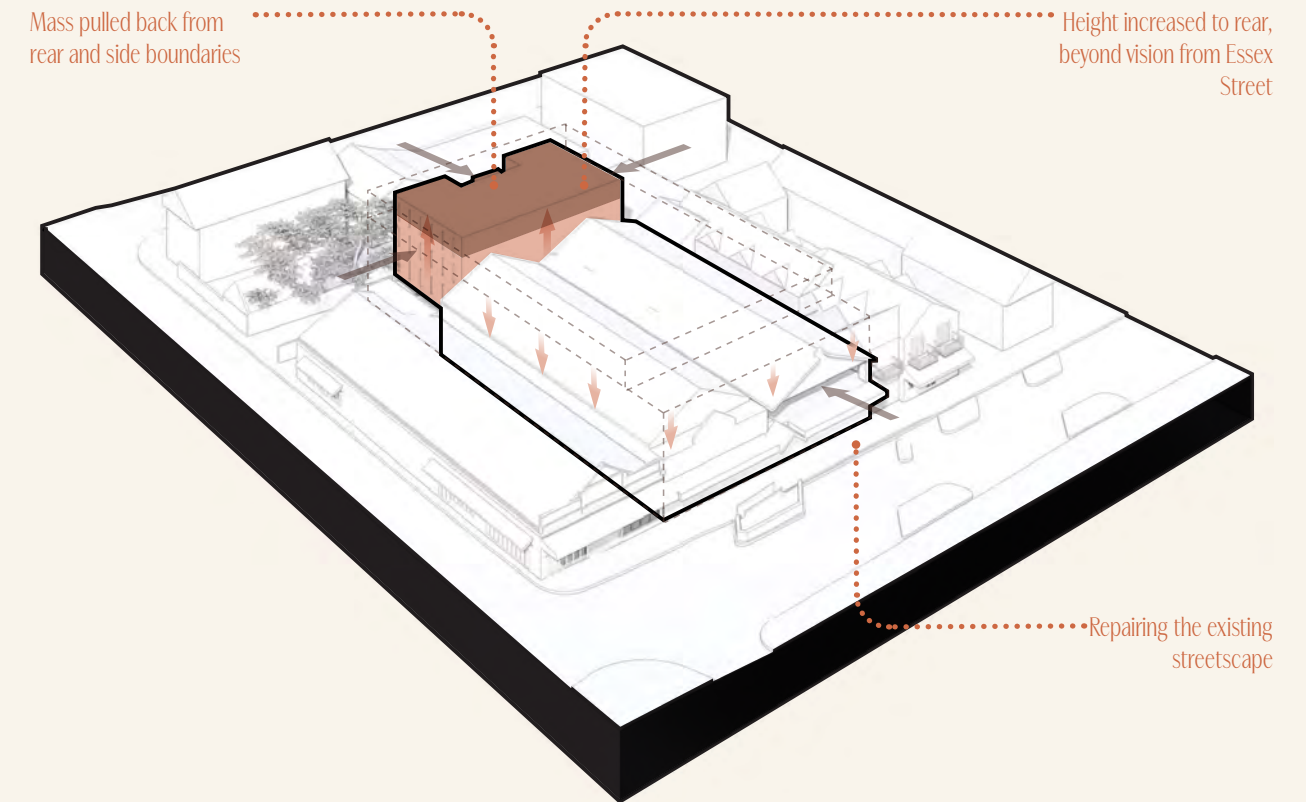
Compliant & Discretionary Massing



Compliant & Discretionary Mass

- 11m Maximum Building Height on Essex Street (3 Storey)
- In accordance with Clause 1.3 of Local Planning Area, the Council may consent to an additional storey subject to upper floor being setback from the street so as to not be visible from the streets adjoining the subject site
 - Maximum external height being 14m
 - Compliance with clause 1.2 of Schedule 1 - Area 1
- Nil side & rear setbacks.
- Potential compliant & discretionary envelope results in a form that overpowers the existing streetscape.

Proposed Massing



Proposed Development

- Overall Height under 14m
- Existing building on Essex Street retained to be consistent with neighbouring podium façades
- Built form at the rear rationalised to a single form, creating a 3m setback to the rear courtyard and side boundaries
- Proposal rationalises the mass to the rear of the property, reducing the visual impact to Essex Street.

Streetscape Massing Impact

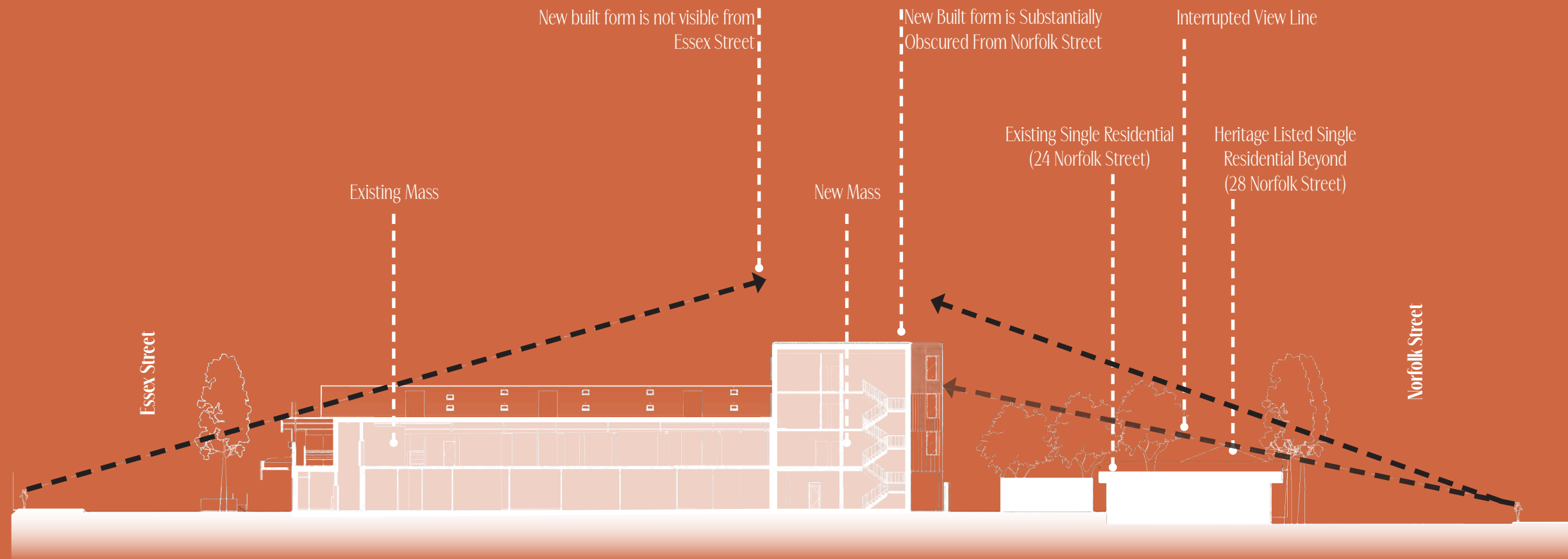


View from corner of South Terrace and Essex Street



View from corner of Norfolk Street and Norfolk Lane

Sectional Impact Study





Street Life



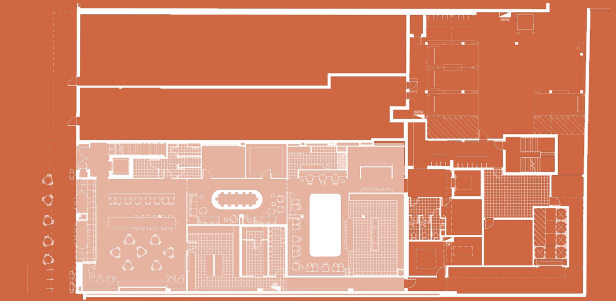
Fremantle DNA



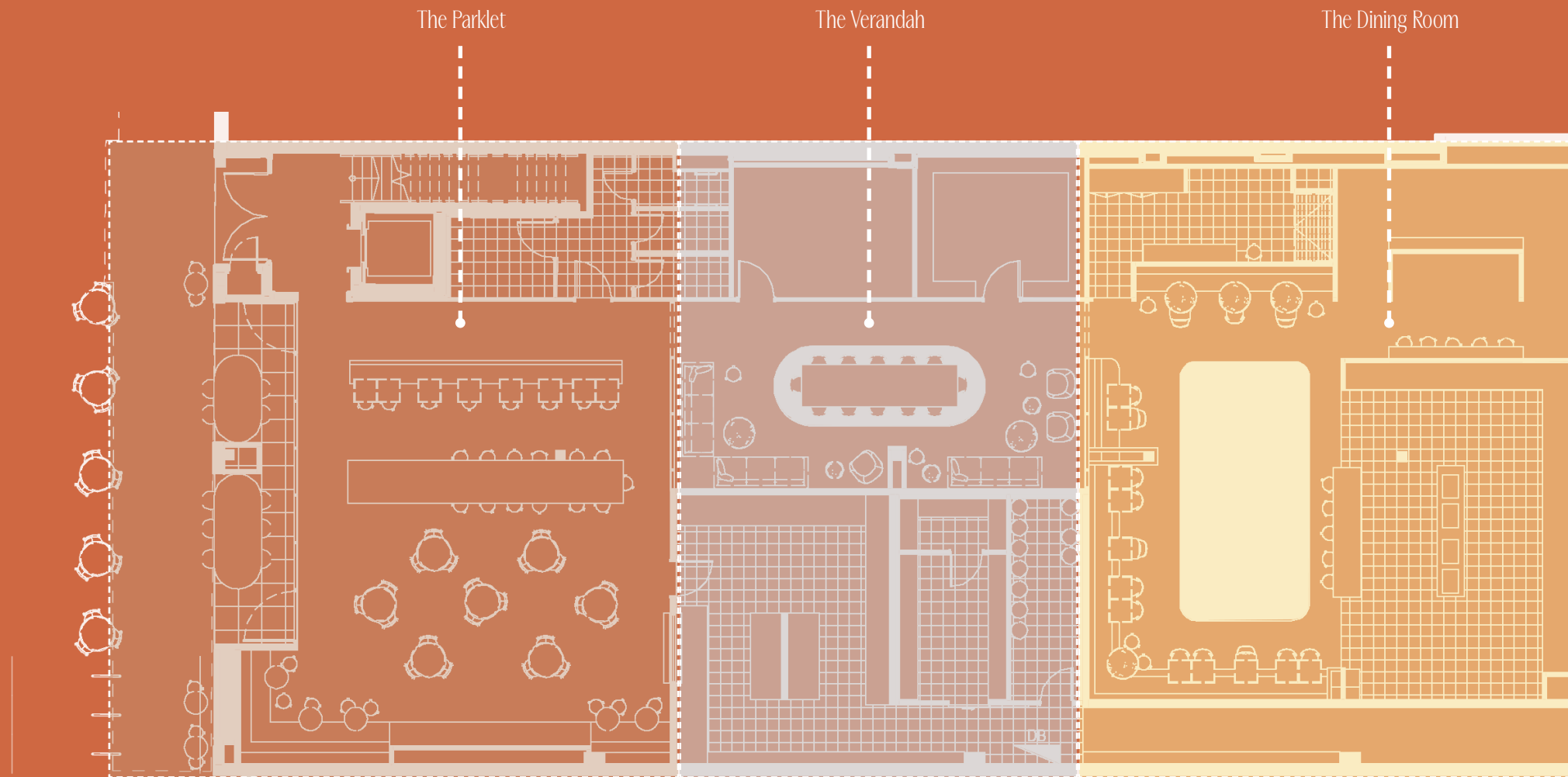
A humble home unique in character. Offering an address that is distinctly of its place...

Lylo Home

Take the journey to your home away from home !

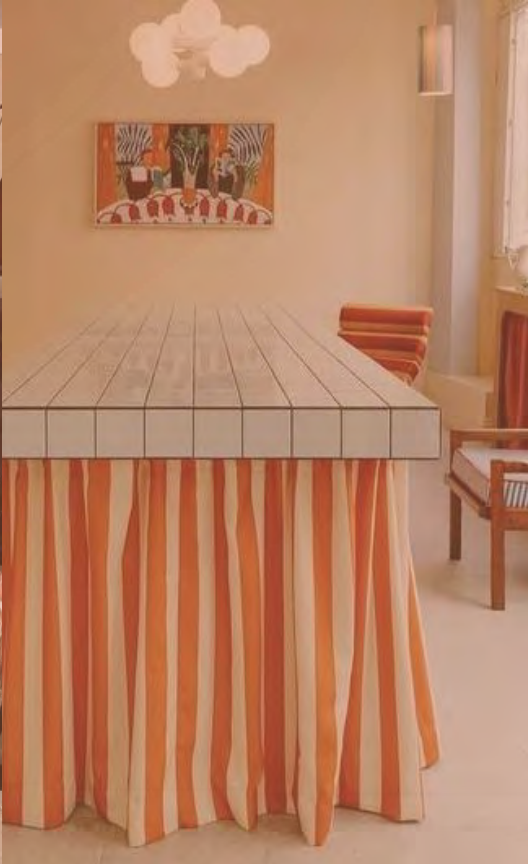


Key Plan (N.T.S)

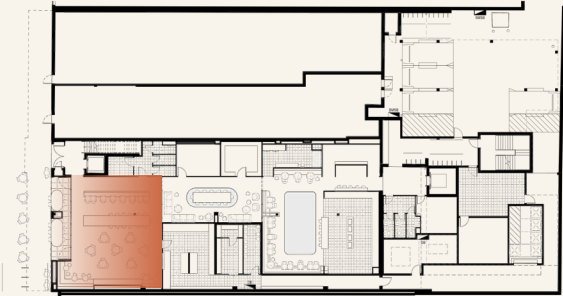


Public

Private



The Parklet & The Verandah

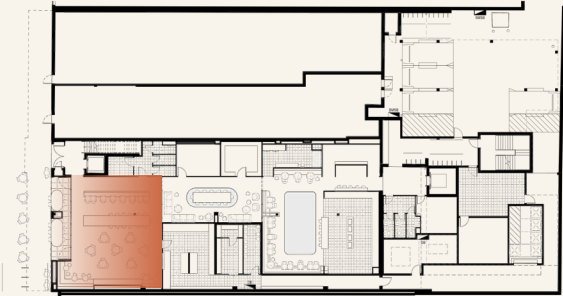


Key Plan (N.T.S)



Mural art to wall and underside of awning
(Art style and extent is indicative only, pending
engagement with local artist)

The Bar



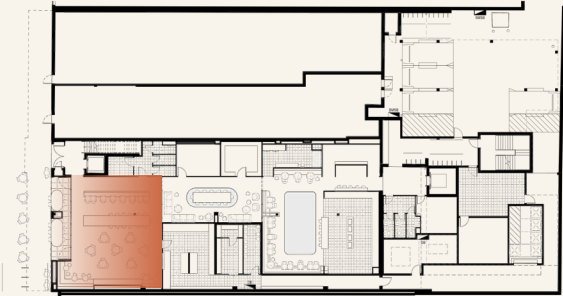
Key Plan (N.T.S)



19 Essex St Fremantle

4 - Functionality & Build Quality 6 - Amenity 7 - Legibility 9 - Community

The Parklet & The Bar



Key Plan (N.T.S)

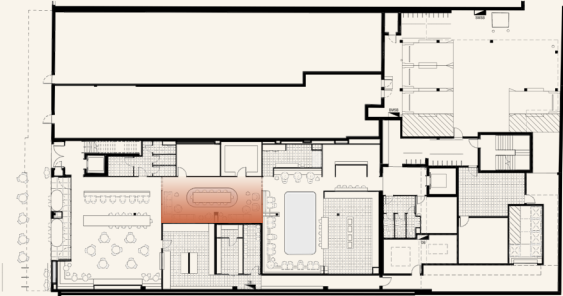




Lylo Check in

The Verandah

The Living Room



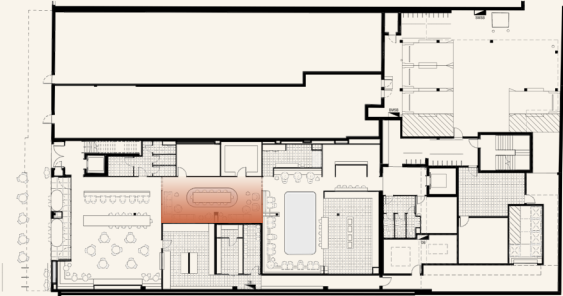
Key Plan (N.T.S)



19 Essex St Fremantle

4 - Functionality & Build Quality 6 - Amenity 7 - Legibility 9 - Community

The Living Room



Key Plan (N.T.S)



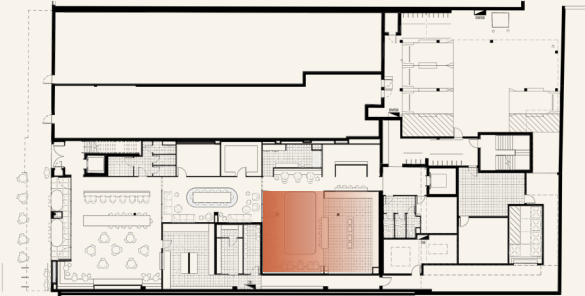


Lylo Dining Room

Welcome Home

19 Essex St Fremantle

The Dining Room



Key Plan (N.T.S)



Landscaping (Ground Floor)



Existing Parklet



Potted Planters



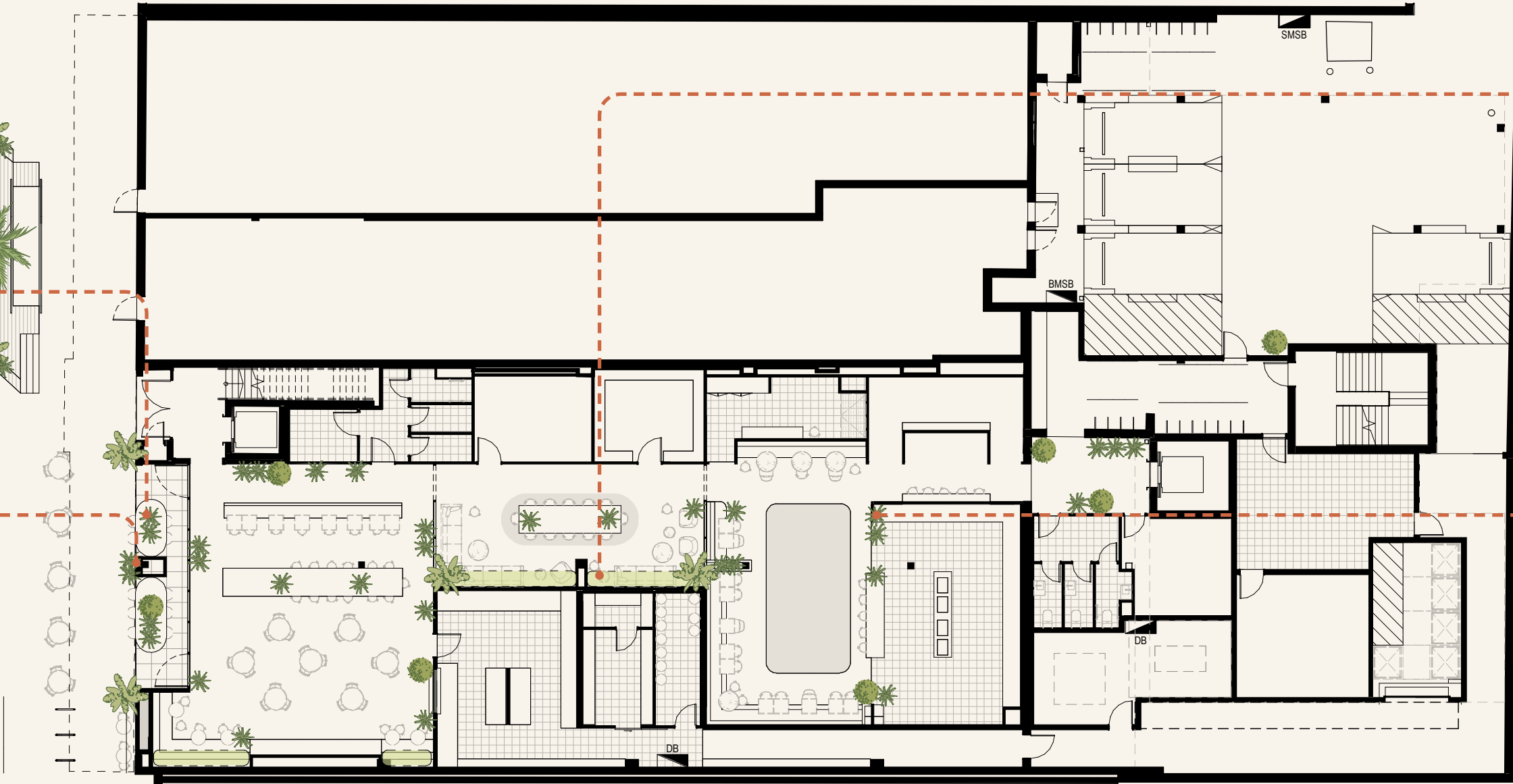
Column Planters



Hanging Planters

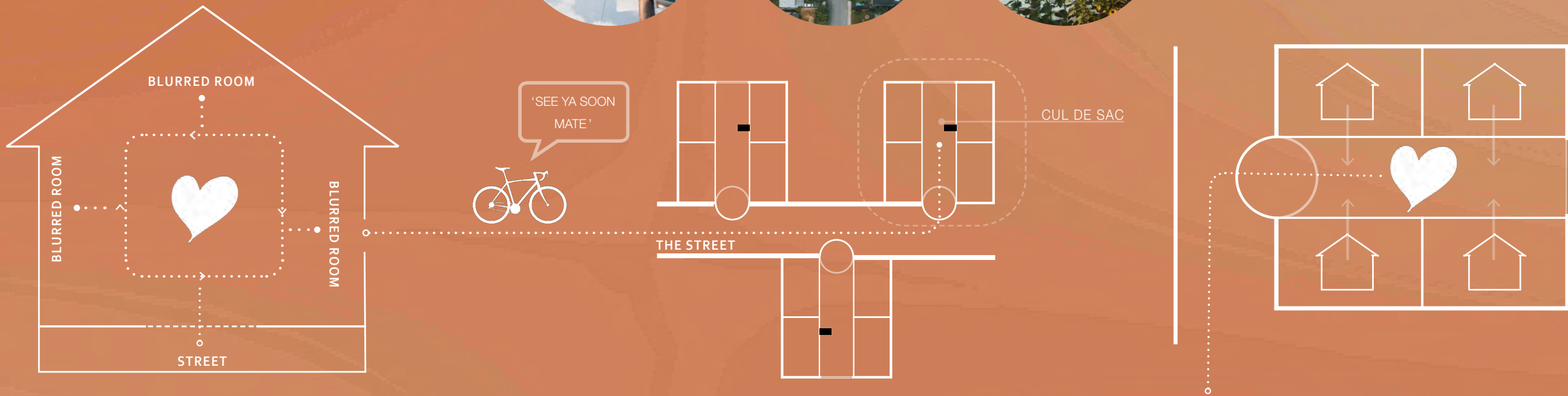


Potted Planters



First Floor Plan

Lylo Street



WELCOME TO OUR HOME,

Welcome to our home. The heart, our reception, our bar, our table.

A space to check in, a space to get a drink, a space to connect, a space to relax.

TAKE THE JOURNEY TO...

Take a ride down the 'street', To your 'cul de sac'. This is your hood.

Reminisce the good times when you'd ride through the suburban streets with all your mates...

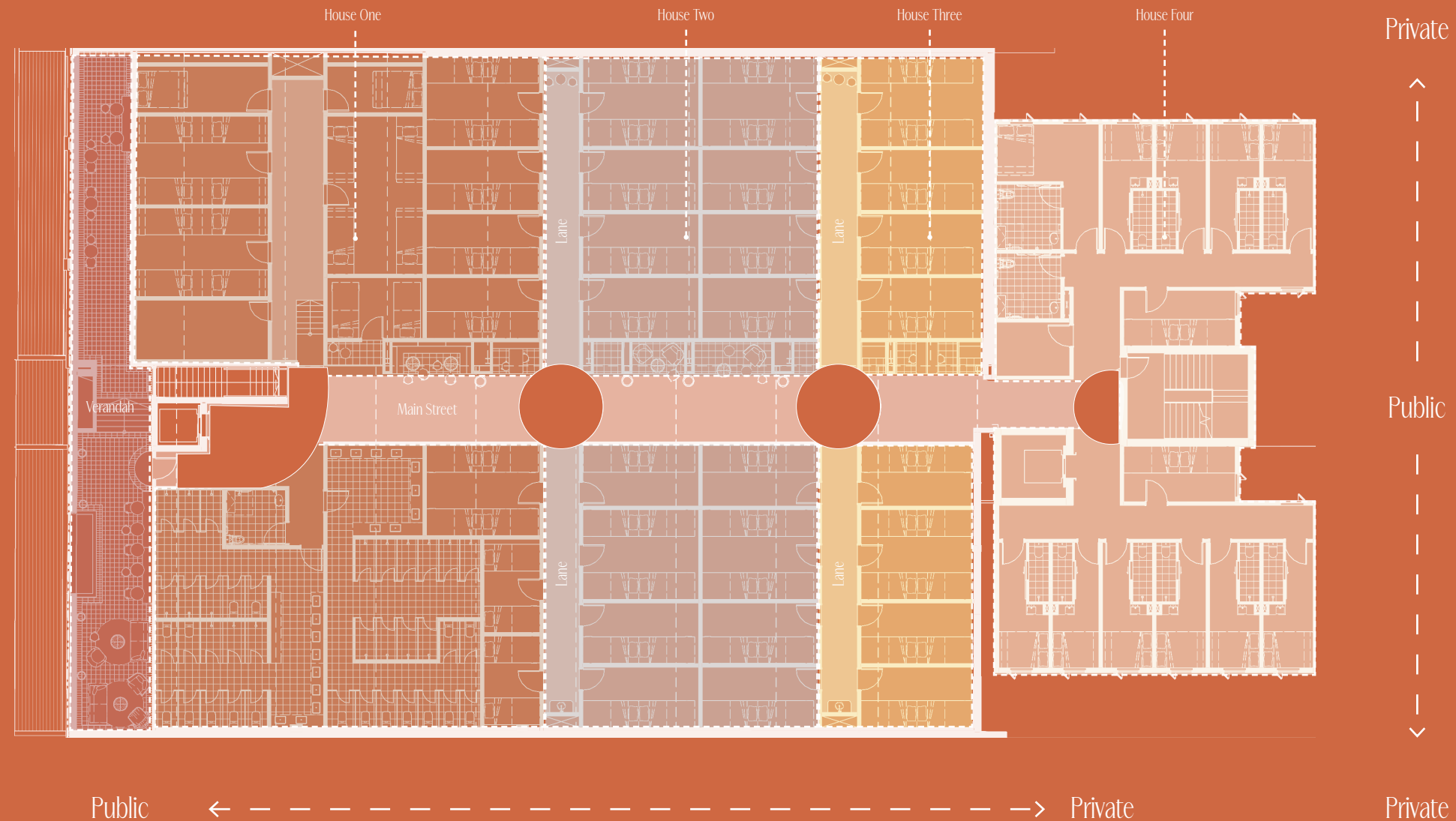
YOUR HOME

Retreat into your home, a space just for you. Safe, clean, warm.

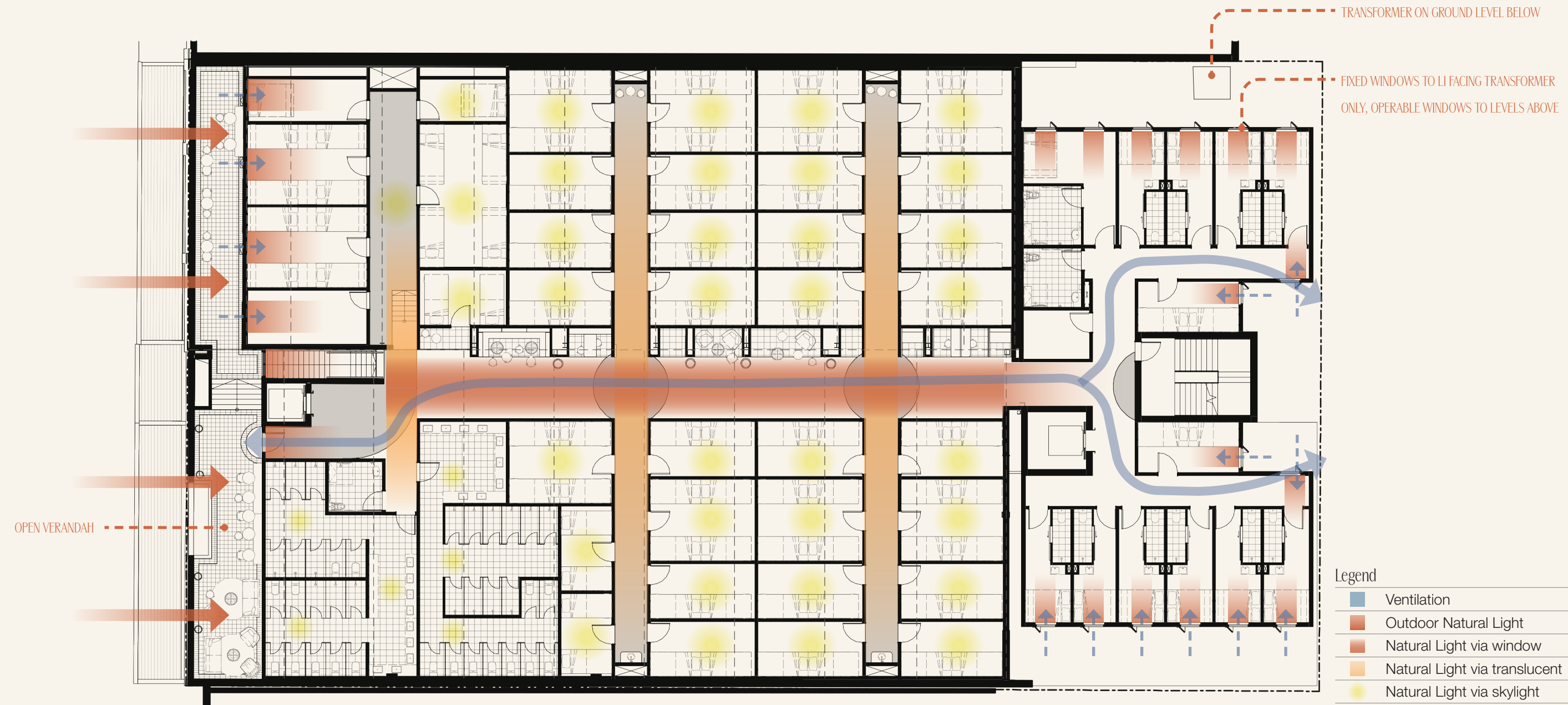
A space that is your retreat, but also a space that excites and connects you to your neighbours.

Lylo Street

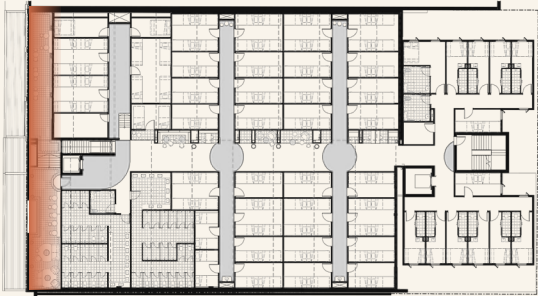
Take the journey to your home away from home !



Natural Light & Ventilation



The Verandah



Key Plan (N.T.S)



19 Essex St Fremantle

4 - Functionality & Build Quality 6 - Amenity



7 - Legibility 9 - Community



rothelowman
59



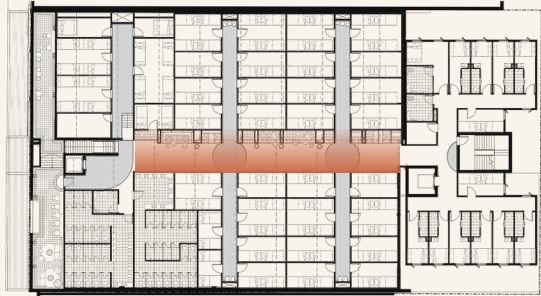
Lylo Street



The street is layered !

Lylo Street Elevation

First Floor



Key Plan (N.T.S)

House One

House Two

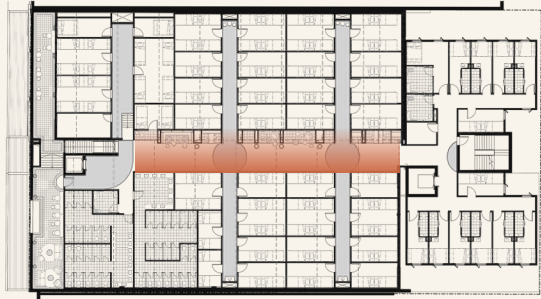
House Three



Take the journey to your home away from home !

Lylo Street

First Floor

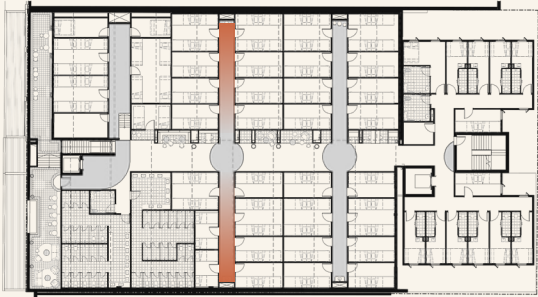


Key Plan (N.T.S)



Lylo Side Streets

First Floor



Key Plan (N.T.S)



19 Essex St Fremantle

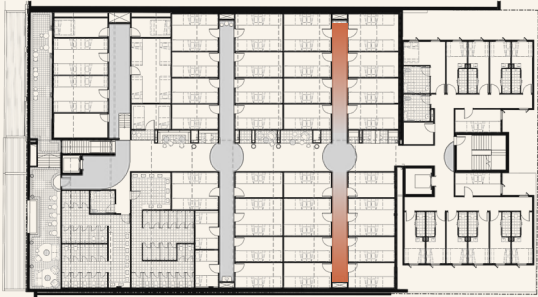
4 - Functionality & Build Quality 6 - Amenity

7 - Legibility 9 - Community

rothelowman

Lylo Side Streets

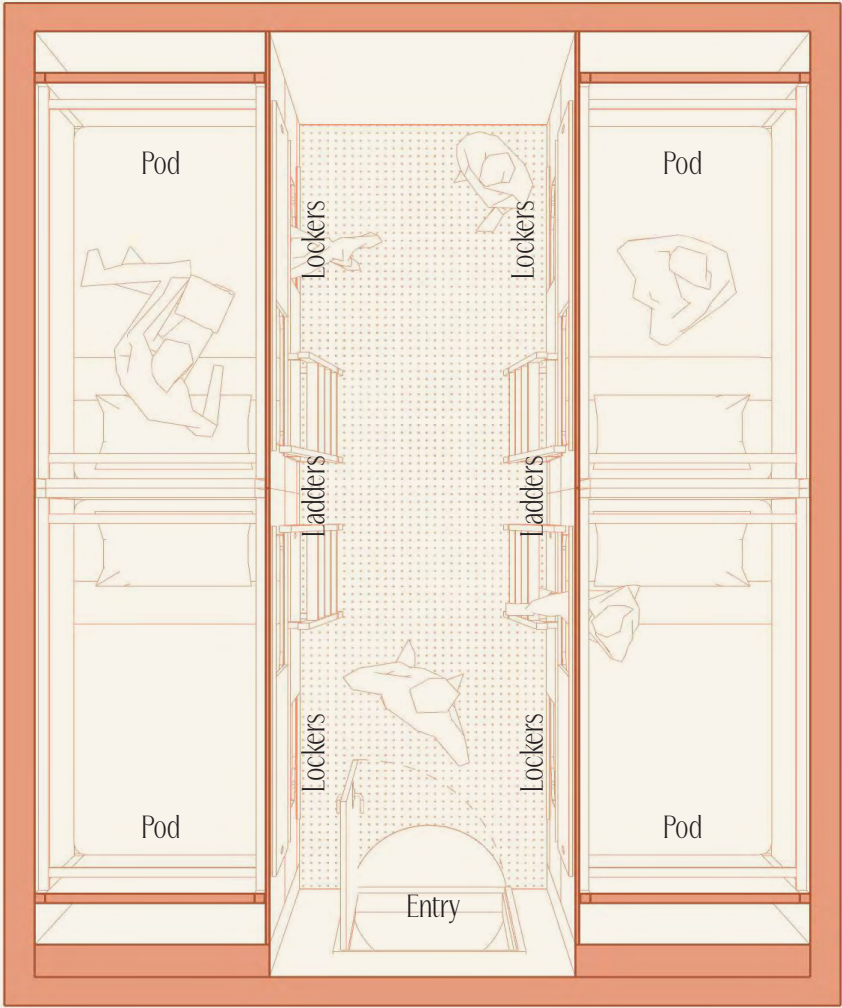
First Floor



Key Plan (N.T.S)



Pod Room Study



Floorplan (Lylo Fremantle)

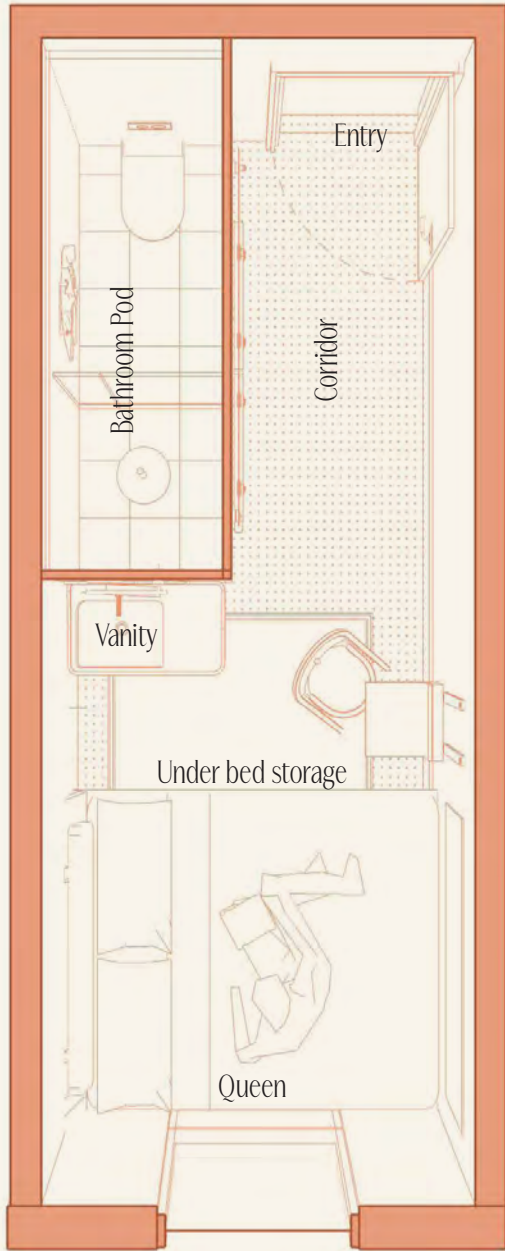


White Card (Lylo Fremantle)



Precedent (Lylo Auckland)

Ensuite Room Study



Floorplan (Lylo Fremantle)



White Card (Lylo Fremantle)



Precedent (Lylo Auckland)

Landscaping (First Floor)



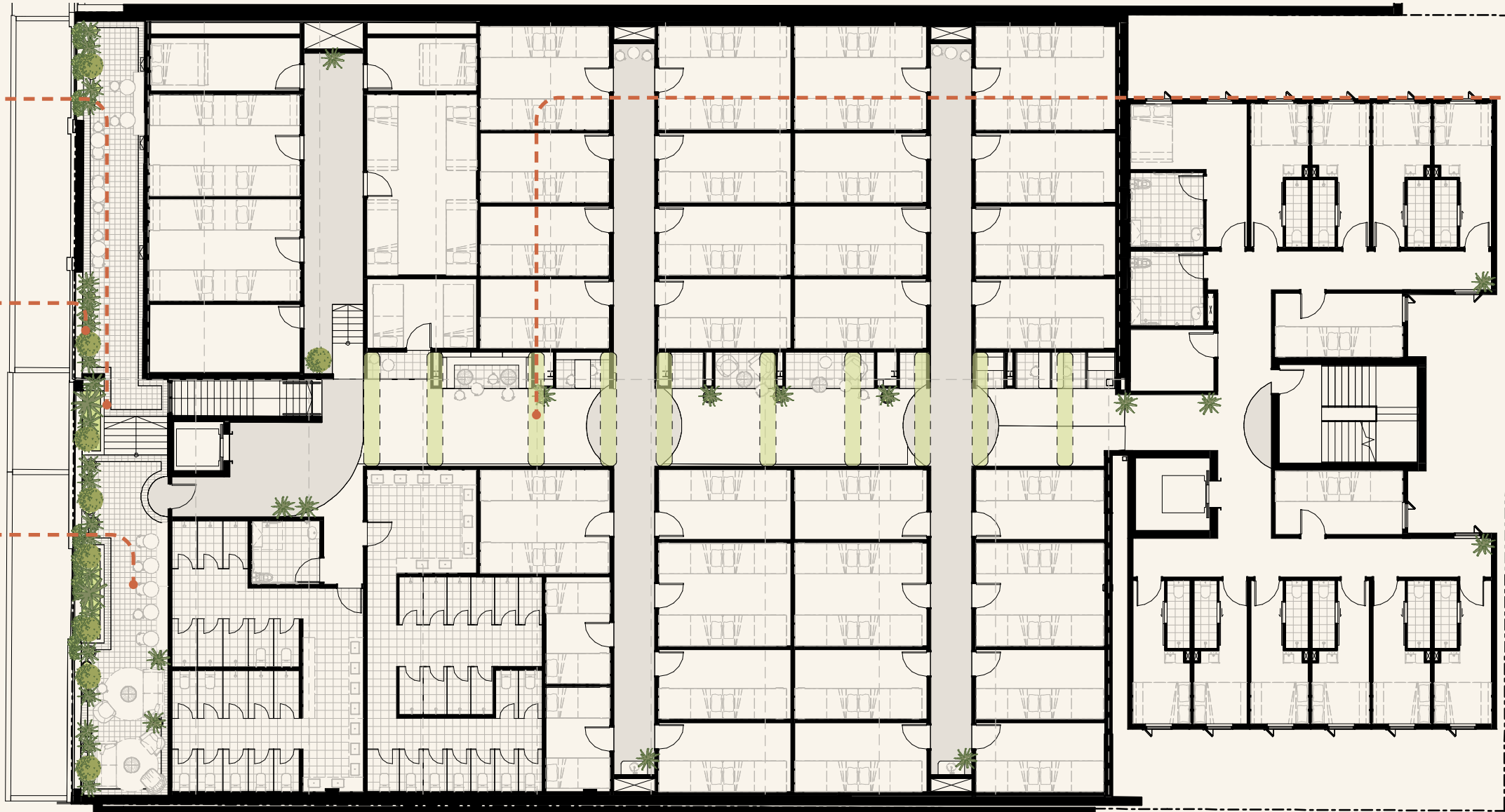
Integrated Planters



Potted Planters



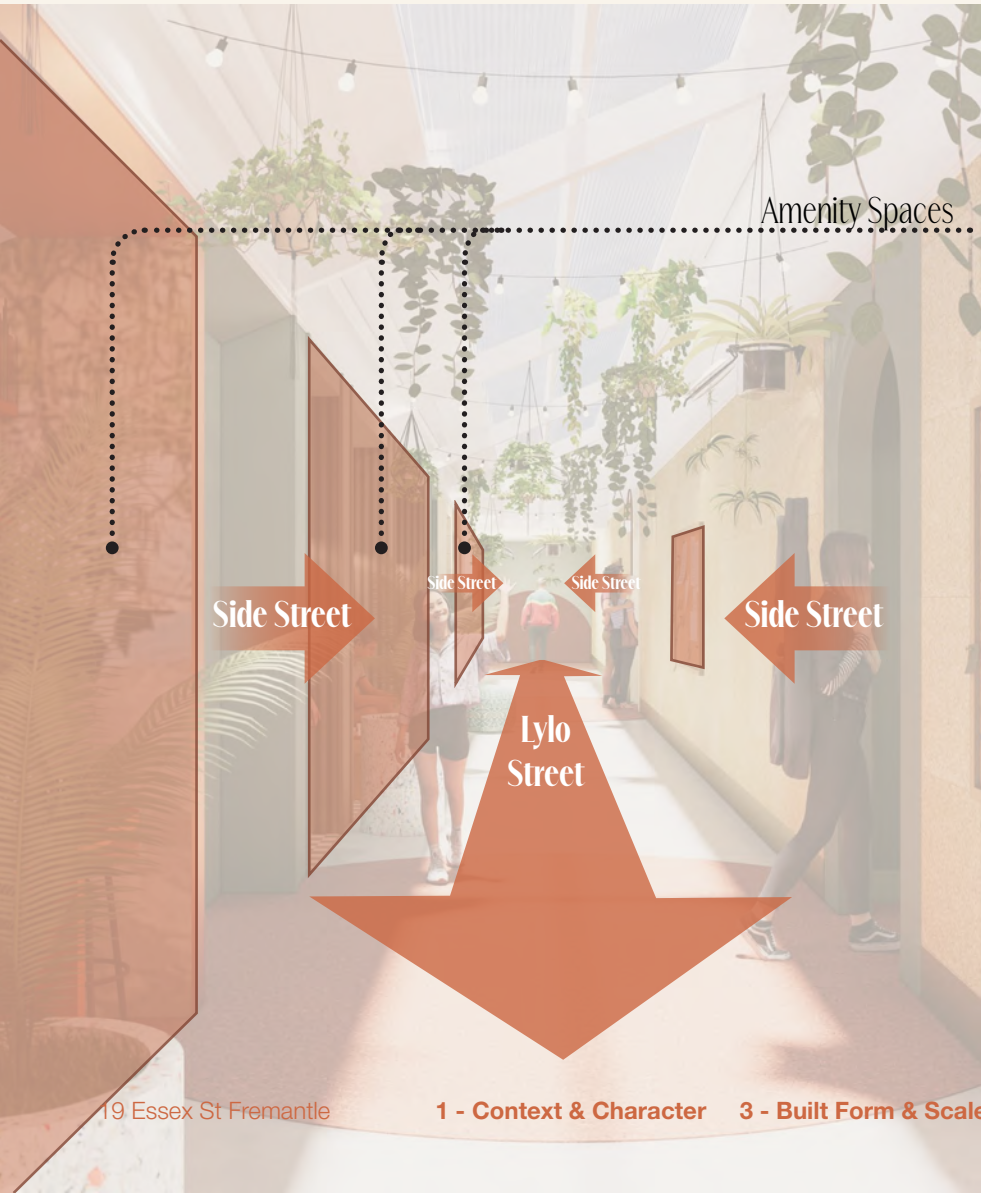
Outdoor Terrace



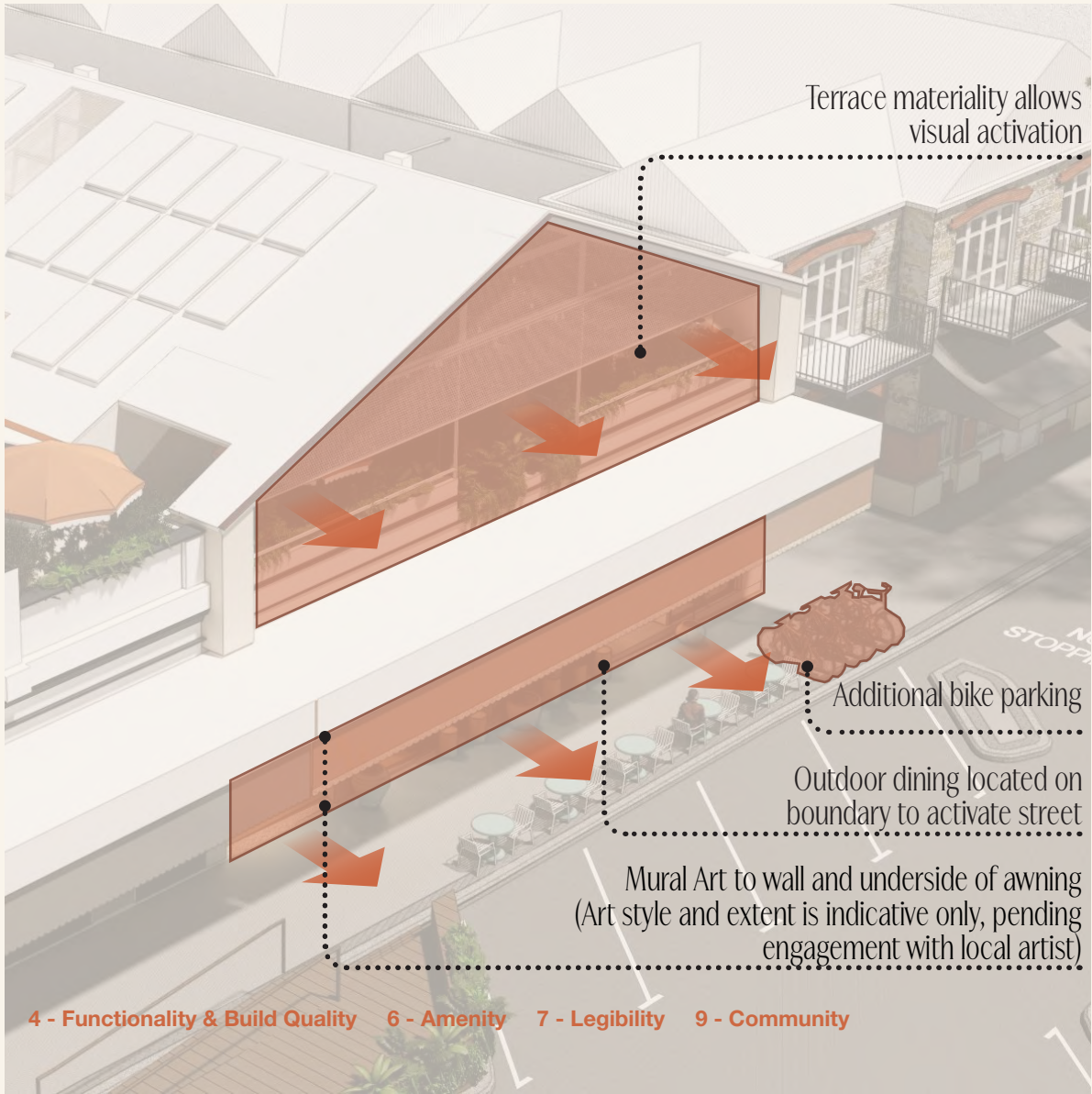
Hanging Planters

Community

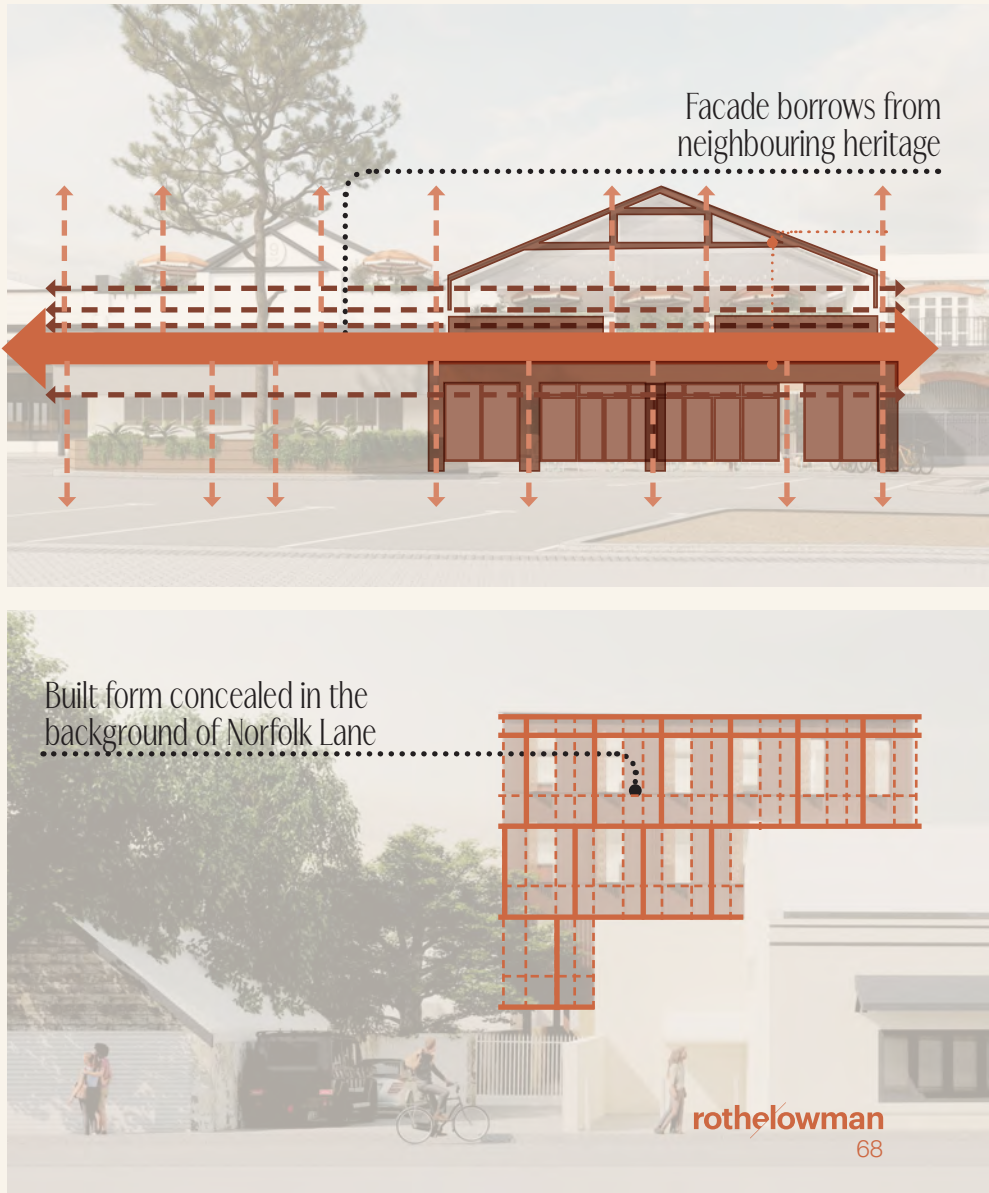
Promotes Social Interactions



Street Activation



Contributes to Heritage Context & Local Identity



Amenity



Communal Kitchen

Communal Dining

Check-In

Bike Store (Precedence)



Communal Laundry

Guest Lockers

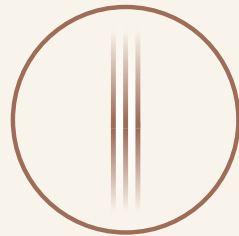
Grab N Go



Safety



Surveillance



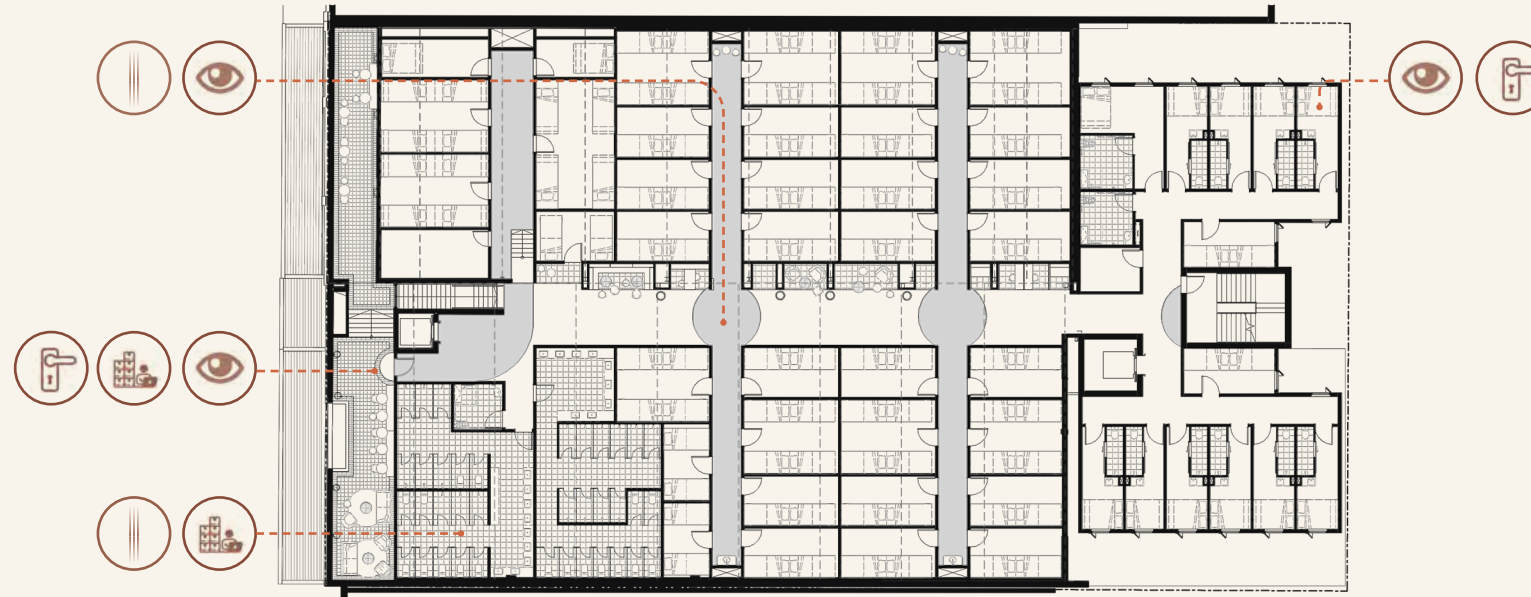
Territorial
Definition



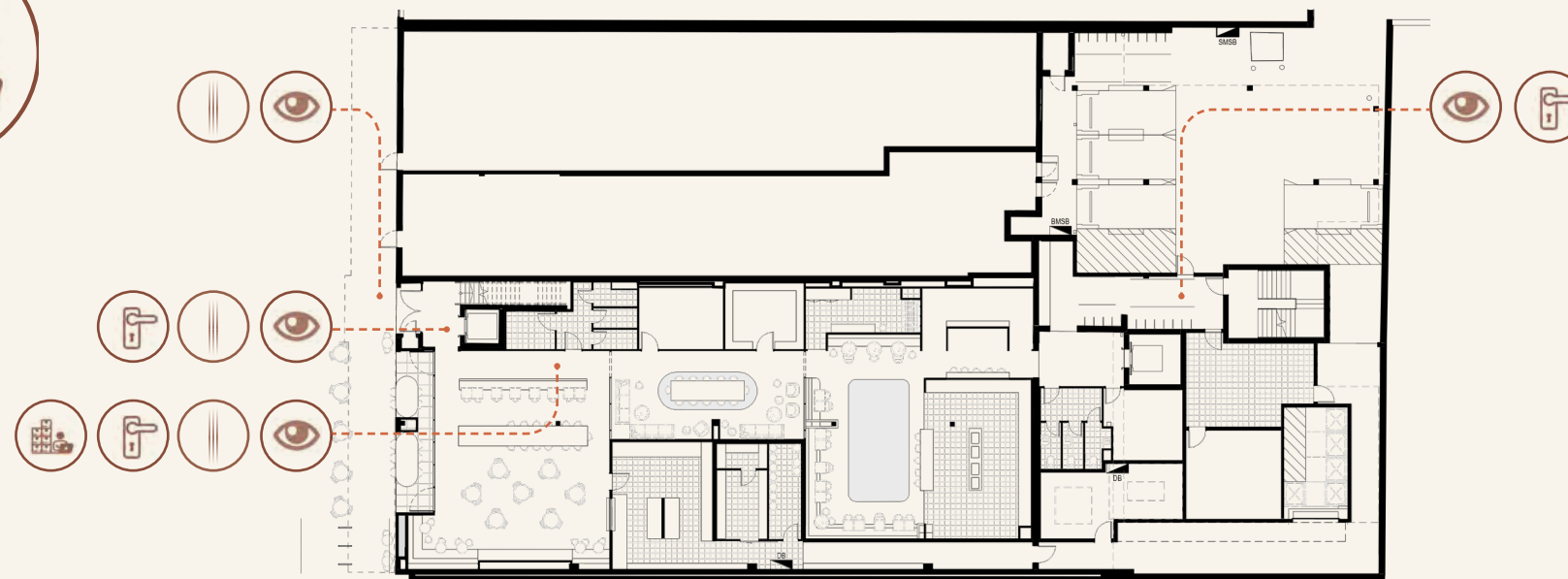
Access Control



Space
Management



First Floor Plan



Ground Floor Plan

Surveillance

- 1.1 Eyes on the Street
 - Communal spaces oriented to the building frontage increase the eyes on Essex Street at all hours.
- 1.2 Clear Sight lines
 - Entries created to allow maximum visual permeation from the street, without compromising a secure entry.
- 1.3 Effective Lighting
 - Primary facade provides ambient lighting to along footpaths.
- 1.4 Technological Surveillance
 - CCTV Cameras in strategic locations.

Access Control

- 2.1 Physical and landscape deterrent
 - Planters, bollards and built in furniture used to restrict vehicular access to pedestrian areas.
- 2.2 Target Control
 - Secure points / key fobs access limit public access through the building
 - CCTV used within the building

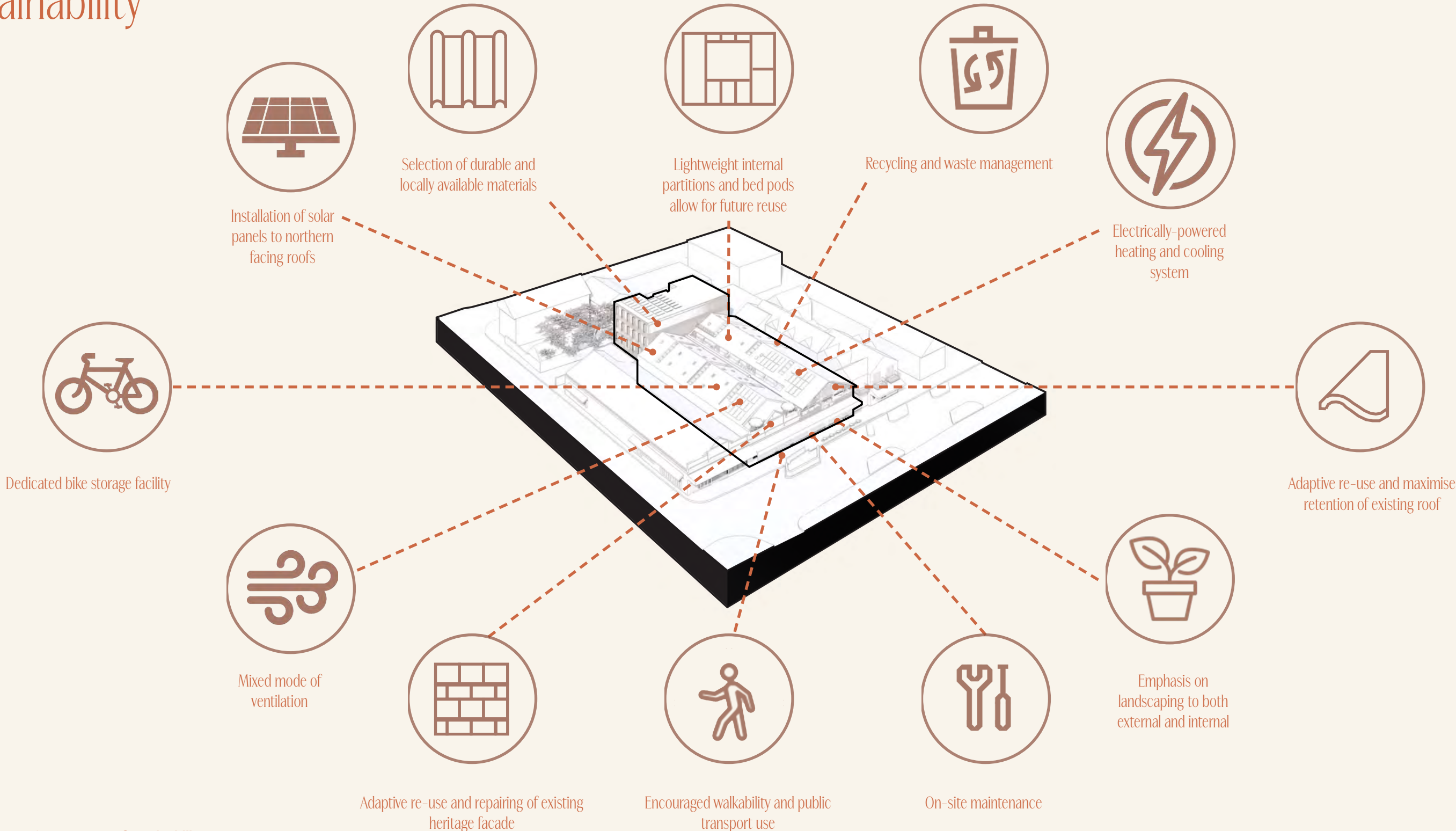
Space Management

- 3.1 Activity Support
 - Public cafe and bar adds pedestrian activity to the front of the building.
- 3.2 Space management
 - Frontage configured to minimise maintenance
 - 24/7 staff on site to ensure continuous space management

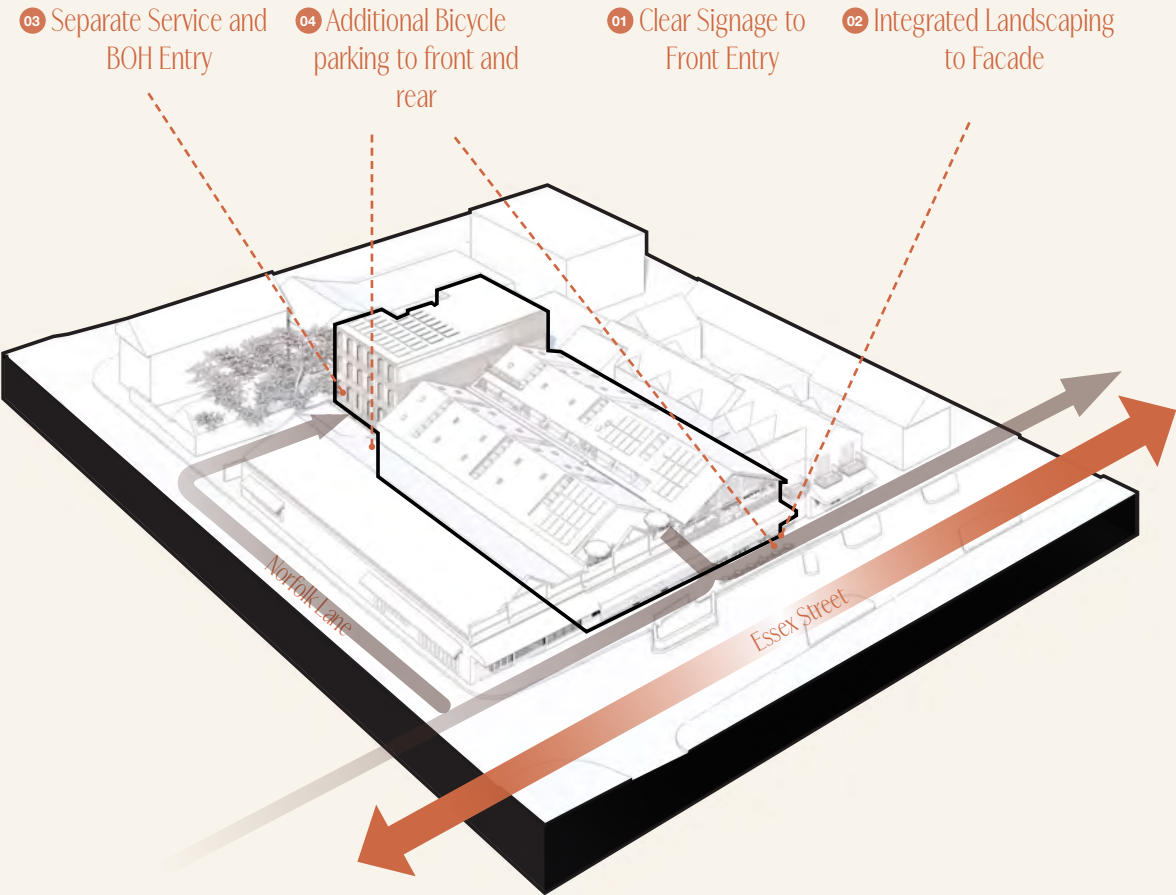
Territorial Definition

- 4.1 Clear Spatial Boundary
 - Public / maintenance entries clearly defined and secured.
 - Private entries secured by locks and fob access.
 - Separated rear service access path
- 4.2 7 - Legibility
 - Wayfinding signage designed into communal spaces and entry points.
- 4.3 Signage
 - Clear signage to front facade and rear facade

Sustainability



Legibility & Wayfinding



Lylo view from Essex St

- 01 Clear Signage to Front Entry
- 02 Integrated Landscaping to Facade

- 04 Additional Bicycle parking



Lylo view from Norfolk Lane, set behind 21 Essex St

- 03 Separate Service and BOH Entry
- 04 Additional Bicycle parking







Thank You.

Appendix 1 – Architectural Drawings

URBIS

HERITAGE IMPACT STATEMENT

19 Essex Street Fremantle
Whadjuk Noongar Country

Prepared for
EVT & BELINGBAK
7 June 2024

URBIS STAFF RESPONSIBLE FOR THIS REPORT WERE:

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Heritage Assistant	Dena Charles
Project Code	P0052039
Report Number	01 07.06.2024 FINAL

Urbis acknowledges the important contribution that Aboriginal and Torres Strait Islander people make in creating a strong and vibrant Australian society.

We acknowledge the Whadjuk people of the Noongar Nation , the Traditional Owners on whose land this project is located.

All information supplied to Urbis in order to conduct this research has been treated in the strictest confidence. It shall only be used in this context and shall not be made available to third parties without client authorisation. Confidential information has been stored securely and data provided by respondents, as well as their identity, has been treated in the strictest confidence and all assurance given to respondents have been and shall be fulfilled.

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dap002/24

14 Jun 2024

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1. EXECUTIVE SUMMARY

Urbis has been engaged by EVT & Belingbak to prepare the following Heritage Impact Statement for the adaptive reuse of 19 Essex Street, Fremantle (hereafter referred to as ‘the site’ or ‘the subject site’). This Heritage Impact Statement will assess the potential impacts of the proposed development on the cultural heritage significance of the place, its context an adjacent heritage places that may be affected.

The site is identified on the City of Fremantle Heritage List as part of the *Mills and Co Building (fmr)* (place no. 3708) and is also located within the *Central Fremantle Heritage Area (Fmr West End Conservation Area)* (place no.22601)

This Heritage Impact Statement will accompany the Development Application (DA) seeking consent for the proposed works.

A detailed impact assessment has been undertaken in Section 6. Overall, this report has found the following:

- 19 Essex Street is not intact and has undergone substantial alteration in the late 20th century including the raising of 19b’s roof to accommodate a first floor and the construction of a new infill building at 19a.
- The proposed works are consistent with the Australian ICOMOS Burra Charter principles, conserving all of the site’s most significant fabric of the 19b Essex Street Facade. Demolition and alterations are reserved for contemporary fabric that does not contribute to the heritage significance of the place.
- The proposed four storey addition is proposed to be located at the rear of the site and only have inconsequential incidental visibility from Essex Street as well as in the background of State listed heritage sites 26-28 Norfolk Street (Place No. 0966) and Fremantle Technical college Annexe (Place No. 01007).
- The modest modular architectural design and red brown steel materiality responds to the existing colour scheme of the streetscape which includes red brick and clay tiles and affirms the hierarchy of the finer architectural detailing of the streetscape and wider precincts significant heritage places.

For the reasons stated above, the proposed works are suitable for approval from a heritage perspective.

1.1. RESPONSE TO DAC COMMENTS

The project team attended a meeting with the City of Fremantle’s Design Advisory Committee (DAC) on the 13th Of May 2024. Comments of most relevance to the cultural heritage significance of the place pertaining to *Principle 1: Context and Character* and are outlined along with where the relevant response can be located.

Table 1: DAC Comments

DAC Comments	Response location
Principle 1 Context and character	
a) <i>The Proponent should ensure the early consideration of the integration of heritage with the new proposal. The Proponent should demonstrate an understanding of what is of heritage significance and what is not, and how much significant fabric is to be retained or demolished and its impact assessment on the heritage values of the place</i>	See section 5.4 Grading of significance
b) <i>Provide a Heritage Impact Statement (HIS) prior to the submission of the Development Application. The impact assessment should address, avoid and propose a mitigation strategy for the demolition of significant fabric and aesthetical streetscape impact. In addition to the</i>	See section 7. Assessment of Heritage Impact

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DAC Comments	Response location
Principle 1 Context and character	
<p><i>Local Heritage Listed structures, the HIS also should address the State Registered Properties near to and adjacent to the site.</i></p> <p><i>The DAC suggested providing a streetscape perspective of the proposal looking north along Essex Street and to include the Fremantle Technical School.</i></p>	
<p><i>c) 19 Essex Street is essentially the amalgamation of two buildings on two lots (19A and 19B). The Proponent should maintain the character of Essex Street by respecting the existing fine grain and rhythm of the lot pattern and buildings.</i></p> <p><i>The DAC recommends a review of the proposed new awning (19A) and its exact horizontal alignment with the existing awning of the Local Heritage Listed building (19B). The awning is proposed to extend across the entire width of the building frontage as a consistent horizontal line; unfortunately, in doing so, the impression is conveyed of one large building.</i></p>	See section 7. Assessment of Heritage Impact

2. INTRODUCTION

2.1. BACKGROUND

Urbis has been engaged by EVT & Belingbak to prepare the following Heritage Impact Statement for the adaptive reuse of 19 Essex Street, Fremantle (hereafter referred to as 'the site' or 'the subject site'). This Heritage Impact Statement will assess the potential impacts of the proposed works on the cultural heritage significance of the place.

The site is identified on the City of Fremantle Heritage List as part of the *Mills and Co Building (fmr)* (place no. 3708) and is also located within the *Central Fremantle Heritage Area (Fmr West End Conservation Area)* (place no.22601)

The proposed adaptive reuse development designed by Rothelowman in consultation with the heritage team at Urbis, seeks to develop the existing two buildings at 19 Essex Street with internal fitout and alterations and a new multistorey addition to the rear to facilitate short stay accommodation.

This Heritage Impact Statement will accompany the Development Application (DA) seeking consent for the proposed works.

Utilising the HCWA Guidelines, this Heritage Impact Statement assesses the potential heritage impacts against the cultural significance attributed to the site by the City of Fremantle.

2.2. SITE LOCATION

The subject site is located at 19 Essex Street, Fremantle. Boundaries are shown below. The subject site includes both buildings 19a and 19b.



Figure 1: Location of the subject site, boundary indicated in red.

Source: Nearmaps with Urbis overlay

2.3. METHODOLOGY AND RELEVANT DOCUMENTS

This Heritage Impact Statement has been prepared in accordance with the HCWA Guidelines and utilises the statements of significance prepared by the City of Fremantle. The philosophy and process adopted is guided by *The Burra Charter: the Australia ICOMOS Charter for Places of Cultural Significance, 2013*.

The proposed works been assessed in accordance with the guiding document prepared by the Department of Planning, Lands and Heritage (DPLH) entitled Heritage Impact Statement – A Guide (HCWA, 2012) to address the following questions:

- How will the proposed works affect the heritage significance of the place?
- What measures (if any) are proposed to ameliorate any adverse impacts?
- Will the proposal result in any heritage conservation benefits that may offset any adverse impacts?

The following documents have informed the preparation of this Heritage Impact Statement:

- Lylo Fremantle DA Issue 1, Rothlowman 5 June 2024
- City of Fremantle Heritage List 2022
- City of Fremantle, Local Planning Policy 3.6 Heritage Areas 2022
- HCWA Register of Heritage Places – Assessment Documentation Fremantle Technical College Annexe 1/10/2002
- HCWA Register of Heritage Places – Assessment Documentation 26-28 Norfolk Street 23/05/1997

2.4. AUTHOR IDENTIFICATION

This report has been prepared by Isabella Bellotti (Senior Heritage Consultant), Wade Goldwyer (Consultant) and Dena Charles (Heritage Assistant). Marc Beattie (Director, Heritage) has reviewed and endorsed the report.

Unless otherwise stated, all drawings, illustrations and photographs are the work of Urbis.

3. SITE CONTEXT

3.1. EXTERIOR CONDITION

The subject site is located at 19 Essex Street on the south side of the street. The site consists of two buildings 19a to the south and 19b to the north. The buildings are setback from the street by a wide pedestrian footpath. An outdoor seating area is located on the existing footpath and around the mature pine tree located in road reserve.

19b is located to the north of the site and consists of part of the former *Mills and Co Building* built in c1907. The building is defined by a single storey brick façade with a modest parapet and central pediment above the stucco cornice. The facade originally featured a central timber entrance door with highlight above a large display window to north and two timber sash windows to the south. Post 1985 the facade was divided into two shops with the northern timber sash windows removed and a new timber framed shop window and door ensemble installed. A contemporary boxed awning shades the entrances and is tied back into the façade.

The parapet appears to have been altered when the first-floor slab was constructed in c1993. A corrugated skillion roof containing plant equipment is located behind the parapet, however historical development applications indicate this area previously had an accessible terrace. The building's corrugated metal gabled form roof is also not original having been raised in 1993 to accommodate the first floor and then extended and reclad in 2013. Internally the building maintains its early timber roof truss system although it has been retrofitted to a new steel roof structure and appears purely aesthetic.

Historical photographs indicate that the facade was originally tuck-pointed red face brick with a rendered parapet and skillion verandah. With cast iron posts. The verandah is no longer extant, and the brick has since been painted.

19a was constructed in 2013. The two-storey building features a gabled roof form echoing the form of the adjacent heritage building. The ground floor has a similar boxed awning to 19b separated by a pitched awning over a central shared entrance to the first floor of both buildings. The ground floor is painted and tiled while the first floor is clad with metal and has aluminium framed glazing.

The two buildings are connected on the first floor by an open aired walkway which is accessed by a staircase and lift on the ground floor.

The rear of both buildings match being a part of the 2013 development. The gabled roofed forms have a ground floor undercroft for carparking and fire escape stair located to the north. The carpark is accessed via a right of way from Norfolk Lane. The shared boundary walls are a combination of brick and limestone with a portion of this wall shared with State Register of Heritage Places no.966 26-28 Norfolk Street.



Figure 2: Existing condition of 19 Essex Street, Fremantle.

Source: Urbis 2024



Figure 3: 19b Essex Street, Fremantle principal frontage.

Source: Urbis 2024

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Figure 4: 19b Essex Street non original entrance.

Source: Urbis 2024



Figure 5: Rear (east) facade of 19 Essex Street Fremantle existing condition.

Source: Urbis 2024



Figure 6: 19b Essex Street view behind the parapet showing skillion roof form.

Source: Urbis 2024



Figure 7: existing condition of the part of the boundary limestone wall to the east shared with 26-28 Norfolk Street.

Source: Urbis 2024

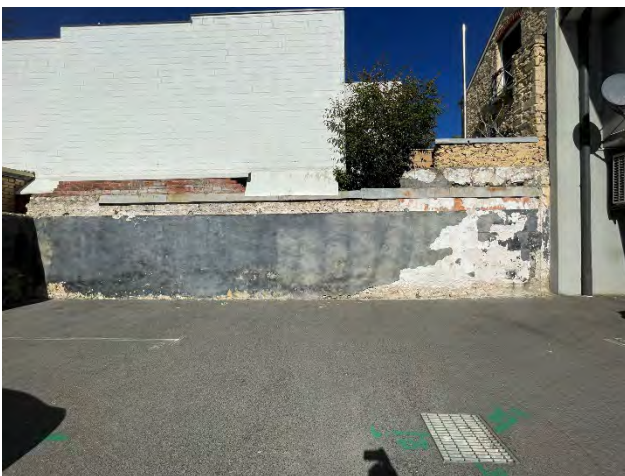


Figure 8: existing condition of the south boundary wall of the carpark.

Source: Urbis 2024



Figure 9: Norfolk Lane approach to the rear carpark of 19 Essex Street, Fremantle.

Source: Urbis 2024

3.2. INTERIOR CONDITION

The ground floor of 19b Essex Street has been divided into two separate venues, an escape room to the north and a hospitality venue in the south. Early timber floorboards and skirtings and micro-orb ceiling linings are still evident in both interiors.

The first floor is contemporary and contains office fitouts connected by an open aired walkway. The extent of original return wall is not apparent, but the first-floor south wall appears entirely contemporary.



Figure 10: existing condition of 19b Essex Street exposed roof.

Source: Urbis 2024



Figure 11: 19b Essex Street early timber roof truss system has been partly retained and used as a non-structural design feature.

Source: Urbis 2024

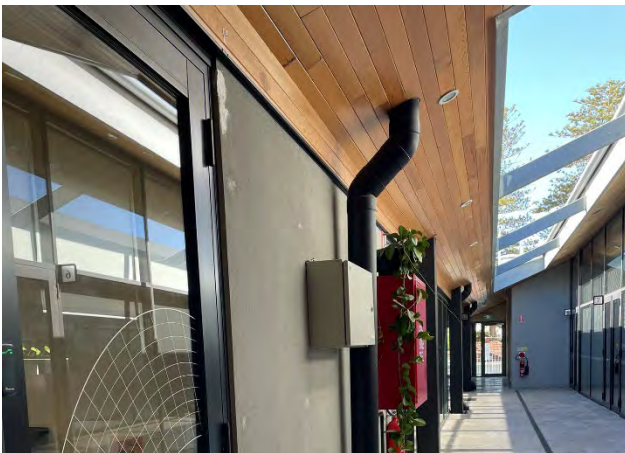


Figure 12: open aired walkway between 19a Essex Street and 19b Essex Street office tenancies.

Source: Urbis 2024

4. HISTORICAL OVERVIEW

- Essex Street was included in the first town plan for Fremantle in 1832 (Figure 13). The subject site (originally part of lots 169 and 170, now 19) was purchased by G. W. Davies, and remained vacant land with a low limestone boundary wall.¹ (Figure 14 & Figure 15).
- The subject site was later subdivided and purchased in 1904-05 by Frederick Instone. Instone was granted approval to build a factory and office in 1906² on the site spanning 19-20 Essex Street.
- Instone is remembered for his woodstoves, some of which are still in Fremantle houses today.³
- In 1907, construction of the brick office and galvanised iron store/factory were completed.⁴
- The original building was 90 x 90 ft. The showroom and office in the front were constructed with brick, and the factory at the rear with iron.⁵
- Plans to extend the building were approved in 1913 as can be seen in a Metropolitan Sewerage map dated 1914 (Figure 16). Completed in 1915 the new section was erected by Mr Instone for Mr Mills & Co and comprised a shop and showroom. A 1915 newspaper advertisement shows the completed building (Figure 19).
- Following the death of Mr Instone, the property was inherited by Charles Purdie, who kept Instone & Co running until the early 1970s⁶.
- In 1941, Allan and Nicholas (architects) were granted approval for proposed alterations to the Instone factory which included: the construction of a new building next to the existing one, the demolition of existing buildings at the rear of the site, removal of a portion of lean-to in the front yard, and the removal of the existing front wall⁷. The addition can be seen in Figure 20.
- By the 1970s the skillion verandah on 19b Essex Street had been removed and replaced by a cantilevered boxed awning, a reiteration of this awning is still extant today. (Figure 20)
- By 1985 the original Instone & Co building was now a retail store called Tropicane (Figure 21).
- The original roof was raised in 1993 to accommodate a first-floor office. It appears that original materials were reused including the timber roof truss structure⁸ This development also included a first-floor terrace behind the original parapet.
- In December 2012, the building 19a built in 1941 was demolished (Figure 23). The rear of the property included the installation of underground utilities. Construction of the existing two storey mixed used building was completed by October 2013 (Figure 24).
- During the 2013 development, the roof of the original Instone & Co building was renewed with new roof sheeting to match 19a (Figure 24). It appears that the new roof sheeting and a new metal roof structure was added at this time over the existing timber structure. This changed the pitch of the roof⁹.

¹ InHerit. 2020. Mills & Co Building (fmr). <https://inherit.dplh.wa.gov.au/Public/Inventory/Details/d9b124c7-ae2d-4d41-b60c-871dc1b24546>

² Ibid.

³ Gillard, G. 2024. Fred Instone. <https://freotopia.org/people/instone.html>

⁴ InHerit. 2020. Mills & Co Building (fmr). <https://inherit.dplh.wa.gov.au/Public/Inventory/Details/d9b124c7-ae2d-4d41-b60c-871dc1b24546>

⁵ Ibid.

⁶ Ibid.

⁷ Ibid.

⁸ Casella Edgar and Wade Architects. 1993. Proposed Restaurant Development 1st Floor Lot No 8 Essex Street Fremantle.

⁹ Proposed Commercial Fit-Out at: Lot 8 / #19 Essex Street Fremantle W.A. 6160. Certified Construction Drawings. 2013.

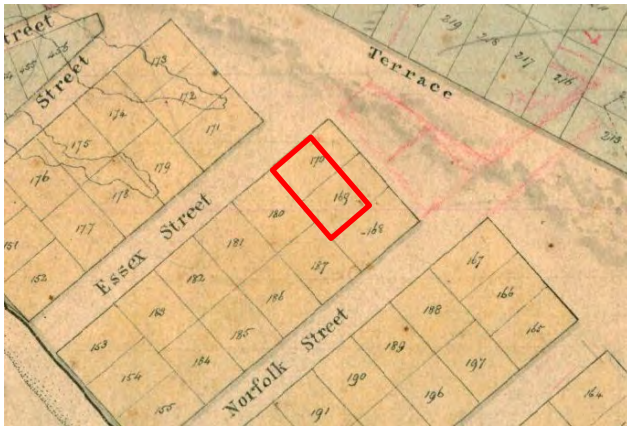


Figure 13: Original 1838 town plan of Fremantle.

Source: Barteaux, J. 2016. *Urban planning as colonial marketing strategy for the Swan River Settlement, Western Australia. With Urbis overlay in red*

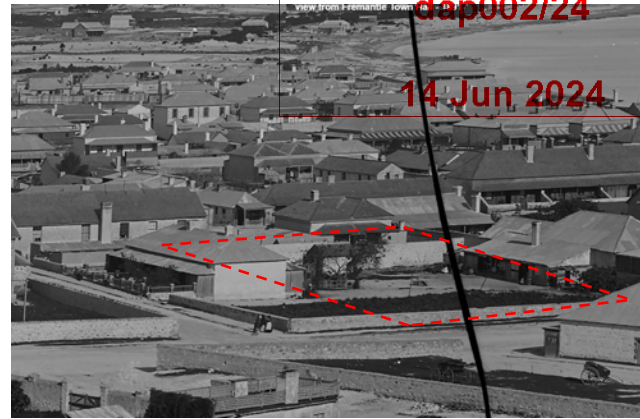


Figure 14: Subject site as viewed from Fremantle Town Hall 1891

Source:
<https://www.historicalpanoramas.com.au/tour/index.html>

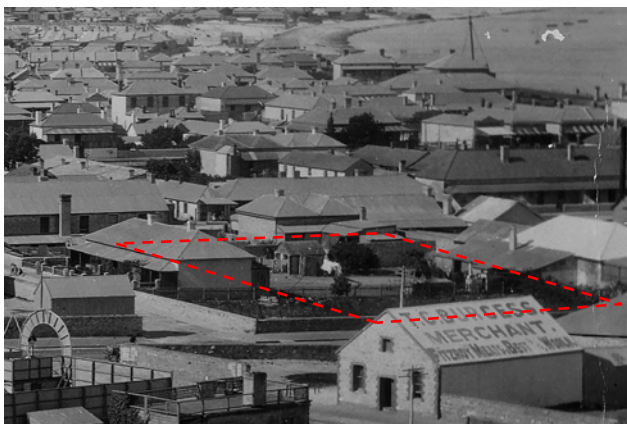


Figure 15: Subject site as viewed from Fremantle Town Hall 1899

Source:
<https://www.historicalpanoramas.com.au/tour/index.html>



Figure 16: photograph of F. Instone and Co Ltd c.1910 now 19b Essex Street.

Source: Fremantle Library Ref: 4376

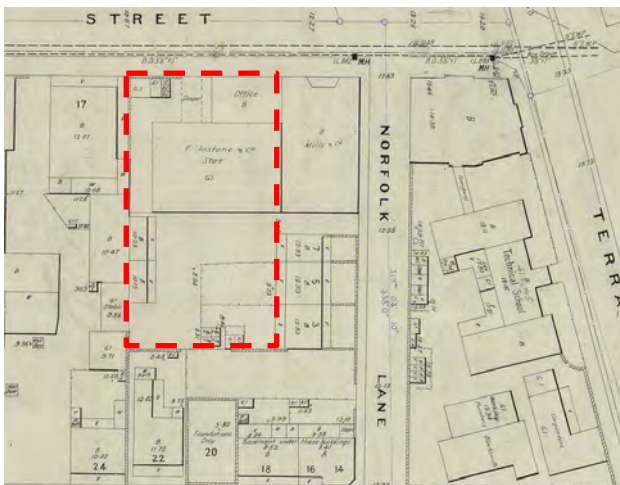


Figure 17: 1914 town plan of Fremantle showing the original site structure and the addition of Mills & Co.

Source: State Records Office of Western Australia.
 RetroMaps with Urbis overlay



Figure 18: Advertisement from 1915 showing the original Instone & Co building, and Mills & Co on the left.

Source: Western Mail (Perth, WA: 1885 – 1954) 25 December 1915: 12.



Figure 19: Mills and Co Buildings 1918 during a Naval procession.

Source: Fremantle Library, Ref: 770B



Figure 20: Image taken in 1970 showing the original Instone & Co building, with the 1941 addition on the right and cantilevered awning.

Source: HistoricalPanoramas.com.au. View from Fremantle Town Hall. 1970.



Figure 21: Photograph from 1985 showing tenancy changes of both Instone & Co, and Mills & Co. The 1941 extension shown on the right remains intact.

Source: SLWA. 313875PD: Harbour City Auctions and Tropicane, 19-21 Essex Street, Fremantle. 1985.



Figure 22: Photograph from 2000 showing the raised roof line from 1993.

Source: SLWA. 370895PD: Essex Street from the corner of South Terrace, Fremantle, Western Australia. 2000.

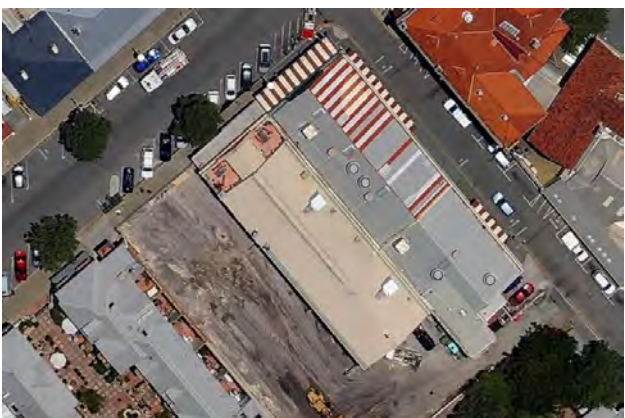


Figure 23: Aerial from 2012 showing the demolition of the 1941 extension.

Source: NearMaps. 2012. 19 Essex St, Fremantle WA 6160, Australia (Sat Dec 15, 2012).



Figure 24: Aerial from 2013 showing the new and existing building at 19a Essex Street.

Source: NearMaps, 2013. 19 Essex St, Fremantle WA 6160, Australia (Mon Oct 28, 2013).

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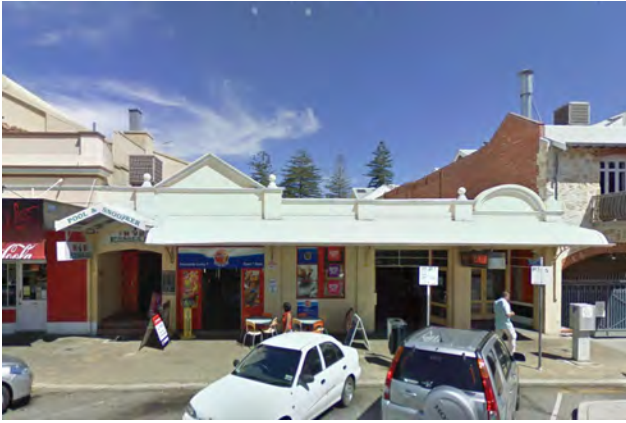


Figure 25: Streetview of 19a Essex Street c2009 showing post 1985s faux heritage masonry building (now demolished)

Source: Google Streetview December 2009

5. HERITAGE SIGNIFICANCE

5.1. HERITAGE LISTINGS

The site has been recognised as a place of cultural heritage value through inclusion on the following statutory heritage lists:

Table 2: 19 Essex Street Fremantle heritage listings

No.	Place or item	Description	Notes
City of Fremantle Heritage List			
03908	Mills & Co Building (fmr)		Listed 8 March 2007
22601	Central Fremantle Heritage Area (Fmr West End Conservation Area)		Listed 8 March 2007
City of Fremantle Heritage Inventory			
	Mills & Co Building (fmr)	Category Level 2.	The City of Fremantle has identified this place as being of considerable cultural heritage significance in its own right within the context of Fremantle and its conservation is a priority.
Classified by the National Trust			
Classified			2 November 1981



Figure 26: Heritage curtilage of 3908 Mills and Co Building (fmr) in red with the extent of the subject site 19 Essex Street in yellow.

Source: City of Fremantle, Public maps

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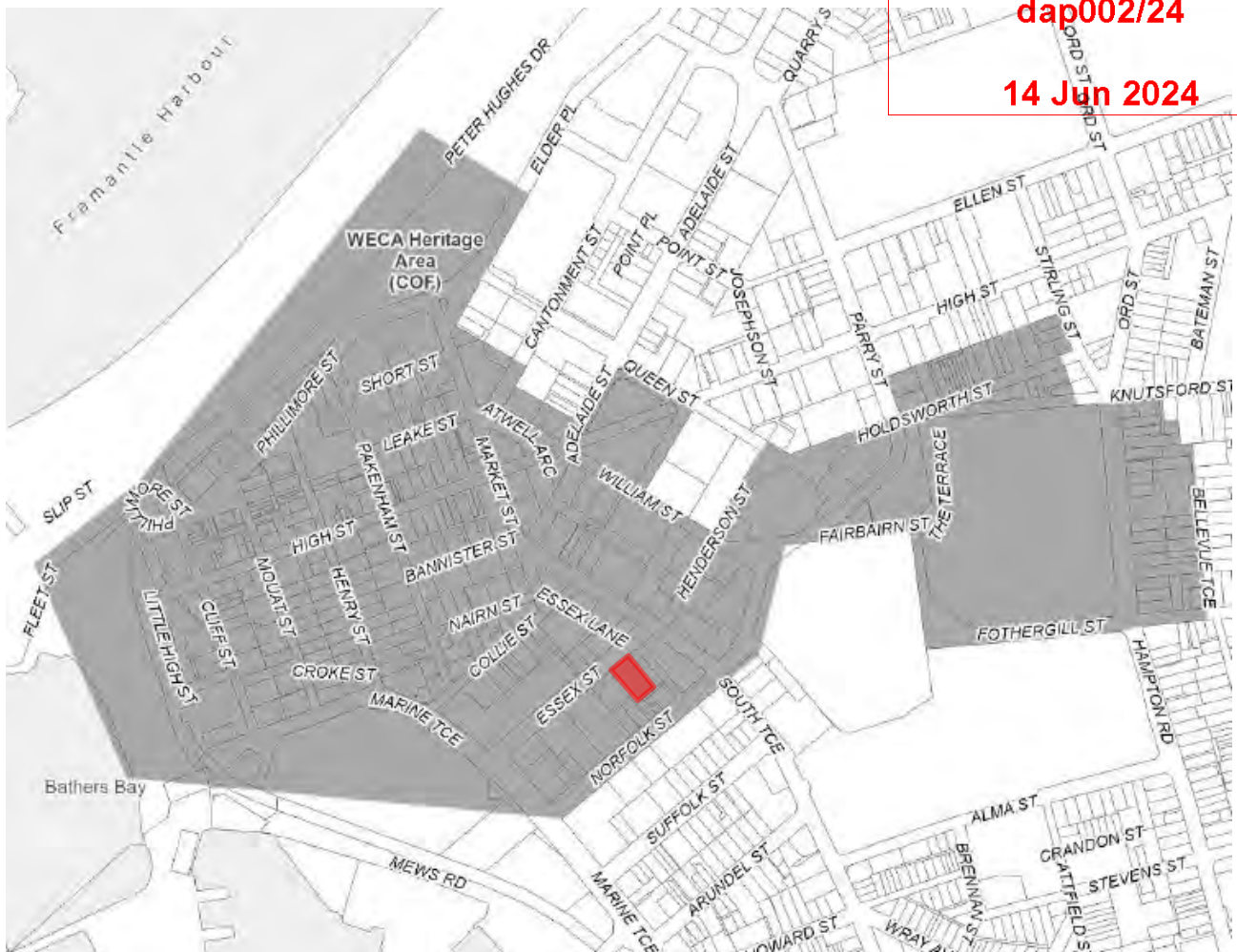


Figure 27: Heritage curtilage of the Central Fremantle Heritage Area (fmr West End Conservation Area) place no 22601 subject site 19 Essex Street is highlighted in red.

Source: City of Fremantle, Local Planning Policy 3.6 Heritage Areas 2022

5.2. STATEMENTS OF SIGNIFICANCE

5.2.1. Fmr Mills and Co Building

The former *Mills and Co Building* is identified as having local social and aesthetic significance. Its statement of significance is as follows.

*Extensively altered single storey commercial building that contributes to the Old Port City streetscape. The place is of social significance as evidenced by its recording by the National Trust.*¹⁰

5.2.2. Central Fremantle Heritage Area

Central Fremantle Heritage Area (Fmr West End Conservation Area) has the following statement of significance:

Central Fremantle Heritage Area (Fmr West End Conservation Area), including a substantial portion of the central area of Fremantle and occupying a partially triangular site with its apex at Arthur Head, the northern boundary formed by the Inner Harbour extending to include the Fremantle Train Station, the south-west boundary being the Indian Ocean and the Fishing Boat Harbour, the base of the triangle to the east formed

¹⁰ InHerit. 2020. Mills & Co Building (fmr). <https://inherit.dplh.wa.gov.au/Public/Inventory/Details/d9b124c7-ae2d-4d41-b60c871dc1b24546>

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by Kings Square and the limestone ridge and extended to include the Fremantle Prison and sites along Bellevue Terrace, has cultural heritage significance for the following reasons:

- *It is a cohesive townscape formed by both natural and man-made features. Within the boundaries the city centre is identifiable from several vantage points as a cohesive whole. The Town Centre is a precinct of considerable significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it retains the original town plan for Fremantle relatively intact and a number of significant buildings dating from the early to mid-twentieth century. Developed according to the 1829 survey by Surveyor General John Septimus Roe, with the street layout east of Market Street running approximately parallel with the original shoreline. Central to this area were two main squares - King's Square and Queen's Square located in the alignment of High Street;*
- *Arthur Head and Esplanade is a precinct of exceptional significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it contains the site of first settlement in the Swan River colony and the first law and order buildings. It also contains the site of the colony's first port.*
- *The Arthur Head and Esplanade precinct comprises the Round House, constructed in 1830-31 and designed by Henry Willey Reveley, an architect and Civil Engineer to the Colony, and associated buildings at Arthur Head. The Esplanade is the site of the first jetty, built 1830;*
- *The Old Port City is a precinct of exceptional significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it has been recognised as the core of commercial activity associate with Fremantle as a port city from the time of earliest settlement, it contains a high concentration of former warehouse and industrial buildings dating from the late nineteenth and early twentieth century and as such continues to provide a rare and intact example of an industrial urban centre in Western Australia.*
- *The area has changed over time however and currently features a predominance of buildings dating from the gold boom of the 1890s to the 1910s. These buildings were mainly constructed as warehouses, commercial premises, hotels and boarding houses associated with the shipping industry, and replaced the cottages and terraces that had previously characterised the area. After experiencing a resurgence of popularity in the 1970s and '80s and America's Cup preparations, when the area was characterised by restaurants and other places of entertainment, a significant proportion of the area is now owned and used by Notre Dame University;*
- *The Convict Establishment is a precinct of exceptional significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it covers the area of the original land grant provided for convict purposes in the 1850s. Over time the area was developed for the Convict Establishment, subsequently Fremantle Prison, the Fremantle Police Station and Court House, the Fremantle Hospital and Fremantle Oval. All these places are considered to be of State significance in their own right;*
- *The convicts were integral to the development and survival of Fremantle and the Swan River Colony and constructed public buildings, roads, bridges, and water systems. On the 40-acre convict grant, the convicts built the prison and houses for the prison hierarchy including 'The Knowle' for Superintendent Henderson (still standing in the grounds of the Fremantle Hospital complex). They went on to build the commissariat stores, the lunatic asylum, the police station, the barracks, the warders' cottages, the boys' school, and Fremantle bridge;*
- *The Fremantle Port is a precinct of exceptional significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it includes the Fremantle harbour, designed by C Y O'Connor and built from 1894-96, Victoria Quay and the associated warehouse area that developed between the harbour and the town centre in the early decades of the Twentieth Century.*

It is noted that the Central Fremantle Heritage Area (Fmr West End Conservation Area) contains:

- *the State Heritage Listed West End Heritage Area*
- *Arthur Head Heritage Area*
- *Portion of the Convict Establishment Heritage Area.* ¹¹

¹¹ City of Fremantle, Local Planning Policy 3.6 Heritage Areas 2022

5.3. ADJACENT HERITAGE PLACES

The subject site is located in a concentrated area of heritage listed places. Those adjacent to the site include the following:

Table 3 Adjacent heritage places

Adjacent Heritage Places		
No.	Place or item	Notes
HCWA Register of Heritage Places		
0966	26-28 Norfolk Street	Listed May 1997
01007	Fremantle Technical College Annexe	Listed March 2007

5.3.1. 26-28 Norfolk Street

The statement of significance for 26-28 Norfolk Street notes:

- 26-28 Norfolk Street, comprising a two-storey stone house with corrugated iron roof, sub-divided into two strata titles, each having a separate garage and outbuildings which once housed earth closets and laundry, has cultural heritage significance for the following reasons:
- its contribution to the corner streetscape and precinct values of Norfolk Street, Parry Street and that section of South Terrace;
 - its association with the evolution of the Fremantle community in the operation of a corner shop and living quarters in the 1860s; and
 - the early use of a flitch beam in the extension to the shop in the 1880s.
 - The garage at the north corner of No 28 Norfolk Street is not included in the assessment.¹²

5.3.2. Fremantle Technical college Annexe

Fremantle Technical College Annexe has been identified as having aesthetic, historic and social value as followed:

- As competently designed and built examples of the Victorian Rustic Style and the Federation Free Style, both the former Infants and Girls School (1877/1878) and Fremantle Technical School (1913) have significant aesthetic value. (Criterion 1.1)
- Fremantle Technical College Annexe makes a significant contribution to the South Terrace streetscape. The two storey Fremantle Technical School (1913) terminates the north end of the site and has a conspicuous presence which gives it strong landmark qualities, particularly in the way it terminates the vista looking west along Henderson Street and its prominence when looking south along South Terrace. (Criterion 1.3)
- The recessed central bay of the single storey former Infants and Girls School (1877/1878) forms a small forecourt enhancing the streetscape qualities of the footpath in that area. (Criterion 1.3)
- Fremantle Technical College Annexe contributes to the significant precinctual qualities of this part of Fremantle which contains Scots Presbyterian Church, the Norfolk Hotel, the Market Buildings and the Sail and Anchor Hotel. (Criterion 1.4)
- Fremantle Technical College Annexe represents the development of educational facilities in the Fremantle in response to the needs of an increasing population, and an education facility has continued to operate from the site from 1877 up until 2001. (Criterion 2.1)
- The former Infants and Girls School (1877/1878) is representative of the establishment of colonial schools in Western Australia in the 1850s. Governor Weld’s Education Act of 1871, which introduced compulsory

¹² InHerit. 2020. 26-28 Norfolk Street <https://inherit.dplh.wa.gov.au/Public/Inventory/Details/743aae32-3870-4d3e-a2c6-dba4b0a3bb11>

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elementary education, resulted in the construction of more of these colonial schools including the former Infants and Girls School in South Terrace. (Criterion 2.2)

- Fremantle Technical School (1913) is representative of the development of a technical education system for post primary aged children in Western Australia. This first saw the conversion of the former Infants and Girls School (1877/1878) as a technical school circa 1902 and the construction of a purpose-built technical school building in 1913. (Criterion 2.2)
- Fremantle Technical School (1913) has associations with Hillson Beasley, Chief Architect of the Public Works Department from 1905 to 1917. (Criterion 2.3)
- Since the construction of the former Infants and Girls School in 1877/1878 and the later construction of the Fremantle Technical School in 1913, Fremantle Technical College Annexe has been in continuous operation as an educational facility and, as a result, has value to the local and wider community. (Criterion 4.1)
- Fremantle Technical College Annexe is of social significance to the Fremantle and wider community. Its value to the community is evidenced by its classification by the National Trust of Australia (WA Branch), its inclusion in the City of Fremantle Municipal Inventory, and its listing in the Register of the National Estate. (Criteria 4.1 & 4.2)¹³

5.4. GRADING OF SIGNIFICANCE

Grading of the cultural heritage significance of each element and architectural component is based on its contribution to the Mills & Co Building (fmr) and the Central Fremantle Heritage Area (Fmr West end Conservation Area). The following gradings have been applied:

Table 4 Relative gradings of significance

Heritage Significance	Definition	Element or architectural component
Primary Significance	Makes a primary contribution to understanding the history and heritage significance of the subject site and/or is included in a statutory heritage listing. Has a high sensitivity to change and should be retained and conserved. Careful alteration and adaption can be considered to facilitate future operations.	<ul style="list-style-type: none"> ▪ Streetscape setting ▪ 19b Essex Street masonry façade ▪ Early portion of east boundary limestone wall adjoining State Heritage Register property (place no.966) at 26-28 Norfolk Street.
Secondary Significance	Makes a secondary contribution to understanding the history and heritage significance of the subject site. Has a moderate sensitivity to change and can be retained, adapted or altered to facilitate future operations. If removed, it should be recorded.	<ul style="list-style-type: none"> ▪ Shopfronts that have been altered ▪ 19b Essex Street Interior fabric relating to original structure including floors, walls and ceilings.
Little Significance	Makes little contribution to understanding the history and heritage significance of the subject site. Has a low sensitivity to change and may be retained, removed, altered	<ul style="list-style-type: none"> ▪ 19b Essex Street roof ▪ 19b Essex Street Cantilevered boxed awning

¹³ HCWA, Register of Heritage Places – Assessment Documentation Fremantle Technical College, annexe.

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Heritage Significance	Definition	Element or architectural component
	and adapted to accommodate the operation of the subject site.	
No Significance	Makes no contribution to understanding the history and heritage significance of the subject site, and may be removed if not damaging an element of greater significance.	<ul style="list-style-type: none"> 19b Essex Street first floor including skillion roof behind parapet. 19a Essex Street in its entirety Rear carpark Rear façade
Intrusive	Detracts from or has an adverse effect on understanding the history and heritage significance of the subject place. These elements should be removed when the opportunity arises.	<ul style="list-style-type: none"> 19b Essex Street pediment medallion signage

6. THE PROPOSAL

6.1. PROPOSED WORKS

The proposal comprises of internal fit out of the two properties 19a and 19b Essex Street and construction of a four-storey addition to accommodate the short stay accommodation facility Lylo.

The proposed works to the subject site as part of this DA are summarised in the below table.

Table 5 Proposed works

Proposed works	Comments
East elevation demolition	The east (rear) elevation of both buildings was constructed in 2013 and are not significant. It is proposed to demolish the existing glazing on both buildings, the metal staircase and landing a portion of the 19a rear wall and the roof sheeting overhang and fascia on both buildings.
Internal fit out the subject site.	<p>The north building 19b was constructed in 1907 and has undergone multiple adaptations reducing the integrity of heritage fabric. The façade is of primary significance and the interior is of secondary significance.</p> <p>The ground floor tenancies in the north building 19b were built in 2013 and will be retained as is.</p> <p>21 Essex Street (half of the Mills and Co Building heritage listing) is located outside of the subject site and will not be affected by the proposed works</p>
Construction of a four-storey addition in the rear carpark	<p>This carpark area has been historically clear of built fabric and is of no contributory significance to both the site and the wider West End Conservation Area.</p> <p>A small portion of the limestone boundary wall adjoining State Heritage Registered place no 966 26-28 Norfolk Street is of primary significance. The wall will be retained and protected during the proposed development.</p>
Installation of skylights in the existing roof cladding	Roof cladding on both buildings was installed c2013 and is of no significance.
19b Facade conservation works	<p>The facade is graded as primary significance. Works to the façade are limited to remediation to make good where required with like for like materials and colour scheme.</p> <p>The shop fronts have been adapted over time and are of secondary significance. They will be unaltered as part of the proposed development.</p> <p>The box awning will be extended to both facades</p>
19b roof terrace	<p>A roof terrace connected to 19a is proposed behind the original parapet of 19b. The existing roof sheeting will be demolished and the contemporary glazed windows on the first-floor elevation demolished. Both of these features were installed as part of the 2013 works and are not significant.</p> <p>New steps on 19a will connect the two terraces.</p>

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Proposed works	Comments
Alterations of the existing 19a Facade	19a Essex Street was constructed in 2013 and is of no significance. Alteration will include demolition of the pitched awning, ground floor glazing, signage and removal of the first-floor lightweight cladding and glazing. The existing cantilevered awning will be retained.

6.2. RELEVANT PLANS

This Heritage Impacts Statement is intended to be read in conjunction with the architectural documents by Rothelowman (DA Issue 1, dated 5.06.2024) and other documents submitted as part of this permit application namely the Architectural Report (dated June 2024). This HIS has relied on these plans and renders for the impact assessment in section 7.

Key extracts from plans submitted to Urbis by Rothelowman are included below. Please refer to the full DA documentation for detailed full-size plans.

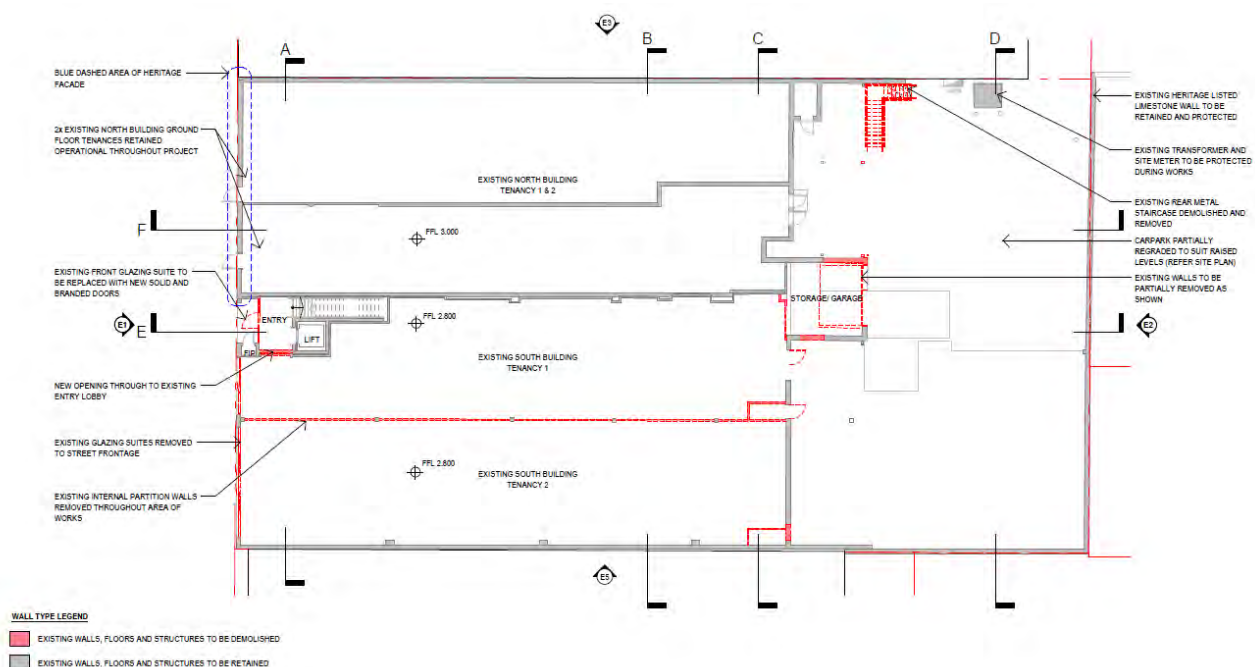


Figure 27: LyLo Fremantle Demolition Ground floor plan
Source: Rothelowman June 2024

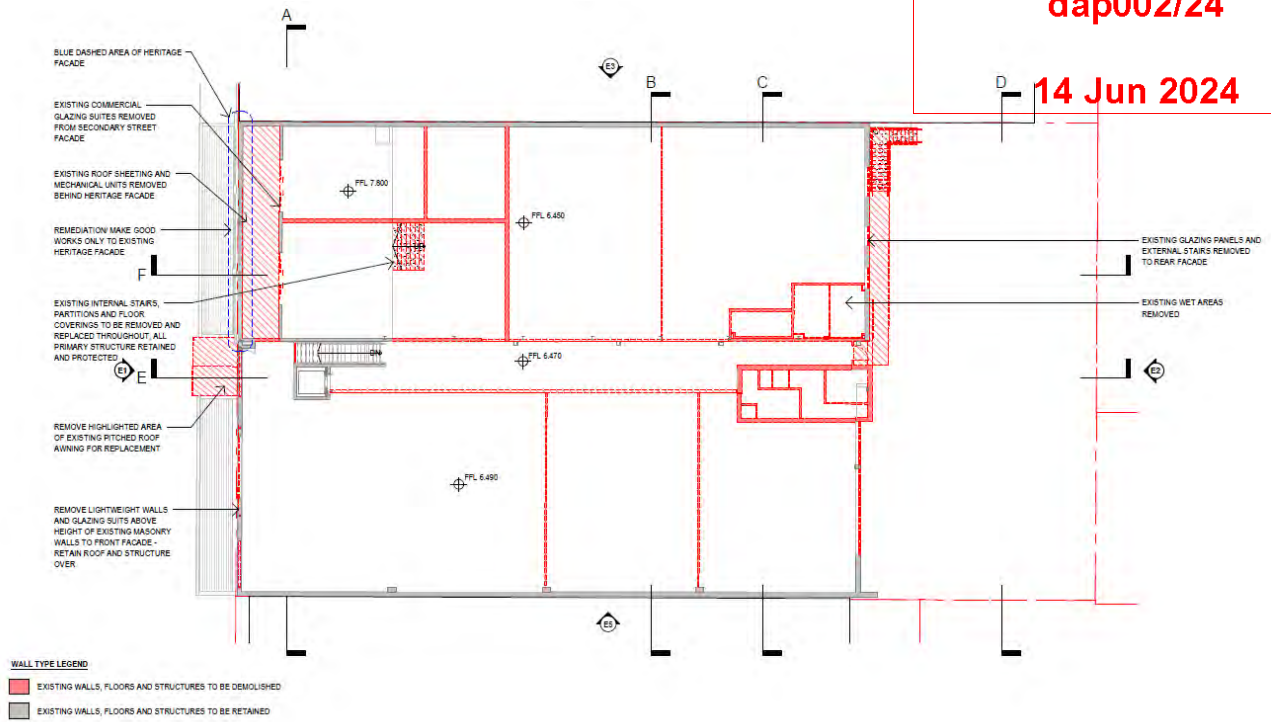


Figure 28: LyLo Fremantle Demolition First floor plan
Source: Rothlowman June 2024

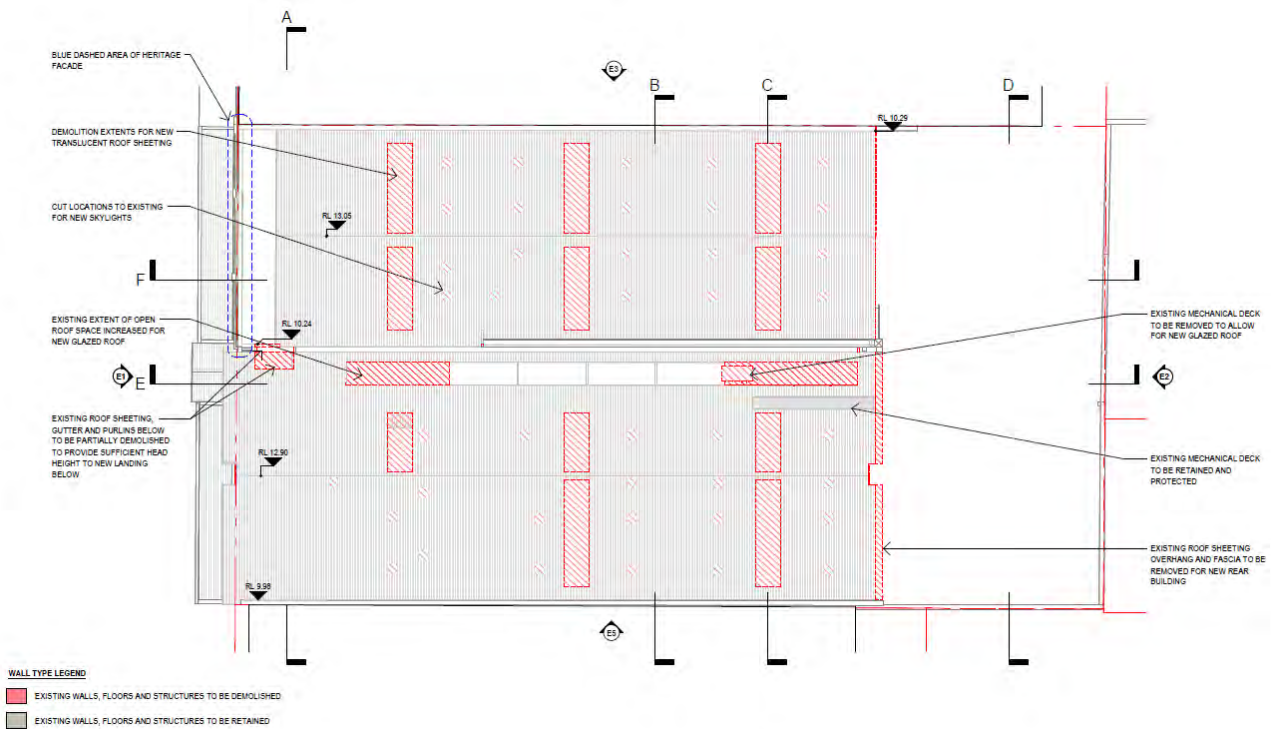


Figure 29: LyLo Fremantle Demolition roof plan
Source: Rothlowman June 2024

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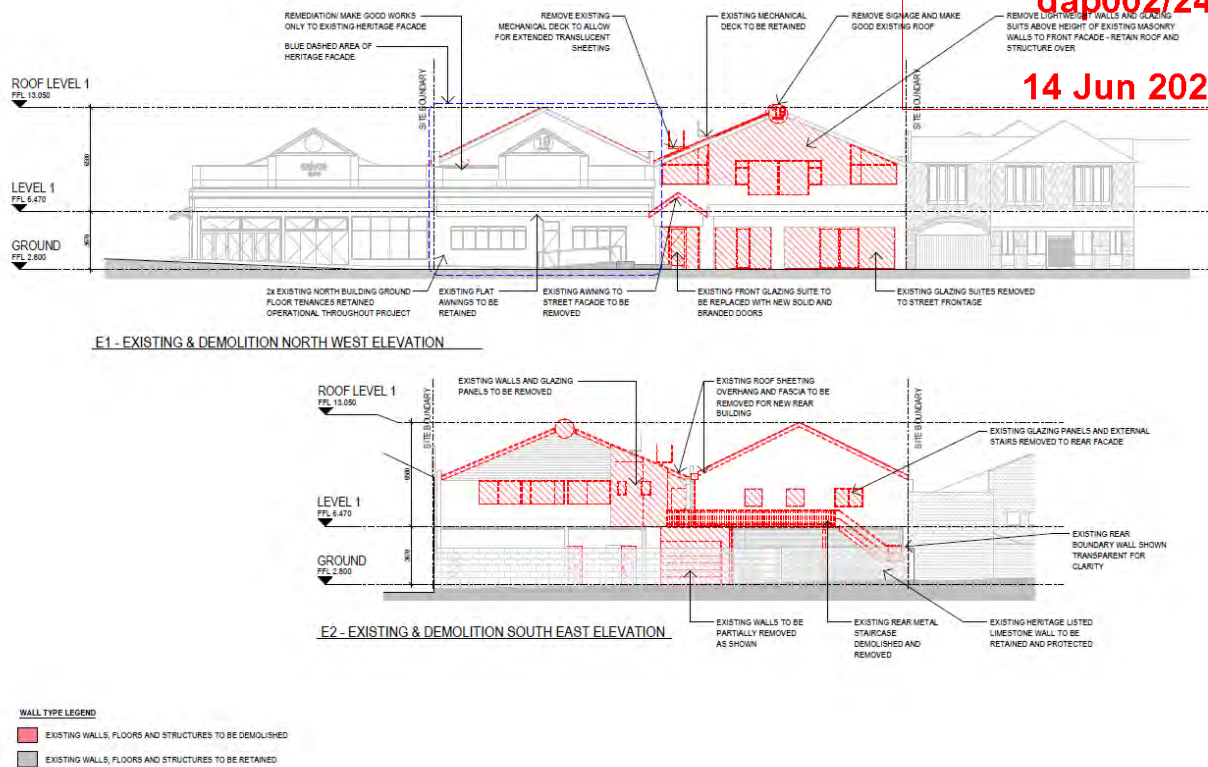


Figure 30: LyLo Fremantle Existing & demolition elevations
Source: Rothlowman June 2024

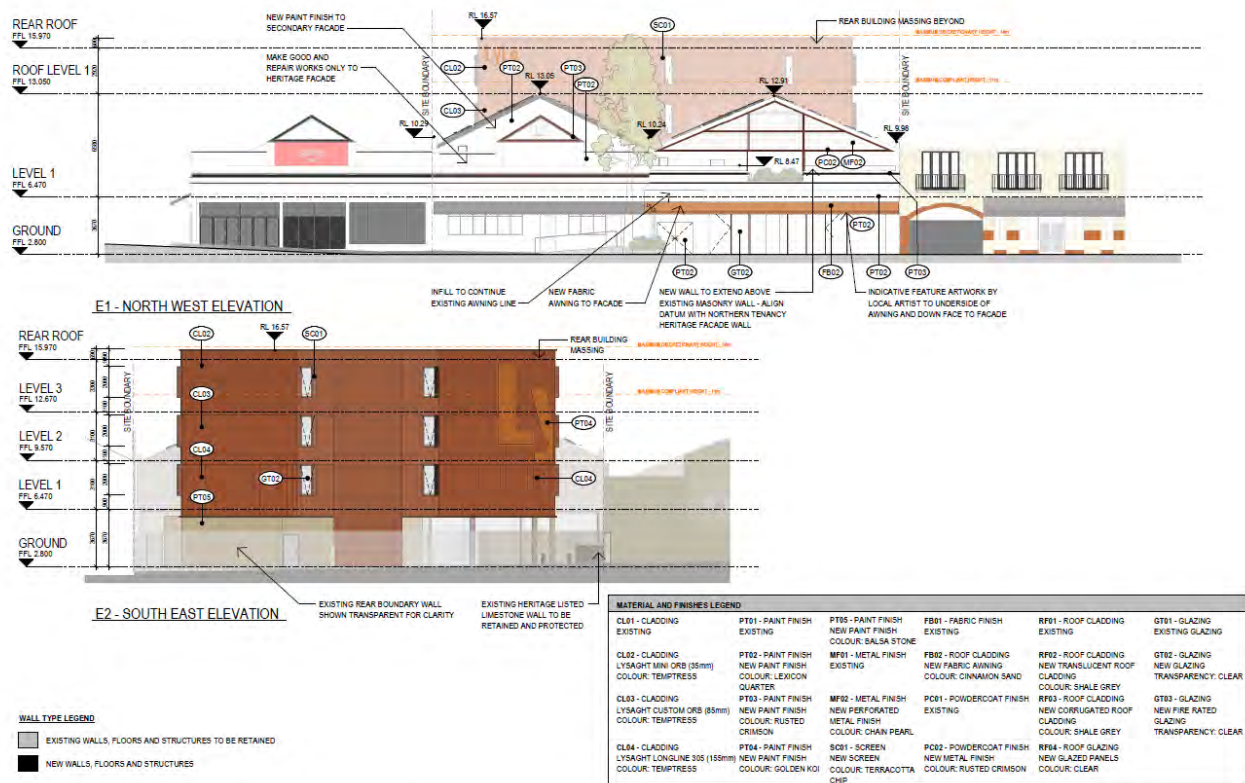


Figure 31: LyLo Fremantle Proposed elevations
Source: Rothlowman June 2024

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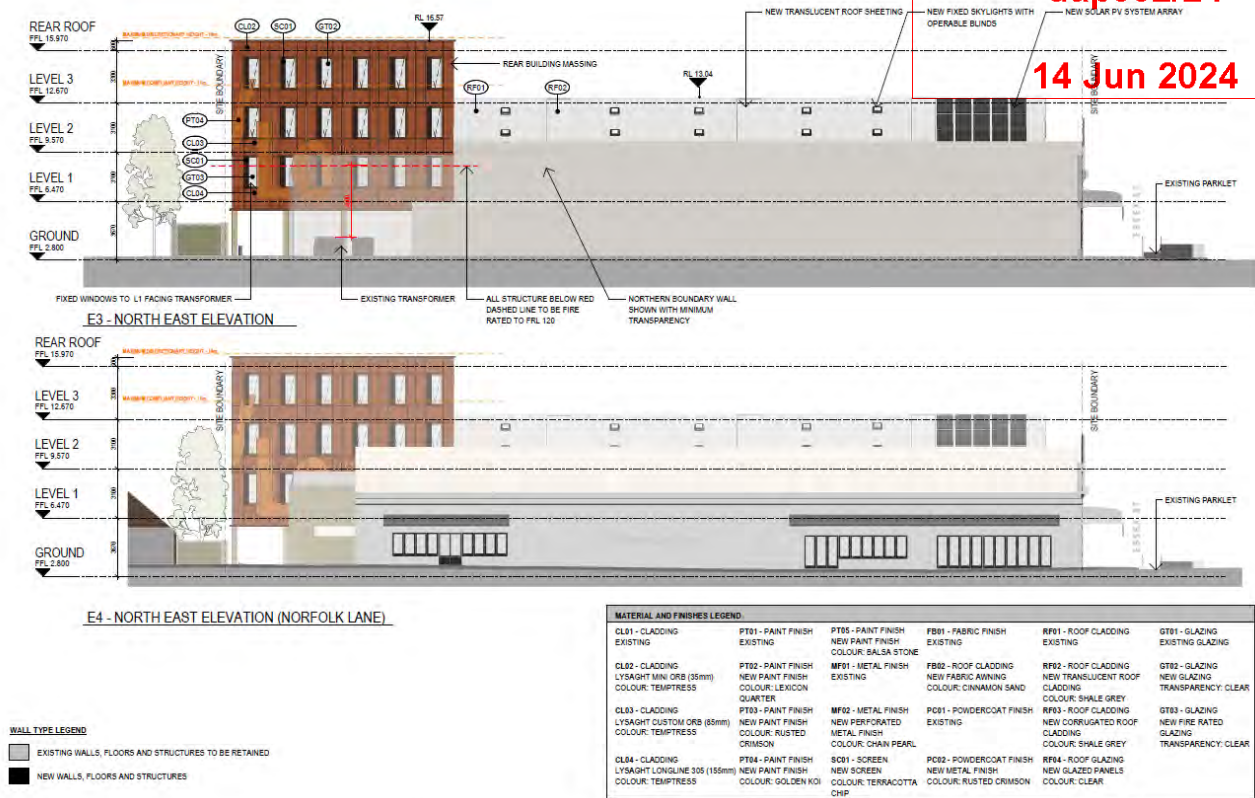


Figure 32: LyLo Fremantle Proposed elevations
Source: Rothlowman June 2024



Figure 33: LyLo Fremantle view from Essex St and Norfolk Lane
Source: Rothlowman June 2024

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Figure 34: LyLo Fremantle, Essex Street streetscape context photomontage.
Source: Rothlowman June 2024



Figure 35: View from corner of south Terrace and Essex Street showing Fremantle Technical College.

Source: Rothlowman June 2024



Figure 36: View from corner of Norfolk Street and Norfolk Lane, showing 26-28 Norfolk Street.

Source: Rothlowman June 2024

7.

ASSESSMENT OF HERITAGE IMPACT

7.1.

19 ESSEX STREET, FREMANTLE

The following impact assessment has assessed the potential heritage impacts of the proposed works at 19 Essex Street utilising the HCWA’s ‘Statement of Heritage Impact’ assessment guidelines.

Table 6 Assessment of Heritage Impact 19 Essex Street Fremantle, City of Fremantle Heritage Listings

Significance	Potential impact	Mitigation and Conservation Benefits
<i>Fmr Mills and Co Building: Statement of Significance.</i>		
<i>Extensively altered single storey commercial building that contributes to the Old Port City streetscape.</i>	<p>Minor impact</p> <p>Construction of the rear addition and alteration of 19b Essex Street facade will increase the scale of built form on the subject site resulting in a minor alteration to the single storey appearance of the place but not impact on any of the remaining original fabric.</p>	<p>Sight line diagrams from the footpath across the street from the principal Essex Street facade indicate that there will be no visibility of the addition behind the heritage facade, not altering the buildings existing streetscape appearance.</p> <p>Incidental views from further north on the intersection of Essex and Norfolk Lane position the four-storey addition comfortably in the background of the diverse streetscape allowing the heritage commercial building to continue to contribute to the Old Port City streetscape. The proposed colour scheme of neutral red/ browns references and compliments Fremantle’s redbrick structures without literally mimicking the material.</p> <p>The existing contributory heritage facade of 19a will be retained and conserved with only minor ‘make good’ works proposed. The development will maintain the heritage building as is which, as its statement of significance notes, has been extensively altered.</p> <p>The building is not currently single storey and has previously been altered to accommodate a first floor. The proposed first floor terrace on 19b will only alter contemporary fabric and be largely concealed behind the original parapet maintaining the appearance of the buildings having a single storey scale when viewed from the streetscape.</p> <p>Alterations to the contemporary 19a facade will improve the existing contemporary building’s appearance in</p>

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		<p>the streetscape by better responding to the architectural character of adjacent 19b façade. This is achieved by matching the parapet height datums and extending the existing awning across the two buildings and installing a fabric awning (sun shade) beneath.</p> <p>The existing 'grey' colour scheme of 19a will be replaced by a predominantly white facade with dark red brown accents more in keeping with the render and stone materiality of Essex Street. A change in colour scheme on 19a's fabric awning will suitably convey the intersection between the heritage and contemporary buildings. The contemporary building will appear more cohesive in the streetscape and better compliment the adjacent former Mills and Co buildings.</p>
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<p><i>The place is of social significance as evidenced by its recording by the National Trust.</i></p>	<p>Positive Impact</p> <p>The proposed development of 19 Essex Street maintains the existing heritage fabric and adaptively reuses the envelope of both buildings on the site. Publicly accessible uses will continue on the ground floor of both buildings conserving the building's community associations, sense of place and social significance in the Fremantle context.</p>	
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Central Fremantle Heritage Area (Fmr West end Conservation Area): Statement of Significance:

Central Fremantle Heritage Area (Fmr West End Conservation Area), including a substantial portion of the central area of Fremantle and occupying a partially triangular site with its apex at Arthur Head, the northern boundary formed by the Inner Harbour extending to include the Fremantle Train Station, the south-west boundary being the Indian Ocean and the Fishing Boat Harbour, the base of the triangle to the east formed by Kings Square and the limestone ridge and extended to include the Fremantle Prison and sites along Bellevue Terrace, has cultural heritage significance for the following reasons:

<ul style="list-style-type: none"> <i>It is a cohesive townscape formed by both natural and man-made features. Within the boundaries the city centre is identifiable from several vantage points as a cohesive whole. The Town</i> 	<p>No discernible impact</p> <p>The proposed development will not alter the significant townscape planning of the Central Fremantle Heritage Area.</p>	
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<p><i>Centre is a precinct of considerable significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it retains the original town plan for Fremantle relatively intact and a number of significant buildings dating from the early to mid-twentieth century. Developed according to the 1829 survey by Surveyor General John Septimus Roe, with the street layout east of Market Street running approximately parallel with the original shoreline. Central to this area were two main squares - King's Square and Queen's Square located in the alignment of High Street;</i></p>		
<ul style="list-style-type: none"> • <i>Arthur Head and Esplanade is a precinct of exceptional significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it contains the site of first settlement in the Swan River colony and the first law and order buildings. It also contains the site of the colony's first port.</i> 	<p>No discernible impact</p> <p>The subject site is not located within the Arthur Head and Esplanade precinct.</p>	
<ul style="list-style-type: none"> • <i>The Arthur Head and Esplanade precinct comprises the Round House, constructed in 1830-31 and designed by Henry Willey Reveley, an architect and Civil Engineer</i> 	<p>No discernible impact</p> <p>The subject site is not located within the Arthur Head and Esplanade precinct.</p>	

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<p>to the Colony, and associated buildings at Arthur Head. The Esplanade is the site of the first jetty, built 1830;</p>		
<ul style="list-style-type: none"> <i>The Old Port City is a precinct of exceptional significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it has been recognised as the core of commercial activity associate with Fremantle as a port city from the time of earliest settlement, it contains a high concentration of former warehouse and industrial buildings dating from the late nineteenth and early twentieth century and as such continues to provide a rare and intact example of an industrial urban centre in Western Australia.</i> 	<p>No discernible impact</p> <p>19b Essex Street part of the former Mills and Co building and warehouse will be retained in full continuing the building's aesthetic contribution to Fremantle's industrial urban centre.</p>	
<ul style="list-style-type: none"> <i>The area has changed over time however and currently features a predominance of buildings dating from the gold boom of the 1890s to the 1910s. These buildings were mainly constructed as warehouses, commercial premises, hotels and boarding houses associated with the shipping industry, and replaced the cottages and terraces that had previously characterised the area. After experiencing a resurgence of popularity in the 1970s and '80s and America's Cup preparations, when the</i> 	<p>Positive impact</p> <p>The c1915 building at 19b will retain its existing streetscape presentation Essex Street providing ongoing interpretation of early 20th century commercial buildings in Fremantle. The building has already been significantly modified but its other half located at 21 Essex Street is more intact and serves as better representation of the building's early condition.</p>	

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<p>area was characterised by restaurants and other places of entertainment, a significant proportion of the area is now owned and used by Notre Dame University;</p>		
<ul style="list-style-type: none"> <p><i>The Convict Establishment is a precinct of exceptional significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it covers the area of the original land grant provided for convict purposes in the 1850s. Over time the area was developed for the Convict Establishment, subsequently Fremantle Prison, the Fremantle Police Station and Court House, the Fremantle Hospital and Fremantle Oval. All these places are considered to be of State significance in their own right;</i></p> 	<p>No discernible impact</p> <p>The subject site is not located within the Convict Establishment precinct.</p>	
<ul style="list-style-type: none"> <p><i>The convicts were integral to the development and survival of Fremantle and the Swan River Colony and constructed public buildings, roads, bridges, and water systems. On the 40-acre convict grant, the convicts built the prison and houses for the prison hierarchy including 'The Knowle' for Superintendent Henderson (still standing in the grounds of the Fremantle Hospital complex). They went on to build the commissariat stores, the lunatic asylum,</i></p> 	<p>No discernible impact</p> <p>The sites heritage building has no contribution to Fremantle's Convict History.</p> <p>The early limestone wall on the rear boundary shared with 26-28 Norfolk Street will be retained and protected throughout the proposed works.</p>	

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<p><i>the police station, the barracks, the warders' cottages, the boys' school, and Fremantle bridge;</i></p>		
<ul style="list-style-type: none"> <i>The Fremantle Port is a precinct of exceptional significance within the Central Fremantle Heritage Area (Fmr West End Conservation Area) because it includes the Fremantle harbour, designed by C Y O'Connor and built from 1894–96, Victoria Quay and the associated warehouse area that developed between the harbour and the town centre in the early decades of the Twentieth Century.</i> 	<p>No discernible impact</p> <p>The site is not located within the Fremantle Port precinct.</p>	

7.2. ASSESSMENT OF IMPACT TO ADJACENT HERITAGE PLACES

Table 7 Assessment of Heritage Impact to Adjacent Heritage Places on the HCWA's State Register of Heritage Places.

Significance	Potential impact	Mitigation
<p>26-28 Norfolk Street (Place No. 0966)</p> <p><i>26-28 Norfolk Street, comprising a two-storey stone house with corrugated iron roof, sub-divided into two strata titles, each having a separate garage and outbuildings which once housed earth closets and laundry, has cultural heritage significance for the following reasons:</i></p>		
<ul style="list-style-type: none"> <i>its contribution to the corner streetscape and precinct values of Norfolk Street, Parry Street and that section of South Terrace;</i> 	<p>Minor impact</p> <p>The proposed four storey addition will have minor visibility behind the significant heritage site when viewed from the opposite site of Norfolk Street.</p>	<p>Visual impacts of the proposed development are mitigated by the existing diverse context of the Norfolk Street and Lane streetscapes and wider precinct. The addition will be largely obscured behind the sites existing mature trees and further dwarfed by Norfolk Lanes avenue of Norfolk Island Pine Trees which dominate as vertical elements in views and vistas of the place.</p> <p>The development will sit comfortably in the background of the significant heritage place and affirms its hierarchy</p>

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		<p>of finer architectural form through modest architectural form and complimentary material palette.</p> <p>The houses will maintain their dominance in the streetscape, with the distinguished corner shop frontage. The addition to 19 Essex Street will have no adverse impact on the house's contribution to the corner streetscape of Norfolk Street and Lane.</p>
<ul style="list-style-type: none"> its association with the evolution of the Fremantle community in the operation of a corner shop and living quarters in the 1860s; and 	<p>No Discernible Impact.</p> <p>The proposed works do not alter the historic and social value of the place.</p>	
<ul style="list-style-type: none"> the early use of a flitch beam in the extension to the shop in the 1880s. 	<p>No Discernible Impact.</p> <p>The proposed development of 19 Essex Street has no physical impact on the adjacent heritage place.</p>	
<p>Fremantle Technical college Annexe (Place No. 01007)</p>		
<p><i>As competently designed and built examples of the Victorian Rustic Style and the Federation Free Style, both the former Infants and Girls School (1877/1878) and Fremantle Technical School (1913) have significant aesthetic value. (Criterion 1.1)</i></p>	<p>No Discernible Impact.</p> <p>The proposed development of 19 Essex Street has no physical impact on the nearby heritage place.</p>	
<p><i>Fremantle Technical College Annexe makes a significant contribution to the South Terrace streetscape. The two storey Fremantle Technical School (1913) terminates the north end of the site and has a conspicuous presence which gives it strong landmark qualities, particularly in the way it terminates the vista looking west along Henderson Street and its prominence when looking south along South Terrace. (Criterion 1.3)</i></p>	<p>No Discernible Impact.</p> <p>The proposed development will be effectively concealed behind the Technical College building when viewed from Henderson Street and South Terrace having no impact on the landmark quality of the significant site.</p>	

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<i>The recessed central bay of the single storey former Infants and Girls School (1877/1878) forms a small forecourt enhancing the streetscape qualities of the footpath in that area. (Criterion 1.3)</i>	<p>No Discernible Impact.</p> <p>The proposed development of 19 Essex Street has no physical impact on the nearby heritage place.</p>	
<i>Fremantle Technical College Annexe contributes to the significant precinctual qualities of this part of Fremantle which contains Scots Presbyterian Church, the Norfolk Hotel, the Market Buildings and the Sail and Anchor Hotel. (Criterion 1.4)</i>	<p>No Discernible Impact.</p> <p>The proposed development of the nearby 19 Essex Street will only have incidental visibility in the background of the Technical College Building. The distance between the two buildings will ensure the Technical College maintains its dominance in precinct due to its scale and mass, and will in no way affect the perception of the heritage place within the immediate streetscape.</p>	
<i>Fremantle Technical College Annexe represents the development of educational facilities in the Fremantle in response to the needs of an increasing population, and an education facility has continued to operate from the site from 1877 up until 2001. (Criterion 2.1)</i>	<p>No Discernible Impact.</p> <p>The proposed development of 19 Essex Street has no physical impact on the nearby heritage place.</p>	
<i>The former Infants and Girls School (1877/1878) is representative of the establishment of colonial schools in Western Australia in the 1850s. Governor Weld's Education Act of 1871, which introduced compulsory elementary education, resulted in the construction of more of these colonial schools including the former Infants and Girls School in South Terrace. (Criterion 2.2)</i>	<p>No Discernible Impact.</p> <p>The proposed development of 19 Essex Street has no physical impact on the nearby heritage place, including its historic and social values.</p>	

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<p><i>Fremantle Technical School (1913) is representative of the development a technical education system for post primary aged children in Western Australia. This first saw the conversion of the former Infants and Girls School (1877/1878) as a technical school circa 1902 and the construction of a purpose-built technical school building in 1913. (Criterion 2.2)</i></p>	<p>No Discernible Impact.</p> <p>The proposed development of 19 Essex Street has no physical impact on the nearby heritage place.</p>	
<p><i>Fremantle Technical School (1913) has associations with Hillson Beasley, Chief Architect of the Public Works Department from 1905 to 1917. (Criterion 2.3)</i></p>	<p>No Discernible Impact.</p> <p>The proposed development of 19 Essex Street has no physical impact on the nearby heritage place, including its historic and social values.</p>	
<p><i>Since the construction of the former Infants and Girls School in 1877/1878 and the later construction of the Fremantle Technical School in 1913, Fremantle Technical College Annexe has been in continuous operation as an educational facility and, as a result, has value to the local and wider community. (Criterion 4.1)</i></p>	<p>No Discernible Impact.</p> <p>The proposed development of 19 Essex Street has no physical impact on the nearby heritage place, including its historic and social values.</p>	
<p><i>Fremantle Technical College Annexe is of social significance to the Fremantle and wider community. Its value to the community is evidenced by its classification by the National Trust of Australia (WA Branch), its inclusion in the City of Fremantle Municipal Inventory, and its listing in the Register of the National Estate. (Criteria 4.1 & 4.2)</i></p>	<p>No Discernible Impact.</p> <p>The proposed development of 19 Essex Street has no physical impact on the nearby heritage place, including its historic and social values.</p>	

7.3. ARCHAEOLOGICAL IMPACTS

Under Clause 7.7.1 of the City's Local Planning Scheme No. 4 (the Scheme), any potential development within a Heritage Area or included on a Heritage List may require archaeological investigations as a condition of development approval.

The following is an assessment of the likely impact of the proposed works on potential archaeological resources within the subject site. Based on the plans received for development application, minor excavation works are required in the rear section of the subject site for the installation of building footings and underground utilities. The rear section has undergone extensive disturbances which has reduced the likelihood for archaeological potential (including Aboriginal and historical).

7.3.1. Historical Archaeology

The subject site is within the curtilage of the heritage listed *Mills & Co Building (fmr)* and the *WEST END CONSERVATION AREA*.

Based on the historical land use and disturbance of the subject site, there is low potential for archaeological resources to survive in a subsurface capacity (i.e. structural remains and general discard items). The proposed works may proceed with caution, based on the Archaeological Finds Procedure in Appendix A.

7.3.2. Aboriginal Archaeology

There are no registered Aboriginal cultural heritage sites registered on the Aboriginal Cultural Heritage Information System (ACHIS). Note should be made that there are no site specific archaeological/ethnographic surveys which cover the subject site.

Historical disturbance across the surface of the subject site is determined to be high. As a result, there is low potential for Aboriginal sites to survive. The proposed works may proceed with caution, based on the Archaeological Finds Procedure in Appendix A.

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8. CONCLUSION

This Heritage Impact Statement has assessed the potential impacts of the proposed development and refurbishment on the heritage significance of 19 Essex Street, Fremantle and adjacent heritage places.

The site is identified on the City of Fremantle Heritage List as part of the *Mills and Co Building (fmr)* (place no. 3708) and is also located within the *Central Fremantle Heritage Area (Fmr West End Conservation Area)* (place no.22601). It is also adjacent to two state registered places: *26-28 Norfolk Street* (Place No. 0966) and *Fremantle Technical college Annexe* (Place No. 01007).

This report has found the following:

- 19 Essex Street is not intact and has undergone substantial alteration in the late 20th century including the raising of 19b's roof to accommodate a first floor and the construction of a new infill building at 19a in 2013.
- The proposed works are consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric.
- The proposed four storey addition will be located at the rear of the site and only have inconsequential incidental visibility from Essex Street as well as in the background of State listed heritage sites 26-28 *Norfolk Street* (Place No. 0966) and *Fremantle Technical college Annexe* (Place No. 01007).
- The modest modular architectural design and red brown steel materiality responds to the existing colour scheme of the streetscape which includes red brick and clay tiles and affirms the hierarchy of the finer architectural detailing of the streetscape and wider precinct's significant heritage places.

For the reasons stated above, the proposed works may be supported for approval from a heritage perspective.

9. REFERENCE LIST

Australia ICOMOS 1999, The Burra Charter: 2013 The Australia ICOMOS Charter for Places of Cultural Significance, Australia ICOMOS, Burwood.

Casella Edgar and Wade Architects. 1993. Proposed Restaurant Development 1st Floor Lot No 8 Essex Street Fremantle.

Gillard, G. 2024. Fred Instone. <https://freotopia.org/people/instone.html>

InHerit. 2020. Mills & Co Building (fmr). <https://inherit.dplh.wa.gov.au/Public/Inventory/Details/d9b124c7-ae2d-4d41-b60c-871dc1b24546>

Proposed Commercial Fit-Out at: Lot 8 / #19 Essex Street Fremantle W.A. 6160. Certified Construction Drawings. 2013.

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10. DISCLAIMER

This report is dated 07 June 2024 and incorporates information and events up to that date only and excludes any information arising, or event occurring, after that date which may affect the validity of Urbis Ltd (**Urbis**) opinion in this report. Urbis prepared this report on the instructions, and for the benefit only, of EVT & Belingbak (**Instructing Party**) for the purpose of HIS (**Purpose**) and not for any other purpose or use. To the extent permitted by applicable law, Urbis expressly disclaims all liability, whether direct or indirect, to the Instructing Party which relies or purports to rely on this report for any purpose other than the Purpose, and to any other person which relies or purports to rely on this report for any purpose whatsoever (including the Purpose).

In preparing this report, Urbis was required to make judgements which may be affected by unforeseen future events, the likelihood and effects of which are not capable of precise assessment.

All surveys, forecasts, projections and recommendations contained in or associated with this report are made in good faith and on the basis of information supplied to Urbis at the date of this report, and upon which Urbis relied. Achievement of the projections and budgets set out in this report will depend, among other things, on the actions of others over which Urbis has no control.

In preparing this report, Urbis may rely on or refer to documents in a language other than English, which Urbis may arrange to be translated. Urbis is not responsible for the accuracy or completeness of such translations and disclaims any liability for any statement or opinion made in this report being inaccurate or incomplete arising from such translations.

Whilst Urbis has made all reasonable inquiries it believes necessary in preparing this report, it is not responsible for determining the completeness or accuracy of information provided to it. Urbis (including its officers and personnel) is not liable for any errors or omissions, including in information provided by the Instructing Party or another person or upon which Urbis relies, provided that such errors or omissions are not made by Urbis recklessly or in bad faith.

This report has been prepared with due care and diligence by Urbis and the statements and opinions given by Urbis in this report are given in good faith and in the reasonable belief that they are correct and not misleading, subject to the limitations above.

Appendix A **ARCHAEOLOGICAL FINDS PROCEDURE**

Archaeological Chance Finds Procedure

Should any archaeological deposits be uncovered during any site works, the following steps must be followed:

1. All works within the vicinity of the find must immediately stop, and the location of the find cordoned-off with signage installed to avoid accidental harm to the archaeological resource. The find must not be moved 'out of the way' without assessment.
2. The site supervisor or another nominated site representative must contact either the project archaeologist (if relevant) or DPLH to contact a suitably qualified archaeologist.
3. The nominated archaeologist must examine the find, provide a preliminary assessment of significance, record the item and decide on appropriate management measures. Such management may require further consultation with DPLH, Whadjuk Cultural Advice Committee and preparation of a research design and archaeological investigation/salvage methodology.
4. Depending on the significance of the find, reassessment of the archaeological potential of the subject area may be required and further archaeological investigation undertaken.
5. Reporting may need to be prepared regarding the find and approved management strategies.
6. Works in the vicinity of the find can only recommence upon receipt of approval from the project archaeologist.

Human Remains Procedure

In the unlikely event that human remains are uncovered during the proposed works, the following steps must be followed:

1. All works within the vicinity of the find must immediately stop and the location should be cordoned-off with signage installed to avoid accidental harm to the remains.
2. The site supervisor or other nominated manager must notify the WA Police and DPLH.
3. The find must be assessed by the WA Police, which may include the assistance of a qualified forensic anthropologist.
4. Management recommendations are to be formulated by the WA Police, DPLH and site representatives.
5. Works are not to recommence until the find has been appropriately managed. If the find is determined to be Aboriginal, then it would need to be managed with the WAC.



Heritage Comments

Address: 19 Essex Street, Fremantle
Application number: DAP002/24
Proposal: 4 Storey tourist accommodation
Requesting officer: Jonathan Dornan
Date: 3/07/2024



19 Essex Street, Landgate aerial photograph, 2023, .

INTRODUCTION

The purpose of this heritage comment is to assess the changes to the place that are proposed in DAP002/24 and the affect that they will have upon the heritage values of 19 Essex Street, the Central Fremantle Heritage Area and the adjacent heritage place 26-28 Norfolk Street. The proposed changes include:

- Repairs and paint of heritage façade to 19B Essex Street
- Modification of façade to 19A Essex
- Demolition of internal walls to ground floor of 19A and to upper floor of 19A & 19B
- New internal walls and fit out to ground floor of 19A and to upper floor of 19A & 19B
- New glazed roof between 19A and 19B
- 4 Storey additions to rear of site

HERITAGE LISTINGS

Heritage Place Name	Mills & Co. Building (Fmr)
State Register of Heritage Places	No
City of Fremantle Heritage List	Yes
City of Fremantle Heritage Area	Central Fremantle Heritage Area
Local Heritage Survey	Yes
Management Category	Level 2
Inherit database place record	3708
Further comment	Adjacent to a place on the HCWA Register of Heritage Places – 26-28 Norfolk Street

RELEVANT PREVIOUS DEALINGS

Recent meetings or discussions:

- Pre-lodgement discussions with applicant

Previous relevant DAs:

- No

Previous relevant legal dealings:

- N/A

BACKGROUND

Mills and Co Building was first identified as a significant heritage place by the Fremantle Society in 1978 and then by the National Trust (WA) in 1981. It was included in the Fremantle Municipal Heritage Inventory in 2000 and the Heritage List in 2008. The place has remained on the Heritage List even after considerable modification in 2012.

A Heritage Impact Assessment prepared by Urbis was included in this application. This HIA includes documentary and physical evidence so this will not be repeated in this heritage comment.



19B Essex Street, Fremantle Society Photographic Collection, 1978.

19A Essex Street is to the left hand side of the photograph.



19 Essex Street, Google Streetview 2023

IMPACT ASSESSMENT

Statement on Heritage Significance of the place

The proposed development of the place was assessed against the following values identified in the statement of significance for the place:

Extensively altered single storey commercial building that contributes to the Old Port City streetscape.	No discernible impact
The place is of social significance as evidenced by its recording by the National Trust.	No discernible impact

Impact on Heritage Values of the place

The impact of the proposed development of the place was assessed using the heritage values from the ICOMOS Burra Charter, 2013:

Aesthetic value	Minor impact	Condition	No discernible impact
Historic value	No discernible impact	Integrity	No discernible impact
Scientific value	No discernible impact	Authenticity	Minor impact
Social value	No discernible impact	Historical evolution	No discernible impact
Rarity	No discernible impact	Streetscape	Minor impact
Representativeness	No discernible impact		

Heritage Impact Comments

19 Essex Street, Fremantle – Local Heritage Value, Fremantle Heritage List

19 Essex Street contains three elements of cultural heritage significance

- 19B façade
- 19B interior of ground floor tenancies
- Limestone wall to rear boundary

19B Essex, the northern half of the existing building, is part of Former Mills and Co Building which was constructed c. 1907. This building originally had a tuckpointed brickwork and render façade, a gable roof and a skillion roof verandah. The building has undergone considerable modification but continues to make a positive contribution to Fremantle and the Central Fremantle Heritage Area. In the 1970s the façade was painted and a box awning installed and in the 1980s a second shopfront was inserted into the facade. In 1993 the roof was raised to accommodate upper floor offices and while the original timber trusses were reused, they are non-structural. In 2012 the roof structure was changed again with the pitch modified to suit the new building constructed at 19A Essex Street. On the ground floor of the building the original floors, walls and ceilings have been retained.

The works proposed in this application will have only a minor impact on the heritage significance of 19B Essex Street. The façade and the shopfronts will undergo some remediation and repainting which not adversely affect heritage fabric or heritage significance. The interior of the ground floor tenancies will undergo some minor changes but will retain original finishes and remain as two tenancies.

19A Essex Street, the southern half of the building is a two-storey addition constructed in 2012 to replace an earlier building from 1941. This part of the building has no heritage significance but is sympathetic with the character of streetscape. The proposed changes will have no impact on the heritage significance of 19 Essex Street or the heritage character of Essex Street.

The proposed four storey accommodation block is located at the rear of the site and will not directly affect any significant heritage fabric at 19 Essex Street or adjoining heritage properties. It will have little visual impact on the presentation of 19 Essex Street and its contribution to the streetscape as it is a considerable distance from the street and will be largely concealed by the existing two storey building at the front of the site.

The limestone wall on the rear boundary of the site will not be affected by this proposal. However, the wall needs to be protected during construction as it will not have a modern structurally designed footing only a thickening of the wall. Any works to this wall must match the existing original stonework and lime pointing and cement mortars and sealers must not be used.

Central Fremantle Heritage Area

The proposed additions and alterations will not adversely affect elements in the Central Fremantle Heritage Area (19 Essex) or the heritage character of the streetscapes of which it is composed.

- The impact on Essex Street will be minimal as the four storey building is largely concealed from view by the existing development on the front half of the site.
- The four storey building will also have minimal impact on Norfolk Lane because the lane is narrow, and the development is set back behind 21 Essex so there is only a small oblique view of the development from the lane.
- The four storey building will be partially visible from Norfolk Street but will not dominate the streetscape as it is set back behind existing development.

26 – 28 Norfolk Street (HCWA 966)

19 Essex adjoins the rear boundary of part of this State Heritage listed property. and shares a section of limestone wall. The fabric of this state listed terrace house, including the limestone boundary wall will not be altered as part of this application.

HCWA has provided comment on this application and advised that the proposal will have only a minor impact on the setting of 26-28 Norfolk Street, however, they have raised concerns about the impact of construction on the wall and the buildings at 26-28 Norfolk Street. HCWA have requested a dilapidation survey be prepared prior to commencement of work and for the building to be monitored during construction and any movement or damage to be reported immediately.

Fremantle Technical College Annexe (HCWA Place 1007)

Fremantle Technical College Annexe is separated from 19 Essex by 21 Essex Street and Norfolk Lane. HCWA has provided comment on this application and advised that the proposal will have only a no impact on the Fremantle Technical College Annexe or its setting. A dilapidation survey was not requested.

Port Flour Mill, 15 Essex Street (HCWA Place 871)

The historic section of Port Flour Mill is separated from 19 Essex Street by a new development on the north side of the block. HCWA has provided comment on this application and advised that the proposal will have no impact on the Port Flour Mill, however, they have raised concerns about the impact of construction on the Port Flour Mill. HCWA have requested a dilapidation survey be prepared prior to commencement of work and for the building to be monitored during construction and any movement or damage to be reported immediately.

RECOMMENDATIONS:

The works proposed in this application are acceptable as they will have only a minor impact on the heritage values of 26-28 Norfolk Street, 19 Essex Street and the Central Fremantle Heritage Area. However, precautions need to be undertaken during construction to ensure that the adjacent State Heritage listed buildings 26-28 Norfolk Street and Port Flour Mill are not adversely affected.

Conditions

1. Refer to HCWA advice dated 29 July 2024.
2. Methodology for stonework repair to be provided as part of the Building Licence documentation. Repairs to match original stone, stonework and pointing in stone type, coursing pattern and mortar composition and colour. Cement mortar or sealers are not to be used.



Department of **Planning,
Lands and Heritage**

OFFICIAL

Your ref: DAP002/24
Our ref: P871-51638
Enquiries: Louise Ryan (08) 6552 4118

Chief Executive Officer
City of Fremantle
planning@fremantle.wa.gov.au

Attention: Jonathan Dornan

Dear Sir

**PORT FLOUR MILL
26-28 NORFOLK STREET, FREMANTLE
FREMANTLE TECHNICAL COLLEGE ANNEXE**

Thank you for your referral to the Heritage Council under the provisions of section 73 of the *Heritage Act 2018*, due to its proximity to the State Registered Heritage Places known as *Port Flour Mill*, *22-28 Norfolk Street* and *Fremantle Technical College Annexe*.

Referral date	27 June 2024
Proposal Description	Four storey tourist development at 19 Essex Street, Fremantle

We received the following information:

Development Application Planning Report prepared by Developed, dated June 2024
Architectural Report prepared by Rothelowman, dated June 2024
Heritage Impact Statement prepared by Urbis, dated 7 June 2024
Drawings prepared by Rothelowman, dated 6 June 2024

DA00.00 Cover Sheet P9	DA02.02 Existing & Demolition Elevations P8
DA00.01 Proposed Site Plan P15	DA02.03 Existing & Demolition Elevations P6
DA01.01 Demolition Ground Floor Plan P9	DA02.04 Proposed Elevations P8
DA01.02 Demolition First Floor Plan P9	DA02.05 Proposed Elevations P8
DA01.03 Demolition Roof Plan P9	DA02.06 Proposed Elevations P6
DA01.04 Ground Floor Plan P9	DA03.01 Sections P8
DA01.05 First Floor Plan P9	DA03.02 Sections P8
DA01.06 L2-L3 Plan P8	DA03.03 Sections P8
DA01.07 Proposed Roof Plan P9	DA04.01 Overshadowing Study P4
DA02.01 Existing & Demolition Elevations P8	

The Heritage Council resolved that the proposal has been considered in the context of the identified cultural heritage significance of the adjacent heritage places and the following advice is given:

Findings

- The proposed development is located adjacent to three State Registered places:
 - *Port Flour Mill* at 15 Essex Street, Fremantle
 - *26-28 Norfolk Street*, Fremantle
 - *Fremantle Technical College Annexe* at 41 South Terrace, Fremantle

- A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant.
- The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind *26-28 Norfolk Street*. It is considered that this will have a minimal adverse impact on the cultural heritage values of *26-28 Norfolk Street*.
- The proposal will not affect the cultural heritage significance of *Port Flour Mill and Fremantle Technical College Annexe*.

Advice

The proposal, in accordance with the plans submitted, is supported subject to the following conditions:

1. A dilapidation survey of the *Port Flour Mill*, *26-28 Norfolk Street* and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken.
2. A program of monitoring any structural movement and potential vibration impacts on the *Port Flour Mill*, *26-28 Norfolk Street* the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer.

We would appreciate a copy of your determination within 10 days after making a decision, as required under regulation 42(3) of the *Heritage Regulations 2019*.

If you have any queries about this advice or how it might be incorporated into your determination, please contact Louise Ryan at louise.ryan@dplh.wa.gov.au or on 6552 4118.

Yours sincerely

A handwritten signature in black ink, appearing to be 'Lara Watson', with several horizontal lines extending to the right.

Lara Watson
A/Director
Historic Heritage Conservation

29 July 2024

cc: Michael Clare, Developed – michael@developedproperty.com.au



Proposed LyLo Short-Term Accommodation

19 Essex Street, Fremantle

Transport Impact Statement

PREPARED FOR:
EVT & Belingbak

June 2024

Document history and status

Author	Revision	Approved by	Date approved	Revision type
Vladimir Baltic	r01	B Bordbar	23/05/2024	Draft
Vladimir Baltic	r01a	B Bordbar	28/05/2024	Final
A Navarro	r01b	B Bordbar	05/06/2024	Revised Final
A Navarro	r01c	B Bordbar	06/06/2024	2 nd Revision

File name: t23.267.vb.r01b

Author: Vladimir Baltic

Project manager: Behnam Bordbar

Client: EVT & Belingbak

Project: 19 Essex Street, Fremantle

Document revision: r01c

Project number: t23.267

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APPENDIX A: PROPOSED DEVELOPMENT PLANS



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1 Introduction

This Transport Impact Statement (TIS) has been prepared by Transcore on behalf of EVT and Belingbak with regard to a proposed LyLo short-term accommodation development to be located at 19 Essex Street, Fremantle, in the City of Fremantle.

The subject site is located at the southern side of Essex Street, and a short distance west of Essex Street/Norfolk Lane intersection. The site currently accommodates a two-storey commercial building with four ground-level retail tenancies and first-floor office tenancies. Refer **Figure 1** for details.



Figure 1: Location of the subject site

The Transport Impact Assessment Guidelines (WAPC, Vol 4 – Individual Developments, August 2016) states: “A Transport Impact Statement is required for those developments that would be likely to generate moderate volumes of traffic¹ and

¹ Between 10 and 100 vehicular trips per hour

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therefore would have a moderate overall impact on the surrounding land uses and transport networks”.

Section 6 of Transcore’s report provides details of the estimated trip generation for the proposed development. Accordingly, as the total peak hour vehicular trips are estimated to be less than 100 trips, a *Transport Impact Statement* is deemed appropriate for this development.

Key issues that will be addressed in this report include the traffic generation and distribution of the proposed development, access and egress movement patterns and parking demand and supply.

2 Development Proposal

The Development Application (DA) for the subject site contemplates repurposing of existing and improved buildings at the site and construction of additional buildings at the rear of the property (over the existing car park) to form a new four-storey short-term accommodation development. The proposed Short-Term Accommodation (Hostel) use will be operated by LyLo, which currently operates across Australia and New Zealand.

Specifically, the hostel will comprise the following components:

Ground Floor:

- Reception, self-check-in area;
- F&B offering to be accessible to the public. This is to include bar, kitchen, cold and dry storage;
- Guest amenities;
- Offices, luggage store;
- Laundry, for guests and staff;
- Plant and equipment; and,
- Car parking.

Level 1 (existing and new build):

- Guest rooms and pods;
- Shared amenities;

Level 2 (new build):

- Guest rooms and pods.

Level 3 (new build):

- Guest rooms and pods.

Guest rooms will comprise a mix of the following room types:

- 8-person pod room (20sqm), no ensuite
- 4-person pod room (10sqm), no ensuite
- Twin room, no ensuite (10sqm)
- Queen ensuite room (15sqm)

With a total of 247 beds.

According to the development plan provided in **Appendix A**, a total of four on-site parking bays are proposed to be provided at the subject site. Two of these will be allocated to the existing (and retained) office tenancies while the other two (one ACROD bay and one service bay) will address the parking demand of the proposed hostel development. The service bay is intended to be utilised by small service vehicles such as vans which will facilitate smaller deliveries (food, linen, and similar).

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A separate bike storage space is also provided on the ground floor at the back end of the building, accessed from the rear entry to the building (via car park).

In addition to on-site parking bays, it is proposed that one of the existing on-street parking bays on Norfolk Lane be converted to a drop-off/pick-up bay for minibuses to facilitate group transport of hotel guests.

A bin storage area is provided at the southeastern corner of the building accessible by the service corridors. Waste collection and deliveries will be accommodated off the Norfolk Lane verge. The bins will be wheeled out to the verge on collection days so that a service truck is not required to enter the site.

Vehicular access to the subject site is provided via the existing crossover on Norfolk Lane, at the southeastern end of the subject site.

Pedestrian access to the proposed development will be facilitated from the existing footpaths on Essex Street.

3 Vehicle Access and Parking

3.1 Access

The existing access system currently serving the site is proposed to be retained in its current location and form. The access crossover at the southeastern end of the site, shared with the adjacent property, provides direct access into the on-site car park at the back of the property via a short, two-way driveway.

Access into the car park is controlled by a sliding security gate.

3.2 Parking Supply

The proposed on-site car parking supply includes a total of four parking bays at the rear of the site including two parking bays for the two existing office tenancies, one ACROD bay and one service bay. In addition to on-site parking bays, it is proposed that one of the existing on-street parking bays on Norfolk Lane be converted to a drop-off/pick-up bay for minibuses to facilitate group transport of hotel guests.

The City of Fremantle's LPS No. 4 provides parking rates for various types of tourist/accommodation developments based on number of bedrooms and public bar areas. However, the City has the capacity to waive or reduce the standard parking requirement (Item 4.7.3 - Relaxation of Parking Requirements), based on a number of criteria, including:

- (i) the availability of car parking in the locality including street parking,
- (ii) the availability of public transport in the locality,
- (iii) any reduction in car parking demand due to the sharing of car spaces by multiple uses, either because of variation of car parking demand over time or because of efficiencies gained from the consolidation of shared car parking spaces,
- (iv) any car parking deficiency or surplus associated with the existing use of the land,
- (v) legal arrangements have been made in accordance with clause 4.7.5 for the parking or shared use of parking areas which are in the opinion of the Council satisfactory,
- (vi) any credit which should be allowed for a car parking demand deemed to have been provided in association with a use that existed before the change of parking requirement,
- (vii) the proposal involves the restoration of a heritage building or retention of a tree or trees worthy of preservation
- (viii) (viii) any other relevant considerations.

While it is acknowledged that the proposed development provides a parking shortfall against the CoF LSP No.4 parking requirements, this issue was discussed in a pre-lodgement on-site meeting with the City it was suggested that the proposal should be assessed in light of its operational characteristics and type of service it provides.

The overwhelming majority of the future hotel guests are overseas/interstate short-term stay backpackers that do not own or require a private vehicle. However, guest opting to hire a car during their stay at the hostel may use one of a number of public pay car parking facilities in close proximity of the site. In addition, ample short-term day (2hrs 9AM-5PM) and unlimited overnight (5PM-9AM) parking is available on Essex Street.

The parking bays on Norfolk Lane and Essex Street can also double-up as drop-off/pick-up bays for passenger vehicles. Accordingly, parking dispensation condition (i) of *Item 4.7.3 - Relaxation of Parking Requirements* is therefore met.

The hostel will offer a courtesy transport service for groups of guests, as required. This service will include use of shuttle mini buses . For this purpose, it is proposed that one of the existing on-street public parking bays on Norfolk Lane be converted to a drop-off/pick-up bay. For practical reasons, the most convenient bay for this purpose would be the existing northernmost public bay on the eastern side of Norfolk Lane (closest to Essex Street intersection). The existing bay may need to be extended, linemarked and signed to ensure its availability throughout the day.

A concept plan of the reconfigured westernmost parking bay on Norfolk Lane can be subsequently prepared in co-operation with the City as part of detailed stage development plans.

Refer **Figure 2** for details on existing available public parking along eastern side of Norfolk Lane, immediately adjacent to the subject site.

One service bay for small commercial vehicles (i.e., vans) will be provided on-site within the 4-bay car park at the rear of the site in line with CoF LSP No.4 parking requirements for such type of vehicles.

Furthermore, as discussed in greater detail in **Section 8** of this report the subject site has excellent public transport coverage available through bus and train services meeting parking dispensation condition (ii) of *Item 4.7.3 - Relaxation of Parking Requirements*.



Figure 2: Southbound view along Norfolk Lane – public on-street parking

Accordingly, it is concluded that the proposed parking strategy for the short-stay accommodation development meets its practical parking demand considering the type and nature of the proposed land use.

4 Provision for Service Vehicles

The City of Fremantle's LPS No. 4 provides parking rates for the type of use, which stipulates requirement for provision of one on-site service vehicle bay.

This condition is met as one parking bay is provided for this purpose on-site within the car parking area at the rear of the site. This bay is accessed via the Norfolk Lane right-of-way crossover which leads directly to the car park. This bay will predominantly be used for smaller and more frequent deliveries such as food, linen and other similar needs. It is recommended that smaller vehicles such as vans be used for such deliveries.

A bin storage area is proposed at the southwest corner of the hostel building (ground level), as shown in the development plan in **Appendix A**. The bin storage area is accessible via service corridors.

Waste collection and deliveries will take place off Norfolk Lane. Bins will be wheeled out on designated collection days for presentation along the driveway. The waste collection truck will temporarily stop on Norfolk Lane, unload the bins and continue travel in forward gear. This type of arrangement is currently in place at the subject site for the existing land uses.

5 Hours of Operation

The proposed development is a hostel/short-term accommodation and as such will operate 24/7.

6 Daily Traffic Volumes and Vehicle Types

6.1 Proposed Development Trip Generation

Provided that the proposed LyLo short-term accommodation development will see a large portion of guests travelling to and from the site by courtesy mini buses, Uber, taxi and predominantly public transport, the traffic generation of the proposed use is expected to be very low.

Therefore, the traffic generation of the proposed development will be insignificant and is not expected to undermine the traffic operations of the external road network.

6.2 Traffic Flow

The limited amount of vehicular traffic generated by the project will be arriving from various directions as the subject site is located centrally within the Fremantle town centre.

6.3 Impact on Surrounding Roads

The WAPC Transport Impact Assessment Guidelines (2016) provides guidance on the assessment of traffic impacts:

“As a general guide, an increase in traffic of less than 10 per cent of capacity would not normally be likely to have a material impact on any particular section of road but increases over 10 per cent may. All sections of road with an increase greater than 10 per cent of capacity should therefore be included in the analysis. For ease of assessment, an increase of 100 vehicles per hour for any lane can be considered as equating to around 10 per cent of capacity. Therefore, any section of road where development traffic would increase flows by more than 100 vehicles per hour for any lane should be included in the analysis.”

It is clear that the traffic increase from the proposed development would be minimal and significantly below the critical threshold (100vph per lane). Hence, as detailed in **Section 6.2**, the proposed development will not increase traffic on any lanes on the surrounding road network by more than 100vph, therefore the impact of the development traffic on the surrounding road network will not be significant.

7 Traffic Management on the Frontage Streets

Essex Street, is a single-carriageway, two-lane road with angled on-street parking and paths on both sides of the road, as shown in **Figure 3**.

Essex Street is classified as an *Access Street* in the *Main Roads WA Functional Road Hierarchy*. It operates under the town centre speed limit zone of 40km/h.



Figure 3. Westbound view along Essex Street in the vicinity of the site

Public pay parking opportunities are in place on both sides with a time-limit of 2hrs between 9AM-5PM, unlimited between 5PM-midnight and free parking for the balance of time.

Norfolk Lane is constructed as a 5.0m wide single-carriageway road with embayed parking on the eastern side and a footpath on the western side. It is classified as an *Access Road* in accordance with the *Main Roads WA Functional Road Hierarchy*). It operates under the town centre speed limit zone of 40km/h (refer **Figure 4**).



Figure 4. Southbound view along Norfolk Lane from Essex Street intersection

A mix of public pay car parking (2hrs limit between 9AM-5PM), motorcycle parking (all day) and taxi rank/rideshare (free between 5PM-9AM) is in place along the eastern side of Norfolk Lane.

Essex Street forms a four-way priority-controlled intersection with Norfolk Lane and Essex Lane adjacent to the subject site.

8 Public Transport Access

The available local public transport services are illustrated in **Figure 5**. A number of bus services operate along South Terrace with bus stops in close proximity (i.e., walking distance) from the site.

These bus routes run between Fremantle Station, Murdoch Station, Cockburn Station, Rockingham Station, and Lakelands Train Station and Hampton Road including two circular routes call a number of key metropolitan landmarks.

Fremantle Train Station which provides connection to the metropolitan passenger rail network is located approximately 600m northwest of the site (walking distance). Both bus stops and train station are accessible by formal path network which is in place throughout the town centre.

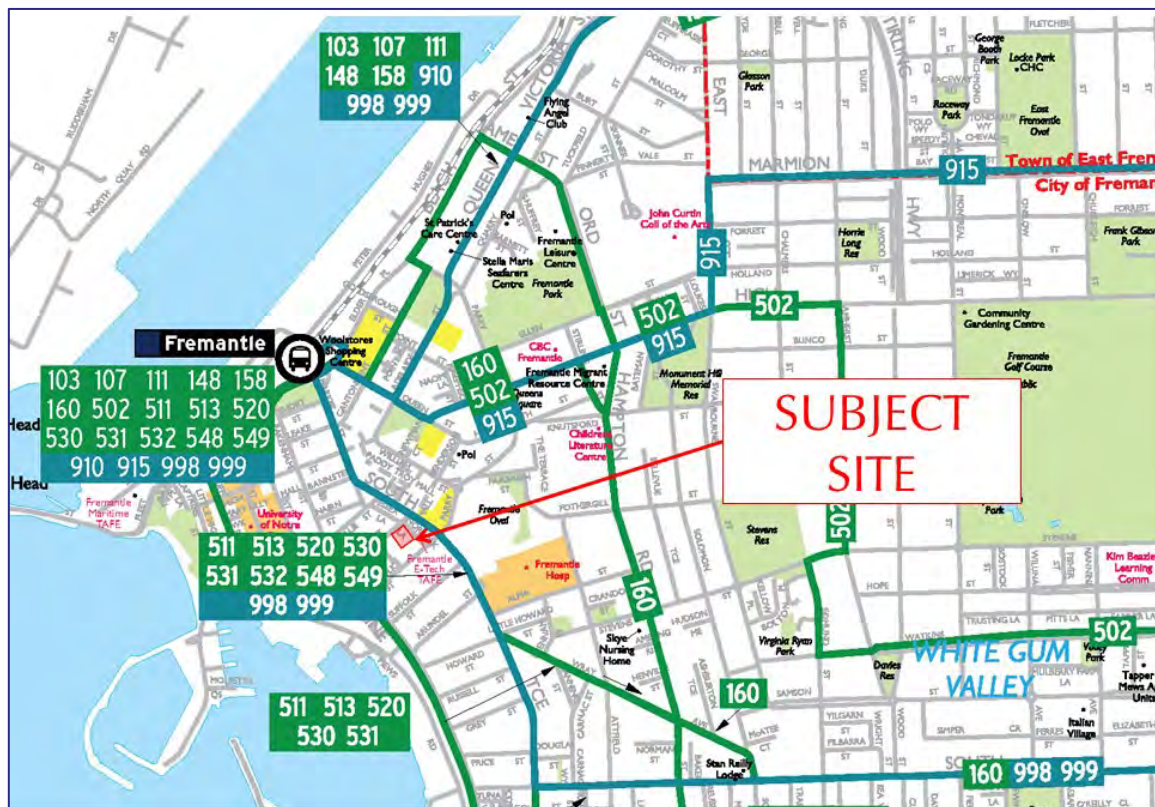


Figure 5: Public transport services (Transperth Maps)

9 Pedestrian Access

Pedestrian access to the subject site is available from multiple directions via the existing extensive footpath network comprising paved footpaths on all roads adjacent to the subject site.

Access into the proposed hostel is proposed directly from Essex Street frontage.

11 Site Specific Issues

No site-specific issues have been identified for the proposed development.

12 Safety Issues

No particular safety issue has been identified for the proposed development.

13 Conclusions

This Transport Impact Statement (TIS) has been prepared by Transcore on behalf of EVT and Belingbak with regard to a proposed LyLo short-term accommodation development to be located at 19 Essex Street, Fremantle, in the City of Fremantle.

The subject site is located at the southern side of Essex Street, and a short distance west of Essex Street/Norfolk Lane intersection. The site currently accommodates a two-storey commercial building with four ground-level retail tenancies and first-floor office tenancies.

The Development Application (DA) for the subject site contemplates repurposing of existing and improved buildings at the site and construction of additional buildings at the rear of the property (over the existing car park) to form a new four-storey short-term accommodation development.

A total of four on-site parking bays are proposed to be provided at the subject site. Two of these will be allocated to the existing (and retained) office tenancies while the other two (one ACROD bay and one service bay) will address the parking demand of the proposed hostel development. The car park area will continue to be accessible via the existing Norfolk Lane crossover.

A separate bike storage space is also provided on the ground floor at the back end of the building, accessed from the rear entry to the building (via car park).

The majority of the hostel guests will be using Uber, taxis, predominantly public transport and to a lesser extent courtesy minibuses, the traffic generation of the proposed development is expected to be very low. As such no adverse impact on the adjacent road network is anticipated.

Smaller deliveries will be accommodated by the proposed on-site service bay, while waste collection activities will be accommodated off Norfolk Lane.

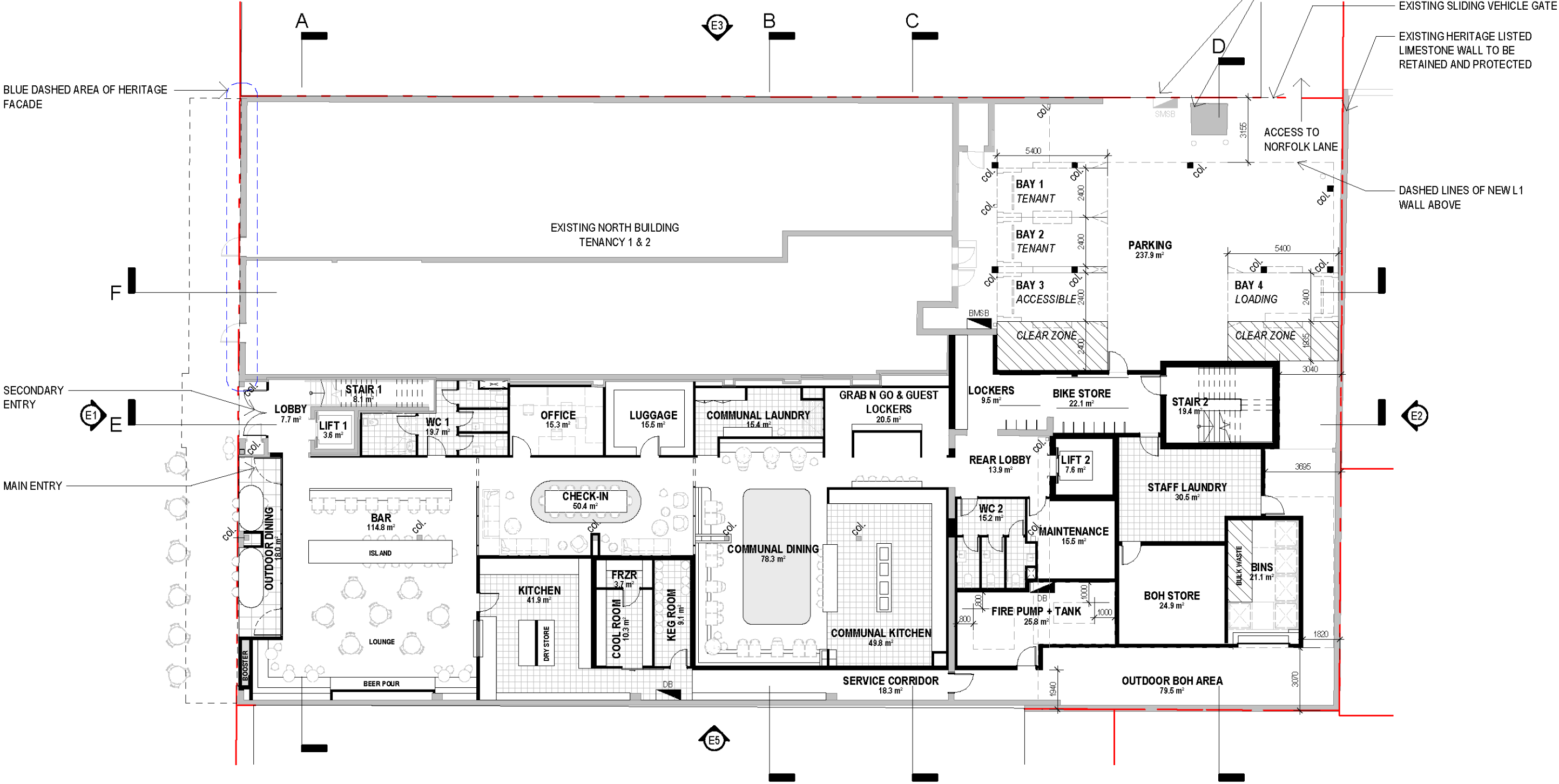
No particular transport or safety issues have been identified for the proposed short-stay accommodation development within the scope of this report.

It is concluded that the findings of this Transport Impact Statement are supportive of the proposed short-stay accommodation development.

Appendix A

PROPOSED DEVELOPMENT PLAN





WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

Revisions	P4	04.09.2024	CONCEPT COORDINATION MEETING
	P5	22.04.2024	ISSUE FOR INFORMATION
	P6	24.04.2024	DAC DRAFT PACK
	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1

Project / **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing / **Ground Floor Plan**

Project No / **223252** Author / **RL** Scale: @ A3 / **1 : 200**

Drawing No. / **DA01.04**

P9 **rothelowman**

Brisbane, Gold Coast,
Melbourne, Perth, Sydney
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EXISTING SEPARATION FROM
HERITAGE FACADE TO PRIMARY
WALL

BLUE DASHED AREA OF HERITAGE
FACADE

GREY DASHED LINES
INDICATIVE LOCATION OF
EXISTING TRUSSES OVERHEAD

ORANGE DASHED LINES OF
GROUND FLOOR WALL BELOW

NEW WALL TO EXTEND ABOVE
EXISTING MASONRY WALL -
ALIGN DATUM WITH NORTHERN
TENANCY HERITAGE FACADE
WALL

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

Revisions	P4	04.09.2024	CONCEPT COORDINATION MEETING
	P5	22.04.2024	ISSUE FOR INFORMATION
	P6	24.04.2024	DAC DRAFT PACK
	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1

Project / **LyLo Fremantle**

Drawing / **First Floor Plan**

Project No / **223252**

Author / **RL**

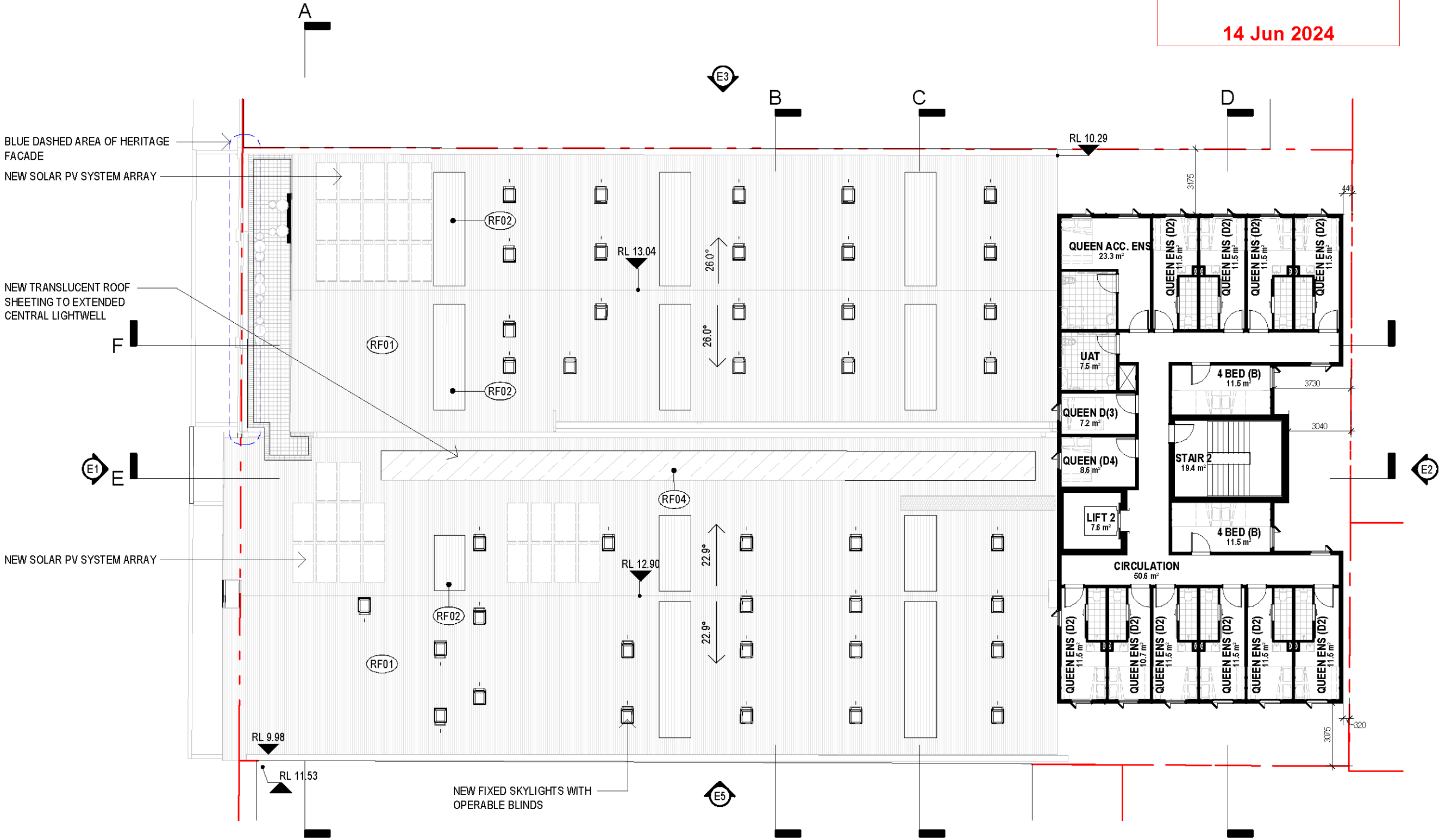
Scale: @ A3 / **1 : 200**

Drawing No. / **DA01.05**

P9 rothelowman

19 ESSEX STREET, FREMANTLE, WA

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

Revisions	P3	04.09.2024	CONCEPT COORDINATION MEETING
	P4	22.04.2024	ISSUE FOR INFORMATION
	P5	24.04.2024	DAC DRAFT PACK
	P6	29.04.2024	DAC ISSUE
	P7	04.06.2024	DA ISSUE
	P8	05.06.2024	DA ISSUE 1

Project

LyLo Fremantle

Drawing

L2-L3 Plan

19 ESSEX STREET, FREMANTLE, WA

Project No

223252

Author

AL

Scale: @ A3

1 : 200

Drawing No.

DA01.06

P8

rothelowman

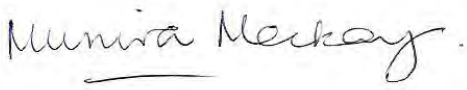


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Design Review Report

19 Essex Street, Fremantle: DR 1

13th May 2024

Design Review Report		
Subject	19 Essex Street, Fremantle (pre-lodgement) – Design Review 1	
Date	13 th May 2024	
Time	2.00pm - 3.15pm	
Location	City of Fremantle	
Design Reviewers	Munira Mackay Marco Vittino Patrick Kosky Flavia Kiperman Tobias Busch	Chair Panel Member Panel Member Panel Member Panel Member
Proponent	LyLo	
Project Team	Rothelowman Developed	
Planning Authority	City of Fremantle	
Staff	Chloe Johnston Erik Dybdahl Jonathan Dornan Gena Binet	Manager Development Approvals Senior Planning Officer Planning Officer Heritage Coordinator
Stakeholders	-	
Declarations	None	
Briefings		
Relevant Authorities	The Planning Officer made a presentation to the Committee.	
Project Team	The Applicant made a presentation to the Committee.	
Design Review Report endorsement		
Reviewers signature	 Munira Mackay	

Introductory Comments	
<p>In accordance with clause 78B(6)(b) of Local Planning Scheme No. 4, Council shall not determine a development application that proposes a building with a building height of 11 metres or greater in any zone other than the Residential or Industrial zones without first referring the application to the Design Advisory Committee for advice and having regard to the advice provided by the DAC. In providing advice to Council, the DAC shall have due regard to the following principles of good design: Character; Continuity and Enclosure; Quality of the public realm, Ease of movement; Legibility; Adaptability; and Diversity.</p> <p>For the purposes of recording the advice of the DAC, the City will record the strengths of the proposal and comments and recommendations in accordance with SPP7.0 Design of the Built Environment, as detailed below.</p>	
Design quality evaluation	
Strengths of the Proposal	<ul style="list-style-type: none"> • A new tourism development proposal in the Fremantle City Centre for visitors who are seeking affordable hotel accommodation. • The LyLo model appears to be successful, with hotels in some of the major cities of New Zealand and in Brisbane; the proposal for Fremantle is the first in WA. • The Hotel model has the clear core values of “Authentic, Home and Local”. • The Hotel is an ‘intense’ development in terms of the high density of visitors accommodated (potentially 248 guests based on the room configurations). This new visitor population should contribute to the life and vibrancy of the city. • A sustainable design approach through the adaptive re-use of the existing two buildings on the site (including a Local Heritage Listed building at 19B). • New 4-storey addition is located at the eastern rear of the site with minimal negative visual and amenity impacts on the streetscape and surrounding existing properties. • Provision of an extensive area of different types of communal facilities for guests. • Activation opportunities of the building interface with Essex Street at both floor levels by way of al fresco dining areas. • Inclusion of accessibility rooms for people with disabilities.
Principle 1 Context character and	<p><i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i></p>
	<p>a) The Proponent should ensure the early consideration of the integration of heritage with the new proposal. The Proponent should demonstrate an understanding of what is of heritage significance and what is not, and how much significant fabric is to be retained or demolished and its impact assessment on the heritage values of the place.</p> <p>b) Provide a Heritage Impact Statement (HIS) prior to the submission of the Development Application. The impact assessment should address, avoid and propose a mitigation strategy for the demolition of significant fabric and aesthetical streetscape impact. In addition to the Local Heritage Listed structures, the HIS also should address the State Registered Properties near to and adjacent to the site.</p> <p>The DAC suggested providing a streetscape perspective of the proposal looking north along Essex Street and to include the Fremantle Technical School.</p> <p>c) 19 Essex Street is essentially the amalgamation of two buildings on two lots (19A and 19B). The Proponent should maintain the character of Essex Street by respecting the existing fine grain and rhythm of the lot pattern and buildings.</p> <p>The DAC recommends a review of the proposed new awning (19A) and its exact horizontal alignment with the existing awning of the Local Heritage Listed building (19B). The awning is proposed to extend across the entire width of the building frontage as a consistent horizontal line; unfortunately, in doing so, the impression is conveyed of one large building.</p>

	d) Consider the rich DNA of the Central Fremantle Heritage Area and how local character, culture and history (indigenous and post-colonial), rather than an 'international' approach, should be woven into the development. Consider a multi-layered approach to 'storytelling' that can be educational and enjoyable for local and overseas visitors, such as the integration of public art, wayfinding, and signage throughout the publicly visible and accessible external and internal areas of the proposal.
Recommendations	<ol style="list-style-type: none"> 1. Demonstrate the integration of heritage with the new proposal. 2. Provide a Heritage Impact Statement (HIS) prior to the submission of the Development Application. 3. Maintain the character of Essex Street by respecting the existing fine grain and rhythm of the lot pattern and buildings; in particular, review the awning design. 4. Consider the rich DNA of the Central Fremantle Heritage Area and how local character, culture and history should be woven into the development.
Principle 2 Landscape quality	<i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i>
	a) Consider the opportunity to provide more landscape and natural amenity (daylight, ventilation) into the central communal "Main Street" on the first floor and the large communal areas at ground level.
Recommendations	1. Consider opportunities to provide more landscape and natural amenity into the communal areas.
Principle 3 Built form and scale	<i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i>
	a) The DAC supports the bulk, height and scale of the new 4-storey accommodation block.
Recommendations	1. None
Principle 4 Functionality and build quality	<i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.</i>
	a) Consider the technical aspects of design at this early stage, including safety and escape for visitors in the event of a fire.
Recommendations	1. Consider the technical aspects of design at this early stage.
Principle 5 Sustainability	<i>Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.</i>
	a. The DAC strongly supports the adaptive re-use of the two buildings for this new proposed hotel, however, consider long term sustainability and the ability to modify the proposal for other different uses in the future (noting, however, comments in Principle 1 about impact of changes on the integrity of the heritage building).
Recommendations	1. Consider long term sustainability and the ability to modify the proposal for other different uses in the future.
Principle 6 Amenity	<i>Good design optimises internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.</i>
	<ol style="list-style-type: none"> The DAC supports the clustering of the hotel rooms into legible "Houses" accessed off the central "Main Street". In view of the number of visitors accommodated in the main part of the hotel, consider the adequacy of the communal restroom 'block' and if re-distribution of the block's showers and toilets into smaller clusters (co-located with the "Houses") could improve accessibility. Generally, consider improvements to the access of natural amenity into the rooms, particularly where between 3 to 8 visitors are accommodated together. Good levels of natural ventilation and daylight are critical for visitors to be comfortable and if staying in the hotel for longer than a few days.

	<p>d) Consider improving the 4-storey block's rear staircase for the access of natural daylight and views out.</p> <p>e) Consider opportunities to improve the current narrow areas of external communal open space (at both levels) at the building's interface with the street.</p> <p>f) Consider improving the constrained front lobby area in view of the potential conflicts between visitors entering the front door, waiting outside lift 1, using stair 1, and squeezing through the narrow gap to the Bar.</p> <p>g) Consider relocating the bike store from the Back of House area to a more prominent and accessible part of the Hotel. Bike use is part of the Fremantle character and should be visible and celebrated.</p>
Recommendations	<p>1. Consider the adequacy of the communal restroom 'block' and possible co-location of facilities with the "Houses".</p> <p>2. Consider improvements to the access of natural amenity into the rooms.</p> <p>3. Consider improving the 4-storey block's rear staircase for the access of natural daylight and views out.</p> <p>4. Consider opportunities to improve the current narrow areas of external communal open space at both levels.</p> <p>5. Consider improving the constrained front lobby area.</p> <p>6. Consider relocating the bike store from the Back of House area to a more prominent and accessible part of the Hotel.</p>
Principle 7 Legibility	<i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i>
	<p>a) The layout of different spaces and circulation throughout the building is generally well considered and legible.</p>
Recommendations	1. None
Principle 8 Safety	<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>
	<p>a) The proposal is for 24-hour use and includes passive surveillance opportunities from openings to help enhance the safety of Essex Street.</p>
Recommendations	1. None
Principle 9 Community	<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
	<p>a) The DAC supports the tourism use and potential for hotel guests to socialise with the community in the Fremantle City Centre.</p>
Recommendations	1. None
Principle 10 Aesthetics	<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
	<p>a) Provide further detail on the materiality of the new 4-storey addition (the different cladding, screen types and colour palette).</p>
Recommendations	1. Provide further detail on the materiality of the new 4-storey addition.

Concluding Remarks

While the DAC provides its initial support for this generally well-considered tourism development proposal in the Fremantle City Centre, the DAC did identify several areas for further consideration:

- **Heritage:** Demonstrate an understanding of the integration of heritage with the proposal; provide a comprehensive Heritage Impact Statement with key street views; and review current design elements that are affecting the integrity of the Essex Street character.
- **Integrate local Fremantle character, culture and history into the development for a strong sense of place.**

- Provide more landscape and natural amenity into the communal areas.
- Review the technical aspects of design at this early stage.
- Consider long term sustainability and the ability to modify the proposal for other different uses in the future (noting the importance of maintaining the integrity of the heritage building).
- Amenity: Suggested improvements to the provision and location of restroom facilities; more natural amenity into guest rooms and the rear staircase; design of the constrained external communal open spaces at both levels and the front lobby; and a more prominent and accessible location for the bike store.
- Provide further detail on the materiality of the new 4-storey addition.

Schedule of Submissions – Tourist Development

Date Commenced: 8 July 2024

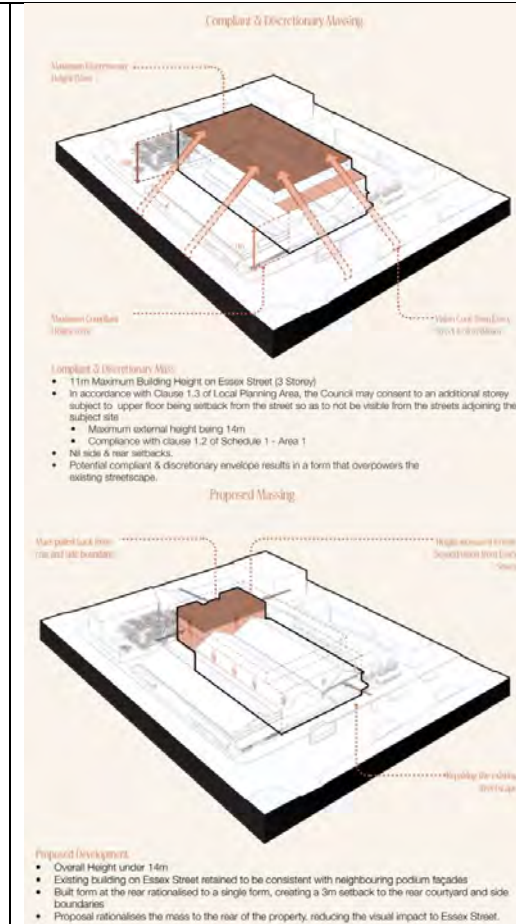
Date Ended: 6 August 2024

Total Submissions Received: 25 Submissions

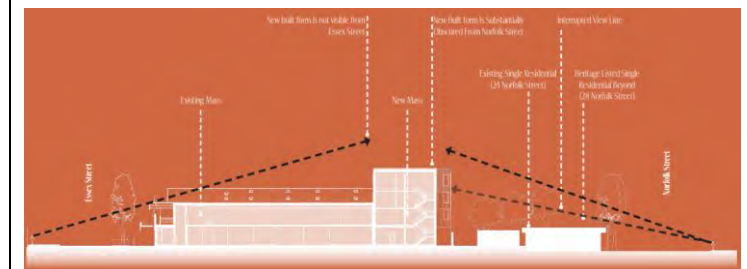
Consultation Method: Letters (200m radius), MySay Webpage, Sign on Site; Press Notices

Ref#	Submission	Applicant Response
1.	Fantastic opportunity to have the CBD developed and draw some visitors	Submission noted.
2.	<p>I am strongly against the proposal.</p> <p>Specific comments:</p> <p>"Vehicle Parking"</p> <p>Even the baseline number of 20 parking spaces is disingenuous. The LPA guidelines of one bay per 4 rooms are based around "normal" hotel rooms with one bed per room. Because of the unusual disposition of this hotel, a more reasonable baseline would be one bay per four beds - on other words, 60 parking bays, not 20.</p> <p>The reasoning behind reducing the parking from a baseline to zero, is flimsy and fatuous. More specifically:</p> <p>"who are very unlikely to be travelling to the Perth Metropolitan Region via car"</p> <p>- As we already learned from the parking problems at the South Mole, many backpackers and travellers do indeed travel with vehicles. And we also learn that they will go to extremes to avoid paying for parking. We already have the development at 65 South Terrace, and the new Police Precinct, both of which were allowed to proceed with underprovisioned parking - the result of which will mean an increase in the already prevalent illegal and overnight parking that occurs on nearby residential streets (Suffolk Street being a prime example). The addition of 250 beds, with zero parking, means there will be a huge influx of additional parking (both legal and illegal).</p> <p>"One-off care hire options, which are parked off-site, such as Car Share"</p> <p>- While this is a lovely idea, and I do really encourage such an option be developed (such as ZipCars in the US), there is zero supporting infrastructure for Car Share right now. As such, it can not be considered a factor in a development application.</p> <p>"The site includes sufficient bicycle parking facilities,"</p>	<p>Vehicle Parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none">o Fremantle Train Station (650m)o Several bus services from South Terrace (60m)o Taxi Rank South Terrace (75m)o Ride share options, such as Uber and Didio One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is</p>

<p>- the plans seem to identify 12 bicycle parking slots. How this is considered “sufficient” for 250 beds escapes me, but it is a patently untrue statement.</p> <p>“Reduction in on-site parking reduces traffic impacts to the congested city centre.”</p> <p>- I find this statement highly contentious. Although I know that this a common “Party Line” that is used to excuse densification-without-infrastructure-investment, really what happens is that people still use vehicles, and stress the already overstressed local parking system.</p> <p>Building Height</p> <p>- This building will have an immediate aspect on the viewshed from Norfolk Street, an important character area of this part of Fremantle. While the “Compliant Height” of 11m would not seem out of character for this area, the proposed variation to 14m is definitely out of character, and will negatively impact the viewshed and character of the area, especially from heritage places such as the Norfolk Hotel.</p> <p>Building Design</p> <p>“The architectural plans provide details of the materials, colours and finishes of the proposed development, which demonstrate a well- considered, visual interesting, and quality materials palette.”</p> <p>- Frankly, I find such statements simply insulting. The Aesthetic design of the 4-story part of the building looks like a cheap attempt at neo-brutalism. Not a single part of the design bears a true relevance to the surrounding area . The comparisons to the Fremantle Technical school are frankly laughable - the only similarity being that both are red.</p> <p>In summary:</p> <p>Too tall - reduce the design to be within the existing guidelines of 11m</p> <p>Too little parking/access - the developers need to invest in providing a minimum of 20 additional parking bays (preferably 60), even if those are offsite</p> <p>Poor design - a square box with a few wibbly bits is not the sort of architecture that merits the other special dispensations that the designers are asking for.</p>	<p>the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p> <p>Bicycle Parking Provision</p> <p>Submission is noted and in response, the plans have been updated to include 27 bicycle bays within the development, ensuring sufficient provision for guests.</p> <p>In addition to providing considerable bicycle parking to guests the 27 bicycle bays will encourage staff to cycle to work as is common practice in the City of Fremantle.</p> <p>Building Heights</p> <p>The development adheres to the allowable height envelope, with a maximum height of 14m (4 storeys) in accordance with Clause 1.3 of the LPS.</p>
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The 4-storey section is located at the rear of the property, away from the adjoining Essex Street, minimizing its impact.



		<p>The design ensures that the development remains concealed from natural sightlines on Essex and Norfolk Streets, with visibility from Norfolk Lane mitigated by the existing structures at 21 Essex Street and the neighbouring tree canopies.</p> <p>Aesthetic Quality The design process carefully integrates layers of characteristics from the surrounding streetscape, ensuring the development is visually harmonious with its context.</p>
3.	<p>Tourist Development! That made me chuckle. I'm happy for development in Freo, but this looks pretty ordinary to me..... I can't see how it improves or adds to the 'vibe' of Freo. although budget accomodation is necessary, this looks like a pretty unappealing place to stay.</p> <p>If you stayed here, would you remember your time in Freo fondly? Who knows.</p> <p>Anyway, so many beds with virtually no parking is an issue for us - we live in Suffolk St and the parking is keen and traffic a little too fast at times- this place won't help that.</p> <p>I don't think this proposal [unlike many others] has much to offer our great town</p>	<p>Parking Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p>

		<p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p>
4.	<p>The effect of building on all the surrounding businesses would be dire.</p> <p>Why not rejuvenate an area with Existing buildings ie Paddy Troy Area... Heritage rejuvenation and it's in the centre of Fremantle</p> <p>4 storey Would not be in keeping with surrounding buildings, What about? not in keeping with everything else</p> <p>Hotels are not at full capacity especially during the winter time... I can't emphasise enough the effect that this would have on my business which is a bed-and-breakfast during construction time. It would be an absolute nightmare.</p> <p>Parking would also be another issue we struggle with the parking at the moment. We do not need another hotel on Essex Street. There is the Esplanade etc.</p>	<p>Issues with Use</p> <p>The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development proposal is entirely consistent with the planning aims for this zone and is highly compatible with the surrounding context of the local area.</p> <p>Whilst we can understand the concerns of all residential land uses within the immediate context every effort has been imposed to reduce impacts, including setting back the upper storeys, providing screening to reduce overlooking, implementing an acoustic report to ensure noise protection.</p> <p>In addition to the above, a development was approved for a change of use in 2023 to include Public Amusement. Additionally, the pre-existing Development Application (DA) from 2020 specified a four-storey tourist accommodation building with additions and alterations to an existing structure. This project is in line with the previously approved uses and aligns with the area's ongoing development strategy.</p>
5.	<p>I definitely do not support this development. The noise levels from the street over the weekend is already beyond acceptable especially later in the night. Adding another 'tourist' accommodation next door would ruin the comfort of the residents of our complex. There is already a huge shortage of parking around Essex Street.</p> <p>The townhouses at 15-17 Essex Street have limited sunlight and this would be ok impacted by a building of 4 storeys next door</p>	<p>Shadow Study Analysis</p> <p>The proposed development casts significantly less shadow than what is permissible under the compliant building envelope. Additionally, it's important to note that the townhouses' courtyards at 15-17 Essex Street are already overshadowed by existing fence lines, as shown in the study. For further details, please refer to</p>

		DA04.01, which provides a breakdown of the overshadowing impact.
6.	<p>We write in support of the proposed redevelopment of the development of 19 Essex Street, Fremantle.</p> <p>Essex street is ready for development and this proposal does not appear to change the streetscape from outside. The proposal to have more accommodation in the down town Freo is very positive and we support the proposal 100%</p>	Submission is noted.
7.	<p>I do not believe that a 4 storey building in this area that will attract increased noise, traffic and generally increased issues will be beneficial to the community. It is a small space for what is being considered and I am worried for the impact on the Fremantle community.</p> <p>Whilst I agree that a larger space for tourists to stay would be beneficial I believe there are more appropriate locations for this.</p>	<p>Scale of Development</p> <p>The proposed development adheres to the allowable building envelope, with a maximum height of 4 storeys (14m) as stipulated in Clause 1.3 of the Local Planning Scheme (LPS). The design has been carefully considered to align with the planning regulations and the character of the surrounding area.</p>  <p>Compliant & Discretionary Allow</p> <ul style="list-style-type: none"> • 11m Maximum Building Height on Essex Street (3 Storey) • In accordance with Clause 1.3 of Local Planning Area, the Council may consent to an additional storey, subject to: upper floor being setback from the street so as to not be visible from the streets adjoining the subject site <ul style="list-style-type: none"> • Maximum external height being 14m • Compliance with clause 1.2 of Schedule 1 - Area 1 • Nil side & rear setbacks. • Potential compliant & discretionary envelope results in a form that overpowers the existing streetscape. <p>Amenity</p> <p>The development will introduce new amenities that will benefit the local community, including a ground-floor bar that is intended to enhance the vibrancy and social atmosphere of the area. This addition is designed to complement the existing offerings in Fremantle, providing a welcoming space for both residents and visitors.</p>

8.	<p>I welcome, more accommodation in the City, but I'm concerned about the lack of car parking for this proposed site. Can Council please consider this as part of the application process?</p>	<p>Parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off car hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p>
9.	<p>I support this submission. Fremantle is in need of higher density and tourist accommodation. I have a office very close by on South Terrace and think more backpackers will be good for the area.</p>	

10.	<p>I do not support this type of development in Fremantle this location is not the right one for a backpacker style accommodation.. especially one with so many beds !! The impact on the locals would be very detrimental to their amenity and rights to peace and quiet in their chosen area. Thus should not be allowed to proceed!</p>	<p>Amenity</p> <p>The development is designed to enhance the local area by introducing new amenities, including a ground-floor bar and restaurant. These features aim to provide additional social and dining options for both residents and visitors, contributing positively to the vibrancy of the surrounding community.</p>
11.	<p>I was made aware of this development application last week when a resident sent a plea for support to the Fremantle History Society. Having had a look at the plans I can understand why they are concerned about the impact this building, if approved, would have upon their own properties. Essex Street and Norfolk Street sit within the Central Fremantle Heritage area, an area renowned for its well-preserved architectural heritage. The existing buildings at 19a have certainly had some unsympathetic renovations over the years, although the original structures built by Frederick Instone between 1913 and 1915 are still evident. Despite the extensive alterations it is deemed significant enough to be included on the City of Fremantle's Local Heritage Survey as being at Level 2, of considerable cultural heritage significance in its own right within the context of Fremantle, its conservation a priority. It is in good company with the Fremantle Technical College and Annex on the corner of South Terrace, Pirate backpackers at no. 11 (formerly Star Hotel), the Esplanade Hotel and the 1890 terrace cottages across the road at nos. 20,22 and 24, as well as the Port Flour Mill. I realise the original building at no. 19b was demolished in 2012, and although little attempt was made blend into its surroundings, the replacement building still 'fitted' within the existing streetscape. It is difficult to understand how a brown, metal-clad, 4-storey box sitting in the midst of 1-2 storey heritage buildings could be considered an appropriate addition to that environment. The Environmental Impact Statement by Urbis is something of an understatement when it describes the development as a 'modest modular design'. Such a structure must have a huge impact upon the neighbouring dwellings, their sunlight and privacy. The Transport Impact Statement concludes that the majority of hostel guests will use Uber, taxis and public transport but even if this is true, four on-site parking bays are not going to be adequate for such a large development. I am not anti-development, growth and change is inevitable, but good development, with respect to a city's history, heritage and culture, and remembering the reasons why we all choose to work, live and spend time here in the first place. I sincerely hope Fremantle Council will reject these plans and send Rothelowman back to the drawing board.</p>	<p>Impact to surrounding Buildings</p> <p>The proposed development carefully considers the existing Development Application (DA), shadow analysis, and allowable building envelope. The design intentionally does not maximize the buildable area to minimize adverse impacts on the wider streetscape and heritage context.</p> <p>Heritage Considerations</p> <p>The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.</p> <p>The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and</p>

		<p>were fully supportive of the proposal. The following response was provided by the HCWA:</p> <ul style="list-style-type: none">- A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant.- The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street.- The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe. <p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <ol style="list-style-type: none">1. A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken.2. A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer
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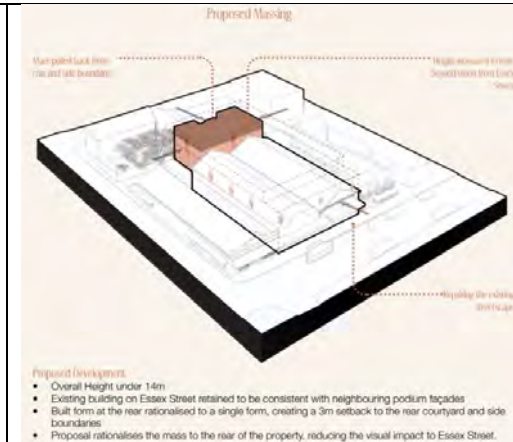
To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.

Streetscape Considerations

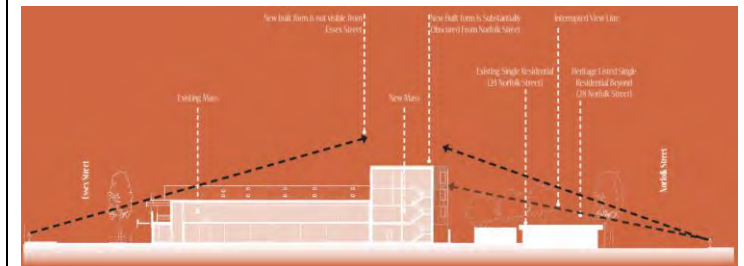


The proposed development complies with the allowable height envelope of 4 storeys (14m) as per Clause 1.3 of the Local Planning Scheme (LPS).

The 4-storey section is positioned at the rear of the property, away from Essex Street, reducing its impact on the streetscape



The design ensures that the development remains concealed from natural sightlines on Essex and Norfolk Streets, with visibility further mitigated by existing structures and tree canopies.



Shadow Study Analysis

The development casts significantly less shadow than what is permissible under the compliant building envelope. For detailed information, refer to DA04.01, which provides a breakdown of the overshadowing impact.

12.

The living spaces, balconies and large rear courtyard of our property, together with the amenity we enjoy in our home at this location will be significantly adversely impacted by this proposed hostel development.
Our purchase of our property in this historic complex was made with full knowledge &

Overshadowing Considerations

The proposed development will cause significantly less overshadowing than allowed within the compliant envelope. Refer to DA04.01 for detailed analysis.

<p>understanding of the Land-use zoning of our site as well as the neighboring sites to our property - (also noting 19 Essex Street as Commercial zoning).</p> <p>Please note that:</p> <p>1: The Port-Mill complex has the highest State & National heritage registration & protection.</p> <p>2: Subterranean retaining, side & rear boundary rubble-limestone walls are comprised within the Heritage listing and protection.</p> <p>3:LAND USE:</p> <p>-We do not agree to any change to the current 'Land-Use' designation of 19 Essex Street.</p> <p>-The land use of the Port-Mill complex is mis-represented on submitted documentation: Specifically: There are 15 Units within the Port Mill complex. ONE of 15 Units is Approved as a Short Stay B&B. One Unit is a small wine bar and one is a small Art gallery. The remaining 12 Units are Residential. Strata Bylaws for The Port Mill complex specifically EXCLUDE use of any other units for Short-Stay accomodation.</p> <p>-We do not want (up to 248) hostel guests residing immediately adjacent to our home- this is ridiculous.</p> <p>-We do not want a 4 storey, 14 metre high accomodation block towering over our home. - The maximum compliant 11 Metre height must never be exceeded.</p> <p>-We do not want overshadowing and overlooking issues impacting our home or the amenity we have to enjoy our balconies and private courtyard & outdoor living spaces. Our access to existing sunlight must never be impacted and our visual privacy must never be impacted.</p> <p>-We do not want the 'personal accumulated' noise impact of up to 248 'Hostel' style guests living 3 metres from our home / outdoor living spaces.</p> <p>-We do not want the noise of residential infrastructure for 248 guests impacting us - including Air-conditioning, bin lids opening /closing / bin movements & delivery services catering for 248 guests as well as proposed bar / cafe facilities.</p> <p>4: ONSITE CAR PARKING:</p> <p>-We do not agree to any reduction whatsoever to the minimum onsite car parking requirements for any new development on this site. Of note is that 3 car parking bays (one of these disabled) and one service bay are proposed. - The minimum car parking requirements for this development is 20 bays. Car parking in central Fremantle is already at a premium. - Such a variation is completely ridiculous. - Anticipating that NONE of the 248 Hostel guests as well as workers at the proposed facility would not have ANY vehicles to park at this premises is ridiculous. - A major adverse impact to the existing, extremely</p>	<p>Heritage</p> <p>The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.</p> <p>The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and were fully supportive of the proposal. The following response was provided by the HCWA:</p> <ul style="list-style-type: none"> - A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant. - The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street. - The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe.
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<p>limited & congested street parking in central Fremantle would certainly be expected. - Many backpackers / hostel staying people have cars / vans in which they travel around Australia! ... To expect zero cars for 248 guests plus staff - is DREAMING!</p> <p>5: BUILDING HEIGHT:</p> <p>- We do not approve of any construction above the compliant level of 11metres. - This would cause an adverse visual and sunlight impact on our property. This would also be an ugly and inappropriate construction within the lower height surrounding buildings - and upon our heritage precinct. What a visual 'sore' within the beauty of old Fremantle such a modern tower would be! - This is unthinkable!</p> <p>-We do not agree with the City of Fremantle - our TRUSTED and funded City council, with using ANY discretionary decision making to agree to any height increase (in this case a proposed 14 metre accommodation tower).</p> <p>Thank you for accepting my submission in direct opposition to this proposed development (#DAP002/24) of 19 Essex Street Fremantle. Please contact me for further input as required.</p> <p>As the council planning officer notes, the decision is based on planning concerns which need to be specifically addressed in your responses as neighbours to the site. Noting the 'amenity' issues eg an escalation of ongoing noise from the hospitality venue, the proximity of noise from the rubbish control which happens for commercial properties often over night with trucks reversing etc etc, potential overlooking of rear courtyards, heritage protection particularly – the vicinity of heritage protected buildings is significant to their continuing significance, obligations to protect the fabric of listed heritage buildings from degradation eg vibration caused by compacting for adjacent building works is likely to cause cracking and movement in the 160 year old limestone construction.</p> <p>It is critical to address the misinformation contained in the submission by the proponents that shows all of 15-17 Essex Street being a hospitality and short term accommodation venue.</p>	<p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <ol style="list-style-type: none"> 1. A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken. 2. A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer <p>To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.</p> <p>Side and Rear boundary Limestone Walls</p> <p>A dilapidation survey, along with a monitoring and structural movement plan, will be required before starting construction to address potential impacts on the heritage-listed limestone walls.</p>
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		<p>Land Use Designation - Change of Use</p> <p>The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development proposal is entirely consistent with the planning aims for this zone and is highly compatible with the surrounding context of the local area.</p> <p>Whilst we can understand the concerns of all residential land uses within the immediate context every effort has been imposed to reduce impacts, including setting back the upper storeys, providing screening to reduce overlooking, implementing an acoustic report to ensure noise protection.</p> <p>In addition to the above, a development was approved for a change of use in 2023 to include Public Amusement. Additionally, the pre-existing Development Application (DA) from 2020 specified a four-storey tourist accommodation building with additions and alterations to an existing structure. This project is in line with the previously approved uses and aligns with the area's ongoing development strategy.</p> <p>Documentation Update</p> <p>The documentation will be revised to accurately reflect the current land use of the Port-Mill complex</p> <p>Allowable Envelope Clarification</p> <p>The proposed development adheres to the allowable building height of 4 storeys (14m), as detailed in point 5.</p> <p>Existing Services Laneway</p> <p>While there are concerns about potential noise, it is important to note that the current access from Norfolk Lane is a service laneway. This should be considered in the context of existing service operations.</p>
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Parking

Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:

- Fremantle Train Station (650m)
- Several bus services from South Terrace (60m)
- Taxi Rank South Terrace (75m)
- Ride share options, such as Uber and Didi
- One-off car hire options, which are parked off-site, such as Car Share.

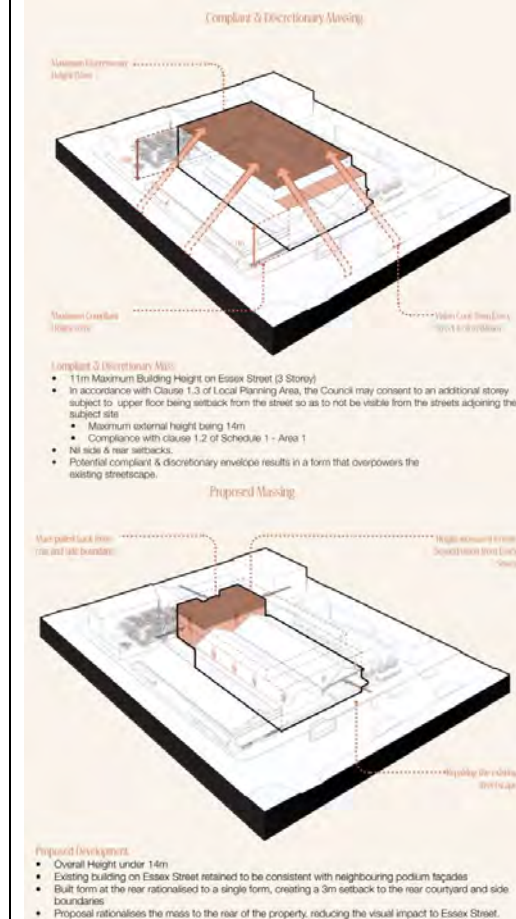
It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.

As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.

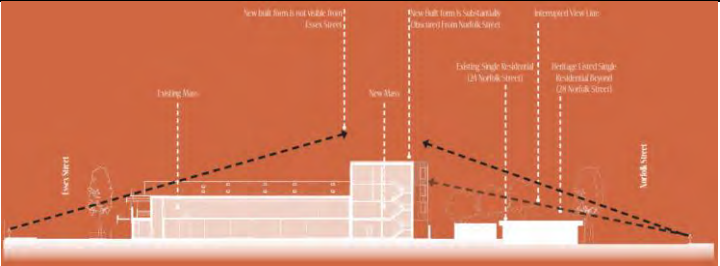
In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.

Building Height

The development adheres to the allowable height envelope, with a maximum height of 14m (4 storeys) in accordance with Clause 1.3 of the LPS.



The 4-storey section is located at the rear of the property, away from the adjoining Essex Street, minimizing its impact.

		 <p>The design ensures that the development remains concealed from natural sightlines on Essex and Norfolk Streets, with visibility from Norfolk Lane mitigated by the existing structures at 21 Essex Street and the neighbouring tree canopies.</p>
13.	<p>We are young Fremantle home owners who have been backpackers, and strongly support a more vibrant and prosperous Fremantle.</p> <p>While we are adamant that a new affordable hotel is a great addition to Fremantle - we think that the building must be tasteful and align with the heritage context.</p> <p>While we like aspects of the proposal (such as the Essex St frontage), we think that the new 4 story structure with Lylo branding is excessive, does not blend into the heritage context and may reduce the value of surrounding heritage properties (in particular, due to the height of the new structure shadowing nearby courtyards).</p> <p>Fremantle is not the Gold Coast - and this must be reflected in all aspects of the proposal. I'm not sure we want this new structure, as proposed, to define the cityscape.</p> <p>With a more modest proposal for the new structure for accommodation, we would support this proposal.</p>	<p>Branding Supergraphic</p> <p>The prominent Lylo branding has been removed from the proposal.</p>
14.	<p>26 – 28 Norfolk Street is a heritage listed property originally constructed in 1860 and has a Statutory Heritage Listing on the State Register. It is a property of significant value to the State and general Fremantle community.</p> <p>I would like to object to the proposed development of 19 Essex Street, Fremantle and raise the following points. Shadowing No shadowing report has been provided, but we assert that the shadowing from a 4 storey building, approximately 2.5 metres from our boundary will be significant. See image below, showing the scale of the building from a Norfolk Lane view:</p> <p>The shadowing will greatly impact upon our ability to maintain our garden and the</p>	<p>Overshadowing Considerations</p> <p>The proposed development will cause significantly less overshadowing than allowed within the compliant envelope. Refer to DA04.01 for detailed analysis.</p> <p>It is important to note that the actual allowable envelope for the development extends to the boundary; the current proposal includes a setback as a concession to minimise impacts.</p>

substantial trees that are flourishing along that boundary. Furthermore, the shadowing will impact on our ability to enjoy our garden and outside space.

Construction Damage 26-28 Norfolk Street was built in 1860, and the building simply sits upon the ground, with no footings. The building is fragile, and construction so close to it has the potential to do unrecoverable damage. The Old Mill Building is in a similar predicament. This building is such a jewel to Fremantle, known to anyone who visits Fremantle. And damage to it would be devastating.

Loss of Privacy With our bedroom situated at the rear of 26 Norfolk Street, the loss of privacy will be significant. The building will provide direct visual access not only to the rear of the building and the verandah where we entertain guests, but also our garden which we use on a daily basis. The drawings show we could have up to 24 windows with a view into our property.

Heritage Impact Statement The Heritage Impact Statement provided by the developer makes very little mention of our property other than the boundary wall. See excerpt below: Construction of a four-storey addition in the rear carpark

This carpark area has been historically clear of built fabric and is of no contributory significance to both the site and the wider West End Conservation Area. A small portion of the limestone boundary wall adjoining State Heritage Registered place no 966 26-28 Norfolk Street is of primary significance. The wall will be retained and protected during the proposed development.

The development of a 4 storey building in this currently vacant area will dwarf the building at 26 – 28 Norfolk Street and the visual impact will be great. This has not been addressed at all.

24 Norfolk Street, Fremantle A proposal to demolish the heritage listed home at 24 Norfolk Street (Ref: DA0148/11), to build a 2-storey apartment complex was refused, and the criteria for that refusal should also be applicable to the development at 19 Essex Street. In particular:

As the site was adjacent to 26-28 Norfolk Street, the development was referred to the Heritage Council of WA for comment. As the 19 Essex Street development is also adjacent to 26-28 Norfolk Street, I strongly feel that again the Heritage Council should be approached for comment. It was acknowledged that Norfolk Street, has several areas of cultural heritage significance – these properties included numbers 8, 12-14, 16, 22, 24 and 26-28 Norfolk Street. It was noted that the scale of these buildings was of consequence in making the properties a cohesive group. Smothering these single storey heritage properties with a four-storey development looming over them will be a huge loss to the

Heritage – Concerns about damage

The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.

The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and were fully supportive of the proposal. The following response was provided by the HCWA:

- A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant.
- The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street.
- The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe.

<p>streetscape that can never be recovered.</p> <p>Visual Impact The development plans included no artists drawings showing the impact of the building on the Norfolk Street streetscape. Even the drawings showing the Essex Street elevations, only show the façade of the existing building, with no representation of the four-storey development at the rear, which will be clearly visible from Essex Street and Norfolk Street. The massive building is completely out of context with its heritage listed neighbours and will be detrimental to the overall historic feel of the West End. I refer to the image below, where the four-storey addition simply evaporates and is apparently invisible. This is an inaccurate representation of the development.</p> <p>Both Essex Street and Norfolk Street have been declared previously by council to be of particular heritage, social and tourism significance due to its unique mix of heritage buildings from various historic periods. An enormous four-storey, plain boxed building will destroy this unique area and set a dangerous precedent for further development. A looming brown box will quite frankly look ridiculous, viewed from the Fremantle Technical College on Norfolk Lane, or imposing itself over the humble cottages on Norfolk Street. Further the site itself is also of significant heritage value. The proposed development seems to mute all its historic appeal, instead applying a bland and modern façade over it. If development on this site is to occur, surely it should enhance the streetscape, and be an improvement on the building. The current proposal adds no value to the historic context of the area. No concession has been made in either the scale, form or materials of the rear four-storey addition to complement the existing heritage buildings or the heritage neighbours on Norfolk Street or the Old Mill Building. This image from the developer's Architectural Report is downright misleading:</p> <p>The image shows a smudge of the rear building in the centre of the photograph. Its implying that a four-storey building (a whole storey more than the apex of the front façade, will only be seen at a height comparable to the second storey. This is a blatant misrepresentation and the developers should be held accountable for providing realistic views of the development, as this proposed view is laughable and completely inaccurate.</p> <p>Discretionary Land Use I understand that the proposal is reliant on the council granting discretionary land use for "Tourist Development". Whilst the importance of bringing tourists to Fremantle cannot be denied, backpacker facilities are already well provided for in Fremantle.</p> <p>Noise I believe a backpacker style development has the potential to create further anti-social behaviour, a problem we are already struggling to battle in Fremantle. A development of this size will result in excessive noise, which will intrude upon the</p>	<p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <ol style="list-style-type: none">2 A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken.3 A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer <p>To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.</p> <p>Streetscape Considerations</p>
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residential properties at the rear and side of the development. I am very concerned about air conditioner noise.

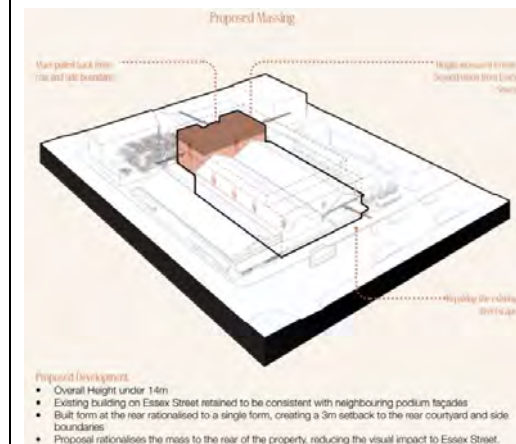
Parking Four parking bays for nearly 300 people is ludicrous. The parking situation in Fremantle is already a point of contention for visitors and residents alike, and no provision is made to provide for either the guests, the staff or the hospitality venues.

In summary, the impact on the heritage streetscape from this development is overwhelming and I would be so disappointed if council approves this development. Four-storey boxes don't belong in the historic area of Fremantle and allowing it will be the beginning of the end for Fremantle. What we have is to be treasured. I consider myself so lucky to be part of Fremantle's residential community and I hope what we have will be protected. I need the City of Fremantle to be our Guardian, and the protector of everything that makes Fremantle unique. In particular, The Old Flour Mill is the real jewel amongst Fremantle's heritage listed buildings, and everything should be done to conserve it and the area surrounding it. The development does not bring added value to the area, but has the potential to greatly diminish it.

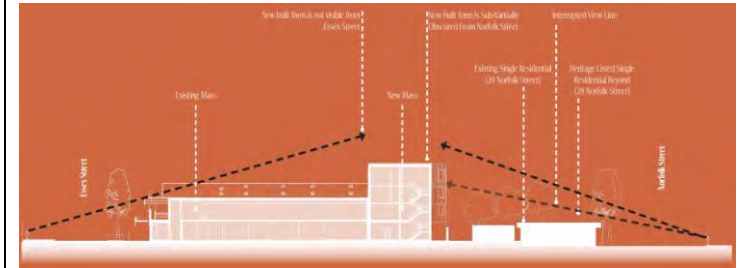


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The 4-storey section is positioned at the rear of the property, away from Essex Street, reducing its impact on the streetscape



The design ensures that the development remains concealed from natural sightlines on Essex and Norfolk Streets, with visibility further mitigated by existing structures and tree canopies.



Aesthetic Quality

The design process carefully integrates layers of characteristics from the surrounding streetscape, ensuring the development is visually harmonious with its context.

Issues with Use

The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development proposal is entirely consistent with the planning aims for this zone and is highly compatible with the surrounding context of the local area.

Whilst we can understand the concerns of all residential land uses within the immediate context every effort has been imposed to reduce impacts, including setting back the upper storeys, providing screening to reduce overlooking, implementing an acoustic report to ensure noise protection.

In addition to the above, a development was approved for a change of use in 2023 to include Public Amusement. Additionally, the pre-existing Development Application (DA) from 2020 specified a four-storey tourist accommodation building with

		<p>additions and alterations to an existing structure. This project is in line with the previously approved uses and aligns with the area's ongoing development strategy.</p> <p>Parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none">○ Fremantle Train Station (650m)○ Several bus services from South Terrace (60m)○ Taxi Rank South Terrace (75m)○ Ride share options, such as Uber and Didi○ One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p>
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		<p>Noise</p> <p>In response to concerns raised about noise the project team engaged the services of a suitably qualified Acoustic Engineer to prepare an Acoustic Report. The report confirmed that whilst the land use may produce noise levels this is likely to be limited to the mechanical services such as the air conditioning plant.</p> <p>It was noted that given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.</p> <p>A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.</p>
15.	<p>Concerns about the development plan</p> <p>1. The proposed 14 metre building height is a concern for shading 17+15 Essex Street (hereafter referred to as The Flour Mill). The Flour Mill includes three commercial buildings and 12 residential homes, and the buildings and courtyards may be permanently shaded. There is also concern that the low-rise Flour Mill, an architectural heritage site in Fremantle, will be damaged.</p> <p>2. According to the planning drawings, at least 12 windows will be visible into The Flour Mill residences, which may infringe on residents' privacy in 7/17 and 8/15 and others.</p> <p>3. The plan for four car parks and 11 bicycle parking spaces is insufficient for a high-density backpacker facility with up to 250 guest per night, It just shows that the people who are proposing this have no idea why these places exist in Japan and what purposes they serve transient occupants, it's not at all sympathetic to the things that make Fremantle a desirable destination or compatible with the surrounding sites, Fremantle is not a dystopian denizen.</p>	<p>Overshadowing</p> <p>The proposed development will cause significantly less overshadowing than allowed within the compliant envelope. Refer to DA04.01 for detailed analysis.</p> <p>It is important to note that the actual allowable envelope for the development extends to the boundary; the current proposal includes a setback as a concession to minimise impacts</p> <p>Overlooking</p> <p>The development has been designed to reduce any potential overlooking concerns that may impact adjoining properties. As shown on the elevation</p>

<p>4. There are expected to be noise issues from 24/7 access by up-to 250 guests, luggage movement, noise from air conditioning, cars arriving and leaving on an already congested street. People already use our driveway at 15-17 as a temporary stopping area for ubers and other drop offs /pickups even though it's a no-stopping zone, some even get out of their cars and are not present with their vehicles when we have arrived home to try and enter our driveway. We have recently replaced the old gates with a new roller door (and the no -standing signs are yet to be replaced to make it clear- people still ignore it anyway even with proper signage) .</p> <p>5. Despite the plan for a high-density 250-guest hotel, only four car parks and 11 bicycle parking spaces will be provided. It is believed in the application that there will be no negative impact on traffic because many customers use public transport, Uber and minibuses. However, in reality, Essex Street parking is busy most days from approx. 8am / mid-morning till late at night, (early hours if weekend, like 3-4am), especially on weekends and school holidays when it sunny. Therefore, many Ubers and taxis use our driveway to pick up and drop off passengers. The new proposal would have 250 customers coming and going 24/7, which could create a high level of transient traffic and disrupt already problematic access by residents of The Flour Mill.</p> <p>6. Since the move of offices and schools to 19 Essex Street, there has been a significant increase in street smoking on Essex Street, resulting in the inflow of second-hand smoke into The Flour Mill. The new proposed development does not appear to provide any smoking areas within the buildings. This raises concerns that even more smokers are likely to smoke on Essex Street, outside or near our building which could have a negative impact on the health of surrounding residents from second-hand smoke and general stink.</p> <p>7. A previous development at 19 Essex Street in 2013 of thew building that is there now caused structural damage and water ingress issues which were not properly addressed by the developer, a Soak well and old stone walls were removed without consulting anyone, causing water issues at The Flour Mill. The site development is a heritage listed building, The Flour Mill, in particular, the section that is on the 17-side adjacent to 19 Essex street was built in early 90's. (the 15 Essex street side was the original Mill built in 1860's) The refurbishment of the existing building at 19 Essex Street and the addition of a four-storey extension to the rear of the site could cause further damage to the Flour Mill on the 17 side, also if there are any piles or other support structures drilled into the ground for a 4 storey building the vibrations of this type of construction could cause damage to the limestone (as shown previously for existing 19 Essex st construction which is not even 4 storey) We are particularly concerned about the potential damage to the foundations of the</p>	<p>drawings screening has been applied where suitable to prevent overlooking to the south west.</p> <p>Parking / Traffic</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p>
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	<p>heritage listed building and walls supporting the carpark below 17 Essex St. and further ongoing issues with poor water containment and drainage into the property and carpark below 17 Essex street. At certain times of the year there is quite a lot of rainfall, and our sump pumps are operating continuously to keep the carpark as dry as possible, this cost us quite a bit extra in power bills.</p> <p>The track record of damages caused previously by the developers of the current 19 Essex street, as well as current tenants smoking outside our building on the street and stinking out the common areas of our home and other issues with this submission mentioned above leave us no option but to strongly oppose its construction in its entirety.</p>	<p>Regarding congestion – the prior access to the development was off the laneway. Reduction on car bays would mean less congestion.</p> <p>Noise</p> <p>In response to concerns raised about noise the project team engaged the services of a suitably qualified Acoustic Engineer to prepare an Acoustic Report. The report confirmed that whilst the land use may produce noise levels this is likely to be limited to the mechanical services such as the air conditioning plant.</p> <p>It was noted that given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.</p> <p>A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.</p> <p>Heritage</p> <p>The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context</p>
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	<p>established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.</p> <p>The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and were fully supportive of the proposal. The following response was provided by the HCWA:</p> <ul style="list-style-type: none">- A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant.- The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street.- The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe. <p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <ol style="list-style-type: none">4 A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken.5 A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of
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		<p>works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer</p> <p>To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.</p>
16.	<p>Development at 19 Essex Street, Fremantle</p> <p>I'll start by saying that my wife and I were only made aware of the proposed development by a concerned resident of Essex Street. Nothing from the City of Fremantle and/or the developer. We received no correspondence or information regarding this development though it quite clearly has a very major direct impact upon us. Not happy.</p> <p>I have since had the opportunity to review that redevelopment proposal. Thankyou concerned resident.</p> <p>Impact on Aesthetics</p> <p>Our house, built in 1962 is a Heritage Listed Home having been placed on the Register of Heritage Places in 1997.</p> <p>Within the 'Register of Heritage Places – Assessment Documentation' it states, "It is valued by the Fremantle community for its intact late 1900's aesthetic architecture.</p> <p>It contributes to west Fremantle's colonial architecture of the convict period. It compliments the aesthetic simplicity of other convict and colonial buildings in the streetscape."</p> <p>The documentation goes further to say,</p> <p>"26-28 Norfolk Street, within the historic West End Precinct of Fremantle, in its prominent corner location at the intersection of Norfolk Street and Norfolk Lane, presages the heritage ambience of the streetscape and precinct values of the immediate environs and the street blocks adjoining Norfolk Street. Further, interest is generated by the contrasting facades on adjoining street frontages. The building reinforces the heritage ambience of its precinct with the diversity of its low profile, its detail, colour and mellowness in that section</p>	<p>Shadowing Analysis / Setbacks</p> <ul style="list-style-type: none"> - The proposed development will cause significantly less overshadowing than allowed within the compliant envelope. Refer to DA04.01 for detailed analysis. - It is important to note that the actual allowable envelope for the development extends to the boundary; the current proposal includes a setback as a concession to minimise impacts. <p>Landscaping</p> <p>Given the proposed development is constrained as it is predominately within an existing building envelope on a City Centre site landscaping provision has been a challenging component to address. However, the proposal has integrated landscaping where possible, this includes indoor planting within common areas, balconies.</p> <p>In addition to the landscaping proposed within the building, the existing parklet has already been</p>

<p>of the Historic Town. It is set among a rich resource of heritage buildings in the immediate vicinity including the Norfolk Hotel, Scots Church, Fremantle Technical School, Essex Street Mill, The Fremantle Pavilion and the Parry Street Grandstand.”</p> <p>In 2011 a development application was made to construct a 2-storey development to our neighbouring property at 24 Norfolk Street, (DA0148/11 refers). This application was rejected and the main reason for refusal was the aesthetic impact it would have to our property at 26-28 Norfolk Street.</p> <p>These previous determinations were made to preserve the history of our building in particular, and the historic aesthetic of the area generally, something that cannot be said for the current proposal at 19 Essex Street.</p> <p>The current proposal for a 4-storey development within 3.0m of the rear boundary of our property is obscene and preposterous. The building will absolutely dwarf our residence and be a visual blight on the landscape. Several references to aesthetics have been made in previous determinations already mentioned. I think its blatantly obvious that the proposed development is detrimental to that aesthetic and should be stopped at all costs. It does not ‘compliment’ other buildings, it does not possess ‘aesthetic simplicity’, has no ‘aesthetic ambience’ and certainly does not have a ‘low profile’ or ‘mellowness’.</p> <p>Shadowing</p> <p>The Four storey proposed development is situated to our north west and is within 3.0m of our rear boundary. Other than being a visual eye-sore to us it will undoubtedly cast a shadow over our property and have impact on our amenity. Perhaps conveniently, no mention of shadowing is made in the proposal.</p> <p>Damage/health of trees and other vegetation</p> <p>The reduced amount of natural light we receive may impact of the health of the large trees we have in our backyard. The possible loss of those trees and other vegetation is in direct contradiction of current thought and policy on the preferred retention of trees within urban areas.</p> <p>Where is this current proposal’s greenscape?</p> <p>Damage to home due to moisture retention and rising damp</p> <p>Though I am not an expert, will the loss of natural light hitting upon the rear of our limestone-built property not impact on the building’s ability to stay dry and not suffer from rising damp and other moisture related issues? In any literature I have read concerning limestone building there is always mention of the building’s ability to ‘breathe’ with adequate ventilation and exposure to sunlight.</p> <p>Damage to building due to construction</p>	<p>enhanced by the landowner who proposes the development.</p> <p>Privacy</p> <p>The development has been designed to reduce any potential overlooking concerns that may impact adjoining properties. As shown on the elevation drawings screening has been applied where suitable to prevent overlooking.</p> <p>Noise / Servicing</p> <p>In response to concerns raised about noise the project team engaged the services of a suitably qualified Acoustic Engineer to prepare an Acoustic Report. The report confirmed that whilst the land use may produce noise levels this is likely to be limited to the mechanical services such as the air conditioning plant.</p> <p>It was noted that given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.</p> <p>A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.</p> <p>Servicing</p> <p>Please note that servicing is proposed to remain as is currently operating with the bar through the Norfolk</p>
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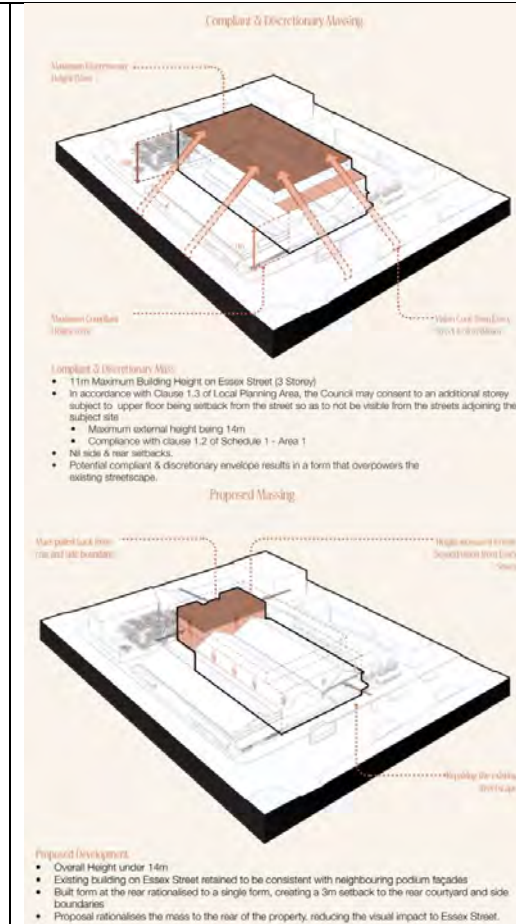
<p>As mentioned, our house is built of limestone, without the use of modern building techniques and materials. It has no footings to speak of and any development within close proxemics of it would surely have a detrimental impact upon the building. I've been involved in construction for the past 17 years and am very aware of the impact of new construction has on adjacent properties, let alone a frail, 162 years old, heritage home.</p> <p>Privacy</p> <p>We will also encounter a loss of privacy should the redevelopment proposal proceed. Four stories high, 3.0m from our rear boundary, looking directly in to our back yard!!! This would also apply to the residences at the Old Flour Mill in Essex Street and also the residence at 24 Norfolk Street. Our main bedroom and entertaining area are situated to the rear of the residence and will be facing the proposed development.</p> <p>Noise Pollution</p> <p>The proposed development has a multitude of noisy reverse cycle air-conditioning units both on and around the facility facing adjacent properties including our own. These may be run 24 hours a day, 7 days a week, 365 days a year. I guess a modern building will have sufficient sound proofing qualities to counter this noise pollution but what about the poor shmuck who has shelled out a small fortune for a heritage building that he cannot change in order to deal with said noise pollution.</p> <p>Have any noise impact studies been undertaken regarding multiple air-conditioning units running simultaneously, the noise generated by the possibility of up toward 300 residents in that one small area let alone any increased business activity, the multitude of service vehicles accessing the development via the rear laneway that immediately abuts my property or the rubbish collection truck attending adjacent to my house in the very early morning collecting the multitude of bins required for all the refuse that will undoubtedly be generated.</p> <p>Waste Management</p> <p>With towards 300 people both residing and working at the proposed development I would imagine a fair amount of waste would be generated. Has any thought gone in to how waste will be dealt with?</p> <p>Current occupants of the Essex Street property subject of this development place their waste bins in the rear laneway that exits on to Norfolk Lane, directly behind our house and immediately adjacent to our garage. There are two adjacent laneways, one servicing 22 through 28 Norfolk Street and the other servicing the Essex Street buildings. Currently the Essex Street tenants and Sala Thai Restaurant (22 Norfolk Street) place their bins in the rear lane ways, invariably blocking or restricting access.</p>	<p>Parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p> <p>Landscaping</p> <p>Given the proposed development is constrained as it is predominately within an existing building envelope on a</p>
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	<p>With somewhere approaching 300 individuals staying or working at the proposed development, not to mention the waste generated by the businesses at that location there will be considerably more waste to deal with.</p> <p>As it is, there has already been numerous clashes between tenants and residents concerning the placement of bins before and after collection in particular from tenants at Salvation Army (21 Essex Street). The bins are regularly targeted by, presumably homeless people, looking for food, clothing, bedding or anything else that may be of value to them. The bins have also recently been the target of an arson attack which very nearly saw the Salvation Army building (21 Essex Street) consumed by fire. If not for my wife who just happened to be awake at that early hour, who notified the Fire Brigade, the outcome may have been far worse than some destroyed 'Wheelie' bins and paint damage to the exterior surfaces of 21 Essex Street.</p> <p>Are myself and my family expected to endure the eye-sore and smell of refuse being heaped up at our rear boundary? So much for the historic aesthetic quality of our Heritage Listed Home. Not to mention the sound that will be generated in the early hours of the morning by the Rubbish Collection trucks/personnel having to deal with all the additional waste generated. I don't suppose it'll matter too much for those in the Essex Street properties because they don't live there and the poor buggers living at 26-28 Norfolk Street can deal with the noise, smell and aesthetics of all that refuse.</p> <p>Parking</p> <p>I believe the proposed development has been pitched as low cost 'back-packer' accommodation. Conveniently very few back-packers have cars to my knowledge therefore no parking is required. Is this development restricted to non-car owning back packers? I suspect not, therefore presumably there will be guests at that establishment that do possess a car or other form of transport. Has any consideration been given to where those people may safely park and secure their vehicles?</p> <p>Conclusion</p> <p>I am vehemently opposed to this building proposal. It will be a blight on the landscape, not in keeping with the historic aesthetic of the area, in complete contradiction of previous 'expert' determination, detrimental to our amenity and historic residence and safety and well-being of my family.</p>	<p>City Centre site landscaping provision has been a challenging component to address. However, the proposal has integrated landscaping where possible, this includes indoor planting within common areas, balconies.</p> <p>In addition to the landscaping proposed within the building, the existing parklet has already been enhanced by the landowner who proposes the development.</p>
17.	<p>oppose the development in the strongest terms on three grounds;</p> <p>1. Impact on the heritage values of the precinct and neighbouring properties in both Essex and Norfolk streets;</p>	<p>Heritage</p> <p>The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS</p>

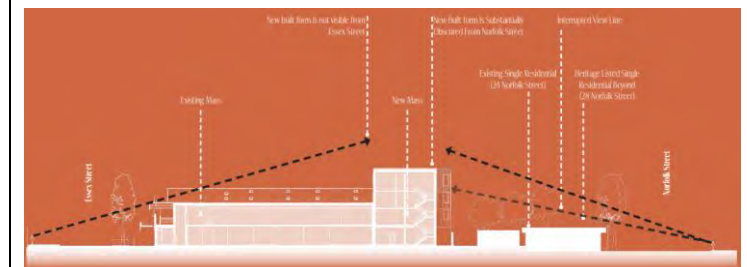
<p>2. The overshadowing and overlooking aspects of the four storey building design to the rear of the property; and</p> <p>3. The impact of noise, traffic and disturbance to existing residences in Essex and Norfolk streets with the dramatic increase in accommodation density.</p> <p>1. The Essex and Norfolk streets have retained much of their character with outstanding heritage buildings such as the Old Port Mill and 28 Norfolk street which are both adjacent to the proposed development. Heritage values are precarious and are dependent on the balance of heritage and newer developments not swinging excessively to the latter. Several newer developments have been sympathetic to the surrounding heritage buildings, with 17 Essex street being a perfect example. The proposed development at 19 will further erode the heritage value of the precinct particularly the 4 storey portion of the building which will be visible from various vantage points in the precinct but particularly from Norfolk street and will be at odds with existing buildings at the South terrace end of Norfolk street.</p> <p>2. The four storey portion of the proposed development will overshadow and windows will overlook my property including two existing bedrooms and outdoor area. The same will be true for 28 Norfolk Street. Ideally, the development should be limited to 2 storey given the heritage and overlooking, overshadowing caused by the 4 storey proposal. At a minimum the proposed windows should be removed from the proposal to the extent they overlook residential properties. Further, the submission for the development of 19 Essex Street describes 15-17 Essex Street as a commercial property. This is blatantly untrue. The vast majority of this property are residences and many of these residences, particularly those in the heritage old port mill building will be negatively impacted by the proposed development design. Sunlight will be substantially reduced in winter as the proposed 4 storey component lies to our North East depending on residence. Also, the visual amenity will be impacted by a building design not sympathetic with the heritage values of the precinct.</p> <p>3. The proposed development will greatly impact noise, traffic and generally activity in the precinct. A precinct which already absorbs the impact of the Esplanade hotel and Pirate back packers within Essex Street. At issue here is the extreme density of the proposed development. I do not believe that such an increase of the accommodation density can be absorbed with the destruction of amenity for existing residents of Essex and Norfolk streets. At a minimum the proposed accommodation density should be halved, consistent with reducing the height of the 4 storey portion of the proposed development. Of particular concern to me is the location of rubbish storage. The proposed location is adjacent to and will undoubtedly create excessive noise, odour and even potential health impacts, particularly given the 24 hours, 7 days a week nature of the proposed development. This is</p>	<p>Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.</p> <p>The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and were fully supportive of the proposal. The following response was provided by the HCWA:</p> <ul style="list-style-type: none"> - A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant. - The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street. - The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe. <p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <p>6 A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to</p>
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	<p>a major concern given the residences in the adjoining properties. And again I emphasise, the proposed development submission fails to identify 15-17 as a residential property.</p>	<p>be prepared by a suitably qualified professional prior to any works being undertaken.</p> <p>7 A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer</p> <p>To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.</p> <p>Overshadowing</p> <p>The proposed development will cause significantly less overshadowing than allowed within the compliant envelope. Refer to DA04.01 for detailed analysis.</p> <p>It is important to note that the actual allowable envelope for the development extends to the boundary; the current proposal includes a setback as a concession to minimise impacts.</p> <p>Noise</p> <p>In response to concerns raised about noise the project team engaged the services of a suitably qualified Acoustic Engineer to prepare an Acoustic Report. The report confirmed that whilst the land use may produce</p>
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		<p>noise levels this is likely to be limited to the mechanical services such as the air conditioning plant.</p> <p>It was noted that given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.</p> <p>A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.</p> <p>Building Height The development adheres to the allowable height envelope, with a maximum height of 14m (4 storeys) in accordance with Clause 1.3 of the LPS.</p>
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The 4-storey section is located at the rear of the property, away from the adjoining Essex Street, minimizing its impact.



		<p>The design ensures that the development remains concealed from natural sightlines on Essex and Norfolk Streets, with visibility from Norfolk Lane mitigated by the existing structures at 21 Essex Street and the neighbouring tree canopies.</p>
18.	<p>History and State and National Heritage listing and Protection I am the owner one of the residential apartments within the heritage listed building which has been inhabited as a residential site since 1862. It has both local, state and national heritage listing and recognition (1A) of exceptional cultural heritage significance. (Refer Heritage of Western Australia Act 1990, State Heritage Council and Fremantle Heritage). Note all development applications must be referred to the Heritage Council for approval. I invite a heritage council member to attend the site to fully view the impact this proposal would have if it was to be progressed further. It is considered the second oldest building in Fremantle's west end after the Roundhouse. Originally it was a flour mill grinding the grain to provide bread for the community. The miller lived onsite as we (the owners) now do and have stewardship over this historic monument and we take this responsibility very seriously. My home (along with other owners) is in the 1862 building which will be extremely adversely impacted should this development proposal progress in any way. As a result I completely oppose this development proposal. Please note damage was previously done to 15/17 Essex St in 2013 when 19 Essex St site was "modernised". This damage consisted of part demolition of heritage protected limestone foundation boundary walls and internal structural damage to several homes. Of significant note the development proposal does not make mention of the heritage status of 15/17 Essex St which is in itself a significant omission of understanding and respect for the permanent 1A status granted at all levels of heritage protection.</p> <p>Land Use In relation to land use the current 19 Essex St building does not have any accommodation, rather it is mixed use office space with a 9:00am - to 5:00pm operational profile. The current development proposal could not be further from this in relation to change of use, that is 247 pod (hostel) accommodation operating 24/7 with no capacity to be managed (governance) by the 2 staff proposed given the scale, density and proposed use. The proposal misrepresents 15/17 Essex St completely, asserting that it is an accommodation facility. Note, page 16 of the proposal shows an image of 13 Essex St which is a modern building, however it has been labelled as 15/17 Essex St; they look nothing alike. Most of</p>	<p>Heritage The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.</p> <p>The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and were fully supportive of the proposal. The following response was provided by the HCWA:</p> <ul style="list-style-type: none"> - A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant. - The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street.

<p>15/17 Essex St is in fact a residential dwelling of 12 owners with a small commercial footprint of a B&B (3 rooms) which is strictly governed and no short stay approved, a small wine bar and an art gallery, the latter two being street frontage and with strict operating compliance. Note nothing at 15/17 Essex St is 24/7 or scale comparative to this proposal, therefore the change of use context could not be more obvious or mal-aligned to the developer's description of 15/17 Essex St. In addition they are requesting discretionary approval to build beyond the 11 metre maximum height limit to 14 metres. The impact on light and privacy would pose a serious breach of existing quality of life for owners. Further, the noise and associated activities of this proposed change of use pose an even greater impact on all owners of 15/17 Essex St. We are already severely impacted by a range of "tourist"/entertainment venues with late night operation in the close vicinity (metres) of our homes, disturbance at all hours of the night are frequent and largely go ignored or unmanaged by the proprietors of these businesses even when reported under environmental health criteria. The last thing we want is a 24/7 "hostel" metres from our home, noting that we are the only residential component (home owners) of Essex St and are often not accurately considered when it comes to planning and "development". It is at the stage currently that the environmental health context (right to a peaceful environment, noise pollution control, disturbed sleep etc) has reached mental health impact. Note 15 Essex St has been an inhabited site since 1862, long before any tourist/ entertainment venues existed in the current density. It is very much a wonderful environment to have a range of venues for residents and visitors alike to visit but this current 24/7 proposal is completely out of context so close to peoples homes. I ask City of Fremantle and DAP to consider would this development proposal be considered appropriate within a few metres of your homes. It will impact on my quality of life and as such I totally oppose this on my grounds cited.</p> <p>On site car parking</p> <p>The development proposal states there is 3 parking bays, the minimum compliance is 20. It seems very clear that the developer's are either not willing to comply or hope that this will go unnoticed. Essex St is already an extremely congested traffic area, becoming more so as new entertainment venues are approved (consider how many have been approved in the last 12months). I invite the City of Fremantle to present a street venue "map" within a 200 sq metre of 15/17 Essex St to gain a visual of all the tourist and entertainment venues in very close proximity to my home. This will provide a visual of the density to which we already live. It is highly unlikely patrons of this proposed short stay hostel will be buying a smart-rider to access buses and as is well know the CAT bus was ceased by council last</p>	<ul style="list-style-type: none"> - The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe. <p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <ol style="list-style-type: none"> 8 A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken. 9 A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer <p>To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.</p> <p>Car Parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p>
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	<p>September despite public requests to maintain this important service to tourists and locals alike. The use of Uber etc adds to the already congested street especially evenings and weekends; just try walking down Essex St on a Friday night to observe the whole parking debacle with very large vehicles overhanging the allocated bays juxtaposed by Uber etc pulling in anywhere posing a risk to pedestrians. The likely patrons of this proposal are likely to have a vehicle of some kind as they are short stay/travelling around the state. As such I completely oppose the 3 car bay proposed by this development.</p> <p>Risk to the Heritage listed building fabric and structure</p> <p>The development proposal plans show that the 4 storey component of the building will be built on a number of pillars to support the overhanging structure. The construction methods required to achieve this will require significant drilling and compacting processes to secure such pillars. This will most definitely pose a risk to the fragile fabric and foundations of 15/17 Essex St, noting these foundations are built on sand, limestone rubble and pieces of random timber as was the building method of the 1860's. I note this is not even a consideration in the development proposal which is a serious omission. A full structural engineering assessment would need to be conducted to correctly inform the City of Fremantle and DAP prior to any decision being considered. As previously stated 19 Essex St works in 2013 demolished (without permission or assessment) parts of the adjoining heritage limestone wall which has structural relevance to the underground of 17 Essex St. Thank you for taking the time to view my submission. Please contact me if further information or clarification is required. I note the meeting for this at council has been set for 11 September. I would like to attend this meeting, unfortunately I will be away from Fremantle in remote WA due to work commitments. Are there any video link platforms available to attend this meeting remotely - Zoom, Teams etc?</p>	<ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p>
19.	<p>We are writing to express our concerns regarding the proposed development at 19 Essex Street, Fremantle (Proposed Development) under application by Kvarken Pty Ltd (Applicant).</p> <p>Backpacker Accommodation</p> <p>Our primary concern relates to the type of accommodation that is being proposed. The Applicant describes the accommodation as a "Hotel" and as an "'intense' development in terms of the high density of visitors accommodated", being approximately 250 persons. The Applicant also states that the Proposed Development is targeted at young persons, who are budget conscious.</p>	<p>Land Use</p> <p>The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development proposal is entirely consistent with the planning aims for this zone and is highly compatible with the surrounding context of the local area.</p> <p>Whilst we can understand the concerns of all residential land uses within the immediate context every effort has been imposed to reduce impacts,</p>

<p>The plans indicate that the Proposed Development has dormitory type accommodation, the vast majority of which are single beds, bunk style.</p> <p>Given the above, no matter how else it may be labelled (pod hotel, LyLo Fremantle), this is backpacker accommodation, and we urge the Council to consider it as such.</p> <p>While we do not object to backpacker accommodation in Fremantle, we do object to this type of accommodation, at this location.</p> <p>The proposed backpacker accommodation is directly next door to 12 residential homes (heritage listed) at 15-17 Essex Street. We are particularly concerned that the Application has failed to take into account or even acknowledge these residential homes – the place where people live.</p> <p>It is our understanding that in order to bring people into Fremantle and to enable sustainable businesses, the Council is desirous of an increase in permanent residents. Not only does this proposal not meet that desire but, further, it actively discourages it. Such a development would significantly impact on the amenity of the 12 residential homes and the broader West End community. 15-17 Essex Street is a very special and an integral part of Fremantle’s character and uniqueness. These buildings are a treasure. It is most discouraging to see this type of development being proposed for such a location.</p> <p>Budget Drinks in the West End</p> <p>Another concern relates to the type of cliental the Small Bar is targeting, as stated in the Applicant’s proposal.</p> <p>While we enjoy the Small Bar scene, we are not supportive of a venue that caters to serve alcohol to a target audience of young persons, who are budget conscious (as stated in the Applicant’s proposal). The service of cheaper alcohol will only increase the anti-social behaviour we are unfortunately experiencing in Fremantle.</p> <p>While we understand the need and desirability of diversity, we believe the hotels along the “cappuccino strip” already provide sufficiently for this demographic as well as others.</p> <p>The West End provides venues such as Darling Darling, Republic of Fremantle, Niew Ruin, Whisper Wine Bar and Strange Company. These venues have contributed toward the emergence of a desirable, sophisticated area in the West End.</p> <p>These venues attract people to our streets and Fremantle.</p> <p>The West End is special. It is unique and highly desirable. The Council has reason to be very proud of what is has preserved.</p> <p>We do not believe the West End is able to maintain its desirability with a venue that is aimed at serving cheaper alcohol to young people. We believe it would be damaging to the reputation of what has finally emerged in the West End and detract visitors away from</p>	<p>including setting back the upper storeys, providing screening to reduce overlooking, implementing an acoustic report to ensure noise protection.</p> <p>In addition to the above, a development was approved for a change of use in 2023 to include Public Amusement. Additionally, the pre-existing Development Application (DA) from 2020 specified a four-storey tourist accommodation building with additions and alterations to an existing structure. This project is in line with the previously approved uses and aligns with the area's ongoing development strategy.</p> <p>Service of Alcohol</p> <p>The service of alcohol is not a planning related matter. However, it is noted that the hotel provider will be required to obtain a liquor license as part of the occupancy/building permit stage of the approval process. The liquor license will only be granted if it is considered consistent with the Liquor Control Act 1988 (the Act).</p> <p>Noise</p> <p>In response to concerns raised about noise the project team engaged the services of a suitably qualified Acoustic Engineer to prepare an Acoustic Report. The report confirmed that whilst the land use may produce noise levels this is likely to be limited to the mechanical services such as the air conditioning plant.</p> <p>It was noted that given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.</p>
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<p>existing businesses.</p> <p>Further, the proposal does not provide sufficient information for us to make any statements regarding the impact to amenity with respect to the proposed small bar. No operating hours are provided and therefore we are unable to comment on any noise concerns. We are also unable to ascertain how the Applicant intends to enforce its small bar licence (120 persons) given the accommodation will provide for approximately 250 people and that the bar will also be open to the general public.</p> <p>We respectfully ask that Councillors consider what the Vision is for the West End and how that Vision aligns with that proposed by the Applicant.</p> <p>City of Fremantle Local Planning Scheme No. 4's Scheme Aims</p> <p>We note the Applicant refers to the City of Fremantle Local Planning Scheme No. 4's Scheme Aims, some of which are as follows and we make comment on:</p> <p>1. The Applicant claims to follow the Scheme Aim to ensure development promotes a sense of community and encourages participation in community life.</p> <p>We note that while the Applicant has promoted a sense of community within its Proposed Development, it has failed to promote any sense of community within the broader context. We are not aware of the Applicant making efforts to previously engage in any community liaison. It has failed to recognise significant residential premises immediately next door to the Proposed Development (15-17 Essex St).</p> <p>2. The Applicant claims to follow the Scheme Aim to protect and conserve Fremantle's unique cultural heritage.</p> <p>We do not believe that such a substantial amount of corrugated iron as a form of cladding is in keeping with the uniqueness of the area.</p> <p>In particular, we do not believe that a building should be branded with such a sizable logo. The proposal indicates a logo incorporated into the façade and taking up approximately 25% of the side surface area. We fail to see how permanent marketing labels on such a grand scale is in keeping with the area or would form part of or enhance our special Fremantle cultural heritage.</p> <p>We ask that the Council please consider how the Applicant proposes to protect the heritage buildings at 15-17 Essex Street, some of which do not have footings.</p> <p>3. The Applicant claims to follow the Scheme Aim to ensure all development complements and contributes to the community's desired identity and character for Fremantle.</p> <p>Please see our comments in paragraph 2 above.</p> <p>4. The Applicant claims to follow the Scheme Aim to reduce the demand for, and balance the provision of parking....</p>	<p>A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.</p> <p>Operating hours need to be included</p> <p>Car parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off car hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p>
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	<p>Simply failing to provide any guest parking does not in itself reduce the demand for it. Given the above, we highly object to the assertion of the Applicant that the Development Proposal “responds to the local context and character, and has high levels of amenity”. We also object to the Applicant’s assertion that “a key aspect of the development proposal is that it provides positive outcomes for the local community.</p> <p>We do not believe the Applicant is aware of the sensitivities of the local community nor that it has any regard for them.</p> <p>Our West End Community</p> <p>We would like to state that we love living in the West End with all it has to offer, including the small bars. Venues such as Strange Company, Republic of Fremantle and Darling Darling have created a largely welcoming environment. These venues carry much less risk of anti-social behaviour over a venue aimed at young persons on a budget.</p> <p>We thank the Council for its diligent work in caring for the community’s interests. It has been instrumental in creating the lovely small bar and dining experience we enjoy.</p> <p>We urge the Planning Officers NOT to recommend the approval of this application and we urge the Council NOT to approve it.</p> <p>We are available to discuss any aspect of our submission.</p>	<p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p>
20.	<p>I’d like to submit my concerns regarding the proposed. development application at 19 Essex St, Fremantle - DAP002/24</p> <p>A lack of consultation and acknowledgement of the A1 heritage listed “Landmark” building and community located at 15-17 Essex St in the Old Flour Mill as well as residents in Norfolk St and a severely reduced amenity on these residents.</p> <p>The plans submitted are very detailed, yet they totally ignore the unique blend of residential and commercial that is a unique and quintessential feature of the West End, and in particular surrounding the Old Flour Mill. The plans actually report that the ‘development site is surround by commercial development’- a blatant mistruth! This lack of consideration will see the amenity at the Old Flour Mill severely compromised especially in relation to increased noise from a designated 24/7 backpacker operation that includes a bar, kitchen and recreation facilities. Increased traffic in the area from such a development is not suitably serviced by the proposed building, with a distinct lack of parking and suitable drop off areas. This area is already underserviced for parking so when you add in servicing for 250 beds, plant noise, removing the associated rubbish with such a high-density development, then impact on surrounding areas is going to be compounded.</p> <p>Remove the addition of the 4-story rear building that is not in keeping with the surrounding landscape and Heritage Architecture and that serves as advertising for the developer.</p>	<p>Heritage</p> <p>The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.</p> <p>The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and</p>

<p>Further it adds to the extremely high-density nature of the project that will impact hugely on the amenity of surrounding residents and has the potential to further contribute to destabilising the fragile heritage Flour Mill that does not have footings.</p> <p>The development is described in the DA as a total of 247 beds and is a very high density building that is going to be imposing both from an overshadowing perspective, a massive increase in utilities, rubbish and servicing 247 beds, as well as reducing the amenity of surrounding residents, in particular those abutting the development. I question the validity of the overshadowing drawings and have attached photos that prove that our living rooms and outside terrace will no longer receive morning sun-see images below taken recently. All of the rubbish produced by this development is also going to be stored along the boundary wall at the rear of the Old Flour Mill. The noise generated from servicing these bins, emptying etc is going to severely impact on existing acoustics, increasing noise at all times of day and night, as this project is identified as being a tourist 24/7 development. Of particular concern is the impact building a 4-story building at the rear of the site could have on the footings and heritage structures in the neighbouring Old Flour Mill. Note that unfortunately renovations at 19 Essex St not only removed old stone walls without consult, but they damaged and removed our storm water system that has resulted in severe water issues that we are still paying for and dealing with today- including having to install an extensive pump system in our garage to deal with the water. No one took responsibility for this damage, and the damage is on ongoing.</p> <p>A Hotel or a Backpackers</p> <p>This development is not a hotel, it is a backpacker's style accommodation that crams guests into a tiny space in order validate the profitability of this project- otherwise this project would not be going ahead. While there is certainly a demand for this type of accommodation, is it worthwhile altering the very quintessential nature of Fremantle and in particular the West End to build it, when there are many other sites and empty buildings in Fremantle that would be more suitable locations, without risking the Old Flour Mill and altering the heritage landscape with a modern 4 story building.</p> <p>A question that hasn't been answered in relation to projected noise from this project it was it based on a hotel or a back packers? And if it was based on backpackers, I would argue that this is not your typical backpackers either. There is no development like this in Western Australia so how can they have codes to apply to a Japanese style pod hotel.</p> <p>Negative Impacts for Residents</p> <p>The heritage impact statement on this proposal totally ignores the old flour mill heritage community next door at 15-17 Essex street and their language used is simply untrue-</p>	<p>were fully supportive of the proposal. The following response was provided by the HCWA:</p> <ul style="list-style-type: none"> - A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant. - The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street. - The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe. <p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <p>10 A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken.</p> <p>11 A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer</p> <p>To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects</p>
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<p>“development site is surrounded by commercial development”. There are 12 residential properties right next door in the Old Flour Mill.</p> <p>This development will severely impact neighbouring residents with increased lights from overlooking windows, overshadowing from the new 4 story building, a decrease in privacy and increased noise both from the bar and recreational facilities, guests and from an increase in utilities such as ventilation and air conditioning, serving rooms and rubbish removal.</p> <p>Of particular concern is that any residents adjacent to this development will be significantly impacted by the increased 24/7 activity this ‘backpackers development will bring impose on our local community.</p> <p>The Heritage West End Precinct and the A1 Heritage Listed ‘Landmark’ Old Mill commune needs to be recognised and preserved.</p> <p>It is very sad when a developer can get support for a development like this in an area like the West End Fremantle, to alter the very quintessential nature of the area, and there is a blatant disregard for that delicate mix of residential and commercial that makes the area so popular with tourists. Buildings like the Old Flour Mill is why tourists come to the West End in Fremantle. They don’t come to see modern 4 story buildings that have a logo splayed all over it and impacts on the very integrity of the area. It is interesting that they have designed the building so that the 4 story can’t be seen from the street and that is because it is not in keeping with the local building heights and architectural theme of the area.</p> <p>Questions</p> <p>Is this project in line with space standards, i.e. minimum light and ventilation standards as well as fire escape requirements.</p> <p>Why were residents in Norfolk St not consulted about this development?</p> <p>How is the mix of the targeted clientele going to be managed with lodging house occupants and backpackers?</p> <p>Despite computer generated images showing no overshadowing, this is clearly not the case and I have photos that show this impact. How can this be addressed?</p> <p>What noise reduction strategies will be put in place to deal with a 24/7 backpackers’ development with a bar, kitchen and recreational areas?</p> <p>Given the nature of the targeted clientele- how will this be adequately managed, i.e. dealing a group of very drunk and rowdy guests at 4 am in the morning?</p> <p>How will a 24/7 backpackers deal with drop offs, linen servicing trucks, rubbish collection, etc to minimise impacting on the amenity of adjoining neighbours?</p> <p>Plant noise- where are the air conditioning units located and how much noise are they</p>	<p>and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.</p> <p>Traffic</p> <p>Traffic is not anticipated to be an issue of this land use. As has been raised as part of this submission car parking is provided in moderation onsite due to the anticipation of staff and guests utilising the generous transport alternatives to the car in the area.</p> <p>Such as:</p> <ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off care hire options, which are parked off-site, such as Car Share. <p>Noise</p> <p>In response to concerns raised about noise the project team engaged the services of a suitably qualified Acoustic Engineer to prepare an Acoustic Report. The report confirmed that whilst the land use may produce noise levels this is likely to be limited to the mechanical services such as the air conditioning plant.</p> <p>It was noted that given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to</p>
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<p>going to make running 24/7?</p> <p>How were noise projections calculated for such a high-density project? This is an unprecedented style of building for WA so what acoustic projections were used?</p> <p>Summary</p> <p>I do not support the proposed development at 19 Essex St in its existing form. I would consider the proposal if they modified the 4 story building at the rear to a maximum of 2 stories so that it will ensure that the heritage look and feel of the West End and in particular in Essex St is maintained and preserved.</p> <p>I am saddened that the council appears to support this proposal at the detriment to the very residents that it represents, who live in the area that will be hugely impacted by the sounds, the increased pedestrian and vehicle traffic and noise, overshadowing and the unpredictable nature of a Japan style pod accommodation project that will cram 250 guests into a very small space.</p> <p>The unique nature of the community at the Old Mill needs to be celebrated and it needs to be preserved. In 160 years will people be coming to look at a rectangular clad building with Lylo written all over it, or will they be peering through the metal gates of the Old Flour Mill taking photos of one of Fremantle's iconic heritage buildings, that is very unique and very fragile, because of the lack of footings.</p> <p>The residents of the Old Flour Mill are the custodians of this fragile heritage building we live in. Our building wouldn't be standing if it hadn't been developed in the 1990's. We pay extremely high strata fees to constantly deal with rising damp, water ingress, unstable walls maintenance and upkeep a building built in the 1860's requires. This development puts the Old Flour Mill at risk, in particular the 4-story building at the back with associated excavation etc, and we, the owners need the council and the state government to work with us to help ensure it survives and is still standing in another 160 years.</p> <p>I am not anti-development and I hope that the Council either alters the project or it is knocked back so that it doesn't alter the very distinct and unique nature of the West End and our community in Essex St, which is unique not just to Fremantle, Perth or WA, but in the whole of Australia.</p>	<p>ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.</p> <p>A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.</p> <p>Car Parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive</p>
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		<p>parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p> <p>Waste</p> <p>The proposed land use provides a compliant bin storage area for storage of refuse and recyclables as was demonstrated within the waste management plan prepared by qualified waste management consultants.</p> <p>Land Use</p> <p>The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development proposal is entirely consistent with the planning aims for this zone and is highly compatible with the surrounding context of the local area.</p> <p>Whilst we can understand the concerns of all residential land uses within the immediate context every effort has been imposed to reduce impacts, including setting back the upper storeys, providing screening to reduce overlooking, implementing an acoustic report to ensure noise protection.</p> <p>In addition to the above, a development was approved for a change of use in 2023 to include Public Amusement. Additionally, the pre-existing Development Application (DA) from 2020 specified a four-storey tourist accommodation building with additions and alterations to an existing structure. This project is in line with the previously approved uses and aligns with the area's ongoing development strategy.</p> <p>Branding Supergraphic</p> <p>The prominent Lylo branding has been removed from the proposal.</p>
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21.	<p>lease accept this submission of STRONG objection to the proposed Tourist Development on the immediate adjacent property of 19 Essex Street, Fremantle. Our grounds for STRONG objection are as follows:</p> <p>1. Previous Tourist Development Approval DA 0322/19: We were the applicant for the previous Development Application for a hotel on the subject site in 2019. Our development application was subject to the DAP process and Statutory assessment as we expect this proposal to be subjected to. After a very lengthy process, we obtained development approval for the proposed hotel. To achieve this outcome, our proposal was subject to significant scrutiny, review and substantial design modifications. For the benefit of the assessing planning officer and City of Fremantle, we provide the following summary of our approved proposal:</p> <ul style="list-style-type: none"> • A minimum of thirteen (13) car bays were provided on site, including sufficient reversing area and access for delivery and waste collection vehicles. • Building consisted of eight (8) hotel rooms only. • Building was three (3) stories with a discrete roof terrace for guest access only. Ground level was for vehicle access only. • Maintaining solar access for adjoining properties was achieved. • Significant side setback of 5.3m was provided from the common boundary with 17 Essex Street, to ensure no impact on the heritage value of 17 Essex Street. • Not modifications were proposed to the existing building. Our brief assessment of the proposed development Tourist Development has identified the following significant variations to the previously approved hotel: • On site car parking reduced to four (4) bays only which is significantly lower than the previously approved thirteen (13) bays. • Tourist accommodation to consist of sixty three (63) bedrooms, of which most rooms can accommodate a minimum of 4 beds. This is in vast contrast to the previously approved eight (8) bedrooms. • Proposed rear building is now four (4) stories high with significantly greater building bulk. • Solar access to all adjoining properties is now significantly impacted. • Side setback to 17 Essex Street reduced to just 3.0m, which is the bare minimum setback for fire separation only. • Substantial modification to the existing building. During the assessment process of our DA in 2019 and 2020, it was impressed very heavily upon our proposal by the DRP that the following key design considerations be achieved: 	<p>Car Parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p> <p>Intensity of land use</p> <p>The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development</p>
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<p>a) The proposed building is kept to 3 stories only with a discrete roof top terrace. Keeping the building bulk visibility very low from both Norfolk Street and Norfolk Lane were paramount.</p> <p>b) Solar access for all adjoining lots be retained, especially that of the existing dwellings at 20 Norfolk Street.</p> <p>c) Greening aspects within the development be provided.</p> <p>d) Vehicle accessibility be retained for both the subject site and the adjoining property at 19b which shares the same rear access. Of particular importance was waste vehicle access, delivery and services vehicle access, and the provision of as much on-site car parking as possible.</p> <p>e) Access to natural light and ventilation, as well as noise attenuation for all adjoining neighbours. In what is a relatively short time frame of just four (4) years, we do not consider the proposed development to be evenly remotely consistent with the existing approval, nor consistent with the key intents of the DRP identified for the subject site. With the above in mind, we continue with the following specific objections to the proposed development:</p> <p>2. Car Parking: What we find most troubling with this proposal is the about this proposal is the increased living density on the subject site with next to no car parking being provided at all. For a tourist accommodation of this size, it is incoherent to argue that people using this hotel will not use private vehicles. Whilst it is easy to walk around Freo, visitors to Freo and this tourist accommodation will want to travel further than just Freo and will therefore hire private vehicles. If these are then parked on the street, further traffic congestion and demand for parking will be greatly exacerbated within and around Fremantle. The TIS (Traffic Impact Statement) provided states: "The majority of the hostel guests will be using Uber, taxis, predominantly public transport and to a lesser extent courtesy minibuses, the traffic generation of the proposed development is expected to be very low. As such no adverse impact on the adjacent road network is anticipated." What evidence has the applicant provided to support such an outlandish statement? With up to, and if not in excess of 250 persons staying in the proposed hostel at any one time, how do they justify only 2 bays being provided for the site. Given the vastness of WA and the attractions on offer across the metropolitan area and beyond, how is it not conceivable that more than two (2) guests at any one time will have personal vehicles of some sort. Additionally, where do staff for the hostel park? The proposed development consists of a bar, kitchen area, staff office, and back of house facilities for the staff including a laundry. Yet no accommodation is made for their on-site parking. We are somewhat baffled the TIS does</p>	<p>proposal is entirely consistent with the planning aims for this zone and is highly compatible with the surrounding context of the local area.</p> <p>Whilst we can understand the concerns of all residential land uses within the immediate context every effort has been imposed to reduce impacts, including setting back the upper storeys, providing screening to reduce overlooking, implementing an acoustic report to ensure noise protection.</p> <p>In addition to the above, a development was approved for a change of use in 2023 to include Public Amusement. Additionally, the pre-existing Development Application (DA) from 2020 specified a four-storey tourist accommodation building with additions and alterations to an existing structure. This project is in line with the previously approved uses and aligns with the area's ongoing development strategy.</p> <p>Height</p> <p>The development adheres to the allowable height envelope, with a maximum height of 14m (4 storeys) in accordance with Clause 1.3 of the LPS.</p>
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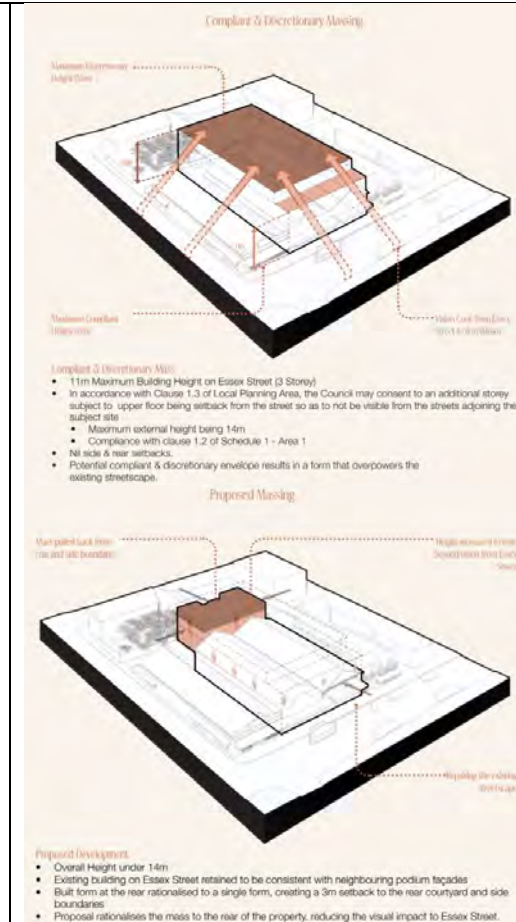
not identify the lack of car parking provision of traffic impact around the subject site as an issue.

3. Service Vehicles and Deliveries: We question the validity of the submitted TIS, particularly in relation to the provision of appropriate access for service vehicles. From our discussions with experienced operators in the tourist accommodation industry, inclusive of bars and restaurants, the provision of 1x car bay for a B99 vehicle is grossly insufficient. The management of food & drink deliveries, as well as the collection and delivery of laundry, waste materials and other deliveries to the site will result in multiple overlap of deliveries and collections. The proposed single loading bay will not be sufficient and will increase the congestion within Norfolk Lane considerably. Our assessment of Norfolk Lane identifies it as a narrow dual access lane with minimal street parking available. If the single loading bay on site is occupied, as are the bays in Norfolk Lane, what alternatives for the overlapping service vehicles that require access to the rear of the subject site are provided or available? We suggest there are no alternatives other than illegal parking within Norfolk Lane directly adding to road congestion and increased risk to pedestrians utilising the narrow path of Norfolk Lane.

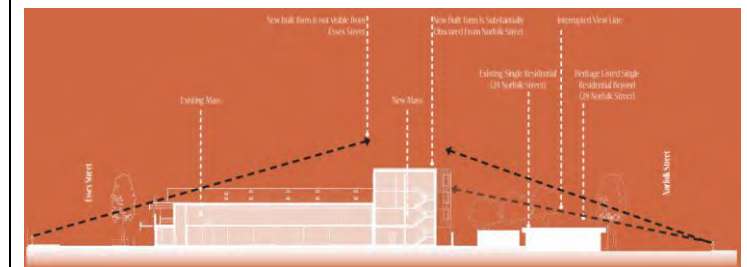
4. Waste Management Plan: Similarly, as stated above, pushing the responsibility of a landowner to provide on site access for service vehicles, the same absolutely applies for Waste Collection vehicles. To suggest that all bins are collected from the footpath of Norfolk Lane is grossly inappropriate and unsatisfactory. Waste should be retained on commercial sites, especially those which provide accommodation and food, as these are often the greatest contributors of waste products which create smell and attract vermin, pests and insects. It is our very real expectation that approval for bin collection from Norfolk Lane will eventually deteriorate into the bins being kept permanently within Norfolk Lane. This will result in significant degradation of the current amenity of Norfolk Lane for residents and pedestrians alike.

5. Building Bulk & Density of Proposal: As identified in item 1 of this objection, the proposed development is vastly inconsistent with the previous approval for the subject site. The previous approval was only granted based on low building impact on the locality and low impact on neighbours. The current proposal has no regard for the development objectives held in high regard by the DRP in 2020. Even if the development objectives have softened or expanded since 2020, the proposed development is still not within any degree of acceptability. The intensity and density of this proposal is excessive.

6. Stormwater Impact on adjoining Lots: During our assessment process in 2020, there was a serious and ongoing drainage issue for the subject site that directly impacted 17



The 4-storey section is located at the rear of the property, away from the adjoining Essex Street, minimizing its impact.



	<p>Essex Street. During periods of high rainfall, the provided on-site stormwater drainage of 19 Essex Street was quickly overwhelmed and overflowed directly into the under-croft carpark of 17 Essex Street resulting in regular flooding. Not only did the stormwater pour into 17 Essex Street from down pipes along the façade of 19 Essex Street, but also poured through the existing walls of the under-croft carpark. To increase the stormwater catchment area on the subject site, where does the applicant propose to send the water?</p> <p>7. Construction Impact: We underground carpark of 17 Essex Street in 2020 and again in 2021, the existing heritage stone wall on the boundary of 19 Essex Street wall was already very weak and in advanced deterioration. This wall WILL NOT sustain any increased load onto the land of 19 Essex Street, and certainly not as close as the proposed hostel intends to be. Any construction on the rear of the subject site is likely to have significant impact on 17 Essex Street from a structural integrity perspective. Given the age and heritage value of 17 Essex Street, any damage that occurs to these buildings during construction will not only be very costly to repair, but in some instance, repairs might not be possible. It is in the City's greatest interest to take construction impacts into serious consideration, as well as the implications acknowledge a heritage report has been provided, however it fails to address the impact any construction on the subject site will have on the heritage building at 17 Essex Street. The impact of this density of construction against a heritage building is very high risk. When we inspected the and responsibility of such impacts.</p> <p>8. Acoustic Impacts: We did not identify any acoustic assessment that had been provided for a 24-hour hostel with excess of 250 beds. Surely this number of budget travellers accessing the area during all hours of the day and night with bar and food facilities on site will generate significant levels of noise for adjoining landowners and should be properly assessed and addressed BEFORE any approvals are issued.</p>	<p>The design ensures that the development remains concealed from natural sightlines on Essex and Norfolk Streets, with visibility from Norfolk Lane mitigated by the existing structures at 21 Essex Street and the neighbouring tree canopies.</p> <p>Overshadowing</p> <p>The proposed development will cause significantly less overshadowing than allowed within the compliant envelope. Refer to DA04.01 for detailed analysis.</p> <p>It is important to note that the actual allowable envelope for the development extends to the boundary; the current proposal includes a setback as a concession to minimise impacts.</p> <p>Side setbacks</p>
22.	<p>I am pleased to provide my views on the above proposal:</p> <p>The Surprise</p> <p>I've studied the DA documents for 19 Essex Street, including the architects plans; the Heritage Impact Statement and the Tourism Report.</p> <p>The developer has done a huge amount of work on this one. I assume this has been done with input from Council and following detailed discussions with the developers.</p> <p>My understanding is that several of the local stakeholders - residents of both Norfolk and Essex Streets, were completely taken by surprise with the plans and the very detailed nature of them, given that there seems to have been no conversations with them on the general nature of the development.</p> <p>There may be commercial privacy reasons for this but it is always regrettable that residents</p>	<p>Building Height</p> <p>The development adheres to the allowable height envelope, with a maximum height of 14m (4 storeys) in accordance with Clause 1.3 of the LPS.</p>

are continually taken by surprise by proposals like this one, which are significant in terms of their potential negative impact on existing ratepayers.

Negotiating a slight reduction in bed numbers

The development is described in the DA as a total of 247 beds. The additional 4th level of accommodation shows 21 beds. So without this extra height floor, they would still create 226 beds. If they are serious about respecting the adjacent building heights and townscape, they should delete this 4th level.

It seems very arguable that their development would surely still be economically feasible without this more visually disruptive additional level. In terms of the total development proposed, 226 beds compared to 247 beds is hardly a deal-breaker. The commercial feasibility of such a proposal should not, and realistically would not rely solely on an additional 21 beds.

The minor reduction in bed numbers would not affect the economic viability of the overall scheme, but it would have a significant effect on the overall preservation of current built forms in this precinct, as well as being an improvement for the current residents in the area.

I believe Fremantle Council needs to undertake this kind of pragmatic negotiation with those who wish to alter the nature of our city.

1. External Colour

2. The tall rear addition is described in the DA as follows: "...responding to the existing colour scheme of the streetscape...

4. This aspect of heritage-precinct infill design is very arguable as a heritage response in this particular case. The new architectural form is described by the developers as being "modest". Despite detailed efforts by the architects to describe how the new openings and walls respond to the existing rhythms of nearby facades, the final result is modest, not noteworthy.

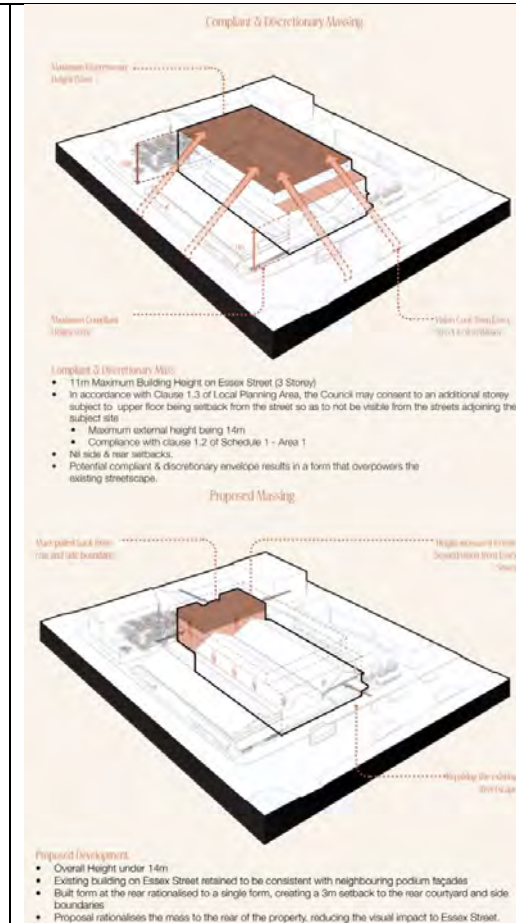
6. I think this is true, but then the plan uses colours and textures designed to attract attention in a kind of "look-at-me" response. This is not what either the "modest" building demands or what the heritage townscape needs.

8. Council should request an understated colour and textural approach to be used. A soft-grey coloured wall treatment would produce a final better overall result for Fremantle.

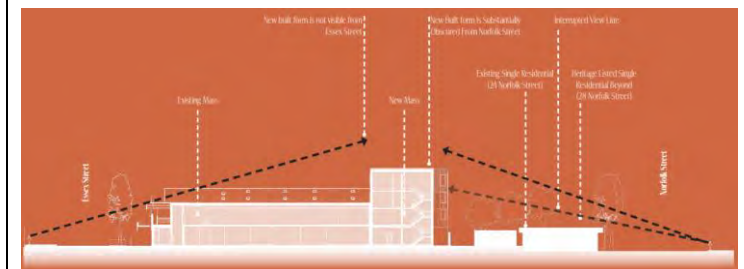
Negative Impacts for Residents

9. The DA Heritage Impact statement seems to ignore No 17 Essex street.

10. The residents adjacent this new development will be significantly impacted by increased 24 hr activity. (The Tourism report in the DA calls the site use 24/7). This impact



The 4-storey section is located at the rear of the property, away from the adjoining Essex Street, minimizing its impact.



<p>will include increased noise which could be substantial, disruptive night-time lights from high-level windows, and increased servicing activity for rubbish and the like.</p> <p>12. Appropriate protection of existing heritage</p> <p>14. Council should also include a development approval condition that directs the developers to ensure that all heritage structures, such as the old limestone wall shared with Norfolk St properties, are well-considered as part of the works.</p> <p>16. It is crucial that these are properly protected, and repaired as part of the work done. The old wall for example should be cleaned, repaired and repointed as appropriate, using a recognised master-mason, and not using modern mortars applied by inexperienced tradies for example.</p> <p>Summary</p> <p>It would be good to see Council dictating and negotiating with developers to achieve the best result for existing residents and ratepayers; and not simply allowing developers to set new agendas regarding all aspects of a developer's preferred outcome.</p>	<p>The design ensures that the development remains concealed from natural sightlines on Essex and Norfolk Streets, with visibility from Norfolk Lane mitigated by the existing structures at 21 Essex Street and the neighbouring tree canopies.</p> <p>Heritage</p> <p>The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.</p> <p>The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and were fully supportive of the proposal. The following response was provided by the HCWA:</p> <ul style="list-style-type: none">- A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant.- The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street.
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		<ul style="list-style-type: none">- The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe. <p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <p>12 A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken.</p> <p>13 A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer</p> <p>To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.</p> <p>Noise</p> <p>In response to concerns raised about noise the project team engaged the services of a suitably qualified Acoustic Engineer to prepare an Acoustic Report. The report confirmed that whilst the land use may produce</p>
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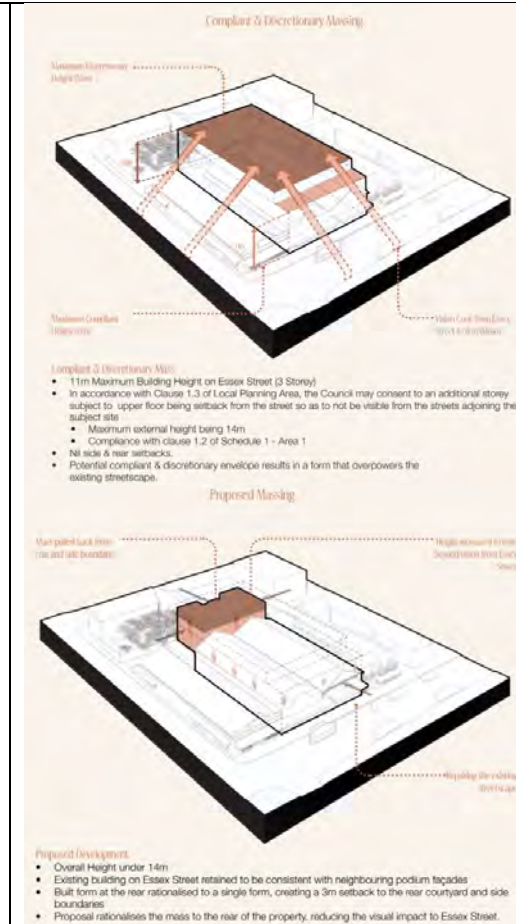
		<p>noise levels this is likely to be limited to the mechanical services such as the air conditioning plant.</p> <p>It was noted that given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.</p> <p>A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.</p> <p>Colour tones</p> <p>The building design and colour tones has lent heavily on the existing character by basing its style off the Fremantle Technical School. The colour palette in particular has focused on the rich copper colouring styles to ensure it fits in within the Essex streetscape.</p>
23.	<p>Thank you for inviting me to respond to the planning proposal Lot 8 Diag 27992 at 19 Essex St. Fremantle. Our residential area includes the Whispers Wine Bar, The Essex Gallery and The Port Mill B&B. Essex St is quiet and charming with several heritage listed buildings constructed early last century. We are the community most likely to suffer negative impact resulting from the proposed development. Our 1860 buildings of fifteen dwellings/businesses is fragile with no footings. The previous 2013 development at 19 Essex St caused considerable damage to our property resulting in falling masonry, water ingress and damage from both their roof and land and also the removal of a stone wall without our permission. It took several years at our great expense and with eventual shire intervention to address these issues. There was no compensation.</p> <p>The reality of 247 beds offering the young a vibrant celebrating venue is a poor fit for this quiet residential centre. There is no dedicated parking space at the front of the building for</p>	<p>Land use</p> <p>The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development proposal is entirely consistent with the planning aims for this zone and is highly compatible with the surrounding context of the local area.</p> <p>Whilst we can understand the concerns of all residential land uses within the immediate context every effort has been imposed to reduce impacts, including setting back the upper storeys, providing screening to reduce overlooking, implementing an acoustic report to ensure noise protection.</p>

	<p>pick up /drop off. No parking space for the services to bar/restaurants/laundry/maintenance or emergency services. No reference to fire or emergency exits/disability compliance. The rear access of 'two way' Norfolk Lane is too narrow to accommodate parked service vehicles.</p> <p>Buses between South Beach and Leighton are no longer free. One needs the right change or a WA smart rider only to board and difficult for short stay interstate or foreign visitors to manage. The number of bicycle park spots offered are not consistent with the 247 proposed visitors.</p> <p>There is no noise management plan to reflect the needs of the residential community.</p> <p>Proposed 247 Room keys used as building access after 10pm. suggests a security nightmare. The proposed building style and height does not fit with this beautiful residential area and heritage dwellings.</p>	<p>In addition to the above, a development was approved for a change of use in 2023 to include Public Amusement. Additionally, the pre-existing Development Application (DA) from 2020 specified a four-storey tourist accommodation building with additions and alterations to an existing structure. This project is in line with the previously approved uses and aligns with the area's ongoing development strategy.</p> <p>Noise</p> <p>In response to concerns raised about noise the project team engaged the services of a suitably qualified Acoustic Engineer to prepare an Acoustic Report. The report confirmed that whilst the land use may produce noise levels this is likely to be limited to the mechanical services such as the air conditioning plant.</p> <p>It was noted that given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.</p> <p>A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.</p> <p>Bicycle bays</p> <p>Submission is noted and in response, the plans have been updated to include 27 bicycle bays within the development, ensuring sufficient provision for guests.</p>
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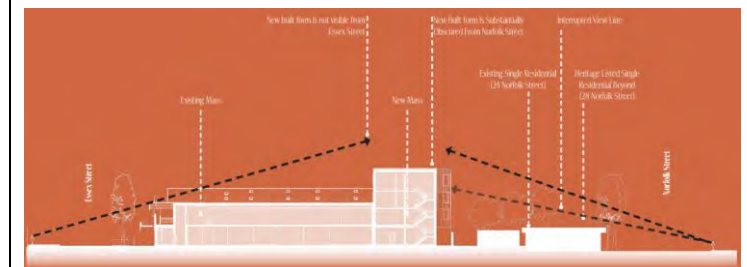
		In addition to providing considerable bicycle parking to guests the 27 bicycle bays will encourage staff to cycle to work as is common practice in the City of Fremantle.
24.	<p>Our home is adjacent to the proposed development of a 4 storey accommodation block at 19 Essex St.</p> <p>We would be directly and majorly adversely impacted should a development of this kind proceed.</p> <p>I have tried to call you this morning & have left a message for you to please recall me.</p> <p>We are extremely concerned about this proposal - and are interested about the CoF involvement to date with the State & National Heritage Councils in relation to this matter - & specifically the potential adverse impact on the unique nature of the Port Mill Complex - which holds the highest levels of recognition & protection on the state & national registers.</p> <p>In addition we are extremely concerned about the adverse impact to our home & the amenity we enjoy at 7/17 Essex Street. - Issues of over-shading & overlooking of our property and our outdoor courtyard, balconies and living spaces, height and bulk of the proposed rear construction on 19 Essex Street, increased noise - from both the massive number of potential backpackers / short stay people at 19 Essex Street & noise of associated infrastructure - noise escalation from a large hospitality venue, Air conditioning, bin & truck movement, vehicular traffic....,</p> <p>In addition there is significant mis-information in the documentation that we have reviewed for DAP 02/24. - Glaring examples are that the Port-Mill Complex is a 'Short-Stay' site.</p> <p>- To be clear with you, this statement is a lie.</p> <p>The Port Mill complex comprises 15 Units. One of the 15 Units is approved as a B&B. The Strata By-Laws specifically exclude any other of the 14 units being used for for any short-stay purpose.</p> <p>We will provide a further personal submission directly through the 'MySay' Portal.</p> <p>In Summary in relation to 19 Essex Street;</p> <p>We object to ANY change whatsoever to:</p> <p>1: Change of 'Land-Use' (We acquired our property fully aware of all 'land-use' designations for ours and all adjacent and csurrounding properties - and expect City of Fremantle to remain fully consistant and compliant to all currently approved land use designations). .</p> <p>2: Any reduction to Onsite Parking requirements - Parking is already at a premium in this congested central Fremantle location - it is ridiculous to consider any reduction.</p> <p>3: Any change WHATSOEVER to Building Height restrictions - specifically noting our</p>	<p>Heritage</p> <p>The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.</p> <p>The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and were fully supportive of the proposal. The following response was provided by the HCWA:</p> <ul style="list-style-type: none"> - A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant. - The development introduces a new visual element to the Norfolk Street and Norfolk Lane streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street. - The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe.

	<p>MAJOR OBJECTION to any 'Discretionary- decision making' by the City of Fremantle (or any other party), to ANY increase to the current building height restrictions.</p>	<p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <ul style="list-style-type: none"> 14 A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken. 15 A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer <p>To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.</p> <p>Overshadowing</p> <p>The proposed development casts significantly less shadow than what is permissible under the compliant building envelope. Additionally, it's important to note that the townhouses' courtyards at 15-17 Essex Street are already overshadowed by existing fence lines, as shown in the study. For further details, please refer to</p>
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		<p>DA04.01, which provides a breakdown of the overshadowing impact.</p> <p>Overlooking</p> <p>The development has been designed to reduce any potential overlooking concerns that may impact adjoining properties. As shown on the elevation drawings screening has been applied where suitable to prevent overlooking.</p> <p>Height</p> <p>Bulk/scale</p> <p>The development adheres to the allowable height envelope, with a maximum height of 14m (4 storeys) in accordance with Clause 1.3 of the LPS.</p>
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The 4-storey section is located at the rear of the property, away from the adjoining Essex Street, minimizing its impact.



		<p>The design ensures that the development remains concealed from natural sightlines on Essex and Norfolk Streets, with visibility from Norfolk Lane mitigated by the existing structures at 21 Essex Street and the neighbouring tree canopies.</p> <p>Noise</p> <p>In response to concerns raised about noise the project team engaged the services of a suitably qualified Acoustic Engineer to prepare an Acoustic Report. The report confirmed that whilst the land use may produce noise levels this is likely to be limited to the mechanical services such as the air conditioning plant.</p> <p>It was noted that given the location of the equipment, compliance at surrounding premises is not considered onerous, with the locating of the equipment behind barriers for visual amenity more than sufficient to ensure that the ambient noise level in the area is not affected by mechanical plant associated with the proposed development.</p> <p>A complete analysis of the noise impact would be undertaken during the design development phase of the project in response to likely development approval conditions for the project.</p> <p>Land use</p> <p>The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development proposal is entirely consistent with the planning aims for this zone and is highly compatible with the surrounding context of the local area.</p>
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		<p>Whilst we can understand the concerns of all residential land uses within the immediate context every effort has been imposed to reduce impacts, including setting back the upper storeys, providing screening to reduce overlooking, implementing an acoustic report to ensure noise protection.</p> <p>In addition to the above, a development was approved for a change of use in 2023 to include Public Amusement. Additionally, the pre-existing Development Application (DA) from 2020 specified a four-storey tourist accommodation building with additions and alterations to an existing structure. This project is in line with the previously approved uses and aligns with the area's ongoing development strategy.</p> <p>Car parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p> <ul style="list-style-type: none">○ Fremantle Train Station (650m)○ Several bus services from South Terrace (60m)○ Taxi Rank South Terrace (75m)○ Ride share options, such as Uber and Didi○ One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and</p>
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		<p>staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p>
25.	<p>My Background I have always lived in the greater Fremantle area. I purchased a piece of Fremantle history and I intend to protect it. I have always known that the Port Flour Mill was an iconic Fremantle site, in a protected Heritage Precinct and that the City of Fremantle would never allow any inappropriate developments to spoil the fabric of the area. When 'growing up' my family and I would be amazed at the Courtyard, its surrounds and that it was, and always will be, a jewel in the crown of the Fremantle Markets Precinct. I have no doubt that locals and visitors alike would not support a totally inappropriate building being built next door to a Heritage gem. The proposed building will severely compromise the structural integrity of the Port Flour Mill and basement car park. It disappoints me that there is a new application to further increase both the density and intensity of the previous Development Approval (DA 0322) that was approved back on 4 February 2020, albeit with many conditions.</p> <p>The Surrounding Properties The Applicant in its Development Application, designates the surrounding properties as follows: The development site is surrounded by commercial development, which includes short stay accommodation, restaurants and offices. Surrounding buildings are predominately one to two storeys, with a number of Heritage and character buildings.</p> <p>My Property The Development Application (DA) consistently refers to the Port Flour Mill as: '2 STORY – SHORT STAY & HOSPITALITY' which is a complete corruption of the truth. There is one unit that presently operates as a B&B but there are 12 residential units that the Applicant never mentions let alone considers in any of its DA. The Port Flour Mill (c.1862) is a permanent entry on the Register of Heritage Places (0871) (Appendix 1). It is also on the City of Fremantle Heritage list. There are 12 residences surrounding an interesting</p>	<p>Heritage The proposed works have been assessed by experienced heritage experts Urbis who have deemed the works consistent with the Australian ICOMOS Burra Charter principles, conserving the most significant fabric of the 19b Essex Street Facade. Importantly, the proposed demolition and alterations are reserved for contemporary non-contributory fabric. New work is clearly identifiable as new but respects the scale form, and materiality of the adjacent context established by heritage fabric. The result is that any proposed modifications will not impact upon the heritage fabric of the building and the additions are setback from the frontage to avoid impacts to adjoining properties.</p> <p>The Heritage Council of Western Australia (HCWA) was consulted as part of the application process and were fully supportive of the proposal. The following response was provided by the HCWA:</p> <ul style="list-style-type: none"> - A portion of the limestone wall (pre dated 1910) on the east boundary of the site, adjoining 26-28 Norfolk Street, is original and significant. - The development introduces a new visual element to the Norfolk Street and Norfolk Lane

<p>courtyard and fountain. It is an icon of the Fremantle area and needs to be considered in any Development Application. (Appendices 2, 3 and 4)</p> <p>According to The Register of Heritage Places: 'The Port Flour Mill has aesthetic significance for the strong contribution it makes to the Essex Street streetscape with its pleasing proportions and a strong, bold facade. The mill has a landmark quality in Essex Street and the interesting internal courtyard space makes a strong contribution to the historic streetscapes of Fremantle'. And in the Statement of Significance: The Port Flour Mill has cultural Heritage significance for the following: a rare example of mid-nineteenth century light industrial development, and representative of pre-goldrush development in Fremantle - the bulk of which was lost due to the rapid and significant changes of the gold boom period (1890 to 1910); and the strong contribution it makes to the streetscape of Essex Street, an important component in the southern edge of Fremantle's historic West End Precinct. Yet, In the conclusion of the Applicant's Heritage Impact Statement it states that: 'The proposed four storey addition will be located at the rear of the site and only have inconsequential incidental visibility from Essex Street as well as in the background of State listed Heritage sites 26-28 Norfolk Street (Place No. 0966) and Fremantle Technical college Annexe (Place No. 01007). The modest modular architectural design and red brown steel materiality responds to the existing colour scheme of the streetscape which includes red brick and clay tiles and affirms the hierarchy of the finer architectural detailing of the streetscape and wider Precinct's significant Heritage places. For the reasons stated above, the proposed works may be supported for approval from a Heritage perspective.' It is incomprehensible to imagine that the proposed four storey addition at the rear of the site would only have inconsequential incidental visibility had the Port Flour Mill been included in any part of the Application. By design or misadventure, the Heritage Consultant or the Applicant did not see Place no. 0871 in the Register of Places. The Port Flour Mill is also one entry prior to the Mills & Co entry on the Fremantle Heritage List which again the Applicant and the Heritage Consultant both deliberately ignored during their detailed planning assessment. It is incredible that the Port Flour Mill could be deliberately ignored on both the State Heritage List and the City of Fremantle Heritage List and therefore it is axiomatic that it was deliberately never considered by the Applicant. (See Appendix 5) The application should be immediately rejected because of this corruption of the truth. After purchasing my Port Flour Mill property, I became very interested in its Heritage, its future maintenance and retention as a key Fremantle Heritage asset. I researched all manner of happenings surrounding the 'Mill' and amongst other things, when the new DA was applied for, 2 I discovered the following:</p>	<p>streetscape which will be visible behind 26-28 Norfolk Street. It is considered that this will have a minimal adverse impact on the cultural heritage values of 26-28 Norfolk Street.</p> <ul style="list-style-type: none"> - The proposal will not affect the cultural heritage significance of Port Flour Mill and Fremantle Technical College Annexe. <p>The HCWA has recommended the following conditions be applied to the development which the developer has agreed to be imposed:</p> <ol style="list-style-type: none"> 16 A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken. 17 A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if any impact occurs and advised on a recommended course of action by a suitably qualified structural engineer <p>To summarise, the development team has taken the time to engage with a heritage expert as part of the preparation of plans to ensure the proposal respects and contributes to existing heritage character. In doing so the HCWA has supported the design. In order to reduce any minimal risk of impact to the neighbours suitable conditions are to be imposed on the development.</p> <p>Overshadowing</p>
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<p>Water Damage</p> <p>That the basement of the Port Flour Mill has been flooded many times because of substandard stormwater drainage from inadequate facilities in 19 Essex Street. It is fair to say, that this unaddressed issue will only get worse if this development is approved. Also, according to the provided Site Plan, the existing Sewer Inspection shafts, and the existing Stormwater Pits and drainage pipes may be buried under the new building. How can any future problems of this nature be investigated, let alone rectified, if the access points to the relevant services are buried?</p> <p>Services</p> <p>The utilities providing all manner of services will be inadequate for the massive increase in living density, staff, bar patrons and associated overload of visitors entering the proposed development.</p> <p>So, will the current</p> <ul style="list-style-type: none"> • Electricity supply be adequate? • Gas supply adequate? • Water supply be adequate? • Stormwater drainage be adequate? • Will the current sewerage infrastructure be adequate <p>And will the anticipated influx of 240 or more guests, extra bar patrons, extra staff and diners stress the current Wi-Fi infrastructure to its limits? Will the current Broadband infrastructure be sufficient?</p> <p>Overshadowing</p> <p>The Overshadowing diagrams provided by the Applicant were inadequate and did not consider the Port Flour Mill residents. The rear residents were also not considered, and they will always have the proposed development in their line of site. I understood that the overshadowing was a key objection in the previous DA. I also believe that the City of Fremantle agreed with the objectors. I know it is a key point of objection for all neighbouring properties.</p> <p>Massing</p> <p>The Massing analysis is inadequate. (See Appendix 6) the Applicant states that if the Development stayed within the rules the: Potential compliant & discretionary envelope results in a form that overpowers the existing streetscape. So, the Applicant seeks discretionary relief by stating that by moving the Big Brown Box to the rear of the property updates proposed massing and: Rationalizes the mass to the rear of the property, reducing the visual impact to Essex Street.' This attempted 'sleight of hand' bears no resemblance</p>	<p>The proposed development casts significantly less shadow than what is permissible under the compliant building envelope. Additionally, it's important to note that the townhouses' courtyards at 15-17 Essex Street are already overshadowed by existing fence lines, as shown in the study. For further details, please refer to DA04.01, which provides a breakdown of the overshadowing impact.</p> <p>Intensity of use</p> <p>The development site is located within the central area of Fremantle city centre - within the City Centre Zone of the Local Planning Scheme. The development proposal is entirely consistent with the planning aims for this zone and is highly compatible with the surrounding context of the local area.</p> <p>Whilst we can understand the concerns of all residential land uses within the immediate context every effort has been imposed to reduce impacts, including setting back the upper storeys, providing screening to reduce overlooking, implementing an acoustic report to ensure noise protection.</p> <p>In addition to the above, a development was approved for a change of use in 2023 to include Public Amusement. Additionally, the pre-existing Development Application (DA) from 2020 specified a four-storey tourist accommodation building with additions and alterations to an existing structure. This project is in line with the previously approved uses and aligns with the area's ongoing development strategy.</p> <p>Car parking</p> <p>Submission is noted. In response, please note that the site is located in close proximity to several alternative transport options in lieu of driving and parking on-site, including the following:</p>
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<p>to reality. If the proposal is approved, there will be a 14-metre monolith at the rear of a Heritage Property in a Fremantle Heritage 3 Precinct overpowering neighbouring Heritage properties. The Applicant fails to prosecute any valid arguments justifying that the height of the proposed building should be increased by 3 metres. The Fremantle Heritage Precinct does not need ANY more detrimental visual impacts on any neighbouring streets let alone one where the mass of has been supposedly ‘rationalized to the rear’. The Applicant has completely ignored the rear neighbours again and the Port Flour Mill residents have not been considered at all. By ‘moving’ the Big Brown Box to the rear of the property does not lessen the overpowering nature of the box.</p> <p>Construction</p> <p>The density of this development will have a deleterious effect on a Heritage building like the Port Flour Mill. A massive increase in load, that is being proposed, will be devastating to the Port Flour Mill structure which is adjacent to and will be connected to 19 Essex St. The Applicant will probably put forward a ‘plan’ to ameliorate the possible construction problems but the risk to the building cannot be underestimated. The Port Flour Mill must be protected at all costs. The only real method to protect the Port Flour Mill buildings is to stop any increased load on the 19 Essex St site. Therefore, the Application should not be approved.</p> <p>Matters of Due Regard</p> <p>During the examination of the provided ‘Matters of Due Regard’ under clause 67 of the LPS Regulations, it was apparent that a ‘Tick the Box’ approach was employed. The following (k through to s) are examples of how some items have been given the ‘tick’ by the Applicant with little or no examination:</p> <p>(k) the built Heritage conservation of any place that is of cultural significance</p> <p>(l) the effect of the proposal on the cultural Heritage significance of the area in which the development is located.</p> <p>Both items k and l rely on the flawed Heritage Impact statement provided by the Applicant.</p> <p>(m) the compatibility of the development with its setting, including —</p> <p>(i) the compatibility of the development with the desired future character of its setting; and</p> <p>(ii) the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development.</p> <p>It is hard to comprehend that the ‘Big Brown Box’ being erected on the rear of the site has no effect on any adjoining land (The Port Flour Mill site). This major addition to the site</p>	<ul style="list-style-type: none"> ○ Fremantle Train Station (650m) ○ Several bus services from South Terrace (60m) ○ Taxi Rank South Terrace (75m) ○ Ride share options, such as Uber and Didi ○ One-off care hire options, which are parked off-site, such as Car Share. <p>It is not considered reasonable to argue that additional accommodation in the city centre will result in more backpackers sleeping in their cars.</p> <p>As discussed in our submitted planning reports the hotel expects a large contingent of international travellers who will be utilising public transport and staying and experience the City of Fremantle. This is the benefit of the short stay accommodation being located within the City Centre.</p> <p>In addition, given the nature of the development as short-stay accommodation, the need for extensive parking is minimized. To address parking concerns, additional bicycle parking and a designated drop-off point for courtesy minibuses on Essex Street are included for visitor convenience.</p> <p>Opening Hours of Bar</p> <p>The opening hours of the bar will be established and enforced as part of the liquor licensing and building/occupancy permit stage of the proposal. It should be noted that the hours are not anticipated to be irregular for the city centre zoning in which the development is located.</p>
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<p>must be built to increase the bed numbers to firstly, justify the initial capital costs and secondly to increase future profit to 'entice' an operator's participation in the project. It therefore stands to reason that the Applicant will 'tick' all the boxes to justify the proposal. This Big Brown Box will be a blot on the Fremantle skyline and no matter which way the developer 'spins it', it will be 'out of place' in the Central Fremantle Heritage Area. 4</p> <p>(n) the amenity of the locality including the following —</p> <ul style="list-style-type: none"> (i) environmental impacts of the development; (ii) the character of the locality; (iii) social impacts of the development; <p>The Applicant proffers that the 'The proposal is compatible with the current and intended amenity of the locality as a vibrant and inviting city centre'.</p> <p>This all-encompassing, fact less, motherhood statement is opinion and aspiration. The overall amenity of the locality will be compromised. The environmental impacts will be seen in risk to the Port Flour Mill building structure as well as water damage, the need for service upgrades and associated future degradation of systems due to increased people traffic in the locality. Overshadowing will change the character of the locality. Also, the sheer volume of the Big Brown Box will make it in the future an unwanted landmark building forever. The massive increase in human and vehicular traffic will change the social impact.</p> <p>(r) the suitability of the land for the development considering the possible risk to human health or safety</p> <p>The Applicant believes that there is no possible risk to human health or safety and therefore has ticked "N/A". A modicum of research into the magnitude of the development would show that just the traffic generated in and around the development would dictate that safety should have been critically assessed. The proposed inadequate car parking area and very narrow right of way is a safety risk with the amount of service vehicles, waste removal vehicles etc required to service a development of this size. The human traffic generated within the buildings and in the outdoor areas will be increased markedly and needs to be considered from a safety perspective. It is incredulous to believe that the Applicant could have 'glossed over' this extremely important question. Did the Applicant want to cover up any safety questions to make the application look easier to approve?</p> <p>(s) the adequacy of —</p> <ul style="list-style-type: none"> (i) the proposed means of access to and egress from the site; and (ii) arrangements for the loading, unloading, maneuvering and parking of vehicles; 	<p>Waste</p> <p>The proposed land use provides a compliant bin storage area for storage of refuse and recyclables as was demonstrated within the waste management plan prepared by qualified waste management consultants.</p>
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<p>The Applicant uses the following ‘Get out of Jail’ clause, ‘The proposed development generally maintains the existing access, egress, loading and waste collection arrangements of the site – designed in consultation with local government technical staff’. This statement by the Applicant deliberately attempts to skew the truth. Under any form of examination, whenever the word “generally” is used, the statement should be tested very stringently. It is worrying that these arrangements have been designed in consultation with local government staff and the tick of approval has been given. Did the Local Government technical staff gloss over the question in the same manner as the Applicant? The proposed development is a massive budget hostel and bar with eating facilities. It will be NOTHING like the current low impact use of the site of two adjacent commercial properties. The current car parking area will be ‘swallowed up’ by new buildings. There will be a marked 5 increase in vehicles movements. It is unbelievable to think that the Applicant can attempt to justify its position. It is very important to ‘see’ what the Applicant has not mentioned in its documentation as well as what it has! Although the proposed Development is categorized as a Tourist Development, it belies the fact that it will be a Budget Hostel crammed into a small footprint in the Essex Street Heritage Precinct. In fact, if the development is approved it will be the largest Budget Hostel in Western Australia, if not Australia. How can this untried monolith be built in the Fremantle Heritage Area, surrounded by Heritage Buildings, with all its unknowns and potential to completely change the Fremantle ‘feel’ and to detrimentally impact the residents of the Port Flour Mill and the integrity of Port Flour Mill buildings as well. The Development is called a variety of descriptions throughout the application such as Lylo Short-term accommodation, Short-Term Accommodation, a Hostel and a Hotel. These differing descriptions emphasize the fact that each report has been produced with a scattergun approach, not understanding how the other ‘expert’ has been advised to continue the thrust or language. Each report has evidently been designed individually and has been produced to overcome obstacles found by the Applicant instead of reviewing all aspects of the development from a holistic approach. If, as was mentioned many times in the Application, the development is a hotel (not a massive Backpackers Hostel) then the mathematics surrounding people, vehicles, waste transfers and the like, used to justify altering Town Planning rules, in all reports is fallacious and the Application should immediately be rejected. As an example, the Applicant states that it would be reasonable (for a variety of spurious reasons and mathematics) that only 4 Car parking spaces would be needed. Would it be contemplated by the City of Fremantle, if another Esplanade Hotel was proposed in the Central Fremantle Heritage area, to allow the Applicant to provide only 4 Car Parking spaces?</p>	
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Car Parking

The Applicant attempts to justify the reduction in Car Parking spaces by trotting out the usual ubiquitous lines as in the following excerpt from the Transport Impact Statement as well as deliberately ignoring the extra number of mandatory car parking spaces required for a Small Bar:

'The overwhelming majority of the future hotel guests are overseas/interstate short term stay backpackers that do not own or require a private vehicle. However, guest opting to hire a car during their stay at the hostel may use one of a number of public pay car parking facilities in close proximity of the site. In addition, ample short-term day (2hrs 9AM-5PM) and unlimited overnight (5PM-9AM) parking is available on Essex Street'.

This statement is lunacy and must be questioned. Residents and visitors alike know and understand that there are limited short term day parking spaces in Essex Street. The Applicant is attempting to justify the need for almost no car parking spaces, by turning an opinion or an aspiration into a fact. Can the Applicant produce any data proving that an overwhelming number of future guests won't use private vehicles and therefore won't require any on-site car parking?

There are very limited spaces in Essex Street from early afternoon to late evening. There is no unlimited overnight parking (5pm to 9am) available on Essex Street. By definition, the Applicant is wrong. There can be no unlimited parking! The Applicant demands that the already stretched public parking bays in Norfolk Lane and Essex St be seconded to this business as dispensation to somehow relax the mandatory parking requirements for a development of this size. The Applicant further requests a permanent reduction to the public parking in Norfolk Lane to facilitate courtesy buses and shuttle buses. The public will be denied further access to very limited parking in Norfolk Lane The Applicant remarkably proposes only 4 on-site car parking spaces but 2 of the 4 spaces are already used by existing office tenancies. The Applicant 'double counts' another parking space by 'providing' an on-site service bay within the 4 car parking spaces. This is definitely 'Magic Pudding' economics. The mathematics are astounding.

The Applicant proposes 4 on-site car spaces but

- Includes Bays 1 and 2 which are currently used by existing Office tenants and
- Includes another on-site car space that will be used as a service vehicle bay A Small Bar is proposed which will markedly increase the number of required car parking spaces. The proposal will not stand up as there is not enough room to provide the required number of car parking spaces. Despite this fact the Applicant has made scant acknowledgement of

the proposed Small Bar let alone any details. Is the Applicant hopeful that ignoring the issue will eventually make the issue 'go away'?

Small Bar

The Applicant states: 'In addition to the Tourist Development land use proposed, the development proposes an additional ancillary 'Small Bar' land use. The Small Bar is essentially part of the Tourist Development, as it will be predominantly for guests of the property. However, similar to 'Hotel' accommodation developments, the small bar would be open to the public – subject to approval of an appropriate liquor licence and relevant conditions on that licence to ensure appropriate operation and management of the bar'. Nothing has been detailed about the facilities, the number of anticipated patrons, opening hours and the like in the application. It is a classic piece of management legerdemain to say very little about the Liquor Licensing aspects of the development in the hope there will be no further scrutiny. Ordinarily an application for a Small Bar would need to address many specific questions but the Applicant sees fit to speak again in generalities and expects acceptance. The Applicant lays out a future that once commenced will be unable to be 'turned around' as it will be predominantly for guests of the property and as the Applicant believes that it is similar to 'Hotel' accommodation developments, the small bar would be open to the public then is it not also fair to also force similar 'Hotel' licensing conditions onto the Applicant. If any one of these 'Hotel' licensing conditions were placed on the development, then the application would be voided 7 immediately. It is also worrying that the Applicant again uses such words as predominately which further clouds the true intent of how the Small Bar will be used and how it will interact with its neighbours. In another section of the documentation the Applicant further clouds the truth by stating: The bar area is designed to be open to members of the public and LyLo in-house guests 7 days a week. We are yet to confirm the operating hours - which can be negotiated with the City as a condition of approval. There are many amenity issues associated with the Small Bar not addressed by the Applicant including opening hours, rowdiness, excess noise and mandatory car parking. The Applicant may contend that as the Liquor Licence cannot be applied for before the approval so therefore it is irrelevant to provide any details. The Applicant should provide at least some transparency and direction. The Applicant must know and understand now how it intends to operate the Bar and should be willing to divulge this relevant information to yet to be affected parties.

- What Liquor Licensing Hours does the Applicant want?
- Will the Applicant want music in the Bar?
- When will it want the music? What will be the noise levels?

- Every day and night?
- Will the Applicant see the Bar as a 'Cash Cow' and at some time in the future advertise the venue to increase patronage?
- What impact would any, or all of this have on the residents of the Port Flour Mill? How can any residents of the Port Flour Mill assess the impact of the proposed bar if the licensing conditions are 'negotiated' with the City 'in camera' and after Objections have closed. It will be too late for the residents to object to the Liquor Licence conditions if the Development is approved! With the Small Bar included in the proposal it is certain that many more car parking spaces should be provided in the development. After my investigations I believe it would be reasonable to suggest that at least another 20 on-site Car parking spaces would be required above what is currently proposed to satisfy Liquor Licensing conditions. It is also incomprehensible to argue that guests using the accommodation, or visitors to Fremantle using the entertainment/dining facilities will never use private vehicles. These vehicles will park on the street and only increase and stretch the already limited parking in this area. This will increase the demand for further street parking and will create more congestion in the surrounding streets. Unbelievably also, there has been no provision made for mandatory Disabled Parking spaces. The proposal must be rejected.

Waste Management

'Waste collection and deliveries will take place off Norfolk Lane. Bins will be wheeled out on designated collection days for presentation along the driveway. The waste collection truck will temporarily stop on Norfolk Lane, unload the bins and continue travel in forward gear. This type of arrangement is currently in place at the subject site for the existing land uses'.

The Applicant states that as this type of arrangement is already in place, there will be no change when the development is built. This statement is patently untrue. How can the Applicant possibly compare the current land use to what is proposed with respect to the existing. The Application should be dismissed immediately due to this gross misrepresentation. There will be 247 guests/backpackers, staff, visitors and bar patrons using the building. The waste generated will be many times greater than is currently the case. The traffic volume (both human and vehicular) will be many times greater than what is currently experienced. A bin storage area is proposed at the southwest corner of the hostel building (ground level), as shown in the development plan in Appendix A. The bin storage area is accessible via service corridors. Again, the above section of the Transport Impact statement needs to be tested. The bin storage area is not directly accessible from

or to the lifts to the ground floor. The internal waste can only be transported from lift 2 via the Bike Store then either the Staff Laundry or around the outside of stairwell 2 to the outside Bin Store. The Kitchen waste can be transported along the service corridor, the outdoor back of house corridor and then to the outside Bin Store. In either case there will be excessive noise generated along the Port Flour Mill walls that cannot be attenuated. Port Flour Mill residents will be severely affected by these frequent waste transfers. There are no details in the proposal with respect to the delivery of goods and services that will be required for '247 guests, 24 hours a day'. Any deliveries to and from the:

- Bar
- Office
- Kitchen
- Freezer
- Cool Room
- Keg Room
- Check in facilities and Cool Room:

Will also need to progress along the service corridor, through the outdoor Back of House area, pass the Back of House Store, the Bin Storage Area, Staff Laundry, Stairwell and through the 1,950mm clear ground zone before entering the parking area proper and eventually accessing Norfolk Lane. There will be a detrimental noise impact on the Port Flour Mill residents. Please see (Appendix 7) On another salient point, the Transport Impact Statement denotes that 'No particular safety issue has been identified for the proposed development'. There is a safety issue surrounding the Clear Zone when a staff member enters or leaves the outdoor Back of House area on the way to facilitate some form of waste or goods transfer. The Clear Zone is only 1,950mm wide and is adjacent to the proposed loading bay. The large Sulo bins will have to be negotiated through the Clear Zone with under 500mm spare on either side. In fact, the journey along the Bin Storage is fraught as there will be under 300mm clear on either side of any 1,100L bin as that passageway is only 1,1820mm wide. 9 The 5 1,100L bins are to reside in the Bin Storage Area but there is no mention where the internal 21 240L and 9 660L bins will reside. It is therefore unclear how the internal waste transfer from these bins to the bins in the storage area will be facilitated and what noise impact that may have on the neighbours The amenity worry is further amplified by this statement in the Waste Management Plan: These internal bins will be collected by the staff/cleaners and transferred to the Bin Storage Area for consolidation into the appropriate bins, as required. This internal servicing method may be conducted outside of main operational hours to mitigate disturbances to staff/visitors.

	<p>During 24 hours per day, the sound of bins full of glass bottles and the like being deposited into other larger bins (in the Bin Storage Area) will never be deadened or forgotten. The continual deadening sounds of the Bin Storage roller doors crashing up and down will never be unheard. This subtle change in wording and or strategy by the Applicant states that This internal servicing method may be conducted outside normal hours 'flagging' the likelihood that the internal waste transfers will be progressed by overnight staff to the detriment of sleeping neighbours in the Port Flour Mill. Again, the residents of the Port Flour Mill are not being considered. If the development is approved the neighbours will never have any noise abatement. The neighbours' complaints will never be addressed. The Service area and the outdoor back of house area will be in reality the outdoor staff smoking area. The noise, chatter and smoke wafting into the courtyards and windows of the Port Flour Mill residents, will be a constant irremediable annoyance 10</p> <p>In summary, The proposed development will increase the traffic, both human and vehicular beyond what was ever contemplated. The density and intensity of the proposal is extremely excessive. The mass of the buildings and the associated stress to all utilities will impact very badly on the Port Flour Mill and the greater Heritage Precinct. The overshadowing will be stark, and the Heritage value of neighbouring properties will be compromised. In all, the developer wants to push the boundaries at the expense of the Heritage Precinct. I urge the City of Fremantle to reject this Development Application.</p>	
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Lylo Fremantle

ESD Opportunities Report

Prepared for: EVT

Project No: SYD2892
Date: 6 June 2024
Revision: 01



Project: Lylo Fremantle
Location: 19 Essex St
Fremantle, WA 6160
Prepared by: ADP Consulting Pty Ltd
Level 6, 33 Erskine Street
Sydney NSW 2000
Project No: SYD2892
Revision: 01
Date: 6 June 2024

Rev	Date	Comment	Author	Signature	Technical Review	Signature	Authorisation & QA	Signature
01	6 June 2024		Jee Wei Tay	JWT	Khyati Saxena	KS	Borris Boschman	BB

Project Team

Client / Principal EVT
Architect Rothelowman
Development Manager Belingbak



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1. Introduction

1.1 Project Overview

This ESD Opportunities Report is intended for distribution to Rothelowman, relevant City Council and relevant members of the project team for the new Lylo Fremantle located at 19 Essex Street, Fremantle WA 6160. The proposed development will consist of the following:

- > Hostel

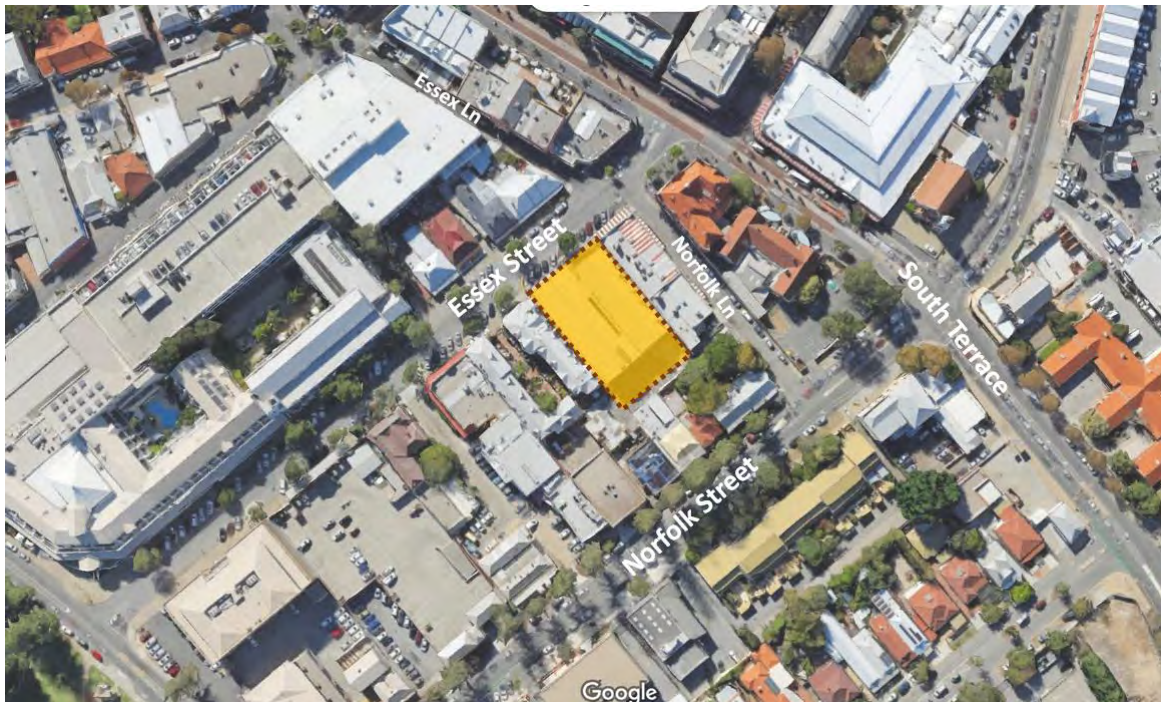


Figure 1 Site Location (19 Essex St, Fremantle WA)

This report details an investigation of Environmentally Sustainable Design (ESD) initiatives that ADP have identified to have appropriate relevance and should be considered for the proposed development.

Following a review of the project technical brief, site location, Council Local Planning Policy, Council Environment initiatives, and architectural drawings, we propose the following areas of sustainability to be explored during the design, construction, and operation of the proposed development:

- > National Construction Code (NCC) 2022
- > Energy & Greenhouse Gas Emissions Reductions
- > Water Efficiency and Conservation
- > Healthy Indoor Environmental Quality (IEQ)
- > Green Star Buildings Rating Tool Sustainable Transport Options
- > Best Practice Waste Management
- > Passive Design principles
- > Low Carbon, circular economy design

1.2 Project Background

The proposed development is located within Climate zone 5 - (mild temperate), as identified by the Australian Building Codes Board (ABCB) and consists of the following NCC Classification Type:

- > Class 3 - Hostels

1.3 Relevant Policies and Guidelines

- > Western Australian Climate Policy
- > Statement of Planning Policy No. 2 – Environment and Natural Resources Policy
- > City of Fremantle Local Planning Policies (LPP)
- > Section J provisions of the NCC 2022 Building (BCA) Code of Australia
- > Green Star Buildings Submission Guideline
- > EVT Sustainability Goals

1.4 Reference Documentation

- > 2024-06-05_223252_Lylo Fremantle – DA Architectural Report_R2.0
- > 2024-06-05_223252_Lylo Fremantle – DA Issue 1_Architectural Drawings Binder

1.5 Sustainability Compliance Targets & Objectives

The primary objective of the development is to be an asset to the local environment and create a positive impact on the surrounding environment and community through its design, in line with the sustainability ambitions of the council.

The following ESD compliance frameworks have been identified for the project:

- > Western Australia Statement of Planning Policy No. 2 – Environment and Natural Resources Policy
- > City of Fremantle Local Planning Policy 2.13 – Sustainable Buildings Design Requirements
- > Section J provisions of the NCC 2022 Building (BCA) Code of Australia

In line with the above governing policies, the proposed development will comply with NCC 2022 Section J Energy efficiency requirements and designed to address the following key sustainability initiatives:

- Whole life carbon emissions reduction
- Energy efficiency prioritising passive design and followed by active design
- Water efficiency and reuse
- Designing for nature
- Active transport
- Circular economy design

2. Sustainable Design Initiatives

2.1 General

This section outlines the project's sustainable design initiatives, in line with the policies and frameworks mentioned above, including additional opportunities that are to be considered during the detailed design stage of the project to support owner's and council's sustainability ambitions and targets.

1. Embodied carbon reduction
2. Energy efficiency and operational carbon reduction
3. Water efficiency and reuse
4. Designing for nature
5. Active transport
6. Circular economy design

2.2 Adaptive Reuse and Embodied Carbon Reduction

The project will largely consist of the repurposing of the current buildings on site. The proposed design will retain/make good the existing heritage façade, as well as the existing structure. A new extension building will occupy at the back of the existing building.

Embodied carbon of a building is the carbon emissions associated with the use of construction materials, transport and activities during construction stage. The embodied carbon at the stage is also referred to as upfront carbon. Embodied carbon also encompasses emissions arising from maintenance, repair, material replacement as well as the activities during the end-of-life stage of the building, which are demolition, waste transport and landfill.

By reusing and retaining the existing building façade and structure, the project will be eliminating majority of the upfront carbon of the building. This translates to approximately 20-30% upfront carbon reduction when compared to building the project from ground up.

For new materials, the project will consider selection of lower carbon alternatives. Some options include:

- > locally manufactured materials
- > steel and aluminium made of recycled content
- > concrete with Supplementary Cementitious Materials (SCM) to replace ordinary Portland Cement
- > bio-based finishes

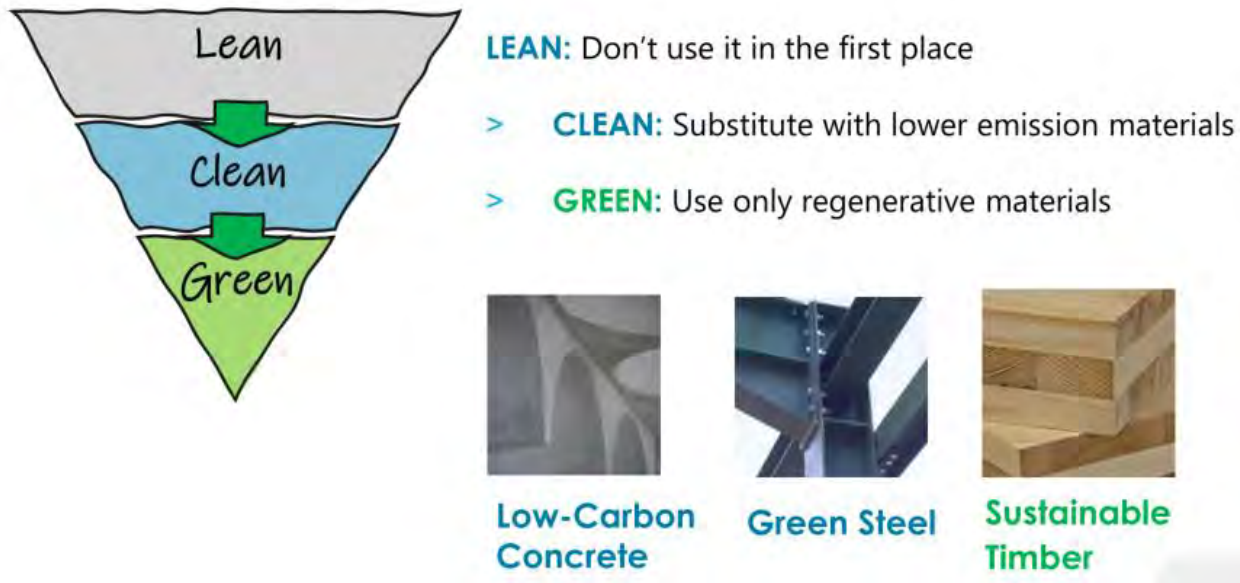


Figure 2 Embodied Carbon Reduction principles

2.3 Energy efficiency and operational carbon reduction

The project team will explore opportunities to reduce the buildings greenhouse gas emissions impact, through the development of energy saving or energy generating design measures. This section discusses design aspects of the building design that are focused on energy efficiency in operation. The building will be designed to be all-electric, eliminating fossil fuel use on site.

2.3.1 Building Fabric Design

The building fabric will be designed to meet Section J NCC 2022 best practice performance standards. The project will explore:

- > Provision of appropriately placed external shading devices to the north, west and east façades providing protection from solar radiation experienced from the low-angle winter sun.
- > Upgrade of existing window systems to double glazing to help reduce heat loss in winter and heat gain in summer, whilst maintaining adequate indoor thermal comfort to the administration building extension.
- > Glazing selections that allow for high levels of Visual Light Transmission (VLT), for useful daylight levels throughout the day
- > Glare mitigation measures through optimising the façade for external shading devices or internal blinds
- > Provision of adequate levels of insulation to the external walls, roof, and exposed floorings that form part of the building thermal envelope
- > Select roof and façade materials with a high solar reflectance to help keep the building cooler on hot sunny days. For example, the project may consider the use of Colorbond Thermatech solar reflectance technology to reflect more of the sun's heat on hot sunny days, which helps keeping the building cooler.
- > Utilise natural ventilation or mechanical assisted cross ventilation potential of the building form
- > Double glazed skylight system allowing daylight, while maintaining thermal comfort in the internal space

2.3.2 Heating, Ventilation, & Air Conditioning (HVAC) Design

- > Allow for operable windows for occupied spaces to reduce the need for mechanical ventilation and air conditioning.
- > Integrate HVAC control to turn off air conditioning in hostel rooms if windows are open for more than 1 minute
- > Select high-efficiency HVAC systems for conditioned spaces and configure for optimum performance including the following features:
 - Demand-controlled ventilation
 - Heat Recovery Ventilation
 - Economy Mode Ventilation
- > Select HVAC equipment that is suitable for low-Global Warming Potential refrigerants (GWP < 10) to minimise the risk of emissions through refrigerant leakage. Where not feasible, project team to explore carbon offsets for emissions related to refrigerant use in the building
- > The ventilation system will be designed to mitigate the entry of outdoor pollutants by ensuring pollution sources and outdoor air intakes are sufficient distanced from each other
- > Access panels are provided for ease of maintenance to ensure equipment and particularly filters continue to supply clean air
- > Ductwork to be cleaned prior to occupation to ensure no dust and other contaminants reduce indoor air quality
- > Direct exhaust to outside will be provided for kitchen and toilet such that pollutants are removed from the building

2.3.3 Lighting Design

- > Reduce the need for artificial lighting by introducing sufficient skylights in all spaces where feasible.
- > Select high-efficiency LED lights to provide adequate lighting levels and colour rendering with minimal energy expenditure.
- > Consider careful design of daylighting controls to adjust electric lighting in response to daylight levels without causing undesirable noticeable switching effects or interactions.
- > Use of motion sensors and light sensors to ensure lighting is automatically dimmed or switched off when not required.
- > Consider Whole-of-Life impacts such as maintenance costs and access for easy maintenance in practice may also be considered

2.3.4 Energy and emissions management

- > Consider energy monitoring to monitor major energy end-use and allow identification of any consumption anomaly during operation
- > Consider ongoing monitoring and reporting of building energy use to inform the emissions reduction targets

2.3.5 Domestic Hot Water

- > The project will explore opportunities to provide hot water via electric heat pump with high efficiency.
- > Pipes to be insulated to reduce heat loss as the hot water is distributed which means less energy is consumed as the water temperature can be lowered

2.3.6 On-site and Off-site renewables

- > With the extensive pitched roof on the existing building, as well as flat roof on the new extension building, there is an opportunity to maximise the installation of Solar PV in the project.
- > The solar PV design and layout will be prioritised to orientate towards the north and avoid overshadowing where possible
- > It is anticipated that the on-site solar PV energy generation will reduce the operational carbon emissions of the hostel operations
- > The project owner can explore GreenPower or Power Purchase Agreement (PPA) with electricity provider to offset the remaining of the operational carbon emissions of the project



2.4 Water efficiency and reuse

The project will explore the opportunities to reduce water consumption and optimise reuse.

- > Ongoing rainwater capture and storage will be explored. The project team will undertake a rainwater capture assessment during the detailed design to confirm roof run off areas are feasible.
- > Where feasible rainwater re-use and harvesting will be considered for non-potable uses (such as toilet flushing, laundry – where appropriate) and landscape irrigation to green areas and landscaping surrounding the development and within the site boundary.
- > To reduce building potable water use, the project considers installing highly efficient WELS rated fittings and fixtures, and appliances.
- > Select low-water and drought resistant native planting for all green spaces on the precinct to minimise or completely omit landscape irrigation

2.5 Designing for Nature

The project will explore following initiatives to minimise its impacts to nature and enhance biodiversity:

- > Understanding the site's historical and current ecological context and using this information to protect and increase the biodiversity on site with native planting integration.
- > Protecting local waterways and reducing the impacts of flooding and droughts by reducing average annual stormwater discharge rates and meeting stormwater pollution reduction targets.
- > Specifically using native plants and landscaping on unused and dedicated green areas on the precinct to provide habitat opportunities for wildlife.
- > Explore vegetated roof with local drought resistant species. The roof can be designed to be lightweight / intensive.



Figure 3 Example of intensive green roof system

2.6 Sustainable Transport

The project will explore the active transport initiatives below to reduce private vehicle usage:

- > Dedicated on site bike storage facility to allow users to utilise bicycle for touring
- > Reduced car parking space on site to encourage public transport use
- > Explore digital display or pamphlets on public transport information in the city
- > Potential onsite bicycle renting to allow customers to explore the city on two wheels



Figure 4 On site bicycle storage facility

2.7 Circular Economy Design

Circular economy design eliminate waste and pollution, circulate products and materials and regenerate nature. Where feasible, the project will adopt circular economy principles in the building design:

- > Avoid internal finishes where appropriate
- > Adopted locally sourced materials to support local economy
- > Prioritise selection of finishes with high recycled content
- > Explore durable and longer lasting materials which will allow easy maintenance/replacement. The internal finishes should support a sustainable branding identity and highlight sustainability as a key part of the customer journey
- > Incorporate Indigenous Design Principles in the design, including biophilic design elements that communicate the local elements of Country. For example internal surfaces, external pavement, landscaping and furniture
- > Kit of part for furniture / joinery items to allow easy replacement

- > Design for disassembly by eliminating chemical connections (binders, glues, adhesives) and using bolted/screwed/nailed connections which are accessible.
- > Develop project specific operational waste management plan:
 - Implementing waste stream separation strategy for waste generated by building occupants in accordance with waste operator requirements
 - Design and allocate adequately sized waste separation and collection areas to ensure maximised recyclability and processing of waste. Outline best practice safety and access requirements for these areas, including identifying safe methods for vehicle access and transfer of waste.
 - Ensure ongoing waste monitoring to provide data for optimisation of the waste management strategy.



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Gold Coast

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Sydney NSW 2000
t. +61 2 8203 5447

London

53-64 Chancery Ln
London WC2A 1QS
United Kingdom
t. +44 74 9478 5117

adpconsulting.com

ESSEX STREET, NO.19 (Lot 8), FREMANTLE – FOUR STOREY TOURIST DEVELOPMENT – (JD DAP002/24)

Site Photos



Figure 1 – View from Essex Street.



Figure 2 – View from Essex Street.



Figure 3 – View from Norfolk Lane along right of way access to the rear of the subject site.



Figure 4 – Rear of the subject site/ the location of the four storey rear addition.

LyLO

Fremantle

Summary – Reasons for Deferral

Whilst the majority of panel members considered the **application had merit**, there was **concern regarding the visual amenity impact of the proposed rear four storey addition on surrounding residences**. Deferral was proposed and supported so that the applicant can **reconsider the height and visual impact of the proposed addition on adjoining single and two storey neighbouring residences to the south and east**. The proposed **waste disposal system may have a detrimental impact on Norfolk Lane** which has limited area for placement of bins for collection. **Review of this element is therefore sought.**

Key Updates

1. Waste Management Strategy

2. Privacy and Neighbouring Amenity

3. Privacy Screens Detailed Study

4. Building Height and Setbacks

5. Built Form and Materiality

6. Car Parking Strategy

7. Response to City of Fremantle LPS No.4 Schedule 7, Clause 1.2

Waste Management Strategy

Waste Management Strategy

NORFOLK LANE

ESSEX STREET

RIGHT OF WAY

ACCESS TO NORFOLK LANE

DASHED LINES OF NEW L1 WALL ABOVE

BLUE DASHED AREA OF HERITAGE FACADE

EXISTING TENANCY 1 ESCAPE ROOM

EXISTING TENANCY 2 THE FLAMING GALAH

LOBBY 7.7 m²

STAIR 1 8.1 m²

LIFT 1 3.6 m²

WC 1 19.7 m²

OFFICE 15.3 m²

LUGGAGE 15.5 m²

COMMUNAL LAUNDRY 15.4 m²

GRAB N GO & GUEST LOCKERS 20.3 m²

CHECK-IN 50.4 m²

BAR 114.8 m²

KITCHEN 41.9 m²

COOL ROOM 10.3 m²

KEG ROOM 9.1 m²

COMMUNAL DINING 78.3 m²

COMMUNAL KITCHEN 49.8 m²

SERVICE CORRIDOR 18.3 m²

LOCKERS 9.5 m²

BIKE STORE 22.1 m²

REAR LOBBY 13.9 m²

LIFT 2 7.6 m²

WC 2 15.2 m²

MAINTENANCE 15.5 m²

STAFF LAUNDRY 26.7 m²

BOH STORE 22.9 m²

OUTDOOR BOH AREA 82.0 m²

BIKES WARE

BINS 24.3 m²

EX. BIKE STORE 14.2 m²

BAY 1 TENANT

BAY 2 TENANT

BAY 3 ACCESSIBLE

BAY 4 LOADING

PARKING 223.7 m²

CLEAR ZONE

STAIR 2 19.4 m²

LOCKERS 9.5 m²

BIKE STORE 22.1 m²

REAR LOBBY 13.9 m²

LIFT 2 7.6 m²

WC 2 15.2 m²

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Increased bin store size to accommodate all tenancies

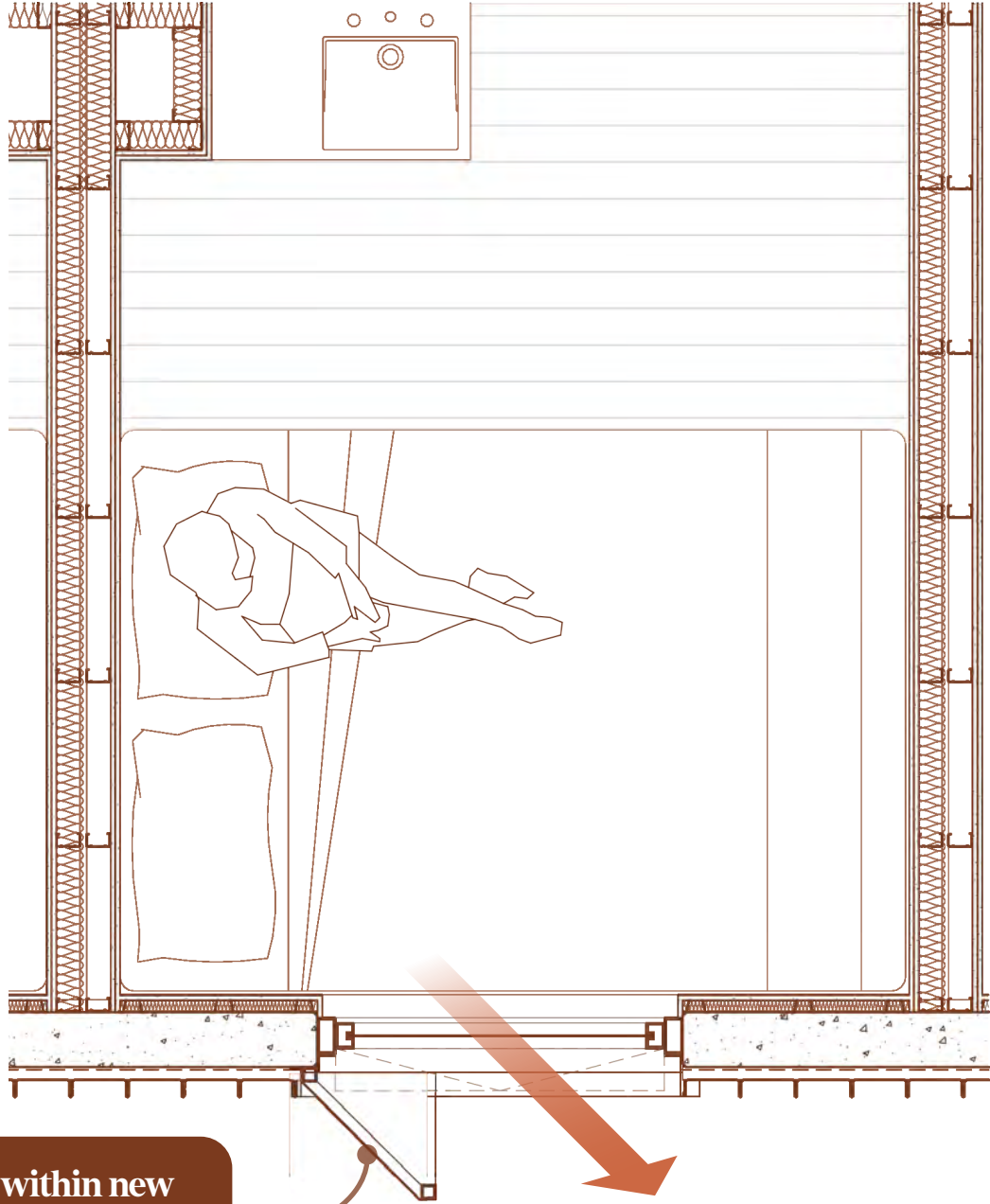
Privacy and Neighbouring Amenity



Privacy Screens Detailed Study



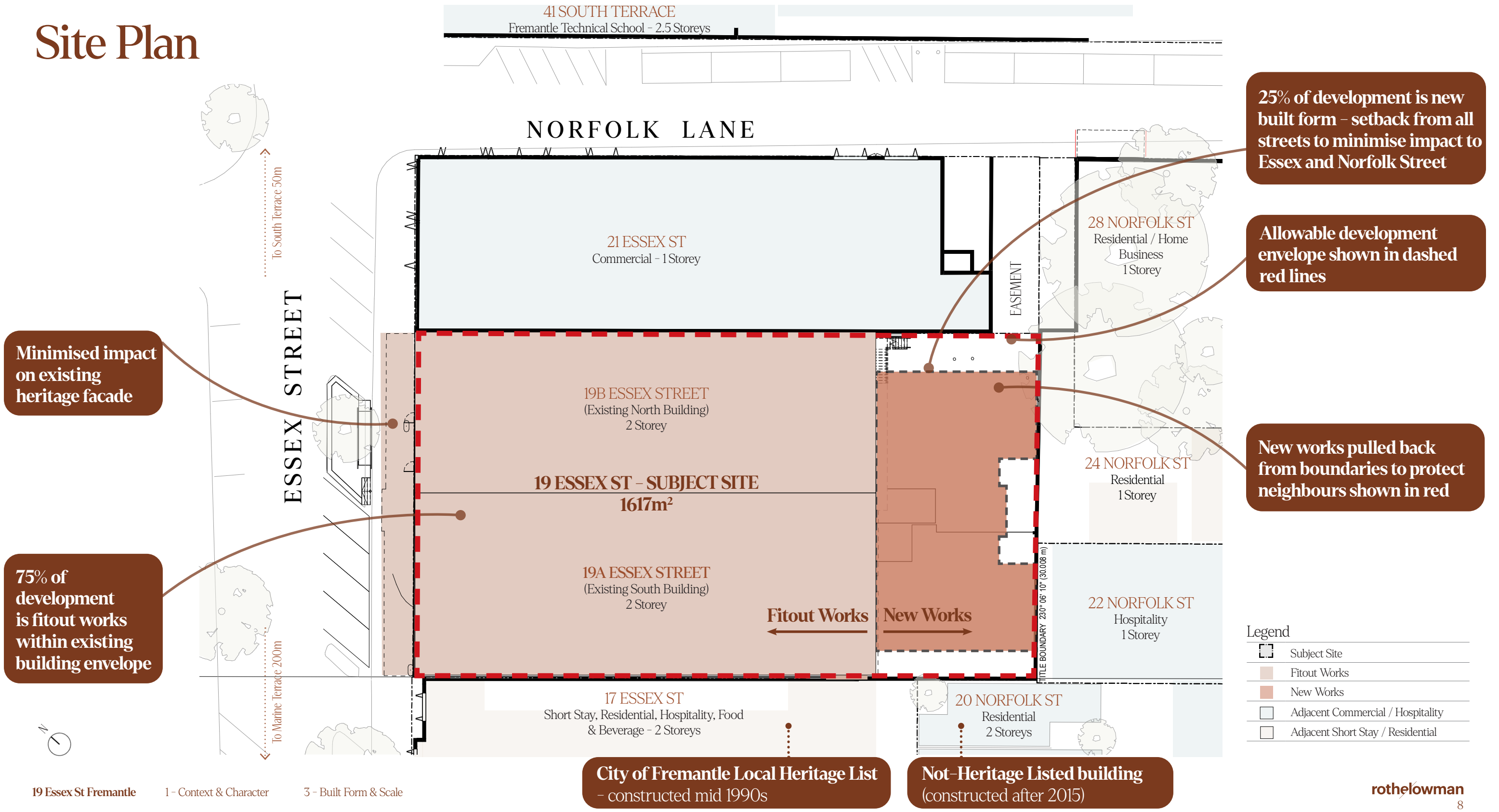
Privacy screens to all windows within new works to ensure privacy and direct views away from neighbours



Privacy and Neighbouring Amenity



Site Plan



Built Form and Scale

"The
DAC supports the
bulk, height and scale
of the new 4 storey
accommodation block."
– Design Review
Report

Compliant and Discretionary Built Form

Maximum
Discretionary Height
(14m)

Maximum
Compliant Height
(11m)

New built form
sits within the
discretionary height
and setbacks

Vision Cone from
Essex Street to 14m
datum through trough
of existing roof

Proposed Built Form

New building
pulled back from
rear and side
boundaries

Height increased to rear,
beyond vision from Essex
Street

New building
pushed to rear of site

Repairing the
existing streetscape

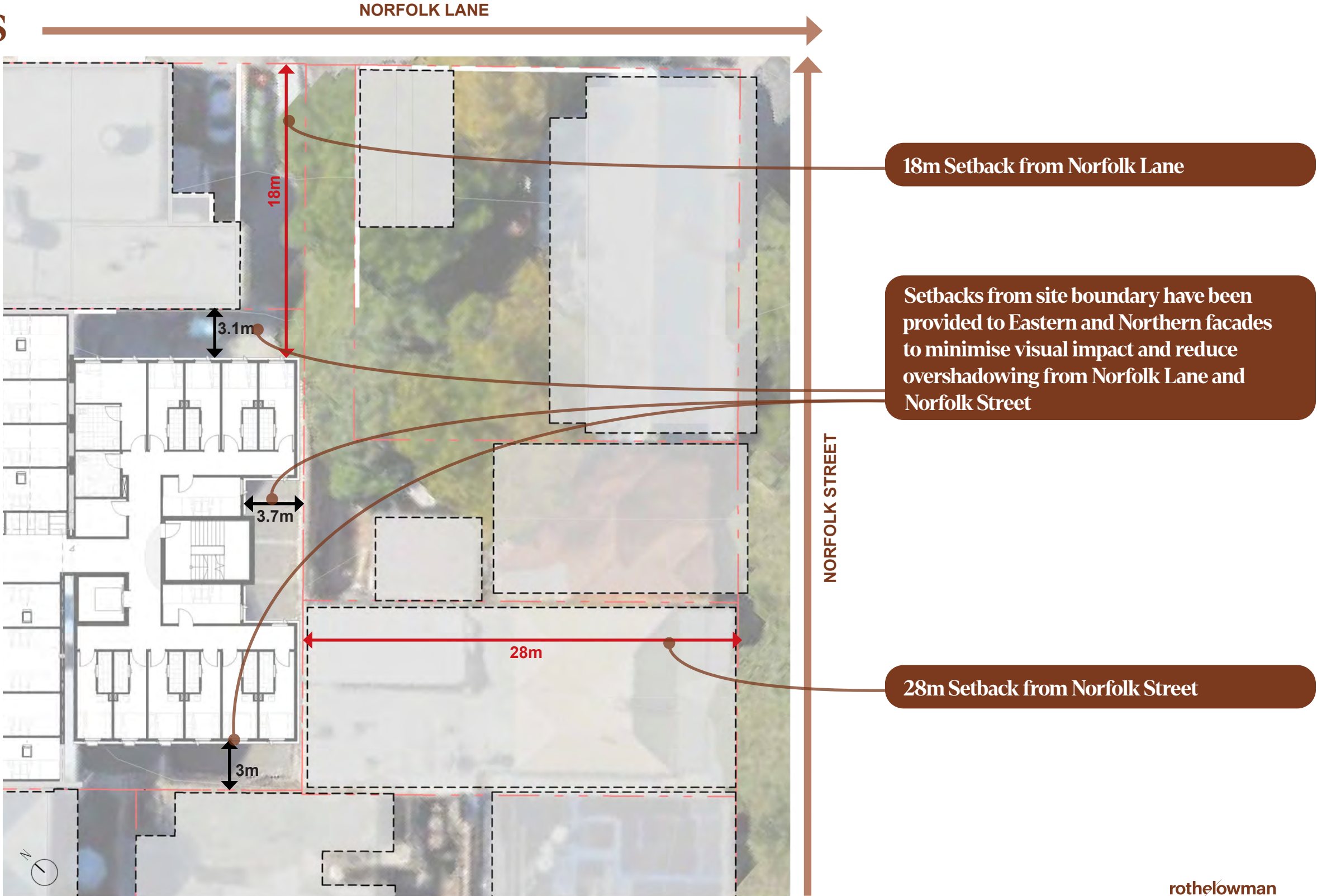
Compliant & Discretionary Mass

- 11m Maximum Building Height on Essex Street (3 Storey)
- In accordance with Clause 1.3 of Local Planning Area, the Council may consent to an additional storey subject to upper floor being setback from the street so as to not be visible from the streets adjoining the subject site
- Maximum external height being 14m
- Compliance with clause 1.2 of Schedule 1 - Area 1
- Nil side & rear setbacks.
- Potential compliant & discretionary envelope results in a form that overpowers the existing streetscape.

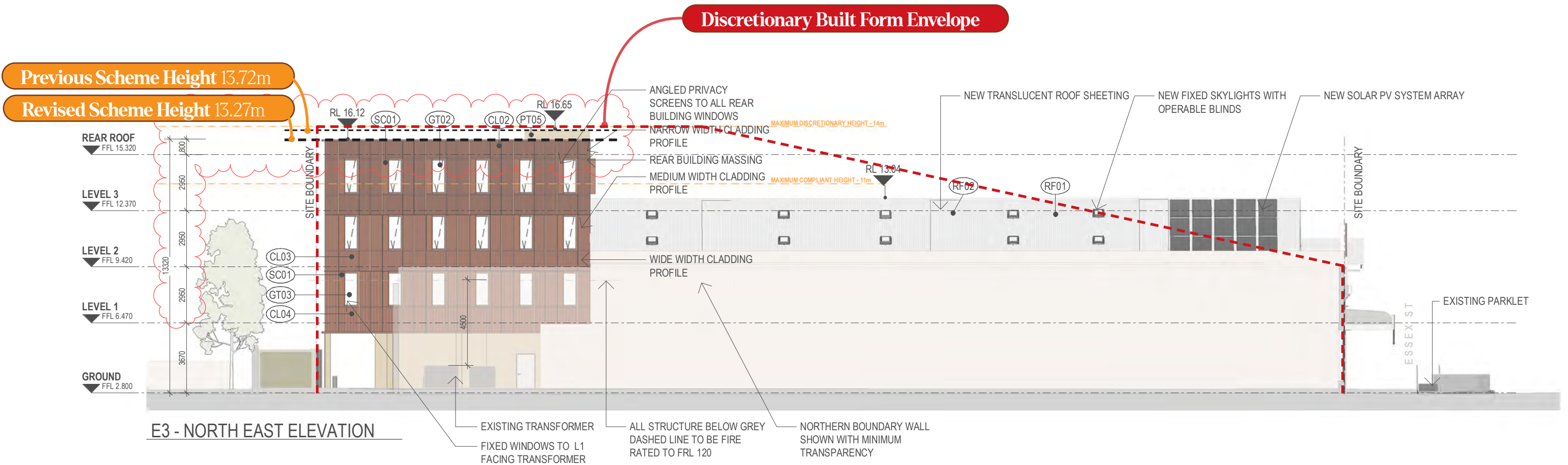
Proposed Development

- Overall Height under 14m
- Existing building on Essex Street retained to be consistent with neighbouring podium façades
- Built form at the rear rationalised to a single form, creating a 3m setback to the rear courtyard and side boundaries
- Proposal rationalises the mass to the rear of the property, reducing the visual impact to Essex Street.

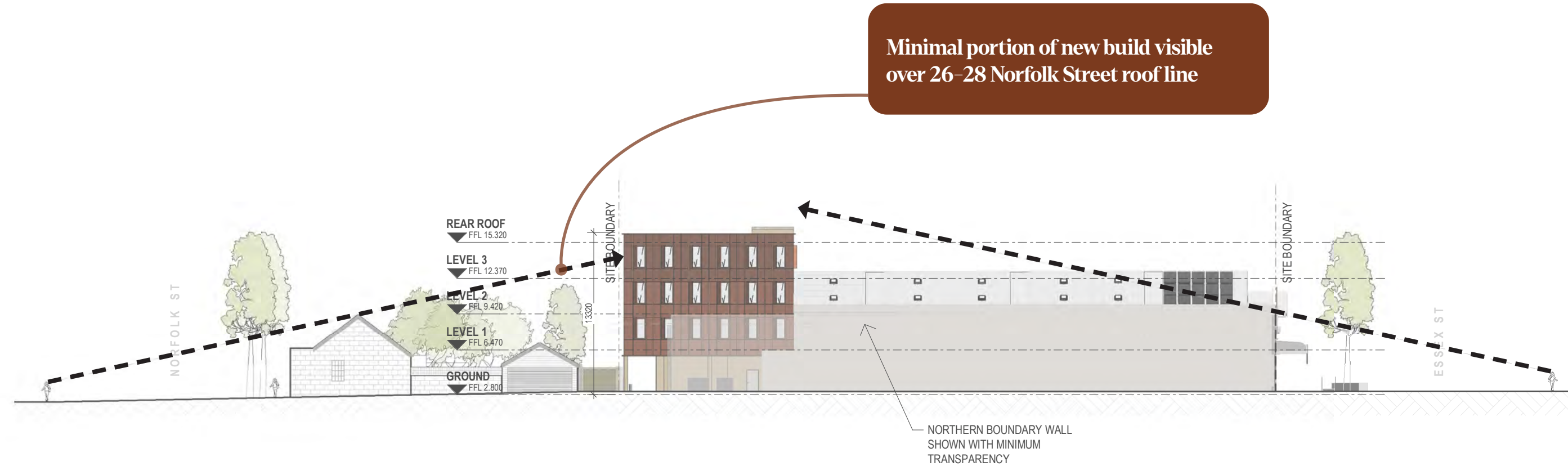
Building Setbacks



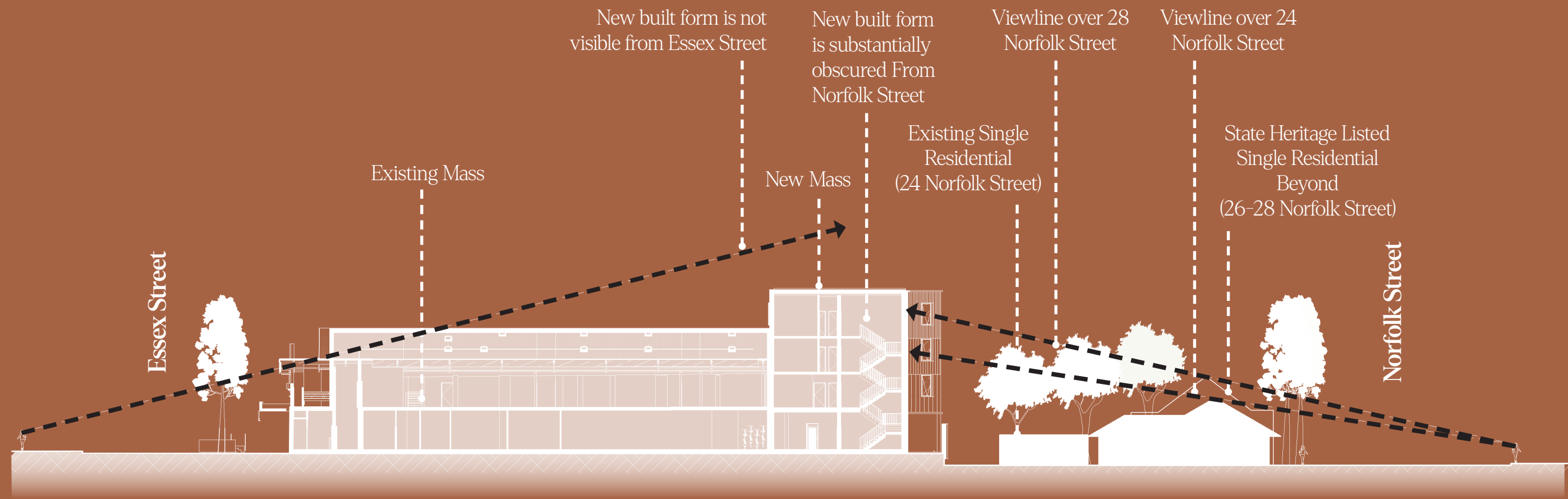
Building Height



Elevation Study – Norfolk Lane



Sectional Impact Study



Built Form – Norfolk Street Impact Study



Previous Scheme



Revised Scheme

Context with Fremantle Technical College

Existing foliage
hidden to reveal
proposed building
(13.27m high with
18.3m setback to
Norfolk Lane)

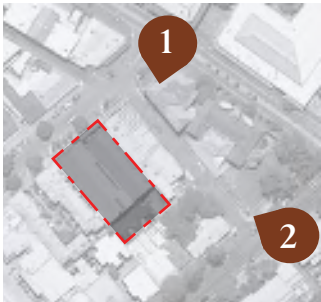
State heritage
listed residential
property – 26–28
Norfolk Street



State Heritage
Listed Fremantle
Technical School
(13m–18m high
with NIL setback to
Norfolk Lane)

Streetscape Massing Impact

"New 4-storey addition is located at the eastern rear of the site with minimal negative visual and amenity impacts on the streetscape and surrounding existing properties." – Design Review Report



1 View from corner of South Terrace and Essex Street



2 View from corner of Norfolk Street and Norfolk Lane

Solar Access

Compliant 3-storey Mass



Compliant Mass Plan View - Winter Solstice 9am

Compliant 3-storey Mass



Compliant Mass Plan View - Winter Solstice 12pm

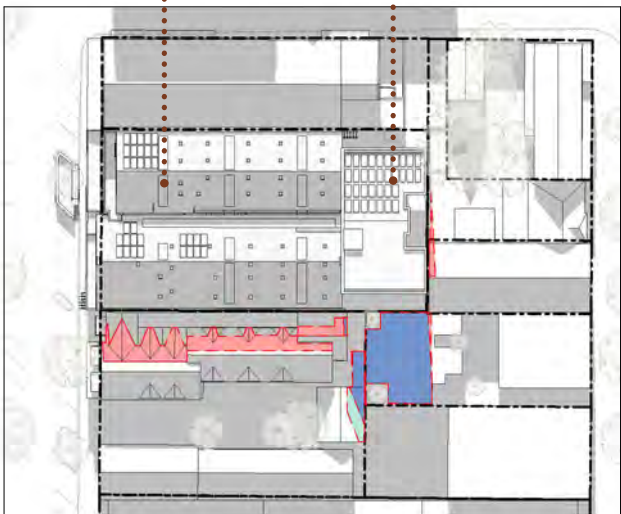
Compliant 3-storey Mass



Compliant Mass Plan View - Winter Solstice 3pm

Compliant built form will cast a larger shadow on neighbouring properties

Existing building New works



Proposed Works Plan View - Winter Solstice 9am

Existing building New works



Proposed Works Plan View - Winter Solstice 12pm

Existing building New works

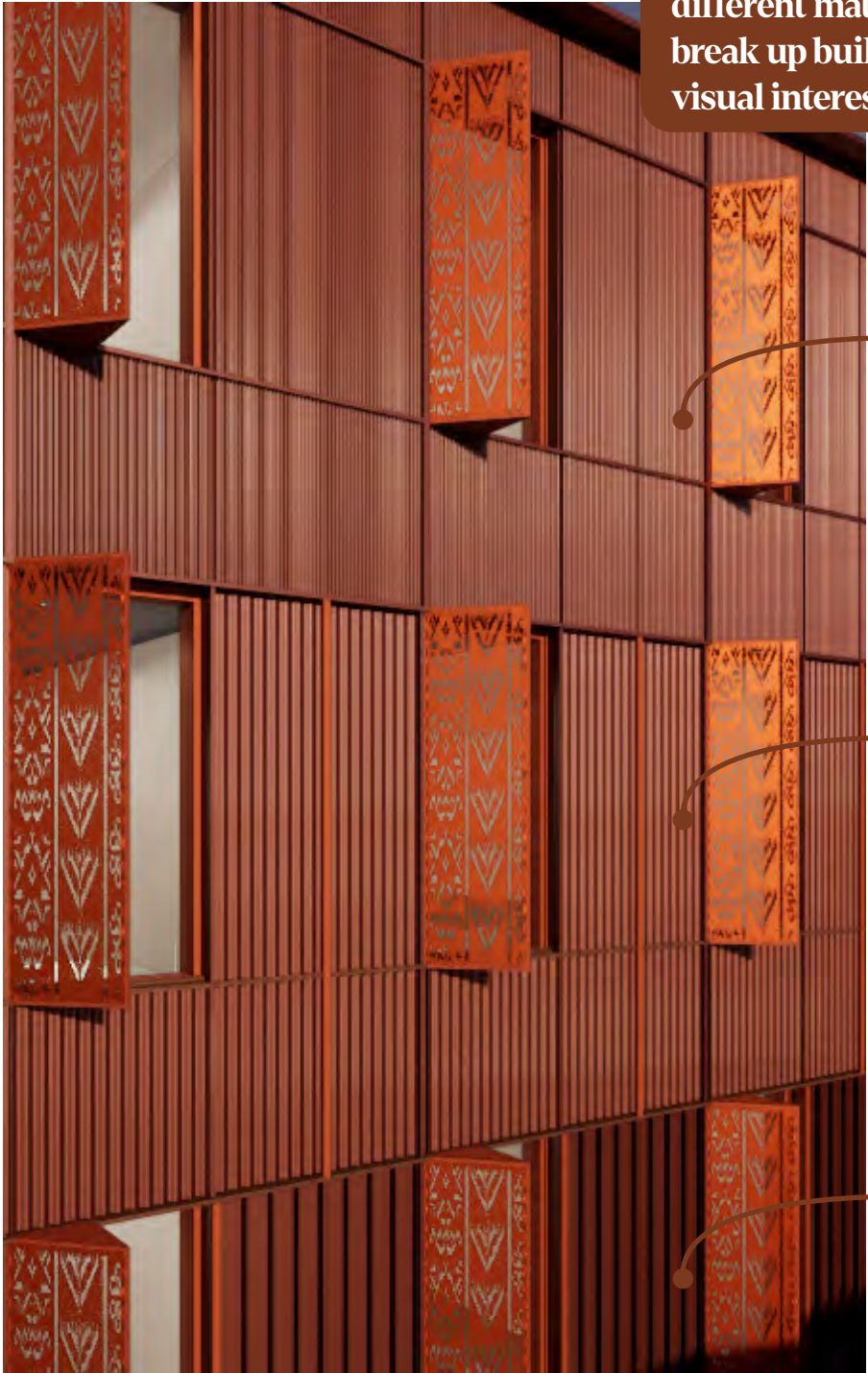
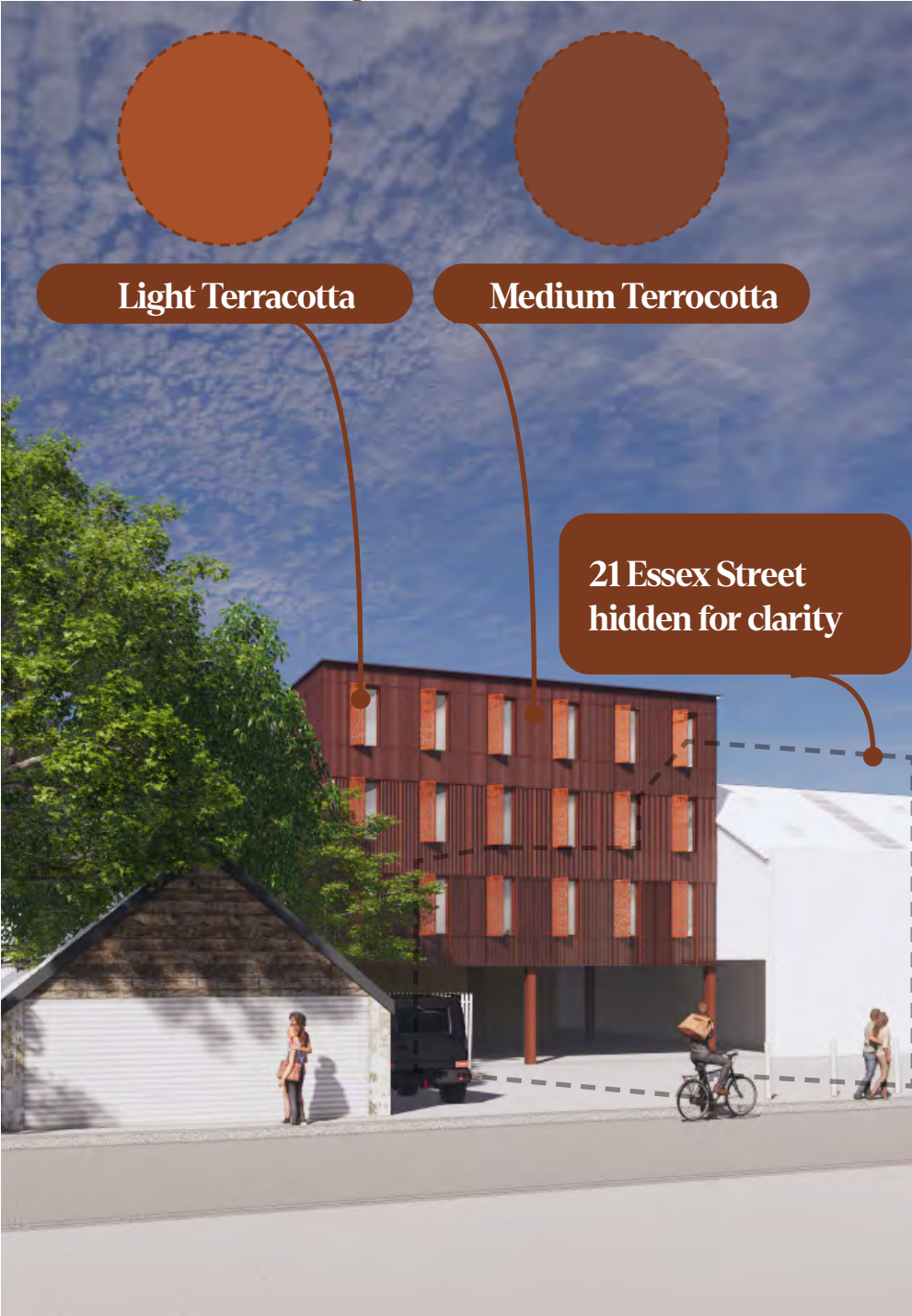


Proposed Works Plan View - Winter Solstice 3pm

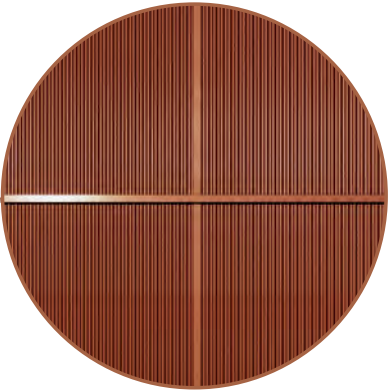
The shadow cast by the new building is substantially less than the allowable compliant built form

- Legend
- Compliant 3-Storey Envelope Shadow
 - Shadow Cast by Proposal
 - Additional Shadow Cast by Compliant Envelope

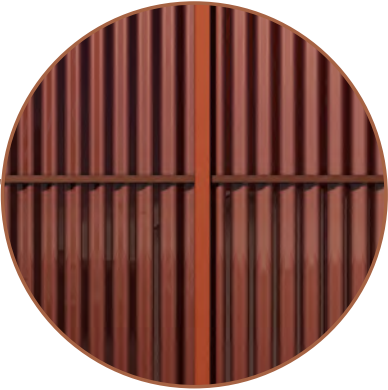
Materiality



Shadow and Light play through different material selections to break up built form and provide visual interest



Mini Orb – 35mm

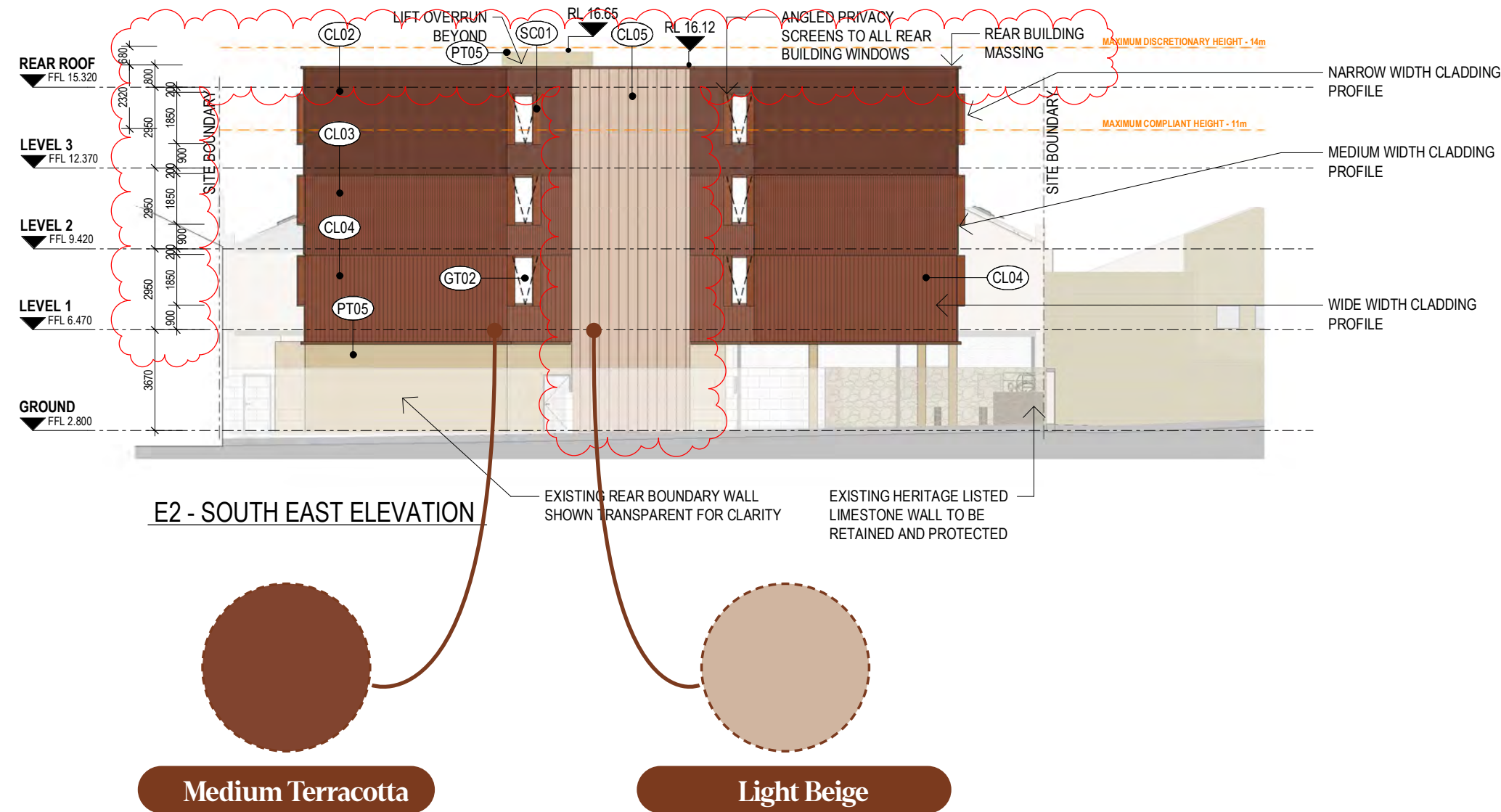


Custom Orb – 85mm



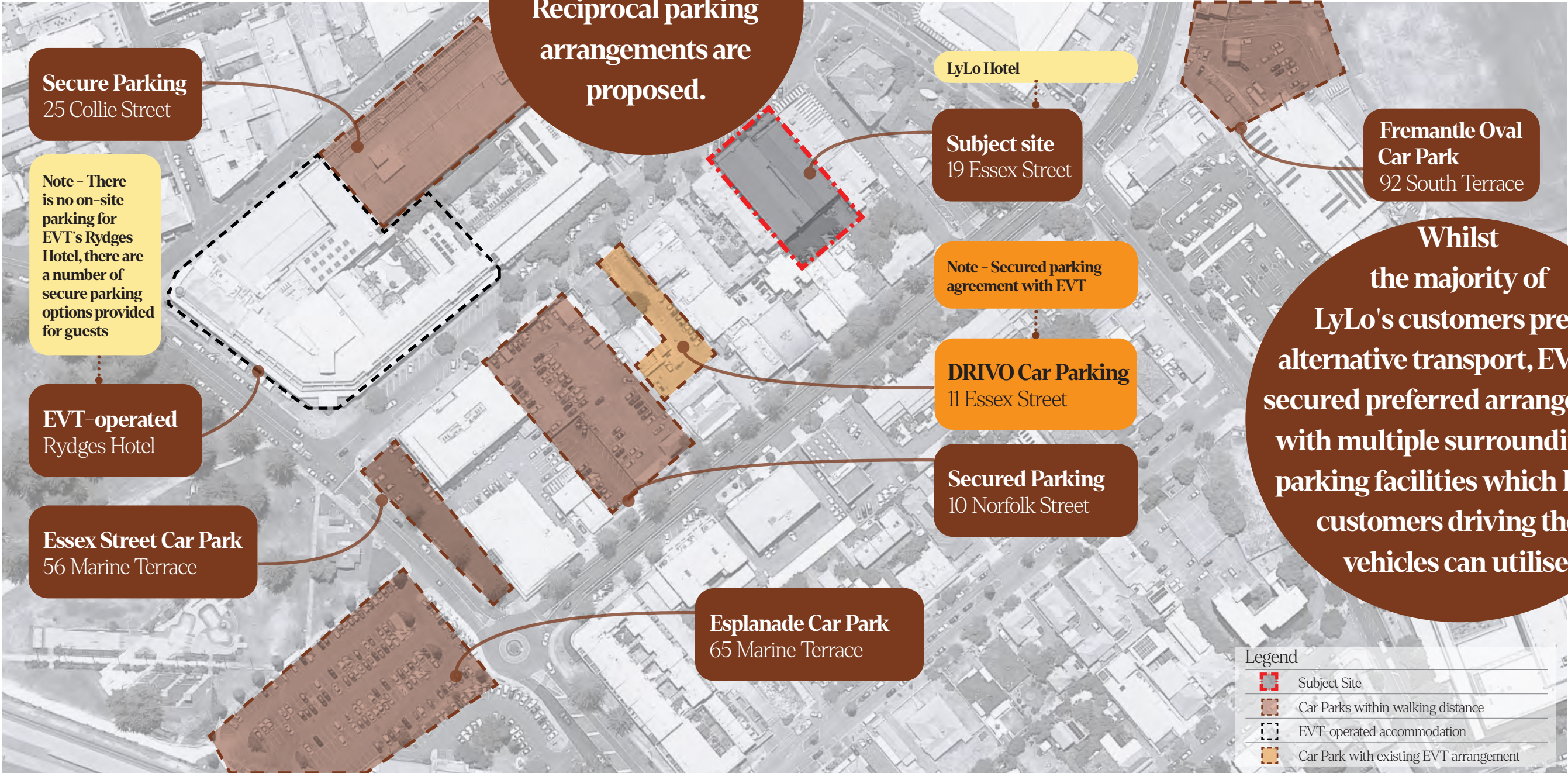
Longline 305 – 155mm

Elevations – Light Coloured Recess to Mitigate Built Form Impact



Car Parking

EVT operates both Rydges and LyLo. Reciprocal parking arrangements are proposed.



Planning Response

City of Fremantle Local Planning Scheme No.4 Schedule 7 – Local Planning Areas

Clause 1.2 – matters to be considered in applying general and specific height requirements

In granting consent to the maximum height prescribed, council shall be satisfied in regard to all of the following:

- a) that the proposal is consistent with predominant, height patterns of adjoining properties and the locality generally,
- b) the proposal would not be detrimental to the amenity of adjoining properties or the locality,
- c) the proposal would be consistent, if applicable, with conservation objectives for the site and locality generally, and
- d) any other relevant matter outlined in council's local planning policies

Planning Response

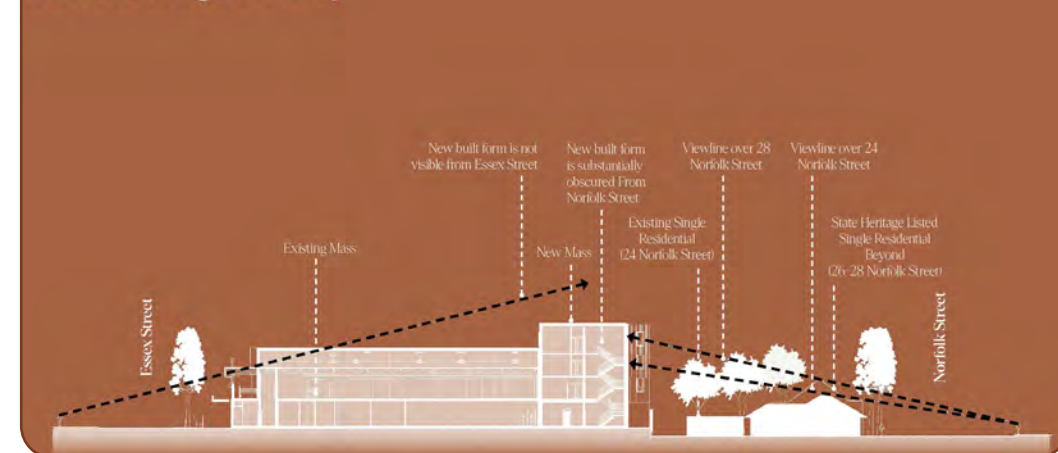
- a) that the proposal is consistent with predominant, height patterns of adjoining properties and the locality generally



Fremantle Technical College landmark/benchmark of height, remains the tallest landmark.

19 Essex St – whilst the new build portion is four storeys, it does not present as four storeys to the street – the streetscape / height pattern remains generally consistent

Sectional Impact Study

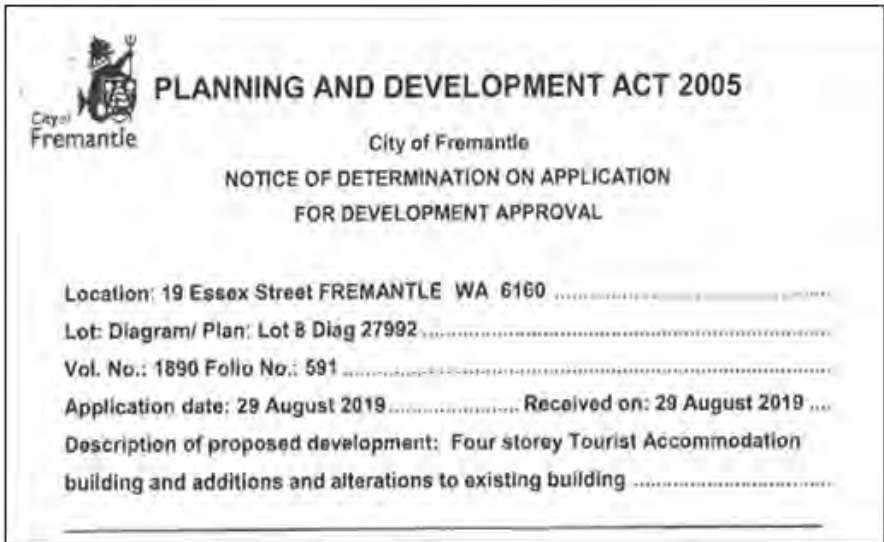


Planning Response

a) that the proposal is consistent with predominant, height patterns of adjoining properties and the locality generally

Four storeys has already been established as an acceptable height for this location / site

A Four Storey Tourist Accommodation development was approved by the City of Fremantle’s Planning Committee 4 March 2020.



Previously Approved Plans



Planning Response

b) the proposal would not be detrimental to the amenity of adjoining properties or the locality

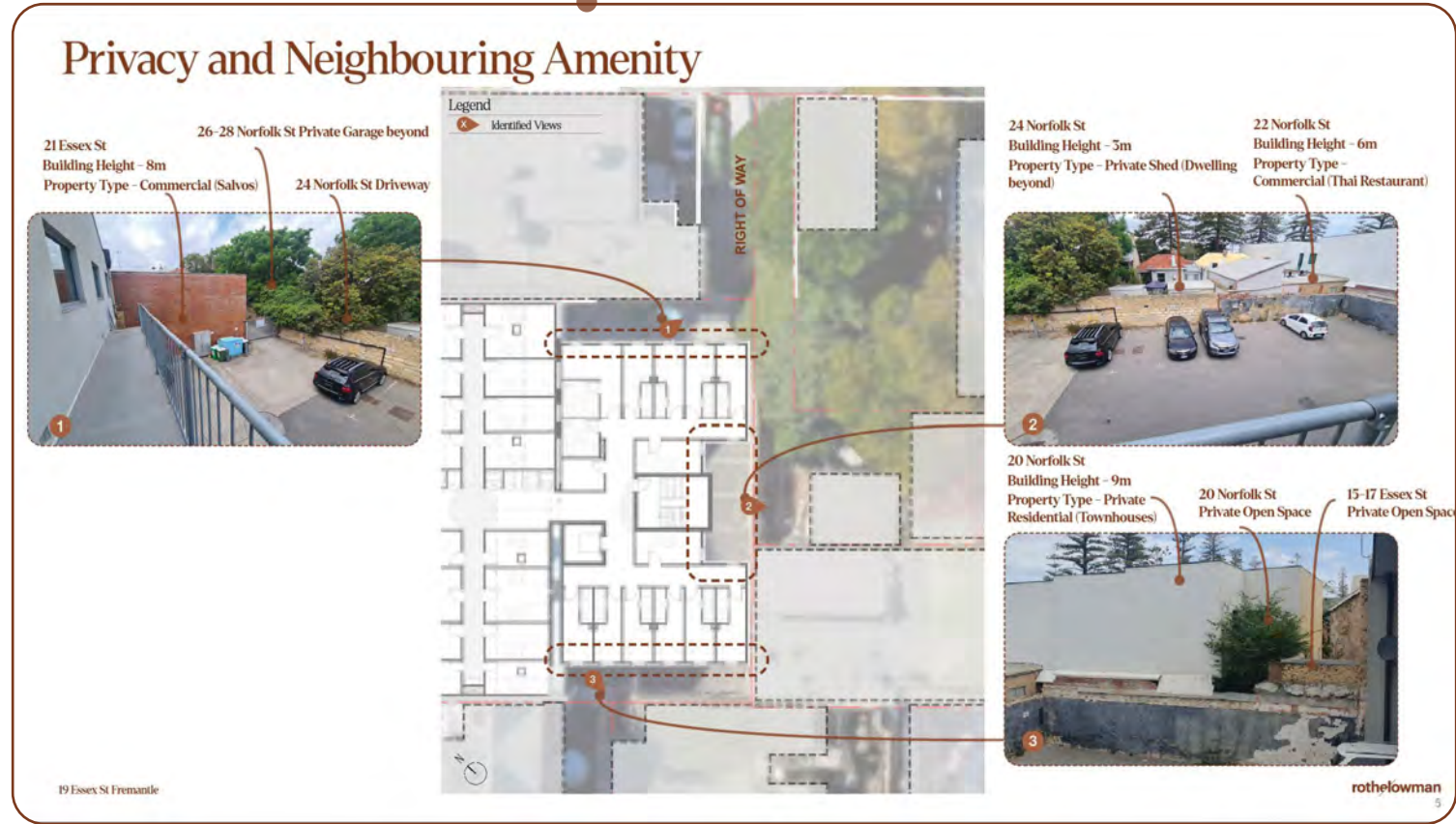
Our own analysis, and that of the City’s planning team in the RAR, have assessed that there is no significant amenity impacts on adjoining properties, such as excessive overshadowing

Our Analysis

RAR Planners Assessment

The height of the proposed building on the subject site is considered acceptable and meets the permitted additional height provisions under Schedule 7 of LPS4, for the following reasons:

- a) The proposed rear addition is setback approximately 40m from the Essex Street lot boundary. The setback of this addition, behind the existing building, will minimise its visibility and effectively reduce any potential detrimental impact on the streetscape as illustrated by Figure 4 below.
- b) The rear addition will be separated from the residential dwellings of 24 and 26 Norfolk Street by a 3m right of way as illustrated by Figure 5 below. Regardless of this, the addition will oppose the parking area/ carport of 24 Norfolk Street only and will not immediately oppose 26 Norfolk Street.
- c) 22 Norfolk Street is utilised for commercial purposes (restaurant). Regardless of this, the rear addition will not directly oppose any customer seating area or other areas considered important to providing customer amenity (there is a blank boundary will abutting the rear boundary of the subject site)
- d) The rear addition will feature a 3m setback from the lot boundary of 15-17 Essex Street will not immediately oppose the rear addition (the adjoining lot is located mostly adjacent the existing building to be retained) and is not located immediately to the south the rear addition (the adjoining site will therefore not experience any significant overshadowing.
- e) As illustrated by Figures 6-12 below, the rear addition is suitably located to minimise impact on the streetscapes of the surrounding area.



Planning Response

b) the proposal would not be detrimental to the amenity of adjoining properties or the locality

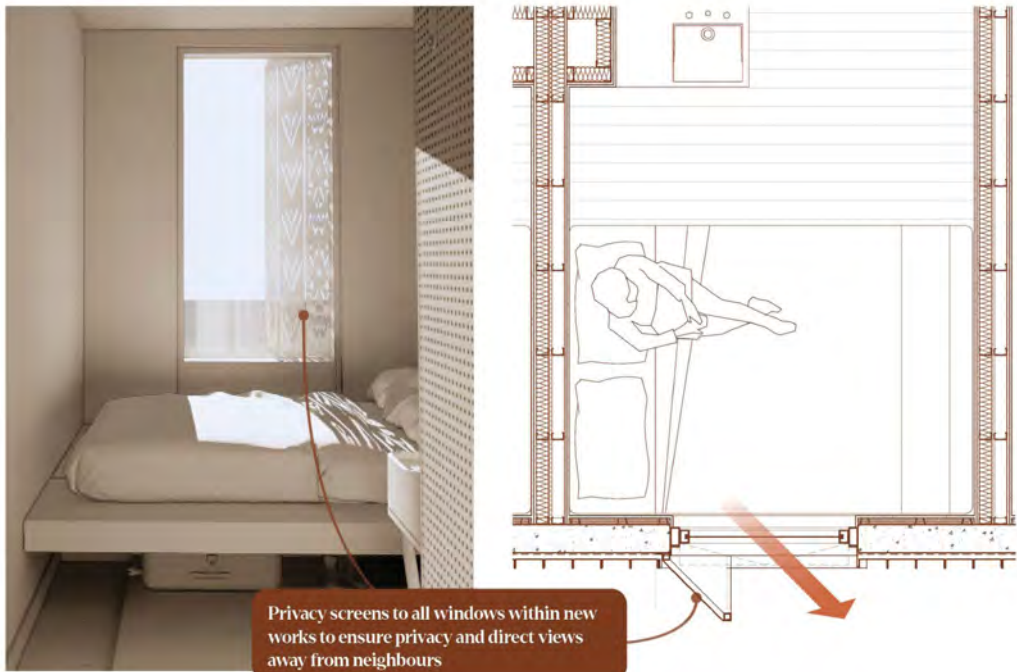
Our own analysis, and that of the City’s planning team in the RAR, have assessed that there is no significant amenity impacts on adjoining properties, such as overlooking

Privacy

Privacy and Neighbouring Amenity



Privacy Screens Detailed Study



Planning Response

b) the proposal would not be detrimental to the amenity of adjoining properties or the locality

Our own analysis, and that of the City’s planning team in the RAR, have assessed that there is no significant amenity impacts on adjoining properties, such as excessive overshadowing

Overshadowing

Solar Access



19 Essex St Fremantle

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Planning Response

c) the proposal would be consistent, if applicable, with conservation objectives for the site and locality generally

Our approach to heritage conservation for the site and for the locality generally have been supported by:

- Our independent heritage consultants and their Heritage Impact Statement
- The City of Fremantle’s heritage coordinator
- The City of Fremantle’s Design Advisory Committee – which includes a heritage expert
- Heritage Council of WA

RAR Report Comment

It is considered that the rear addition is consistent in bulk and scale with various buildings in the surrounding area and blends in with the varying bulk and scale of character of the Central Fremantle Heritage Area

The appearance of the front section of the lot will remain as per existing buildings on the site

The adaptation of the building to facilitate the tourist development will allow for the continued viability of the building and safeguard its long-term future

Heritage Council Advice

The Heritage Council resolved that the proposal has been considered in the context of the identified cultural heritage significance of the adjacent heritage places and the following advice is given:

Advice

The proposal in accordance with the plans submitted is supported subject to the following conditions:

1. A dilapidation survey of the Port Flour Mill, 26-28 Norfolk Street and the limestone boundary wall is to be prepared by a suitably qualified professional prior to any works being undertaken.
2. A program of monitoring any structural movement and potential vibration impacts on the Port Flour Mill, 26-28 Norfolk Street the limestone boundary wall is to be implemented at the commencement of works. The Heritage Council is to be notified immediately if impact occurs and advised on a recommended course of action by a suitably qualified structural engineer.

RECOMMENDATIONS:

The works proposed in this application are acceptable as they will have only a minor impact on the heritage values of 26–28 Norfolk Street, 19 Essex Street and the Central Fremantle Heritage Area. However, precautions need to be undertaken during construction to ensure that the adjacent State Heritage Listed buildings 26–28 Street and Port Flour Mill are not adversely affected.

Planning Response

d) any other relevant matter outlined in council’s local planning policies

No substantial variations are sought to any local planning policy.

Our assessment, and the City’s planning team assessment in the RAR, has assessed the application to meet the relevant policy objectives and requirements, with standard conditions applied where necessary.

- Community consultation – has been completed and our plans revised to address privacy, amenity and waste management.
- Design Advisory Committee – design review completed, and proposal supported
- Fremantle Port Buffer – DA condition has been included in the RAR.
- Public art – to be provided on-site. DA condition has been included in the RAR.
- Sustainability – a report has been included with a DA and outlines the sustainability benefits.

RAR Policy Assessment

Local Policies

- | | |
|--|---|
| <ul style="list-style-type: none">• Local Planning Policy 1.3 – Community Consultation on Planning Proposals• Local Planning Policy 1.6 – Heritage Assessment and Protection• Local Planning Policy 1.9 – Design Advisory Committee and Principles of Design• Local Planning Policy 1.10 – Construction Sites• Local Planning Policy 2.3 – Fremantle Port Buffer Area Development Guidelines | <ul style="list-style-type: none">• Local Planning Policy 2.13 – Sustainable Buildings Design Requirements• Local Planning Policy 2.19 – Contributions for Public Art and/or Heritage Weeks• Local Planning Policy 2.24 – Waste Management Plans for New Development• Local Planning Policy 3.6 – Heritage Areas |
|--|---|

Thank you.

DRAWING LIST			
NUMBER	NAME	REV.	REVISION DESCRIPTION
DA00.00	Cover Sheet	P10	DA ISSUE RFI 1
DA00.01	Proposed Site Plan	P16	DA ISSUE RFI 1
DA01.01	Demolition Ground Floor Plan	P9	DA ISSUE 1
DA01.02	Demolition First Floor Plan	P9	DA ISSUE 1
DA01.03	Demolition Roof Plan	P9	DA ISSUE 1
DA01.04	Ground Floor Plan	P10	DA ISSUE RFI 1
DA01.05	First Floor Plan	P9	DA ISSUE 1
DA01.06	L2-L3 Plan	P9	DA ISSUE RFI 1
DA01.07	Proposed Roof Plan	P10	DA ISSUE RFI 1
DA02.01	Existing & Demolition Elevations	P8	DA ISSUE 1
DA02.02	Existing & Demolition Elevations	P8	DA ISSUE 1
DA02.03	Existing & Demolition Elevations	P6	DA ISSUE 1
DA02.04	Proposed Elevations	P9	DA ISSUE RFI 1
DA02.05	Proposed Elevations	P9	DA ISSUE RFI 1
DA02.06	Proposed Elevations	P6	DA ISSUE 1
DA03.01	Sections	P9	DA ISSUE RFI 1
DA03.02	Sections	P9	DA ISSUE RFI 1
DA03.03	Sections	P9	DA ISSUE RFI 1
DA03.04	Wide Format Section	P1	DA ISSUE RFI 1
DA04.01	Overshadowing Study	P5	DA ISSUE RFI 1

DEVELOPMENT SUMMARY										
LEVEL	EXISTING BUILDING					REAR BUILDING				TOTAL ROOMS
	4 BED (A)	4 BED (B)	8 BED (C)	8 BED (C2)	8 BED (C3)	QUEEN ACC. ENS. (D)	QUEEN ENS. (D2)	QUEEN (D3)	QUEEN ACC. (D4)	
GROUND	0	0	0	0	0	0	0	0	0	0
LEVEL 1	3	23	2	1	8	1	10	2	0	50
LEVEL 2	2	0	0	0	0	1	10	1	1	15
LEVEL 3	2	0	0	0	0	1	10	1	1	15
Grand total	7	23	2	1	8	3	30	4	2	80

PROPOSED CAR PARKING		
LEVEL	CARPARKS	BICYCLE PARKS
	COMMERCIAL	
GROUND	4	27

REFUSE - BINS	
BIN TYPE	TOTAL
BIN 1100L	5



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23 Aug 2024

Revisions		
P5	22.04.2024	ISSUE FOR INFORMATION
P6	24.04.2024	DAC DRAFT PACK
P7	29.04.2024	DAC ISSUE
P8	04.06.2024	DA ISSUE
P9	05.06.2024	DA ISSUE 1
P10	19.08.2024	DA ISSUE RFI 1

Project
LyLo Fremantle
19 ESSEX STREET, FREMANTLE, WA

Drawing
Cover Sheet

Project No
223252

Author
RL

Scale: @ A3
1 : 200

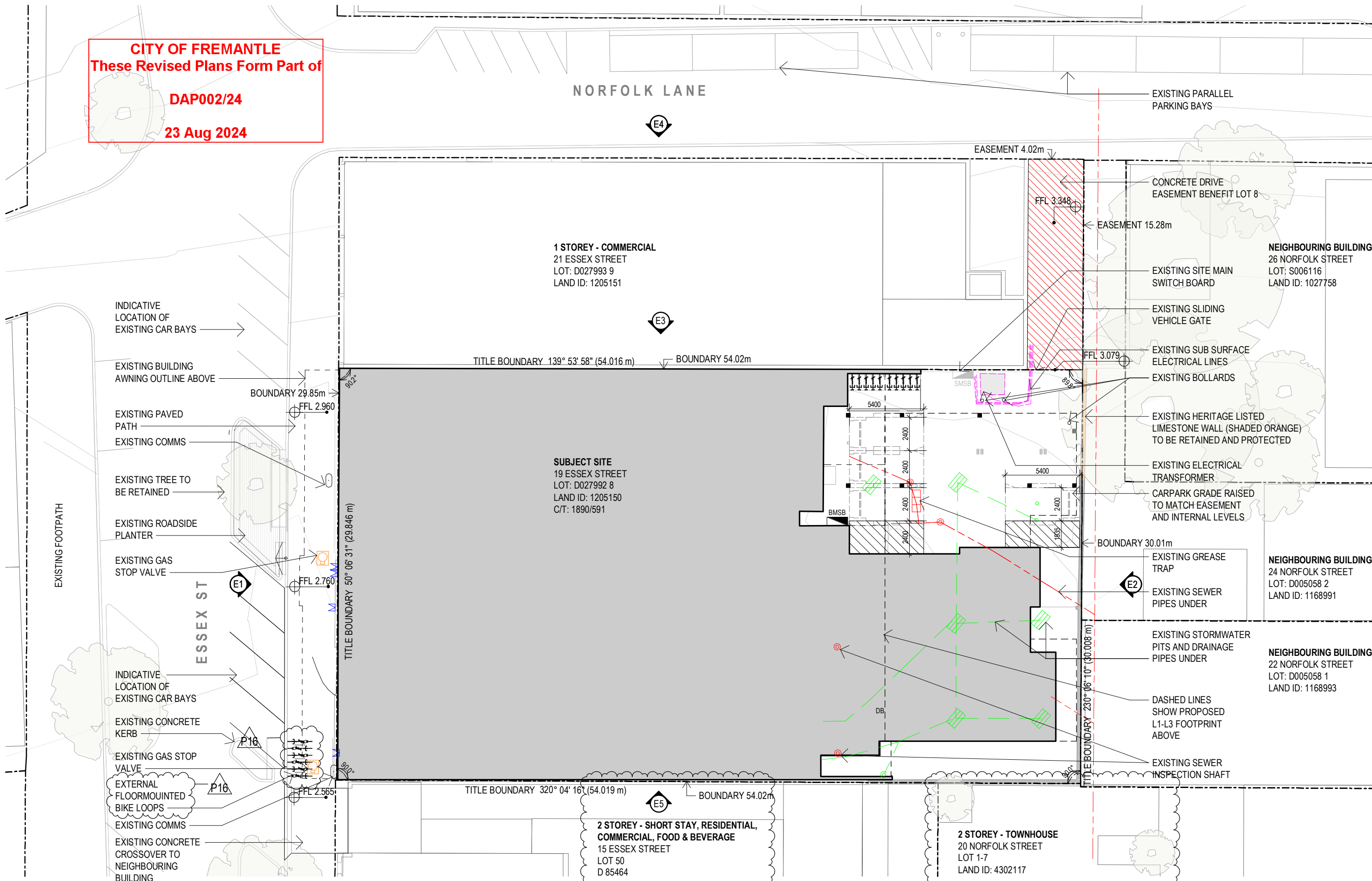
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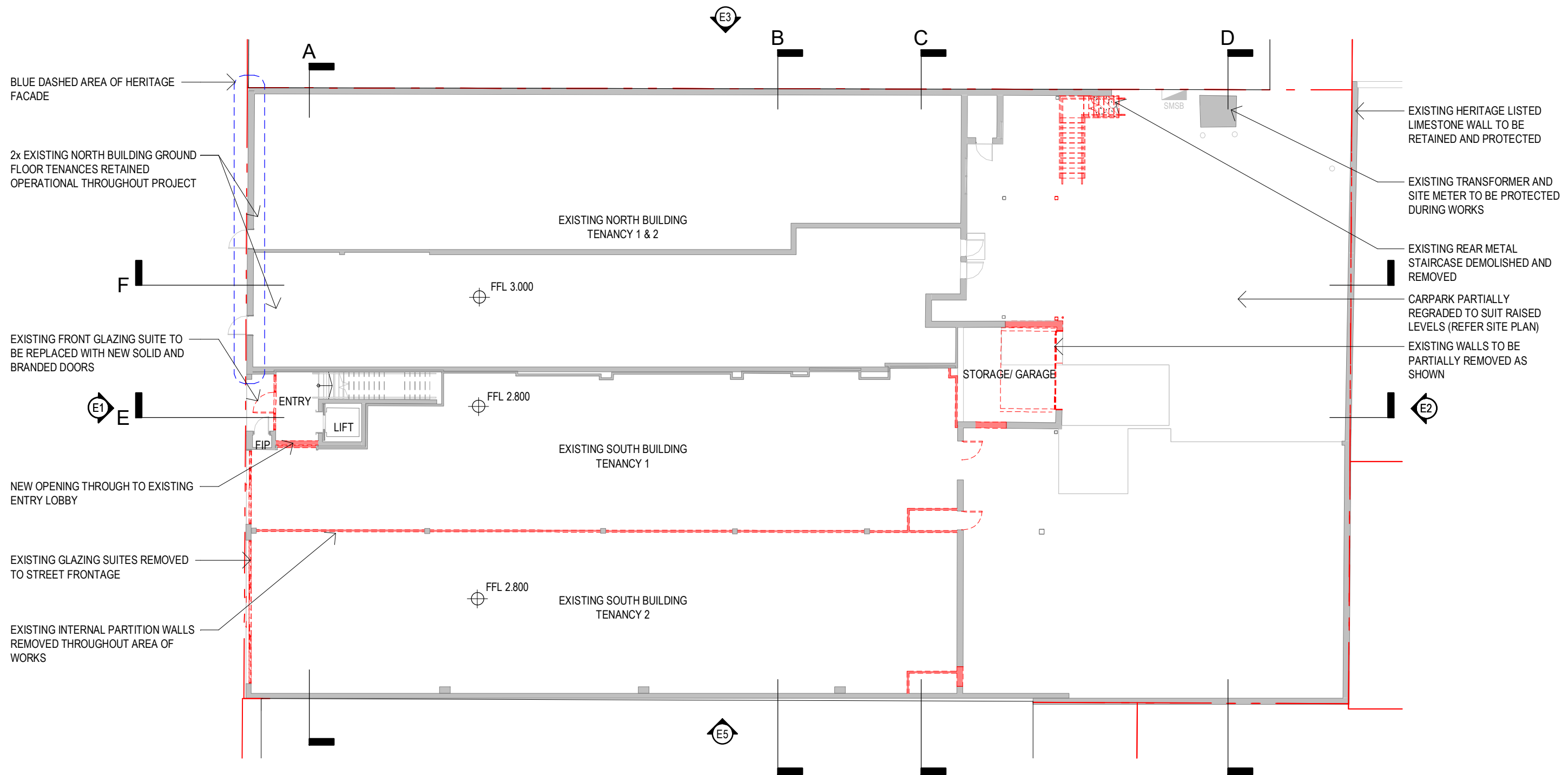
P10

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE DEMOLISHED
- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED

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23 Aug 2024

Revisions	P4	04.09.2024	CONCEPT COORDINATION MEETING
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	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **Demolition Ground Floor Plan**

Project No **223252** Author **AL**

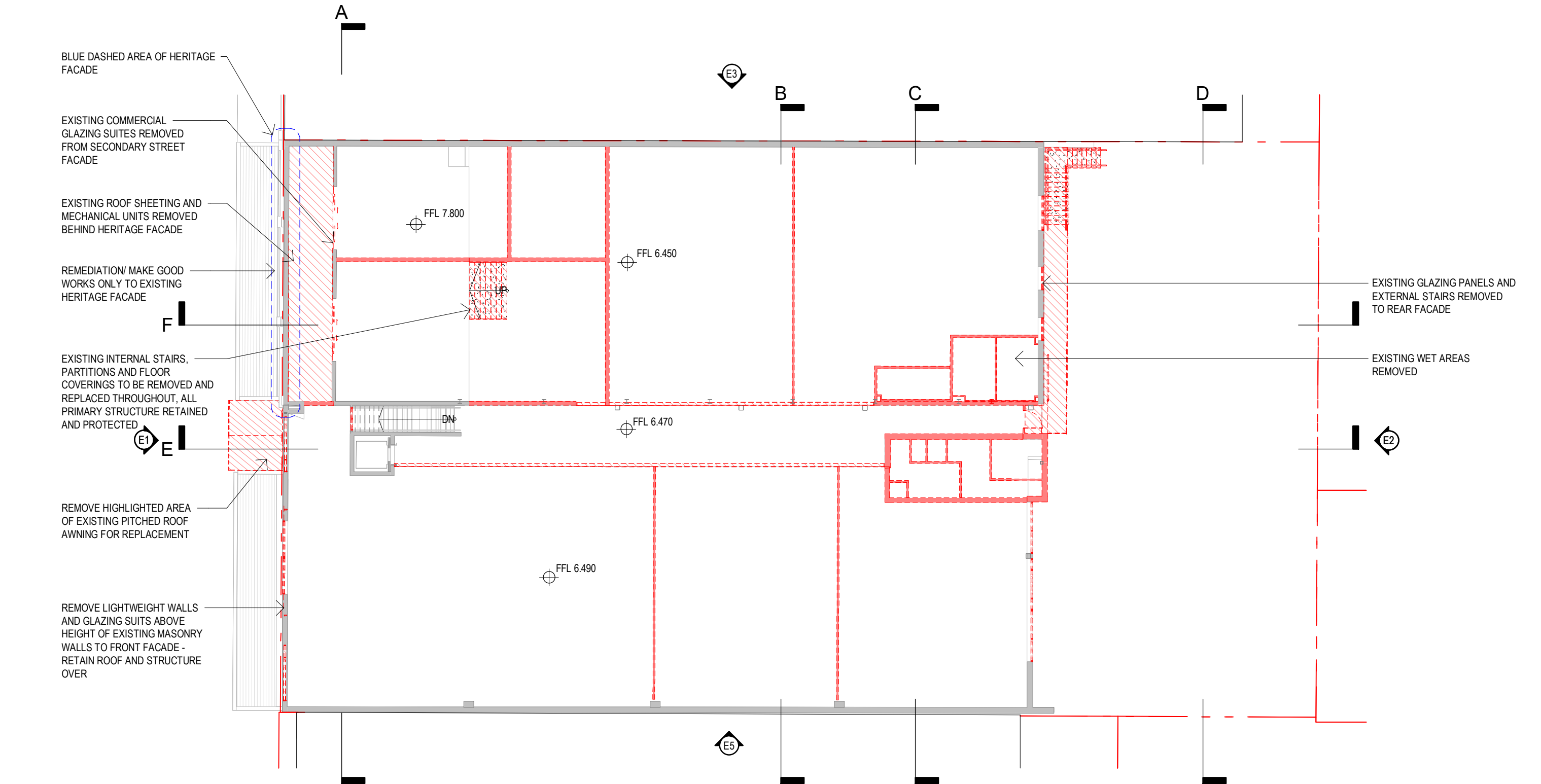
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Drawing No. **DA01.01**

P9

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	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **Demolition First Floor Plan**

Project No **223252** Author **AL**

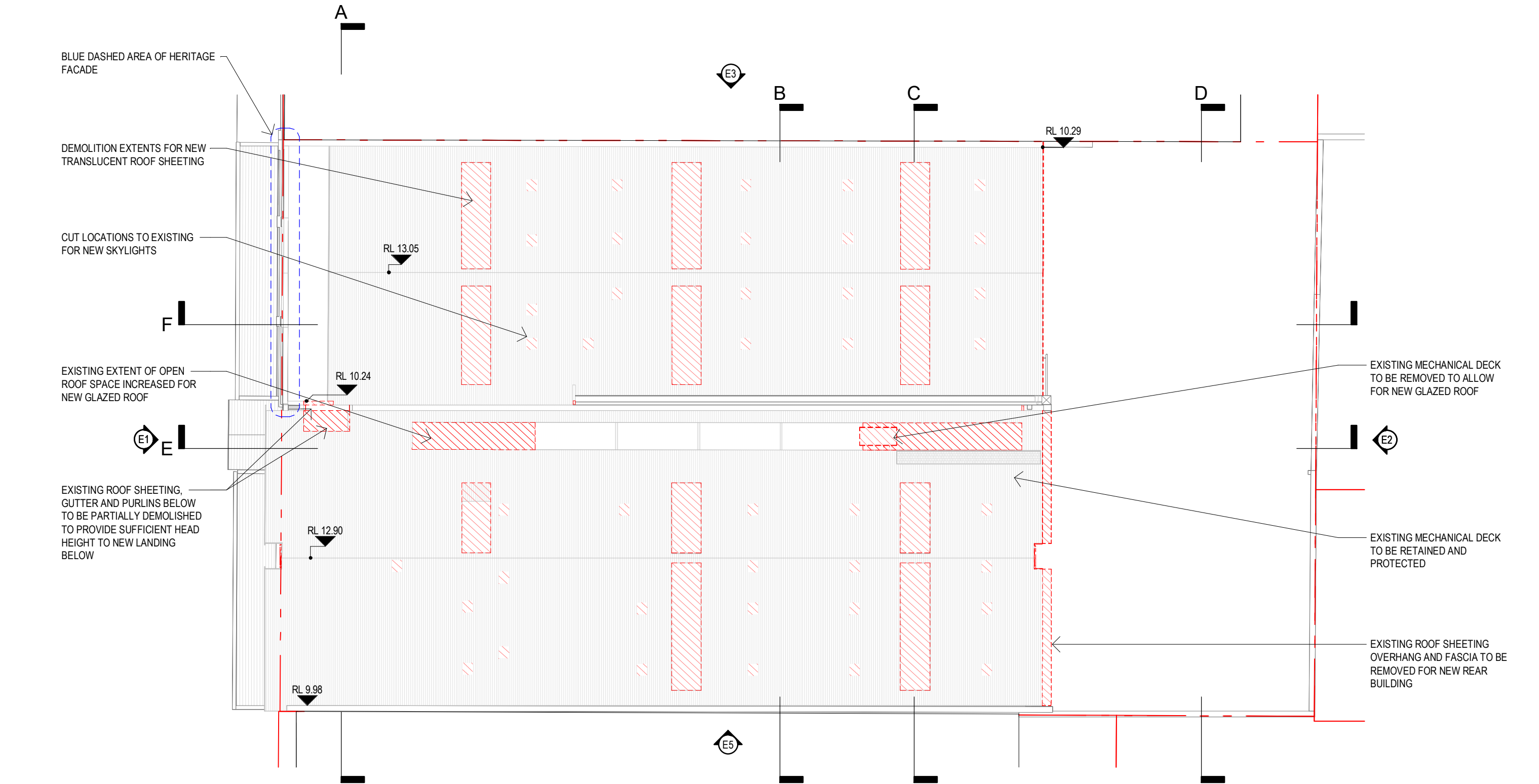
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Drawing No. **DA01.02**

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	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **Demolition Roof Plan**

Project No **223252** Author **AL**

Scale: @ A3 **1 : 200**

Drawing No. **DA01.03**

P9

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EXISTING SEPARATION FROM
HERITAGE FACADE TO PRIMARY
WALL

BLUE DASHED AREA OF HERITAGE
FACADE

GREY DASHED LINES
INDICATIVE LOCATION OF
EXISTING TRUSSES OVERHEAD

ORANGE DASHED LINES OF
GROUND FLOOR WALL BELOW

NEW WALL TO EXTEND ABOVE
EXISTING MASONRY WALL -
ALIGN DATUM WITH NORTHERN
TENANCY HERITAGE FACADE
WALL

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

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	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1

Project **LyLo Fremantle**

19 ESSEX STREET, FREMANTLE, WA

Drawing **First Floor Plan**

Project No **223252**

Author **RL**

Scale: @ A3 **1 : 200**

Drawing No. **DA01.05**

P9

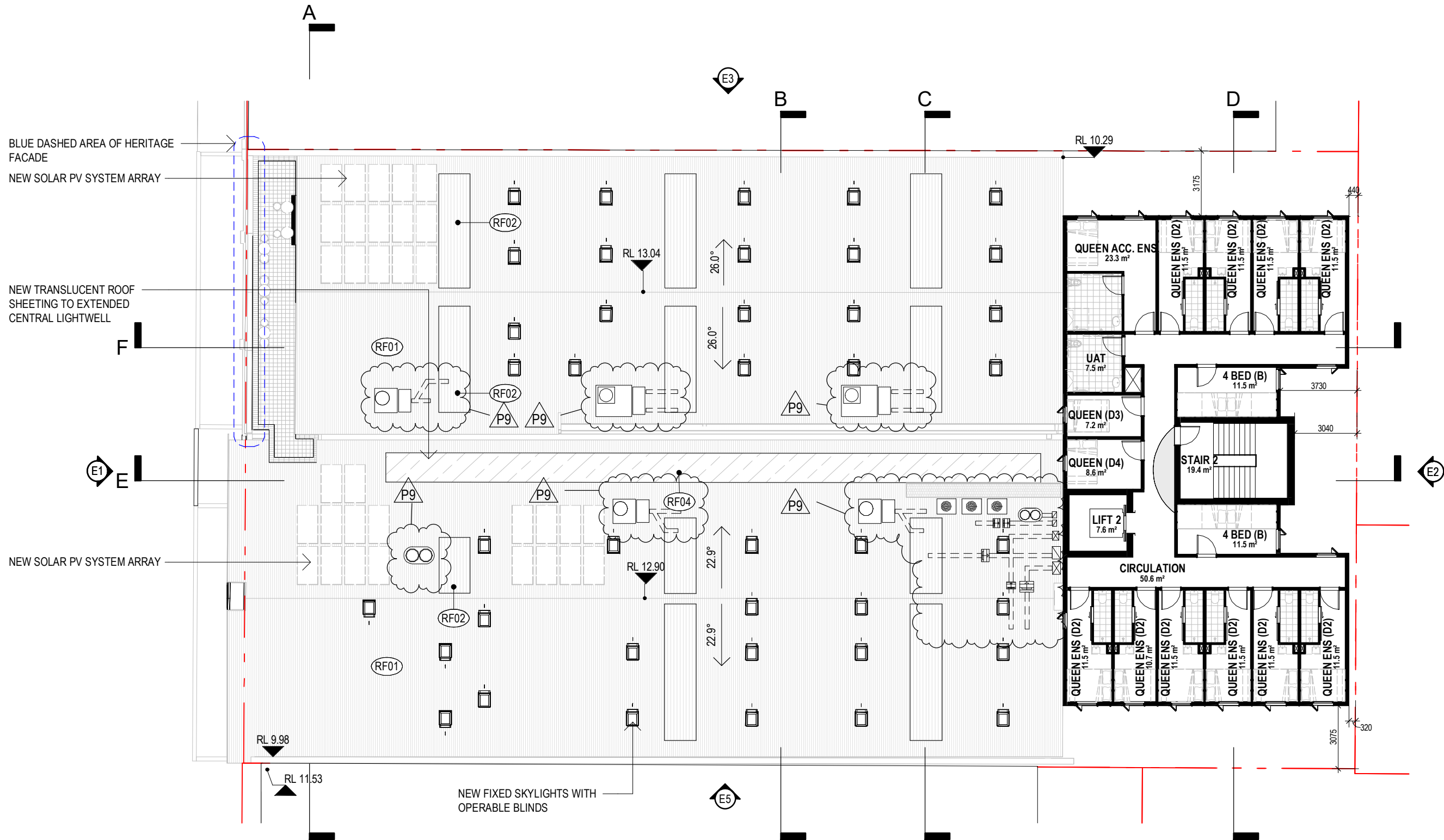
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- NEW WALLS, FLOORS AND STRUCTURES

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	P7	04.06.2024	DA ISSUE
	P8	05.06.2024	DA ISSUE 1
	P9	19.08.2024	DA ISSUE RF1 1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **L2-L3 Plan**

Project No **223252** Author **AL**

Scale: @ A3 **1 : 200**

Drawing No. **DA01.06**

P9

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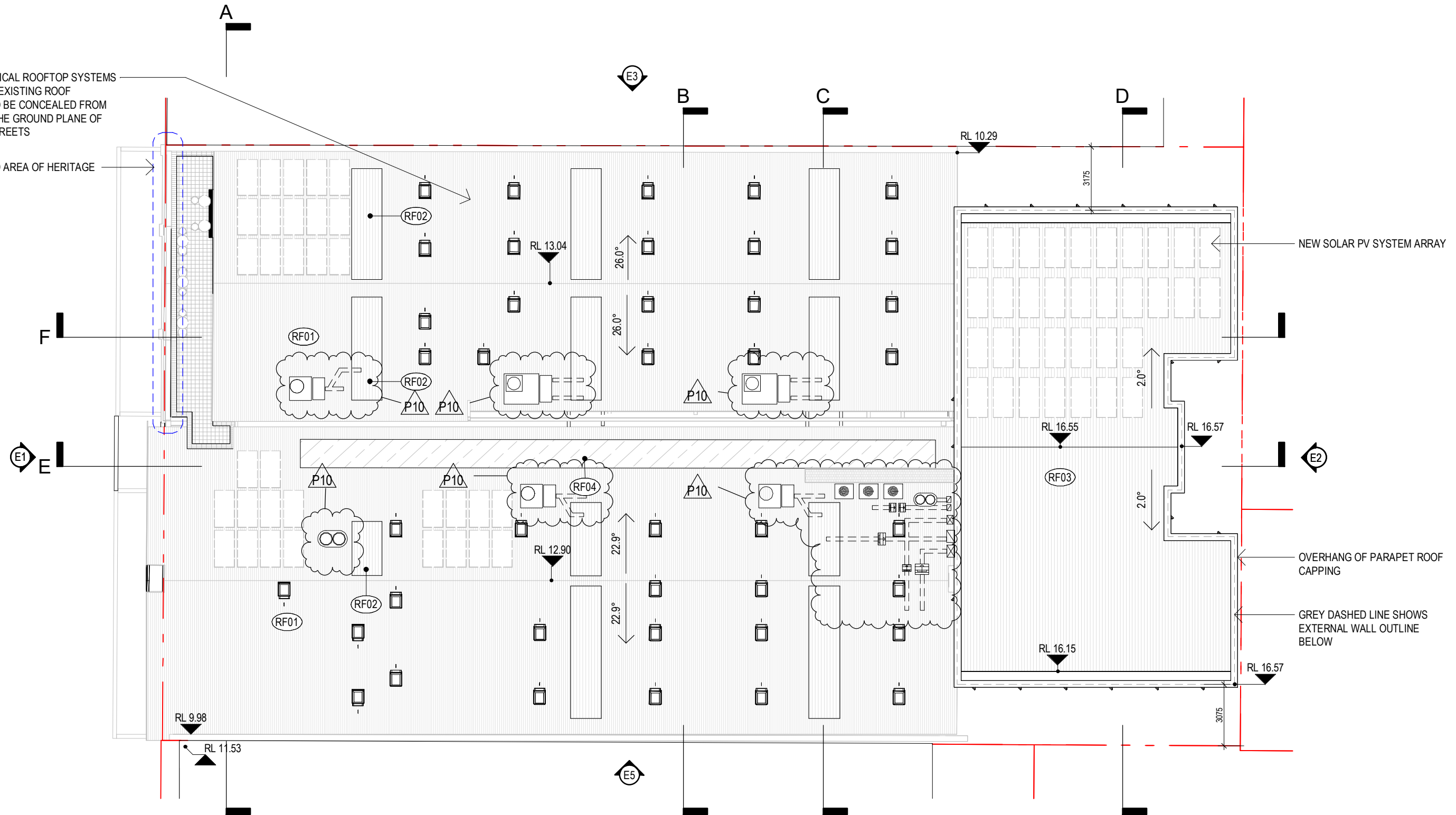
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NEW MECHANICAL ROOFTOP SYSTEMS
LOCATED ON EXISTING ROOF
SHEETING, TO BE CONCEALED FROM
VIEW FROM THE GROUND PLANE OF
ADJACENT STREETS

BLUE DASHED AREA OF HERITAGE
FACADE



WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

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	P7	29.04.2024	DAC ISSUE
	P8	04.06.2024	DA ISSUE
	P9	05.06.2024	DA ISSUE 1
	P10	19.08.2024	DA ISSUE RF1 1

Project / **LyLo Fremantle**

19 ESSEX STREET, FREMANTLE, WA

Drawing / **Proposed Roof Plan**

Project No / **223252**

Author / **AL**

Scale: @ A3 / **1 : 200**

Drawing No. / **DA01.07**

P10

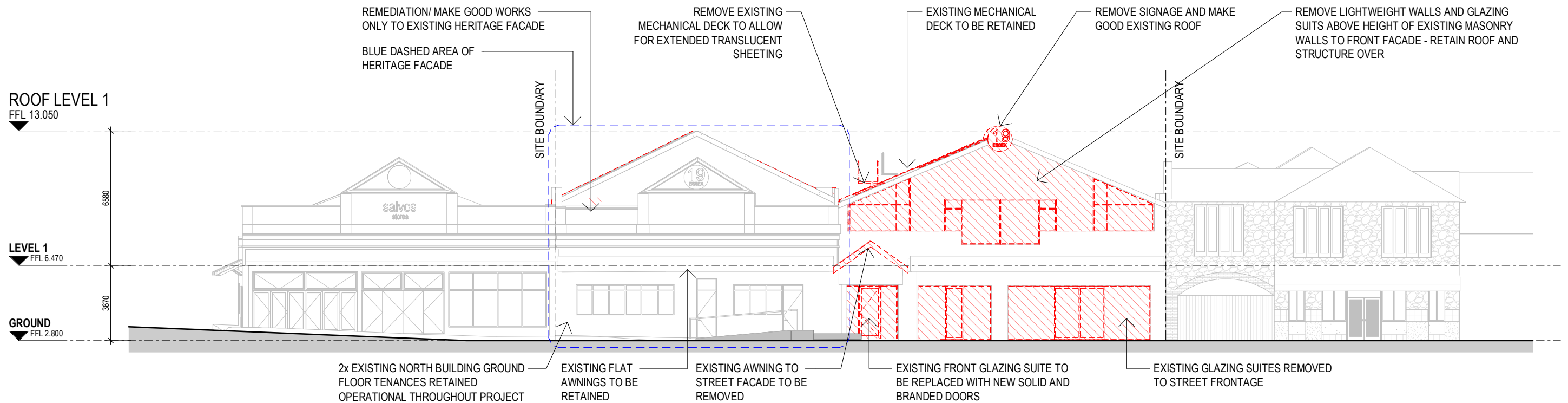
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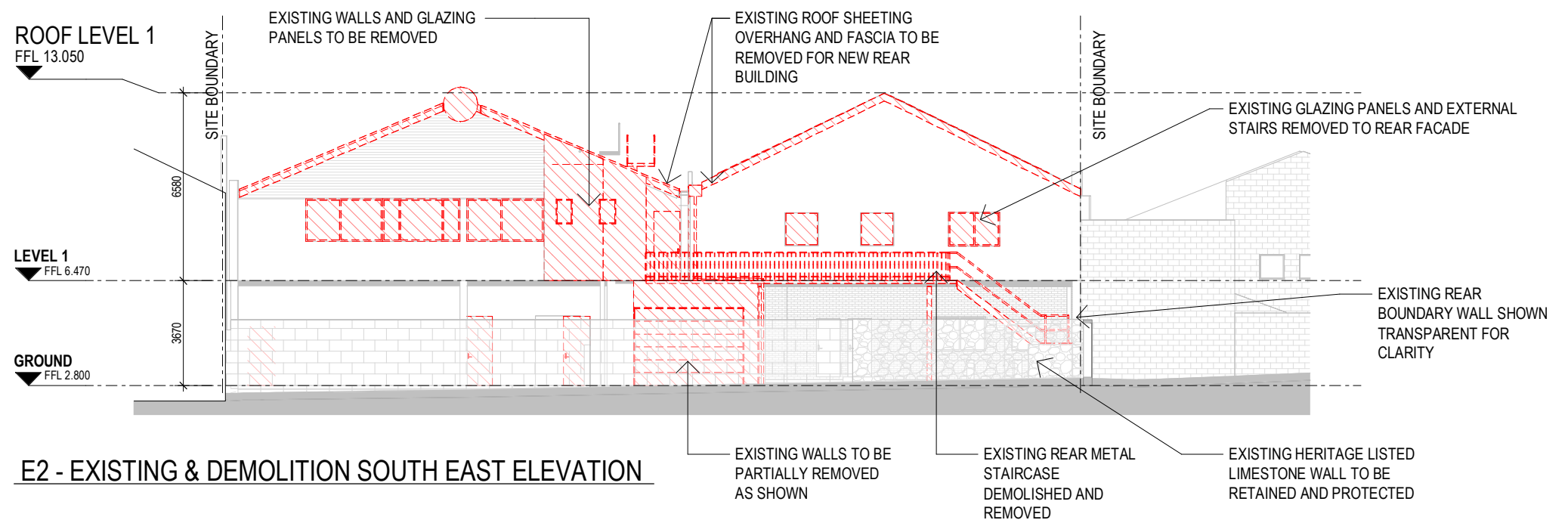
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23 Aug 2024

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E1 - EXISTING & DEMOLITION NORTH WEST ELEVATION



E2 - EXISTING & DEMOLITION SOUTH EAST ELEVATION

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE DEMOLISHED
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	P6	29.04.2024	DAC ISSUE
	P7	04.06.2024	DA ISSUE
	P8	05.06.2024	DA ISSUE 1

Project
LyLo Fremantle
19 ESSEX STREET, FREMANTLE, WA

Drawing
Existing & Demolition Elevations

Project No
223252
Author
AL
Scale: @ A3
1 : 200

Drawing No.
DA02.01

P8

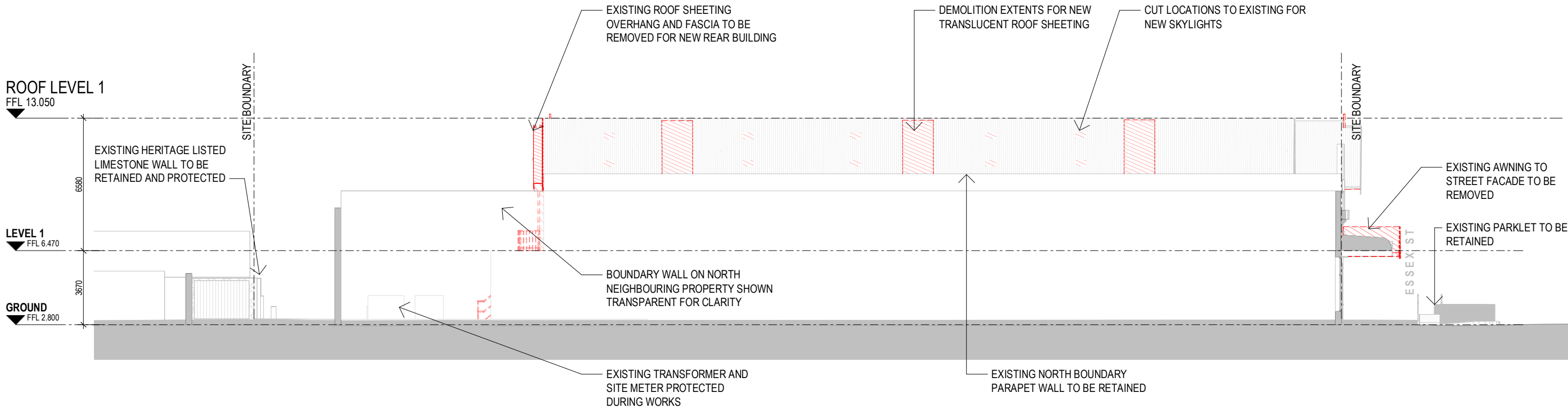
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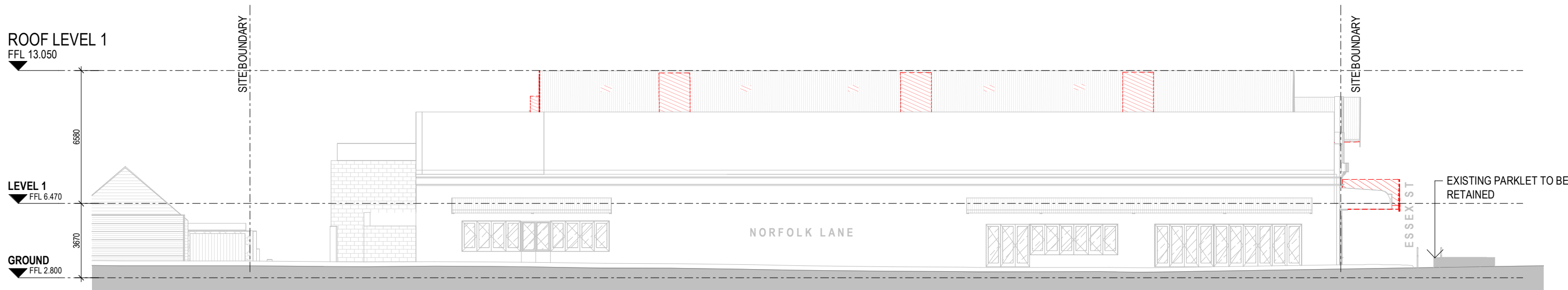
DAP002/24

23 Aug 2024

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E3 - EXISTING & DEMOLITION NORTH EAST ELEVATION



E4 - EXISTING & DEMOLITION NORTH EAST ELEVATION
(NORFOLK LANE)

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE DEMOLISHED
- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED

Revisions	P3	04.09.2024	CONCEPT COORDINATION MEETING
	P4	22.04.2024	ISSUE FOR INFORMATION
	P5	24.04.2024	DAC DRAFT PACK
	P6	29.04.2024	DAC ISSUE
	P7	04.06.2024	DA ISSUE
	P8	05.06.2024	DA ISSUE 1

Project
LyLo Fremantle
19 ESSEX STREET, FREMANTLE, WA

Drawing
Existing & Demolition Elevations

Project No
223252
Author
AL
Scale: @ A3
1 : 200

Drawing No.
DA02.02

P8

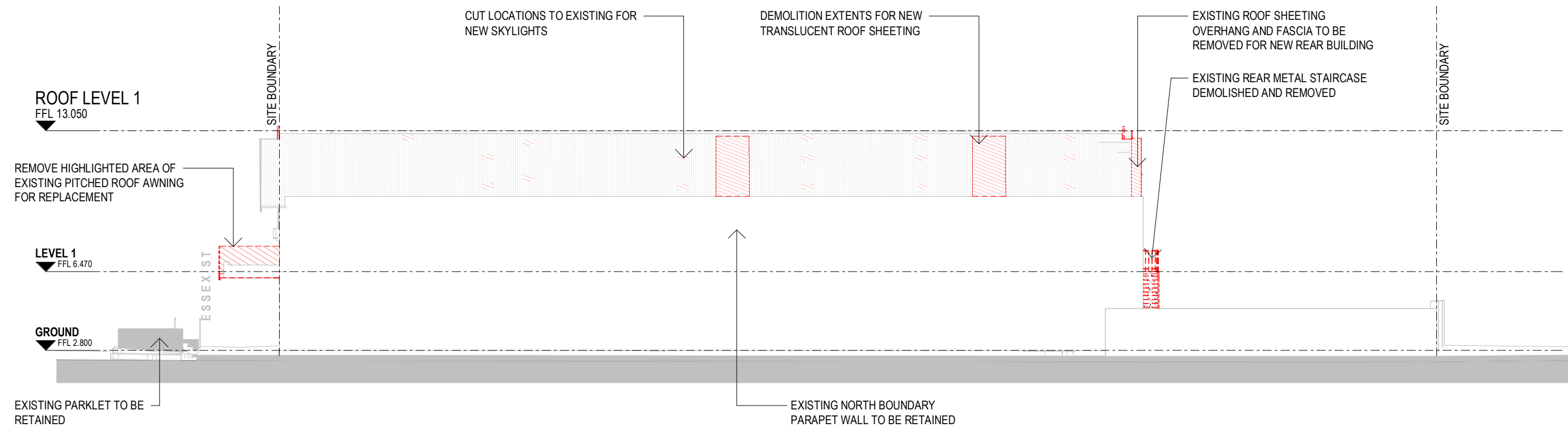
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E5 - EXISTING & DEMOLITION SOUTH WEST ELEVATION

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Revisions	P1	04.09.2024	CONCEPT COORDINATION MEETING
	P2	22.04.2024	ISSUE FOR INFORMATION
	P3	24.04.2024	DAC DRAFT PACK
	P4	29.04.2024	DAC ISSUE
	P5	04.06.2024	DA ISSUE
	P6	05.06.2024	DA ISSUE 1

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Project
LyLo Fremantle

19 ESSEX STREET, FREMANTLE, WA

Drawing
Existing & Demolition Elevations

Project No
223252
Author
AL
Scale: @ A3
1 : 200

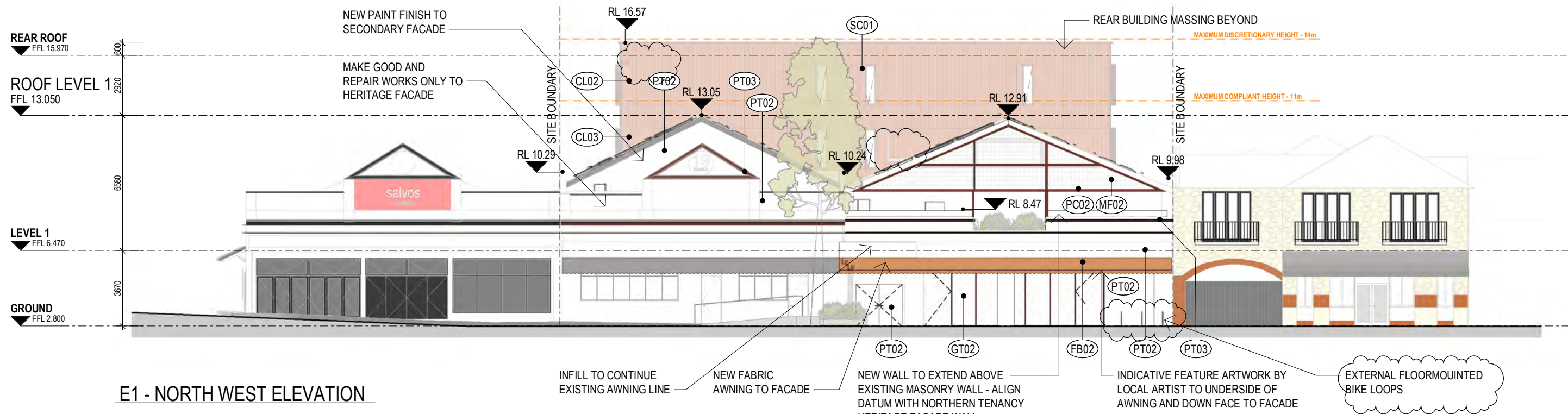
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DA02.03

P6

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E1 - NORTH WEST ELEVATION



E2 - SOUTH EAST ELEVATION

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

MATERIAL AND FINISHES LEGEND

CL01 - CLADDING EXISTING	PT01 - PAINT FINISH EXISTING	PT05 - PAINT FINISH NEW PAINT FINISH COLOUR: BALSA STONE	FB01 - FABRIC FINISH EXISTING	RF01 - ROOF CLADDING EXISTING	GT01 - GLAZING EXISTING GLAZING
CL02 - CLADDING LYSAGHT MINI ORB (35mm) COLOUR: TEMPTRESS	PT02 - PAINT FINISH NEW PAINT FINISH COLOUR: LEXICON QUARTER	MF01 - METAL FINISH EXISTING	FB02 - ROOF CLADDING NEW FABRIC AWNING COLOUR: CINNAMON SAND	RF02 - ROOF CLADDING NEW TRANSLUCENT ROOF CLADDING COLOUR: SHALE GREY	GT02 - GLAZING NEW GLAZING TRANSPARENCY: CLEAR
CL03 - CLADDING LYSAGHT CUSTOM ORB (85mm) COLOUR: TEMPTRESS	PT03 - PAINT FINISH NEW PAINT FINISH COLOUR: RUSTED CRIMSON	MF02 - METAL FINISH NEW PERFORATED METAL FINISH COLOUR: CHAIN PEARL	PC01 - POWDERCOAT FINISH EXISTING	RF03 - ROOF CLADDING NEW CORRUGATED ROOF CLADDING COLOUR: SHALE GREY	GT03 - GLAZING NEW FIRE RATED GLAZING TRANSPARENCY: CLEAR
CL04 - CLADDING LYSAGHT LONGLINE 305 (155mm) COLOUR: TEMPTRESS		SC01 - SCREEN NEW SCREEN COLOUR: TERRACOTTA CHIP	PC02 - POWDERCOAT FINISH NEW METAL FINISH COLOUR: RUSTED CRIMSON	RF04 - ROOF GLAZING NEW GLAZED PANELS COLOUR: CLEAR	

Revisions	P4	22.04.2024	ISSUE FOR INFORMATION
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	P7	04.06.2024	DA ISSUE
	P8	05.06.2024	DA ISSUE 1
	P9	19.08.2024	DA ISSUE RFI 1

Project / **LyLo Fremantle**

19 ESSEX STREET, FREMANTLE, WA

Drawing / **Proposed Elevations**

Project No / **223252** Author / **AL** Scale: @ A3 / **1 : 200**

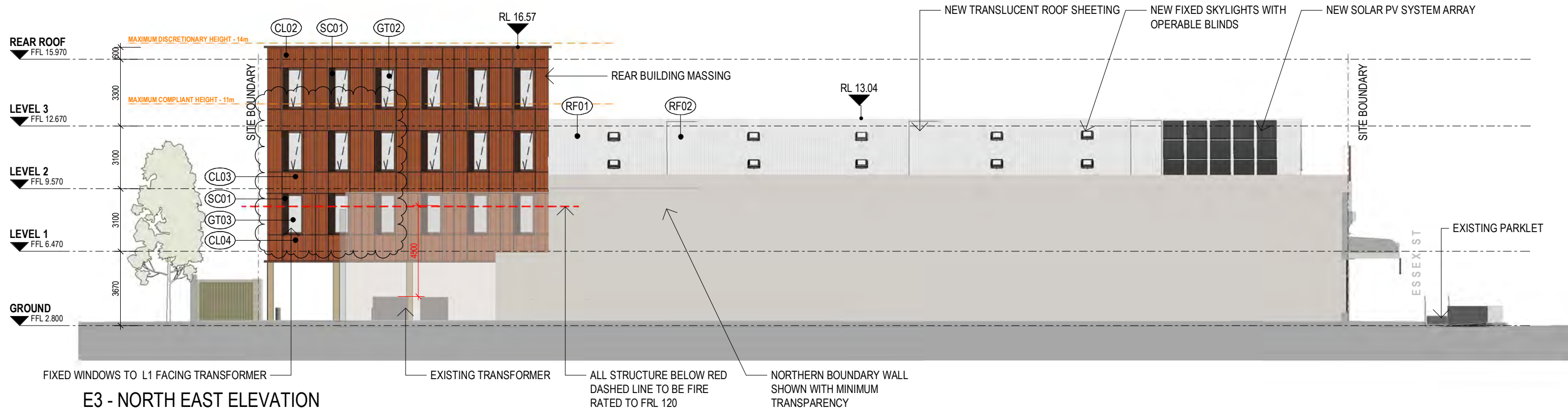
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E4 - NORTH EAST ELEVATION (NORFOLK LANE)

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
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MATERIAL AND FINISHES LEGEND					
CL01 - CLADDING EXISTING	PT01 - PAINT FINISH EXISTING	PT05 - PAINT FINISH NEW PAINT FINISH COLOUR: BALSA STONE	FB01 - FABRIC FINISH EXISTING	RF01 - ROOF CLADDING EXISTING	GT01 - GLAZING EXISTING GLAZING
CL02 - CLADDING LYSAGHT MINI ORB (35mm) COLOUR: TEMPTRESS	PT02 - PAINT FINISH NEW PAINT FINISH COLOUR: LEXICON QUARTER	MF01 - METAL FINISH EXISTING	FB02 - ROOF CLADDING NEW FABRIC AWNING COLOUR: CINNAMON SAND	RF02 - ROOF CLADDING NEW TRANSLUCENT ROOF CLADDING COLOUR: SHALE GREY	GT02 - GLAZING NEW GLAZING TRANSPARENCY: CLEAR
CL03 - CLADDING LYSAGHT CUSTOM ORB (85mm) COLOUR: TEMPTRESS	PT03 - PAINT FINISH NEW PAINT FINISH COLOUR: RUSTED CRIMSON	MF02 - METAL FINISH NEW PERFORATED METAL FINISH COLOUR: CHAIN PEARL	PC01 - POWDERCOAT FINISH EXISTING	RF03 - ROOF CLADDING NEW CORRUGATED ROOF CLADDING COLOUR: SHALE GREY	GT03 - GLAZING NEW FIRE RATED GLAZING TRANSPARENCY: CLEAR
CL04 - CLADDING LYSAGHT LONGLINE 305 (155mm) COLOUR: TEMPTRESS		SC01 - SCREEN NEW SCREEN COLOUR: TERRACOTTA CHIP	PC02 - POWDERCOAT FINISH NEW METAL FINISH COLOUR: RUSTED CRIMSON	RF04 - ROOF GLAZING NEW GLAZED PANELS COLOUR: CLEAR	

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	P7	04.06.2024	DA ISSUE
	P8	05.06.2024	DA ISSUE 1
	P9	19.08.2024	DA ISSUE RFI 1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **Proposed Elevations**

Project No **223252** Author **AL** Scale: @ A3 **1 : 200**

Drawing No **DA02.05**

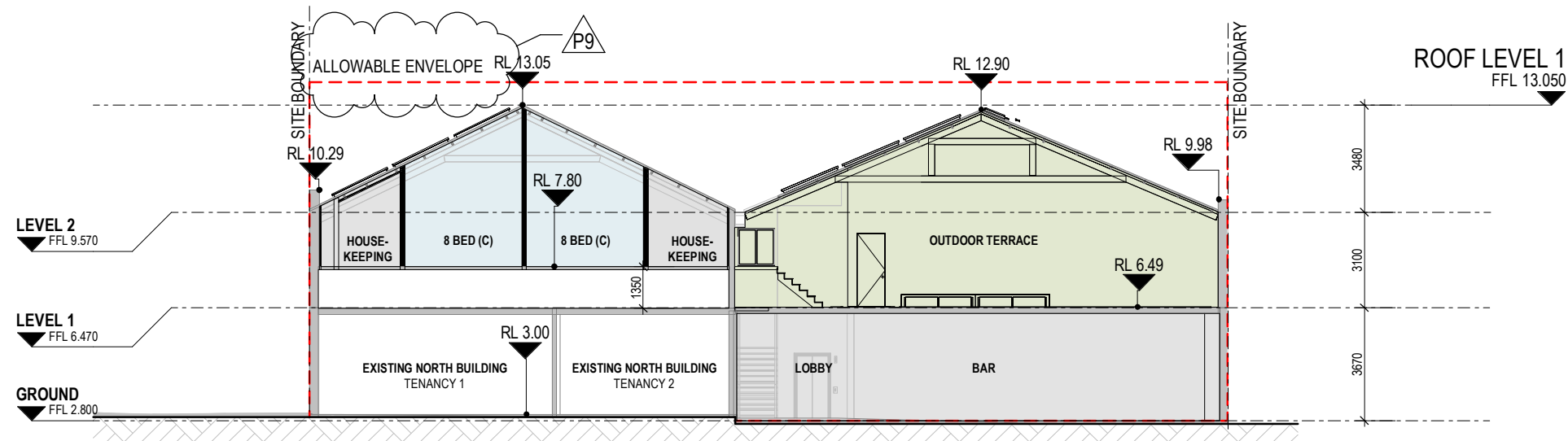
P9

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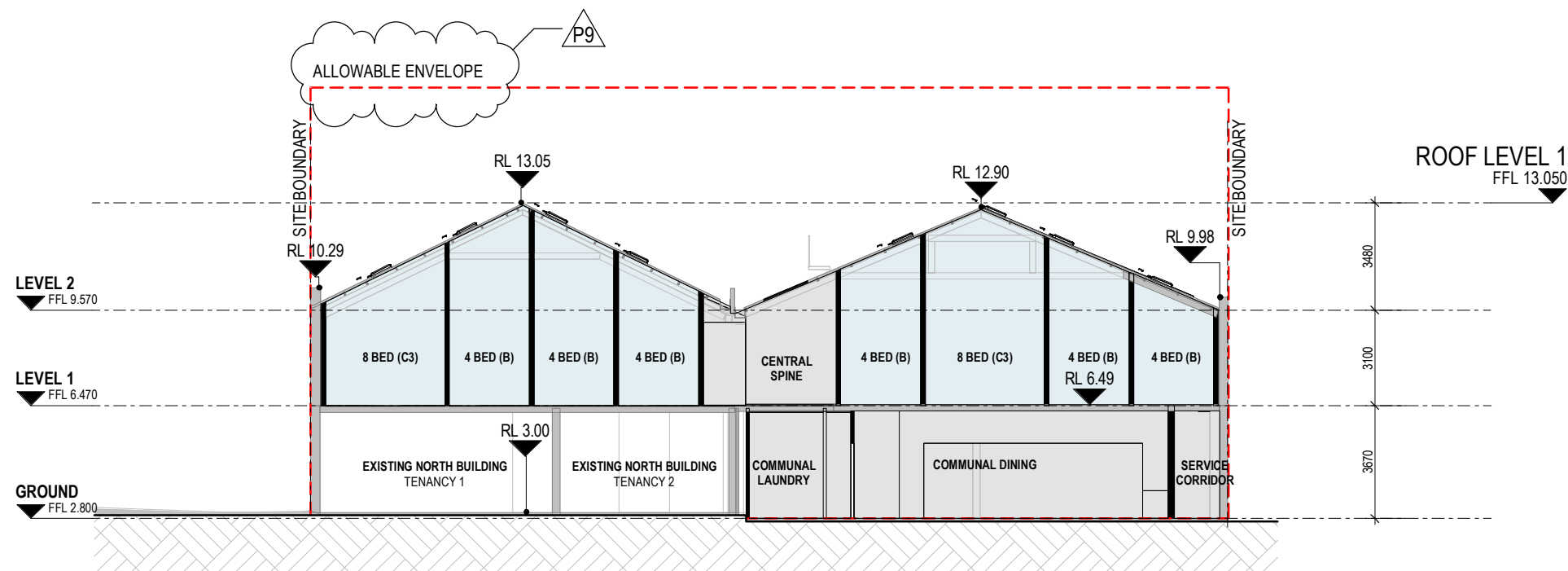
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Section AA



Section BB

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

ROOM LEGEND

- ROOMS
- OTHERS (NON-HABITABLE ROOMS)
- EXTERNAL SPACE

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Project / **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing / **Sections**

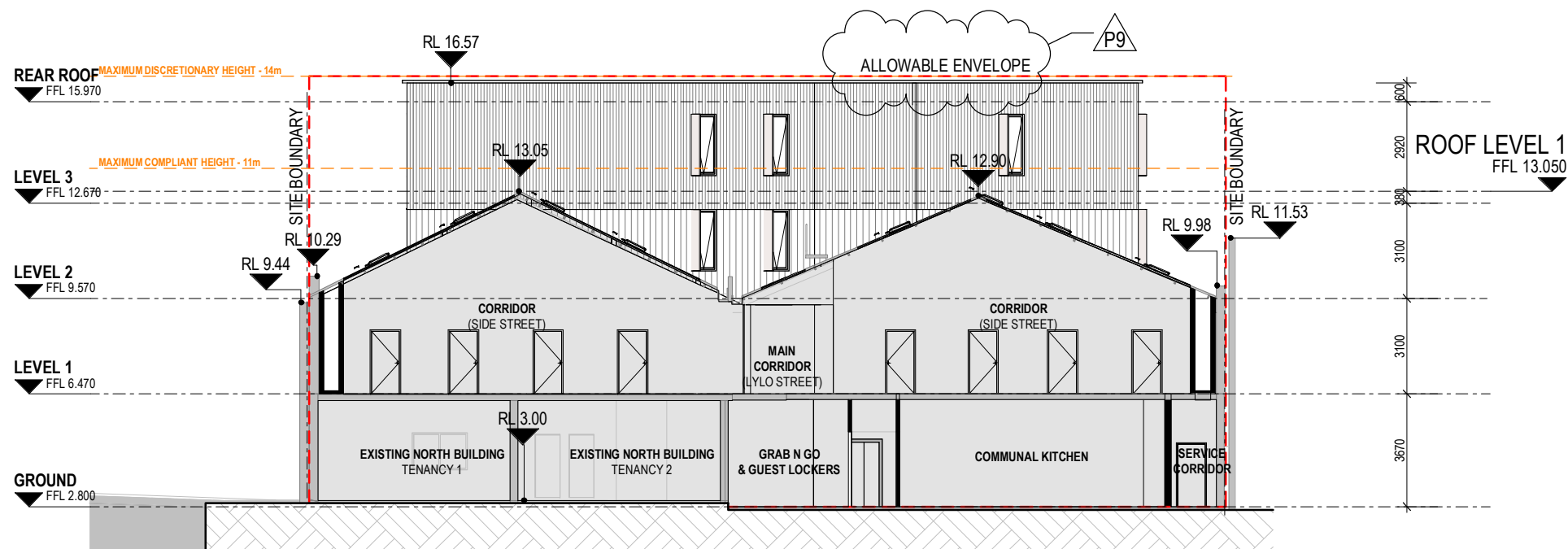
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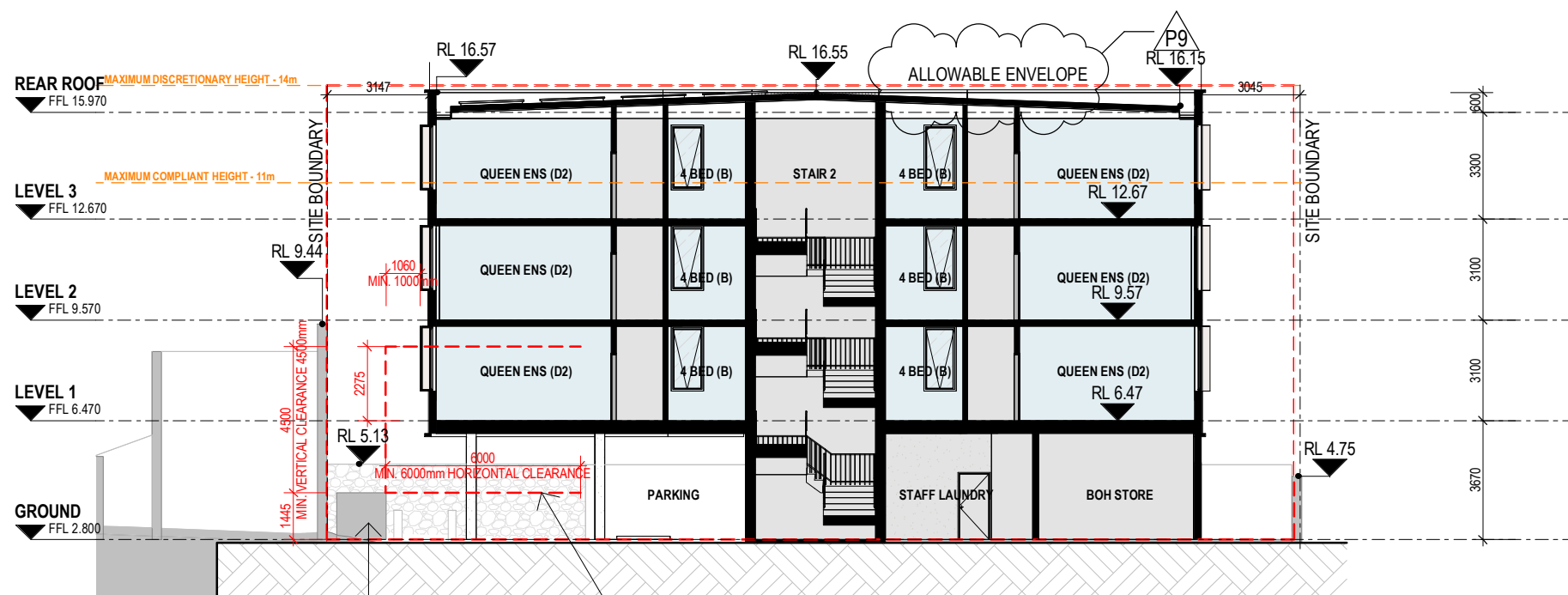
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Section CC



Section DD

WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

ROOM LEGEND

- ROOMS
- OTHERS (NON-HABITABLE ROOMS)
- EXTERNAL SPACE

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P9	19.08.2024	DA ISSUE RFI 1

Project / **LyLo Fremantle**

19 ESSEX STREET, FREMANTLE, WA

Drawing / **Sections**

Project No / **223252** Author / **AL**

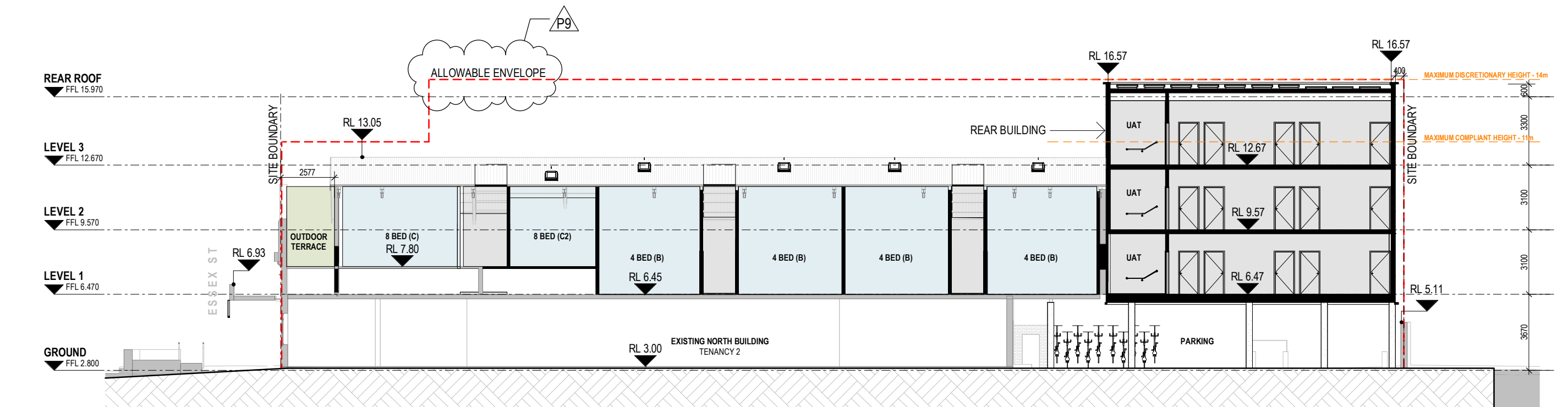
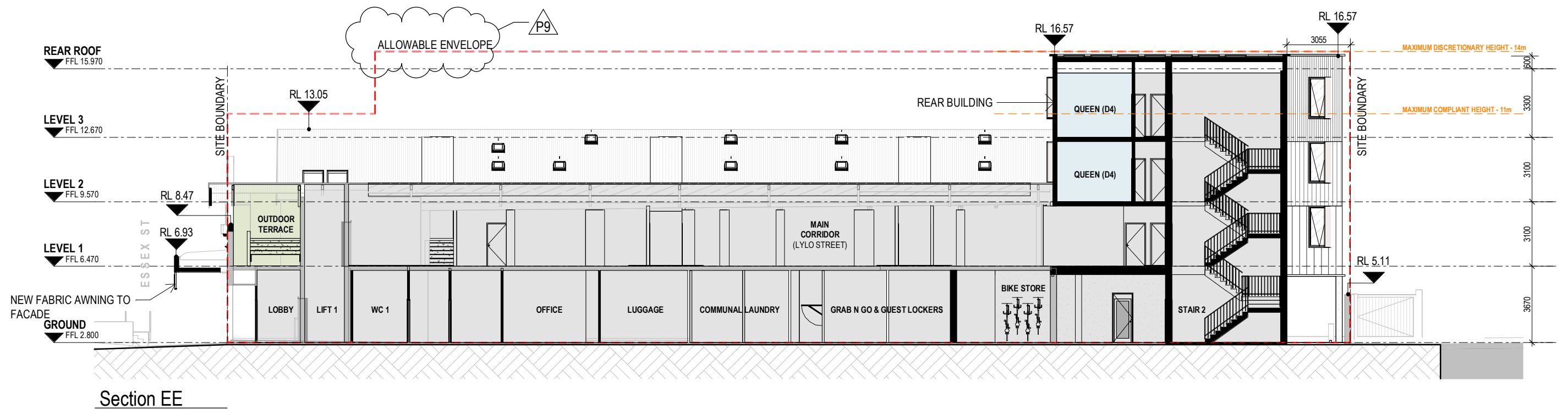
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Drawing No. / **DA03.02**

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WALL TYPE LEGEND

- EXISTING WALLS, FLOORS AND STRUCTURES TO BE RETAINED
- NEW WALLS, FLOORS AND STRUCTURES

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ROOM LEGEND

- ROOMS
- OTHERS (NON-HABITABLE ROOMS)
- EXTERNAL SPACE

Revisions		
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P9	19.08.2024	DA ISSUE RFI 1

Project **LyLo Fremantle**
19 ESSEX STREET, FREMANTLE, WA

Drawing **Sections**

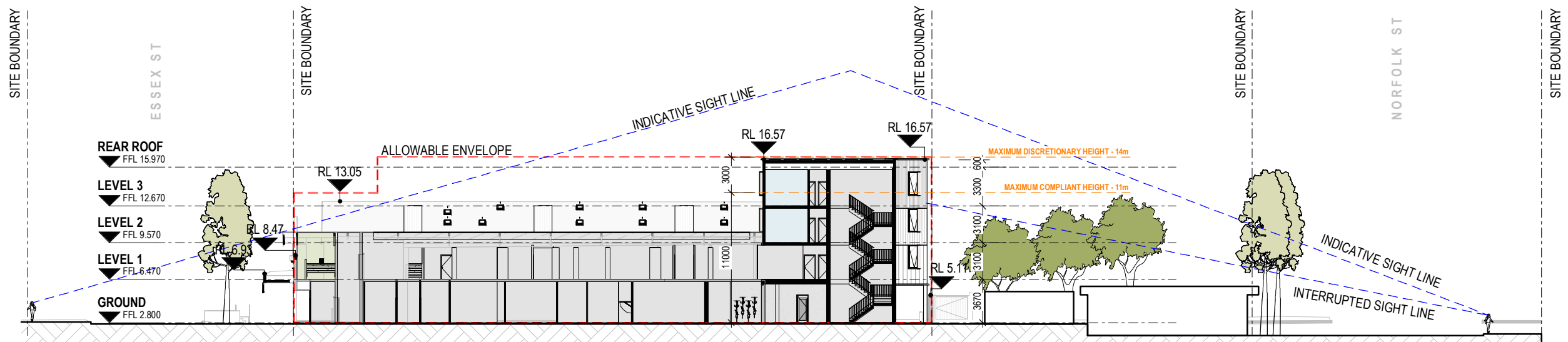
Project No **223252** Author **AL** Scale: @ A3 **1 : 200**

Drawing No **DA03.03**

P9

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P1



LOCAL PLANNING SCHEME NO. 4

- SCHEDULE 7 — LOCAL PLANNING AREAS (DEVELOPMENT REQUIREMENTS)
- 1.3.1 SPECIFIC DEVELOPMENT CONTROLS FOR SUB AREAS

Despite the general height requirements outlined in 1.1 above, building height shall be limited to a maximum height of three storeys (maximum external wall 69/143 height of 11* metres as measured from ground level with a maximum roof plain pitch of 33 degrees).

Council may consent to an additional storey subject to —

- Where a site meets any of the requirements of Clause 1A(a)-(e) of the deemed provisions, the upper level being sufficiently setback from the street so as to not be visible from the street(s) adjoining the subject site.
- maximum external wall height of 14* metres, and
- compliance with clause 1.2. above.

*Inclusive of roof parapet and spacing between floors.

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23 Aug 2024

Revisions / P1 19.08.2024 DA ISSUE RF1 1

Project / **LyLo Fremantle**

Drawing / **Wide Format Section**

Project No / **223252**

Author / **RL**

Scale: @ A3 / **1 : 400**

Drawing No. / **DA03.04**

P1

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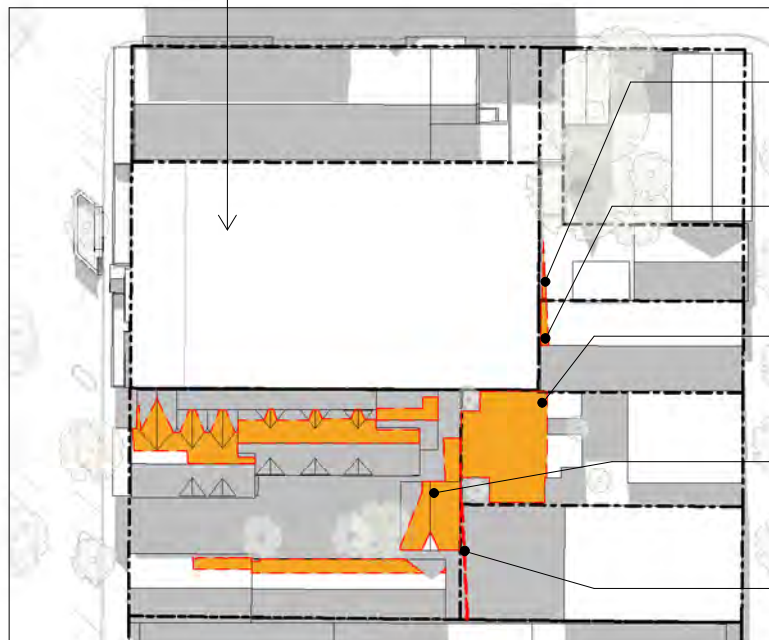
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19 ESSEX STREET



24 NORFOLK STREET
EXIS. SITE: 347m²
COMP. SHADOW: 152m²
COMP. SHADOW TO SITE
AREA RATIO: 26.2%

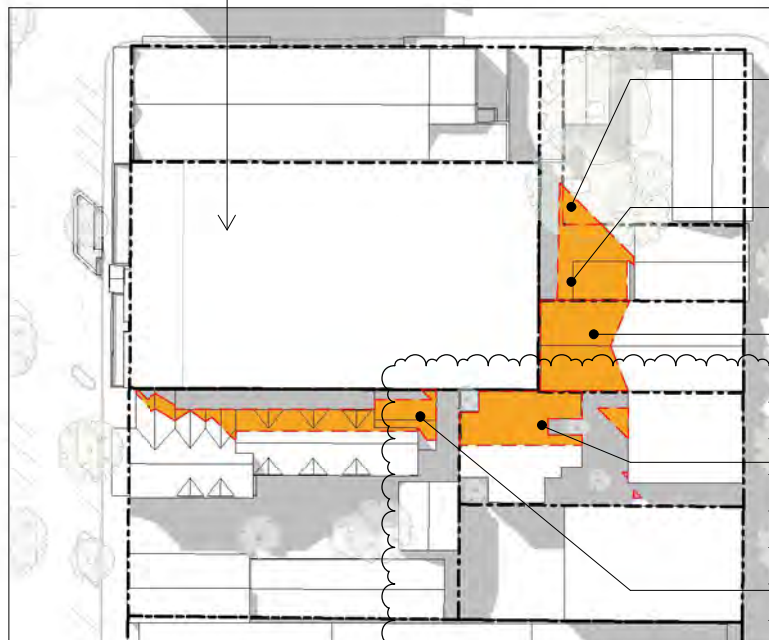
22 NORFOLK STREET
EXIS. SITE: 327m²
COMP. SHADOW: 152m²
COMP. SHADOW TO SITE
AREA RATIO: 26.2%

20 NORFOLK STREET
EXIS. SITE: 580m²
COMP. SHADOW: 152m²
COMP. SHADOW TO SITE
AREA RATIO: 26.2%

17 ESSEX STREET
EXIS. SITE: 1299m²
COMP. SHADOW: 277m²
COMP. SHADOW TO SITE
AREA RATIO: 21.3%

18 NORFOLK STREET
EXIS. SITE: 580m²
COMP. SHADOW: 2m²
COMP. SHADOW TO SITE
AREA RATIO: 0.3%

19 ESSEX STREET



26 NORFOLK STREET
EXIS. SITE: 556m²
COMP. SHADOW: 13m²
COMP. SHADOW TO SITE
AREA RATIO: 2.3%

24 NORFOLK STREET
EXIS. SITE: 347m²
COMP. SHADOW: 89m²
COMP. SHADOW TO SITE
AREA RATIO: 25.6%

22 NORFOLK STREET
EXIS. SITE: 327m²
COMP. SHADOW: 124m²
COMP. SHADOW TO SITE
AREA RATIO: 37.9%

20 NORFOLK STREET
EXIS. SITE: 580m²
COMP. SHADOW: 107m²
COMP. SHADOW TO SITE
AREA RATIO: 18.4%

17 ESSEX STREET
EXIS. SITE: 1299m²
COMP. SHADOW: 109m²
COMP. SHADOW TO SITE
AREA RATIO: 8.4%

19 ESSEX STREET



26 NORFOLK STREET
EXIS. SITE: 556m²
COMP. SHADOW: 34m²
COMP. SHADOW TO SITE
AREA RATIO: 6.1%

24 NORFOLK STREET
EXIS. SITE: 347m²
COMP. SHADOW: 175m²
COMP. SHADOW TO SITE
AREA RATIO: 50.4%

22 NORFOLK STREET
EXIS. SITE: 327m²
COMP. SHADOW: 282m²
COMP. SHADOW TO SITE
AREA RATIO: 86.2%

Compliant Mass Plan View - Winter Solstice 9am

Compliant Mass Plan View - Winter Solstice 12pm

Compliant Mass Plan View - Winter Solstice 3pm

19 ESSEX STREET PROPOSED REAR BUILDING



24 NORFOLK STREET
EXIS. SITE: 347m²
COMP. SHADOW: 0m²
COMP. SHADOW TO SITE
AREA RATIO: 0%

22 NORFOLK STREET
EXIS. SITE: 327m²
NEW SHADOW: 0m²
NEW SHADOW TO SITE
AREA RATIO: 0%

20 NORFOLK STREET
EXIS. SITE: 580m²
NEW SHADOW: 145m²
NEW SHADOW TO SITE
AREA RATIO: 25.0%

17 ESSEX STREET
EXIS. SITE: 1299m²
NEW SHADOW: 37m²
NEW SHADOW TO SITE
AREA RATIO: 2.8%

18 NORFOLK STREET
EXIS. SITE: 580m²
NEW SHADOW: 4m²
NEW SHADOW TO SITE
AREA RATIO: 0.7%

19 ESSEX STREET PROPOSED REAR BUILDING



26 NORFOLK STREET
EXIS. SITE: 556m²
NEW SHADOW: 1m²
NEW SHADOW TO SITE
AREA RATIO: 0.2%

24 NORFOLK STREET
EXIS. SITE: 347m²
NEW SHADOW: 67m²
NEW SHADOW TO SITE
AREA RATIO: 19.3%

22 NORFOLK STREET
EXIS. SITE: 327m²
NEW SHADOW: 98m²
NEW SHADOW TO SITE
AREA RATIO: 29.9%

20 NORFOLK STREET
EXIS. SITE: 580m²
NEW SHADOW: 62m²
NEW SHADOW TO SITE
AREA RATIO: 10.7%

17 ESSEX STREET
EXIS. SITE: 1299m²
NEW SHADOW: 0m²
NEW SHADOW TO SITE
AREA RATIO: 0%

19 ESSEX STREET PROPOSED REAR BUILDING



26 NORFOLK STREET
EXIS. SITE: 556m²
NEW SHADOW: 18m²
NEW SHADOW TO SITE
AREA RATIO: 3.2%

24 NORFOLK STREET
EXIS. SITE: 347m²
COMP. SHADOW: 149m²
COMP. SHADOW TO SITE
AREA RATIO: 42.9%

22 NORFOLK STREET
EXIS. SITE: 327m²
COMP. SHADOW: 206m²
COMP. SHADOW TO SITE
AREA RATIO: 62.9%

Proposed Works Plan View - Winter Solstice 9am

Proposed Works Plan View - Winter Solstice 12pm

Proposed Works Plan View - Winter Solstice 3pm

LEGEND

- COMPLIANT ENVELOPE SHADOW
- SHADOW CAST BY PROPOSAL
- ADDITIONAL SHADOW CAST BY COMPLIANT ENVELOPE

P5

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P5	19.08.2024	DA ISSUE RFI 1

Project

LyLo Fremantle

Drawing

Overshadowing Study

Project No

223252

Author

RL

Scale: @ A3

1 : 1000

Drawing No.

DA04.01

P5

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19 ESSEX STREET, FREMANTLE, WA

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PART C – TOWN OF BASSENDEAN

1. Declarations of Due Consideration

2. Disclosure of Interests

3. Form 1 DAP Applications

- 3.1 Lot 85 (No.94) West Road, Bassendean - Childcare Premises –
DAP/24/02721

4. Form 2 DAP Applications

Nil.

5. Section 31 SAT Reconsiderations

Nil.

Part C - Item 3.1 – LOT 85 (No. 94) WEST ROAD, BASSENDEAN- PROPOSED CHILD CARE PREMISES

Form 1 – Responsible Authority Report

(Regulation 12)

DAP Name:	Metro Inner DAP
Local Government Area:	Town of Bassendean
Applicant:	Briscola Pty Ltd
Owner:	Briscola Pty Ltd
Value of Development:	\$2.1 million
Responsible Authority:	Town of Bassendean
Authorising Officer:	Alex Snadden – Manager Planning and Regulation
LG Reference:	DA 2024-066
DAP File No:	DAP/24/02721
Application Received Date:	17 June 2024
Report Due Date:	20 November 2024
Application Statutory Process Timeframe:	90 Days (plus 79 additional days as agreed)
Attachment(s):	<ol style="list-style-type: none"> 1. Location Plan 2. Development Plans 3. Schedule of Submissions 4. Council Minutes (extract only) 5. Design Review Panel Advice 6. Town Planning Statement 7. Operational Management Plan 8. Transport Impact Statement (including addendums) 9. Independent Review of Transport Impact Statement (including addendums) relating to Site Access 10. Acoustic Assessment 11. Environmentally Sustainable Design Report 12. Arboricultural Report

Responsible Authority Recommendation

At the Ordinary Council Meeting of 19 November, 2024, the Town of Bassendean Council voted not to support the Officer Recommendation provided below as that of the Responsible Authority, for the following reasons:

Reasons for Responsible Authority Recommendation

- 1) The Council is not satisfied that the proposed use is compatible with or complementary to the surrounding residential zone under Local Planning Scheme No. 11, for the following reasons:
- 2) The proposed development results in a shortfall of four vehicle parking bays, as assessed against Local Planning Policy No. 8 – Car Parking and End-of-Trip Facilities, which is likely to adversely impact the amenity of the surrounding locality.

Details:

Region Scheme	Metropolitan Region Scheme
Region Scheme - Zone/Reserve	Urban
Local Planning Scheme	Town of Bassendean Local Planning Scheme No. 11 (LPS11)
Local Planning Scheme - Zone/Reserve	Residential (R20)
Use Class and permissibility:	Child Care Premises - 'A'
Lot Size:	1,258sqm
Existing Land Use:	Vacant land
State Heritage Register	No
Local Heritage	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Heritage List <input type="checkbox"/> Heritage Area
Design Review	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other
Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

This report relates to a proposed 'Child Care Premises' at Lot 85 (No. 94) West Road, Bassendean. The proposal involves:

- A Child Care Premises capable of accommodating up to 80 children (aged 0 to 5 years old) and 14 staff.
- Child care services to be provided between 7.15am and 6.30pm; Monday to Friday (excluding public holidays).



Above: proposed Child Care Premises

Background:

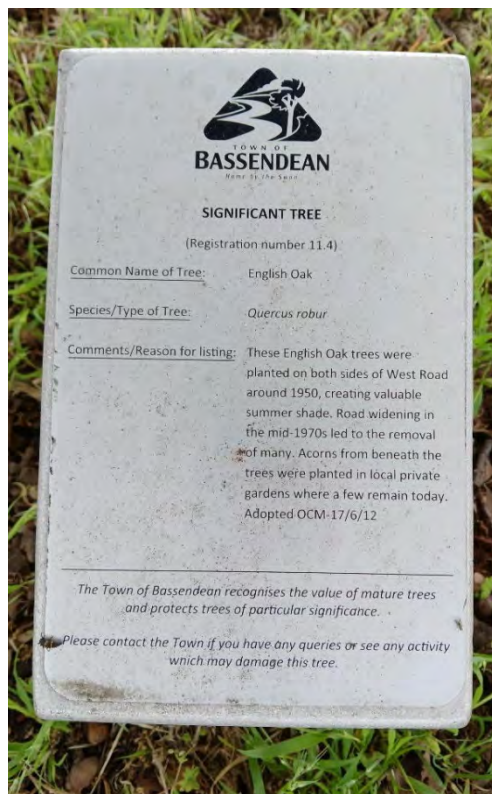
An application has been made to develop a purpose-built Child Care Premises at Lot 85 (No. 94) West Road, Bassendean. The applicant has opted for the application to be determined by the Metro Inner Development Assessment Panel (DAP).

Site context

The subject site is located at the corner of West Road and Bridson Street, Bassendean; approximately 600m south-east of the Bassendean Town Centre. The 1,258sqm site is vacant, regular in shape and relatively flat. The land is zoned 'Urban' under the Metropolitan Region Scheme (MRS) and 'Residential' (R20) under the Town of Bassendean Local Planning Scheme No. 11 (LPS11). Surrounding properties are zoned 'Residential' and comprise low-density housing. The site has several attributes considered desirable for child care premises, including its location at the corner of two local distributor roads, being free from identified risk of flood or bushfire, and its proximity to Bassendean Primary School, located 150m north of the site.

Streetscape Character

Built form within proximity to the site comprises a mix of federation, inter-war and post war housing set amongst later infill housing styles. West Road, in proximity to the site, is lined by street trees predominantly comprising mature English Oaks planted circa 1950. Collectively, the trees represent a pattern of planting that make a significant contribution to the streetscape character and aesthetics of West Road. The verge abutting the eastern boundary of the site contains four street trees, including three English Oaks. All of the street trees can be retained as part of the development, subject to modifications to the access arrangements as discussed in detail below.



Above: English Oaks within the West Road verge abutting the site.

Legislation and Policy:

Legislation

- Planning and Development Act 2005
- Planning and Development (Local Planning Scheme) Regulations 2015
- Planning and Development (Development Assessment Panels) Regulations 2011
- Metropolitan Region Scheme
- Town of Bassendean Local Planning Scheme No. 11

State Government Policies

- State Planning Policy 7.0 – Design of the Built Environment

Local Planning Policies

- Local Planning Policy No. 8 - Car Parking and End of Trip Facilities
- Local Planning Policy No. 9 - Design Review Panel
- Local Planning Policy No. 12 – Residential Development and Fences
- Local Planning Policy No. 14 - Stormwater
- Local Planning Policy No. 15 - Public art
- Local Planning Policy No. 16 - Advertising and signage

Strategic Planning Framework

- Town of Bassendean Local Planning Strategy

Other

- Draft Position Statement – Child Care Premises (WAPC)
- AS/NZS 2890.1:2004 Parking facilities – Off-street car parking

Consultation:

Public Consultation

The application was advertised for 14 days (between 21 June and 5 July 2024). Consultation involved:

- Notification letters sent to owners and occupiers of properties within 100m of the site and with frontage to West Road, Bridson Street and/ or Hyland Street;
- Signs displayed onsite
- Notification on the Town's website and social media platforms
- Hard copies made available for inspection at the Town's customer service office and library.

64 written submissions were received during the consultation period, including:

- 61 objections (including 1 petition); and
- 3 submissions in support

A schedule of submissions is included as **Attachment 3** to this report.

Revised plans and additional information were lodged following the close of the advertising period. Further consultation was not carried out.

Key issues raised in submissions are as follows:

Issue Raised	Officer comments
Land use	Land use is discussed in the planning assessment section below.
Traffic	Traffic is discussed in the planning assessment section below.
Parking	2 additional parking bays have been included since the application was advertised. Parking is discussed in the planning assessment section below.
Access	Access is discussed in the planning assessment section below.
Built form	Built form is discussed in the planning assessment section below.
Waste management	Waste management is discussed in the planning assessment section below.
Supply and demand for child care services	Of those submissions that made reference to the availability of local child care services, both an undersupply and oversupply was noted. As per the WAPC Draft Position Statement – Child Care Premises, a proponent is not required to demonstrate sufficient demand exists for a child care facility.
There are other, more suitable locations	The suitability of alternative sites is not a relevant planning consideration. This application is required to be determined on its own merit.
Alternative uses of the site are preferred (ie. residential)	The preference of an alternative use is not a relevant planning consideration. This application is required to be determined on its own merit.
Site safety and security	The proposal is not considered to present a safety risk to the community.
Impact on property values	Impact on property values is not a relevant planning consideration.

Referrals/consultation with Government/Service Agencies

The application was referred to the Water Corporation, who own property within proximity to the site. Comments received confirm the site is adequately serviced by reticulated water and sewage services.

Design Review Panel Advice

The proposal was reviewed by the Town's Design Review Panel (DRP) on 4 occasions. Following each review, the applicant made changes in response to the Panel's recommendations. On 8/11/2024, the DRP Chair confirmed the Panel supports the proposal against all 10 principles of good design as outlined in State Planning Policy No. 7 – Design of the Built Environment; subject to the inclusion of a heat pump and onsite rainwater storage, which are recommended in accordance with a condition of approval.

Meeting minutes of each Design Review are included as **Attachment 5** to this report. A summary of the DRP and Chair's evaluation of the proposal at each review is provided in **Table 1** below.

Table 1: Summary of DRP Advice

	DR1 9/2/24	DR2 12/7/24	DR3 25/9/24	DR4 8/11/24
P1 – Context and character				
P2 – Landscape quality				
P3 – Built form and scale				
P4 – Functionality and build quality				
P5 – Sustainability				
P6 – Amenity				
P7 – Legibility				
P8 – Safety				
P9 – Community				
P10 - Aesthetics				

Planning Assessment:

The proposal has been assessed against relevant legislative requirements as outlined above. Matters identified as key considerations for the determination of the application are discussed in detail below.

Land use

The subject site is zoned 'Urban' under the MRS, and 'Residential' (R20) under LPS11. 'Child Care Premises' is an 'A' use in the zone, which means the use is not permitted unless the local government (or in this instance the Metro Inner DAP) has exercised discretion by granting development approval following advertising.

In considering whether a 'Child Care Premises' is an appropriate use of the site, the following objectives of the 'Residential' zone as outlined in LPS11 are relevant:

- a) *To provide for a range of housing and a choice of residential densities to meet the needs of the community.*
- b) *To facilitate and encourage high quality design, built form and streetscapes throughout residential areas.*
- c) *To provide for a range of non-residential uses, which are compatible with and complimentary to residential development.*

As per the LPS11 zoning table and zone objectives, non-residential land uses may be considered within the residential zone where such developments demonstrate high quality design and built form outcomes; the ongoing use will not adversely impact on the residential character of the locality; and reasonable expectations of residential amenity are maintained. The Western Australian Planning Commission's 'Draft Position Statement: Child Care Premises' ('WAPC Draft Position Statement') provides guidance to decision-makers in this regard; identifying impacts of noise, traffic, parking and pedestrian safety as key considerations. These matters are discussed below.

Design quality

The applicant has engaged with the Town's DRP on 4 separate occasions. Following each review, revised plans and additional information have been submitted to address the Panel's recommendations. The proposal, as presented for determination, includes significant improvements to the initial submission, including improved architectural treatments to the street facing building facades, redesigned elements to reduce building bulk as viewed from adjoining residences, confirmed sustainability commitments, improved universal access and landscape screening to the car park. The proposal, as presented for determination, is supported by the Town's DRP against all 10 principles of good design as outlined in State Planning Policy 7.0 – Design of the Built Environment, subject to the inclusion of onsite rainwater storage and a heat pump hot water system to reflect the sustainability initiatives detailed in the Environmentally Sustainable Design (ESD) Report (prepared by Thermarate, dated 31/10/2024) and included as **Attachment 11** to this report.

Built form

In order to ensure non-residential development in the residential zone is consistent with the expectations of the size, scale and siting of development in a residential area, clause 32 (1) of LPS11 stipulates that new non-residential development within the zone is to comply with the provisions of the Residential Design Codes Volume 1 Part B (R-Codes) with respect to building setbacks, height and open space. Compliance with the R-Codes may be demonstrated by:

- adherence to prescribed 'deemed-to-comply' development controls, or
- a merit-based assessment of a proposal met against relevant 'design principles'.

The following elements of the proposed development require a merit-based assessment against the design principles of the R-Codes:

a) Primary street setback

Buildings are generally required to be setback 3m from primary street boundaries. The gatehouse is proposed with a nil setback to the primary street boundary (West Road).

Design Principles	Comment
P2.1 Buildings set back from street boundaries an appropriate distance to ensure they: <ul style="list-style-type: none"> • contribute to, and are consistent with, an established streetscape; • provide adequate privacy and open space for dwellings; • accommodate site planning requirements such as parking, landscape and utilities; and • allow safety clearances for easements for essential service corridors. 	The gate house is a small, open sided structure that presents as a feature of the overall design. The gatehouse positively contributes to the overall aesthetics of the development without detracting from the established character of the streetscape. The structure does impact on privacy, reduce the provision of open space, or landscaping onsite and therefore, can be supported against the design principles.

<p>P2.2 Buildings mass and form that:</p> <ul style="list-style-type: none"> • uses design features to affect the size and scale of the building; • uses appropriate minor projections that do not detract from the character of the streetscape; • minimises the proportion of the façade at ground level taken up by building services, vehicle entries and parking supply, blank walls, servicing infrastructure access and meters and the like; and • positively contributes to the prevailing or future development context and streetscape as outlined in the local planning framework. 	
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b) Secondary street setback

The Town of Bassendean Local Planning Policy No. 12 – Residential Development and Fences, provides an alternative ‘deemed-to-comply’ development standard to allow verandahs with a nil setback to a secondary street, subject to eaves, gutters and roofs being setback at least 450mm from the lot boundary. The verandah proposed adjacent to the Bridson Street lot boundary (including the eaves, gutter and roof) has a nil setback to the secondary street.

Design Principle	Comment
<p>P2.1 Buildings set back from street boundaries an appropriate distance to ensure they:</p> <ul style="list-style-type: none"> • contribute to, and are consistent with, an established streetscape; • provide adequate privacy and open space for dwellings; • accommodate site planning requirements such as parking, landscape and utilities; and • allow safety clearances for easements for essential service corridors. <p>P2.2 Buildings mass and form that:</p> <ul style="list-style-type: none"> • uses design features to affect the size and scale of the building; • uses appropriate minor projections that do not detract from the character of the streetscape; • minimises the proportion of the façade at ground level taken up by building services, vehicle entries and parking supply, blank walls, servicing infrastructure access and meters and the like; and 	<p>Setting off the verandah eaves, gutter and roof to comply with the policy will detract from the overall aesthetics of the development and its contribution to the streetscape. The verandah is unenclosed and does not detract from the Bridson Street streetscape and therefore, can be supported against the design principles of the R-Codes.</p>

<ul style="list-style-type: none"> • positively contributes to the prevailing or future development context and streetscape as outlined in the local planning framework. 	
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c) Boundary wall height

Boundary walls with a maximum height of 3.5m are generally permitted. The boundary wall abutting the western lot boundary has maximum height of 3.6m.

Design Principle	Comment
<p>P3.2 Buildings built up to boundaries (other than the street boundary) where this:</p> <ul style="list-style-type: none"> • makes more effective use of space for enhanced privacy for the occupant/s or outdoor living areas; • does not compromise the design principle contained in clause 5.1.3 P3.1; • does not have any adverse impact on the amenity of the adjoining property; • ensures sunlight to major openings to habitable rooms and outdoor living areas for adjoining properties is not restricted; and • positively contributes to the prevailing or future development context and streetscape as outlined in the local planning framework. 	<p>The boundary wall is proposed adjacent to the outdoor living area of the house on the adjoining property (4 Bridson Street). Building to the boundary promotes privacy of this space and assists in mitigating impacts of noise associated with the child care premises.</p> <p>The wall length is consistent with what is to be expected in a low density (R20) residential area; and the 0.1m excess wall height is minor, and can be supported against the relevant design principles of the R-Codes noting the orientation of the subject and adjoining site does not result in an overshadowing concern, and the fact the wall is not visible from the street.</p>

d) Building height

The proposal complies with the maximum wall and roof height prescribed for residential development, with the exception of the patio to the upper-level play deck, which has a wall height of 7.6m, in lieu of the 7.0m permitted.

Design Principle	Comment
<p>P6 Building height that creates no adverse impact on the amenity of adjoining properties or the streetscape, including road reserves and public open space reserves; and where appropriate maintains:</p> <ul style="list-style-type: none"> • adequate access to sunlight into buildings and appurtenant open spaces; • adequate daylight to major openings into habitable rooms; and • access to views of significance. 	<p>The patio is set back from lot boundaries so as not to impact the amenity of adjoining properties or restrict access to sunlight to buildings and appurtenant open spaces.</p> <p>Although the wall height of the patio is the same as the remainder of the upper floor, it technically represents a variation to the deemed-to-comply provisions of the R-Codes, noting the patio is to be constructed over a portion of the site that naturally lies lower than the remainder. The design of the patio has been guided by the DRP to simplify and balance the</p>

	overall design of the development to improve the aesthetic appeal of the overall development as viewed from the street and can be supported against the relevant design principles of the R-Codes.
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Noise

The impact of noise on nearby residential uses is a key consideration in determining whether the proposed Child Care Premises is compatible with surrounding residential development; and therefore an appropriate use within the residential zone. An Acoustic Assessment prepared by ND Engineering (Revision 7; dated 7/11/2024) included at **Attachment 10** to this report, provides details of expected noise levels associated with children playing outside, the use of the car park and the operation of mechanical plant and equipment. The assessment recommends a range of physical and operational management measures to ensure the Child Care Premises will not adversely impact the amenity of residence by virtue of excessive noise. Those relevant to the application under consideration are as follows:

Physical noise barriers

- 2.1m high fencing to western lot boundary, comprising a standard 1.8m high colorbond fence below a 0.3m clear polycarbonate topper
- 2.1m high colorbond fencing to northern lot boundary, plus crank wall extending 1.5m within the lot boundary
- 2.1m high fence to upper floor play deck, inclusive of a clear polycarbonate backing to infill panels with a surface density of 15kg/sqm
- 1.8m high perimeter fencing to West Road and Bridson Street lot boundaries, inclusive of a clear polycarbonate backing to infill panels with a surface density of 15kg/sqm
- Air conditioning units being located in accordance with the recommendation of an acoustic assessment submitted in conjunction with a building permit application
- Signage being displayed within carpark to requesting staff/parents not to slam car doors or play loud music

Operational management measures

- Hours of operation being limited to between 7.00am and 7.00pm; Monday to Friday (excluding public holidays), with staff arrivals/ departures also being limited to between 7.00am and 7.00pm only;
- Management of play areas, limiting 'free play' of older children to the upper level play deck only.

As outlined in the Operational Management Plan (included as **Attachment 7**), it is proposed the Child Care Premises will operate between 7.15am and 6.30pm, allowing staff arriving at 7.00am to open the centre prior to the arrival of children. Adherence to the Operational Management Plan is recommended as a condition of approval.

It is noted that it is the obligation of the centre operator to ensure noise emissions from the Child Care Premises, as received at nearby sensitive receivers, do not exceed those recommended by the *Environmental Protection (Noise) Regulations 1997*. The Town is satisfied, based on the information provided, that the Child Care Premises can comply with assigned noise levels and therefore, will not unreasonably impact on the residential character of the locality, or amenity of adjoining residential uses subject to the implementation of physical and operational noise management measures being put in place.

Traffic

The site is located on the corner of West Road and Bridson Street, both of which are designed and designation as 'local distributor' roads designed to carry up to 6,000 vehicles per day. The Child Care Premises is expected to generate up to 348 vehicle movements per day. The Town is satisfied the local road network has the capacity to accommodate the expected increase in local traffic, noting recent traffic counts provided in the addendum to the Transport Impact Statement (TIS) prepared by KCTT (Revision C dated 7/6/2024), included as **Attachment 8**.

Parking

The proposal involves 18 onsite parking bays, including 14 tandem bays and 1 ACROD bay. The onsite parking provision complies with the car parking rates recommended by the WAPC Draft Position Statement, however represents a 4-bay parking shortfall when considered against the requirements of the Town of Bassendean Local Planning Policy No. 8 – Parking and End of Trip Facilities.

Provision	Requirement	Proposal
Town of Bassendean Local Planning Policy No. 8 – Parking and End of Trip Facilities	22 parking bays	18 parking bays (4 bay shortfall)
WAPC Draft Position Statement – Child Care Premises	16 onsite car parking bays	18 parking bays (2 bay surplus)

The Transport Impact Statement prepared by KKCT (Revision D; dated 31/10/2024) provides details of expected demand for onsite parking facilities during peak pickup and drop off periods based on data collected from operational centers. The Statement supports the provision and functionality of the parking facilities proposed, subject management of the car park as outlined in the Operational Management Plan included as **Attachment 7** to this report. Based on this, it is considered that both the provision, and layout of the car parking bays proposed is sufficient to accommodate peak demand of the centre. It is recommended some of the parking bays be marked 'staff only', noting the tandem arrangement is not functional for parent drop-off/ pick-up.

The development also provides 4 secure bicycle parking facilities and access to shower and change room facilities for those staff members who choose to cycle to the centre.

Vehicle access

Vehicular access to the site is proposed via a 6m wide crossover to West Road. The following are relevant in considering appropriate access arrangement for this site:

- Desire to retain street trees within the West Road verge
- Obstructions to sightlines of drivers entering West Road
- The proximity of the vehicular access point to the West Road/ Hyland Street intersection and the recommended 'restricted area' as outlined in Australian Standard AS2890.01- Parking Facilities Part 1: Off-street car parking (AS2890.01)

Street tree retention

The applicant has submitted an Arboriculture Report (prepared by ArborCulture Australia Pty Ltd; dated 24/6/2024) in support of the application. The report is included as **Attachment 12** to this report and includes a construction impact assessment which demonstrates the mature English Oak tree within proximity to the proposed access point to the site can be retained, subject the implementation of measures outlined in the tree protection plan (also provided within the report). Town staff are satisfied that subject to the implementation of the tree protection plan, the access arrangement proposed will not adversely impact on the long-term health of the tree.

Obstruction to driver sightlines

Sightlines at the vehicular access point are non-compliant with the minimum sightline distances recommended by AS2890.01, as two of the four street trees within the West Road verge will obstruct driver sightlines when exiting the site. In a letter dated 30/9/2024 (included in **Attachment 8**), the applicant's traffic consultant described driver's exit from the site as follows:

"the driver will slowly exit and will check the road for on-coming traffic as they are crossing the property line... as the driver approaches the carriageway, and prepares to execute turning maneuver, they will be able to check the road again as the sightline is uninterrupted".

The applicant's traffic consultant concludes the 'rolling approach' described above will allow drivers to easily spot oncoming traffic and therefore, are highly unlikely to be exposed to any undue safety risks.

The Town commissioned an independent review of the transport impact statement and subsequent advice by the applicant's traffic consultant. The independent traffic consultant's advice, included at **Attachment 9**, confirms they disagree with the conclusion that the obstructions to sightlines do not present a safety concern. In order to retain the trees, while also maintaining adequate driver sightlines, the consultant recommends nibs be installed on West Road within proximity to the site,

narrowing the carriageway by 1.5m to 2m. This will ensure drivers exiting the sight will have a clear line of sight in both directions without necessitating the removal of street trees. Town staff agree with the recommendation of the independent traffic consultant as outlined in **Attachment 9**, and recommend nibs be installed within the West Road carriageway, and any associated road line-marking be carried out. All works are to be completed at the developers cost and to the specifications of the Town.

Proximity to West Road/ Hyland Street intersection

AS2890.01 recommends crossovers not be located within close proximity to intersections. Specifically, the standard prescribes a 'restricted area', whereby vehicle access points are not recommended within 6m of the tangent point of an intersection. The access arrangement proposed does not comply with this Australian Standard.

The applicant's traffic consultant, noting the non-compliance with the Australian Standard, provided an assessment of the proposed access location using a 'Safe Systems Framework', included in **Attachment 8** to this report. The safety implications of the non-compliance were considered in the context of alternative (compliant) options for vehicle access to the site. The findings suggest that despite the non-compliance, the access arrangement proposed meets the 'Safe System' principles and would not result in a situation that is less safe than alternative, compliant options for vehicular access to the site.

The independent traffic consultant highlighted that due to obstructions to sightlines described above, the access arrangement proposed does not represent a safe option; however subject to the installation of nibs within the West Road carriageway to allow exiting vehicles to pull further forward and achieve adequate sight distance, the crossover location is considered safe and can be supported (refer to **Attachment 9**).

Waste Management

A private waste collection is intended to service the Child Care Premises. The onsite waste storage facilities proposed are suitable to accommodate the amount of waste expected to be generated onsite and fully enclosed, provided with a hose-cock and floor waste to manage/contain odour.

Signage

Signage complies with the Town's Local Planning Policy No. 16 – Advertising and Signage, subject to signs being flush mounted or painted onto the fence. This is recommended as a condition of approval.

Public Art

In accordance with the Town of Bassendean Local Planning Policy No. 15 – Public Art, a developer is required to make a public art equivalent to 1% of the estimated cost of development. The public art liability may be made in the form of provision of art on the development site, or a cash contribution. The applicant has not yet advised of the intended method of satisfying the policy requirements. A condition of approval is recommended to ensure the public art liability is met.

Conclusion:

Child Care services provide an essential service to the community and convenient access to quality child care services is crucial in meeting the needs of children and their families. Child care premises are commonly found in both commercial and residential zones. In considering whether a child care premises is appropriate in a residential zone, the impact on the streetscape, and residential character and amenity of the locality are key considerations.

As discussed in the report, the expected impacts of the Child Care Premises on surrounding residential land uses have been demonstrated, and where necessary, appropriate measures have been identified to ensure the operation of the Child Care Premises does not have an unreasonable adverse impact on residential character and amenity. It is therefore recommended the application be approved, subject to conditions.

Officer Recommendation

It is recommended that the Metro Inner Development Assessment Panel resolves to:

1. **Approve** DAP Application reference DAP/24/027201 and accompanying plans in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of the Town of Bassendean Local Planning Scheme No. 11, subject to the following conditions:

Conditions:

General:

1. Pursuant to clause 26 of the Metropolitan Region Scheme, this approval is deemed to be an approval under clause 24(1) of the Metropolitan Region Scheme.
2. This decision constitutes planning approval only and is valid for a period of 4 years from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect.
3. This approval is for a 'Child Care Premises' as defined by the Town of Bassendean Local Planning Scheme No. 11.

Operational:

4. The Child Care Centre shall operate in accordance with the Operational Management Plan, prepared by Lateral Planning – Revision 1 (dated 4/11/2024), at all times.
5. The 'Child Care Premises' shall accommodate a maximum of 80 children at any one time.
6. No more than 14 staff are permitted to be present on site at any one time.

7. The Child Care Premises is permitted to operate between 7.15am and 6.30pm, Monday to Friday only. The Child Care Premises is not permitted to operate on Public Holidays.
8. Staff are not permitted to arrive onsite prior to 7.00am; and must vacate the site prior to 7.00pm.

Noise:

9. Prior to the initial occupation or use, noise barriers and acoustic treatments shall be installed in accordance with Figures 5.2a; 5.2b and 5.2c of the Acoustic Assessment prepared by ND Engineering – Revision 7 (dated 7/11/2024). Noise barriers and acoustic treatments must be maintained onsite for the life of the development and to the satisfaction of the Town of Bassendean. Details of noise barriers and acoustic treatments shall be depicted on plans submitted with an application for a building permit.
10. Air conditioning units shall be located in accordance with the recommendations of an acoustic assessment submitted to, and approved by, the Town of Bassendean prior to the submission of an application for a building permit application.

Built form:

11. The external finish of the boundary wall must match/ compliment the remainder of the external appearance of the development to the satisfaction of the Town of Bassendean.
12. All works, including earthworks and footings, are to be contained within the boundaries of the subject lot.

Engineering:

13. Stormwater must be contained and disposed of onsite. If ground conditions are deemed unsuitable for onsite disposal via infiltration (as verified by a geotechnical investigation), a combination of utilising the Town's stormwater drainage system and containment on site will be permitted at the landowners/ developers cost. Prior to the submission of an application for a building permit, a stormwater management plan providing details and calculations must be submitted for approval by the Town of Bassendean.

Parking and access

14. Prior to the initial occupation or use, 18 car parking bays must be provided onsite in accordance with the approved plans. Car parking bays and manoeuvring spaces must comply with Australian Standard AS/NZ 2890.1: 2004 Parking facilities – Off-street car parking. Accessible parking bay/s must comply with AS/NZ 2890.6: 2009 Parking facilities – Off-street parking for people with disabilities and be maintained onsite for the life of the development.
15. Prior to the initial occupation or use, 4 bicycle parking bays must be provided onsite in accordance with the approved plans. Bicycle parking facilities shall comply with AS/NZ 2890.3:2015 – Parking Facilities Part 3: Bicycle Parking and be maintained onsite for the life of the development.

16. Prior to the initial occupation or use, vehicle parking, access and circulation areas must be sealed, kerbed, drained and line marked in accordance with the approved plans to the satisfaction of the Town of Bassendean.
17. Prior to the initial occupation or use, parking bays depicted as bay 2, 4, 6, 8 and 9 to 14 on approved plans shall be marked as 'staff-only' bays and remain that way for the life of the development.
18. Prior to the submission of an application for a building permit, engineering drawings must be submitted to, and approved by the Town of Bassendean, detailing the construction of "solid nibs" and line marking within the 'West Road' road reserve and in proximity to the subject site to ensure entering sight distances are compliant with Australian Standard AS2890.01 Parking facilities Part 1: Off-street car parking standards. Detailed engineering drawings are to be accompanied by an Arborist report detailing the impact the works are likely to have on trees within the 'West Road' verge and any measures recommended to ensure the trees are not damaged as the result of works.
19. Prior to the initial occupation or use of the development the applicant/landowner must construct solid nibs and complete line marking within the 'West Road' road reserve in accordance with the approved engineering drawings and recommendations of an Arborist Report. All works are to be completed at the applicant/ landowner's cost and to the satisfaction of the Town of Bassendean.
20. Prior to initial occupation or use, redundant crossovers shall be removed and kerbing, verge, and the footpath reinstated with grass or landscaping to the satisfaction of the Town of Bassendean.
21. The crossover shall be constructed in accordance with the approved plans and Town of Bassendean's Crossover Policy Specifications.

Landscaping

22. No wall, fence or landscaping greater than 0.75m in height (measured from the natural ground level at the street boundary), shall be constructed or installed within 1.5m of a vehicular accessway (including vehicular accessways to adjoining sites).
23. Prior to the commencement of use landscaping is to be installed in accordance with the approved landscaping plan and thereafter maintained for the life of the development to the satisfaction of the Town of Bassendean.
24. The Construction Impact Mitigation Methods (Section 5.1) and Tree Protection Plan (Section 6) detailed in the Preliminary Arboriculture Report prepared by ArborCulture Australia Pty Ltd (Revision 1 dated 24/6/2024) shall be implemented at all times, to the satisfaction of the Town of Bassendean.
25. In the event a street tree within the West Road verge abutting the site is damaged, removed or suffers irreversible effects to its health as a result of the development, and within the first three years after completion of the development, the applicant/ owner will be liable to pay the Town of Bassendean for the value of the tree (based on the Burnley Method of Amenity Tree Evaluation - Moore, 2006), plus the cost of the removal of the tree.

Miscellaneous:

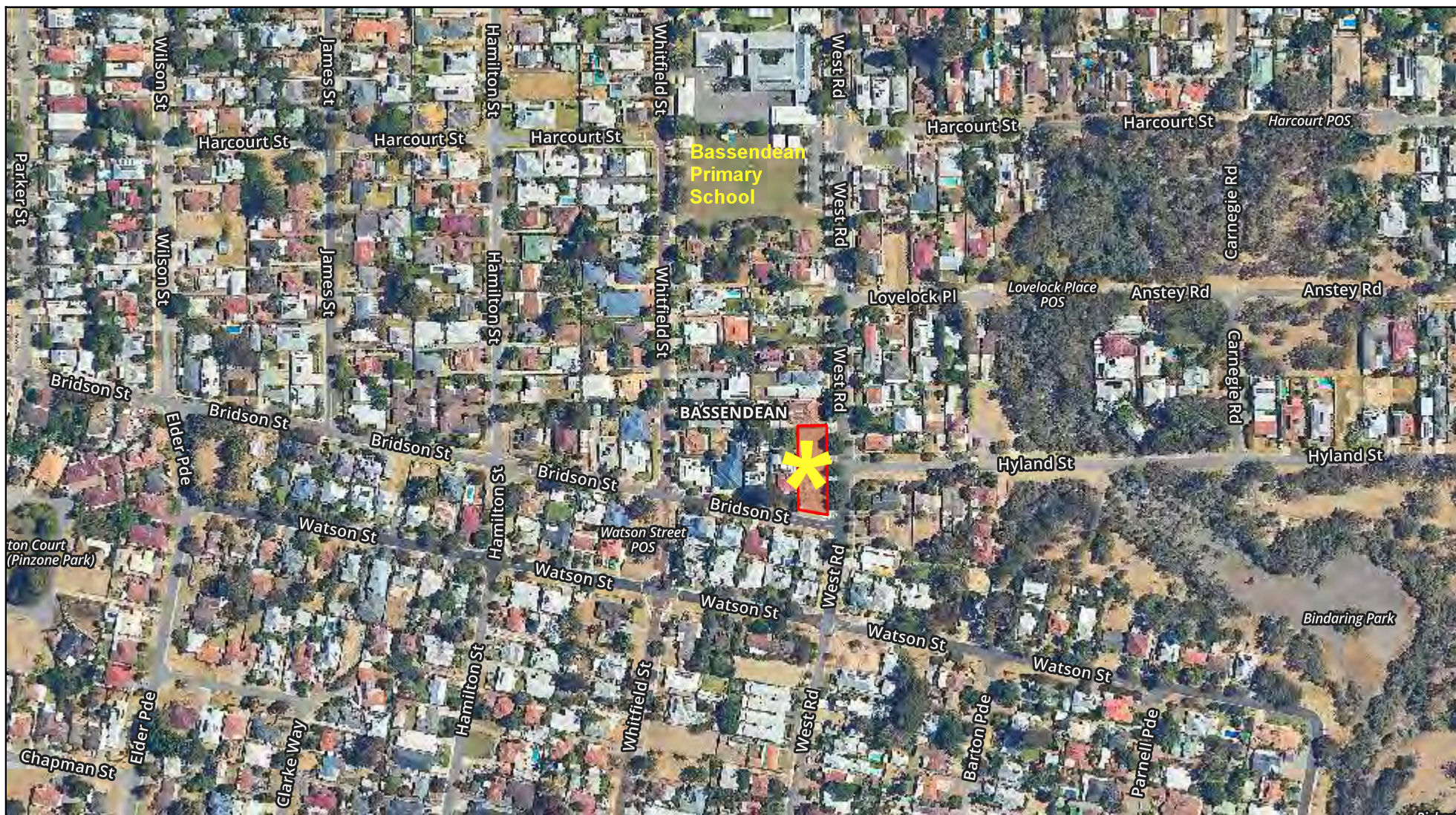
26. In accordance with the Environmentally Sustainable Design (ESD) Report (prepared by Thermarate; Version 4; dated 31/10/2024), onsite rainwater storage and a heat pump hot water systems are to be installed onsite prior to the commencement of use. Details of onsite rainwater storage and a heat pump hot water systems are to be depicted on plans submitted in association with an application for a building permit.
27. Prior to the initial occupation or use, all of the sustainability initiatives outlined on the approved development plans, Environmentally Sustainable Design (ESD) Report (prepared by Thermarate; Version 4; dated 31/10/2024) and as required in accordance with conditions of this approval, must be implemented onsite. Certification confirming that sustainability initiatives have been completed shall be submitted to the Town of Bassendean prior to the commencement of use.
28. Prior to the submission of an application for a building permit, the applicant/owner is to comply with the Town of Bassendean Local Planning Policy No. 15 – Public Art, through the contribution of a sum of 1% of the estimated cost of the development towards public art, being either:
 - a. Payment to the Town of Bassendean the value of 1% of the construction value; or
 - b. The provision of public art of site to the minimum value of 1% of the construction value to the satisfaction of the Town of Bassendean. The approved public art concept shall be implemented prior to the occupation of the development and maintained for the life of the development to the satisfaction of the Town of Bassendean.
29. Prior to the submission of an application for a Building Permit, a lighting plan demonstrating compliance with Australian Standard AS4282-1997 Control of the obtrusive effects of outdoor lighting shall be submitted to, and approved by the Town of Bassendean. The lighting plan must be prepared by a suitably qualified person, implemented prior to the commencement of use to the satisfaction of the Town of Bassendean.
30. A Construction Management Plan shall be submitted to, and approved by the Town of Bassendean prior to commencement of works. The Construction Management Plan shall address dust, noise, waste management, storage of materials, traffic management, site access, safety/security and protection of public assets. The Construction Management Plan is to be complied with for the duration of the construction of the development.



Advice Notes

- a) Nil.

Reasons for Officer Recommendation

As detailed in the report, the proposed use is considered to be compatible with, and complimentary to the surrounding residential zone. It has been demonstrated the provision of onsite parking bays is sufficient to accommodate demand, and therefore, the use will not adversely impact on the amenity of the surrounding locality.



 <p>Town of Bassendean 35 Old Perth Road Bassendean WA 6054 Phone: 9377 8000 Fax: 9279 4257 Email: mail@bassendean.wa.gov.au</p>	<p>Disclaimer: The Town Of Bassendean accepts no responsibility for the accuracy of this image or the results of any actions taken when using this image. This map is based on information provided by and with the permission of the Western Australian Land Information Authority.</p>	<p>Location 1 - Lot 85 (No. 94) West Road, Bassendean</p>	<p>12/11/2024</p>	
			<p>Scale: 1:4000</p>	

Briscola PTY LTD

Address:Lot 85 (#94) West Road, Bassendean

Childcare Centre

Job Number: 23019

Drawing No	Description
01	Cover Page
02	3D
03	Existing Site Survey
04	Site Plan
05	Context Plan
06	Aerial Map
07	Ground Floor Plan
08	First Floor Plan
09	Roof Plan
10	Elevations

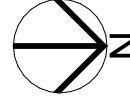




19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	10.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	17.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24

Revision Description 19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
3D

Scale:	Sheet Size:
	A1

Project No:	Revision Number:
23019	19.00

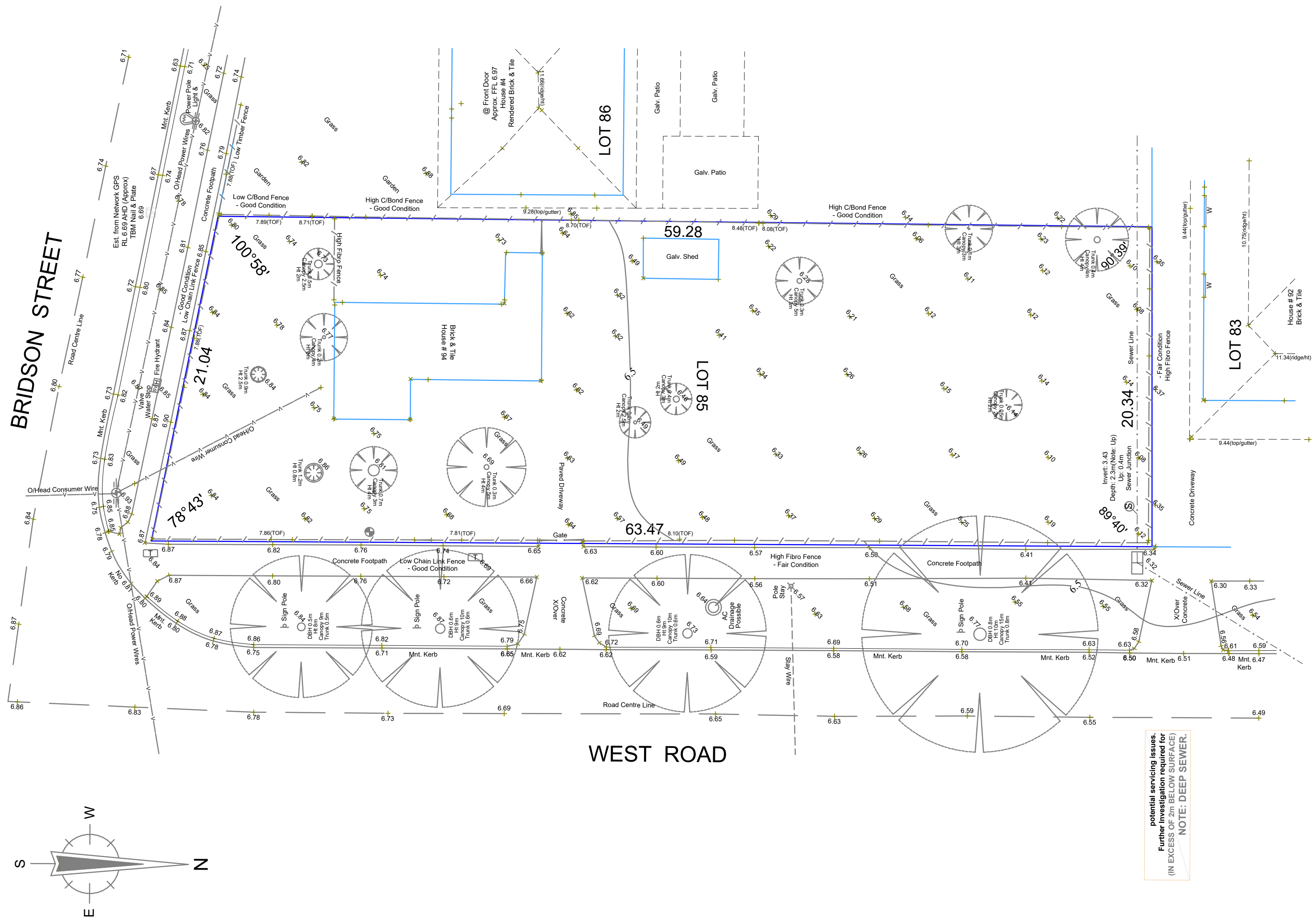
Drawing No.:
02 of 10



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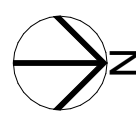


FEATURE SURVEY		STREET NAME : West Road	LOT # : 85	HOUSE # : 94	SUBURB : Bassendean	LOCAL AUTHORITY : Town of Bassendean	LOT AREA : 1239m ²	R CODE : R20	SURVEY DATE : 21/11/2022	SCALE AT A3 SIZE : 1:250
CLIENTS DETAILS:		LEGEND								
Briscola Pty Ltd		WATER METER SEWER AC TELSTRANBN PIT POWER POLE TREE (TO SCALE) W WINDOW								
LEVEL DATUM : AHD (Approx.)		DWG REF : West 94 F - v1.0 (TOF) TOP OF FENCE TEMP. BENCHMARK								



19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	15.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	31.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision	Description	19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Existing Site Survey

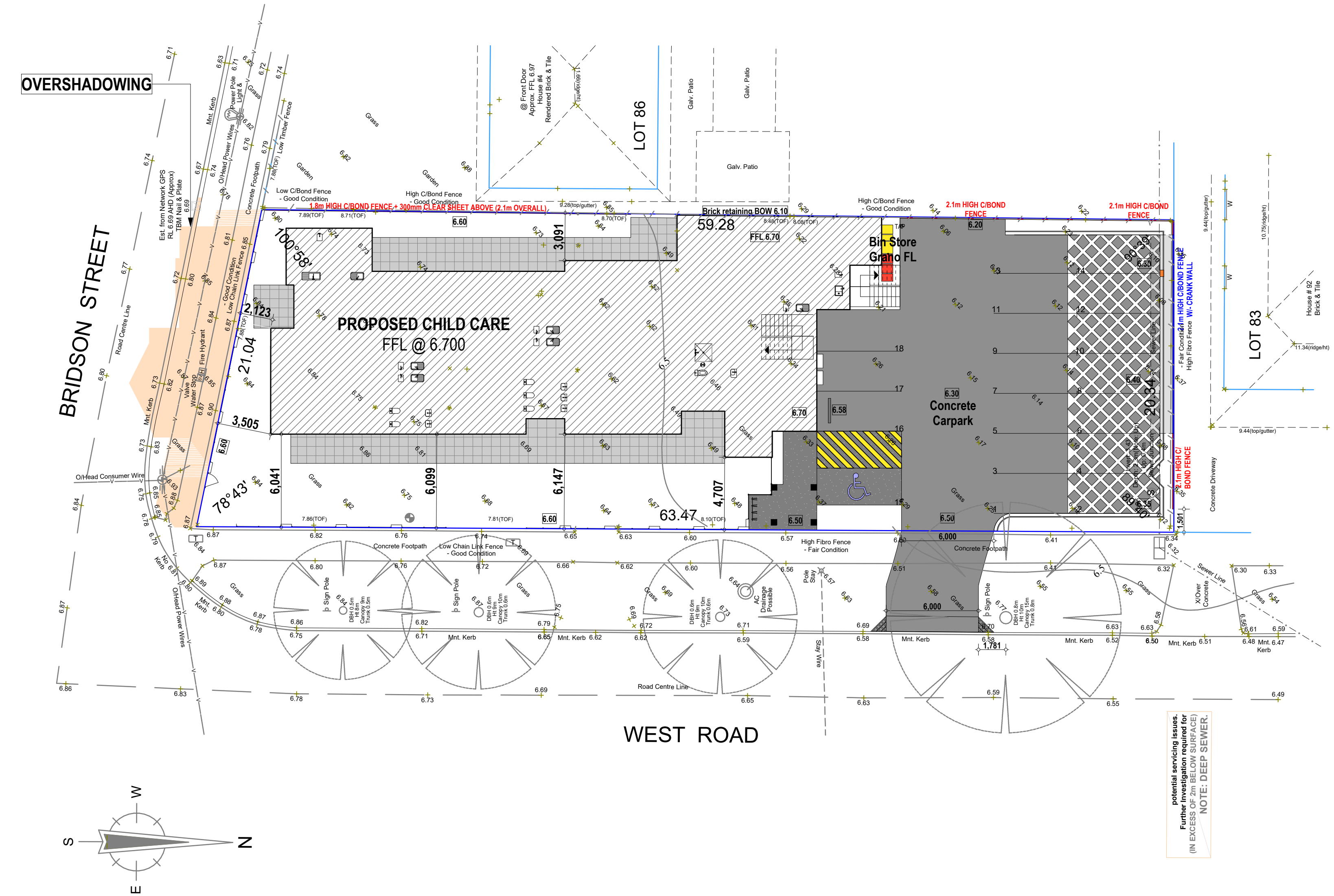
Scale:	Sheet Size:
1:200	A1
Project No:	Revision Number:
23019	19.00

Drawing No.:
03 of 10

Vision CONSULTING		T: (08) 6144 0000 F: (08) 6144 0099 59 SCARBOROUGH BEACH RD, SCARBOROUGH WA 6019 Email: info@visionsurveys.com.au www.visionsurveys.com.au		IMPORTANT NOTES: THE BOUNDARY CANNOT BE VERIFIED DUE TO LACK OF SURVEY MARKS/ PEGS. ALL BUILDING DIMENSIONS & FEATURES ARE APPROXIMATE. ONLY BOUNDARY POSITIONS HAVE BEEN TAKEN FROM BIL DINGS, FENCING, RETAINING WALLS AND OTHER TYPICAL FEATURES LOCATED ON THE BOUNDARY WHICH MAY OR MAY NOT BE ON THE CORRECT ALIGNMENT AND ARE TO BE VERIFIED WHEN REPOSED. BEFORE ANY WORK IS STARTED ON SITE OR PLANS ARE PRODUCED BY DESIGNERS/ ARCHITECTS, THE BOUNDARIES MUST BE REPOSED AND EXACT POSITIONS MARKED TO EXISTING STRUCTURES AND FENCING. VISION SURVEYS ACCEPTS NO RESPONSIBILITY FOR ANY CHANGES TO THE PARCEL OR PORTION OF THE PARCEL OR LAND SHOWN ON THIS SURVEY INCLUDING BUT NOT LIMITED TO ANY ADJOINING NEIGHBOURS LEVELS AND FEATURES THAT HAVE OCCURRED AFTER THE DATE OF THIS SURVEY. THIS PLAN IS INTENDED FOR THE DEPT OF PLANNING & INFRASTRUCTURE ONLY. SEWER / DRAINAGE MAY VARY FROM SCHEMATIC PRESENTATION/ CHECK WITH APPROPRIATE AUTHORITY BEFORE ADOPTION OF POSITION. CHECK TITLE FOR EASEMENTS / COVENANTS ETC.		TELSTRA YES SEWERAGE YES DRAINAGE GOOD VEG. / SOIL AS DESCRIBED		Urban Development Institute of Australia MEMBER SINCE 2022/2023 www.udi.org.au			
PLAN / DIAG / SP	P1784	ELECTRICITY	OVERHEAD	TELSTRA	YES	SEWERAGE	YES	DRAINAGE	GOOD	VEG. / SOIL	AS DESCRIBED
WATER	YES	GAS	YES								



FEATURE SURVEY		STREET NAME : West Road	LOT # : 85	HOUSE # : 94	SUBURB : Bassendean	LOCAL AUTHORITY : Town of Bassendean	LOT AREA : 1239m ²	R CODE : R20	SURVEY DATE : 21/11/2022	SCALE AT A3 SIZE : 1:250
CLIENTS DETAILS:		LEGEND								
Briscola Pty Ltd		WATER METER SEWER AC TELSTRANBN PIT POWER POLE TREE (TO SCALE) W WINDOW								
LEVEL DATUM : AHD (Approx.)		DWG REF : West 94 F - v1.0								
		(TOF) TOP OF FENCE TEMP. BENCHMARK								



Zone	Area	Perim
Stone FF	4.22	8.850
Bin	5.52	11.756
First Floor	111.61	52.380
Childcare Centre	440.77	122.789
	562.22 m ²	195.774 mm

Zoning:	Residential R20
Policies:	Residential
Heritage:	No
Bushfire:	No
BAL:	TBA
Acoustic:	No
Sewer:	Inside North
Power:	Overhead
Coastal:	No
Water:	Meter on Site
Wind Rating:	No

13.00	Changes	28.10.24
14.00	Changes	18.10.24
15.00	Changes	10.10.24
16.00	Changes	04.09.24
17.00	Changes	30.08.24
18.00	Changes	31.07.24
19.00	Changes	26.05.24
20.00	Changes	25.04.24
21.00	Changes	19.01.24

Revision	Description
1	19.01.24

Project North:	
Client Briscola PTY LTD	
Project Name Childcare Centre	
Project Address Lot 85 (#94) West Road, Bassendean	
Drawing Title: Site Plan	
Scale: 1:200	Sheet Size: A1
Project No: 23019	Revision Number: 19.00
Drawing No.: 04 of 10	

T: (08) 6144 0000 F: (08) 6144 0099
59 SCARBOROUGH BEACH RD,
SCARBOROUGH WA 6019
Email: info@visionsurveys.com.au
www.visionsurveys.com.au

PLAN / DIAG / SP	P1784	TELSTRA	YES
ELECTRICITY	OVERHEAD	SEWERAGE	YES
WATER	YES	DRAINAGE	GOOD
GAS	YES	VEG. / SOIL	AS DESCRIBED

IMPORTANT NOTES:
THE BOUNDARY CANNOT BE VERIFIED DUE TO LACK OF SURVEY MARKS/ PEGS. ALL BUILDING DIMENSIONS & FEATURES ARE APPROXIMATE. ONLY BOUNDARY POSITIONS HAVE BEEN TAKEN FROM THE DEDICATED FENCING, RETAINING WALLS AND OTHER TYPICAL FEATURES LOCATED ON THE BOUNDARY WHICH MAY OR MAY NOT BE ON THE CORRECT ALIGNMENT AND ARE TO BE VERIFIED WHEN REPOSED.
BEFORE ANY WORK IS STARTED ON SITE OR PLANS ARE PRODUCED BY DESIGNERS/ ARCHITECTS, THE BOUNDARIES MUST BE REPOSED AND EXACT POSITIONS MARKED TO EXISTING STRUCTURES AND FENCING. VISION SURVEYS ACCEPTS NO RESPONSIBILITY FOR ANY CHANGES TO THE PARCEL OR PORTION OF THE PARCEL OF LAND SHOWN ON THIS SURVEY INCLUDING BUT NOT LIMITED TO ANY ADJOINING NEIGHBOURS LEVELS AND FEATURES THAT HAVE OCCURRED AFTER THE DATE OF THIS SURVEY.
THIS PLAN IS INTENDED FOR THE DEPT OF PLANNING & INFRASTRUCTURE ONLY.
SEWER / DRAINAGE MAY VARY FROM SCHEMATIC PRESENTATION/ CHECK WITH APPROPRIATE AUTHORITY BEFORE ADOPTION OF POSITION.
CHECK TITLE FOR EASEMENTS / COVENANTS ETC.



Hawaiian Bassendean



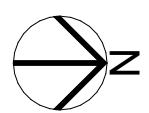
Commerical Development



Bassendean Primary School

19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	10.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	11.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision Description		19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Context Plan

Scale:	Sheet Size:	A1
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Project No:	Revision Number:
23019	19.00

Drawing No.:
05 of 10

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18.00	Changes	18.10.24
17.00	Changes	10.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	11.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
10.00	Changes	19.01.24

Revision	Description	19.01.24
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Client
Briscola PTY LTD

Project Name
Childcare Centre

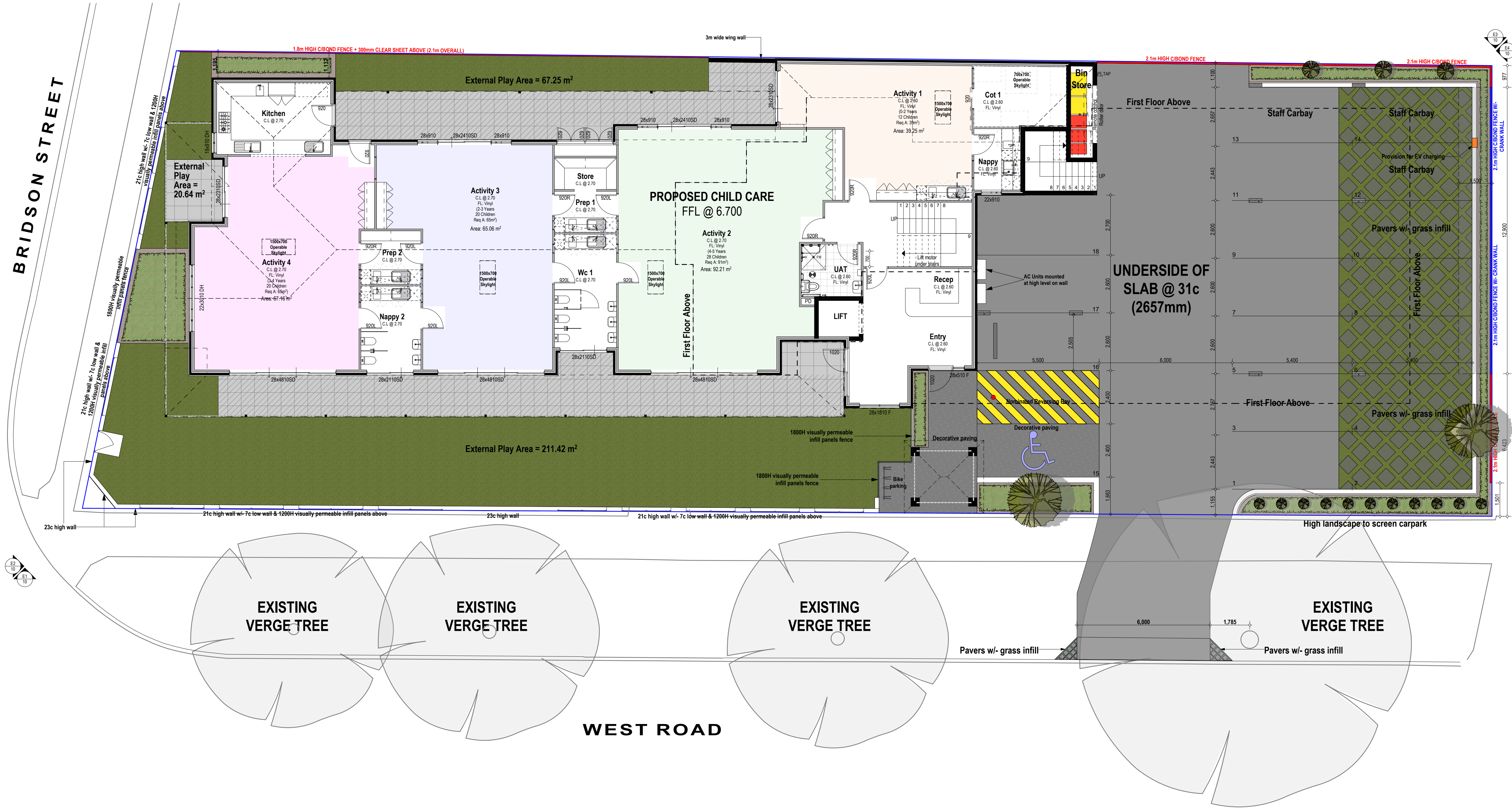
Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Aerial Map

Scale:	Sheet Size:
1:200	A1
Project No:	Revision Number:
23019	19.00

Drawing No.:
06 of 10

Child / Room Calculations				
Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	39.25m ²	3
Activity 2	4-5	28	92.21m ²	3
Activity 3	2-3	20	65.06m ²	4
Activity 4	3-4	20	67.16m ²	2
Total Internal =		80	263.68m ²	14
(Min 3.25m ² per child)				(Min 260m ² req)
Total External Play Area =		80	560.56m ²	
(Min 7m ² per child)				(Min 560m ² req)
Parking Calculations				
(As per local planning policy)				
Description	Required			
1 bay per employee	14			
1 bay per 10 childs	8			
Required =	22			
Provided =	18			



Zone	Area	Perim
Store FF	4.22	8.850
Bin	5.62	11.755
First Floor	111.61	52.380
Childcare Centre	440.77	522.780
	562.22 m ²	195.774 m

Site Calculations	
Site Area:	1,258m ²
Building Footprint:	457m ²
Site Coverage:	36.32%
Allowable Site Coverage:	50%

Zoning:	Residential R20
Policy:	Residential
Heritage:	No
Bushfire:	No
BAL:	TBA
Acoustic:	No
Sewer:	Inside North
Power:	Overhead
Coastal:	No
Water:	Meter on Site
Wind Rating:	No

19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	10.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	11.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision	Description	19.01.24

Project North:	
Client	
Briscola PTY LTD	
Project Name	
Childcare Centre	
Project Address	
Lot 85 (#94) West Road, Bassendean	
Drawing Title:	
Ground Floor Plan	
Scale:	Sheet Size:
1:100	A1
Project No:	Revision Number:
23019	19.00
Drawing No.:	
07 of 10	

Child / Room Calculations

Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	39.25m ²	3
Activity 2	4-5	28	92.21m ²	3
Activity 3	2-3	20	65.06m ²	4
Activity 4	3-4	20	67.16m ²	2

Total Internal = 80 263.68m² 14
(Min 3.25m² per child) (Min 260m² req)

Total External Play Area = 80 560.56m²
(Min 7m² per child) (Min 560m² req)

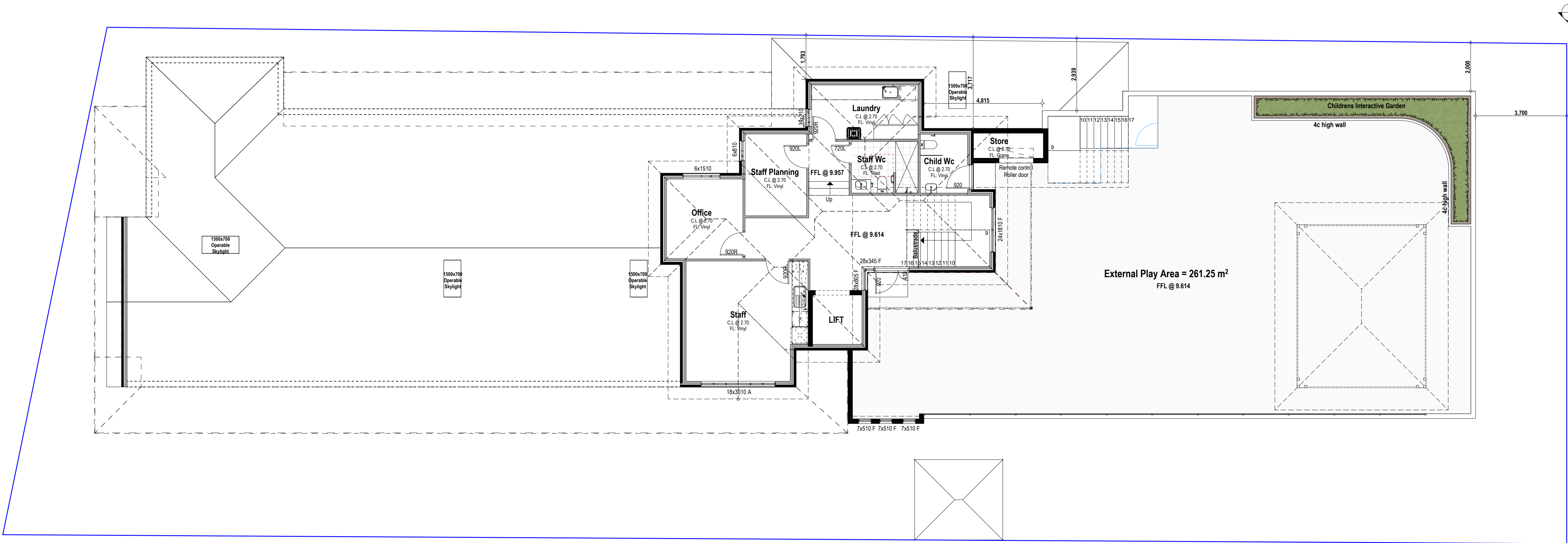
Parking Calculations

(As per local planning policy)

Description	Required
1 bay per employee	14
1 bay per 10 child	8

Required = 22

Provided = 18



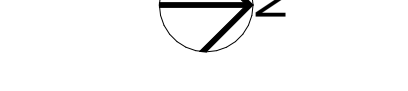
Zone	Area	Perim
Stone FF	4.22	8.80
Bin	5.62	11.76
First Floor	111.61	52.30
Childcare Centre	440.77	122.78

562.22 m ²	195,774 mm
Zoning:	Residential R20
Policies:	Residential
Heritage:	No
Bushfire:	No
BAL:	TBA
Acoustic:	No
Sewer:	Inside North
Power:	Overhead
Coastal:	No
Water:	Meter on Site
Wind Rating:	No

19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	16.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	31.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24

Revision	Description	Date
19.01	24	

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
First Floor Plan

Scale:
1:100

Sheet Size:
A1

Project No:
23019

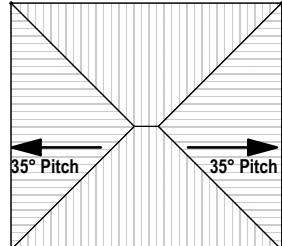
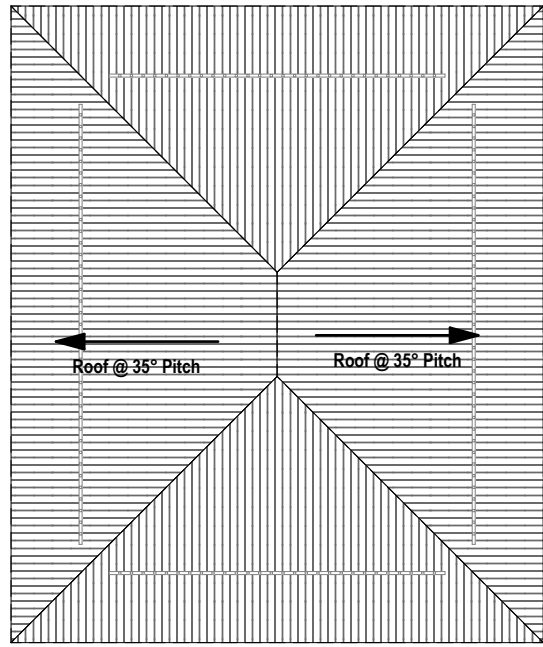
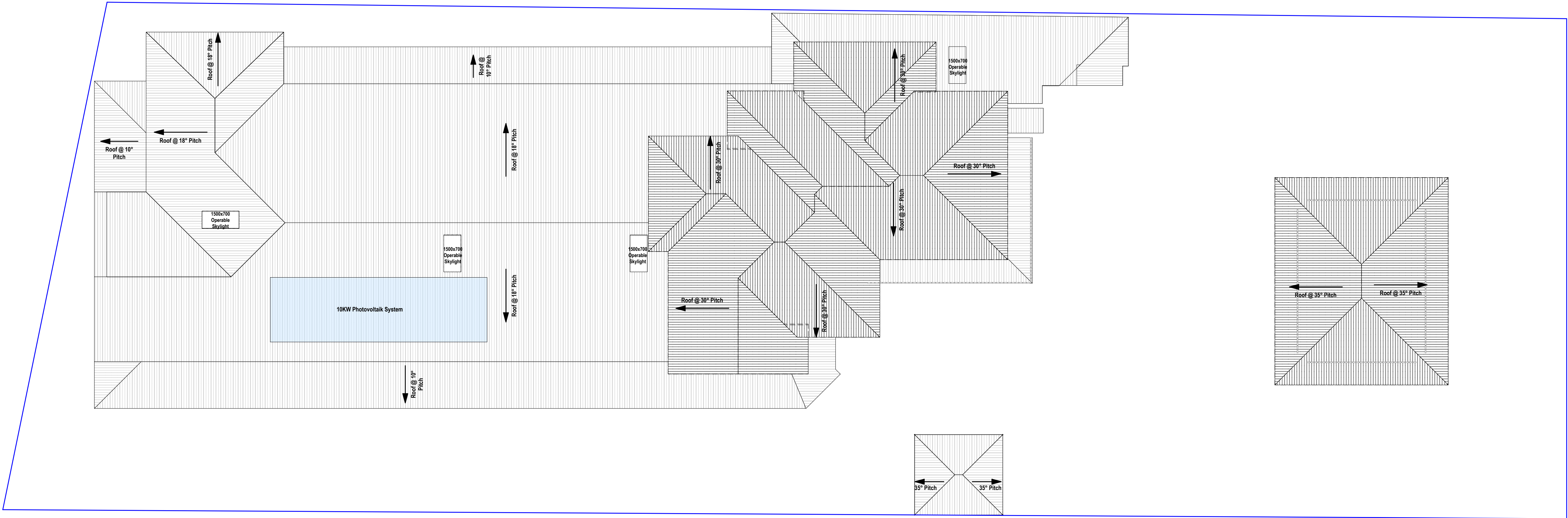
Revision Number:
19.00

Drawing No.:
08 of 10

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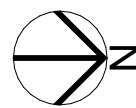
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19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	10.10.24
16.00	Changes	04.09.24
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14.00	Changes	13.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision	Description	19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:

Roof Plan

Scale:

1:100

Sheet Size:

A1

Project No:

23019

Revision Number:

19.00

Drawing No.:

09 of 10

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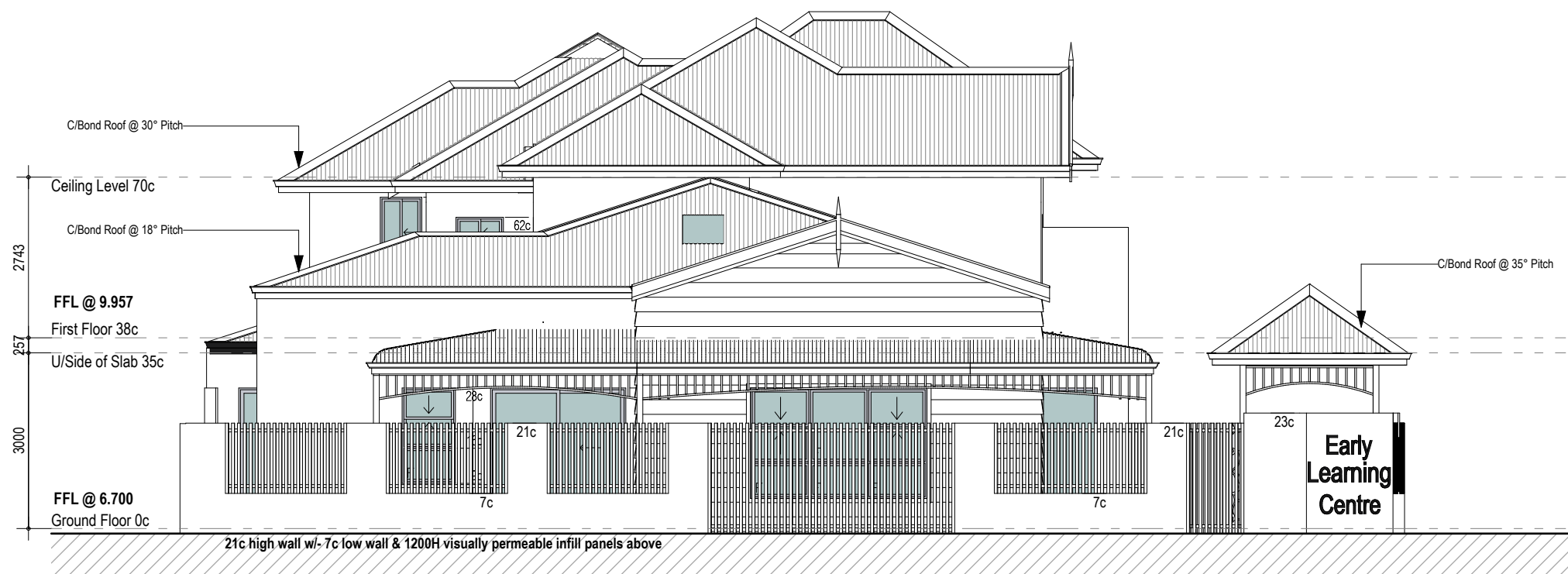
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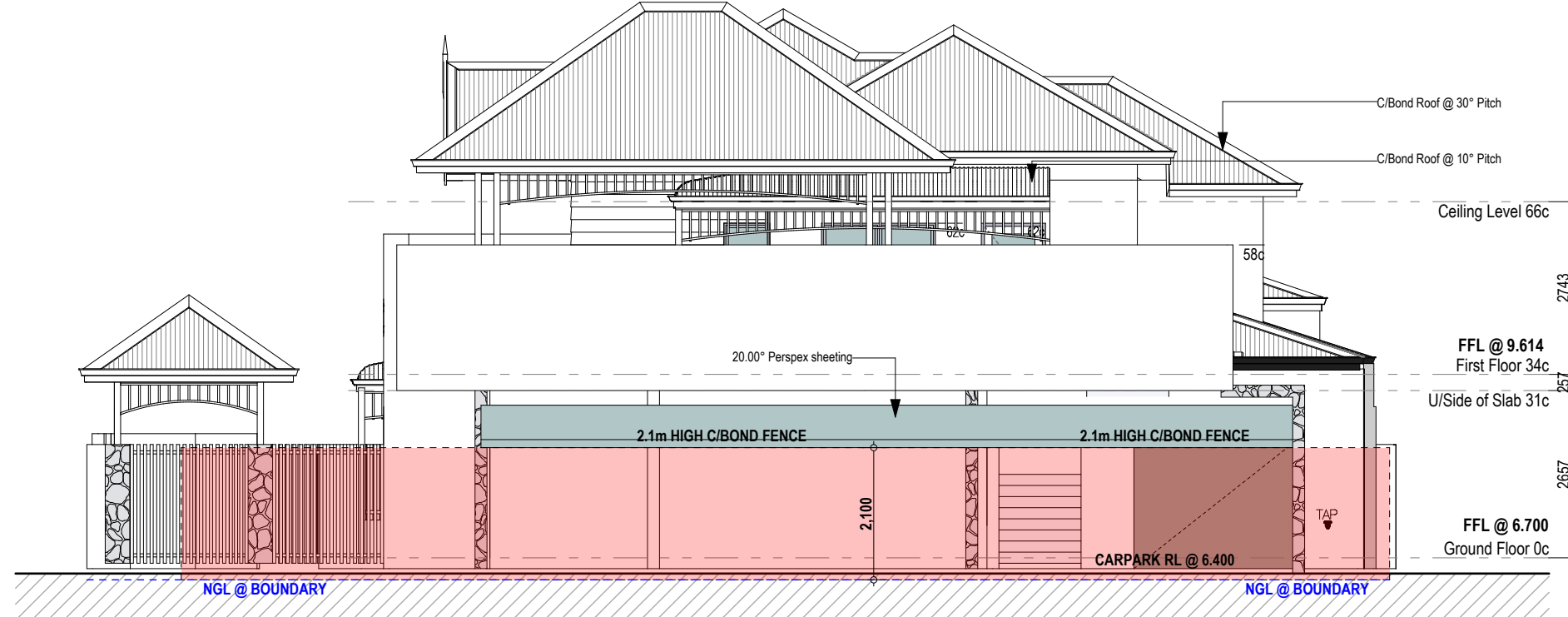
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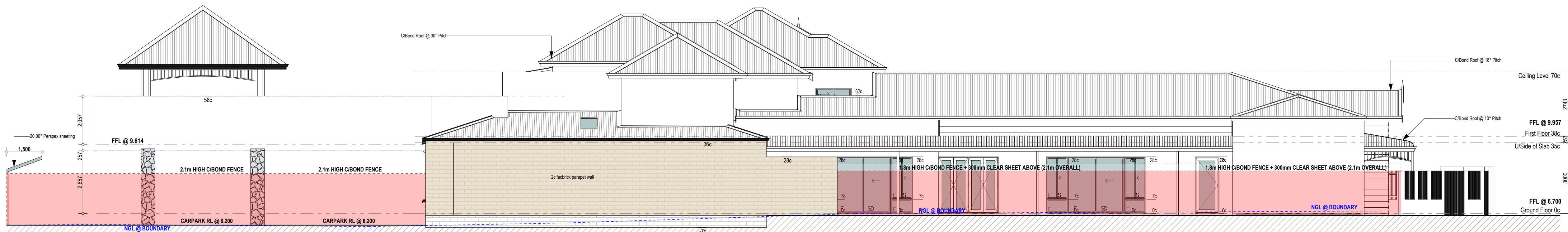
E1 East Elevation
1:100



E2 South Elevation
1:100



E4 North Elevation
1:100

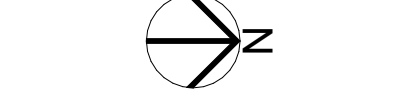


E3 West Elevation
1:100

19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	16.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	13.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24

Revision Description

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Elevations

Scale: 1:100
Project No: 23019
Revision Number: 19.00

Drawing No.:
10 of 10

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PLANT SCHEDULE				
	SPECIES	SPACINGS	SIZE	QTS
TREES				
Pca	PYRUS CALLERYANA	AS SHOWN	90 L	7
Jmi	JACARANDA MIMOSIFOLIA	AS SHOWN	90 L	1
SHRUBS				
Hsc	HIBBERTIA SCANDENS	AS SHOWN	140 MM	18
Lof	LAVANDULA OFFICINALIS	AS SHOWN	140 MM	7
Rof	ROSEMARY OFFICINALIS	AS SHOWN	140 MM	6
Eni	EREMOPHILA NIVIA	AS SHOWN	140 MM	10
Cca	CONOSTYLIS CANDICANS	AS SHOWN	140 MM	10
Dre	DIANELLA REVOLUTA	AS SHOWN	140 MM	29
Cun	CHAMELAUCIUM UNCINATUM	AS SHOWN	140 MM	25

TREES:



PYRUS CALLERYANA



JACARANDA MIMOSIFOLIA

SHRUBS:



DIANELLA REVOLUTA



HIBBERTIA SCANDENS



CHAMELAUCIUM UNCINATUM



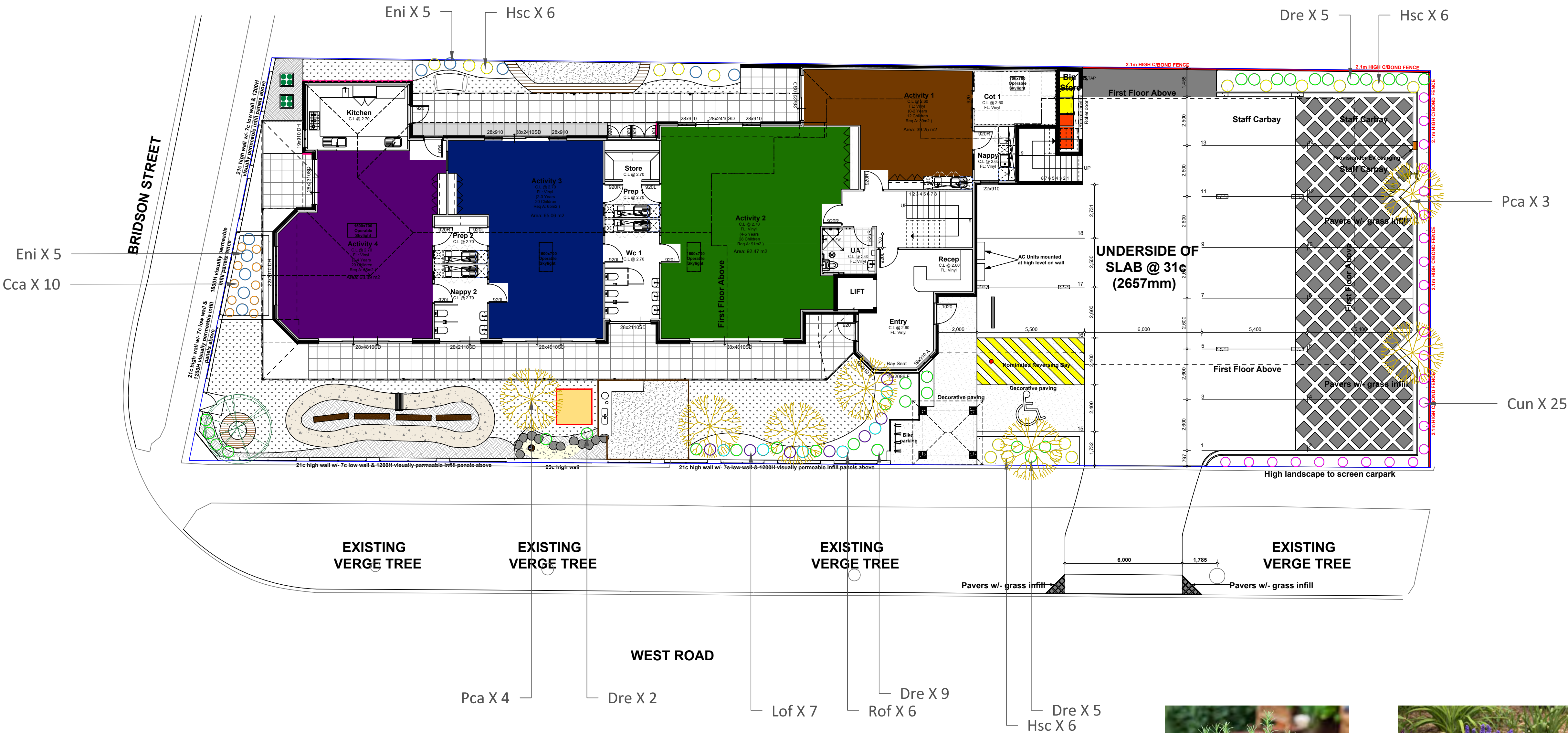
EREMOPHILA NIVIA



ROSEMARY
OFFICINALIS



LAVANDULA
OFFICINALIS

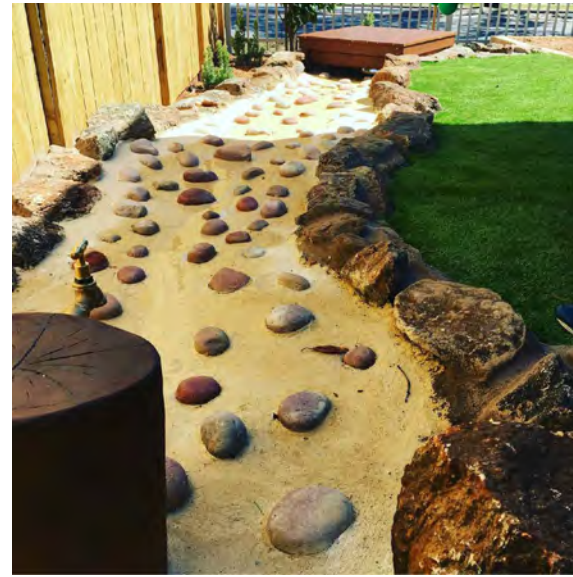


P.O. Box 774
e-mail: admin@childscapes.com.au
web: childscapes.com.au

PROJECT		DRAWING				ISSUE			REVISIONS			
BASSENDEAN		PLANTING PLAN				ISSUED FOR FEEDBACK			No	DATE	DRAWN	DETAILS
CLIENT -		DRAWING NO L-02	SCALE 1:125	SHEET A1	REVISION 5	DRAWN NK	DATE 16/09/24		0	24/01/24	NK	PLANTING- ISSUED FOR FEEDBACK
									1	01/04/24	NK	PLANTING- ISSUED FOR FEEDBACK
									2	06/06/24	NK	PLANTING- ISSUED FOR FEEDBACK
									3	07/06/24	NK	PLANTING- ISSUED FOR FEEDBACK
									4	13/09/24	NK	PLANTING- ISSUED FOR FEEDBACK
									5	16/09/24	NK	PLANTING- ISSUED FOR FEEDBACK



CUBBY HOUSE



CREEK BED



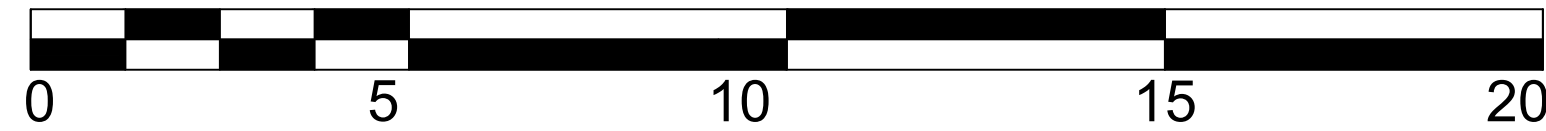
FLOOR LEVEL DECK



BIKE TRACK

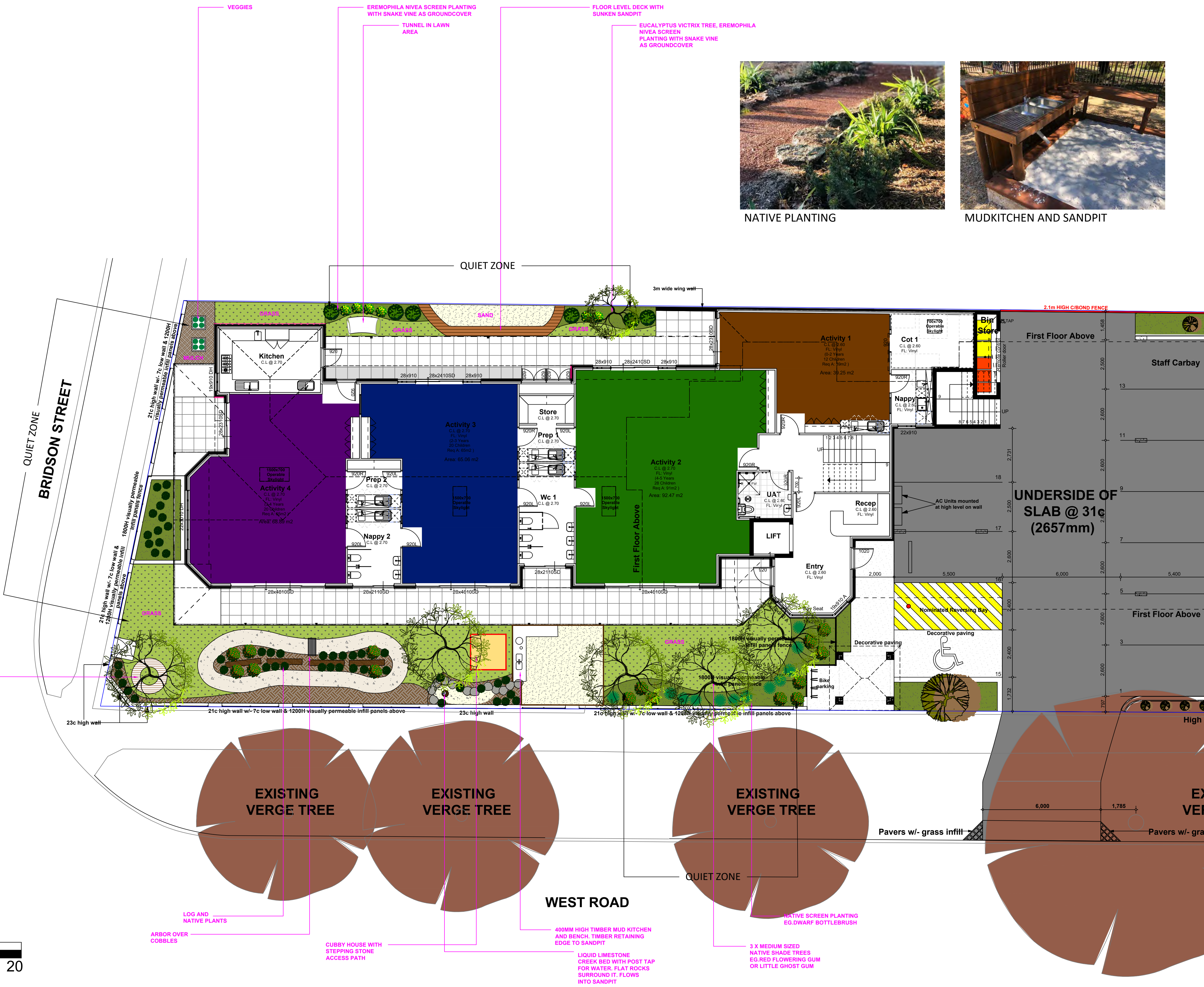


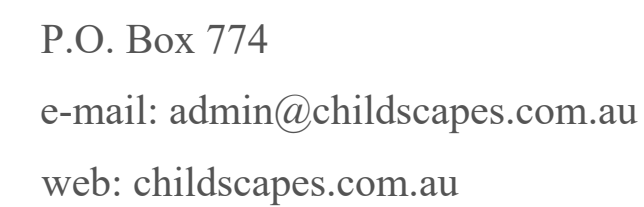
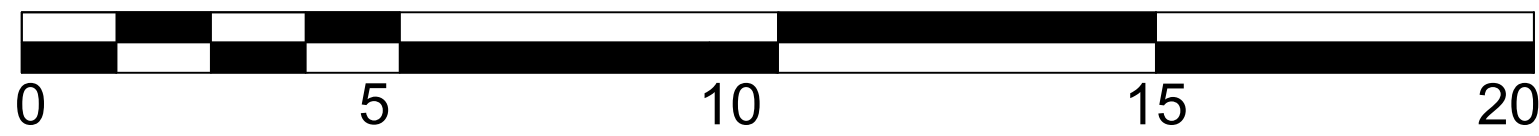
ARBOR



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e-mail: admin@childscapes.com.au
web: childscapes.com.au

PROJECT		DRAWING				ISSUE			REVISIONS	
BASSENDEAN		OVERALL CONCEPT PLAN				ISSUED FOR FEEDBACK			No	DATE
CLIENT	-	DRAWING NO	SCALE	SHEET	REVISION	DRAWN	DATE		DRAWN	DETAILS
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									NK	CONCEPT- ISSUED FOR FEEDBACK
									NK	CONCEPT- ISSUED FOR FEEDBACK
									NK	CONCEPT- ISSUED FOR FEEDBACK
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									NK	CONCEPT- ISSUED FOR FEEDBACK
		L-01	1:100	A1	7	NK	05/09/24		NK	CONCEPT- ISSUED FOR FEEDBACK





PROJECT	DRAWING				ISSUE			REVISIONS				
	OVERALL CONCEPT PLAN				ISSUED FOR FEEDBACK			No	DATE	DRAWN	DETAILS	
BASSENDEAN upper floor	CLIENT -	DRAWING NO L-03	SCALE 1:75	SHEET A1	REVISION 3	DRAWN NK	DATE 10/09/24		0	10/06/24	NK	CONCEPT- ISSUED FOR FEEDBACK
									1	06/09/24	NK	CONCEPT- ISSUED FOR FEEDBACK
									2	09/09/24	NK	CONCEPT- ISSUED FOR FEEDBACK
									3	10/09/24	NK	CONCEPT- ISSUED FOR FEEDBACK

Product Code	4379
Application	Synthetic Lawns/Landscaping
Pile Height (mm)	30mm
Colours Available	Field Green/Olive Green with multi tone sub-pile
Total Product Weight	1780g/m ²
Available Width	3.71m
Standard Roll Length	20m
Line Systems	N/A
Stabilising Infill	Fine grained silica sand (12kg/sqm)
Performance Infill	N/A
Warranty	15-Year Limited Warranty
Machine Gauge	12.7mm
Stitch Rate	17 per 100mm
Yarn Weight	1,000g/m ²
No. of Tufts (m²)	13,386
Perforated	Yes
Yarn Construction	IR-reflective CoolPlus, Two Tone Polyethylene Monofilament Texturised Monofilament Sub-pile (Straw/Green)
Linear Density	600 Tex/490 Tex
Environment	Environmentally friendly yarns, which are heavy metal free
UV Stability	The yarn is protected against UV degradation to the highest level, as specified in the yarn manufacturer's warranty terms and conditions.
Construction	Multilayer/Polypropylene/Polyester
Primary Layers	Black
Compound Base	Hybrid Emulsion
Tuft Anchorage	40 Newtons (minimum)
Antioxidising Agent	Present
Yarn Origin	Australia
Country of Manufacturer	Australia



SUMMER COOL 30

- Summer Cool 30 is one of All Seasons Synthetic Turf's residential lawns.
- It's versatile & suitable for front and rear lawns, play areas, balconies & commercial areas.
- It has 30mm pile height and is made with the latest COOLPlus Yarn Technology.
- It has a natural look and feel, is 100% Australian made and comes with a 15-year warranty.

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- The Summer Cool 30 is totally safe with no contaminants.
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Schedule of Submissions - Proposed Child Care Premises – Lot 85 (No. 94) West Road, Bassendean – DAP/24/02721

No	Submission	Themes	Officer Response
1	<p>I am writing to formally express my opposition to the proposed development of a two-storey childcare centre on West Rd. As a resident of this street, I have significant concerns regarding the impact this development will have on our community.</p> <p>Firstly, West Road is already a very busy street with existing parking issues. The proposal includes only 16 parking bays, which I believe is grossly inadequate for a facility of this nature. The insufficient parking provision will inevitably lead to increased congestion and further exacerbate the parking problems for local residents.</p> <p>Moreover, the introduction of a two-storey childcare centre will result in a loss of amenities for the surrounding neighbours. The increase in noise levels and traffic will disrupt the quiet enjoyment of our homes. The scale and nature of the proposed development are not in keeping with the residential character of our street, making it an inappropriate development for this area.</p> <p>In conclusion, the proposed location and scale of this development are not suitable for West Road. I urge you to reconsider the approval of this project to preserve the quality of life for the existing residents.</p>	<ul style="list-style-type: none">• Parking adequacy• Noise• Traffic• Scale and character of built form	<p>2 additional parking bays have been included since the application was advertised. Parking adequacy , noise, traffic and built form are discussed in the report</p>
2	<p>As a resident of Town of Bassendean (south side of Guildford Road), community member and a parent who sends their child to Bassendean Primary School, I do not support the development of the Child Care Centre on West Road</p> <p>Concerns and issues:</p> <ul style="list-style-type: none">• significant increase in traffic - people using the facility are likely doing so in order to work which would mean dropping children off in a car• It doesn't align to 8 out of 10 design principles• Noise for community• there's not enough parking according to Bassendean's own standards (requires 21 - it is 5 short)• I'm not sure how the education standard is to have 8 cars for 13 staff (at minimum) <p>I support the town planning & revitalising Old Perth Road. I support the development of A childcare centre West road is a community street and we should keep it as such.</p>	<ul style="list-style-type: none">• Traffic• Deign standard (does not meet SPP 7.0)• Noise• Parking	<p>2 additional parking bays have been included since the application was advertised. Up to date traffic counts for the surrounding street network have also been provided to demonstrate the network has capacity to accommodate increased traffic.</p> <p>Parking, noise, traffic and compliance with State Planning Policy 7.0 – Design of the Built Environment are discussed in the report</p>
3	<ul style="list-style-type: none">• Refer to attachment (submission of owner of adjoining property)	<ul style="list-style-type: none">• Scale of built form (R-Code variations, lack of provision for services, overshadowing, boundary wall height) suggests the lot is too small and massing and height of the development is inappropriate.• Noise• Inappropriate use of a residential zoned property• Insufficient demand for child care services in the area• Access• Parking (tandem arrangement, manoeuvring, provision)• Inefficient solar panels due to orientation• Impact on street tree• Fails to meet SPP 7.0 as noted by DRP• Finished levels are to be confirmed• The site would be better used for residential purposes• Suggests intentions of alternative use (ie. office space ect) similar to other centre operated by the applicant	<p>The suitability of the use, built form, including height, scale and setbacks, are discussed in the report. Noise, the outcome of the design review process, demand for child care services, protection of street trees, access, parking and traffic area also addressed.</p> <p>The design has been modified since advertised. Variations to the deemed-to-comply provisions of the R-Codes that remain are detailed in the report and considered against the relevant design principles. Finished site levels have also been confirmed and an arborist report has been prepared outlining tree protection measures necessary to retain street trees. The car park has been modified to include 2 additional car parking bays. The car park design and manoeuvring comply with the Australian Standards. Noise modelling has been updated and adheres with best practice acoustic modelling. The Transport Impact Statement has also undergone review of an independent traffic consultant as discussed in the report.</p> <p>There is nothing to suggest an office, or alternative use is to be included in the centre. If this were to be proposed, further approval would be required.</p> <p>Contemplating alternative, preferred uses for the site, or the fact that there may be alternative sites suitable to accommodate a child care premises in the area are not relevant considerations. The DAP is required to consider the proposal on its merits.</p>

4	<p>I am an owner occupier on Devon Road, Bassendean. Please see comments regarding the above mentioned development.</p> <p>No recent publicly available traffic counts - Bassendean Primary School and The Last Crumb will be high traffic numbers at the same time as the daycare. The area is already struggling with the amount of cars parked along the road. During peak times, which the daycare will add to, it is not possible to reach 50km. I would estimate 30km at the most.</p> <p>Car bays - the number listed doesn't cover enough for all staff and parents, therefore, street parking will be required. There isn't enough room for street parking on either road. Bridson Street is a popular street from West Road to Guildford Road. Traffic issues will be at peak times and busier during the day. All of these issues with children around will no doubt decrease the speed even more.</p> <p>Street parking will increase foot traffic and young children foot traffic. Street parking will spill over into surrounding streets.</p> <p>I do not believe public transport will be used by families and/or staff.</p> <p>Acoustic issues - will affect surrounding streets with increased traffic. The neighbouring properties will have to deal with 80 children ranging from babies to young children.</p> <p>The statement proposing that 'some staff can cycle or catch public transport' is unpredictable.</p> <p>Childcare peak times are very similar to Primary school peak times.</p> <p>Daycares in Bassendean are currently full, therefore, I believe that the proposed Daycare will be at capacity.</p> <p>Traffic volumes listed are too old. The Last Crumb has added a lot to all the adjoining roads and street parking.</p> <p>Proposed 80 children require more than 13 staff.</p> <p>Vehicular crash information - I don't believe this can be presented as this is a residential corner and the proposed is a childcare with at least 93 cars visiting per day.</p> <p>On a personal note, as I am planning to grow my family in the near future, I would love a daycare walking distance to my house. However, I do not believe the size and location of this daycare is desirable.</p>	<ul style="list-style-type: none">• Traffic (peak drop off times will be similar to school, out of date traffic data)• Parking – insufficient provision onsite. The proposal will add to an existing on street parking issue. On-street parking is unsafe and undesirable in this location• Noise impact on adjoining residence• Noise impacts of increased traffic• Insufficient information – traffic counts, unreasonable to expect staff/ parents to walk, cycle or use public transport, inaccurate staffing numbers	<p>Updated traffic counts and confirmation of expected staff numbers were provided post-advertising and are referenced in the report. 2 additional parking bays were included post-advertising.</p> <p>Traffic, parking and noise are discussed in the report.</p>
5	<p>I live at -- Whitfield St, Bassendean. Very close to the corner on West Rd and Bridson St, lot 85 where a proposal for a child care centre has been submitted. This is not an appropriate use of this site, right in the middle of a residential area which has a school and extremely busy cafe close by. Parking is already a nightmare on Whitfield and West road at peak times and this would add greatly to the chaos, congestion on those streets and not to mention safety for children.</p> <p>My child, as well as a few others have already had near misses crossing Bridson street on the supposed 'safe active street' on their way to school. There is no signage/crossing here for the dozens of children who cross each day and if there is a child care centre on the near by corner the traffic along Bridson (and similar story on West Rd) will increase greatly.</p> <p>West Rd, around the school and last crumb is a total nightmare. Worse at peak times and continuous congestion all day due to the cafe. This has put many residents off side already. So many people parallel parking on streets that mean residents can't even park at their own property (my parents live in Harcourt and this is the case most days) A child care CTR on West Rd will mean people will be pulling up and stopping all along Bridson/West and neighbouring Street and would be a nightmare. You have to consider the safety of children walking across these streets adjacent to proposed centre and the fact you have no traffic plan in place. Cars and drivers drive like they own the road, it will only be a matter of time before there is a serious incident.</p> <p>Not to mention that this lot is smack bang in the middle of a residential area. Keep it residential, use a better location for child care CTR like the old Organi Kids site/hardware store/car sales lot on old Perth road. Surely these are more suitable.</p>	<p>Parking – existing parking issue CCC should be in commercial area/ town centre Increase in traffic</p>	<p>Traffic and parking are discussed in the report. The suitability of the child care centre on land zoned 'residential' is also discussed.</p>
6	<p>I live on the corner of Shackleton Street and Eileen Street.</p> <p>Decisions to block off Fisher, Chapman, and Old Perth Road from Guildford Road have seriously eroded my quality of life as Council continues to approve developments that funnel more and more traffic down my street. The creation of the playground at Sandy Beach is another poorly thought through proposal with no consideration given to the amount of increased traffic funnelled down Shackleton St. The unexpected success of the Last Crumb is another factor that increases traffic going down Shackleton St. The renovated Bassendean Hotel has again, significantly added to the amount of traffic that goes down Shackleton St to and from the venue.</p> <p>This proposed development would again increase traffic and no regard is given for how this decision will further increase traffic going past my door.</p> <p>I have protested for years about Council decisions that increase traffic flow down Shackleton St. I see the significant traffic slowing infrastructure in Whitfield St and in Grey St and want similar consideration given to improve my quality of life which is increasingly eroded by Council decisions.</p> <p>Any decisions to approve further development that would increase the traffic down Shackleton St must be accompanied by consideration of the traffic impact of these developments. There is a serious need for holistic, intelligent planning decisions that enhances the quality of life of those of us who used to live on a quiet suburban street.</p> <p>I am not against infill housing, but traffic calming, like that in Whitfield and Grey Streets, is essential in order to restore some of the quality of life that we used to enjoy.</p>	<p>Object to increase traffic in the broader area</p>	<p>Traffic is discussed in the report.</p>
7	<p>I am writing in relation to planning submission DA-2024-066 concerning the proposed development of a Child Care Premises at Lot 85 (No. 94) West Road, Bassendean (corner of West Road and Bridson Street). My family and I wish to formally express strong objections due to several significant concerns:</p> <p>The proposed location on West Road, already known for heavy traffic, raises serious safety concerns given the presence of young children at a daycare centre.</p> <p>Insufficient parking facilities on West Road, compounded by existing congestion from Bassendean Primary School and the Last Crumb Cake Shop, may lead to cars parking on the road itself, potentially endangering children.</p> <p>Personal experiences include witnessing near-miss incidents at the intersection and an unfortunate an incident involving a neighbour's dog on Bridson Street.</p> <p>Considering the predominantly residential nature of the area, the introduction of a daycare centre would disrupt the established residential character of the neighbourhood.</p> <p>These concerns primarily stem from safety issues that directly impact our community, especially as residents in close proximity to this intersection.</p>	<p>Traffic Insufficient on-street parking in the area Impact of commercial use on residential amenity/ character</p>	<p>The applicant has demonstrated parking demand can be accommodated onsite.</p> <p>Traffic, parking and the likely impact on residential amenity is discussed in the report.</p>

8	<p>I wish to oppose this development as I believe it is inappropriate for the residential location in which it is planned. My reasons are listed below;</p> <p><input type="checkbox"/> Increased Traffic and Congestion: Parking and traffic is already an issue in this area and this would exacerbate the problem on what is already a very busy corner/street. Childcare centres typically result in a significant increase in traffic during drop-off and pick-up times. This will lead to congestion, particularly on the corner of 2 busy streets as per the proposal.</p> <p><input type="checkbox"/> Insufficient Parking: There is not enough provision for parking for the centre, as 16 bays is only enough for the staff. There is already limited parking on the street, plus the cars from the local school and The Last Crumb Cafe are parked in this area during their hours of operation. The childcare centre will exacerbate this problem. The 16 parking bays proposed for the centre is inadequate, and will lead to parking overflow into surrounding streets and increased competition for parking spaces among residents. There is clearly insufficient space on the site for more parking.</p> <p><input type="checkbox"/> Noise Pollution: Childcare centres generate considerable noise from children playing and carers shouting, which will be disruptive to the tranquility of our residential street. This will negatively impact the quality of life for residents who value a quiet environment, particularly those who live next door or close by.</p> <p><input type="checkbox"/> Safety Concerns: Increased traffic and congestion will raise safety issues, especially for young children who live in the area. The higher volume of vehicles will make it more dangerous for children to play outside or for residents to walk or cycle in the neighbourhood. This is a very walkable neighbourhood, particularly with children from the school, the increased traffic on this corner will clearly pose safety risks for them.</p> <p><input type="checkbox"/> Loss of Residential Character: A large, two-storey childcare centre will be out of character with the existing residential buildings. This will affect the aesthetic and communal feel of the street, potentially impacting property values and the overall atmosphere of the neighbourhood. We bought in a quiet part of a residential street, we don't want incompatible (or indeed any) businesses being built next to our homes.</p> <p>There are currently around 23 childcare facilities in Bassendean, including one on the same block as this proposed childcare, this is an unnecessary development which will adversely affect the current residents for the above reasons listed.</p>	<ul style="list-style-type: none">• Increase in traffic/ safety• Insufficient parking onsite• Noise impacts on neighbours• Built form inconsistent with residential character• Adverse impacts on property values• Incompatible use in residential area• 	<p>Additional parking bays were included post-advertising.</p> <p>An acoustic assessment demonstrates the proposal can comply with the Environmental Protection (Noise) Regulations 1997, subject to management measures as outlined in the report and recommended conditions of approval.</p> <p>Traffic, parking, built form and noise are discussed in the report. Town staff agree that sufficient demand exists in the area for child care services.</p>
9	<p>I would like to comment on the proposed child care premises at West Road Bassendean (DA-2024-066). I strongly disagree with the proposal.</p> <p>Firstly while I understand there may be a need for child care in the Bassendean area, to me there seem to be so many more places for a proposed commercial child care centre to go than on a residential housing lot.</p> <p>West Road / Bridson Street is a busy intersection. Buses travel through it along West Road all day. Cars use it to bypass busy Guildford Road. Speed dampening humps have been added to slow the speed of the vehicles as it also adjoins the Whitfield Active Street (which is a great Town of Bassendean initiative).</p> <p>More vehicles accessing not only the West Road / Bridson Street intersection but the West Road / Hyland Street intersection (of which I found no data for in the road use assessment), will make access to the day care tricky during the typical drop off and pick up times. Plus only having available a small number of car parking bays will mean these two intersections will now have cars parked on roads and verges causing more havoc.</p> <p>I live one road over from Bridson Street. I do not wish to hear the noise from the day care centre. I did not purchase land and build a house where I did to have to listen to this extra noise, and potentially have increased road traffic as people may be attempting to avoid the West Road / Bridson Street intersection. I purchased where I did for the serenity, the trees, the birds. I purchased as I wished to live near other residents, not commercial businesses. I feel this is an insult to have a commercial business thrown at us, especially the people who will be joining fences and living across the road.</p> <p>I hope common sense prevails and the commercial business is encouraged to acquire land along Old Perth Road somewhere which is in dire need of rejuvenation. More people dropping off and picking up children along Old Perth Road will hopefully mean the struggling small businesses along the strip will get a jolt of customers and be able to stay servicing the people of Bassendean.</p>	<p>Insufficient onsite parking increase on street parking in proximity to intersections (safety) noise increase traffic on local streets commercial development not appropriate in residential areas</p>	<p>Additional parking bays were included post-advertising. Traffic, noise and parking are discussed in the report. The suitability of the child care centre on land zoned 'residential' is discussed in the report.</p>
10	<p>Comments regarding development of an early learning centre at this location are:</p> <p>- Majority of the elevation design is in keeping with the town and looks very promising. However the car park and elevated slab is not and looks like a shopping centre! - The car parking and flow is not sufficient. Verge parking or road side drop off will be expected and will create a traffic nightmare at peak times.</p> <p>- The assumption of local residents along kids to the centre is sound, the assumption of parking at Bassendean Primary or on Whitfield is erroneous. On street or verge parking on Hyland would be the highly likely behaviour (insert parking behaviour of Last Crumb patrons or Bassendean Primary parents here).</p> <p>- While now at a reduced frequency (30min intervals) the bus 45 movements at Hyland / West will create impact on traffic flow; especially considering the turning arc required by buses leaving Hyland onto West and ad hoc parking (see above).</p> <p>- The traffic assessment references bus 55 which is no longer correct (see above).</p> <p>- Parking availability assessment at West Rd and Whitfield St in September 2023 reflects a poor representative sample (expected to be 16 days of the month prior to the start of Term 3 holidays on 23 Sept).</p> <p>- There does not appear to be any reference to security measures or site access limitations (i.e. closed car park) at times outside of operating hours.</p> <p>Definitely not against the idea of a centre, it's just the vehicle aspect that needs further thinking.</p>	<p>In principle support – however traffic and parking needs to be addressed</p> <ul style="list-style-type: none">- Carpark and elevated outdoor play area is inappropriate/ too commercial- Inappropriate carpark layout and flow will lead to onstreet parking, which is already at capacity- Adhoc parkigh will cause issues for bus access at Hyland/West Rd instersection- Traffic modelling is not sufficient (noting school holidays)	<p>The street elevation has been updated since advertising and in response to advice of the DRP.</p> <p>Built form and parking are discussed in the report.</p>
11	<p>As a resident only a block away, and someone who uses Bridson/ West Rd daily waking to and from Bassendean Primary School, I am highly concerned about the increased traffic and parking at this proposed site. This area is already highly congested at most times of the day, with cars but also children walking to and from school. The parking along West Rd is pushed to capacity due to the success of Last Crumb. There will be increased traffic entering and exiting West Rd at the exact times the road is already at its peak for both school drop off/ pick up/ cafe service. It creates an unnecessary risk to pedestrians and will force further congestion and traffic chaos with more people opting to drive in. As a parent, I wouldn't feel comfortable with my child riding a bike past a large driveway, with vehicles entering and exiting at that time of day. You will have eliminated a sage way for parents and children to walk/ ride to school safely, causing further pressure on verges and residents surrounding the school.</p> <p>The use of the site is not appropriate for a two story childcare service. If you need proof for this, you should stand on Wilson St for the hour before school drop off/ pick up times and see the increased traffic. That site is absolutely appropriate as there no other main service along that road that it impacts.</p> <p>As a local resident I voice my concern with the issue, predominantly the type of service being suggested and the scale of the size. Your proposal advert also shows a single story building, when the proposal is cleary for a two story building. Please choose a site more appropriate, such as the excellent Wind in the Willows site, which is close in proximity to the school, but not so close it becomes another traffic problem for residents in the area.</p>	<p>Parking Traffic Reduced incentive for children to walk or ride to school Size and scale is inappropriate in this location Too close to school – traffic issues</p>	<p>Traffic and parking are discussed in the report.</p> <p>The suitability of the child care centre on land zoned 'residential' is discussed in the report.</p>

12	<p>I have added to my submission below the required reference No and included that the council and the department of Planning reject this proposal on West rd. I have done this to ensure the Dept of Planning accept my submission,</p> <p>While it looks like from what you're saying in your email that the Town has placed signage on the fence of the property in question and notified residents with in 100 meters of the proposed site, one just wonders how many of the Users of West rd can get a opportunity to read what on display on the fence because of the closed section on West rd and Bidson st due to Water Corp sewage replacement pipes thats been taking place, I fully appreciate that the Town has no control of that situation,</p> <p>It is a real concern that a developer can by pass the Town of Bassendean and go straight to the Department of Planning which really restricts proper process and proper community consultation , that I find is limiting community involvement process is disappointing</p> <p>I see there is a community consultation regarding the proposed Child care centre on the corner West rd and Bridson street, it appears the town are wanting to consult just only the people in close vicinity being to able to participate in the community consultation. this I find really not satisfactory as the town staff thought it was ok just to consult people in Parker Street and Watson street from Old Perth rd to Palmerston street regarding the consultation on the Bassendean Hotel re development ,when in fact the whole community used the pub and wanted to participate in the consultation one could have now the same applies to with this proposed child care location, one can say the community at large who live in Bassendean use West rd for a whole heap of reasons , including going down to Sandy Beach and accessing Ashfield by driving up Reid street . considering the amount of traffic that use West rd every day warrants a wider community consultation is my view ..</p> <p>I would like to submit my concerns below regarding the prosed child care centre at lot 85 - No 97 West rd see my submission below</p> <p>The fact that a Child Care centre which is a business being considered in a residential area really should not accepted by council. Currently West rd has traffic and parking issues because of the Last Crumb bakery which locals now describe as a night mare . the traffic at the corner of Bidson and West rd is already where drivers need to be cautious when entering West rd , just having a Child Care centre at that location will compound the situation even more for drivers wishing to enter West rd. all safety aspect needs to be considered .</p> <p>The amount of traffic currently using West rd has substantially increased and is busy already and its impacting on residents living on West rd experiencing with school drop off and pick ups at Bassendean Primary where 266 children attend that school just for starters , then people going to the very popular Last Crumb for their cakes and pies making West rd a very busy place iwth on street parking being premium during opening hours and school hours,</p> <p>The prospect of having a Child care centre just down the road a bit would just compound the problem even more with staff and parents accessing the child care centre on a very busy comed on Bidson st and West rd adding more traffic to this residential area is looking like a possible disaster could made because of bad planning decisions made by the Department of Planning .</p>	<ul style="list-style-type: none">- Inappropriate (commercial)use in a residential zone- Increased traffic presents a safety concern	<p>Traffic, and the suitability of a child care centre on land zoned 'residential' is discussed in the report.</p>
13	<p>Regarding the planning submission DA-2024-066 for the development of a Child Care Premises at Lot 85 (No. 94) West Road, Bassendean (corner of West Road and Bridson Street), my family and I wish to express strong objections due to several concerns:</p> <ol style="list-style-type: none">1. The location on West Road, which is already heavily trafficked, poses significant safety risks with a daycare centre accommodating young children.2. Insufficient parking on West Road, exacerbated by existing congestion from Bassendean Primary and the Last Crumb Cake Shop, will likely lead to cars parking on the road, potentially endangering children.3. Personal observations include near-miss incidents at the corner and an unfortunate incident involving a neighbour's dog on Bridson Street, highlighting existing safety issues.4. Given that the area is primarily residential, a daycare centre would appear incongruous and disrupt the neighbourhood's residential character. <p>These concerns are rooted in safety issues that directly affect our community, particularly as someone living in close proximity to this intersection.</p>	<ul style="list-style-type: none">- Increased traffic will impact safety- Insufficient on street parking- Unsafe intersection (West/ Bridson)- Daycare use inconsistent with residential character	<p>Additional parking bays have been included post-advertising. Traffic, parking and the suitability of the child care centre on land zoned 'residential' is discussed in the report.</p>
14	<p>I write to you with regard to the above town planning application. I live on Watson Street - just around the corner from the proposed child care site.</p> <p>Whilst I support opportunities for new business in the town I do not support the location of this service. Currently West Rd is heavily congested at school times during the week and on weekends due to the proximity to the last crumb cafe and during the football season. When the cafe is receiving deliveries during the week there is often large trucks further congesting west road making it dangerous for oncoming traffic.</p> <p>We often have to deviate around North Road/Old Bassendean Pde to avoid the single lane of traffic that West Road becomes. I am also concerned about the safety of children on bikes crossing Bridson street and using West road to get to school, lets add in the poor rubbish trucks and the postman into the mix and it's a recipe for disaster.</p>	<p>Inappropriate location</p> <p>Increased traffic is a safety issue</p>	<p>The suitability of the location and traffic are discussed in the report.</p>
15	<p>G'day, I live 4 doors down from where the proposed childcare centre will be built. I think it's a wonderful idea for the area, and I really hope it goes ahead.</p>	<p>Support noted</p>	<p>Noted</p>
16	<p>I write in reference to this proposal stating the following objections:</p> <p>Bridson Street is already a distributary road carrying a large amount of traffic servicing Bassendean Primary School and is a main thoroughfare for trucks wishing to access West Rd, North Road and Bassendean village centre, avoiding traffic lights at Collier Rd, Old Perth Road and Lord Street. This despite being one of the narrowest roads in Bassendean with little set back from the road to the homes on Bridson St. This development can only add to the heavy traffic load already experienced by the residents.</p> <p>Parking on West is already a nightmare on due to the popularity of Last Crumb and the location of the school so Bridson Street would then become a car parking zone to accommodate the extra traffic generated by this development. The proposed 15 parking bays is insufficient given that at most are likely to be taken up by staff. Given that lack of parking was cited as the reason for turning down the application for redevelopment as a microbrewery on the corner of Old Perth Road and Hamilton Street on an already commercial site and in a commercial area, because of the impact on local residents I don't see how this new proposal can even be considered.</p> <p>There appears to ample child care facilities in the locality, namely Mercy Care Child Care (Early Learning Centre) Kathleen St, Wind in the Willows on Wilson Street, Casa Mia (Montessori) child care (Early Learning) Whitfield Street and Camp Australia West Road. We don't have that many families living in this part of Bassendean, so where will the clientele come from?</p> <p>I sincerely ask that you consider the detrimental impact on our enjoyment of living in Bassendean and our day to day lives if this development goes ahead.</p>	<ul style="list-style-type: none">- Increased traffic on Bridson Street- Insufficient on street parking- Lack of parking onsite will lead to reliance on on-street parking- Sufficient child care services exist in the area for the community	<p>Additional parking bays were provided post-advertising. Each application must be determined based on its individual merits. Traffic, parking and the demand for child care services is discussed in the report.</p>

17	<p>I am writing to formally object to the proposed establishment of a childcare facility at Lot 85, No 94 West Road, Bassendean. My objections are as follows:</p> <ol style="list-style-type: none">1. The land was originally designated for residential use, and any commercial development may have implications for property values that were assessed under residential zoning.2. Bridson Street is classified as a distributor within the town and is already heavily congested with traffic, including heavy trucks avoiding Guildford Road traffic lights at intersections such as West Road, Old Perth, and Collier Road.3. Bridson Street is notably one of the narrowest streets in this historic part of Bassendean, with minimal verge width, which exacerbates traffic flow and pedestrian safety concerns.4. This street serves as a primary route for accessing Bassendean Primary School and is frequently used by trucks heading towards West Road, North Road, and the Bassendean village center to circumvent traffic lights at intersections like Collier Road, Old Perth Road, and Lord Street. It is important to note that traffic often exceeds the designated 50 km/h speed limit.5. Despite being one of the narrowest roads in Bassendean, Bridson Street lacks sufficient setback from residences, intensifying concerns related to traffic safety and noise levels.6. Existing parking challenges on West Road, exacerbated by popular local establishments like Last Crumb and the proximity of Bassendean Primary School, make it likely that Bridson Street would experience increased parking congestion if this childcare facility is approved.7. The proposed provision of 15 parking bays appears insufficient, especially considering that most spaces are likely to be utilized by staff. There are significant concerns regarding where clients and visitors will park.8. The recent decision to reject a microbrewery redevelopment application on Old Perth Road and Hamilton Street, based partly on parking availability and its impact on local residents, sets a precedent that should be considered in evaluating this childcare proposal.9. Local childcare needs are adequately served by existing facilities such as Mercy Care Child Care (Early Learning Centre) on Kathleen Street, Wind in the Willows on Wilson Street, and Casa Mia (Montessori) on Whitfield Street. Given the relatively low population of families in this specific area of Bassendean, questions arise regarding the source of clientele for an additional childcare center.10. As a long-term resident and ratepayer of Bridson Street, I am particularly concerned about the safety implications of introducing additional vehicular traffic to an already hazardous street, particularly concerning ingress, egress, and maintenance of the street's verge.11. A significant portion of our neighbourhood comprises elderly residents who spend considerable time at home. The anticipated increase in noise levels from dawn to dusk due to this development could significantly disrupt their quality of life. <p>I urge you to consider the adverse impact this proposed childcare facility would have on our community's quality of life and daily routines.</p>	<ul style="list-style-type: none">- Impact of commercial development on residential property values- Bridson has existing traffic congestion, narrow width- Traffic safety and noise- Will result/ increase parking issues associated with primary school and the Last Crumb- Insufficient parking bays onsite- Sufficient child care services exist in the area- Safety implications of additional traffic/ on street parking- Noise	<p>Traffic, parking, noise, demand for child care services and impacts on property values are discussed in the report.</p>
18	<p>I am writing as a concerned resident of 90 West Road to formally object to the proposed child care centre at 94 West Road. The reasons for my objection are detailed below.</p> <p>Firstly, the current infrastructure of West Road is not suitable for the increased traffic that a child care centre would generate. As a resident, I have observed the significant congestion that already occurs, particularly during school hours. The addition of a child care centre catering to 80 children will only exacerbate this issue, creating a dangerous and chaotic environment for both residents and visitors. The proposed 15 car parking bays are grossly inadequate to accommodate the volume of cars during peak drop-off and pick-up times. This will inevitably lead to parking overflow onto the street, causing further congestion and safety hazards.</p> <p>Secondly, West Road has traditionally been a residential area. The introduction of a commercial entity such as a child care centre disrupts the character of our neighborhood. This transformation from a quiet residential street to a commercial one is concerning and sets a precedent that could lead to further commercial developments in the future. There are already designated commercial spaces nearby that are better suited for such facilities. For instance, the child care centre on Palmerston Street is situated on a wider road and is nearly adjacent to commercial areas, thereby minimizing disruption to residential life.</p> <p>Additionally, the close proximity of another school in the area already contributes to significant traffic. Adding another high-traffic establishment on West Road will only compound the issue, making it increasingly difficult for residents to navigate their own street and potentially putting children at greater risk.</p> <p>In light of these points, I strongly urge the Bassendean Council to reconsider the approval of the child care centre at 94 West Road. I recommend that alternative locations be explored, particularly in areas that are better equipped to handle the associated traffic and parking requirements.</p> <p>Thank you for considering my concerns. I trust that the council will take into account the significant impact this development will have on the residents of West Road and make a decision that prioritizes the safety and well-being of our community.</p>	<ul style="list-style-type: none">- Road network cannot cater for increased traffic- Inadequate parking provided on site- Commercial development is inconsistent with residential character- Too close to school – compounded traffic issues	<p>Traffic, parking and the suitability of the child care centre on land zoned 'residential' is discussed in the report.</p>
19	<p>Haven't read anything of the supporting documents. Just let them build it, we need more childcare here. Waiting lists are getting out of control.</p>	<ul style="list-style-type: none">- There is a need for additional child care services in the area	<p>Noted.</p>
20	<p>Subject Matter: To NOT allow the site at 94 West Road, Bassendean to be developed into an Early Learning Centre (DA-2024-066). The site is zoned Residential R20 under the Town of Bassendean's local planning scheme 11 and a commercial development should NOT be approved by the DAP.</p> <p>We, the undersigned, are strongly against the proposed development of 94 West Road Bassendean into an Early Learning Centre and do not believe the zoning should allow for this development. This is a residential area, and we strongly believe it should remain residential.</p> <p>The proposed commercial development will impact our right to quiet enjoyment of our properties and cause undesirable traffic issues. The location of 94 West Road is on the corner of Bridson Street, which is used as a main artery to/from Guilford Road and is busy during peak times. Further, Hyland Street intersection is directly opposite 94 West Road, together with increased traffic from the busy Coffee Sop - Last Crumb and the Bassendean Primary School it will create absolute traffic and parking chaos furthermore, the development does not cater for the required number of parking bays.</p> <p>Fundamentally a commercial development of this nature needs to be in the appropriately zoned precinct and allow the appropriate number of car bays which this development application does not and as such should be rejected.</p>	<ul style="list-style-type: none">- Commercial development should not be approved on residential land- Noise- Traffic- Insufficient parking- 	<p>Traffic, parking, noise and the suitability of the child care centre on land zoned 'residential' is discussed in the report.</p>
21	<p>I write on behalf of my husband, ____, and myself. We have lived 100m from the proposed childcare centre for twelve years.</p> <p>We OPPOSE the Proposed Childcare Centre on 94 West Road, on the corner of Bridson & Hyland Streets.</p> <ul style="list-style-type: none">• This site is zoned residential and has been bought by a Commercial venture.• West Road is a busy Neighbourhood Connector and traffic is already at capacity servicing the local community, Primary School, bus route and busy café.• The corner location does not support drop off and picking up of small children. There is no further capacity for street parking.• There are several childcare centres in Bassendean - we need more residential housing.• There are other, more suitable sites in Bassendean for childcare services.• The Development Application has been made to the DAP, bypassing the community. <p>We received a letter from Briscoll Properties hand delivered in our letterbox on Tuesday 25th June (although I was at home) and received a letter from Town of Bassendean</p>	<ul style="list-style-type: none">- Commercial development is inappropriate in the residential zone and should be located in the Town Centre- Traffic- No capacity for on street parking- Residential development is preferred due to housing shortage- There are more suitable sites for a child care centre in the area	<p>Contemplating alternative, preferred uses for the site, or the fact that there may be alternative sites suitable to accommodate a child care premises in the area are not relevant considerations. The DAP is required to consider this proposal on its merits.</p> <p>Traffic, parking, and the suitability of the child care centre on land zoned 'residential' is discussed in the report.</p>

	<p>by post yesterday Wednesday 26th June. Written submissions were supposed to be received by 5:00pm Friday 5th July, although the advertising period for DAP should be 14 days which would be the following Friday 12th. The advertising near the site is currently obscured by the very imposing sewer roadworks on the intersection of Bridson, West and Hyland Streets. 14 days isn't sufficient time to digest the amount of documentation about this proposal. Given that the RDAP report apparently said that this project rates high for "Community" and in their report they say that their projects are informed by communities, not imposing generic developments upon, communities. As such we were surprised to learn about this commercial proposal in a residential area, through the Bassendean telegraph.</p> <p>Quiet Enjoyment</p> <p>In Western Australia, tenants have the right to quiet enjoyment of their premises, owned or rented, without disruption. This means that the third parties cannot to do anything or allow anything to happen that may interfere with the tenant's reasonable peace, comfort, or privacy. The location of a childcare centre, on a busy intersection, in a residential area will cause significant disruption to the quiet enjoyment of all surrounding residents, and we are the ones that will have to live with the consequences of this poor decision, in perpetuity. It is unfair that the residents should have to suffer the consequences of a poor business decision by parties that do not live in this community. And it is unfair that the application goes directly to the Development Approval Panel, circumventing genuine Community Consultation and that the onus is put on the community to defend our quiet enjoyment.</p> <p>Residential Land Use</p> <p>West Road is a Neighbourhood Connector within a residential area, connecting the activity centre of Old Perth Road at the northern end to Sandy Beach Reserve at the southern end. Town of Bassendean have recently undertaken urban design visioning process of "Bassendream" and a rigorous review of the Local Planning Policy No.11. BOTH concurred that West Road is residential connector within a Residential precinct. There are many areas within the Town where Childcare services would be more appropriate. It is a pity that the developers did not</p> <p>a) consult the community, or</p> <p>b) do their Due Diligence.</p> <p>Both would have revealed that this site is not appropriate and they would have discovered there are another two sites that would be ideal, and mutually beneficial to the developer and the community.</p> <p>Bassendream</p> <p>Housing - supported:</p> <ul style="list-style-type: none">• better quality residential infill• developing more mixed use precincts (e.g. apartments on top of shops)• higher density close to train stations and train stations <p>Activity Centres – supported:</p> <ul style="list-style-type: none">• mixed use on Old Perth Road and within walking distance of the train stations:• diversity of landuses within town centres.• integrating community services and facilities with commercial places• more people living in Bassendean town centre <p>Bassendream</p> <p>Your Town Your Centre Your Street Apartments and mixed-uses near the Bassendean train station and on Old Perth Rd</p> <ul style="list-style-type: none">• Higher density development in centres and along transport corridors• Reduced density further from town centres and train stations• Prioritisation of active and public transport• Combination of commercial, community and residential uses• A balance between increased development and retained character <p> Tree and open space maximisation</p> <ul style="list-style-type: none">• Sustainability initiatives• Moderate density increases• Communal/shared space• Build higher to minimise building footprint and maximise open space <p>All of these points support locating a childcare centre closer to the town centre, where there is mixed use and higher density residential.</p> <p>"Childcare Desert"</p> <p>According to the mapping, the majority of the Perth Metro area is "Childcare Desert", including National Parks that are shaded red... because they lack childcare... There are childcare centres within the town and also closer to the work of the parents, as often it is preferred to seek childcare closer to the workplace to reduce time needed to commute outside work hours, i.e reducing the time required for childcare before and after work, and the time allowance to get to pick up after leaving work. That is to say, not all parents that live in Bassendean would prefer childcare closer to home. Many parents expressed their concern at the location of the proposed childcare centre. We are not averse to childcare centres within the Town, but this is objectively a terrible location. It is unfortunate that the developers proceeded so far with the proposal without seeking consultation with the surrounding community and without doing their Due Diligence. However, it is not the fault of the community that they have invested without investigating thoroughly.</p> <p>Housing Affordability</p> <p>As per Perth and Peel 2034, Bassendean is required to provided more residential dwelling units. This residential block had one house owned by an elderly lady. The developers bought this block that is zoned Residential at a cheaper price than had they bought a block zoned commercial. Several local residents were interested in the block and had been watching the block, but it sold as soon as it came to market at a higher than residential price. This effects the cost of residential land, and therefore housing affordability, when the block value is bumped up by a commercial business. It also removes one residential dwelling unit that could potentially be 3 townhouse dwelling units, having a nett gain for housing stock in Bassendean.</p> <p>Potential Childcare Sites</p> <p>It is not as though this is the only location for a childcare centre in Bassendean. In fact, there are at least two other sites that are far more suitable for childcare. The first is on the corner of Old Perth Road and Hamilton St, Bassendean The frontage onto Old Perth Road, with substantial area for parking at the rear. It is located 50m from Palmerston Park. The neighbour on Old Perth Road is a wine bar that operates at different hours so there could be reciprocal rights of parking. It is located adjacent to St Michael's School, and near to aged care. There is plenty of space on Hamilton street for Pick Ups & Drop Offs and potential shopfront for supporting business space for supplementary cashflow, and convenient childcare for surrounding businesses. Old Hardware Store, corner Old Perth Road & Hamilton St Old Hardware Store - parking</p> <p>The other potential site is the existing Child Health centre at the end of James St, Bassendean. The site is nestled between St Michael's School and BIC reserve, and walking distance to Bassendean Train Station for parents commuting to the City for work. The site is owned by the Town. The Town had agreed to relocate the Child Health Centre but does not have the funds. This would be potentially a mutually beneficial colocation of Child Health Service and Child Care Facilities. The Child Health centre could be run from the vacant old Post Office on the corner of Old Perth Road and Wilson St. This site has excellent parking and universal access.</p> <p>The bottom line is that 94 West Road is zoned residential and it is not the only potential site for childcare in Bassendean. The other sites are far more appropriate for the business and for the surrounding areas.</p> <p>It is unfortunate, but the onus of the company that they have invested in a site that is not appropriate for use.</p> <p>Transport</p> <p>94 West Road is at the intersection of West Road, Bridson Street and Hyland Street. West Road is a 'Neighbourhood Connector, connecting the shopping centre, Old Perth Road precinct and Steel Blue oval to the north, to the residential area south of Guildford Road. West Road also connects the suburbs north of the train line, Eden Hill,</p>		
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	<p>Success Hill to the southside. Guildford Road arcs in a north easterly direction, as such both West Road and Bridson Street/Strickland Street both carry the residents of Bassendean to and from Guildford Road, intersecting at the proposed childcare centre on the corner of Bridson St & West Road. Residents east of West Road travel along Hyland St, then zig zag across West Road onto Bridson St to get to Guildford Road.</p> <p>Parking Requirements</p> <p>The Last Crumb & Co café and bakery is 220m from the site, and Bassendean Primary School is 300m from the site. Bassendean Primary School generates traffic mainly around drop off and pick up times each day, which are the same times as drop off and pick up for Childcare Centres. The primary school is especially busy on Fridays when there is school assembly, or when there are events at the school such as school carnivals.</p> <p>Parking on West Road – Sunday 30/06/2024 11am</p> <p>The Last Crumb & Co café /bakery generates an enormous amount of traffic and parking throughout the week. The parking spills over on to surrounding residential streets</p> <p>The last Crumb & Co. generate an extreme amount of traffic and parking, especially around school drop off time. This parking has spread to surrounding residential streets as well as both sides of West Road, causing traffic issues as it effectively becomes one lane. The clientele park over 150m in each direction from the café, within 100m from the site.</p> <p>Community Consultation</p> <p>This development has been sent directly to DAP, bypassing Community Consultation. In fact, we did not receive the notification letter from Council until Wednesday 26th June, shortening out 14 days to respond. The signs at the site have been obscured by substantial sewer works by Water Corp, meaning that there is not access to the signs by pedestrians or daily traffic. It is also currently school holidays when there is less incidental traffic, although that is superseded by the temporary fencing corral – therefore many members of the community were not aware of the intended Development Application. I hope that this extremely rocky start to community consultation isn't an indication that this development is considered a fete accompli. The onus has been put on the neighbours to defend this change of use, when we are the ones that will have to live with the effects on a daily basis, affecting our quiet enjoyment of our homes, the most substantial investments of our lives. The closure for submissions should have been postponed by at least a week or two due to the delayed delivery of the letters notifying residents. We are not opposed to childcare services in Bassendean, but we are STRONGLY OPPOSED to the development of a Childcare Centre "in this location" and ask that this Development Application be refused and considered in a more appropriate location.</p> <p>We hope that future proposed community developments involve genuine community consultation, not go straight to DAP. This process puts residents on the back foot, puts the onus on us to defend our basic rights to quiet enjoyment and gives developers the upperhand in appealing decisions, as we have to raise funds to defend our basic rights.</p>		
22	<p>I am writing to express my deep concerns regarding the proposed daycare center at Lot 85 No. 94 West Road, Bassendean, which is of great interest and importance to our community.</p> <p>One of the primary concerns I have is the potential increase in traffic on West Road, which already serves as a major bus route. With the addition of drop-offs and pick-ups associated with the daycare center, the volume and frequency of vehicle movements could significantly increase traffic issues. This poses not only inconvenience but also safety risks, particularly for children and pedestrians crossing the street. The daycare center's location at a busy T-junction further exacerbates safety concerns. The combination of increased traffic from drop-offs and pick-ups, alongside existing bus and pedestrian traffic, poses additional risks.</p> <p>Moreover, I am deeply concerned about the parking situation. The fact that the entry to the daycare center's parking lot is on the same street as the bus route raises serious safety concerns. This arrangement could lead to conflicts between vehicles entering and exiting the daycare premises and buses or other traffic using the same route. Additionally, the limited number of onsite parking bays—only 15—are insufficient to accommodate the needs of both staff and parents during drop-off and pick-up times. With 13 staff members utilizing parking spaces, only 2 bays remain for parents to safely manage the drop-off and pick-up of approximately 80 children.</p> <p>Furthermore, the issue of waste management is crucial. Proper disposal of garbage and diapers is essential to prevent any potential odors or hygiene concerns that could adversely affect nearby residential properties.</p> <p>Another significant concern is the fact that the land is zoned residential; the introduction of an establishment which is non-residential would impact significantly on the privacy and tranquility of surrounding houses, particularly with the proposed double-storey structure. This could potentially compromise the privacy and living conditions of neighboring residents.</p> <p>In conclusion, while I recognize the importance of providing childcare services within our community, it is imperative that these services are implemented in a manner that respects and accommodates the existing residents.</p> <p>Thank you for your attention to these important matters.</p>	<ul style="list-style-type: none">- Traffic- Access- Parking- Waste management (odour management)- Privacy (2 storey development)- Non-residential use in the residential zone	<p>Additional parking bays were included post-advertising. Traffic, access, parking, waste management and the suitability of a child care centre on land zoned 'residential' is discussed in the report.</p> <p>Upper floor windows are located to avoid overlooking of adjoining residential properties</p>
23	<p>As discussed below, there are several areas in which believe the proposed childcare is deficient and consequently should be rejected by the relevant competent authorities. In this submission I discuss:</p> <ul style="list-style-type: none">• The claimed need for childcare facilities at this location;• The overall suitability of the proposal;• The provision of parking on the site in the proposal;• Effect on local vehicular traffic;• Safety of the proposed parking area. <p>Issues</p> <p>Need for Childcare Facilities at This Location</p> <p>The covering letter makes some claims about the need for a childcare facility at this location. These should be examined and considered with care.</p> <p>The claim of a "childcare desert" firstly should be noted as somewhat dated. There are currently at least 7 childcare centres in the Bassendean-Ashfield-Eden Hill area. The map provided shows that this area is far from a high priority area in terms of provision of childcare places in the Perth metropolitan area and does not take account of recently opened centres. The map does also show that in the adjacent area of Guildford there is a very high rate of provision of childcare places – all of which are accessible to the people of Bassendean-Ashfield-Eden Hill.</p> <p>Furthermore, the report does not take into account the structure of work and transport in the Perth metropolitan area. In fact, many parents like to be able to take their young children to childcare places closer to the centre of Perth where they work. In that way they are able to spend longer with their offspring each day. In other words, access to childcare should not be assessed only on "local" places.</p> <p>In summary, a claim that this is a good location in terms of serving the needs of the people of Perth or specifically the Town of Bassendean, does not withstand close</p>	<p>Lack of demand for child care services in the area</p> <p>Parking - Internal layout, provision, design and functionality</p> <p>Insufficient on street bays to cater for overflow parking</p> <p>Insufficient parking on site will lead to congestion at access point</p> <p>Delivery vehicles manoeuvres have not been shown</p>	<p>Parking, access and traffic are discussed in the report. The applicant is not required to demonstrate demand for the service as part of their application.</p> <p>The design of the car park has been updated post-advertising. Manouvering areas and parking bays meet the relevant Australian Standard</p> <p>The proposal has undergone further review by the DRP, and is supported against all 10 principles of good design outlined in SPP 7.0.</p>

<p>scrutiny. This is without consideration of the location and design specific issues addressed below.</p> <p>Overall Assessment by DRP</p> <p>Apparently the proposal was assessed by a Design Review Panel (DRP) in February of 2024. Judging by the assessment summary provided in the proponent’s submission (reproduced below) the DRP did not rate it highly. As can be seen in the table, only two of the 10 Design Principles were given the “green light” while the other 8 were rated in the “orange light” (apparently meaning unsatisfactory but not the worst possible). It is claimed that the proposed development has been amended in response, but I was unable to find what those amendments are.</p> <p>In the comments I will touch on some of the matters apparently covered by these design principles. Most alarming is the vehicle parking arrangement in the proposal. This is a severe safety issue for the users – particularly young children – of the proposed centre. These are exacerbated, however, by the location and consequent road traffic difficulties.</p> <p>Parking Provision on Site</p> <p>The report <i>Transport Impact Statement</i> by KCTT (KC Traffic and Transport Pty Ltd), paid consultants to the proponents of the childcare centre proposal, purports to analyse parking requirements and assess proposed parking arrangements and local traffic implications.</p> <p>There are, however, several aspects of these matters that have not been addressed or only partially addressed. I note that the report has apparently used for assessment the old LPS 10 rather than the current LPS 11 (refer page 5 of report). It also wrongly refers to Transperth bus route number 55 rather than the current 45 (pages 6 and 18). It further fails to note on page 6 that the on-street parking near the Bassendean Primary School is highly contested throughout the morning by the patrons of the extremely popular The Last Crumb bakery and café, essentially rendering these parking places unavailable to users of a childcare centre at the site proposed.</p> <p>On page 9 of the KCTT report the first point suggests that the proposed centre can place an extra burden on parking near the Bassendean Primary School. However, the cited reason can also work in the other direction. That is, a parent having secured a parking spot at the childcare centre and unloaded one child there may be more than a little inclined to walk with their other child or children to the primary school and not take the risk of being unable to get a convenient parking spot at the PS, not to mention go through the drill of having to get the primary school child back into the car, buckled up, etc. Similar consideration hold at pick up time as well. Clearly the suggestion that the proximity to the primary school might lessen the need for parking at the childcare centre should be discounted.</p> <p>The consultants seem to have missed the point that the LPS parking space requirements already take into account that some local people might walk to the proposed centre, so that should not be counted as a double discount to required number of parking spaces.</p> <p>It is also noted that as stated there 4 bicycle parking spaces – for staff. So that does not lessen the requirement for patron, customer or supplier spaces.</p> <p>Much (but thankfully, not all) of the report’s estimates and assumptions are based on averages (e.g. page 10, average dwell time). These are not appropriate because systems are not undone by averages. It is the extremes that cause chaos and major problems. It is therefore more appropriate to use assumptions based on more atypical parameters such as 85th percentiles. [To be fair to the report, this has been done in some places.]</p> <p>In regards to use of AS2890 (2004) as the basis for assessments of adequacy of parking areas, it should be recognized firstly that this non-compulsory standard was formulated more than two decades ago and that secondly, it represents minimum acceptable standards (as envisaged at that time) rather than optimum or desirable standards. Something that should be recognised is that the Australian vehicle fleet has increased in average, typical and 85th percentile vehicle width and (especially) length. Nowhere in Australia is this more likely to be true than in the suburbs of Western Australia.</p> <p>For this reason professionals in the design of car parks now recommend larger (wider and longer) vehicle parking spaces in public car parks. For example, see current information on trafficparking.com.au.</p> <p>Report (2023) by Traffic and Parking Systems Pty Ltd concluded that vehicles using car parks have got longer or wider since AS/NZ 2890.1 (2004) was written and that in answer to the question “Do we need to change our car park design standards ?” the TPS investigations give support to at least considering the following changes to car parking design having regard to the need to minimise consequences for land, structure and other construction costs.</p> <p>a. Maintain the length of bays at 5.40m across all categories of user, providing the following minimum aisle and bay dimensions are implemented.</p> <p>b. Increase minimum bay widths to 2.60m across all categories of user, excepting increase widths to 2.70m for short term convenience parking where parking is specifically intended for the loading/unloading of goods and passengers (eg. childcare centres etc.). [my emphasis added]</p> <p>c. Increase minimum aisle dimensions across all categories of user to 6.20m, excepting increase to 6.50m for short term convenience parking (eg. 15 minutes duration). [my emphasis added]</p> <p>d. Increase the width of Disabled Parking Bays to 2.6m together with a minimum adjacent loading zone of no less than 2.0m</p> <p>I include here a quote from “Car Park Layout Consultancy & Design Standards Australia” (trafficparking.com.au)</p> <p>In Australia, car park design standards mandate that disabled parking spaces be significantly wider and longer than other spaces. This is to allow for enough space for a wheelchair to maneuver on and off the ramp or hoist. These designated spaces should be clearly marked so that they are easily identifiable.</p> <p>Disabled parking spaces need to be wider and longer than other spaces in a car park, with an area of at least 1.2 m × 1.2 m beside or to the rear of the vehicle, as well as another 1.2 m × 1.2 space beyond that for wheelchair maneuvering.</p> <p>The proposal does not meet these standards and should not be accepted especially in view of the discussion of safety below in this submission.</p> <p>Effect on Local Vehicular Traffic</p> <p>The KCTT report also contains discussion of effects on local vehicular traffic (it is after all titled “Transport Impact Statement”). It includes some data on anticipated trips generated from and attracted to the proposed childcare centre. These show significant additional trips to the north and south of the proposed centre along West Road. As part of this it shows (average only) number of anticipated entries and exits from the car parking area.</p> <p>However, since these data are not linked to and interpreted in view of the complexity of entry to, exit from and turning within the parking area and consequent waiting times from executing these maneuvers, they are rather meaningless. The rather alarming complexity of these factors, with a particular focus on safety, are discussed below.</p> <p>I note also that the comment on page 21 of the report about Bridson Street providing a “Good Riding Environment”, must have been provided by someone on a different Bridson Street or was thinking only about the smoothness of the road surface.</p> <p>The fact is that the entry to the parking area, in particular, has great potential to cause major difficulties and hold ups to users of West Road. This is because of the small number of parking spaces (compared to potential users) and the elaborate maneuvers that will prevent simple entry to and egress from the parking area. Those wishing to enter the parking area will be forced to wait on West Road until those inside have completed their maneuvers and are out of the way and that additionally there is a parking space free.</p> <p>Safety of Parking Area</p> <p>Tied with the effects of traffic movements on West Road is the matter of safety within and adjacent to the parking area. The KCTT report includes as Appendix 3 some vehicle turning plans – the last 5 diagrams of the Appendix. These are based on maneuvers to be executed by a 5.2 m long vehicle (think Toyota Camry with tow bar). What they reveal might well be regarded as a safety nightmare and a vehicle repairer’s picnic.</p> <p>For example, one of the diagrams shows the 5.2 m long vehicle entering the western most vehicle bay. It shows that the vehicle needs to go unrealistically (i.e., very close) to other parked vehicles and sweeping the pedestrian area at the bottom the stairs</p> <p>The report does not include anything resembling a Vehicle Movement Safety Analysis and Assessment (VMSAA).</p> <p>In my view (to mention just a few of the issues raised by the diagrams):</p> <p>The movements should have been correctly interpreted in the report. For example, they show no allowance for space around vehicles in executing turns.</p> <p>The potential movement of young children in the car parking area should have been considered.</p> <p>There are far too few maneuvers examined (which in the case of the KCTT report just means diagrams generated).</p> <p>The selected vehicle size is inadequate to capture the danger and complexity of vehicle movements for entry to parking spaces, exit from parking spaces and turning within</p>		
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	<p>the parking area. For example, what would be required of a driver of a Ford Ranger, currently Australia's biggest selling vehicle and not uncommonly used by parents to transport their children. Delivery vehicle maneuvers should have been considered.</p> <p>I believe that on safety grounds alone, the childcare proposal should be rejected.</p>		
24	<p>My office has been contacted by a large number of concerned residents regarding the plans to develop the 2 storey childcare premises, which plans to cater for up to 80 children and 13 staff. The hours of operation are intended to be between 7:00am and 6:30pm.</p> <p>My constituents have expressed strongly held views against the development on various grounds. Residents are opposed to a large scale commercial development in an area zoned Residential R20. West Road and the adjoining streets are part of a quiet residential area. The proposed development is contrary to the current zoning. To approve the development would make a mockery of the whole planning process.</p> <p>The existence of a school on West Road and a bakery does not change the residential character of the street. Both the school and the shop have been in that location for decades and are permitted under the planning scheme. The Childcare centre is not permitted under the planning scheme and was not envisaged by existing or new residents in the area.</p> <p>Another issue that has been raised relates to the increase in traffic which will occur because of the development. The Childcare centre will require up to 80 pickups and 80 drop-offs per day plus staff movements. In the narrow street already congested by school traffic the potential for traffic chaos is obvious. It is unwelcome and unsafe for the young children who we are encouraging to walk to school.</p> <p>There 16 car parks proposed for a commercial provider consisting of 13 staff and care providers for up to 80 children. It is most unlikely offsite parking could be utilised as already there is a lack of parking spaces available due to the location of the Bassendean Primary School and the very popular and busy Last Crumb Coffee shop. The potential for additional noise and movement of cars into and out of the area is unwelcome.</p> <p>Some residents are also concerned about the early and late opening times proposed by the developer.</p> <p>My last comment relates to the minimal period of time open for the community to respond. For a development of this scale, I would anticipate that residents would be entitled to a consultation period equivalent to proposals co-ordinated by the Town of Bassendean. I am told that as little as 41 houses were sent a letter regarding the proposed development.</p> <p>Residents have also told me the A3 sign at the site has been missed given its minimal size and because the sign is obscured due to Bridson Street and the footpath being currently closed due to work being undertaken by the Water Corporation.</p> <p>For these reasons I do not support this DAP application.</p>	<ul style="list-style-type: none">- Commercial development in residential zone- Traffic- Parking	<p>Additional parking bays have been provided post-advertising. Operating hours have also been revised, with the operator agreeing not to open prior to 7.15am to limit noise impacts on neighbours.</p> <p>Scale, traffic, parking, and the suitability of the child care centre on land zoned 'residential' is discussed in the report.</p> <p>The application was advertised as prescribed under the Planning and Development (Local Planning Scheme) Regulations 2015.</p>
25	<p>The developer's application for a Day Care centre in Bassendean is inaccurate and lacks integrity. There has been a deliberate minimisation of facility amenities and deflection of effects on surrounding residents in order to have this proposal get through on desired budget without full respect of the highly predictable outcomes. This does not give confidence in the integrity of this proposal.</p> <ul style="list-style-type: none">- They have used an uncertified Day Care internet search engine as their validation. It lists 23 day care facilities listed for Bassendean, the developer indicates from this resource that there are no vacancies available, when it clearly shows 16 daycare centres have vacancies.- The proposed day care centre is surrounded within residential area. *See Map. It is not adjacent to a school, as the school is 300 metres away with several houses between the school and the proposed development. This stretch is one of the safest walking routes to Bassendean Primary school as it does not have any existing commercial enterprises. <p>Existing residents right to peace & quiet</p> <p>In residential area, not in commercial. Residents have bought their properties based on what is existing.</p> <p>Parking</p> <p>Minimum of 13 staff – where do the other 5 cars park. Also, these parks are back to back, causing issues when people need to leave. Staff will park on roads or verges for convenience. There is no quick drop location.</p> <p>Developers justify the parking reduction by distributing peak and non peak times. However peak times will require a full staff of plus 13 at the same time 80 families will be needing parking. Cars will park on road constricting passing traffic to single lane, whilst also close to two street corners restricting visible access to traffic. Most families will drive there as part of their ongoing route to work. Safety of young pedestrians walking to school will be of concern with all the additional driveway movements.</p> <p>Allowance for delivery vans and tradespeople, how has this been factored in.</p> <p>Parents are more likely to drop off younger children first at daycare, followed by siblings to school.</p> <p>Land Size</p> <p>The small land allotment directly next door to residents, will destroy their right to peace and quiet. With an additional upper storey playground required the noise will echo out.</p> <p>Roads</p> <p>Intersecting roads are only 8 metres wide. This already creates difficulty passing when vehicles are parked on the side roads. Major bus route, with bus stop opposite road about 20 metres away. These roads are already quite busy which will be exacerbated by approx. 190 plus extra car movements per day. Major route to school. This stretch from Bridson to Palmerston is the longest area of footpath without a road.</p> <p>Please see attached map showing safety hazards and areas of concern with this location. I am not objecting to extra daycare facilities in Bassendean, just the proposed location. It should be in a more commercial area with wider roads and better access in and out.</p>	<p>Application contains misinformation Insufficient demand for child care service in the area Insufficient parking Traffic and capacity of the road network Commercial use is unsuitable in residential area Noise Impact on residential character and amenity Insufficient road network/ capacity</p>	<p>The tandem car park arrangement is not unique in a child care setting, noting short-term (parent) and long term (staff) parking needs differ.</p> <p>Traffic, noise, parking, and the suitability of the child care centre on land zoned 'residential' is discussed in the report.</p>
26	<p>I am a resident and the owner occupier... across the road to the proposed development site. I have been a resident of Bassendean for 25 years – 13 years. I am strongly against the development of Lot 85 (No. 94) West Road, Bassendean ("the site") into a childcare centre.</p> <p>My views, objections and concerns are listed below.</p>	<p>Suggestion the proposal involves a parent works space and other services similar to other centres Suggests misinformation on staff numbers as no mention is made of catering or reception staff Use is inconsistent with residential character of the area</p>	<p>Additional parking bays were included post-advertising. The applicant has demonstrated parking demand can be accommodated onsite. Designated staff bays are detailed in the operational management plan.</p> <p>Parking, built form, access (including street tree retention), suitability of the child care centre on land zoned 'residential' and noise are discussed in the report.</p>

	<p>visitors will park on the surrounding roads and verges directly outside the childcare centre, on West Rd and Bridson Street. This is grossly unfair to local residents to have excessive overflow parking and crowding outside of their properties along West Rd, Hyland Street and Bridson Street, and it creates Traffic and safety issues.</p> <p>There is a pedestrian access gate on the Bridson Street corner which will further promote the use of off site road/ verge and surrounding property parking. This will also pose further risk to residents with the traffic safety issues the T junction already has.</p> <p>The Parking and traffic issues discussed in this section are incredibly important for the surrounding residents right to peaceful enjoyment of their property in a residential area.</p> <p>Secondary Street Setback</p> <p>Please refer to Architectural drawing 07of 10 Ground Floor Plan. The plan shows the verandah and supporting structure built to the boundary. The verandah exceeds 10m in length and the eaves, gutters and roofs are NOT setback 450mm from the boundary. As a neighboring property I object to the deviation from the RD Codes.</p> <p>Noise and Disruption</p> <p>This amount of noise and chaos that the childcare will generate is grossly unfair, impacts my right to peaceful enjoyment of my property and is significantly inconsistent with the residential area. If the property was developed with residences, the above would not be an issue.</p> <p>Crime prevention through environmental design</p> <p>Please refer to the carpark plan above.</p> <p>There is no site security documented to the carpark. The open to street plan will provide concealed isolated areas causing risk to the neighboring residences with antisocial and undesirable behaviour. If the carpark is lit at night to mitigate this, it will impact the adjoining properties causing discomfort and affecting rights to peaceful enjoyment of residences. If a carpark fence and gate are designed, it will impact the streetscape by having continuous 1.8m high fencing to the entire site which is a poor design outcome. Refer to The Operational Management plan page 8 point 7 states: <i>the carpark security gate will remain open throughout the day</i>. The developer should confirm the gate design and provide detailed elevations of the streetscape.</p> <p>There is direct access to the first floor playground via the carpark stairs on the west wall. There seems to be a gate documented at the top of the stair however this creates a zone for unauthorized access and impacts adjoining properties.</p> <p>Waste management</p> <p>Please see the following bin store layout and refer to the Operational management plan and the sustainability Statement.</p> <p>The acoustic report stated the AC units are to be mounted at a high level inside the bin store. Bin store walls are at 1.8m high so the units will be visible from the street and compromise the bin store use.</p> <p>The Operational Management Plan states <i>A separate FOGO waste collection service is not proposed. All FOGO waste will be included in the General Waste collection service</i>. This does not align with the Town of Bassendeans waste management policy. The proposed waste management strategy contradicts the Sustainability Statement 'Garden to Plate' program and 'Recycling and repurposing' philosophy.</p> <p>Any development MUST at a minimum adhere to the Towns waste management policy and have sufficient area to accommodate the bins. If the developer took the sustainability statement seriously, they would incorporate separate storage for comingled recycling, cardboard and soft plastic.</p> <ul style="list-style-type: none">Arborist report for the significant tree adjacent to the crossover <p>The crossover design is a deviation to the council standard detail. It directs 2 way traffic over a public pedestrian access way causing further safety concerns to local children walking to the Primary school. The significant tree location has been blamed for the use of tandem parking in the carpark. The developer should provide an arborists report confirming a tree protection zone and to further confirm the crossover construction and potential vehicle movement will NOT impact the trees health and wellbeing.</p> <p>In conclusion, and in reference to all points and matters listed above, I strongly disagree with the development of 94 West Road Bassendean into a childcare centre. I do not believe it is an appropriate location and the site should remain residential. Given the housing shortage WA is facing, the site would be better suited as a residential development.</p> <p>Majority of the surrounding residents and community do not support this proposed development as indicated by the number of signatures on the petition that has been provided to the Town of Bassendean and The Hon. David Kelly's office.</p>		
27	<p>We strongly oppose this development for the below reasons:</p> <ul style="list-style-type: none">Purely on the basis of traffic management problems367 students attend the primary school and 42 teachers and staff are at the school. Some growth is expected in the coming yearsThe school has no local intake area, students are likely to be enrolled from both within and without the areaThe school only has 2 street frontages, the primary one being West RoadThere is limited on-street parking on West Road, but can only be used by vehicles travelling northParking on-site is limited to about 22 bays and are reserved for teachers and staffThe idea that there are no traffic issues around the school is nonsense as noted in the report. The fact that parents will drop off their kids at the school and just toddle off down the road to the ELC, is rubbishThe report makes no mention of Last Crumb that generates a LOT of traffic in both directionsThe ELC is expected to have 80 kids with 13 staff and 16 car bays, 5 below the 21 required. So there is hardly enough bays for staff and if parents are dropping off in the car park cars will backing up and possibly into the streetThere is no on-street parking on the side road to the ELC and the road is narrowThe elevated play area may generate excess noise	Traffic Parking Noise	Traffic, parking and noise are discussed in the report

	<ul style="list-style-type: none">It's a bit ridiculous thinking the ELC staff will mostly be dropped off or ride bikes.There is already existing congestion on West Road and this development will only compound it. <p>We have previously shared, with the council, parking issues on our street from Last Crumb - this would make it even worse. We hope that the developer reconsiders this as chatting with our neighbours, we all agree that this is not the right block or area of the Town for a childcare centre.</p>		
28	<p>Please accept my objection to this:</p> <p>Already too much traffic there. Is a great spot to build houses. We surely need more houses in this housing crises. There are much better spots in Bassendean to put a child care centre</p>	<p>Traffic</p> <p>Other uses are preferred on this site</p> <p>More suitable sites exist for a child care centre</p>	<p>Traffic is discussed in the report</p> <p>Contemplating alternative, preferred uses for the site, or fact that there may be alternative sites suitable to accommodate a child care premises in the area are not relevant considerations. The DAP is required to consider the proposal on its merits.</p>
29	<p>As a homeowner / rate payer living within fifty metres of the proposed development. I submit my objection to the proposal for the concerns summarised below.</p> <p>Increased Traffic: The intersection of Bridson St , West Rd and Hyland St is already busy. This has recently been increased by the growing popularity of the Last Crumb Bakery (approximately 170m away) creating traffic congestion on most days. The Bassendean Primary School (approximately 200m away) with its 300 students and staff is busy at drop off and pick up times. This a bus route, Buses pass the proposed carpark entry regularly, running along both West Rd and Hyland St (Route 45) West road is narrow (approximately 7.9m wide) and if vehicles are parked along the kerb there is only passing room for one vehicle. This causes drivers distraction and to take risks.</p> <p>Pedestrian Safety: In addition to the increased traffic concerns above there will be an increase of hazards for local pedestrians. The proposal indicates carpark entry off the West Rd side, with the proposed number of Children and staff, I would expect 150 to 200 traffic movements per day across a suburban footpath crossover. The footpath is a regularly used by children walking to and from school, often without adult supervision. The footpath is used by residents as a popular walking route into the Old Perth Road Shopping area. The potential for pedestrian / vehicle collision, does not align with the Town desire to be pedestrian friendly. The lack of onsite parking in the proposed design will create traffic congestion. There are 15 bays for 13 staff, which leaves 2 spare bays. Where will the 80 parents be able to park and drop off , food deliveries etc.?</p> <p>Increased Noise Levels: The proposal indicates that potentially over ninety individuals could be present at the facility during operating hours. The lot size is 1239m2 , the play area is on the first storey , well above Street level and open air. Considering the number of people in a relatively small and elevated area the noise levels would likely exceed those acceptable for a residential area.</p> <p>Childrens safety: The raised first floor play area presents a hazard for the children with a potential fall from height. There is also a potential risk for items to be dropped to ground level, creating a struck by hazard.</p> <p>Better suited development locations: There are likely more suitable locations in the Town of Bassendean for the Proposed development. Old Perth Road and Guildford Road are closer to Shopping and Transit hubs; these areas are already considered to be preferred areas for higher density developments.</p> <p>Better use of residential land: This lot may be better suited to a residential development for family homes, given the current housing shortage in the Greater Perth area.</p> <p>I am not opposed to the Town of Bassendean increasing and improving facilities available to residents, I simply feel this location is not a suitable location for the development proposed.</p>	<p>Traffic</p> <p>Pedestrian safety at access point</p> <p>Insufficient parking</p> <p>Noise</p> <p>Safety associated with multi-level facility (children falling, throwing items from heights)</p> <p>Better suited locations, better use of the subject site</p>	<p>Children's safety can reasonably be addressed via appropriate management of the centre.</p> <p>Contemplating alternative, preferred uses for the site, or fact that there may be alternative sites suitable to accommodate a child care premises in the area are not relevant considerations. The DAP is required to consider the proposal on its merits.</p> <p>Deliveries will be made outside of peak drop-off/pick-up hours and be accommodated within the carpark</p> <p>Traffic, access, parking and noise are discussed in the report</p>
30	<p>I am writing to express my strong opposition to the proposed child care facility on West Road, which I believe would significantly impact the residential nature and livability of our neighborhood.</p> <p>As a long-term resident of West Road for over 17 years+, I have witnessed significant changes in the area, particularly with respect to traffic and parking challenges. When I first moved in, there was already an established school, and I accepted the associated traffic and parking issues as part of living in close proximity to it. Despite the inconvenience of my verge being used for school and other parking and the constant destruction of sprinklers, I have never once complained, even when the installation of the safe street on Whitfield Street exacerbated the parking issues on West Road.</p> <p>Additionally, the increase in activity at the local bakery on West Rd, The Last Crumb, has also contributed to the congestion. This bakery is open everyday and is frequented by people all over Perth, parking is a constant issue. While I commend their success (and again I purchased knowing there was already a deli there) the resultant increase in traffic and demand for parking has added to the already strained situation. Added further by their recent win for Perths best bakery.</p> <p>However, the proposal to convert a residential block into a child care facility is a step too far. The intersection where the development is proposed is already busy and problematic. Introducing a child care centre will only compound the existing traffic and parking issues, making it difficult for residents and their families to find parking and navigate the streets safely.</p> <p>While I understand the need for child care facilities, I firmly believe this location is not suitable due to the existing conditions. There are undoubtedly more appropriate areas within our community where such a facility would not disrupt the residential character and already burdened infrastructure of the neighborhood.</p> <p>Furthermore, it is concerning that the developers can't adequately inform the neighbors about this proposed change. The signage alerting residents to the development has been placed at a time when the road is blocked off and inaccessible to cars or pedestrians. This lack of transparency and consideration for the community is unacceptable and only adds to the residents' frustrations.</p> <p>For these reasons, I vehemently oppose the development of a child care centre on West Road and urge the developers to consider alternative locations that would better serve the community's needs without compromising the quality of life for existing residents.</p>	<p>Traffic</p> <p>Parking</p>	<p>Traffic and parking are discussed in the report.</p>
31	<p>I am strongly against the proposed development of a childcare centre on 94 West road Bassendean. This residential address is on an intersection with high traffic volume all the time as well as along the bus route. It will cause more traffic congestion and car accidents if a childcare centre is being built there. Traffic on west road is bad enough with existing vehicles being parked around the Bassendean Primary school and The Last Crumb bakery during the day. We don't need a childcare centre on this proposed address mentioned above. Please advise the developer to build a childcare centre elsewhere.</p>	<p>Traffic</p> <p>Parking</p>	<p>Traffic and parking are discussed in the report</p>

32	<p>I wish to express my objection to the proposed child care premises at Lot 85 West Road Bassendean.</p> <p>The reasons I would not like to see a two storey facility accommodating up to 80 children with 13 staff are as follows :-</p> <p>Parking - the proposed 15 bay on site parking is in my opinion far too inadequate. It would not accommodate a safe drop off and pick up zone for parents. West Road is a busy road and in particular at that site so close to a busy cafe would make it quite congested at peak times.</p> <p>Noise - as this is primarily a residential area, to introduce a commercial entity as large as this with 80 young children I would think the noise so close to residential properties would not be pleasant for those residents. I am assuming there would be an outside play area for the children to play in.</p> <p>The amount of time given to the residents to consider this proposal is in my opinion inadequate. The letter is dated 21st June 2024 and submissions are due in by 5th July 2024. 14 days is simply not enough time.</p> <p>Despite my objections to this particular proposal I believe we do require more child care premises in the Bassendean area.</p>	Inadequate parking Noise	Additional parking bays were included post advertising. Parking and noise are discussed in the report
33	<p>We are writing to outline our objection to the proposal to build this centre on the corner of Bridson St & West Road. This intersection is already one of the busiest in the town as both streets are main thoroughfares into and out of the town, so we are of the belief that this centre will only add to further traffic flow in the immediate vicinity.</p> <p>There is already congestion on West Rd around the school and the Last Crumb Cafe most days of the week, and to my mind adding further traffic into the mix will only exacerbate this. During busy periods, it is often unsafe to be travelling more than 20 km per hour due to the number of people crossing the road and the limited car parking space around the Last Crumb, with many people driving further south along West Rd before trying to do U-Turns which often holds up traffic, adding to the congestion.</p> <p>We are also concerned about business premises encroaching on the residential part of the town, and if this application is successful it would set a precedent for subsequent applications and further expansion of business ventures into the residential precinct.</p> <p>Another issue of concern is the impact of noise on neighbouring properties. The noise of children playing during break times at the school is clearly audible in our part of Bridson Street - at times it is quite loud depending on the wind direction. We don't think it is reasonable to have up to a further 80 children playing outdoors 2 doors from our property, as the attendant noise levels would be at a greater volume due to the proximity of the centre. There are many older residents in the neighbourhood who spend the majority of the day at home, so the extra noise throughout the day would be unwelcome.</p> <p>In short, we believe this proposal would have an overall negative impact on the immediate neighbourhood, so we are strongly opposed to it being approved on the basis of the reasons outlined above.</p>	Traffic Approval of commercial uses in residential zone Noise	The suitability of the child care centre on land zoned 'residential', traffic and noise are discussed in the report.
34	<p>As a local resident of Bassendean with young children I would usually be very happy to hear that new child care centre has been proposed (DA-2024-066).</p> <p>However after reading it seems the facility will only congest the already busy area around West Rd, cause more traffic issues at the same time of day the school will be busy as well as the last crumb cafe. It seems poorly planned.</p> <p>I'd also like to note that the public was informed that the brewery in central Bassendean was rejected due to parking issues. It seems disingenuous to now have a proposal which will very clearly not provide effective parking, when another proposal was rejected for that exact reason.</p> <p>I believe an alternative location for the childcare centre would be ideal and would like to note my disagreement with the proposal.</p>	Traffic Parking	Traffic and parking are discussed in the report
35	<p>I am writing to voice my OBJECTION to the proposal DA-2024-056 on the following grounds;</p> <ul style="list-style-type: none">• The site is zoned residential, not commercial and DA-2024-056 is not a permitted use of the site• There is already inadequate parking in the area and this development would exacerbate that• A business of this type will reduce amenity for all nearby residents due to traffic congestion, noise and reduced road safety• The plans do not correctly depict the proposal - for example the West Rd Elevation does not even show the existing street trees• There is a shortage of HOUSING in WA, meanwhile Bassendean already has several day care centres. This site should be used for housing, which is why it is zoned residential. <p>Also I would like to state that it is a complete sham of a consultation for it to held over a 2 week period, during which the site in question is subject to a road closure for sewer replacement works, and any on-site signage pertaining to the DA would not been visible by people who normally use or drive by this part of Town.</p>	Permissibility of the use in the zone Parking Traffic Noise The site should be developed for housing	<p>The permissibility of a child care premises on land zoned 'residential', noise, parking and traffic are discussed in the report.</p> <p>Contemplating alternative, preferred uses for the site, is not a relevant consideration. The DAP is required to consider the proposal on its merits.</p>
36	<p>The proposed application for a child care facility on West Rd in Bassendean is one that will put our local community at risk, due to a few safety concerns.</p> <p>The proposed location is only 200 metres down the road from the local Bassendean Primary school and a highly popular local bakery Last Crumb. West Rd is already very busy with the Bus routes of no 45 and local traffic accessing the bakery and drop off and pick up for the school.</p> <p>The site is zoned as a residential site, to place a 2 storey child care centre along with staff will add significant traffic congestion to a small road that was not intended to cope with that volume of traffic. Even with significant urban infill of subdivisions in the area the roads are already too congested.</p> <p>Many residents walk their children to school and the added traffic make it another stress that local parents need to navigate. Daily I witness impatient drivers rushing around trying to drop or pick up their kids from school and another facility that allows quick drop off's and pick up's in the area makes it very worrying for local residents.</p> <p>I feel that there would be better sites available to cater for this facility and would not cause the same traffic concerns if built elsewhere. No 94 West Rd should only be used for families to build their home and live in Bassendean, enjoy the tree's, the river and all that our beautiful suburb has to offer.</p>	Traffic	<p>Contemplating alternative, preferred uses for the site, or fact that there may be alternative sites suitable to accommodate a child care premises in the area are not relevant considerations. The DAP is required to consider the proposal on its merits.</p> <p>Traffic is discussed in the report</p>
37	<p>I am writing to express my concern with the development of a child care centre at No. 94 West Rd. This area is zoned residential and does not have the traffic infrastructure to support its proposed use. The same road already has many traffic issues due to the school and the Last Crumb Bakery, and the addition of a child care centre on a corner block will cause significant traffic problems, and potential safety issues in the residential area.</p> <p>For the above reason I disagree with the proposal.</p>	Traffic	Traffic is discussed in the report
38	<p>I am writing to express my opposition to the planned childcare centre on West Road. My primary concern is around the increased traffic. This area of West Road is already very busy due to Bassendean Primary School and Last Crumb Bakery. Last Crumb Bakery was recently awarded the title of 'Best Bakery in Perth' and ever since, there have been lines of people and cars up and down the street. I will note that this was commonplace before their win.</p>	Traffic Parking	Additional parking bays were included post advertising. Parking and traffic are discussed in the report

	<p>In my view West Road will simply be unable to accommodate the increased traffic from the parents of up to 80 children dropping their children to daycare, particularly when the majority of the available parking spots will be taken by staff. I note that the proposal assumes parents will predominantly walk or catch public transport to take their children to daycare. With respect - this is an assumption only possible by someone who has never tried to get a child to daycare.</p> <p>West Road is approximately 15 minutes walk from the nearest train station - significantly more with a toddler who refuses to sit in the pram and gets distracted in shiny objects. There are few nearby bus routes, and carting a child and all their required accessories on a bus is a special kind of hell. This does not even take into account days when it is too hot or wet to walk/catch public transport with a young child. It is fanciful to suggest that the majority of parents would not be driving to the proposed day care centre.</p>		
39	<p>I am writing to express my comments on the proposed Child Care Premises at Lot 85 (No. 94) West Road, Bassendean. I am writing as both a concerned resident and as an Early Childhood Educator with 14years in the sector and as a service director with the knowledge and internal operations knowledge to speak on the issues at hand.</p> <p>I am against this proposal in it's current state.</p> <p>The proposal holds no merit in the stated employees vs child occupancy. The parking is not adequate for the use of the site to which would cause congestion to surrounding roads and undue risk to the local community including pedestrians and road users.</p> <p>At an occupancy/licensing of 80children of that age cohort, you are looking at around 20 staff on premises per day if not more and this doesn't include families and visitors needing to use bays as-well. The proposal states 13staff but this would be the main educators on the floor with the children, this wouldn't include lunch cover educators, the cook, administrators/managers and if required inclusion support educators. It is negligent of the developer to omit this integral information from the proposal.</p> <p>15 car bays is not sufficient. As a comparison Nido Bassendean is licensed for 83 children and have 31 car bays. This is also way too close to residential properties, have you ever heard 80children playing outside.....it's chaotic and loud and unfair on neighbouring properties.</p> <p>It also is not necessary for the community to have an additional childcare centre as there are already 8 in the area and only 2 of these services have waitlists, the other services are not at capacity and hold ample vacancies for the community to utilise. The area also will not see the residential growth to warrant the need for this in the foreseeable future. There is also an extreme workforce shortage of Educators to staff even the services that are existing/established. If this proposal was to go ahead it would make it even harder for other local services to maintain their staffing for the children who are enrolled. This would leave families having to move their settled children to new environments.</p> <p>The zoning should be and is for residential housing. Selling residential blocks to commercial entities makes housing less affordable and less accessible within the community.</p> <p>This doesn't directly affect me in any way but I am still very much against it for the reasons stated above.</p> <p>If it is to go ahead, then the occupancy needs to be reduced and the parking increased at a minimum.</p>	<p>Incorrect staffing numbers are noted</p> <p>Parking</p> <p>Noise</p> <p>Oversupply of child care services in the area</p>	<p>Staffing numbers have been updated post advertising (from 13 to 14 staff). Staff numbers noted meet the ratio required under the relevant regulations.</p> <p>Additional parking bays have also been included post advertising.</p> <p>Parking and noise are discuss in the report.</p> <p>The applicant is not required to demonstrate demand for services.</p>
40	<p>I am writing this as a concerned resident in relation to the proposed childcare development at Lot 85 (No. 94) West Road Bassendean, Reference Number DA-2024-066. I don't believe this property is suitable for the proposed development for a number of reasons.</p> <p>Traffic on West Road is currently congested at certain times of the day due to Bassendean Primary School and The last crumb business.</p> <p>Other than the allocated parking bays already in place at the front of Bassendean Primary, I believe that there should be no parking on West Road whatsoever as it is a main arterial road within Bassendean with heavy traffic flow at certain times of the day.</p> <p>The proposed development to me also falls short in terms of parking bays as the majority of children are 'dropped off' and 'picked up' at the same time. 80 children and 16 car bays, including staff and accrod is nowhere near enough which will result in vehicles parking on Bridson Street, Hyland Street, Watson Street and West Road for sustained periods as well as on verges as they already do on West Road. This will have a flow on effect which will then push other people wanting to park in the area, for reasons other than the childcare, to other streets such as Whitfield Street and Devon Road (these being customers of The Last Crumb mainly) as it has already happened. The Last Crumb is a classic example of what inferior planning creates as it is just ridiculous currently with the vehicle chaos.</p> <p>Another issue is the lack of setback of the development and the property being situated on a corner which will block vision to and from West Road which again creates a dangerous environment.</p> <p>The zoning of the property is R20 and the 'A' classification for this use on that particular property is there for a reason as this proposal is just not suitable.</p> <p>A childcare centre in Bassendean in the correct location with the correct zoning and the basic requirements along with the policing of traffic and the increased number of vehicles not impacting the Town in anyway is fine. Lot 85 West Road Bassendean falls short in every aspect.</p> <p>This property is part of a residential zoning that should have only residential properties so immediate land users are not impacted in such a negative way.</p> <p>The best way to sum this up is that approving a childcare development on this site is just a recipe for disaster and after seeing what I have seen in that immediate area already, the increased traffic flow along with a substantial building with minimal setback, will only lead to unsafe environment, regardless of any other factors.</p> <p>I currently own two properties in Bassendean and have lived in the area since 1970. I am all for progression but done in a manner that does not impact the safety of our residents.</p>	<p>Insufficient parking</p> <p>The building encroaches sightlines at West/ Bridson</p> <p>Traffic</p>	<p>Parking. traffic and discussed in the report</p> <p>The development does not impact sightlines at the West Road/ Bridson Street intersection.</p> <p>The suitability of a child care premises in a residential zone is discussed in the report.</p>
41	<p>I am writing to express my opposition to the planned childcare centre on West Road. My primary concern is around the increased traffic. This area of West Road is already very busy due to Bassendean Primary School and Last Crumb Bakery. Last Crumb Bakery was recently awarded the title of 'Best Bakery in Perth' and ever since, there have been lines of people and cars up and down the street. I will note that this was commonplace before their win.</p> <p>In my view West Road will simply be unable to accommodate the increased traffic from the parents of up to 80 children dropping their children to daycare, particularly when the majority of the available parking spots will be taken by staff. I note that the proposal assumes parents will predominantly walk or catch public transport to take their children to daycare. With respect - this is an assumption only possible by someone who has never tried to get a child to daycare.</p> <p>West Road is approximately 15 minutes walk from the nearest train station - significantly more with a toddler who refuses to sit in the pram and gets distracted in shiny objects. There are few nearby bus routes, and carting a child and all their required accessories on a bus is a special kind of hell. This does not even take into account days when it is too hot or wet to walk/catch public transport with a young child. It is fanciful to suggest that the majority of parents would not be driving to the proposed day care centre.</p> <p>Please don't hesitate to contact me if you would like to discuss further.</p>	<p>Traffic</p> <p>Parking</p>	<p>Traffic and parking are discussed in the report</p>
42	<p>As a neighbouring property home owner (81b West road) I wish to pass comment on the proposed childcare centre at Lot 85 (no. 94) West Road in Bassendean (DA-2024-066) .</p> <p>Whilst I do not have any concerns with the proposed use, I do have concerns regarding traffic and the shortfall in parking. West road has had a large increase in the number of vehicles in recent years with the upgrade to Sandy Beach play space and popularity of Last Crumb contributing. Housing Density has also increased with the road often being filled with parked vehicles on the side of the road from both residents and visitors. The proposal to offset the parking shortfall by using street parking by the school is impractical as this is already in short supply and peak times will likely be the same.</p> <p>15 parking bays for 13 staff is quite insufficient and reliance that parents will walk is out of their operational control. There are currently no official street parking bays directly</p>	<p>Parking</p> <p>Traffic</p>	<p>Additional parking bays have been included post advertising. Traffic and parking are discussed in the report</p>

	<p>adjacent, most likely due to the proximity of intersections and bus routes, increasing the likelihood of verge parking. As the drop off is likely to be undertaken with young children, verge parking will be inevitable. Having had daycare drop offs as a parent for a number of years with multiple children, proximity is key. No sane busy parent will park across a busy street or down the road when they can simply stop on the street by the front door.</p> <p>Childcare is a critical service and I am supportive of this use. Proximity to the school is also of benefit however unless traffic and parking mitigation measures are put in place (provision of parking not prevention) along this section of road I do not believe it is a suitable location.</p>		
43	<p>I provide the following concerns and objections regarding the proposed development of a Child Care Premises at Lot 85 (No. 94) West Road, Bassendean (corner of West Road and Bridson Street) - DA-2024-066.</p> <p>I strongly oppose the development of Lot 85 (No. 94) West Road, Bassendean into a childcare centre.</p> <p>Firstly, the location of the proposed development is in an area that it is all residential. Allowing a commercial development in a residential area causes a negative impact on the surrounding properties on West Rd, Bridson St and Hyland St. It impacts the tranquil nature of this part of the suburb and the privacy of residents. The large childcare development is not consistent with the area and it extremely unfair on nearby residents.</p> <p>The location of the proposed development also causes significant traffic and parking concerns. The entry to the Childcare premises on West Rd is opposite Hyland St, which is part of a bus route, causing safety and traffic flow issues with cars needing to enter into and out of the childcare, as well as busses traveling along West Rd, and traffic using Hyland St and Bridson Street as route to Guildford Road will cause traffic chaos and significant safety issues for pedestrians and vehicles.</p> <p>There are a large number of children that use West Rd to walk and ride to school, especially during Peak times, and having a large number of cars coming in and out of the childcare entry on West Rd is a serious safety issue. Not to mention the number of other pedestrians that use this route to walk pets or to walk to the town centre, it is not the right place to have cars pulling in and out.</p> <p>I am also very concerned regarding the lack of parking onsite and the overflow parking that will occur on West Rd, Hyland St and Bridson Rd. Not having the adequate number of bays as required by local planning will mean vehicles will park on the side of the road and verge, causing further safety issues and impact traffic flow. Further it is unfair for residents in a residential area such as this, to put up with cars parked on the side of the road and the increased noise, disruption and traffic from a large childcare centre. Having cars parked on the side of the road, will also impact the ability of cars pulling in and out of the childcare car park having clear sight of vision – further impacting the safety of pedestrians and other vehicles.</p> <p>Given the traffic from the primary school and the increased traffic as a result of the popular Last Crumb Coffee shop, this development will put even more pressure on traffic flow and parking congestion for nearby residents.</p> <p>The location should remain residential, and the childcare development should not be approved.</p>	<p>Inappropriate use in a residential area</p> <p>Privacy</p> <p>Traffic</p> <p>Parking</p> <p>Access</p> <p>Noise</p>	<p>The suitability of the child care centre on land zoned 'residential', traffic, parking, access and noise are discussed in the report.</p> <p>The proposal does not result in overlooking of adjoining properties.</p>
44	<p>I strongly object to the proposed development of a Childcare facility at 94 West Rd Bassendean.</p> <p>The site is a prime multi unit residential zoned site that would better serve our community if the residential zoning is maintained & developed into multi housing units during the housing crisis we are currently experiencing.</p> <p>Council has been talking about redeveloping the Wilson St carpark site. A childcare facility of 80 children capacity could be part of this redevelopment. The Wilson St carpark site has public open space closeby so the children can run & play in the fresh air & not have their play restricted to an enclosed building as would be the case in the 94 West Rd site.</p> <p>I live in West Rd & use West Rd for my daily commute. It is already difficult to navigate the West Rd Primary School area with parents parking wherever they can find a spot during peak school drop off & pickup.</p> <p>I cannot even begin to imagine adding another 80 children to the mix & the danger the site poses to young infants/toddlers who move like the wind. I have a beautiful 2 yo Granddaughter who in her excitement to get to the car after daycare has provided me with some heart stopping moments. The number of onsite parking bays provided by the proposed Childcare Facility is woefully inadequate. It only provides sufficient car bay for the staff but where do the parents of the 80 children proposed will visit the facility daily, park their car for drop off & pickup?</p> <p>Please use this prime Residential zoned site to provide urgently needed multi unit housing.</p>	<p>The site would be better used for multi residential purposes.</p> <p>Alternative, preferred sites exist for a childcare centre.</p> <p>Parking</p> <p>Traffic</p>	<p>Contemplating alternative, preferred uses for the site, or fact that there may be alternative sites suitable to accommodate a child care premises in the area are not relevant considerations. The DAP is required to consider the proposal on its merits.</p> <p>Traffic and parking are discussed in the report.</p>
45	<p>My family and I have lived on West Road for almost twenty years. Our home is located on the opposite side of West Road and three doors up from the planned day care centre. Over the time we have lived here, pedestrian movement and traffic volume (inclusive of heavier construction vehicles) has increased due to housing density and population, the <i>Last Crumb</i> becoming hugely popular (with people moving across the road as if it was a footpath), the children's playground at Sandy Beach and the increase in numbers at Bassendean Primary School (almost 20% since 2020).</p> <p>The planned day care adds to that load – a concentration of vehicular and pedestrian activity, particularly at peak times. There is little in the way of infrastructure at the day care or strategy to manage the increase or minimise risk to residents, pedestrians and road users. This increased 'load' and less than required parking is not wanted and increases risk and disruption to local people.</p> <p>I understand the <i>Development Assessment Panel</i> relies on the technical documents outlined <i>Application for Development Approval</i>. It does not account for what Bassendean is to people, and fails to respect the essence of the Town of Bassendean consultations and community voice embedded in what we want for our local area. The <i>Technical Findings</i> in the <i>Traffic Impact Statement</i> is based on a number of generic assumptions undermining, but not limited to, the above. This includes:</p> <ul style="list-style-type: none">- Little care for the impact on our area citing there will be a <i>moderate traffic impact</i> to the surrounding road network. We don't want any further impact.- The limited parking at the day care centre is justified by the findings of the <i>Traffic Impact Statement</i> are based on a formulaic calculation about how long parking bays would be utilised by clients of the centre.- The '<i>Customer Parking</i>' calculation is reliant on each person adhering to a designated time within peak periods (in an orderly one after the other style) and a 'magical average' that declares there will be no problem with congestion.- Increased congestion is inevitable. The movement will have '<i>an adverse impact on the amenity of the locality.</i>'- The assumption does not adequately account for times, hard to settle children and the potential 45% of client movement within a peak time, ie people will not wait for their 'ten-minute spot'.- The '<i>shared parking principle</i>' calculation is flawed. Shared areas are already chaotic at peak times (due to the before-mentioned changes in the area).- The <i>Traffic Impact Statement</i> also states there have not been any accidents within the vicinity of the day care. That is false. In the past two years, there has been two collisions outside my house alone. Thankfully no one was injured, perhaps accounting for the error in the report. A survey of local residents may well reveal many more vehicle collisions (albeit without human injury...yet).- People will be travelling in and out of the car park with the cross over within metres of two T-junction intersection (Bridson Street and Hyland Street). Visibility is already compromised at these intersections and increased traffic and verge parking will make it more dangerous. The following is from an RAC media release in October 2020 – published prior to the increase in numbers at our local primary school and before the <i>Last Crumb</i> became the Mecca it is today. <i>Between 2015 and 2019, 214 pedestrians under the age of 18 were killed or seriously injured on WA roads. Around 50 per cent were hit by vehicles while crossing the road, and nearly one in five occurred between the hours of 3pm and 4pm.</i> " <p>"Young pedestrians are extremely vulnerable road users and it only takes a split-second for a tragedy to occur." Tragically, 179 people were killed or seriously injured on WA roads last October – 17 were under the age of 18. We are not in South Perth or Stirling – nor do we aspire to be (two of the areas cited and mentioned to me in my</p>	<p>Traffic</p> <p>Parking</p> <p>Access</p> <p>Sightlines</p> <p>Inappropriate use outside of the town centre</p>	<p>The suitability of a child care premises on land zoned 'residential', traffic, parking, access and sightlines are discussed in the report.</p>

	<p>inquiries in defence of the day care location). We live in Bassendean. We participated in the <i>Bassendream consultation</i> with a view to ensuring the ‘... <i>relaxed and quiet village...</i>’ and the utility of safe walkways is maintained (yes – even if you live on West Road!). The country-style and feel as one drives down West Road, contributes to the Bassendean feel. It doesn’t need to be eroded any further.</p> <p>The agreement and understanding for Bassendean (and Eden Hill and Ashfield) is that developments be concentrated on the central hubs. More suitable locations for a day care would be close to or incorporated into the Wilson Street/Park Lane development, vacancies on Old Perth Road – closer to public transport and close to St Michael’s school.</p> <p>I’m also concerned about how many more <i>A Use</i> classes within the R20 zone close to where I live will emerge and whether this <i>A Use</i> would set a precedent for the travesty set by the Development Assessment Panel to again not communicate or consult with the local community.</p> <p>In addition to my comments and concerns I am interested in what the Town of Bassendean can impact (if anything) to address the concerns of local people.</p>		
46	<p>I provide the following views and objections to be presented to the Metro Inner Development Assessment Panel (DAP) in considering the proposed development of a Child Care Premises at Lot 85 (No. 94) West Road, Bassendean (corner of West Road and Bridson Street) - DA-2024-066. I am a resident and owner occupier of 92 West Road Bassendean (next door to the proposed development site) and have been a resident of Bassendean (and in this location) for over 50 years. I am strongly against the development of Lot 85 (No. 94) West Road, Bassendean (“the site”) into a childcare centre. My views, objections and concerns are listed below.</p> <p>Zoning</p> <p>The site is currently zoned Residential R20 under the Town of Bassendean’s Local Planning Scheme 11 and the surrounding area of the site is characterised by low density housing all of which are R20 or R25. The Town of Bassendean has worked continuously to ensure low density residential is maintained within the area and undoubtedly this is what residents of this area want. Developing the site into a commercial premises, being a large-scale childcare centre, in the middle of residential homes certainly is not appropriate nor consistent with the existing area. Allowing a commercial development on a residential zoned block will set a precent for similar approvals on other streets of Bassendean which should be discouraged because the Town of Bassendean has spent years growing a quiet neighbourhood with low density housing, being one of the most desirable facets of Bassendean. This site should remain residential as the low-density zoning is embedded in this part of Bassendean and heavily contributes to the value and desirability of the properties in the area. Having a large childcare centre developed right next door to my property, grossly devalues my property, as well as the surrounding properties. The value and charm of Bassendean, and particularly this part of the community, will be severely impacted and goes against all the Town of Bassendean has done to support the wishes of the majority of residents in the area and maintained the low-density residential zoning of the area.</p> <p>Parking and Traffic Issues</p> <p>According to the development application, the site has 16 parking spaces, 1 of which is ACROD at all times, 8 of which are designated ‘Staff Only’, leaving 7 Bays to be used for additional staff and visitors. This is a site with the potential to have daily traffic generation of 346 (in accordance with the Transport Impact Statement included as part of the development application) and only 16 car bays to accommodate for this. The minimum required car bays for a childcare in accordance with Local Planning Policy 8 (“LPP8”) is 21. I re-enforce that 21 is the minimum in accordance with LPP8. As a comparison, Nido Early School on Railway Pde Bassendean, which has a similar number of approved places as the proposed development, has 31 parking bays. The offsite parking facilities on West Rd referred to in the application are located in front of Bassendean Primary School and the very busy Last Crumb Coffee Shop. These bays should not be considered as appropriate offsite bays for the proposed childcare. The Town of Bassendean is well aware of the parking and traffic issues as a result of the popularity of The Last Crumb, and these bays are rarely “free” to be used as overflow for the proposed childcare, especially during morning peak time (7:30am-9:30am in accordance with page 11 of the Transport Impact Statement). I have attached some images below of the full parking on West Road outside Bassendean Primary and The Last Crumb to highlight the lack of available on street parking. These images were taken on Monday 1 July 2024 – It is school holidays, therefore even without the school drop off traffic, there are no free bays available.</p> <p>I also draw your attention the image above where a vehicle has parked blocking access to a resident’s driveway, further highlighting issues that will occur due to inconsiderate behaviour that cannot be controlled when there is a commercial operation in a residential area. Furthermore, Page 9 of the Transport Impact Statement states that the results of an on-street parking survey for West Rd and Whitfield St just before 8am and around 4:30pm in September 2023 showed “an abundance” of on street parking. A number of important factors need to be addressed, as follows: 8am is generally before school drop off. The peak times for the childcare drop off is between 7:30am and 9:30am. The survey only considered the availability “just before 8am” and did not take into account this total peak timeframe. As such this is not a true reflection of the available street parking bays. The survey was taken in September 2023. There was a School Holiday period during September. Was the survey taken during school term or school holidays – as again this significantly impacts the amount of available parking. The operators of the childcare centre will also be unlikely to “police” where staff and visitors are parking. If there are no vacancies in the onsite parking, it is most likely that visitors will park on the surrounding roads directly outside the childcare centre, including on West Rd in front of my and adjacent properties and on the verges.</p> <p>Not only is it grossly unfair for the local residents to have excessive overflow parking and crowing outside of their properties along West Rd, Hyland Street and Bridson Street, but it creates Traffic and safety issues. There are safety concerns for children coming out of cars when being dropped off if the cars are parked on the side of the road or the verge because of the insufficient parking provided. During the peak time of 7:30am and 9:30am according to the Transport Impact Statement, approximately 60 children will be dropped off. It is clear the safety issues that this presents with this number of children. This safety issue extents further due to the site being at the intersection of West Rd/Bridson St and West Rd/Hyland St. West Road can be busy as a bus route and with cars coming along West Road in both directions, and also coming in and out of the two intersections of Bridson St and Hyland St. Bridson St is used as a direct route to/from Guildford Rd, therefore can have significant traffic flow. I know from personal experience from living in the vicinity the traffic in the area, especially when leaving my driveway I often have to wait quite a while for a clear way to proceed. I can’t image what traffic disaster this childcare will cause, with 346 vehicles needing to enter and leave the childcare during the day and the overflow car parking on the road because there is inadequate parking onsite. There will also be significant safety risks for pedestrians who use the foot path on West Rd in front of the proposed childcare, including children who walk and ride to school.</p> <p>With the number of cars coming in and out of the proposed childcare, and with overflow cars parked on the side of the road or verge blocking a clear view of the footpath, it certainly is extremely concerning that a serious incident could occur. When cars have been parked on West Road due to overflow from Bassendean Primary School and Last Crumb it is extremely dangerous to pull out of my driveway as it significantly impairs the ability to see traffic. Parking on the side of the road will also cause traffic congestions and safety issues as cars need to weave between parked cars to be able to travel along West Rd (at 50km/h). I also draw your attention to the parking on Wilson St outside Wind in the Willows daycare centre to further highlight the importance of the parking issue. Cars are parked on both sides of the street due to the daycare, especially during peak drop off and pick up times and staff also are parking on the street. Wilson St is a much quieter ‘back street’ than West Road, without the added traffic from Bassendean Primary School and The Last Crumb being 800 meters away. The Transport Impact Statement includes traffic volumes of the area at page 7. It should be noted The West Rd traffic volumes are from 2021/2022, before the increased popularity of The Last Crumb and it appears the readings are from a location outside the Bassendean Shopping Centre and Oval – therefore not near the proposed development. These figures cannot be relied upon as an indication of the traffic in and around Bassendean Primary/The Last Crumb and the proposed site.</p> <p>The Parking and traffic issues are incredibly important for a residents right to peaceful enjoyment of their property, which is in an area of mainly residential properties. The residents in the vicinity of the proposed development are already dealing with the increased traffic and parking issues from the popular coffee shop, which flows through to the weekend. This proposed childcare development will add more pressure to this traffic and parking problem on a week daily basis, giving little respite to the residents and causing further chaos and traffic safety concerns.</p>	<p>Scale of the development is inconsistent with the low density residential nature of the area</p> <p>Potential for approval to set a precedent for similar developments</p> <p>Adverse impacts on property values</p> <p>Parking</p> <p>Traffic</p> <p>Noise</p> <p>Lack of demand for additional child care service in the area</p>	<p>The suitability of a child care premises on land zoned ‘residential’, including considerations of appropriate scale, are discussed in the report.</p> <p>Each application is to be considered based on its merits. Approval of the subject proposed would not set a precedence for rezoning of the land or additional commercial development in the residential zone.</p> <p>Property values, parking, traffic, noise, and demand for child care services are discussed in the report.</p>

	<p>Noise and Disruption</p> <p>According to applicants planning statement, there will be 68 total places for children aged 3-5years. These children (unlike the smaller children (0-2years), will have play time outside. The external play area is directly adjacent to my main living areas and my windows are approximately 3.4meters from the boundary. The large number of children playing during the day will cause an unfair amount of noise and disruption. I have attached images from my window to highlight the issues this causes. Furthermore, the car park is located right against my boundary, with cars arriving from 6:30am and leaving at 7pm, Monday to Friday.</p> <p>According to page 17 of the Transport Impact Statement included as part of the application, the proposed childcare centre will generate 346 vehicular trips per day. This is potentially 346 vehicles coming in and out 3.4meters away from my living area windows. The noise and pollution this generates is unacceptable and completely inconsistent with the surrounding area that is all low density residential. Albeit there are noise control measures contained in the Acoustic Assessment, they do not fully mitigate the noise and disruption of a large number of children playing outside (according to Acoustic report - normally 20 to 40 and occasionally as many as 70), or the number of vehicles coming in and out. I am home during the day so this will cause great disturbance. This amount of noise and chaos that the childcare will generate is grossly unfair, impacts my right to peaceful enjoyment of my property and is significantly inconsistent with the residential area. If the property was developed with residences, the above would not be an issue.</p> <p>Advertising to Public for Comment</p> <p>It must be noted, that although a statutory advertising period of 2 weeks to submit feedback was provided, the timing and advertising of the proposed development could have been given more thought. Most of my direct surrounding neighbours were not even aware of the development proposal until I raised it with them. Although, a sign was placed on the proposed development site it was placed in a position that is very unclear and hidden away. The footpath on West Rd in front of the site is currently closed, as are the roads surrounding the property (both West and Bridson) due to the current wastewater pipeline upgrade works being completed.</p> <p>It does not give residents the opportunity to provide feedback if the advertising is not clear to ensure residents are made aware. Childcare as the proposed Development</p> <p>The cover letter provided by the applicant in relation to the proposed development refers to a study undertaken by The Mitchell Institute for Education and Health Policy stating that Bassendean is termed a 'childcare desert' in support of its application to develop the site into a childcare. The research only focused on one type of childcare, centre-based day care and did not take into account other type childcare services, such as small home-based day cares. Furthermore, it should be noted most of Australia is classed as a childcare desert, not to mention the majority of the Perth metro area. I have attached an excerpt below from the report. The Bassendean area is in no more need of a childcare centre than any other surrounding area.</p> <p>There are several childcare centres all within a 4km radius of the proposed site, I have listed these below with the number of approved places (in accordance with www.acecqa.gov.au), as well as the proximity to the proposed development. –</p> <p>Buttercups Childcare and Early Learning Centre – 147 Walter Rd Bassendean. Approved for 49 Places (3.5kms from the proposed site)</p> <p>Mercy Care Early Learning Centre – 159 Guildford Rd Bassendean Approved for 55 Places (1.2kms)</p> <p>Nido Early School – 72 Railway Pde Bassendean. Approved for 83 Places 31 car bays (1.9kms)</p> <p>-Wind in the Willows – 28 Wilson St Bassendean. Approved for 55 Places (850Meters)</p> <p>- Wind in the Willows – 2 Colstoun Rd Ashfield. Approved for 35 Places (2.3kms)</p> <p>I make the following comments:</p> <p>With regards to Wind in the Willows Bassendean, it is located within a residential area however the following important and significant points should be noted:</p> <p>It is approved for 55 Places, being a much smaller day care facility.</p> <p>- Entry is from a quiet back street, Wilson St.</p> <p>- The parking on Wilson Street is outrageous and chaotic, with cars parked on both sides of the street, especially during peak drop off and pick up times and staff also parking on the street. The childcare centre contributes greatly to this parking congestion, and it is a much smaller daycare than the proposed development</p> <p>With regards to Nido Early School, which has a similar number of approved places as the proposed development, it has 31 parking bays – which is double the amount in the proposed development and it is also located in a commercial space, not a residential area as the proposed development.</p> <p>The cover letter also states that according to Care for Kids (www.careforkids.com.au) Wind in the Willows Daycare does not have any vacancies. Upon review of the Care for Kids site, vacancies were available in 9 daycares in the Bassendean/Lockridge/Eden Hill area, including Nido Early School and Mercy Care Early Learning Centre which are both within 2Km's of the proposed site (as at June 2024).</p> <p>Conclusion</p> <p>In conclusion, and in reference to all points and matters listed above, I strongly oppose the development of 94 West Road Bassendean into a childcare centre. I do not believe it is an appropriate location and the site should remain residential. Given the housing shortage WA is facing, the site would be better suited as a residential development. The majority of the surrounding residents and community also do not support this proposed development as indicated by the significant number of signatures on a petition that was generated, a copy of which has been provided to the Town of Bassendean and The Hon. David Kelly's office.</p>		
47	As a Bassendean local home owner I would like to register my opposition to the childcare centre being built on this residential zoned block. The traffic on the surrounding streets is already too excessive and this block would be much better suited to residential rather than commercial.	Traffic	Traffic is discussed in the report
48	I am in favour; I don't think Bassendean needs to be turned into a giant car park. Perth needs to join the ranks of other cities and become sustainable to accommodate humans, not cars. I believe that limiting the amount of space dedicated to cars is a good use of spatial resources. It will also encourage public transport use, and walking. I'd rather build a community than a car park.	Support	Noted
49	<p>I am vehemently opposed to the application to develop a Child Care Premises at Lot 85 (No. 94) West Road, Bassendean (corner of West Road and Bridson Street).</p> <p>The site is zoned 'Residential' (R20) and should be developed for housing only.</p> <p>There is a nationwide housing shortage. Individuals who try to circumvent local planning scheme to build anything other than housing in an area zoned for housing should be opposed using all means available.</p> <p>This is purely being done for profit as there are sufficient childcare facilities throughout Bassendean and surrounding areas.</p> <p>Notwithstanding the above, if the local council rejected the planning application for the brewery on Old Perth Road next door to Berties on the grounds of insufficient parking, why should this application not be rejected for the same reason?</p>	<p>Residential development on the site is preferred to address housing crisis</p> <p>Lack of demand for additional childcare services in the locality</p> <p>Parking</p>	<p>Contemplating alternative, preferred uses for the site is not a relevant consideration. The DAP is required to consider the proposal on its merits.</p> <p>Demand for child care services and parking are discussed in the report.</p>
50	<p>The site is zoned 'Residential' (R20). Under the Town's Local Planning Scheme No. 11. 'Child Care Premises' is an 'A' use in the Residential Zone; and is not permitted. This activity belongs in the District Centre. We have just undertaken years of work to finalise Local Planning Scheme 11 to the satisfaction of the WAPC. There will be a great deal of respect earned within our community if the DAP uses our shiny new Scheme to guide its decision and to maintain the intent and permitted uses of that document.</p> <p>In considering whether to use its discretion to contravene the Local Planning Scheme the Panel should consider:</p>	<p>Land use permissibility</p> <p>The use should be located in the Town Centre</p> <p>Access</p> <p>Parking provision and layout</p> <p>Limited options exist for safe onstreet parking</p>	<p>The number of bays and design of parking spaces has been revised post advertising and now complies with the Australian Standards.</p> <p>The fact that there may be an alternative site suitable to accommodate a child care premises in the area is not relevant considerations. The DAP is required to consider the proposal on its merits.</p>

	<p>Parking and access - see Architects Plan p.4, 6 & 7:</p> <ul style="list-style-type: none">· The design of the 15-bay parking area to accommodate staff and allow for the comings and goings of up to 80 client drop-offs and pick-ups looks to be an unusually awkward and unworkable design for a public facility - inadequate at best and unsafe in terms of the single point of entry and exit onto West Rd, near the zig-zag intersections of Bridson St and Hyland St.· Street drop-off/pick-up at this location is inadequate given the proximity of the intersections, and the access point to the car park, which is likely to largely occupied by staff anyway.· An option, not shown on this application, but likely to be pursued by the applicant if approval is granted, would be to allow parallel parking on the verge which will be<ul style="list-style-type: none">o Limited due to the proximity of the Bridson St intersectiono An unacceptable threat to the existing treasured Significant Oak trees on the verge ando Narrowing the verge outside the facility would increase risk to children walking to and from Bassendean Primary School to the north. Parallel parking exists outside the school but it does not encroach onto the verge.· Transperth bus 45 turns in and out of Hyland St directly opposite the carpark entry. Not clearly indicated on the drawings.· The angle that Bridson St enters West Rd, not being 90 degrees, further increases the awkwardness of the traffic flow at this point.. <p>I drive this route daily and know well how congested it is already at the beginning and end of every school day, compounded by the high traffic volume and parking demands created by the Last Crumb patrons. I hope you can make the DAP see that this is indeed an unsuitable location for a high use public facility,</p>	<p>Impact of uncontrolled parking on street trees Traffic</p>	<p>Land use permissibility, access, parking and traffic are discussed in the report</p>
51	<p>I would like to register my opposition to the application for a childcare centre at 94 west road.</p> <p>I live on Watson street and the traffic already on west road is dismal with the school and last crumb vying for parking spots. To drive up and down west road is already a hazard before you add in a childcare centre with drop off / pick up.</p> <p>We need residential housing in a residential area in an already tight housing market .</p> <p>I believe there are more suitable options for this centre and not 94 west road.</p>	<p>Traffic Residential development is preferred</p>	<p>Contemplating alternative, preferred uses for the site, or the fact that there may be alternative sites suitable to accommodate a child care premises in the area are not relevant considerations. The DAP is required to consider the proposal on its merits.</p> <p>Traffic is discussed in the report</p>
52	<p>I would like to object to a daycare of this size being built in this location. Parking will be a massive issue. The end of Bridson street is a busy thoroughfare at start and end of work and school days and parking sounds extremely inadequate to cater for staff / and families.</p> <p>The impact on neighbours will be huge and I can only assume it will reduce the value of and resale potential for adjoining properties.</p> <p>I would support family daycare but not a daycare of this size which will impact west road and Bridson Street residential properties and create issues with traffic congestion and flow</p>	<p>Parking Traffic Impact on property values Inappropriate scale for residential area</p>	<p>The suitability of the land use, parking, traffic and impact on property values are discussed in the report.</p>
53	<p>We are writing to raise our concerns about the application to develop a Child Care Premises at Lot 85 (No. 94) West Road, Bassendean (corner of West Road and Bridson Street).</p> <p>We reside a 400m walk to the location of the proposed development location at 39 Harcourt Street Bassendean.</p> <p>We note the key points to the application</p> <ul style="list-style-type: none">• A 2-storey, purpose-built Child Care Premises capable of accommodating up to 80 children (0 to 5 years old) and 13 staff;• Childcare services to be provided Monday to Friday between 7.00am and 6.30pm;• 15 onsite car parking bays, accessed via West Road. <p>During the week, we leave the house to walk to/from the station to commute to the city to work or drop off/pick up our son at Bassendean Primary which is located between out house and 94 West Road. This is at times which are peak drop off/pick up times for the proposed childcare centre.</p> <p>The key concerns we have about the application to develop are as follows:</p> <ul style="list-style-type: none">• Additional traffic stopping, parking and reversing on West Road and nearby streets in an already very busy residential area due to Bassendean Primary and The Last Crumb café.• The key risks additional traffic causes to children in the vicinity (both being dropped off/walking to school) and with their parents at the café.• The key risks additional traffic causes to pedestrians in the vicinity. Visiting cars have more often than not blocked off the one footpath between our house and the school which means we need to walk on the verge/road with increased traffic and is risky with a 5 year old whom we are trying to instil good road rules to.• The key risks additional traffic causes to local traffic in the immediate vicinity. On the occasions we need to drive our car out from our house during peak times – it is a nightmare. This would only increase with a childcare centre in close proximity. <p>We have seen firsthand how much impact during operating hours, and especially at peak times the Last Crumb has had on additional traffic in an area which is residential.</p> <ul style="list-style-type: none">• With another busy venture within 300 meters of the café and school – We are not convinced the location is suitable for this.• It is not so much the traffic driving up and down West Road – but more so the parking of cars, getting kids out of the cars/going to the cafe, coming back to the car, and reversing out into an already congested street which is our main concern.• We don't drive out at peak time often as we choose to use public transport to get to work – but if we need to leave at peak time in the car – there has been several occasions where we have nearly been hit in our car due to cars manoeuvring (usually in an illegal manner) at the top end of our street. This is a vast difference to pre the Last crumb gaining immense popularity – when these incidents never occurred. <p>We are not against change, and very much support and welcome the activation of Old Perth Road, and the increase of density to the designated area that the town has mapped out. We fully support the need for additional housing and businesses to support the activation of Old Perth Road. But, this proposed development is outside that area of proposed increase in density, and puts additional pressure in an area which is not well suited to more traffic (from on ongoing issues generated by two busy places i.e. the Last Crumb and Bassendean Primary, in which this proposed site will impact on this negatively once more).</p>	<p>Traffic Parking</p>	<p>A reversing space is provided onsite to allow vehicles to exit in a forward gear</p> <p>Traffic and parking are discussed in the report</p>

54	<p>I wish to express my strong objection to the above development proposed for West Road, Bassendean.</p> <p>The proposed child care centre will be in close proximity to both the primary school and the very popular cafe. These establishments already cause significant traffic congestion in that part of West Road. Adding another business to that part of the road will make traffic completely unmanageable. The proposed child care centre plans to service up to 80 children and consequently up to 80 parents will be parking for several minutes, twice a day. i cannot see how this can be accommodated by 15 car park bays when there will be 13 staff. The intersections at Hyland and Bridson are already very busy intersections as drivers come from North Road to other parts of Bassendean and Ashfield. I also wish to object to the misleading illustration of the child care centre. It does not show any of the surrounding area and also does not show a high fence which will need to surround the centre in order to keep the children safe. Allowing the children to have any access to West Road or Bridson Street will place them in very obvious danger.</p> <p>I would be grateful if you would forward my objection to the DAP.</p>	Traffic Parking	Traffic and parking are discussed in the report. Fencing is shown on updated plans.
55	<p>Roads were closed and school holidays occurs during advertising period</p> <p>View of the site during advertising was obstructed</p> <p>Traffic</p> <p>No consideration for drop off and pick up of children</p> <p>A more suitable site should be selected</p>	Traffic	Traffic is discussed in the report
56	<p>We are writing to express our concerns and objections regarding the proposed development of a childcare premises at Lot 85 (No. 94) West Road, Bassendean, as detailed in the DAP application. Our primary concerns are as follows:</p> <p>Zoning and Land Use</p> <p>The site is zoned for residential use under the Local Planning Scheme No. 11 (LPS11). Introducing a commercial facility such as a childcare centre in a residential zone is inconsistent with the current zoning regulations and undermines the residential character of the neighbourhood.</p> <p>Increased Traffic</p> <p>The development will increase traffic in the area, particularly during peak drop-off and pick-up times. The Traffic Impact Statement acknowledges a potential rise in traffic, but the mitigation measures suggested are insufficient.</p> <p>West Road and Bridson Street are local distributor roads with existing high traffic volumes, especially during school hours due to the proximity of Bassendean Primary School. Additional traffic from the proposed childcare centre will exacerbate congestion and pose safety risks to residents and school children.</p> <p>Noise Pollution</p> <p>The Acoustic Assessment provided by the developer does not adequately address the potential noise impact on neighbouring properties. The proposed operating hours from 7:00 AM to 6:30 PM, including outdoor play from 7:00 AM to 6:00 PM, will result in noise disturbance.</p> <p>Noise from children playing outdoors, staff movements, and increased vehicle activity will disrupt the quiet residential environment.</p> <p>Pedestrian Safety</p> <p>The increased traffic will heighten the risk of vehicle-pedestrian interactions in an area already frequented by school children and residents. The Traffic and Parking Management plan does not sufficiently mitigate these risks.</p> <p>The proposed entry and exit points for the car park will add to the already busy pedestrian pathways, further endangering the safety of pedestrians.</p> <p>Parking</p> <p>The development provides 16 car parking bays, which is a shortfall compared to the required 21 bays based on the car parking standards for such a facility. This deficit will likely lead to on-street parking, causing inconvenience and potential hazards for residents.</p> <p>Considering these concerns, we urge the Town of Bassendean and the Metro Inner Development Assessment Panel (DAP) to reject the development application. The proposal is not aligned with the current residential zoning, and the anticipated negative impacts on traffic, noise, and pedestrian safety are significant.</p> <p>We recommend exploring alternative sites more suited to accommodating a childcare centre without compromising the residential nature and safety of the local community.</p> <p>Thank you for considering our submission.</p>	The use is inconsistent with residential zoning and character Traffic Noise Access Parking	Additional parking bays have been included post advertising. The permissibility of the land use, traffic, noise access and parking are discussed in the report.
57	<p>I am writing to express my objection to the proposed development application for a child care centre on West Road. As a nearby resident (88A West Road, Bassendean), I have significant concerns regarding the impact of this development on our community.</p> <p>One of the main concerns is the issue of traffic congestion. West Road is a main thoroughfare and already experiences heavy traffic due to the close proximity of Bassendean Primary School and Last Crumb Co, and adding a child care centre without adequate parking facilities will exacerbate this problem. The safety of pedestrians and residents, especially children, could be compromised due to increased vehicular movement in an already congested area.</p> <p>Furthermore, the proposed site lacks sufficient parking spaces to accommodate the number of staff and parents who would be dropping off and picking up children on a daily basis. This shortfall is likely to result in cars parking on residential verges on West Road and surrounding streets, further congesting the area and potentially causing traffic hazards.</p> <p>I believe that a thorough traffic impact assessment and a comprehensive parking plan are essential before considering any approval for this development. West Road is currently closed due to sewerage works and at this time does not truly reflect the level of traffic which we usually experience on our street. It is crucial to ensure that the proposed child care centre does not further contribute to the traffic issues we are already experience or compromise the safety and convenience of residents in the neighbourhood.</p> <p>As a concerned resident, I urge the Metro Inner Development Assessment Panel (DAP) to reconsider this application and to prioritise the wellbeing, safety and wishes of the community. I respectfully request that my objections be taken into account during the decision making process.</p>	Traffic Parking	Traffic and parking are discussed in the report
58	<p>I am writing to express my concern regarding the proposed childcare centre being located on West Road.</p> <p>West Road has had a significant increase in traffic over recent years and putting a childcare centre there will further increase the traffic. I am concerned that there is not sufficient infrastructure (including parking) for the proposed childcare centre at this location and suggest an alternative location be identified.</p> <p>I would add my signature to the circulating petition, however I am unable to do so in the required timeframe. Please consider this email as the addition of my name to the petition.</p>	Traffic Parking	Traffic and parking are discussed in the report
59	<p>We are writing to express our strong opposition and concern to the proposed childcare centre development on the corner of Bridson Street and West Road, Lot 85 (No. 94) West Road, Bassendean - reference number DA-2024-066. As a resident of this neighbourhood (Bridson Street), we are deeply concerned about the potential negative impact this project could have on our community.</p> <p>The location of the proposed childcare centre, situated on the corner of two busy local distributor roads, and 500 metres from our home, raises significant concerns regarding increased traffic congestion in a residential area and associated safety risks. The introduction of additional vehicles, particularly during peak hours, is likely to exacerbate existing traffic problems and create delays for both residents and commuters alike.</p> <p>This will cause undue stress to local residents and families who live on the streets which will become heavily utilised thoroughfares. Currently it's difficult during peak times to turn onto or from Guildford Road from Shackleton Street/Bridson Street, with this proposal further contributing to increased volumes entering and exiting, presenting a serious safety and congestion concern. Residents of Bridson Street since 2020, daily we witness high volumes of vehicles travelling on our street, with many speeding well</p>	Traffic Scale of the development is not appropriate in a residential area Noise Parking	<p>Updated traffic counts and additional onsite parking bays were provided post advertising.</p> <p>The suitability of the use (including the scale) in a residential area, traffic, noise and parking are discussed in the report.</p>

	<p>in excess of the 50kph speed limit (despite multiple speed bumps), making entering and exiting our home and having small children present a safety challenge each day. The daily traffic movements of “346” additional vehicle trips as a direct impact of this development, of which majority will occur on Bridson Street and West Road, is not ideal.</p> <p>Lot 85 (No. 94) West Road is currently and appropriately zoned as ‘Residential’ (R20) – as it should remain. The Traffic Impact Statement for the proposal sites traffic count data for the main areas of impact from only 21/22 (“2,673 <i>vehicles per day</i>” for our street - Bridson), this therefore is conservative and is not reflective of the growth in the area and change in the Perth housing market with more occupants (and their cars) occupying dwellings due to rental and housing shortage crisis. Adequate monitoring of the impacted roads and more recent data obtained with further analysis completed should be a bare minimum prerequisite of submitting such a proposal.</p> <p>Moreover, the anticipated influx of vehicles navigating to and from the childcare centre poses a serious safety hazard, especially for pedestrians and cyclists who already contend with heavy traffic flow along this inner suburb route. The potential for accidents and disruptions to the flow of traffic is a legitimate concern that must be addressed before any development proceeds.</p> <p>The Traffic Impact Statement also makes claims of other surrounding roads absorbing “<i>significantly less traffic than West Road; moreover, the traffic would be dispersed, so the impact can be considered negligible.</i>” – this claim is not true of the likely impact to our street Bridson street. The same report specifies the road network increase is considered ‘<i>moderate traffic impact</i>’ - which is not negligible (definition of negligible being ‘so small or unimportant as to be not worth considering; insignificant’. Claiming to purport negligible impact is simply an unfair claim as it is not factual nor does the business making this proposal live locally so their primary concern is purely commercial in nature with little regard to local impact.</p> <p>A proposal of this category is better suited to a more appropriate location i.e. closer to the main busy roads of the town centre of Bassendean (Old Perth Road). The scale (and noise generation) of the 2-story proposal (catering to 80 children) feels out of environmental context in proximity to the surrounding residential homes and feel this will have adverse quality of life impacts on immediate surrounding residents which we think will outweigh benefits of this specific proposal. Furthermore, the number of parking bays feels insufficient – currently dangerous with two intersections (Hyland and Bridson) and sometimes poor visibility from residents parking. Overflow parking and traffic are highly likely to negatively impact the flow of traffic on West Road and presenting pedestrian safety concerns (we walk past the proposed driveway and drop off point on foot every day) - particularly for children going to the nearby school.</p> <p>In light of these considerations, we urge you to reconsider the suitability of this location for a childcare centre. We acknowledge the synergy of having the Bassendean Primary School nearby, however the on-street parking referenced to support the development is already used to capacity by the Last Crumb Cake Co café customers and the Bassendean Primary School. This will further congest and increase danger of an already heavily utilised (pedestrians and cars) road and area. Alternative nearby sites that do not compromise the safety and convenience of local residents should be explored to ensure a balanced approach to community development whilst also addressing Bassendean’s growing needs for Childcare and supporting local working families.</p> <p>Please note, we haven’t considered this proposal flippantly or lightly, being a couple who require daycare. Thank you for considering our views on this matter. We trust that you will take into account the concerns of residents like us who are directly impacted and have to encounter the long-lasting impacts of a once off development approval for a non-resident seeking to capitalise.</p>		
60	<p>I would like to state that myself and my family oppose the proposed childcare centre on the Cnr of West Rd and Bridson St in Bassendean for the following reasons:</p> <p>15 car bays are no where near enough and when they mention 13 staff that is a gross understatement for up to 80 children. The 13 staff are probably the fully qualified staff but there would surely be others working there so there goes the 15 parking bays. Where would the other staff park and where would parents park for dropping off and picking up their children. You must know that West Road is already a very busy road as is Bridson Street and parking along there is always taken up with the Last Crumb customers and the parents of school children morning and afternoons. It would be ludicrous to add another business to become more congested and even more dangerous.</p> <p>According to google there are 23 childcare centres in Bassendean and 16 of them have vacancies so why would we need another one and this block is currently zoned R20 residential. If there is a need for another centre then it needs to go in a much safer and less busy place to ensure complete safety for all.</p>	<p>Parking Lack of demand for child care services</p>	<p>The applicant confirmed up to 14 staff will be onsite at any one time. Parking and demand for child care services are discussed in the report.</p>
61	<p>I am writing as a rate payer living at 125 Whitfield Street, Bassendean.</p> <p>As parents of small children, my husband and I are supportive of the proposed childcare centre. We don’t have enough childcare options in Bassendean and it’s difficult to get the care you need in the area.</p> <p>We would be interested in potentially enrolling our children as it would be within walking distance for us. We are not concerned about parking as we currently have only 3 drop off bays at our daycare and there are no issues. We also would walk to the centre anyways.</p> <p>We love the continued progression in the area including the park upgrades at sandy beach and love having a vibrant cafe like last crumb so close by!</p> <p>Please let us know if you require any additional information.</p>	<p>Support Additional child care services are required in the area</p>	<p>Demand for child care services is discussed in the report.</p>
62	<p>I provide the following concerns and objections regarding the proposed development of a Child Care Premises at Lot 85 (No. 94) West Road, Bassendean (corner of West Road and Bridson Street) - DA-2024-066.</p> <p>I strongly oppose the development of Lot 85 (No. 94) West Road, Bassendean into a childcare centre. Firstly, the location of the proposed development is in an area that it is all residential. Allowing a commercial development in a residential area causes a negative impact on the surrounding properties on West Rd, Bridson St and Hyland St. It impacts the tranquil nature of this part of the suburb and the privacy of residents. The large childcare development is not consistent with the area and it extremely unfair on nearby residents.</p> <p>The location of the proposed development also causes significant traffic and parking concerns. The entry to the Childcare premises on West Rd is opposite Hyland St, which is part of a bus route, causing safety and traffic flow issues with cars needing to enter into and out of the childcare, as well as busses traveling along West Rd, and traffic using Hyland St and Bridson Street as route to Guildford Road will cause traffic chaos and significant safety issues for pedestrians and vehicles.</p> <p>There are a large number of children that use West Rd to walk and ride to school, especially during Peak times, and having a large number of cars coming in and out of the childcare entry on West Rd is a serious safety issue. Not to mention the number of other pedestrians that use this route to walk pets or to walk to the town centre, it is not the right place to have cars pulling in and out.</p> <p>I am also very concerned regarding the lack of parking onsite and the overflow parking that will occur on West Rd, Hyland St and Bridson Rd. Not having the adequate number of bays as required by local planning will mean vehicles will park on the side of the road and verge, causing further safety issues and impact traffic flow. Further it is unfair for residents in a residential area such as this, to put up with cars parked on the side of the road and the increased noise, disruption and traffic from a large childcare centre. Having cars parked on the side of the road, will also impact the ability of cars pulling in and out of the childcare car park having clear sight of vision – further impacting the safety of pedestrians and other vehicles.</p> <p>Given the traffic from the primary school and the increased traffic as a result of the popular Last Crumb Coffee shop, this development will put even more pressure on traffic flow and parking congestion for nearby residents. The location should remain residential, and the childcare development should not be approved</p>	<p>Inappropriate commercial use in residential area Privacy Traffic Parking Access Sightlines Noise</p>	<p>Additional parking bays were included following advertising. The permissibility of a child care premises on land zoned ‘residential’, traffic, parking, access, sightlines and noise a are discussed in the report.</p> <p>The proposal does not result in overlooking of any adjoining property.</p>
63	<p>I apologise for contacting you on a different matter, as I realise you are a very busy man. I have only recently heard about the Child Care Centre being proposed for the block at the corner of West Road and Bridson Street Bassendean. I am led to believe that the applicants have bypassed the Town of Bassendean and gone direct to Planning, which I am assuming they are entitled to do.</p> <p>I am strongly opposed the location of the Child Care Centre, not only for the poor neighbours who are going to have to put up with a lot more traffic and noise in their</p>	<p>Traffic Noise Access</p>	<p>Traffic, noise and access are discussed in the report</p>

	<p>residential street, but also for the safety of the children and public using these roads. I am not against progress, as we have some amazing examples in Bassendean, but West Road in Particular was never designed for the ever increasing traffic. The roads are not wide enough to accommodate the traffic. The Last Crumb has created a icon for Bassendean. However, the parking on West Road is an accident waiting to happen. So close to a school and children being children I am amazed someone hasn't been seriously injured or worse. The women attending there seem to have "baby Brain", a known condition. They leave the car doors open into traffic. They pull out into traffic. Anyway not my business now, but I can't say the same of the Child Care Centre. My concerns are:</p> <ol style="list-style-type: none">1. Proximity to neighbours and lack of their quiet enjoyment.2. Buses travelling down West Road to turn at Hyland Street, which they have always done.3. Children using West Road and Bridson Street to walk to school.4. Lack of safe entry and exit onto busy streets.5. A child is going to be killed on West Road. <p>I have told my neighbours that I am going to make a placard and chain myself to the fencing at the block. Naked. Not a good look and it may very well scar someone for life, butt that is how strongly I feel. My neighbours have laughed. (I wouldn't do that, but I wish I did). Could you imagine the media coverage it would get as I am no pretty, petite filly.</p> <p>Would you be kind enough to tell me your opinion of the project and what I can do to try and draw attention to it.</p>		
64	<p>We, the underside, are strongly against the proposed development of 94 West Road, Bassendean into an Early Learning Centre and do not believe the zoning should allow for this development. This is a residential area, and we strongly believe it should remain residential. The proposed commercial development will impact our right to quiet enjoyment of our properties and cause undesirable traffic issues. The location of 94 West Road is on the corner of Bridson Street, which is used as a main artery to/ from Guildford Road and is busy during peak times. Further, Hyland Street intersection is directly opposite 94 West Road, together with increased traffic from the busy coffee shop – Land Crumb and the Bassendean Primary School it will create absolute chaos furthermore, the development does not cater for the required number of parking bays.</p>	<p>Use Noise Traffic Parking</p>	<p>Suitability of the use, noise, traffic and parking are discussed in the report.</p>
	<p>Water Corporation: Reticulated water is currently available to the subject area. The applicant will be responsible for funding any costs of a water connection (meter) and make payment of the appropriate fees and charges and Standard Infrastructure Contributions attributed to the nominated size and flow rate of the connection/s required. The requirement for a new and/or additional fire service connection (meter) will incur additional costs, attributed to the nominated size of the connection/s required. These connections will also be subject to an agreement at the time of application. A water connection may result in the installation of a backflow protection device. The applicant must consult a suitably qualified licensed plumbing contractor or hydraulic consultant to assess the developments requirements. The cost of the installation and ongoing testing and maintenance is the responsibility of the lot owner.</p> <p>Wastewater Reticulated sewerage is currently available to the subject area. All sewer main extensions, if required for the development site, should be laid within the existing and proposed road reserves, on the correct alignment and in accordance with the Utility Providers Code of Practice. It should be noted that existing sewerage mains are located within the subject land. Any portion of the proposed building which is within the zone of influence to sewer main may require suitable footings in accordance with our technical guidelines. Please refer to our website: www.watercorporation.com.au/Developing-and-building/Working-near-assets.</p> <p>Approval for works Any works carried out in proximity to our Assets must receive prior approval by applying for an Asset Protection Risk Assessment (APRA). To assess whether the proposed development will require an APRA, details of the Prescribed Proximities are available on our website: www.watercorporation.com.au/Developing-and-building/Working-near-assets/Approval-for-works.</p> <p>Building Approval Application The applicant is required to submit a Non Residential Application by using our online portal, BuilderNet: login-buildernet.watercorporation.com.au. Attachments required for approval will include: ⌘ Final construction site & architectural floor plans ⌘ Engineer certified piling detail plans (if required) ⌘ Hydraulic Plans – Water & Wastewater ⌘ Trade Waste Application Form - www.watercorporation.com.au/Help-and-advice/Tradewaste/Apply/Apply-to-discharge-trade-waste/Application-forms ⌘ Trade Waste Supplement Form (Food Preparation) The information provided above is subject to review and may change. If the proposal changes, please contact us to confirm that this information is still valid. Please provide the above comments to the landowner, developer and/or their representative. Should you have any queries or require further clarification on any of the above issues, please do not hesitate to contact our Enquiries Officer.</p>	<p>The site has access to water and sewerage services</p>	<p>Availability of services is noted</p>

Motion/Officer Recommendation – Item 12.8

MOVED Cr Tallan Ames, Seconded Cr Paul Poliwka

That Council resolve to endorse the Officer's Recommendation as that of the Responsible Authority; and recommend the Metro Inner Development Assessment Panel approve the application subject to conditions.

Voting requirements: Simple Majority

LOST 3/3

For: Cr Jamayne Burke, Cr Paul Poliwka and Cr Tallan Ames

Against: Cr Kathryn Hamilton, Cr Ken John and Cr Jennie Carter

The result was a tied vote and the Presiding Person exercised her authority in accordance with *section 5.21.(3) of the Local Government Act* to cast a second vote against the motion.


Reasons for not supporting the Officer's recommendation

Council resolved to not support the officer's recommendation to the Metro Inner Development Assessment Plan for the proposed Child Care Premises at Lot 85 (No. 94) West Road, Bassendean for the following reasons:

- The Council is not satisfied that the proposed use is compatible with or complementary to the surrounding residential zone under Local Planning Scheme No. 11, for the following reasons:
 - The proposed development results in a shortfall of four vehicle parking bays, as assessed against Local Planning Policy No. 8 – Car Parking and End-of-Trip Facilities, which is likely to adversely impact the amenity of the surrounding locality.

DR3 – Design review report and recommendations

This report is prepared by the panel coordinator and checked by the design review Chair. To maintain the integrity and independence of the design review process this report should be attached, unedited to Council reports and (if applicable) the Development Assessment Panel Responsible Authority Report.

Local government	Town of Bassendean	
Date	08 November 2024	
Location	Offline	
Panel members	Kris Mainstone	Chair
Proponent/s	Trish Byrne Frank Macri	Owner (R-Point Property) Builder (Macri Builders)
Conflict of Interest	None declared	
Briefings		
Development assessment overview	Patricia Hirst	Senior Planning Officer
Technical issues	Patricia Hirst	Senior Planning Officer
Design review		
Proposed development	Child Care Premises	
Property address	Lot 85 (No. 94) West Road, Bassendean	
Background	<p>3 previous DRP reviews have occurred previously. The DRP and applicant have agreed to an offline process to resolve final few items.</p> <p>The land is zoned 'Residential' (R20) under the Town of Bassendean Local Planning Scheme No. 11. 'Child Care Premises' is an 'A' use in the zone.</p> <p>The 1,258sqm site is located on the corner of Briston Street and West Road. Surrounding sites have been developed for low density housing. The site is located approximately 200m from Bassendean Primary School and 600m from the Bassendean Town Centre.</p>	
Proposal	Child Care Premises	
Applicant/representative	Trish Byrne	Owner (R-Point Property)
Key issues/recommendations	<p>The proposal has continued to progress through further updates and design development.</p> <p>Several improvements have been provided including;</p> <ul style="list-style-type: none">• The architectural treatment of the upper level play deck.• Confirmed sustainability commitments• Improved upper level universal access.• Increased width to NE corner carpark screening. <p>The panel is supports the proposal design.</p>	
Chair signature		

DR1 – Design review report and recommendations (Part 2/3)

Design quality evaluation		
		<i>Supported</i>
		<i>Pending further attention</i>
		<i>Not Supported</i>
		<i>Yet to be Addressed</i>
Principle 1 - Context and character		<i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i>
		1a. Simplified upper deck area improves the design's relationship to its context
Principle 2 - Landscape quality		<i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i>
		2b. Landscape screening of the car parking has been increased in width and practicality. Landscaping is now sufficient to screen parked cars. 2c. Artificial turf is supported with use of cool turf products. 2e. Justification of the functionality and flow of the landscape outdoor play space design has been provided.
Principle 3 – Built form and scale		<i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i>
		3b. The relocated first floor gazebo and upper floor fencing/walling has been redesigned and has improved. This can now be supported 3e. Entry has been simplified and has improved legibility and formal presentation
Principle 4 - Functionality and build quality		<i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.</i>
		4a. Updated UA access to first floor office and staff room is great improvement. And can be supported.
Principle 5 - Sustainability		<i>Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.</i>
		5a. 10kw PV system has been confirmed and is a great outcome. Rainwater storage and Heat Pump HWS to be conditioned in planning approval. 5c. A skylight has been added to Activity Room 1 as recommended
Principle 6 - Amenity		<i>Good design optimises internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.</i>
		No change
Principle 7 - Legibility		<i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i>
		No change
Principle 8 - Safety		<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>
		8b. Principle 8 Safety is supported. The provision of partial fencing between entry and carpark is strongly recommended.


Principle 9 - Community		<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
		No Change
Principle 10 Aesthetics		<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
		10a. The faux heritage aesthetic has been simplified to be more balanced between context and a more contemporary design. 10b. The aesthetics of the upper-level play area has been simplified and is more balanced. The relocation of the gazebo structure to align with carpark columns feels more logical and improves the overall composition.

DR1 - Design review report and recommendations Part 3/3					
Design Review Progress					
	<i>Supported</i>				
	<i>Pending further attention</i>				
	<i>Not supported</i>				
	<i>Yet to be addressed</i>				
	DR1 (9 Feb 24)	DR2 (12/7/2024)	DR3 (25/09/24)	DRP 4 (8/11/24)	
Principle 1 - Context and character					
Principle 2 - Landscape quality					
Principle 3 - Built form and scale					
Principle 4 - Functionality and build quality					
Principle 5 - Sustainability					
Principle 6 - Amenity					
Principle 7 - Legibility					
Principle 8 - Safety					
Principle 9 - Community					
Principle 10 - Aesthetics					







DR3 – Design review report and recommendations

This report is prepared by the panel coordinator and checked by the design review Chair. To maintain the integrity and independence of the design review process this report should be attached, unedited to Council reports and (if applicable) the Development Assessment Panel Responsible Authority Report.

Local government	Town of Bassendean	
Item no.	1	
Date	25 September 2024	
Time	10:30am	
Location	Town of Bassendean	
Panel members	Kris Mainstone Brett Wood-Gush Rachael Lewis Simon Venturi	Chair
Local government officers	Alex Snadden Patricia Hirst Rachel Neve	Manager Planning and Regulation Senior Planning Officer Planning Officer
Proponent/s	Trish Byrne Frank Macri	Owner (R-Point Property) Builder (Macri Builders)
Apologies	Philip Gresley	
Conflict of Interest	None declared	
Briefings		
Development assessment overview	Patricia Hirst	Senior Planning Officer
Technical issues	Patricia Hirst	Senior Planning Officer
Design review		
Proposed development	Child Care Premises	
Property address	Lot 85 (No. 94) West Road, Bassendean	
Background	A pre-lodgement design review was carried out on 9/2/2024. A development application was subsequently lodged (accepted by the Town on 17/6/2024). The submission includes a Traffic Impact Statement, Acoustic Assessment and Sustainability Statement. The applicant has opted to have the application determined by the DAP. The land is zoned 'Residential' (R20) under the Town of Bassendean Local Planning Scheme No. 11. 'Child Care Premises' is an 'A' use in the zone. The 1,258sqm site is located on the corner of Briston Street and West Road. Surrounding sites have been developed for low density housing. The site is located approximately 200m from Bassendean Primary School and 600m from the Bassendean Town Centre.	
Proposal	Child Care Premises	
Applicant/representative	Trish Byrne	Owner (R-Point Property)

Key issues/recommendations	<p>The proposal has improved through the simplification of heritage references, new fencing variation, redesigned parking, and a more generous entry path.</p> <p>Several improvements suggested by the Panel require resolution, these include;</p> <ul style="list-style-type: none"> • The architectural treatment of the upper level play deck. • Confirmed, detailed and noted sustainability commitments • Improved upper level universal access. • Increased width to NE corner carpark screening. • Others detailed in the report below. <p>Of these items, the architectural treatment of the <u>upper level play deck</u> is particularly unsuccessful and requires focussed attention. Options for this element can be reviewed separately via email should the applicant wish to expedite the process.</p> <p>The Panel is Supportive Pending addressing the listed items.</p>
Chair signature	

DR1 – Design review report and recommendations (Part 2/3)

Design quality evaluation		
		<i>Supported</i>
		<i>Pending further attention</i>
		<i>Not Supported</i>
		<i>Yet to be Addressed</i>
Principle 1 - Context and character		<i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i>
		1a. The design direction of the proposal contributes to the local area in using a traditional and character driven solution. The proposal is appropriate in terms of height, bulk and scale; and sympathetic to the streetscape and residential context and character of the area.
Principle 2 - Landscape quality		<i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i>
		<p>2b. Landscape screening of the car parking area still to be satisfactorily resolved. Depth of reversing aisle is greater than required 1m. Suggest moving parking west approx. 400mm to increase landscape buffer to 900mm.</p> <p>2c. Artificial turf is limited to landscape on structure with reduced extents. Can be supported if demonstrated that usability can be achieved through shading and use of cool turf products. Recommend further reduction of extents and greater use of alternative materials (soft fall, sand, mulch).</p> <p>2c. Jacaranda tree has been removed from landscape design. Still no additional natives introduced</p> <p>2e. No improvement to the functionality and flow within landscaped outdoor play spaces. The landscape design should reflect the practicalities of its use and maximise opportunities for supervision of children, access and opportunities for children's play.</p> <p>2f. A detailed landscape design brief stipulating key elements of an appropriate landscaping outcome has been provided.</p>

Principle 3 – Built form and scale		<i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i>
		<p>3a. The built form and scale of the proposal is generally appropriate.</p> <p>3b. The relocated first floor gazebo has not improved the resolution of the car park/ upper floor play space structure which is inconsistent with the area and the remainder of the design. Requires further design work to resolve.</p> <p>3c. Aspects of the design representative of ‘faux heritage’ have been revised to be a contemporary interpretation of heritage form.</p> <p>3e. No attempt has been made to redesign the form of the entry to the building. There is an opportunity to improve the legibility and formal presentation to the street whilst also resolving functional issues.</p>
Principle 4 - Functionality and build quality		<i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.</i>
		<p>4a. Notwithstanding the meeting of technical universal access requirements, the Panel notes the lack of provision of universal access requirements for the upper level is still a poor outcome. Options to reconfigure the internal layout of the first floor to align services with those on the ground floor and remove the need for steps to accommodate changes in floor level is recommended. Should this not be readily achieved, suggest lowering the staff room as a minimum.</p> <p>4b. Shared reversing bay arrangement is functional and efficient. Flows between the car park, entry/reception area is improved but requires partial fencing/balustrading to contain children exiting.</p> <p>4d. Nappy Room 2 has been revised and is supported.</p>
Principle 5 - Sustainability		<i>Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.</i>
		<p>5a. The measures noted within the sustainability report need to be added to the drawings as hard commitments. Elements like water storage tanks and heat pump hot water systems need to be shown on plans. Size of PV system needs to be confirmed.</p> <p>5b. The panel commends the commissioning of a sustainability report.</p> <p>5c. A skylight has been added to Activity Room 2. Consideration should be given to the incorporation of a skylight to Activity Room 1.</p>
Principle 6 - Amenity		<i>Good design optimises internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.</i>
		<p>6a. Further resolution of noisy play strategy is required.</p> <p>6b. The relocated emergency access stairs location is greatly improved and supported.</p>
Principle 7 - Legibility		<i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i>
		<p>7a. The design of the Entry has good legibility. Notes on the aesthetics and function are covered elsewhere within this report.</p>
Principle 8 - Safety		<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>
		<p>8b. Principle 8 Safety is supported subject to provision of partial fencing between entry and carpark.</p>

Principle 9 - Community		<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
		9a. The Panel commended the applicants on their vision for the use of the site as a childcare centre, and the contributions this use will bring in respect to economic and social benefits to the community.
Principle 10 Aesthetics		<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
		<p>10a. The faux heritage aesthetic has been simplified to be more balanced between context and a more contemporary design.</p> <p>10b. The aesthetics of the upper-level play area are still unsuccessful and require significant design work. The relocation of the gazebo and the extensive solid wall has not improved the design. Consideration should be given to breaking up the horizontality of this element by introducing visually transparent elements, planters and changes in setback. Grounding the structure by continuing walls or columns to the carpark level may assist. The stepping of columns into a different alignment is difficult to resolve with the chosen aesthetic.</p> <p>10c. The revised white picket fencing to the ground level street interface has improved through more articulation.</p> <p>10e. The entry/ reception area will significantly benefit from redesign to improve aesthetics.</p>

DR1 - Design review report and recommendations Part 3/3


Design Review Progress

	Supported			
	Pending further attention			
	Not supported			
	Yet to be addressed			
	DR1 (9 Feb 24)	DR2 (12/7/2024)	DR3 (insert date)	
Principle 1 - Context and character				
Principle 2 - Landscape quality				
Principle 3 - Built form and scale				
Principle 4 - Functionality and build quality				
Principle 5 - Sustainability				
Principle 6 - Amenity				
Principle 7 - Legibility				
Principle 8 - Safety				
Principle 9 - Community				
Principle 10 - Aesthetics				







DR2 – Design review report and recommendations

This report is prepared by the panel coordinator and checked by the design review Chair. To maintain the integrity and independence of the design review process this report should be attached, unedited to Council reports and (if applicable) the Development Assessment Panel Responsible Authority Report.

Local government	Town of Bassendean	
Item no.	1	
Date	12 July 2024	
Time	10:00am	
Location	Town of Bassendean	
Panel members	Philip Gresley Brett Wood-Gush Rachael Lewis Simon Venturi	Chair
Local government officers	Alex Snadden Patricia Hirst Rachel Neve	Manager Planning and Regulation Senior Planning Officer Planning Officer
Proponent/s	Trish Byrne Frank Macri	Owner (R-Point Property) Builder (Macri Builders)
Observer/s		
Conflict of Interest	None declared	
Briefings		
Development assessment overview	Patricia Hirst	Senior Planning Officer
Technical issues	Patricia Hirst	Senior Planning Officer
Design review		
Proposed development	Child Care Premises	
Property address	Lot 85 (No. 94) West Road, Bassendean	
Background	<p>A pre-lodgement design review was carried out on 9/2/2024.</p> <p>A development application was subsequently lodged (accepted by the Town on 17/6/2024). The submission includes a Traffic Impact Statement, Acoustic Assessment and Sustainability Statement.</p> <p>The applicant has opted to have the application determined by the DAP.</p> <p>The land is zoned 'Residential' (R20) under the Town of Bassendean Local Planning Scheme No. 11. 'Child Care Premises' is an 'A' use in the zone.</p> <p>The 1,258sqm site is located on the corner of Briston Street and West Road. Surrounding sites have been developed for low density housing. The site is located approximately 200m from Bassendean Primary School and 600m from the Bassendean Town Centre.</p>	
Proposal	Child Care Premises	
Applicant/representative	Trish Byrne	Owner (R-Point Property)
Key issues/recommendations	<p>The Panel thanks the Applicant for the consideration given to the DRP1 Panel comments. The proposal has improved through the widening of verandahs, increase in roof pitch, bike parking to the entrance arbor, setting back the kitchen off the boundary, introducing a reversing bay, articulating the front fencing with integrated signage, and amending the vehicle crossover to enable the retention of the existing verge tree.</p>	

	<p>However, there remain a number of design opportunities that remain unexplored which require resolution prior to receive Panel support. These include;</p> <ul style="list-style-type: none"> • The functionality, safety, amenity, legibility, and aesthetics of the main entry to the building. • The height and character interpretation of the front fencing. • The architectural treatment of the upper level play deck. • Sustainability commitments • Others detailed in the report below. <p>The Panel is Supportive Pending Further Attention Another review is recommended.</p>
Chair signature	

DR1 – Design review report and recommendations (Part 2/3)

Design quality evaluation		
		<i>Supported</i>
		<i>Pending further attention</i>
		<i>Not Supported</i>
		<i>Yet to be Addressed</i>
Principle 1 - Context and character		<i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i>
		1a. The design direction of the proposal contributes to the local area in using a traditional and character driven solution. The proposal is appropriate in terms of height, bulk and scale; and sympathetic to the streetscape and residential context and character of the area.
Principle 2 - Landscape quality		<i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i>
		<p>2a. Measures to retain the street tree are commended.</p> <p>2b. Landscape screening of the car parking area requires further attention. It has not been demonstrated the 0.5m wide garden is sufficient to accommodate landscape to effectively screen the car parking area.</p> <p>2c. Artificial turf is not supported by the Panel and should be removed from the landscape concept completely. Consideration should be given to alternative materials (soft fall, sand, mulch).</p> <p>2c. Consideration should be given to native shade trees appropriate for a childcare setting. Shade trees should be incorporated into the first floor outdoor play space Concerns were raised around the Jacaranda tree and the attraction of bees on ground, which is a safety concern.</p> <p>2e. The functionality and flow within landscaped outdoor play spaces is critical to the landscape design. The landscape design should reflect the practicalities of its use and maximise opportunities for supervision of children, access and opportunities for children's play.</p> <p>2f. The Panel notes that while a conceptual landscape design may be considered, the plan should be guided by a detailed landscape design brief stipulating key elements of an appropriate landscaping outcome; including the amount of shade, trees, deep soil areas, play spaces and operational aspects to be embedded in the landscaped areas.</p>

Principle 3 – Built form and scale		<i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i>
		<p>3a. The built form and scale of the proposal is generally appropriate.</p> <p>3b. However, the formal composition and resolution of the car park/ upper floor play space structure is inconsistent with the area. Consideration should be given to relocating the first floor gazebo to incorporate this element into the car parking structure.</p> <p>3c. Those aspects of the design representative of ‘faux heritage’ should be revised and replaced to incorporate a contemporary interpretation of heritage form. This language should be further developed to resolve a cohesive design language across the design.</p> <p>3d. The replication of the scalloped “picket fence” motif applied to the upper level play is not supported.</p> <p>3e. The Panel recommends reconsidering the form of the entry to the building. There is an opportunity to improve the legibility and formal presentation to the street whilst also resolving functional issues (listed below).</p>
Principle 4 - Functionality and build quality		<i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.</i>
		<p>4a. Notwithstanding the meeting of technical universal access requirements, the Panel notes the lack of provision of universal access requirements for the upper level is a poor outcome. Options to reconfigure the internal layout of the first floor to align services with those on the ground floor and remove the need for steps to accommodate changes in floor level is recommended.</p> <p>4b. Previous recommendations to improve functionality and movement between the entry space and eastern verandah have not been taken on board. Flows between the car park, entry/reception area and activity rooms remain problematic. Fencing necessary to contain children exiting the centre will further restrict flows. This arrangement requires further attention. Consideration should be given to modifications to the location of the reversing bay, design of the entry/ reception space and doors to increase circulation space and improve flows at the centre entry.</p> <p>4c. The functionality of the parking area and the practicality of the tandem parking arrangement proposed is questionable. The applicant should liaise further with Town staff regarding this aspect of the proposal.</p> <p>4d. Nappy Room 2 interrupts circulation around the centre and reduces functionality of the outdoor play space.</p>
Principle 5 - Sustainability		<i>Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.</i>
		<p>5a. The incorporation of sustainability principles in the operational aspects of the proposal is supported, however ESD measures referenced in the application are non-comital. Specific details of measures to be incorporated into the development should be clearly noted, rather than ‘provision for’.</p> <p>5b. Additional sustainability measures should be imbedded in the built form and design beyond the minimum standards required by the BCA. It is recommended the applicant engage an ESD professional in this regard.</p> <p>5c. Consideration should be given to the incorporation of a skylight to Activity Room 2 and improved access to northern light to the upper level.</p>
Principle 6 - Amenity		<i>Good design optimises internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.</i>
		<p>6a. Consideration should be given to the use of landscaping to reduce the impact of noise on neighbouring properties, by limiting access for children and appropriately guiding the location of noisy play.</p> <p>6b. The Panel suggests exploring relocating the emergency access stairs to above the bin store</p>

		to alleviate concerns regarding stairs located on the boundary.
Principle 7 - Legibility		<i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i>
		7a. The design of the Entry space will benefit from the use of an alternative material or be designed to be a feature of the building to improve legibility and contrast. This will also assist in creating a typological legibility by carefully and delicately acknowledging the nature of the building as a commercial facility.
Principle 8 - Safety		<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>
		8a. The inclusion of a reversing bay improves safety by reducing the need for reversing onto West Road when the carpark is full. 8b. More thought needs to be given to management of children exiting the centre into the car park. Any fencing to be proposed should consider impacts on overall flows at the entry point to the centre. 8c. Concerns were raised around the Jacaranda tree and the attraction of bees to the ground of the play space.
Principle 9 - Community		<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
		9a. The Panel commended the applicants on their vision for the use of the site as a childcare centre, and the contributions this use will bring in respect to economic and social benefits to the community.
Principle 10 - Aesthetics		<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
		10a. Since DRP1 the architectural features of the proposal have developed, against Panel advice, into a more faux heritage proposal. It is recommended that the architectural response re-develop a more contemporary approach. The general formal design response to the activity and other internal spaces are generally acceptable (except the entry) but the faux heritage detailing approach creates difficulty in resolving the less traditional elements such as the carpark into an appropriate response. 10b. Currently the aesthetics of the upper-level play area is unsuccessful. The use of the scalloped pickets on the first floor play area is problematic and not typically seen on double story character/ heritage homes. Consideration should be given to contemporary ways to use pickets to reduce the faux feel of this element of the design. There is opportunity to create a language that responds to the character of the area without introducing traditional details and elements. This will assist the proposal in creating a more cohesive presentation to the street, 10c. The use of extensive white picket fencing to the ground level street interface is unauthentic as the heights proposed are significantly higher than traditional front fencing in the area. The increased articulation of the fencing with solid panels and integrated signage is seen by the Panel as an improvement, however this important element needs additional consideration before it can be supported. 10d. Modifications to the roof pitch are supported. The roof pitch of incidental structures (gate house and gazebo) should be modified to match. 10e. The entry/ reception area will significantly benefit from redesign to improve aesthetics.

DR1 - Design review report and recommendations Part 3/3


Design Review Progress

	Supported		
	Pending further attention		
	Not supported		
	Yet to be addressed		
	DR1 (9 Feb 24)	DR2 (12/7/2024)	DR3 (insert date)
Principle 1 - Context and character			
Principle 2 - Landscape quality			
Principle 3 - Built form and scale			
Principle 4 - Functionality and build quality			
Principle 5 - Sustainability			
Principle 6 - Amenity			
Principle 7 - Legibility			
Principle 8 - Safety			
Principle 9 - Community			
Principle 10 - Aesthetics			








DR1 – Design review report and recommendations

This report is prepared by the panel coordinator and checked by the design review Chair. To maintain the integrity and independence of the design review process this report should be attached, unedited to Council reports and (if applicable) the Development Assessment Panel Responsible Authority Report.

Local government	Town of Bassendean	
Item no.	1	
Date	9 February 2024	
Time	10:00am	
Location	Town of Bassendean	
Panel members	Philip Gresley Brett Wood-Gush Rachael Lewis Simon Venturi	Chair
Local government officers	Alex Snadden Patricia Hirst Rachel Neve	Manager Planning and Regulation Senior Planning Officer Planning Officer
Proponent/s	Alan Stewart Trish Byrne Frank Macri	Planning Consultant (Lateral Planning) Owner (R-Point Property) Builder (Macri Builders)
Observer/s		
Conflict of Interest	None declared	
Briefings		
Development assessment overview	Alex Snadden	Manager Planning and Regulation
Technical issues	Alex Snadden	Manager Planning and Regulation
	Patricia Hirst	Senior Planning Officer
Design review		
Proposed development	Child Care Premises	
Property address	Lot 85 (No. 94) West Road, Bassendean	
Background	The 1,258sqm site is zoned Residential (R20) under the Town’s Local Planning Scheme No. 10. ‘Child Care Premises’ is listed as an ‘A’ use in the zone. The site is currently vacant. The site is not subject to any constraints or risk likely to influence the design. Four (4) mature street trees exist in the West Road verge abutting the site. Low density housing exists on adjoining lots to the north and west, as well as opposite the site. The site is located approximately 200m from Bassendean Primary School and 600m from the Bassendean Town Centre.	
Proposal	Child Care Premises	
Applicant/representative address to the design review panel	Alan Stewart	Lateral Planning
Key issues/recommendations	The proponent is thanked for a clear presentation of a project that has many redeeming qualities. Panel comments include; - The proposal is appropriate in terms of height, bulk and scale; and sympathetic to the streetscape, context and character of the area. - The provision of a detailed landscaping plan at this early stage is commended.	

	<p>Consideration should be given to appropriate species (including native species), increasing natural shade and soft landscaping in outdoor play spaces.</p> <ul style="list-style-type: none"> - Consideration should be given to opportunities to incorporate additional but subtle design elements that better reflect the commercial nature of the use. This should include rethinking the entry building in terms of materiality, colour and form. - Signage should also be considered and integrated carefully. - Public Art should be considered. - The internal layout of the building is generally good; however the panel identifies various opportunities to improve internal and external functionality. - The location of the car park and access point is logical, however there are some concerns around the functionality of the parking area (i.e tandem bays and lack of turning bay) and potential impacts on street trees of significance must be considered. - The proposal provides good pedestrian entry off the street. - It is recommended an ESD professional be employed to advise on sustainability initiatives that may be incorporated into the development.
Chair signature	

DR1 – Design review report and recommendations (Part 2/3)

Design quality evaluation		
		<i>Supported</i>
		<i>Pending further attention</i>
		<i>Not Supported</i>
		<i>Yet to be Addressed</i>
Principle 1 - Context and character		<i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i>
		1a. The design direction of the proposal contributes to the local area in using a traditional and character driven solution. The proposal is appropriate in terms of height, bulk and scale; and sympathetic to the streetscape and residential context and character of the area.
Principle 2 - Landscape quality		<i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i>
		<p>2a. The proponent is commended on the preparation of a detailed landscaping plan at this early stage and in consideration of the careful preservation of existing street trees of significance. This should be continued through the next design phases with appropriate tree preservation clauses to be included in the construction specification.</p> <p>2b. The Panel recommends the review of species selected, with an aim to increase the provision of natural shade to outdoor play spaces; prioritize those species suited to local conditions with high probability of long-term survival – particularly natives; and those species that are safe and practical in an early childhood setting. Artificial turf is not supported by the Panel, and it should be replaced with alternative natural materials.</p> <p>2c. The Panel suggests hard and soft landscaping in external play spaces be integrated to deliver a greater range of experiences in external spaces, while facilitating supervision of children.</p> <p>2d. Additional landscaping to screen the car parking is encouraged.</p>
Principle 3 – Built form and scale		<i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i>

		<p>3a. The built form and scale of the proposal is generally appropriate.</p> <p>3b. The Panel encourages improvements to the building's response to the Bridson Street frontage; including increasing the setback of the boundary wall (kitchen) from the street boundary and the use of landscaping to soften the interface.</p>
Principle 4 - Functionality and build quality		<p><i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.</i></p>
		<p>4a. The Panel queried the practicalities of the upper-level external play space given the lack of connectivity to an internal activity space. The proponent explained this was an acceptable management strategy. The Panel recommends that the functional experience of movement between levels be further explored. This includes the space around landings and access door orientation. The Panel also noted the lack of provision of universal access requirements between the lift and the outdoor play area.</p> <p>4b. The Panels suggests paths of travel between kitchen and activity rooms be considered in the context of food service and delivery. Appropriate width of verandahs providing weather protection and sealed surfaces suitable for wheeling meal carts should be considered. There is some concern about the lack of connection between the Kitchen and Activity Spaces 2 and 3.</p> <p>4d. The Panel suggests improving the functionality of movement between the Entry space and the eastern verandah. As the main circulation path there is a dog leg, small single door, and an uncovered section of verandah, which could all be improved.</p> <p>4e. The Panel suggests functionality and outside circulation is generally good; however notes toilets (Nappy Room 2) interrupts circulation and reduces functionality of the outdoor play space.</p> <p>4f. The Panel notes supply and functionality of parking facilities is important in this location and queries the practicality of the tandem parking arrangement proposed. The lack of a reversing bay in the car parking area appears problematic and a safety concern. The proponent should provide appropriate swept path diagrams demonstrating the functionality of the carpark.</p> <p>4g. There are concerns around the proximity of the bin store to the entrance and potential odour issues in this area.</p>
Principle 5 - Sustainability		<p><i>Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.</i></p>
		<p>5a. The Panel acknowledges and commends the focus on sustainability principles in the day-to-day operations and programs offered by the childcare centre. There is an opportunity to further enhance this program by introducing clear and visible sustainability outcomes into the building and landscape design.</p> <p>5b. The Panel encourages the engagement of an ESD professional to consider measures built into the design, including selection of materials, electric vehicle charging points, heat pumps, water catchment and optimal performance location of solar panels.</p> <p>5c. The panel would encourage the consideration of more access to northern light and limit unshaded east/west openings.</p> <p>5d. The inclusion of permeable paving to the carpark is supported.</p>
Principle 6 - Amenity		<p><i>Good design optimises internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.</i></p>
		<p>6a. The Panel commented whether roof skylights will improve light into activity rooms noting the east-west orientation.</p> <p>6b. The Panel commented that acoustic control requirements might be assisted by providing soft landscaping on the west side of the deck to keep children/ noise further away from the boundary.</p>
Principle 7 - Legibility		<p><i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i></p>

		<p>7a. There is an opportunity to improve the general legibility of the project which include:</p> <ul style="list-style-type: none"> • Integrated signage to the corner fence, which should be chamfered and incorporated into landscaping. • Revisiting the design of the Entry space which might benefit from the use of an alternative material or be designed to be a feature of the building to improve legibility and contrast. This will also assist in creating a typological legibility by carefully and delicately acknowledging the nature of the building as a commercial facility. <p>7b. The Panel acknowledged that the location of the carparking is logical considering the constraints of the site.</p> <p>7c. The Panel commented that wider verandah and paths would allow for improved accessibility around the site.</p> <p>7d. The Panel supports the clarity of the separate pedestrian entrance at the street.</p>
Principle 8 - Safety		<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>
		8a. The Panel cautions a lack of reversing bay, which would result in vehicles reversing onto West Road should the carpark be full. The proponent should provide appropriate swept path diagrams demonstrating the functionality and safety of the carpark.
Principle 9 - Community		<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
		<p>9a. The Panel commended the applicants on their vision for the use of the site as a childcare centre, and the contributions this use will bring in respect to economic and social benefits to the community.</p> <p>9b. The Panel suggests the proponents consider utilizing their public art liability onsite to provide a positive contribution to the streetscape.</p>
Principle 10 Aesthetics		<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
		<p>10a. The Panel generally supports the aesthetic of the proposed childcare centre. However, it is recommended that the proposed roof pitch should be increased to meet more traditional roof pitches within the area. The low, pitched roof to the entry facing West Road is out of place and should also be increased.</p> <p>10b. The Panel generally supports the white picket fencing as it provides appropriate interaction with the street. It is recommended however that more detail and solid panels are introduced to better respond to the rhythm of the existing residential streetscape. There is also an opportunity to incorporate signage/ public art.</p> <p>10c. The Panel cautions that the quality of architectural details of the proposal are very important to capture the intent of the surrounding heritage inspiration. This does not mean replicating heritage details, rather ensuring that the level of quality of detailing and associated proportions are maintained.</p> <p>10d. The Panel requests additional design work to be completed for the upper-level play area to enhance the aesthetic character including the grey element to the north. This is an important item that needs to be further developed.</p>

DR1 - Design review report and recommendations Part 3/3

Design Review Progress

	Supported		
	Pending further attention		
	Not supported		
	Yet to be addressed		
	DR1 (9 Feb 24)	DR2 (insert date)	DR3 (insert date)
Principle 1 - Context and character			
Principle 2 - Landscape quality			
Principle 3 - Built form and scale			
Principle 4 - Functionality and build quality			
Principle 5 - Sustainability			
Principle 6 - Amenity			
Principle 7 - Legibility			
Principle 8 - Safety			
Principle 9 - Community			
Principle 10 - Aesthetics			

An aerial photograph of a residential neighborhood, overlaid with a semi-transparent blue filter. The image shows several houses with various roof colors (brown, grey, blue) and green trees. A large, rectangular, light-colored vacant lot is the central focus. A road runs horizontally across the middle of the image, and another road runs vertically on the right side. In the top right corner, there is a white graphic element consisting of two perpendicular lines forming an L-shape.

Application for Development Approval

Child Care Centre

No.94 West Road
Bassendean

LATERAL
PLANNING

Document Control

Reference	0307
Location	Lot 85 (No.94) West Road, Bassendean
Client	Briscola Pty Ltd
Document Title	Application for Development Approval - Child Care Centre
Document File Name	0307 Town Planning Statement.docx
Document Date	31 May 2024
Document Version	Revision 00
Author	Adrian Dhue and Alan Stewart

Contents

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1.0 Introduction

Lateral Planning acts for Briscola Pty Ltd, the registered proprietor of the land situated at Lot 85 (No.94) West Road, Bassendean ('site').

This Town Planning Statement has been prepared in support of an Application for Development Approval ('Application') for the construction of a Child Care Centre on the site.

The Town Planning Statement provides an assessment of the proposed development against the applicable town planning framework and demonstrates the proposal is consistent with the amenity and orderly and proper planning of the locality. The Application is accompanied by the following technical documents.

Document	Consultant
Feature Survey	Vision Surveys
Geotechnical Report	Structerre
Architectural Drawings	Macri Builders
Landscape Plan	Childscapes
Town Planning Statement	Lateral Planning
Operational Management Plan	Lateral Planning
Acoustic Assessment	ND Engineering Consulting
Traffic Impact Statement	KCTT
Sustainability Statement	Briscola Pty Ltd
Stormwater Concept Plan	Structerre

Table 1: Consultant Team

2.0 Subject Site

2.1 Overview

Local Authority	Town of Bassendean
Locality	Bassendean
Address	No.94 West Road, Bassendean
Cadastral	Lot 85 on Plan 1784
Certificate of Title	Volume 596 Folio 156
Registered Proprietor	Briscola Pty Ltd
Land Area	1,258m ²
Frontages	West Road 63.5 metres Bridson Street 21.1 metres
Existing Land Use	Vacant Site

Table 2: Site Overview

2.2 Context

The site is situated 10 kilometres north-east of Perth in the suburb of Bassendean, in the Town of Bassendean ('Town'). Guildford is 1.5 kilometres to the north-east and the Swan River is situated less than 800 metres to the east. Originally known as West Guildford, the locality of Bassendean has transitioned from an agricultural area to a predominantly residential neighbourhood with a population of approximately 15,000 people. Bassendean Town Centre is 700 metres to the north where a mix of retail, commercial, recreation and civic uses can be found.

The site is located on the north-west corner of West Road and Bridson Street. The surrounding area is characterised by low density housing of one to two storeys, including traditional homes and contemporary infill housing. Bassendean Primary School is 100 metres to the north within the same street block as the site.

West Road is a local distributor road comprising a single traffic lane in each direction with a footpath on each side of the street. On-street parking bays are provided to the north of the site in the vicinity of the primary school. Bridson Street is also a local distributor road with one traffic lane in each direction and a footpath provided within the verge on the north side of the street. There is a power pole in each of the verge areas abutting the site. While there are no trees in the verge of Bridson Street, the West Road verge contains four mature trees.

The site is serviced by public transport (bus services) with Bus No.45 running in a north-south direction along West Road and in an east-west direction along Hyland Street, with bus stops immediately south of the site.

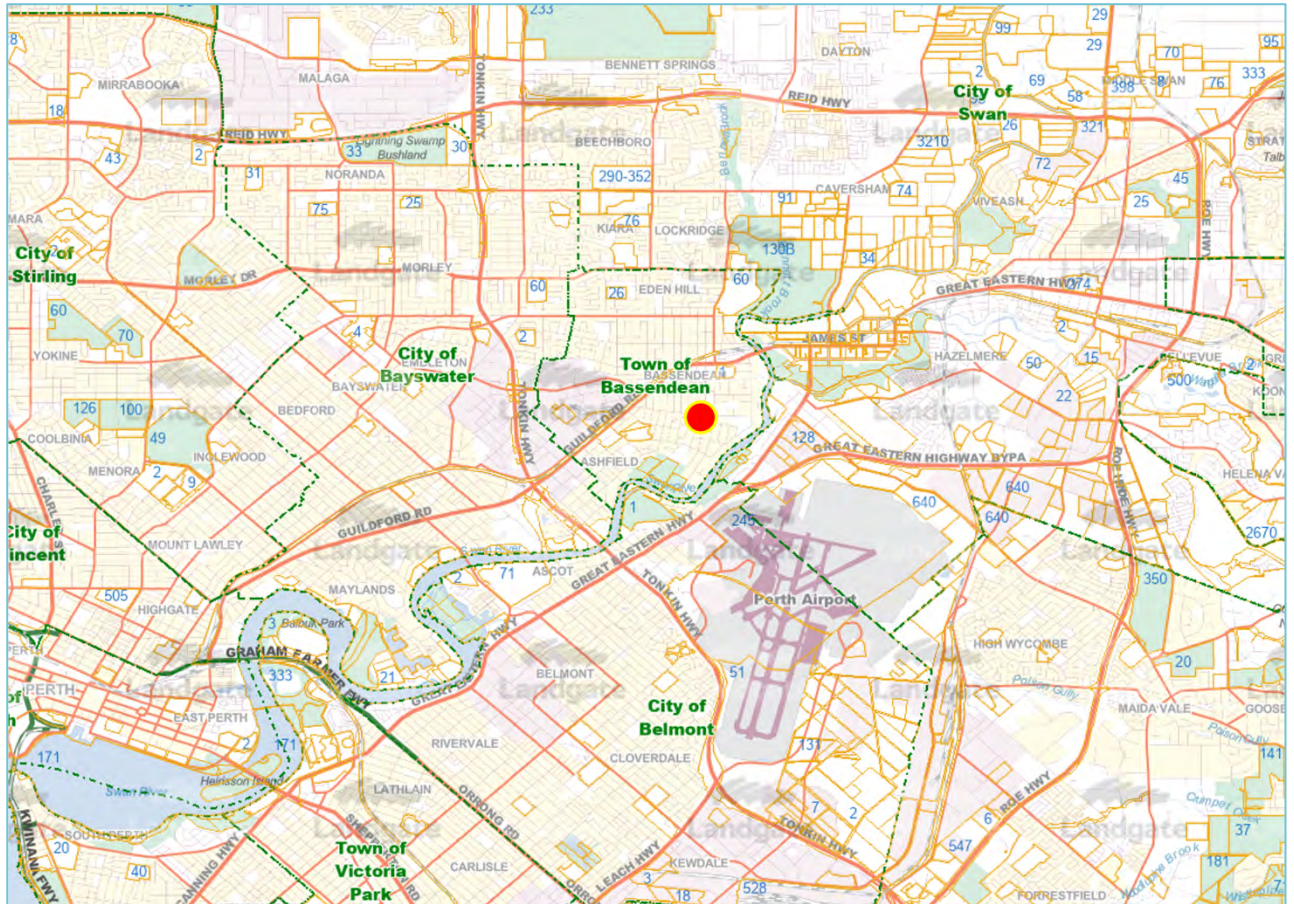


Figure 1: Regional Context

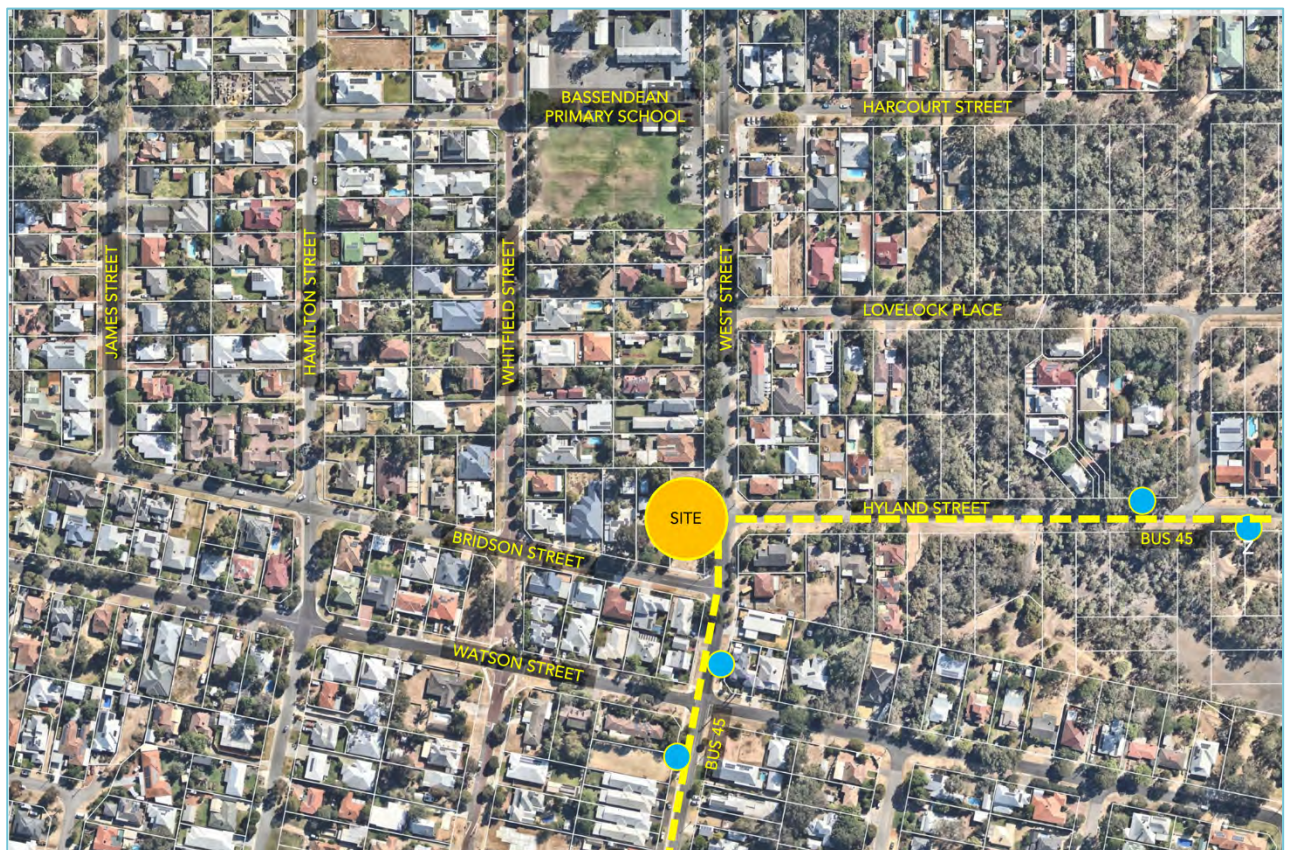


Figure 2: Local Context

2.3 Characteristics

The site has a land area of 1,258m², with frontages of 63.5 metres to West Road and 21.1 metres to Bridson Street.

The site was previously occupied by a single storey dwelling constructed in the 1950's, with access from a crossover off West Road opposite the intersection with Hyland Street. The dwelling was demolished in early 2024 to make way for the proposed development.

The site is relatively flat with a level of approximately 6.3 metres AHD. The Perth Groundwater Map indicates a (maximum) groundwater level of 3.37 metres AHD in the vicinity of the site, being a depth of approximately 2.95 metres below ground level. The surface geology is described as 'Guildford Clay' with a moderate to low risk of acid sulfate soils. There is no vegetation on the site. The four large trees in the West Road verge are proposed to be retained.



Figure 3: Site and Surrounds

3.0 Description of Proposed Development

Item	Proposed			
Description of Development	Child Care Centre			
External Play Area	Total External Play Area		561m ²	
Activity Rooms	Room	Age (Years)	Area	Places
	1	0 to 2	39m ²	12
	2	4 to 5	93m ²	28
	3	2 to 3	65m ²	20
	4	3 to 4	68m ²	20
	Total		265m²	80
Staff	Educators		12	
	Centre Manager		1	
	Total		13	
Operating Times	Trading Days		Monday to Friday	
	Staff Arrival / Departure		6.30am to 7.00pm	
	Customer Arrival / Departure		7.00am to 6.30pm	
	External Play Areas		7.00am to 6.00pm	
Parking	Car Bays		16	
	Bike Bays		4	
Trees	Trees Removed - Verge		0	
	Trees Retained - Verge		4	
	Trees Removed - Site		0	
	Trees Retained - Site		0	
	Trees Proposed - Site		5	

Table 3: Summary of Proposed Development

4.0 Town Planning Considerations

4.1 Metropolitan Region Scheme

The site is zoned Urban under the Metropolitan Region Scheme ('MRS'). No portion of the site is reserved under the MRS.

4.2 State Planning Policies

4.2.1 State Planning Policy 7.0 – Design of the Built Environment

State Planning Policy 7.0 – Design of the Built Environment ('SPP7.0') seeks to achieve 'good design' through the application of ten (10) Design Principles.

The proposed development was considered by the City's Design Review Panel ('DRP') on 9 February 2024 to ensure the design responds appropriately to the Design Principles of SPP7.0.

In its summation of the proposed design, the DRP stated that:

The proposal is appropriate in terms of height, bulk and scale; and sympathetic to the streetscape, context and character of the area.

With respect to Design Principle 1 – Context and Character, the DRP observed that:

The design direction of the proposal contributes to the local area in using a traditional and character driven solution. The proposal is appropriate in terms of height, bulk and scale; and sympathetic to the streetscape and residential context and character of the area

With respect to Design Principle 9 – Community, the DRP advised as follows:

The Panel commended the applicants on their vision for the use of the site as a child care centre, and the contributions this use will bring in respect to economic and social benefits to the community.

The DRP's assessment is summarised in the table below. The design of the development has been amended in response to the DRP comments, as

Design Principle	DRP 9/02/2024	Design Principle	DRP 9/02/2024
Principle 1 - Context and Character		Principle 6 - Amenity	
Principle 2 - Landscape Quality		Principle 7 - Legibility	
Principle 3 - Built Form and Scale		Principle 8 - Safety	
Principle 4 - Functionality & Build Quality		Principle 9 - Community	
Principle 5 - Sustainability		Principle 10 - Aesthetics	

Table 4: Design Review Panel Assessment (9 February 2024)

4.3 City of Bassendean Local Planning Scheme No.11

4.3.1 Zoning

The site is zoned 'Residential' under Local Planning Scheme No.11 ('LPS11'). West Road and Bridson Street are identified as 'Local Distributor Road' reserves under LPS11. A residential density code of R20 applies to the site.

Land to the north, south, east and west is also zoned 'Residential' with a density code of R20. Bassendean Primary School to the north is within a Local Reserve for Public Purposes (Education).

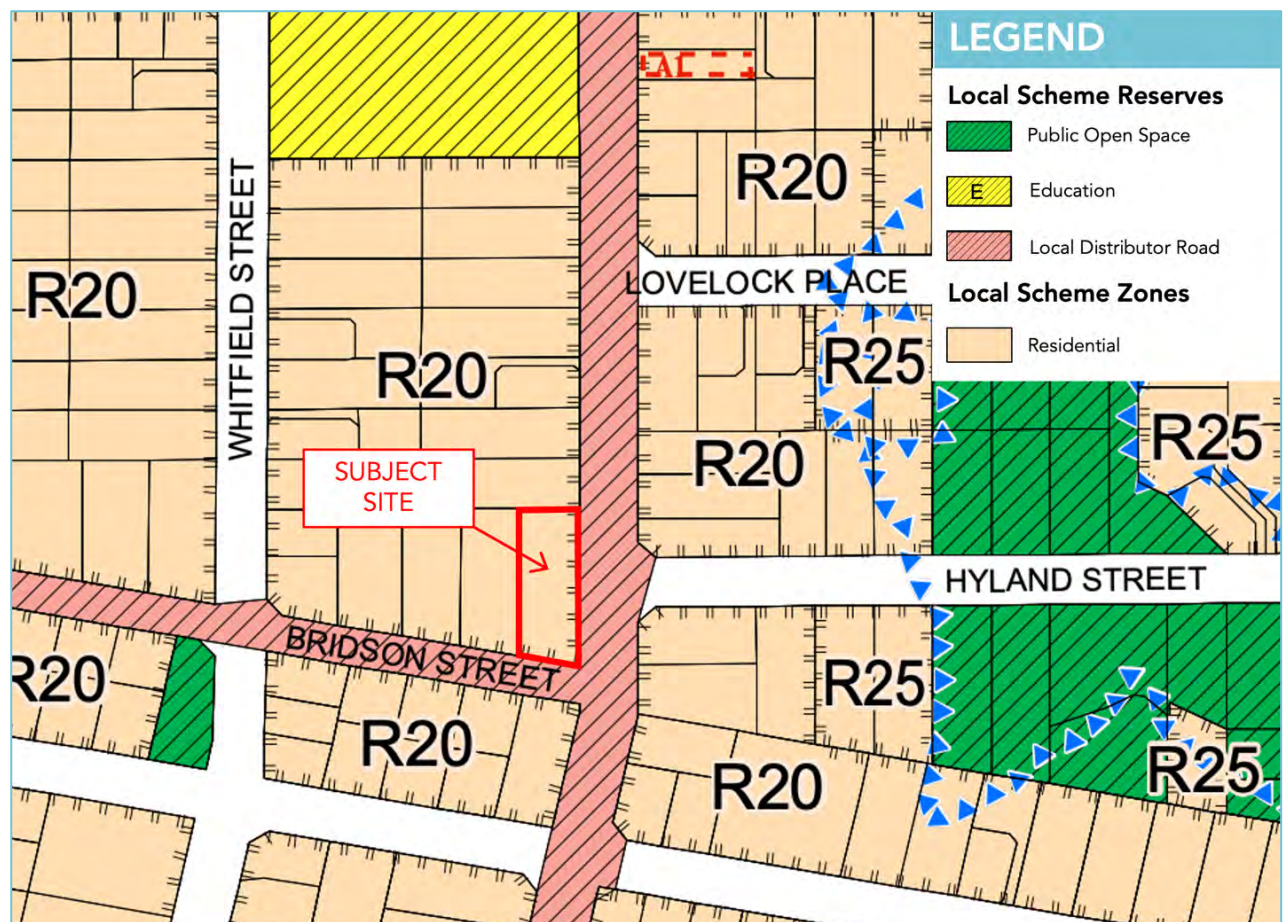


Figure 4: LPS11 Zoning Map

4.3.2 Land Use

The proposed use falls within the definition of a 'Child Care Premises' under LPS11, which is designated as a 'A' use in the 'Residential' Zone. Clause 18 (2) of LPS11 states the following with respect to an 'A' use:

- (a) Means that the use is not permitted unless the local government has exercised its discretion by granting development approval after advertising the application in accordance with clause 64 of the deemed provisions.

The proposed use is therefore capable of being approved at the discretion of the decision-maker, after advertising and having regard to all relevant planning considerations.

4.3.3 Special Control Areas

The site is not within any Special Control Areas under LPS11.

4.3.4 Development Requirements

Clause 32 (including Table 5) of LPS11 states all non-residential development in the Residential zone shall comply with the provisions of the Residential Design Codes (RD Codes') with respect to setbacks, building height and open space, unless otherwise specified in a Precinct Structure Plan or Local Planning Policy. Local Planning Policy 12 – Residential Development and Fences ('LPP12') sets out setback variations to the RD Codes. The site is not subject to a Precinct Structure Plan. The table below summarises the extent to which the proposed development satisfies the Deemed-to-Comply provisions of the RD Codes with respect to setbacks, building height and open space for land coded R20.

Standard	Required	Proposed
Primary Street Setback	Building Façade: 6m Setback can be averaged	Building Façade - Ground & 1 st Floor <ul style="list-style-type: none"> Minimum: 4.7m to 6.2m Average: >6m
	Verandah: 3m	Verandah: 4.3m
	Entry Portico: 3m	Entry Portico: Nil
Secondary Street Setback	Building: 1.5m	Building: 1.8m to 4.2m
	Verandah / Patio: Nil LPP12 states a verandah may have a nil setback to a secondary street provided that: <ul style="list-style-type: none"> It is not more than 10m in length; It is not more than 2.7m in height; It is behind the primary street setback; Eaves, gutters & roofs are setback 0.45m. 	Verandah / Patio: Nil The verandah / patio fronting Bridson Street exceeds a length of 10m.
Side Setback - West Boundary	<u>Ground Floor</u> <ul style="list-style-type: none"> Kitchen 1.1m Activity Rooms 1.5m 	<u>Ground Floor</u> <ul style="list-style-type: none"> Kitchen 1.03m Activity Rooms 3m to 3.8m
	<u>First Floor</u> <ul style="list-style-type: none"> Laundry / Store: 1.2m External Play Area: 1.8m Office / Staff Planning: 3.3m 	<u>First Floor</u> <ul style="list-style-type: none"> Laundry / Store: 1.7m to 1.8m External Play Area: 1.5m to 1.7m Office / Staff Planning: 3.8m to 5.7m
	<u>Boundary Walls</u> Permitted to a maximum height of 3.5m and one-third the length of the boundary behind the street setback (permitted length 19.2m)	<u>Boundary Walls</u> Height: 2.1m to 4.7m Length: 26m
Rear Setback – North Boundary	External Play Area: 2m	External Play Area: 5.1m - 5.6m

Open Space	Open Space: 50%	Open Space: 50% Includes portion of 1 st Floor External Play Area above Ground Floor entry / reception, but excludes the portion above the car park. If the entire 1 st Floor External Play Area is included, Open Space exceeds 65%.
Building Height	Wall Height: 7m Roof Height: 10m	Wall Height: 6.8m Total Height: 9.5m

Table 5: Residential Design Codes Assessment

Primary Street Setback

The variation to the Deemed-to-Comply primary street setback requirement is limited to the entry portico on West Road. If treated like a verandah / portico, a 3 metre setback is required, whereas a nil setback is proposed. The proposed portico is a single storey structure with a pitched roof that will emphasise the pedestrian entry to the premises. The portico has a length of only 3.6 metres, being only 5% of the primary street frontage length, and will not detract from the established streetscape character of the locality.

Secondary Street Setback

In accordance with Alternate Deemed-to-Comply C2.2 in Local Planning Policy 12, a verandah, patio or equivalent structure may have a nil setback to a secondary street, provided it is not more than 10 metres in length and 2.7 metres in height, located behind the site's primary street setback, and the eaves, gutters and roof are setback 0.45 metres (from a side / rear boundary).

In this instance, the building is provided with a verandah / equivalent structure fronting Bridson Street, which has a length of 13 metres and a height of 2.7 metres (measured to the lowest point of roof). The structure has a setback ranging from 0 to 2.8 metres, while a portion of the 1st Floor gable roof is setback 1.1 metres (in lieu of 1.5 metres).

The proposed variations to the secondary street setback are minor and will not detract from the streetscape and amenity of the locality. The building is designed to address Bridson Street in a traditional manner, consistent with the character of the locality, and the verandah will provide shade to the external play area. A previously proposed side (west) boundary wall to the kitchen has also been removed to reduce the visual impact of the building on the streetscape.

Side Boundary Setback

Variations to the side (west) boundary setback are limited to the following.

- Ground Floor – Kitchen (1.1m required; 1.03m proposed); and
- First Floor – External Play Area (1.8m required; 1.5m to 1.7m proposed)

The setback for the external play area wall has been calculated independently to the laundry / store wall. If calculated as one wall, the required setback is 2.3 metres for the external play area (height <5 metres) and 3.3 metres for the laundry / store (height < 7 metres).

Boundary Wall

- Height of 3.5m and length of 19.2m permitted;
- Height of 2.1m to 4.7m and length of 26m proposed.

The setback variations to the west boundary are relatively minor. The variation to the kitchen wall is only 7 centimetres and will not have any noticeable visual or functional impact. The kitchen wall is also screened by established landscaping in the front garden of the adjacent property.

With respect to the boundary wall, the portion that exceeds a height of 3.5 metres is limited to a length of only 2.8 metres, being the portion screening the upper-most part of the escape stair. The portion of the boundary wall exceeding 19.2 metres in length is limited to the wall to the car park, which has a height of only 2.1 metres, which is similar to a standard boundary fence (noting the height of 2.1 metres is consistent with the recommendations in the Acoustic Assessment). The boundary wall will not have an excessive bulk when viewed from the adjacent property.

The setback variation to the External Play Area will not result in any loss of privacy, daylight or ventilation for the adjoining property to the west. The proposed walls do not have any openings and will not result in any loss of privacy for the adjoining residents. The setbacks improve the functionality of the Child Care Premises without any adverse impacts on the amenity of the area.

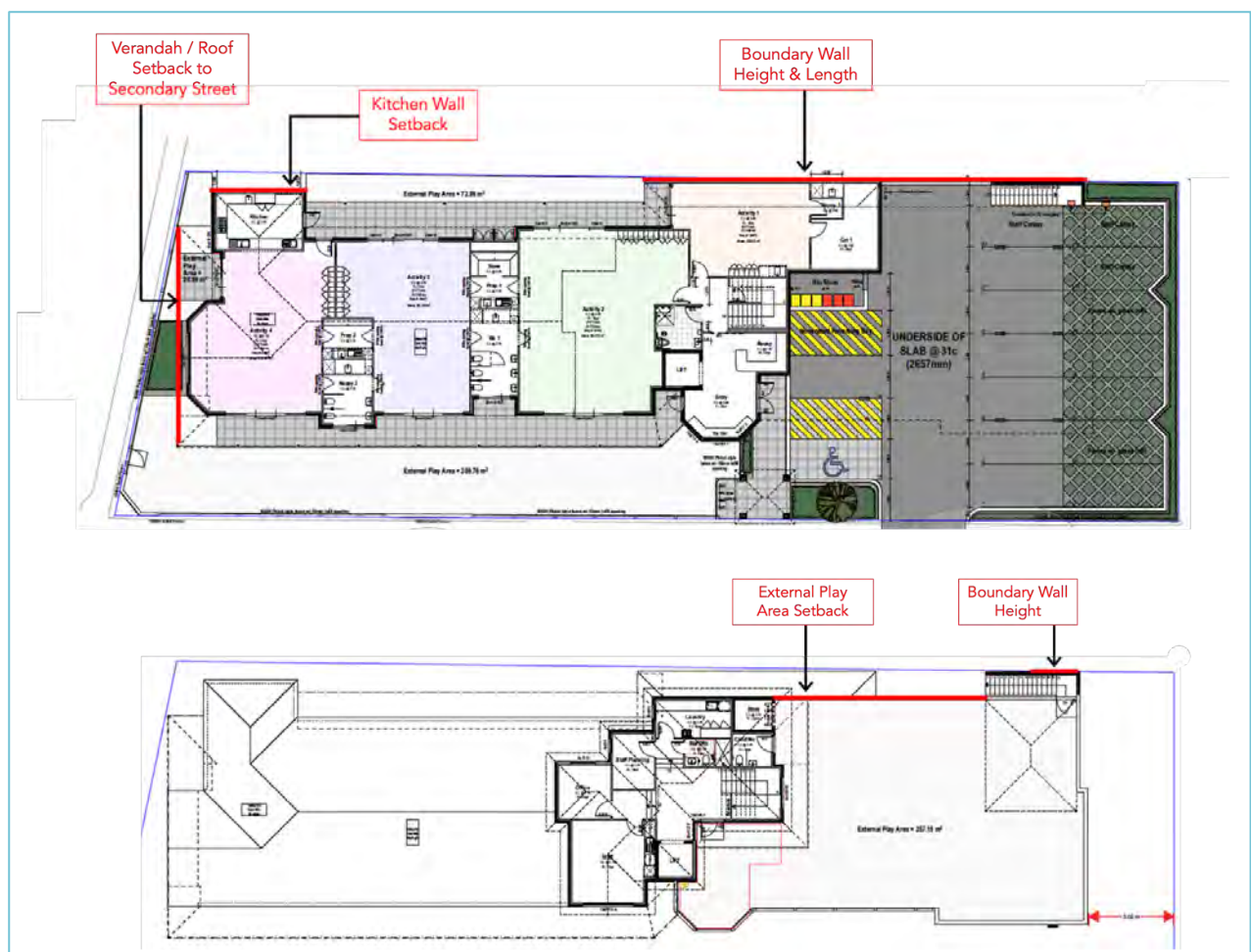


Figure 5: Setback Variations

4.3.5 Local Planning Policies

Local Planning Policies adopted pursuant to LPS11 that may be relevant to the consideration of this Application include:

- Local Planning Policy 8 – Car Parking and End of Trip Facilities ('LPP8');
- Local Planning Policy 12 – Residential Development and Fences ('LPP12');
- Local Planning Policy 15 – Public Art ('LPP15'); and
- Local Planning Policy 16 – Advertising and Signage ('LPP16').

4.3.5.1 Local Planning Policy 8 – Car Parking and End of Trip Facilities

LPP8 sets out car and bicycle parking standards for non-residential throughout LPS11.

Policy Statement

The LPP8 Policy Statement reads:

The Town recognises the need to ensure adequate car parking is provided to support land uses. The balance between providing too much or too little car parking has implications on affecting the areas character, amenity and vibrancy as well as the feasibility and affordability of the use and development of land.

This policy seeks to provide the framework in which car parking ratios are to be applied, incentives to reduce car parking (where appropriate) and the provision of end of trip facilities. This Policy shall be used to guide decision making on applications which involve variations to the minimum car parking standards required in Local Planning Scheme No.10.

Policy Objectives

The Objectives of LPP8 are:

- a) *To stipulate the minimum number of parking and end of trip facilities to be provided to support development, as well as the circumstances in which the Town will consider a reduction in parking bays.*
- b) *To establish the requirements for the design and access of parking facilities.*
- c) *To balance the provision of sufficient on-site car parking with the need to encourage pedestrians and promote alternative transport methods.*

Due consideration has been given to the Policy Statement and Objectives with respect to the provision of parking for the proposed Child Care Premises.

Car Parking

Car Parking Requirement

The minimum car parking requirement applicable to a Child Care Premises is set out in Table 1 of LPP8, as summarised below.

Standard	Requirement	Response
Car Parking	Staff Car Parking:	Total Car Bays Provided 16
	• 1 bay / staff member (13) 13	As per the Operational Management Plan, car parking bays will be allocated as follows:
	Customer Car Parking:	
	• 1 bay / 10 places (80) 8	• Staff Only Bays: 8
	Total Required: 21	• Additional Staff / Visitor Bays (non-peak times): 7 • Customer Only Bays (peak times): 7 • ACROD Bay (all times): 1

Table 6: Car Parking Summary

Variations to Car Parking Requirement

A total of 21 car bays are required, whereas the Application proposes 16 car bays, resulting in a shortfall of 5 car bays. Notwithstanding, Clause 6.1.2 of LPP8 contemplates the ability for the car parking requirement to be varied. Clause 6.1.2 reads:

The car parking requirements applicable under Table 1 may be varied, having regard to:

- The nature of the proposed development;*
- The number of employees likely to be employed on site;*
- The anticipated demand for parking; and*
- The orderly and proper planning of the locality.*

Variations will only be supported where it will not adversely affect access arrangements, the safety of pedestrians or persons in vehicles, open bay, street trees or service infrastructure and the number of car parking bays to be provided will be adequate for the demands of the development, having regard to the likely use of the car parking bays, the availability of off-site parking facilities and the likely use of alternative means of transport.

The amount of parking provided is sufficient to meet parking demand from customers, staff and other visitors throughout the day, due to the different peak operating times of each user group. This allows car bays to be allocated to specific user groups at different times of the day to meet demand.

Customer Parking

A total of seven (7) car bays will be set aside exclusively for customer use during the morning drop-off peak and afternoon pick-up peak.

As set out in the Traffic Impact Statement, assuming a conservative 10-minute average length of stay, each car bay can accommodate 6 separate drop-offs per hour, or 12 drop-offs over two hours. With 7 dedicated customer car bays during peak times, a total of 84 individual car drop-offs / pick-ups can be accommodated over a two-hour peak period, which is more than the maximum capacity of the Child Care Premises.

The Traffic Impact Statement anticipates the AM peak hour of the premises will be 7:30am to 8:30am when 40.55% of customers (32) will arrive, with the PM peak hour being 4.30pm to 5.30pm when 45.21% of customers (36) will depart. With each customer car bay accommodating 6 separate drop-offs per hour, the provision of 7 dedicated customer bays is sufficient to meet anticipated demand during the peak hours.

It is important to note the Traffic Impact Statement assumes full occupancy with all children being driven to the premises in separate vehicles. In practice, the premises will rarely (if ever) operate at full capacity due to day-to-day enrolments, absentees, and market conditions. In addition, not all customers arrive by car and many children will have siblings at the premises, further reducing the number of individual car trips. In addition, the site's proximity to Bassendean Primary School will further reduce the number of individual car trips, as parents with children at both the primary school and child care premises will likely only make one trip to the locality (i.e. they may park at or near the school and walk the short distance to / from the child care premises).

The provision of 7 dedicated customer bays during the morning and afternoon peak is therefore adequate to meet parking demand from the proposed development.

Staff Parking

The number of staff at the premises on any given day will vary depending on enrolments in each age group, with staff work times staggered to meet the needs of the centre throughout the day. Two (2) staff will typically be rostered to open the premises and commence work by 7am. No staff would arrive prior to 6.30am. During a typical day (for a full occupancy scenario), it is expected that up to 8 staff would be rostered to commence work prior to 8.30am, increasing to a maximum of 13 staff during the day. Staff levels will gradually decrease as shifts finish and children are collected. No more than 6 to 8 staff would typically be working at the commencement of the afternoon peak (4.30pm), with 2 staff typically rostered to close the premises at 6.30pm. All staff would depart by 7pm latest.

A total of 8 car bays will be set aside exclusively for staff use throughout the entire day, with an additional 7 car bays available for additional staff / visitor parking between 9am and 4.30pm when customer parking demand is low. A total of 13 car bays will therefore be available for staff parking when the highest number of staff (13) will be present on the site.

The provision of staff parking assumes 100% occupancy of the premises, which is unlikely to occur. Furthermore, not all staff will drive in separate cars, and some staff will arrive by means other than driving (walking, cycling or public transport).

Other Visitors & Service Vehicles

Other visitors to the premises include service vehicles, suppliers and potential clients who may wish to meet with staff and / or inspect the centre. Such visitations are infrequent and will be scheduled to occur during the day (between 10.30am and 3.30pm) when use of the car park is at its lowest. A minimum of two (2) car bays will be available for visitor parking during this time.

ACROD Parking

One (1) car bay will be available for ACROD permit parking, which may include customers, staff and other visitors.

Design of Car Park

The design of the car park complies with AS2890.1. In accordance with LPP8, two staff car bays are provided with an electric vehicle charge point and all of the tandem bays are allocated to long-term staff parking for the child care centre, which is the only tenancy on the site.

Operational Management Plan

The Operational Management Plan accompanying the Application demonstrates how the car parking bays on the site will be allocated to meet the different peak parking demand times of each user group, as outlined above and depicted in the figure below.

This approach is consistent with the intent of Clause 6.3 (Shared Parking) of LPP8, which makes provision for the joint use of car parking facilities. Clause 6.3.1 states:

1. *The Town may consider varying the minimum car parking standards outlined in Table 1 where shared parking arrangements are proposed. In determining whether reciprocal parking is appropriate, the Town will consider:*
 - a) *Whether the peak hours of operation of each site are suitable for a shared arrangement;*
 - b) *Whether a shared arrangement will impede the use of delivery or service areas on the shared site;*
 - c) *Whether adequate car parking is likely to be available at all times for both sites;*
 - d) *Whether the parking facilities are conveniently located to both developments;*
 - e) *The relationship between the proposed development and the shared such will be such that the shared car parking bays are likely to be used by persons using the proposed development; and*
 - f) *The parking facilities serving the uses will be located on the one lot, or if located on a separate lot, the parking arrangements are permanent (e.g. through an easement, amalgamation, legal agreement, condition of approval, or any other formal arrangement acceptable to the Town.*

While Clause 6.3.1 applies to the reciprocal use of parking facilities by land uses on different properties in a locality, the **shared parking principle** contemplated by Clause 6.3.1 is relevant to the proposed development and can be applied in a similar way through implementation of the Operational Management Plan.

With the deployment of these operational measures, sufficient car parking will be available to meet demand from different users of the child care centre throughout the day. The site's proximity to public transport, together with the provision of bike parking and end-of-trip facilities, will further reduce private car usage and encourage alternative modes of transport.

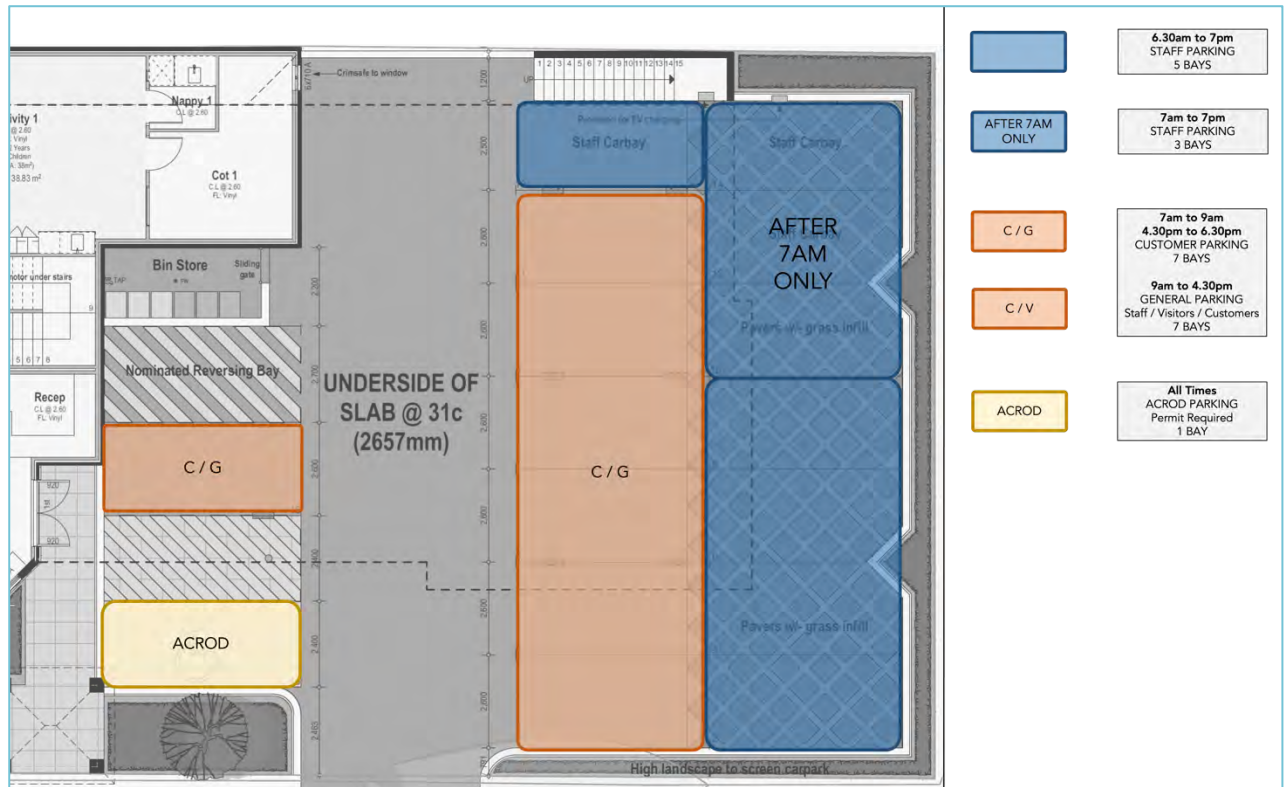


Figure 6: Car Park Allocation Plan

Part 9A of Deemed Provisions

Part 9A of the Deemed Provisions deals with car parking. Deemed Provision 77D (2) allows the City to exercise discretion and vary a minimum on-site parking requirement provided the City is satisfied with respect to the following:

- (a) that reasonable efforts have been made to comply with the minimum on-site parking requirement without adversely affecting access arrangements, the safety of pedestrians or persons in vehicles, open space, street trees or service infrastructure; and
- (b) that... the lower number of car parking spaces would be adequate for the demands of the development, having regard to the likely use of the car parking spaces, the availability of off-site parking facilities and the likely use of alternative means of transport.

With respect to Sub-Clause (a) of Deemed Provision 77D (2):

- All reasonable efforts have been made to comply with the on-site parking requirement without adversely affecting access, safety, open space, verge trees and infrastructure;
- The design of the car park complies with AS2890.1;
- No verge trees or infrastructure are affected by the proposed development. The car park is designed to ensure the retention of the established verge trees in West Road. The verge trees restrict the options available for the crossover location and, as a consequence, it is necessary to provide the majority of car bays in a tandem arrangement;
- A separate pedestrian path will provide a safe means of access to the premises; and
- The design allows all vehicles using the car parking bays to enter and exit in forward gear.

With respect to Sub-Clause (b) of Deemed Provision 77D (2):

- The proposed number of car bays is adequate to meet demand from customers, staff and other visitors throughout the day, as set out in the Operational Management Plan;
- The Traffic Impact Statement concludes that sufficient car parking is provided on-site to meet anticipated demand. The Traffic Impact Statement estimates the peak parking demand of the child care centre will occur between 7.30am and 8.30am when 40.55% of attendees (32) are expected to arrive, and between 4.30pm and 5.30pm when 45.21% of attendees (36) will depart, equating to a peak parking demand of 6 customer car bays. Outside of peak times, all staff will have access to a car bay;
- The provision of bike bays and end-of-trip facilities, the availability of a bus service, and the site's proximity to Bassendean Primary School and on-street parking in West Road, will further reduce demand for car parking within the site;
- The provision of 16 car bays for an 80-place child care centre equates to 1 car bay for every 5 places. This is the same minimum rate recommended by the Department of Planning, Lands and Heritage in its Draft Position Statement for Child Care Premises (November 2022); and
- The provision of 1 car bay for every 5 places equates to the average rate of parking provided at 10 other recently approved child care centres in the metropolitan area, consistent with the intent of the Draft Position Statement.

For all of these reasons, it is considered sufficient car parking is provided on-site to meet demand and the proposed parking shortfall will not have an adverse impact on the amenity of the locality.

Bicycle Parking and End-of-Trip Facilities

Whilst LPP8 does not specify a bicycle parking requirement for child care premises, it is proposed to provide 4 visitor bike racks near the entry to the Child Care Premises. As no bicycle bays are required, there is no requirement to provided end-of-trip facilities. Despite this, the First Floor staff bathroom includes a shower / change facility and lockers will be provided in the staff room.

4.3.5.2 Local Planning Policy 12 – Residential Development and Fences

LPP12 varies some of the Deemed-to-Comply provisions of the RD Codes, as discussed above.

4.3.5.3 Local Planning Policy 15 – Public Art

LPP3 requires non-residential development with an estimated cost exceeding \$2 million to provide public art to the value of 1% of the construction cost. The estimated construction cost of the development is \$3 million meaning public art to the value of \$30,000 is required. Details of the public art strategy can be provided in accordance with a condition of Development Approval.

4.3.5.4 Local Planning Policy 16 – Advertising and Signage

LPP16 provides guidance for advertising signs. A Signage Strategy can be submitted pursuant to a condition of Development Approval. In accordance with LPP16, certain signs are exempt from the requirement to obtain Development Approval. A separate Development Application will be submitted for any proposed signage that does not meet the exemption criteria under LPP16.

4.3.6 Built Form and Character Study

The Built Form and Character Study was commissioned to inform the review of the Town's Local Planning Strategy and is appended to the Town's endorsed Local Planning Strategy (2023). The site is located in Character Area 22 – Devon Road of the Built Form and Character Study. The Preferred Character Statement for Character Area 22 is shown below.

PREFERRED CHARACTER STATEMENT		
<p>To maintain the current character of the area while supporting contemporary design and future development that is sympathetic of the Character Area and reflective of the Town of Bassendean's Core Character.</p> <p>This will be achieved by:</p> <ul style="list-style-type: none"> Encouraging the retention of large trees and formalised front gardens. Ensure that the single and two storey scale of dwellings is retained. Introducing a consistent street tree planting pattern. 	<ul style="list-style-type: none"> Maximising areas of soft landscaping and discouraging large areas of hard landscaping. Ensuring that new development and additions respect the predominant scale of the Character Area. Encouraging visually permeable front fencing or perimeter plantings. 	<ul style="list-style-type: none"> Avoiding car parking structures that dominate the streetscape and dwelling. Encouraging design that integrates with the Federation and Inter-War dwelling styles of the Character Area.

Figure 7: Built Form and Character Study – Precinct 22 Preferred Character Statement

In assessing the proposed development against Design Principle 1 – Context and Character of SPP7.0, the Town's DRP observed as follows:

The design direction of the proposal contributes to the local area in using a traditional and character driven solution. The proposal is appropriate in terms of height, bulk and scale; and sympathetic to the streetscape and residential context and character of the area

Character Element	Design Response
Lot Frontage	<ul style="list-style-type: none"> Existing lot width / configuration maintained.
Vegetation	<ul style="list-style-type: none"> Verge trees retained and soft landscaping proposed to street setback areas.
Siting	<ul style="list-style-type: none"> Setbacks to street frontages with minimal side boundary walls. Building addresses both street frontages.
Building Height & Form	<ul style="list-style-type: none"> 1 to 2 storeys, with upper floor at rear to reduce bulk on Bridson St.
Building Materials & Design	<ul style="list-style-type: none"> Traditional materials - weatherboard cladding with pitched colorbond roof. Materials reflect Core Character Elements. Residential look to emulate the desired character style.
Front Boundary Treatment	<ul style="list-style-type: none"> Perimeter planting – cottage garden design. Visually permeable picket fence.
Car Parking	<ul style="list-style-type: none"> Car bays at rear of site, screened behind verge trees

Table 7: Response to Built Form and Character Study – Precinct 22 Preferred Character Statement

4.3.7 Deemed Provisions

Deemed Provision 67 sets out the various matters that a decision-maker is required to consider in determining this Application. The table below explains how the Application addresses each of the relevant matters listed in Deemed Provision 67.

Deemed Provision 67		Response
(a)	Local Planning Scheme	The Application is capable of approval under LPS11.
(b)	Orderly and proper planning	The use is consistent with the orderly and proper planning of the locality. There are no draft planning proposals relevant to the Application.
(c)	State Planning Policies	State Planning Policy 7.0 – Design of the Built Environment.
(d)	Environmental Protection Policies	Not applicable.
(e)	Any policy of the WAPC	WAPC Draft Position Statement - Child Care Premises.
(f)	Any policy of the State	Not applicable.
(g)	Local Planning Policies	Local Planning Policy 8 – Car Parking and End of Trip Facilities; Local Planning Policy 12 - Residential Development and Fences; Local Planning Policy 15 – Public Art; and Local Planning Policy 16 – Advertising and Signage.
(h)	Structure Plans, Centre Plans and Local Development Plans	Not applicable
(i)	Review of Local Planning Scheme	Not applicable
(j)	Reserved land	Not applicable
(k)	Built heritage conservation of any place of cultural significance	The development does not have an adverse impact on the built heritage conservation of any place of cultural significance.
(l)	Cultural heritage significance	The site is not within an area of cultural heritage significance. The development will not have any effect on a known site or place of Aboriginal heritage significance.
(m)	Compatibility with setting	The development is compatible with its setting, being a low impact non-residential use within the residential zone.
(n)	Amenity of the locality:	
	(i) Environmental impacts	The proposal will not have any adverse impact on the environment.
	(ii) Character of locality	The design of the development is compatible with the character of the area.
	(iii) Social impacts	The development will not have any adverse social impacts.
(o)	Effect on natural environment	The development will not have an adverse effect on the natural environment.
(p)	Landscaping and tree retention	Landscaping, including tree retention and tree planting, is proposed.
(q)	Environmental risks	None
(r)	Risk to human health or safety	None
(s)	Access and parking	Parking for 16 cars is provided with access from West Road. The Operational Management Plan demonstrates how the car bays will be allocated during the day to meet peak demand from different user groups.

(t)	Traffic impacts	The traffic generated by the development will not have an adverse effect on traffic flow and safety. Refer to Traffic Impact Statement.
(u)	(i) Public Transport	Bus 45 runs along West Road, between Bayswater and Bassendean Train Stations. Bassendean Station on the Perth-Midland Line is 1km to the north of the site.
	(ii) Public Utilities	All utilities required to service the development are available, including water, sewer, and power.
	(iii) Waste Management	A bin store is proposed of sufficient capacity to service the development. Refer Operational Management Plan.
	(iv) Pedestrian & Cyclist Access	4 visitor bike bays are proposed near the entry to the child care premises. End-of-trip facilities will be provided for staff. A pedestrian path connects the building entry to the car park and footpath.
	(v) Elderly & Disability Access	One 'ACROD' bay is provided.
(v)	Loss of community benefit or service	<p>The Application will not result in any loss of a community service.</p> <p>The Child Care Premises will benefit the local community by providing improved access to child care services in the area.</p> <p>A study by Victoria University (<i>Deserts and Oases: How Accessible is Childcare in Australia? March 2022</i>) found that between 2.78 and 6.17 children in Bassendean compete for each child care place, compared to a nation-wide median of 2.6 children per place. Out of 23 neighbourhoods in Bassendean, 18 were classified by the study as "childcare deserts".</p> <p>In response to Design Principle 9 – Community of SPP7.0, the Town's Design Review Panel advised:</p> <p><i>The Panel commended the applicants on their vision for the use of the site as a child care centre, and the contributions this use will bring in respect to economic and social benefits to the community.</i></p>
(w)	History of the site	No relevant site history.
(x)	Impact on the community	<p>It is not considered the development will have an adverse community impact.</p> <p>The Child Care Premises will benefit the local community by providing improved access to child care services in the area.</p> <p>The Town's Design Review Panel observed that the use of the site as a child care premises will result in economic and social benefits to the community.</p>
(y)	Submissions on the proposal	To be determined
(za)	Comments from agencies	To be determined
(zb)	Other planning considerations	None

Table 8: Deemed Provisions

5.0 Conclusion

This Town Planning Statement has been prepared in support of an Application for Development Approval for the construction of a Child Care Centre on the land situated at Lot 85 (No.94) West Road, Bassendean ('site').

The site is ideally placed to accommodate a Child Care Centre. The site is zoned Residential, has frontage to two Local Distributor Road, is situated on a corner, and is within walking distance of Bassendean Primary School.

The Child Care Centre is a compatible non-residential use that will provide residents of Bassendean and surrounds with improved and convenient access to child care services.

The Traffic Impact Statement finds that the road network surrounding the site can successfully accommodate additional traffic associated with the development and that sufficient parking is provided on-site to meet the parking demand of the Child Care Centre.

The Acoustic Assessment finds that the Child Care Centre is capable of satisfying applicable noise regulations and will not be affected by excessive levels of traffic noise.

The Operational Management Plan explains how the premises will be operated to mitigate any adverse impacts on the locality with respect to car parking, noise and waste.

Accordingly, the proposed development satisfies the relevant considerations of Deemed Provision 67 of LPS11, is consistent with the principles of orderly and proper planning, and will not have any detrimental impact on the amenity of the locality.

An aerial photograph of a residential neighborhood, overlaid with a semi-transparent blue filter. The image shows several houses with varying roof colors (brown, grey, blue) and green lawns. A large, rectangular, light-colored vacant lot is the central focus. To the right of the lot, a road curves. The text 'Operational Management Plan' is written in white, sans-serif font across the top half of the image.

Operational Management Plan

Child Care Centre

No.94 West Road
Bassendean

LATERAL
PLANNING

Document Control

Reference	0307
Location	Lot 85 (No.94) West Road, Bassendean
Client	Briscola Pty Ltd
Document Title	Operational Management Plan
Document File Name	0307 Operational Management Plan.docx
Document Date	4 November 2024
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Author	Alan Stewart

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1.0 Introduction

This Operational Management Plan ('OMP') has been prepared in support of an Application for Development Approval for a Child Care Centre at Lot 85 (No.94) West Road, Bassendean ('site'). The purpose of the OMP is to document how the Child Care Centre will operate to minimise impacts on the locality.

2.0 Site Description

Local Authority	Town of Bassendean
Locality	Bassendean
Address	No.94 West Road
Cadastral	Lot 85 on Plan 1784
Certificate of Title	Volume 596 Folio 156
Registered Proprietor	Briscola Pty Ltd
Land Area	1,258m ²

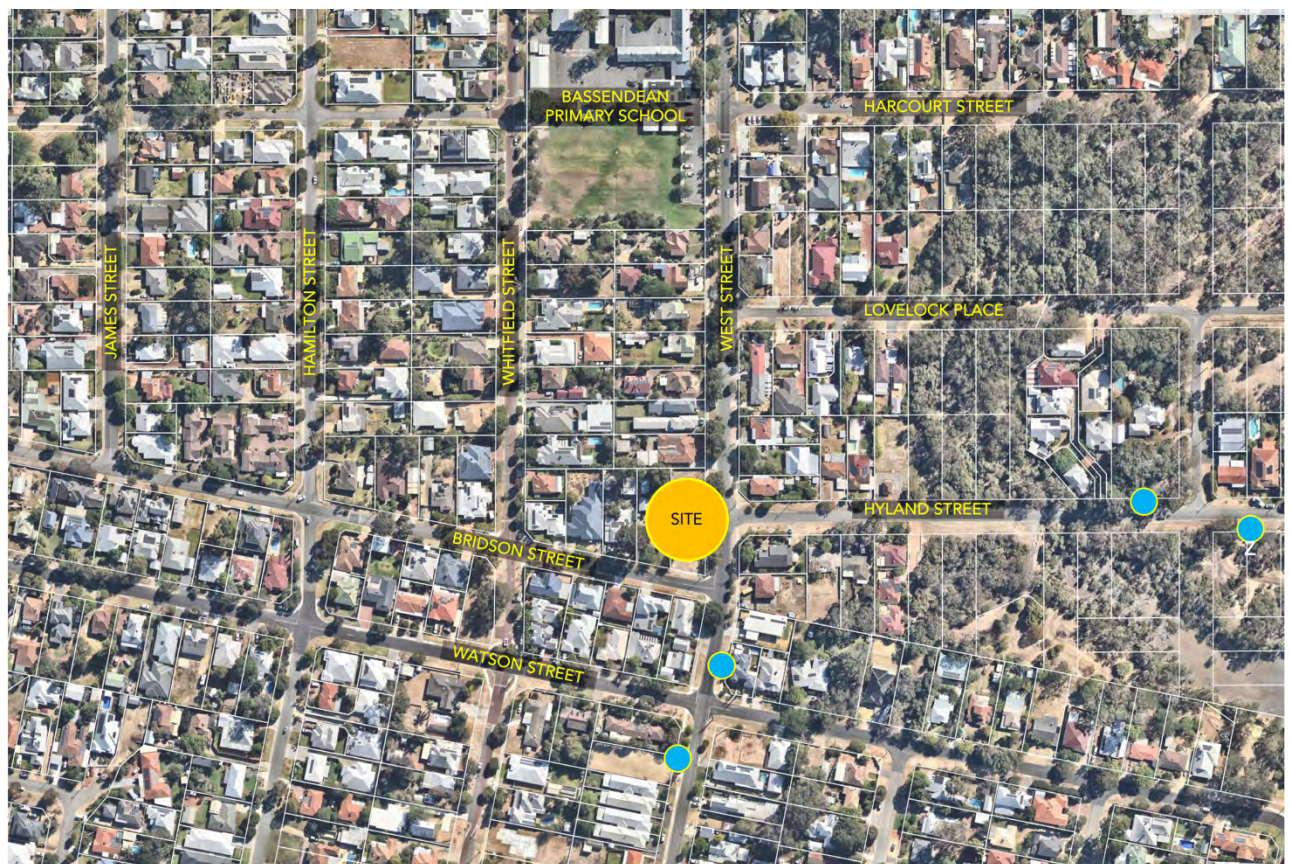


Figure 1: Site Location

3.0 Proposed Development

3.1 Operating Times

Business Days	Monday to Friday
Staff Arrival / Departure	7:00am to 7:00pm
Customer Arrival / Departure	7:15am to 6:30pm
External Play Areas ('EPA')	7:00am to 6:00pm

3.2 Capacity

The table below shows the proposed capacity of the Child Care Centre for each age group, together with the required minimum number of educators.

Activity Room	Age Group	Places	Educators	Indoor Area	External Area
1	0 to 2	12	3	39m ²	73m ²
2	4 to 5	28	3	93m ²	210m ²
3	2 to 3	20	4	65m ²	21m ²
4	3 to 4	20	2	68m ²	257m ²
Total		80	12	265m²	561m²
<p>Required Indoor Area: 3.25m² per child.</p> <p>Required Outdoor Area: 7m² per child.</p> <p>Required Educators:</p> <ul style="list-style-type: none"> 0 to 2 age group: 1 supervisor per 4 children 2 to 3 age group: 1 supervisor per 5 children 3 to 5 age group: 1 supervisor per 10 children 					

3.3 Staff

The maximum number of staff at the Child Care Centre will be 14, as shown below.

Required Educators	12
Additional Educator	1
Centre Manager	1
Total	14
<p>Required Educators (12): This is the minimum number of educators that will be required on those occasions when the Child Care Centre operates at full capacity across all age groups.</p> <p>Additional Educator (1): An additional educator may be employed on those occasions when the Child Care Centre operates at full capacity to provide additional coverage when an Educator is required to leave the floor to undertake other tasks such as food preparation.</p> <p>Centre Manager (1): The Centre Manager is a qualified Educator and can provide cover on the floor, if required.</p>	

Educators

In practice, the number of staff on any given day will vary depending on enrolments in each age group, with staff work times staggered to meet the needs of the centre throughout the day.

Typical staff levels in a full occupancy scenario would be as follows:

- Two (2) staff will be rostered to open the premises and commence work at 7.15am. No staff would arrive prior to 7am.
- A further 8 educators would typically be rostered to commence work during the morning peak (up to 9.30am).
- The highest staffing level occurs between 9.30am and 3.30pm when up to 13 educators could be working.
- Staff levels gradually decrease after 3.30pm, as shifts finish and children are collected. No more than 10 educators would typically be working at the start of the afternoon, decreasing to 6 educators by 5.30pm.
- Two (2) educators will be rostered to close the premises at 6.30pm. All staff would depart by 7pm latest.

Centre Manager

The Centre Manager is a qualified Educator and can provide cover on the floor, if required.

Maximum Staff

The table below shows the proposed maximum number of staff at the premises throughout the day assuming a full occupancy scenario.

STAFF	TIME				
	OPENING	MORNING DROP-OFF	OFF-PEAK	AFTERNOON PICK-UP	CLOSE
	7am to 7.15am	7.15am to 9.30am	9.30am to 3.30pm	3.30pm to 6.30pm	6.30pm to 7.00pm
Educators	2	10	13	10	2
Manager			1	1	
TOTAL	2	10	14	11	2

TYPE	MAXIMUM CAR BAYS REQUIRED DURING THE DAY				
	OPENING	MORNING DROP-OFF	OFF-PEAK	AFTERNOON PICK-UP	CLOSE
	7am to 7.15am	7.15am to 9.30am	9.30am to 3.30pm	3.30pm to 6.30pm	6.30pm to 7.00pm
Staff	2	10	14	11	2
Customers	0	6	2	6	0
TOTAL	2	16	16	17	2

Turning Zone

As the ACROD bay will be used infrequently, the access zone adjacent to the ACROD bay will be available as a turning zone to allow vehicles to turnaround and exit the car park in forward gear in the unlikely event that all car bays are occupied.

Bicycle Parking

A total of 4 bike racks will be provided for use by staff and visitors. The bike bays are located near the front entry, with direct access from the footpath on West Road. Staff will have access to a shower / change facility and secure lockers will be provided in the staff room.

Public Transport

The site is serviced by public transport (bus services). A bus route runs in a north-south direction along West Road and in an east west direction along Hyland Street, with bus stops immediately south of the site, as shown below.

Bus No.	Description	Bus Stops
45	Bayswater Train Station – Bassendean Train Station	West Road Nos.15565 (north) and 15566 (south)

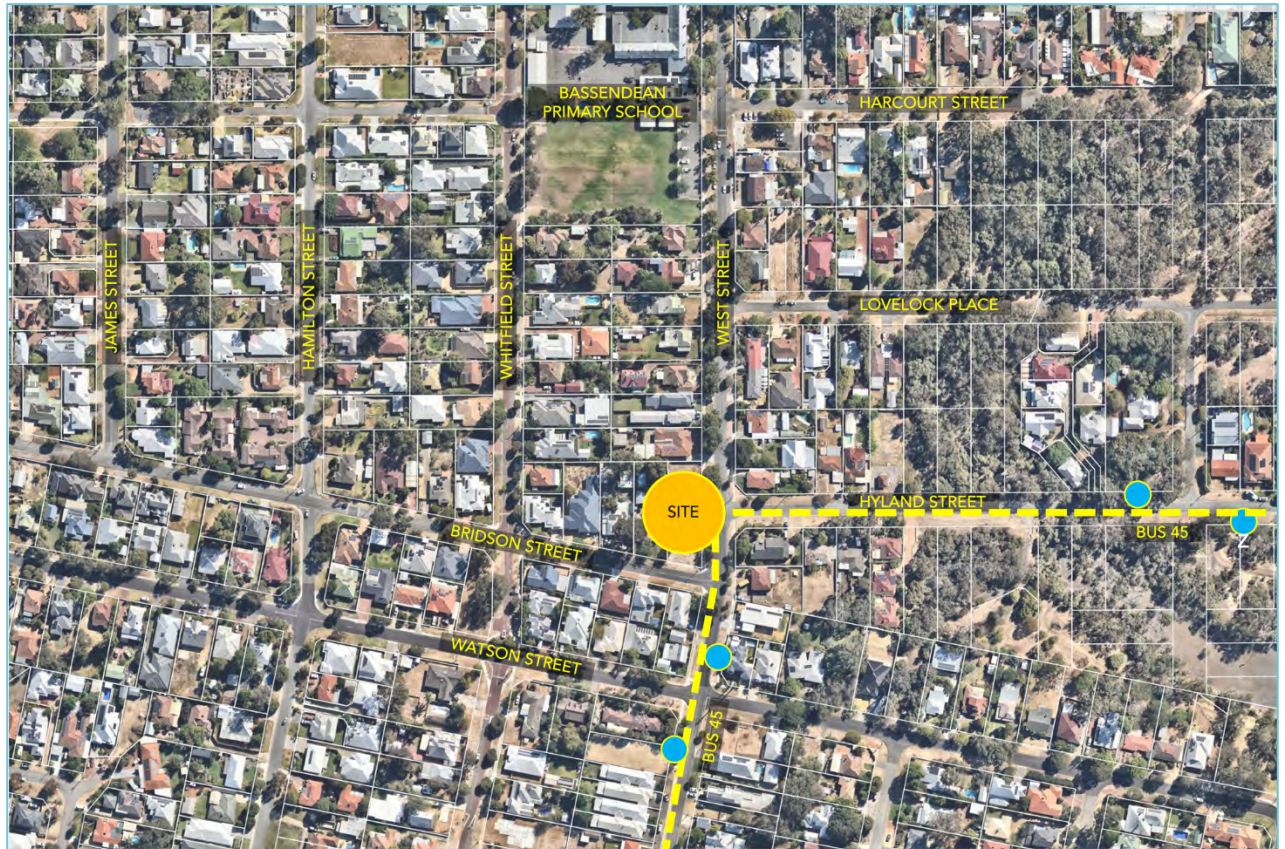


Figure 3: Bus Services

4.2 Traffic and Parking Management

Operational Measures

The following operational measures will be implemented to mitigate any potential adverse impacts associated with traffic and parking.

These measures will be implemented on an on-going basis by Child Care Centre management, pursuant to a condition of Development Approval that gives effect to this OMP.

Operational Measures – Traffic and Parking

Car Park

1. Line-marking and signage will be installed to depict the authorised use of each car bay throughout the day.
2. Staff, customers and other visitors will be advised of the requirement to park in designated car bays only.
3. Customers will be requested not to park or stand their vehicle in any roads surrounding the site and to observe on-street parking restrictions.
4. Staff will be responsible for monitoring use of the car park and customers will be reminded of the car park operation procedures on an as-required / as-needed basis.
5. Management of the Child Care Centre will endeavour to schedule non-urgent visitations and deliveries during non-peak times only (10.30am to 3.30pm), when vehicle movements at the Child Care Centre will typically be at their lowest.
6. All suppliers / contractors will be advised that access to the site during the peak morning drop-off and peak afternoon pick-up times will not be permitted (except for emergency maintenance and / or when unavoidable).
7. The car park security gate will remain open throughout the day.

Public Transport and Cycling

8. Staff will be advised of available bus services and encouraged to use public transport to access the premises.
9. Staff will also be advised of the provision of bike parking and end-of-trip facilities, which include a shower / change room and secure lockers.

5.0 Noise

5.1 Overview

Environmental Protection (Noise) Regulations

The Child Care Centre is required to satisfy the *Environmental Protection (Noise) Regulations 1997* and will incorporate measures to mitigate the impacts of noise on nearby properties.

5.2 Noise Management

The Acoustic Assessment recommends various noise mitigation measures relating to the construction and operation of the premises. The construction measures will be implemented prior to occupation of the premises, pursuant to a suitable condition of Development Approval. The operational measures will be implemented on an on-going basis by Child Care Centre management, pursuant to a condition of Development Approval that gives effect to this OMP.

Operational Measures

The following operational noise mitigation measures will be implemented.

Operational Measures - Noise Mitigation	
Hours of Operation	
1.	The Child Care Centre is to be operational for customer use from 7.15am to 6.30pm Monday to Friday, excluding public holidays.
2.	Staff will be instructed not to arrive prior to 7.00am and to be off site by 7.00pm.
External Play Areas	
3.	Children are not permitted outdoors for play purposes prior to 7.00am and after 6.00pm.
4.	Fixed play equipment should be non-metallic. If metal fixed play equipment is used, then hollow metal sections shall be filled with expanding foam or sand.
5.	Concrete or brick paved areas, if any, should be minimised and where practicable covered with synthetic grass to minimise noise of play equipment on hard surfaces.
6.	There are no play restrictions on the Babies (0 to 2 years old) or Toddlers (2 to 3 years old).
7.	There are play restrictions on Pre-kindy (3 – 4 years old) and Kindy (4 – 5 years old) as follows:
a.	Free play activity areas within the Level 1 outdoor play area only;
b.	Quiet area on the Ground Floor south outdoor play area;
c.	Quiet area on the Ground Floor east outdoor play area; and
d.	Not permitted on the Ground Floor west outdoor play area.
Music	
8.	Keep external windows and doors closed when playing music indoors.
9.	Do not play music outdoors (except light children's music if authorised by the Local Authority).
Car Park	
10.	Staff will be instructed not to arrive prior to 7.00am and to be off site by 7.00pm.
11.	Staff will be instructed to park in the designated staff parking bays only.
12.	Signage will be placed in the car park advising staff / visitors not to slam doors or play music in the car park.

6.0 Waste

6.1 Waste Generation Rates

The Commercial and Industrial Waste Management Guidelines published by the Western Australian Local Government Association ('WALGA') include estimated waste generation rates for various land uses. The WALGA rates were derived from the Better Practice Guidelines for Waste Management and Recycling in Commercial and Industrial Facilities published by the New South Wales Environmental Protection Authority. Neither document includes a waste generation rate for Child Care Centres.

Waste generation rates published by the City of Melbourne and City of Casey in Victoria suggest a Child Care Centre generates 350 litres of General (including FOGO) waste and 350 litres of Recyclable waste per 100m² of floor area per week. A review of other Waste Management Plans indicates this rate is based on 7 days a week, equating to 50 litres per trading day per 100m².

A waste generation rate of 50 litres per trading day per 100m² has been applied, with floor area based on the total area of the Activity Rooms.

A separate FOGO waste collection service is not proposed. All FOGO waste will be included in the General waste collection service.

6.2 Estimated Waste Generation

General and Recyclable Waste

It is estimated the proposed Child Care Centre will generate the following amount of waste.

Item	General Waste	Recyclable Waste
Waste Generation Rate	50 litres / 100m ² / day	50 litres / 100m ² / day
Activity Room Floor Area	265m ²	265m ²
Trading Days	5	5
Daily Waste	133 litres	133 litres
Weekly Waste	665 litres	665 litres

Other Waste Requirements

- Liquid or Hazardous Waste: Not Applicable
- Medical Waste: Not Applicable
- Food Processing: Not Applicable

6.3 Bin Selection

Bin Size and Collection Frequency

- General Waste: 240 litre bins collected weekly;
- Recyclable Waste: 240 litre bins collected weekly.

Type and Number of Bins

Item	General Waste	Recyclable Waste
Collection Frequency	Weekly	Weekly
Waste per Week	665 litres	665 litres
Number of Bins	3 x 240 litre bins	3 x 240 litre bins
Capacity of Bins	720 litres	720 litres
Total Bins Required	6 x 240 litre bins	

6.4 Bin Selection

The Bin Store is of sufficient size to accommodate the required bins, as illustrated below.

The Bin Store is located in the undercroft car park at the rear of the building and can easily be accessed by staff. The Bin Store will have a roller door and is fully enclosed. The Bin Store will be fitted with a tap and floor waste and all finished surfaces will be impervious.

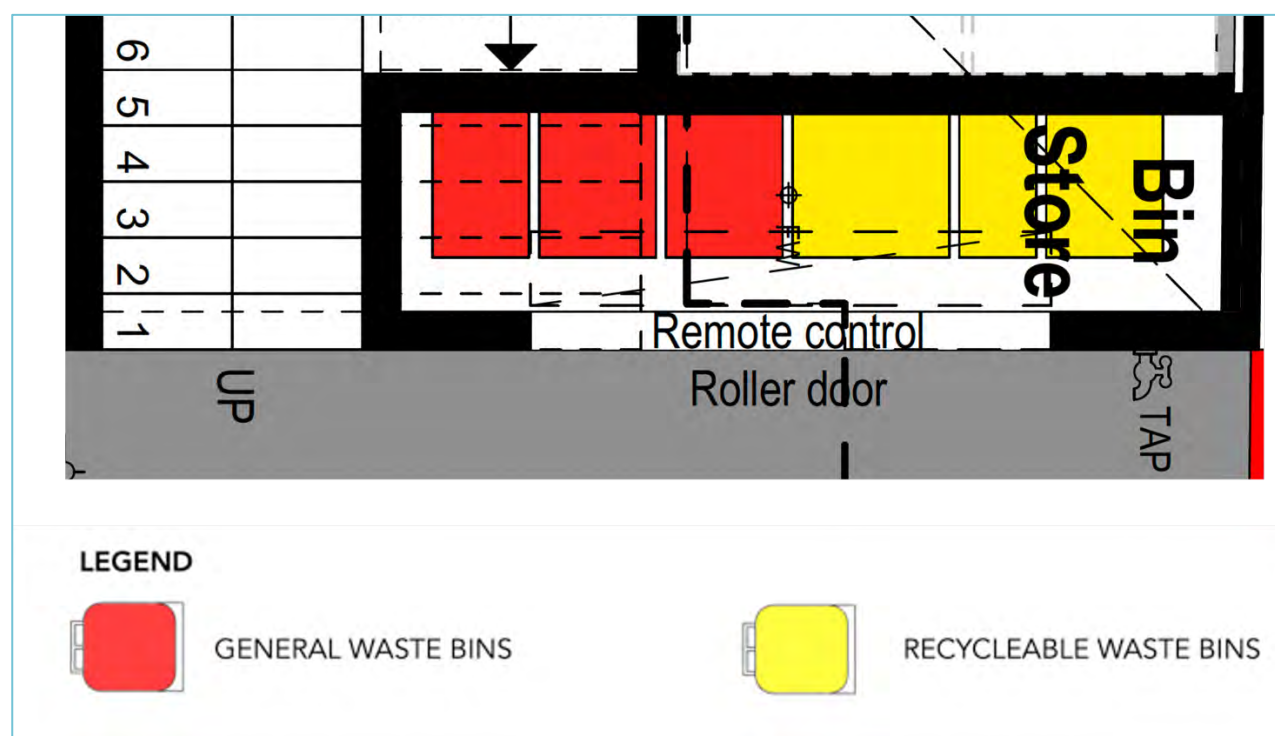


Figure 4: Bin Store

6.5 Waste Collection

Collector

Private Contractor.

Collection Point

It is intended that waste be collected by a private contractor.

Waste collection will be scheduled to occur during the day (between 10.30am and 3.30pm) to avoid conflicts with the peak drop-off and pick-up times for the proposed Child Care Centre.

Waste can be collected from the West Road verge in front of the site. Staff will be responsible for wheeling bins to the kerb in the evening before the day of collection and returning bins to the compound after collection. Given the limited number of bins (6), the bins could be placed in immediately adjacent to the crossover, as shown below.

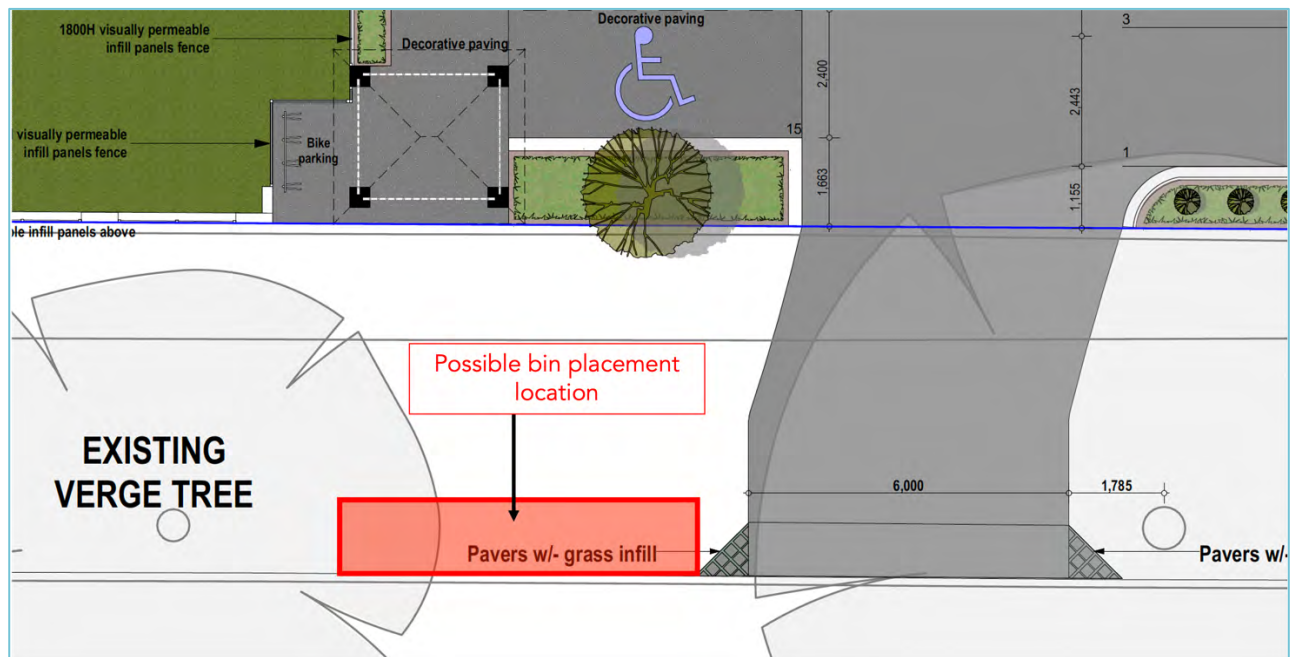


Figure 5: Possible Bin Placement (Verge Collection)

7.0 Review of Operational Management Plan

Management of the Child Care Centre will review the OMP on an annual basis to ensure it remains effective and consistent with regulatory requirements.

Complaints relating to the operation of the Child Care Centre should be directed to the Centre Manager, who will endeavour to respond in a timely manner.

The contact details of the Child Care Centre shall be included in this OMP prior to the Child Care Centre commencing operations, as per the details below (to be completed).

Contact Details	
Trading Name of Centre	
Name of Centre Manager	
Telephone Number – Business Hours	
Telephone Number – After Hours	
Email Address	

TRANSPORT IMPACT STATEMENT

94 West Road

Bassendean

October 2024

Rev D

The logo for Kcctt features the letters 'kcctt' in a bold, dark red, sans-serif font. To the left of the 'k' are three short, parallel, dark red diagonal lines.

PART OF  **Premise**

HISTORY AND STATUS OF THE DOCUMENT

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Rev A Draft	19.12.2023	M Kleyweg	M Kleyweg	20.12.2023	Issued for Review
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Rev D	31.10.2024	M Kleyweg	M Kleyweg	31.10.2024	Proposed Layout Amended

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Appendices

Appendix 1 - The layout of the proposed development

Appendix 2 - Transport Planning and Traffic Plans

Appendix 3 - Vehicle Turning Circle Plans

1. Executive Summary

- The project location is No. 94 West Road, Bassendean.
- The subject lot is currently vacant.
- The proposed development is a childcare centre with a capacity for 80 children.

Technical Findings

- The proposed development is expected to generate 348 vehicular trips per day, 64 vehicular trips in the AM peak and 56 vehicular trips in the PM peak.
- According to WAPC this is considered as a moderate traffic impact to the surrounding road network.
- Three major routes are expected to be utilised for access to the subject site:
 - To/from the north via West Road
 - To/from the south via West Road
 - To/from the east via West Road > Hyland Street

Relationship with Policies

- According to the Town of Bassendean Local Planning Policy No. 8, the proposed development will require 22 carparking bays.
- The proposed development plans show 18 parking bays. The calculated shortfall is 4 bays. However, the 18 bays aligns with the DPLH Draft Position Statement on Child Care premises of one space per 5 children.
- KCTT have provided a detailed breakdown of required parking in Section 2.8, based on extensive experience in this field.
- The analysis has shown that the maximum required parking would be 6 for parents and 8 for staff members in the busiest hours of the day – a parking demand of 14 parking bays.
- Therefore, a total of 18 car parking bays would satisfy all parking requirements and would align with the DPLH position.
- ACROD Provision – the proposed development will meet the requirement for 1 ACROD bay.

Conclusion

- As stated above, the proposed development will generate up to 348 vehicular trips per day and 64 vehicular trips in the peak hour.
- West Road is classified as a Local Distributor as per MRWA classification with the maximum desirable volume of 6,000 vehicles per day. Currently, there are no publicly available traffic counts for this section of West Road. However, this road section provides connection to higher hierarchy road network for residential developments. Therefore, it is unlikely that maximum desirable volume will be exceeded with the additional traffic from the proposed development.
- Other surrounding roads would absorb significantly less traffic than West Road; moreover, the traffic would be dispersed, so the impact can be considered negligible.
- In summary, KCTT believe that the proposed development will not negatively impact the surrounding road network.

2. Transport Impact Statement

2.1 Proposal

Rpoint Properties engaged KCTT to prepare a Transport Impact Statement (TIS) for the proposed Childcare centre at 94 West Road, Bassendean.

The proposed development will have capacity for 80 children.

The subject site will have an access to/from West Road, north of Hyland Street.

This report will primarily address the level impact of the proposed development and the requirements for integration of the proposed development with the surroundings, namely the existing and planned immediate road network.

2.2 Location

Street Number	94
Road Name	West Road
Suburb	Bassendean
Description of Site	The subject site is currently vacant. The proposed development is a Childcare centre with a capacity for 80 children and 13 staff members.

2.3 Technical Literature Used

Local Government Authority	Town of Bassendean
Type of Development	Individual Development – Childcare Centre
Is the NSW RTA Guide to Traffic Generating Developments Version 2.2 October 2002 (referenced to determine trip generation / attraction rates for various land uses) referenced?	YES
Which WAPC Transport Impact Assessment Guideline should be referenced?	Volume 4 - Individual Developments Volume 5 - Technical Guidance
Are there applicable LGA schemes for this type of development?	YES
<i>If YES, Nominate:</i>	
Name and Number of Scheme	Local Planning Scheme No. 10
Are Austroads documents referenced?	YES

2.4 Land Uses

Are there any existing Land Uses	NO
Proposed Land Uses	
How many types of land uses are proposed?	One (1)
Nominate land use type and yield	Childcare Centre <ul style="list-style-type: none"> - 80 children - 13 staff members
Are the proposed land uses complementary with the surrounding land-uses?	YES

2.5 Local Road Network Information

How many roads front the subject site?	Two (2)
<i>Name of Roads Fronting Subject Site / Road Classification and Description:</i>	
Road Name	West Road
Number of Lanes	two way, one lane (no linemarking), undivided
Road Reservation Width	20m
Road Pavement Width	8m
Classification	Local Distributor
Speed Limit	50kph or State Limit
Bus Route	YES
<i>If YES Nominate Bus Routes</i>	55
On-street parking	YES - Near the Bassendean Primary School – app. 150-200m from the subject site
Road Name	Bridson Street
Number of Lanes	two way, one lane (no linemarking), undivided
Road Reservation Width	15m
Road Pavement Width	7m
Classification	Local Distributor
Speed Limit	50kph or State Limit
Bus Route	NO
<i>If YES Nominate Bus Routes</i>	
On-street parking	NO

2.6 Traffic Volumes

Road Name	Location of Traffic Count	Vehicles Per Day (VPD)	Vehicles per Peak Hour (VPH)				Heavy Vehicle %	Date of Traffic Count	If older than 3 years multiply with a growth rate
			AM Peak Time	AM Peak - Peak VPH	PM Peak Time	PM Peak - Peak VPH			
Bridson Street	East of Elder Parade	2,673	07:45	– 280	15:00	– 250	5.2%	2021/2022	–
Old Perth Road	West of Hamilton Street	3,763	08:00	– 332	15:30	– 439	4.8%	2022/2023	–
West Road	South of Guildford Road	7,046	08:00	– 631	14:45	– 617	6.0%	2021/2022	–

*Note** - These traffic counts have been obtained from the MRWA Traffic Map.

*Note*** - Location of the West Road traffic count is outside the Bassendean shopping centre and Bassendean Oval (Refer to Appendix 2)

2.7 Vehicular Crash Information

Is Crash Data Available on Main Roads WA website? YES

If YES, nominate important survey locations:

Location 1 Intersection of Bridson Street and West Road
Location 2 Intersection of West Road and Hyland Street – no crashes were reported in the 5-year period

Period of crash data collection 01/01/2019 - 31/12/2023

The following tables show crash rates and crash densities in Perth Metropolitan area on local roads and state roads for the period from 2017 to 2022, as obtained from Main Roads WA on the 31st May 2022 by email request:

Crash Density and Crash Rate on Metropolitan Local Roads Network only				
	All Crashes		Serious Injury Crashes (Fatal+Hospital)	
	Average Annual Crash Density (All Crashes/KM)	Average Annual Crash Rate (All Crashes/MVKT)	Average Annual Crash Density (Ser. Inj. Crashes/KM)	Average Annual Crash Rate (Ser. Inj. Crashes/MVKT)
Metro Local Roads - Midblock	2.51	0.95	0.12	0.05
Metro Local Roads - All	5.23	1.98	0.24	0.09

Note: Based on 5-years data for the period 2017 to 2021.

Definitions of acronyms and terms used in this analyse can be found below:

- PDO Crash - a crash that results in property damage only (major or minor) and does not require hospitalisation or medical treatment, as listed in Main Roads WA's Crash Analysis Reporting System (CARS).
- KSI Crashes - Killed and serious injury crash
- MVKT - Million Vehicle Kilometres Travelled.

Intersection Name	Road Hierarchy	Speed Limit	Crash Statistics			
			No of KSI Crashes	No of Medical Attention Crashes	No of PDO Major Crashes	No of PDO Minor Crashes
Bridson Street / West Road	Access Road / Local Distributor	50kph or State Limit / 50kph or State Limit	1	0	0	0

MR Type	Involving Overtaking	Involving Parking	Involving Animal	Involving Pedestrian	Entering / Leaving Driveway	Other / Unknown
Count	0	0	0	0	0	1

No of MVKT Travelled at Location	App. 6,000 VPD * 365 * 5 years * 0.3 km = 3.29 MVKT
KSI Crash Rate	1 KSI crashes / 3.29 MVKT = 0.3 KSI crashes/MVKT
All Crash Rate	1 crashes / 3.29 MVKT = 0.3 crashes/MVKT
Comparison with Crash Density and Crash Rate Statistics	KSI crash rate of 0.3 is higher than the network average of 0.09 KSI Crashes per MVKT for Local Roads Network. All crashes rate of 0.3 is lower than the network average of 1.98 Crashes per MVKT for Local Roads Network. Given this is an isolated incident in the 5-year period, KCTT believe that the existing intersection does not pose a significant safety risk in the surrounding area.

2.8 Vehicular Parking

Local Government

Town of Bassendean

Local Government Document Utilised

Local Planning Policy No 8 Policy: Car Parking and End-of-Trip Facilities

Description of Parking Requirements in accordance with Scheme:

Child Care Premises:

1 bay for every 10 children the facility is designed to accommodate, plus 1 bay per staff member

Note: DPLH Draft Position Statement: Child Care Premises provides a rate of 1 bay per 5 children.

Calculation of Parking

Land Use	Requirements	Yield	Total Parking
Childcare Centre	1 per 10 children	80 children	8
	1 per staff member	14 staff members	14
Total Car Parking Requirement			22
Total Volume of Parking Provided by Proponent			18

Justification

According to the Town of Bassendean Local Planning Policy No. 8, the proposed development will require 22 carparking bays. The proposed development plans show 18 parking bays. The calculated shortfall is 4 bays.

In addition to aligning with the DPLH Draft Position Statement, KCTT believe that the following points justify the calculated shortfall:

- Bassendean Primary School is located within a 200m walking radius. Parents who have children enrolled in both primary school and the proposed childcare centre are likely to park at the provided primary school parking and walk to the childcare centre.
- The proponent has provided an on-street parking survey for West Road and Whitfield Street just before 8:00 and around 16:30 in September 2023.
- The results have shown that average availability at this time is 73% and 94% in AM and PM peak respectively on West Road and 75% and 73% in AM and PM peak respectively on Whitfield Street.
- Therefore, there is an abundance of on-street parking in the vicinity of Bassendean Primary School.
- The childcare centre is located in a residential area. Parents can walk with children if they live nearby.
- 4 bicycle parking spaces are provided for staff members.

However, to justify the shortfall further, KCTT provided the required parking breakdown below.

Given the nature of the proposed land use and site context, the following points inform KCTT's opinion that the proposed car parking provision can meet the development demands:

- It is expected that some staff members could cycle/walk or be dropped off to work, therefore not requiring a parking bay for their shift. Not all staff members will work at one time.
- It is highly unlikely that the childcare centre would operate at its maximum capacity at all times.
- The peak time for childcare centres is typically a 2-hour period. The average length of stay, as stated in NSW RTA - Guide to Traffic Generating Developments, is 6.8 minutes. Our experience in surveying dwell times for childcare centres outside of commercial zones confirms this finding. Even assuming conservative 10 minutes average length of stay, the actual arrival/departure rate of parents' vehicles is likely to be spread throughout the 2-hour peak time.

The following tables were derived through many years of practice and research in this field that our office completed. We have worked with several established childcare providers who have provided sign-in data for a full week. The percentages outlined below have emerged as the current average arrival/departure pattern. As per our transport impact assessment, the estimated average dwell time is 10 minutes, which is significantly higher than the dwell time suggested by NSW RTA Guide to Traffic Generating Developments.

While this pattern shows that up to 95% of children attend for the day (as practically recorded), the distribution still does not allow for siblings attending the centre. Furthermore, the distribution assumes that all children in attendance are driven to the childcare in a separate personal vehicle (not walked or brought on bicycles); therefore, the distribution below has a degree of conservatism.

The below information is from an existing childcare centre, licenced to cater for up to 82 children. The following table is a sample of the busiest week in this practice which demonstrates the average occupancy and daily breakdown in May 2019 (prior to COVID-19 outbreak). The occupancy varies during weekdays and across the age groups within the centre. The total average occupancy for the May 2019 month is recorded as 76%, which is slightly above the documented average WA occupancy rates for 2019 recorded as 69%. The current documented national average occupancy as reported by Early Years Research 2022 is 62.85%.

	Date	Total entries in	Total absent	Total attending	Total occupancy	Total attendance
M	6/05/2019	50	2	48	61%	96%
T	7/05/2019	69	6	63	84%	91%
W	8/05/2019	69	7	62	84%	90%
T	9/05/2019	71	5	66	87%	93%
F	10/05/2019	62	9	53	76%	85%

We base our modelling off our own direct childcare experience which indicates a 95% occupancy as a most conservative possible day.

In our previous experience, we have come across data indicating that siblings usually make up 15-25% of attendees. In these cases, more than one child will be brought in a single vehicle, reducing the parking requirement.

The tables below were developed on the following assumptions assessing the worst-case scenario:

- The arrival percentage is derived from data provided to KCTT and described above.
- It was assumed there were no siblings in the centre.
- It was assumed that all children in attendance would be driven to the centre.

Table 1 – Children Sign-In times

Sign-in Time	Extracted Arrival Percentages (of the maximum number of children)	Expected Number of Children Signing In	Parking demand (assumed dwell time 10 minutes per vehicle)
07:00 - 07:30	13.97%	11	4
07:30 - 08:30	40.55%	32	6
08:30 - 09:30	30.68%	25	5
09:30 - 10:30	7.67%	6	1
After 10:30	1.37%	1	1
Total:	94.25%	75 children (80 children – 100% capacity)	

Table 2 – Children Sign-Out times

Sign-Out Time	Extracted Departure Percentages (of the maximum number of children)	Expected Number of Children Signing Out	Parking demand (assumed dwell time 10 minutes per vehicle)
Before 13:30	0.55%	0	0
13:30 - 14:30	1.92%	2	1
14:30 - 15:30	11.23%	9	2
15:30 - 16:30	24.93%	20	4
16:30 - 17:30	45.21%	36	6
17:30 - 18:30	10.41%	8	3
Total:	94.25%	75 children (80 children – 100% capacity)	

The above Tables 1 and 2 above show that the visitors' parking demand is the strongest from 07:30 – 08:30 in the AM Peak and 16:30 – 17:30 in the PM Peak.

When applied to the subject development with the assumed dwell time of 10 minutes per vehicle, the subject childcare centre would require a maximum of 6 bays in AM and PM peak to cater for the expected parking demand of the pick-up / drop-off function.

The proponent has advised that 8 staff members at most would be present on-site by 08:30 and in the PM peak period 16:30 – 17:30. Therefore, 8 staff carparking bays would be required in peak hours and 8 parking bays would be available for visitors.

Having in mind that the above analysis is conducted with no consideration of having siblings in the childcare centre; and with the assumption that all children are driven to the centre, and that all staff members will arrive with their own vehicle (no carpooling or alternative transportation), KCTT believe that the proposed parking provision would be sufficient for all parking requirements of the proposed childcare centre.

Therefore, with a provision of 18 carparking bays, KCTT believe that all parking requirements will be catered for.

KCTT believe that the ACROD shared space could be used for turning around on-site instead of a separated reversing bay. This would allow for an additional parking bay which would providing more on-site parking opportunities.

2.9 Compliance with AS2890 Parking facilities

Which Austroads documents are referenced?	<ul style="list-style-type: none"> • Australian/New Zealand Standard, Parking facilities, Part 1: Off-street car parking - AS 2890.01 • Australian/New Zealand Standard, Parking facilities, Part 6: Off-street parking for people with disabilities – AS2890.06
Number of Parking Bays on-site	<ul style="list-style-type: none"> • 18 bays
Proposed development User Class	<ul style="list-style-type: none"> • 1A - Residential, domestic and employee parking • 3 - Short-term city and town centre parking, parking stations, hospital, and medical centres
Driveway category and dimensions	<ul style="list-style-type: none"> • Category 1 access driveway • 5.8m driveway width • 6m driveway length

2.9.1 Compliance Overview

FULL COMPLIANCE		PARTIAL DEPARTURE	FULL DEPARTURE	NOT APPLICABLE
Element	Compliance	Comment		
Car Bay Class 1A	FULL COMPLIANCE			
Car Bay Class 3	FULL COMPLIANCE			
Aisle width	FULL COMPLIANCE			
Blind Aisle Extension	FULL COMPLIANCE			
Reversing bay	FULL COMPLIANCE	ACROD shared space to be used for turning		
Columns' location	FULL COMPLIANCE			
Location of driveway	FULL COMPLIANCE			
Sight distance requirements at access driveways	FULL COMPLIANCE			
Minimum sight lines for pedestrian safety	FULL COMPLIANCE			

2.9.2 Comparison of proposed layout to AS2890.01 requirements

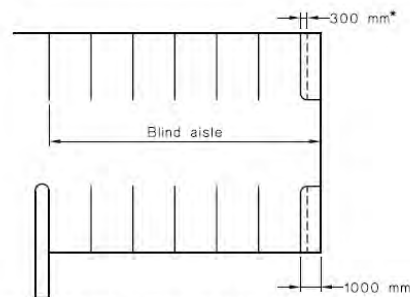
Parking Bay Type	AS2890.1:2004 Off-street car parking AS2890.6 Off-street parking for people with disabilities					
	Parking Bay Length		Parking Bay Width		Aisle Width	
	Required	Proposed	Required	Proposed	Required	Proposed
All bays at 90° (User Class 1A) STAFF	5.4m	5.4m	2.4m	2.5 - 2.6m	5.8m	6m
All bays at 90° (User Class 3) VISITORS	5.4m	5.4m – 5.5m	2.6m	2.6m	5.8m	6m
ACROD Parking	5.4m	5.5m	2.4m–ACROD 2.4m–shared space	2.4m–ACROD 2.4m–shared space	5.8m	6m

Name other requirements in the AS2890.1:2004 document.

“Blind aisles

At blind aisles, the aisle shall be extended a minimum of 1 m beyond the last parking space, as shown in Figure 2.3, and the last parking space widened by at least 300 mm if it is bounded by a wall or fence.

In car parks open to the public, the maximum length of a blind aisle shall be equal to the width of six 90 degree spaces plus 1 m, unless provision is made for cars to turn around at the end and drive out forwards.



“Additional widening required if there is a wall or fence at the side of the last space, see Clause 2.4.1(b)(ii).

DIMENSIONS IN MILLIMETRES

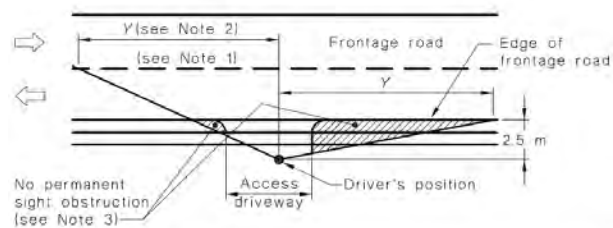
FIGURE 2.3 BLIND AISLE EXTENSION

Blind aisle

More than 1m required is provided

“Entering sight distance

Unsignalized access driveways shall be located so that the intersection sight distance along the frontage road available to drivers leaving the car park or domestic driveway is at least that shown in Figure 3.2.”

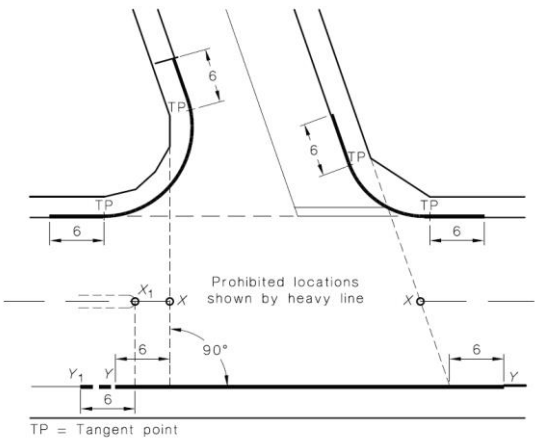


Frontage road speed (Note 4) km/h	Distance (Y) along frontage road m		
	Access driveways other than domestic (Note 5)		Domestic property access (Note 6)
	Desirable 5 s gap	Minimum SSD	
40	55	35	30
50	69	45	40

Sight distance requirements at access driveways

When the driver positions themselves properly, Stopping Sight Distance (SSD) values of 45m both to the north and the south can be achieved, ensuring adequate visibility and maintaining safety. The only location where the sightline is partially obstructed is when driver is positioned exactly 2.5m from the edge of the carriageway. Refer to Drawing S40 of the Appendix 3 for Sight distances review.

“ Access driveway location
Driveway Categories 1 and 2 At unsignalized intersections of sub-arterial, collector or local streets with each other or with an arterial road, access driveways in Categories 1 and 2 (see Table 3.1) shall not be located in the sections of kerb shown by heavy lines in Figure 3.1. This requirement shall not apply to accesses to domestic driveways in the kerb section opposite the entering road at any intersection including signalized intersections. Furthermore, it shall not apply to any access driveway serving a property which would otherwise be denied access due to the physical impossibility of meeting the requirement.”



Access driveway location	<p>Given its proximity to two intersections of West Road with Hyland Street and Bridson Street, there are limited options for placing of the crossover/driveway. KCTT believe that the proposed location is the best possible, having in mind the necessity of retaining existing trees, the position of nearby intersections and respective traffic volumes on these intersections. Hyland Street carries significantly less traffic than Bridson Street due to low number of residential units surrounding it and because it is not a viable route for high passing traffic volumes.</p> <p>Therefore, positioning the crossover as far away possible from West Road / Bridson Street is a desirable outcome.</p>
--------------------------	---

2.9.3 **Vehicle Swept Paths**

Have Vehicle Swept Paths been checked for Parking? YES

If YES, provide description of performance:

The proposed parking area has been checked with a standard B99 Passenger Vehicle 5.2m.

No navigability issues have been found.

Please refer to the swept path analysis plans provided in Appendix 3.

2.10 **Bicycle Parking**

Local Government	Town of Bassendean
Reference Document Utilised	Local Planning Policy No 8 Policy: Car Parking and End-of-Trip Facilities

Description of Parking Requirements in accordance with Scheme:

The LPP No. 8 does not provide bicycle parking rates for the land use - Childcare Centre.

Total Volume of Bicycle Parking Required	N/A
Total Volume of Bicycle Parking Provided by Proponent	4

Justification

The proposed development will provide 4 bicycle bays. According to LPP No. 8, developments that provide 3-5 bicycle bays are required to provide 1 shower and change facility. The proposed development plans suggest full compliance with this requirement.

2.11 ACROD Parking

Class of Building Class 1b (Child Care Centre);
 Does this building class require specific provision of ACROD Parking? YES
 Reference Document Utilised Building Code of Australia
 Description of Parking Requirements:
"Class 1b — 1 space for every 100 carparking spaces or part thereof."

Parking Requirement in accordance with regulatory documents

Land Use	Requirements	Yield	Total Parking
Child care	<i>1 space for every 100 carparking spaces or part thereof</i>	18	1
Total Volume of ACROD Parking Required			1
Total Volume of ACROD Parking Provided by Proponent			1

Justification

The proposed plans demonstrate 1 ACROD bay, meeting the requirements outlined by the Building Code of Australia.

2.12 Delivery and Service Vehicles

Guideline Document used as reference NSW RTA Guide to Traffic Generating Developments
 Requirements
Other uses - 1 space per 2,000m²

Parking Requirement in accordance with regulatory documents

Land Use	Minimum Requirements	Yield	Total Parking
Childcare Centre	<i>1 space per 2,000m²</i>	Less than 2,000m ²	1
Total Volume of Service and Delivery Parking Required			1
Total Volume of Service and Delivery Parking Provided by Proponent			N/A

Justification

The proposed development will not require a dedicated service/delivery bay. Deliveries will likely be conducted outside of peak hours and the delivery vehicle will utilise one of the available bays.

2.13 Calculation of Development Generated / Attracted Trips

What are the likely hours of operation?	06:30 - 18:30
What are the likely peak hours of operation?	AM peak 07:30 - 08:30 PM peak 16:30 - 17:30
Do the development-generated peaks coincide with existing road network peaks?	YES
If YES, Which:	Partially AM peak
Guideline Document Used	NSW RTA Guide to Traffic Generating Developments
Rates from above document:	Child Day Care: <ul style="list-style-type: none"> 0.8 trips in AM Peak per child 0.7 trips in PM Peak per child <p><i>It should be noted that these rates are given for a 2-hour peak period. For the purposes of this report KCTT will use the worst-case scenario where the two-hour traffic volume will be attracted to the development within one hour.</i></p>

Given that the WAPC Transport Assessment Guidelines and NSW RTA Guide to Traffic Generating Developments do not offer daily vehicular trip generation rate for these land uses KCTT have assumed the following to apply:

Childcare centres vehicular daily trips can be assumed to be 4 VPD per child and 2 VPD per employee. Each parent will make 2 vehicular trips when dropping off the child to the daycare centre and 2 vehicular trips when picking the child up. Employees will make 1 vehicular trip arriving to work, and another vehicular trip when leaving work. For the calculations below, a conservative approach has been applied, **showing the theoretical maximum number of children, under the assumption that all children are driven to school, there are no siblings in the centre and no sick children absent from the centre.**

Land Use Type	Rate above	Yield	Daily Traffic Generation	Peak Hour Traffic Generation	
				AM	PM
Childcare Centre	4 VPD per child+2 VPD per employee 0.8 VPH AM Peak per child 0.7 VPH PM Peak per child	80 children 14 staff members	348	64	56

Does the site have existing trip generation/attraction?	NO
What is the total impact of the new proposed development?	The proposed development is expected to generate 348 daily vehicular trips, 64 vehicle trips in the morning peak and 56 in the evening peak. According to WAPC, this is considered a moderate traffic impact on the surrounding road network.

2.14 Traffic Flow Distribution

How many routes are available for access / egress to the site? Three (3)

Route 1 / Movement 1

Provide details for Route No 1 To/from the north via West Road
 Percentage of Vehicular Movements via Route No 1 55% [191 VPD; AM 35 VPH; PM 31 VPH]

Route 2 / Movement 2

Provide details for Route No 2 To/from the east via West Road > Hyland Street
 Percentage of Vehicular Movements via Route No 2 5% [18 VPD; AM 3 VPH; PM 3 VPH]

Route 3 / Movement 3

Provide details for Route No 3 To/from the south via West Road
 Percentage of Vehicular Movements via Route No 3 40% [139 VPD; AM 26 VPH; PM 22 VPH]

Note - For a more detailed plans of the estimated vehicular traffic volumes and distribution please refer to the plans provided in Appendix 2.

2.15 Public Transport Accessibility

How many bus routes are within 400 metres of the subject site? One (1)
 How many rail routes are within 800 metres of the subject site? None

Bus Route	Description	Peak Frequency	Off-Peak Frequency
55	Perth - Bassendean via Lord Street & Guildford Road	15-20 minutes	60 minutes

Walk Score Rating for Accessibility to Public Transport

32 | Some Transit. A few nearby public transportation options.

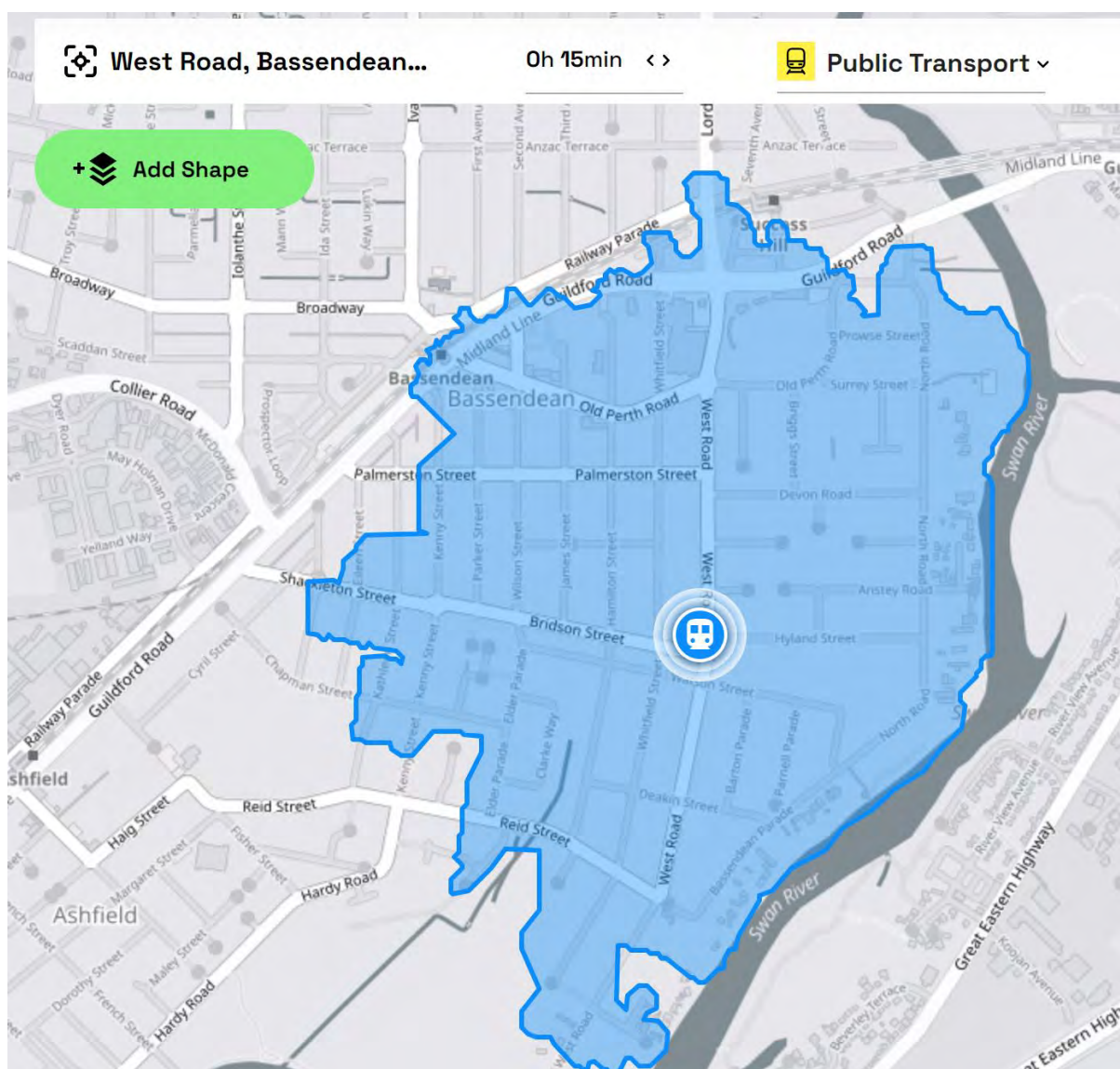


Figure 1 - Public transport availability from the subject site

2.16 Pedestrian Infrastructure

Describe existing local pedestrian infrastructure within a 400m radius of the site:

Pedestrian paths are available on Bridson Street and West Road and on most other surrounding roads. Refer to Appendix 2, drawing S04 for graphic representation of pedestrian paths.

Does the site have existing pedestrian facilities YES

Does the site propose to improve pedestrian facilities? NO

What is the Walk Score Rating?

53 | Somewhat Walkable. Some errands can be accomplished on foot.

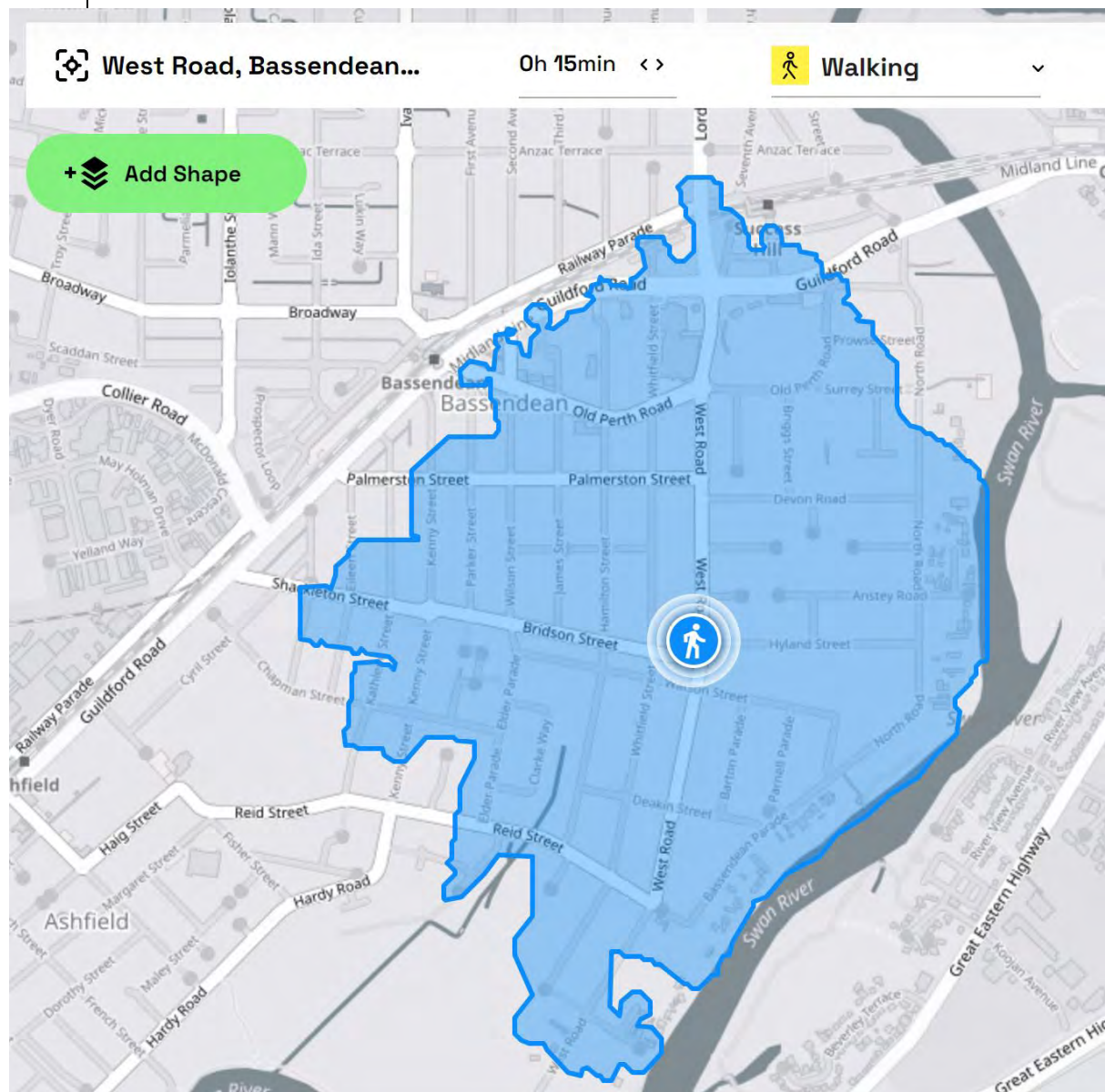


Figure 2 - Walking catchment from the subject site.

2.17 Cyclist Infrastructure

Are there any PBN Routes within an 800m radius of the subject site? YES

If YES, describe:

Classification	Road Name
"Other Shared Path(Shared by Pedestrians and Cyclists)"	West Road
"Good Road Riding Environment"	Bridson Street; West Road; North Road; Bassendean Parade; Elder Parade
"Perth Bicycle Network - Continuous Signed Routes"	NE14 – Wilson Street, Elder Parade, Chapman Street, Kenny Street;

Are there any PBN Routes within a 400m radius of the subject site? YES

If YES, describe:

Classification	Road Name
"Good Road Riding Environment"	Bridson Street
Does the site have existing cyclist facilities?	YES
Does the site propose to improve cyclist facilities?	YES

If YES, describe the measures proposed.

4 bicycle bays are proposed to promote alternative means of transportation.

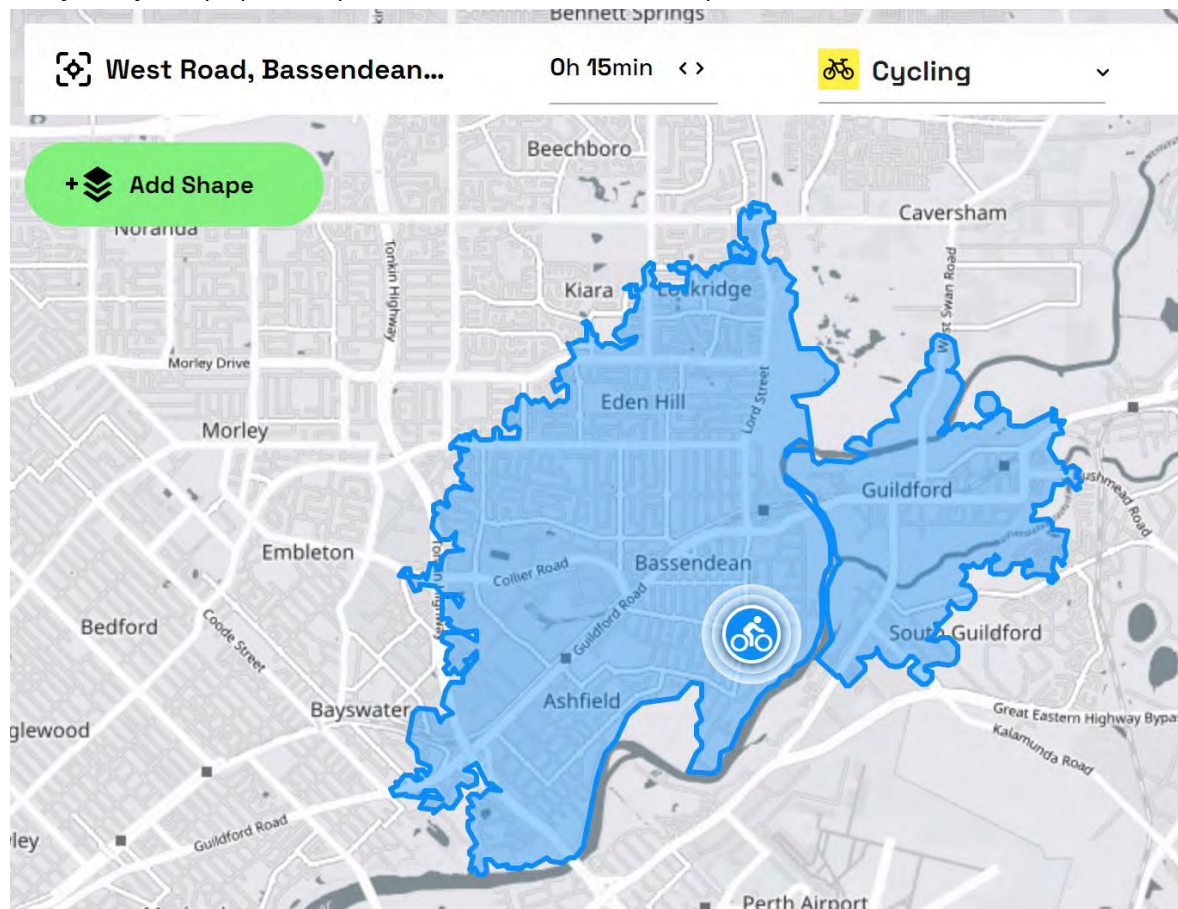


Figure 3 - Cycling catchment from the subject site.

2.18 Site-Specific Issues and Proposed Remedial Measures

How many site-specific issues need to be discussed? One (1)

Site-Specific Issue No 1

Parking Shortfall

Remedial Measure / Response

According to the Town of Bassendean Local Planning Policy No. 8, the proposed development will require 22 carparking bays.

The proposed development plans show 18 parking bays. The calculated shortfall is 4 bays.

KCTT have provided a detailed breakdown of required parking in Section 2.8, based on extensive experience in this field.

The analysis has shown that the maximum required parking would be 6 for parents and 8 for staff members in the busiest hours of the day, assuming the 'worst-case' scenario – a parking demand of 14 bays.

Therefore, the total provision of 18 car parking bays would meet the parking demand.

Appendix 1

The Layout of the Proposed Development

Briscola PTY LTD

Address:Lot 85 (#94) West Road, Bassendean

Childcare Centre

Job Number: 23019

Drawing No	Description
01	Cover Page
02	3D
03	Existing Site Survey
04	Site Plan
05	Context Plan
06	Aerial Map
07	Ground Floor Plan
08	First Floor Plan
09	Roof Plan
10	Elevations

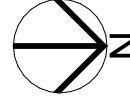




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18.00	Changes	18.10.24
17.00	Changes	10.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	13.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24

Revision Description 19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
3D

Scale: Sheet Size: A1

Project No: 23019 Revision Number: 19.00

Drawing No.: 02 of 10

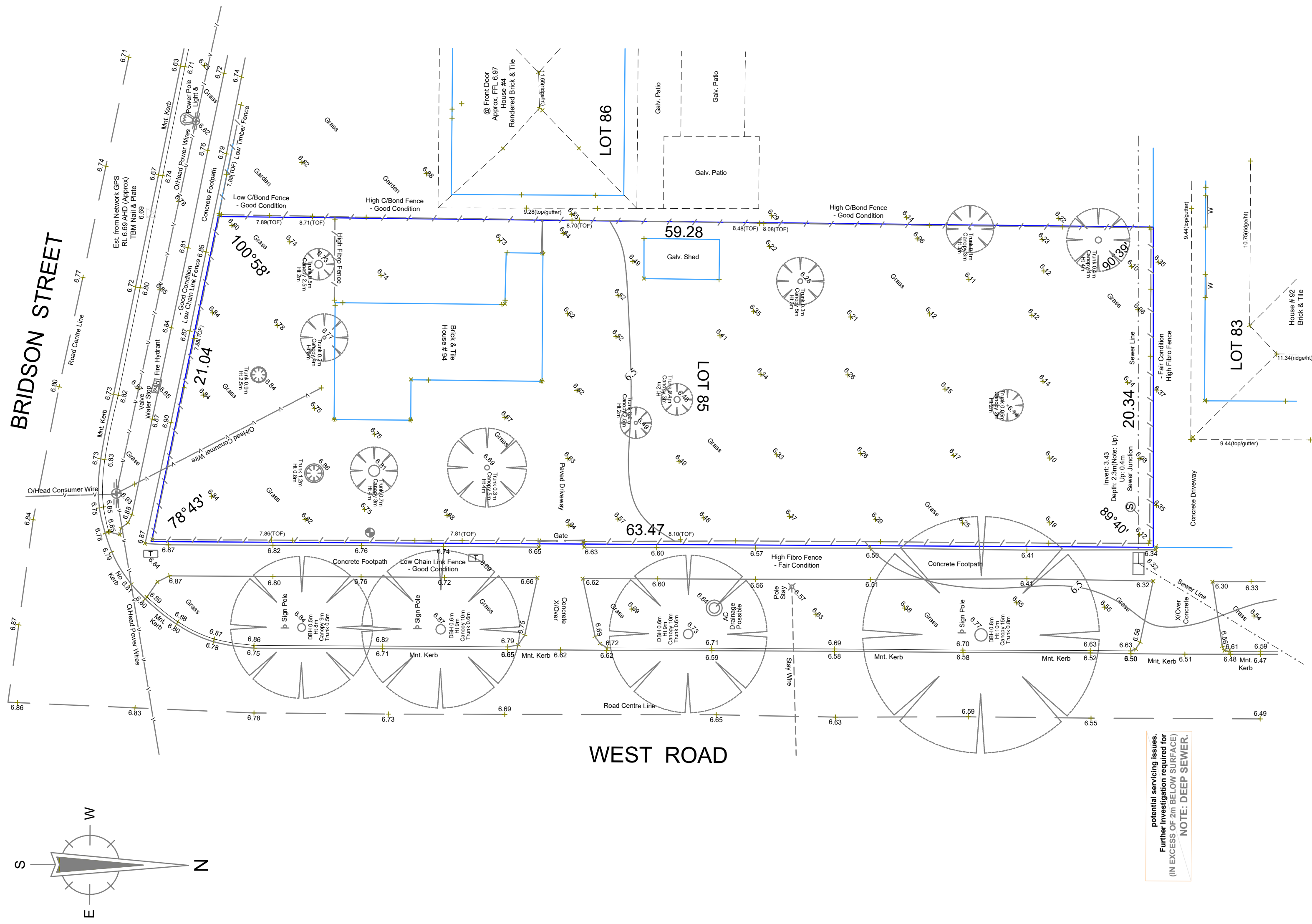


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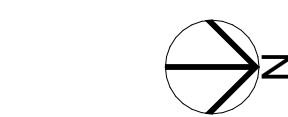


FEATURE SURVEY		STREET NAME : West Road		LOT # : 85	HOUSE # : 94	SUBURB : Bassendean	LOCAL AUTHORITY : Town of Bassendean	LOT AREA : 1239m ²	R CODE : R20	SURVEY DATE : 21/11/2022	SCALE AT A3 SIZE : 1:250
CLIENTS DETAILS:											
Briscola Pty Ltd											
LEVEL DATUM : AHD (Approx.)		DWG REF : West 94 F - v1.0									
LEGEND											
WATER METER		SEWER AC		TELSTRANBN PIT		POWER POLE		TREE (TO SCALE)		W WINDOW	
SEWER CONN.		(TOF) TOP OF FENCE		TEMP. BENCHMARK							



19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	15.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	31.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision	Description	19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Existing Site Survey

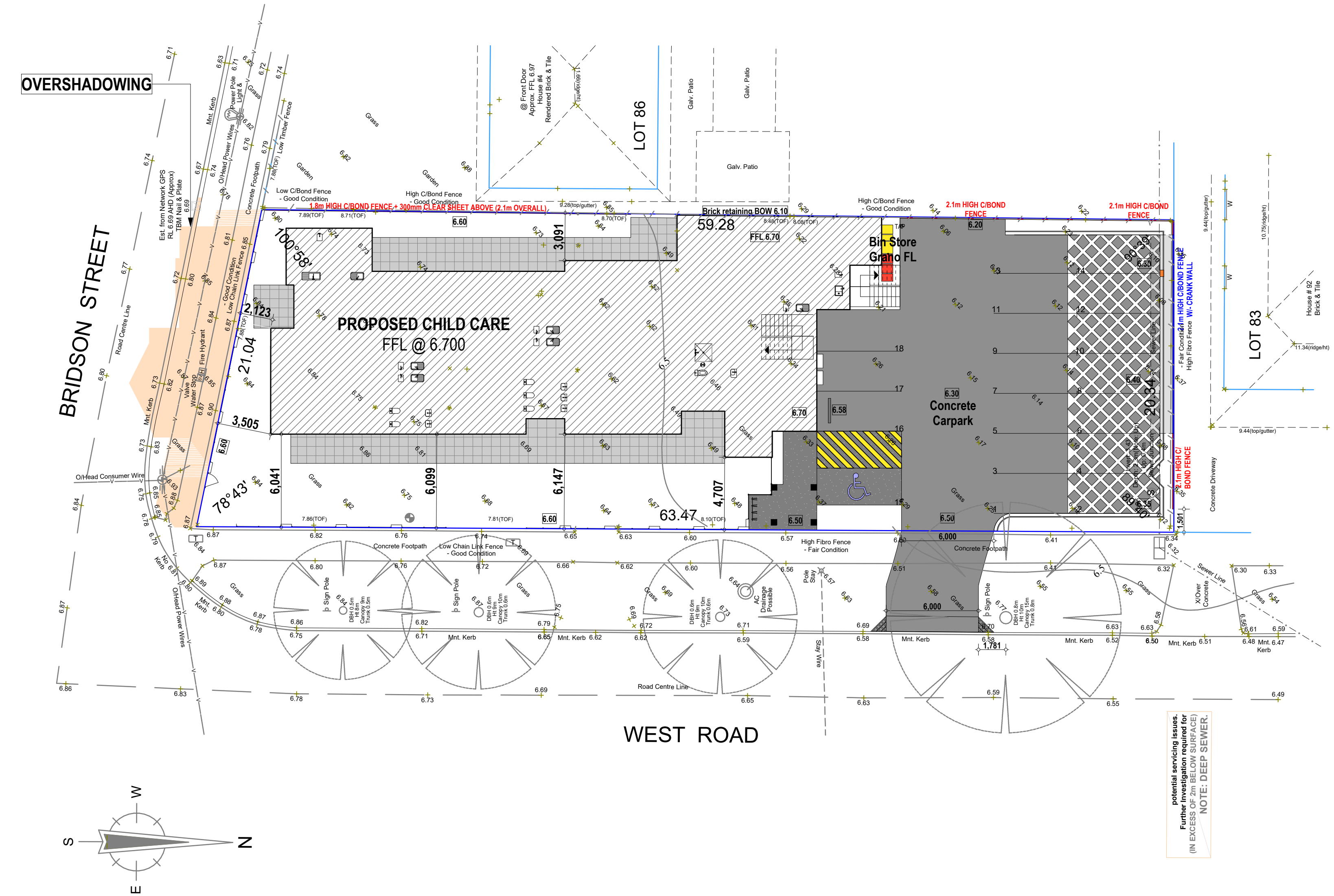
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Project No:	Revision Number:
23019	19.00

Drawing No.:
03 of 10

Vision CONSULTING		T: (08) 6144 0000 F: (08) 6144 0099 59 SCARBOROUGH BEACH RD, SCARBOROUGH WA 6019 Email: info@visionsurveys.com.au www.visionsurveys.com.au		IMPORTANT NOTES: THE BOUNDARY CANNOT BE VERIFIED DUE TO LACK OF SURVEY MARKS/ PEGS. ALL BUILDING DIMENSIONS & FEATURES ARE APPROXIMATE. ONLY BOUNDARY POSITIONS HAVE BEEN TAKEN FROM BILDING, FENCING, RETAINING WALLS AND OTHER TYPICAL FEATURES LOCATED ON THE BOUNDARY WHICH MAY OR MAY NOT BE ON THE CORRECT ALIGNMENT AND ARE TO BE VERIFIED WHEN REPOSED. BEFORE ANY WORK IS STARTED ON SITE OR PLANS ARE PRODUCED BY DESIGNERS/ ARCHITECTS, THE BOUNDARIES MUST BE REPOSED AND EXACT POSITIONS MARKED TO EXISTING STRUCTURES AND FENCING. VISION SURVEYS ACCEPTS NO RESPONSIBILITY FOR ANY CHANGES TO THE PARCEL OR PORTION OF THE PARCEL OR LAND SHOWN ON THIS SURVEY INCLUDING BUT NOT LIMITED TO ANY ADJOINING NEIGHBOURS LEVELS AND FEATURES THAT HAVE OCCURRED AFTER THE DATE OF THIS SURVEY. THIS PLAN IS INTENDED FOR THE DEPT OF PLANNING & INFRASTRUCTURE ONLY. SEWER / DRAINAGE MAY VARY FROM SCHEMATIC PRESENTATION/ CHECK WITH APPROPRIATE AUTHORITY BEFORE ADOPTION OF POSITION. CHECK TITLE FOR EASEMENTS / COVENANTS ETC.		TELSTRA YES SEWERAGE YES DRAINAGE GOOD VEG. / SOIL AS DESCRIBED		Urban Development Institute of Australia MEMBER SINCE 2022/2023 www.udi.org.au			
PLAN / DIAG / SP	P1784	ELECTRICITY	OVERHEAD	TELSTRA	YES	SEWERAGE	YES	DRAINAGE	GOOD	VEG. / SOIL	AS DESCRIBED
WATER	YES	GAS	YES								



FEATURE SURVEY		STREET NAME : West Road	LOT # : 85	HOUSE # : 94	SUBURB : Bassendean	LOCAL AUTHORITY : Town of Bassendean	LOT AREA : 1239m ²	R CODE : R20	SURVEY DATE : 21/11/2022	SCALE AT A3 SIZE : 1:250
CLIENTS DETAILS:		LEGEND								
Briscola Pty Ltd		WATER METER SEWER AC TELSTRANBN PIT POWER POLE TREE (TO SCALE) W WINDOW								
LEVEL DATUM : AHD (Approx.)		DWG REF : West 94 F - v1.0								
		(TOF) TOP OF FENCE TEMP. BENCHMARK								



Zone	Area	Perim
Stone FF	4.22	8.850
Bin	5.52	11.756
First Floor	111.61	52.380
Childcare Centre	440.77	122.789
	562.22 m ²	195.774 mm

Zoning:	Residential R20
Policies:	Residential
Policy:	
Heritage:	No
Bushfire:	No
BAL:	TBA
Acoustic:	No
Sewer:	Inside North
Power:	Overhead
Coastal:	No
Water:	Meter on Site
Wind Rating:	No

13.00	Changes	28.10.24
14.00	Changes	18.10.24
15.00	Changes	10.10.24
16.00	Changes	04.09.24
17.00	Changes	30.08.24
18.00	Changes	31.07.24
19.00	Changes	26.05.24
20.00	Changes	25.04.24
21.00	Changes	19.01.24

Revision	Description
1	19.01.24

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Project Name Childcare Centre	
Project Address Lot 85 (#94) West Road, Bassendean	
Drawing Title: Site Plan	
Scale: 1:200	Sheet Size: A1
Project No: 23019	Revision Number: 19.00
Drawing No.: 04 of 10	

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PLAN / DIAG / SP	P1784	TELSTRA	YES
ELECTRICITY	OVERHEAD	SEWERAGE	YES
WATER	YES	DRAINAGE	GOOD
GAS	YES	VEG. / SOIL	AS DESCRIBED

IMPORTANT NOTES:
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Hawaiian Bassendean



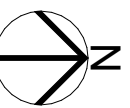
Commerical Development



Bassendean Primary School

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12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision/Description		19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Context Plan

Scale: Sheet Size: A1

Project No: 23019 Revision Number: 19.00

Drawing No.: 05 of 10



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13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
10.00	Changes	19.01.24

Revision	Description	19.01.24
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Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Aerial Map

Scale:	Sheet Size:
1:200	A1
Project No:	Revision Number:
23019	19.00

Drawing No.:
06 of 10

Child / Room Calculations				
Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	39.25m ²	3
Activity 2	4-5	28	92.21m ²	3
Activity 3	2-3	20	65.06m ²	4
Activity 4	3-4	20	67.16m ²	2
Total Internal =		80	263.68m ²	14
(Min 3.25m ² per child)				(Min 260m ² req)
Total External Play Area =		80	560.56m ²	
(Min 7m ² per child)				(Min 560m ² req)
Parking Calculations				
(As per local planning policy)				
Description	Required			
1 bay per employee	14			
1 bay per 10 childs	8			
Required =	22			
Provided =	18			



Zone	Area	Perim
Store FF	4.22	8.850
Bin	5.62	11.755
First Floor	111.61	52.380
Childcare Centre	440.77	522.780
	562.22 m ²	195.774 m

Site Calculations	
Site Area:	1,258m ²
Building Footprint:	457m ²
Site Coverage:	36.32%
Allowable Site Coverage:	50%

Zoning:	Residential R20
Policy:	Residential
Heritage:	No
Bushfire:	No
BAL:	TBA
Acoustic:	No
Sewer:	Inside North
Power:	Overhead
Coastal:	No
Water:	Meter on Site
Wind Rating:	No

19.00	Changes	28.10.24
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14.00	Changes	11.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision	Description	19.01.24

Project North:	
Client	
Briscola PTY LTD	
Project Name	
Childcare Centre	
Project Address	
Lot 85 (#94) West Road, Bassendean	
Drawing Title:	
Ground Floor Plan	
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Project No:	Revision Number:
23019	19.00
Drawing No.:	
07 of 10	

Child / Room Calculations

Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	39.25m ²	3
Activity 2	4-5	28	92.21m ²	3
Activity 3	2-3	20	65.06m ²	4
Activity 4	3-4	20	67.16m ²	2

Total Internal = 80 263.68m² 14
(Min 3.25m² per child) (Min 260m² req)

Total External Play Area = 80 560.56m²
(Min 7m² per child) (Min 560m² req)

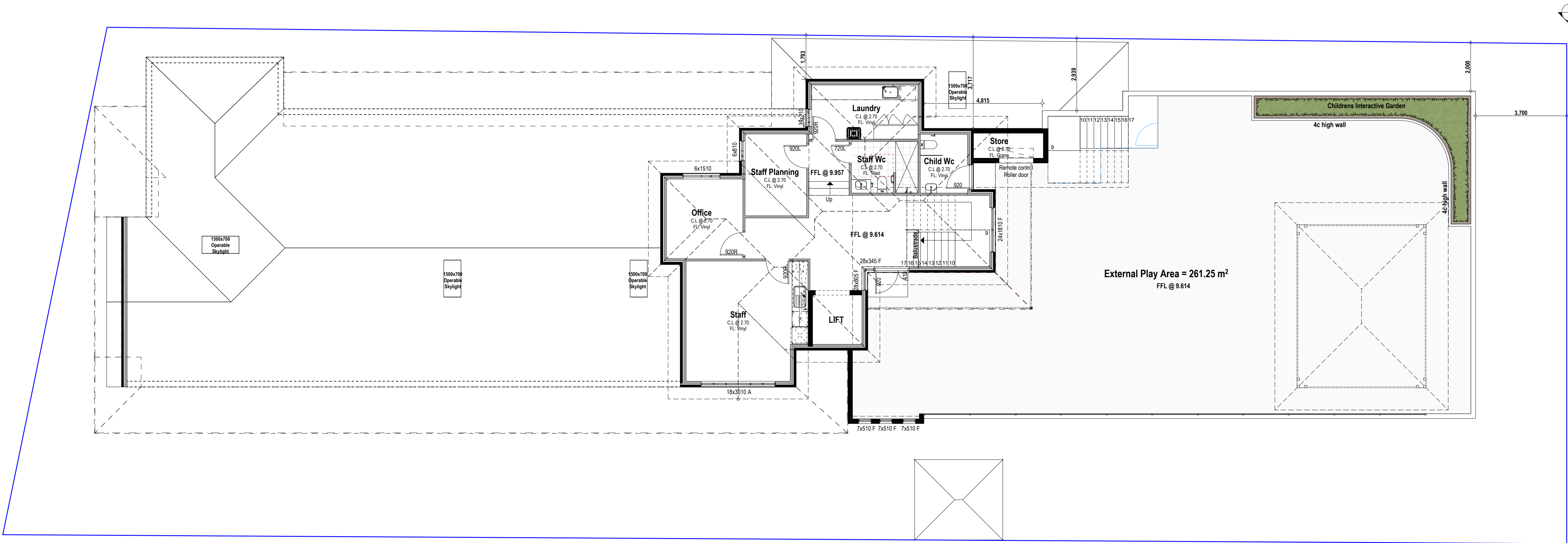
Parking Calculations

(As per local planning policy)

Description	Required
1 bay per employee	14
1 bay per 10 child	8

Required = 22

Provided = 18



Zone	Area	Perim
Stone FF	4.22	8.80
Bin	5.62	11.76
First Floor	111.61	52.30
Childcare Centre	440.77	122.78

562.22 m ²	195,774 mm
Zoning:	Residential R20
Policies:	Residential
Heritage:	No
Bushfire:	No
BAL:	TBA
Acoustic:	No
Sewer:	Inside North
Power:	Overhead
Coastal:	No
Water:	Meter on Site
Wind Rating:	No

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13.00	Changes	26.05.24
12.00	Changes	09.05.24
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Revision	Description	Date
19.01	24	

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

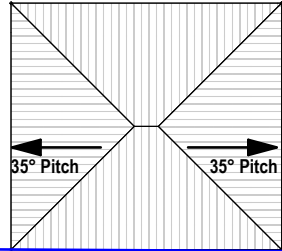
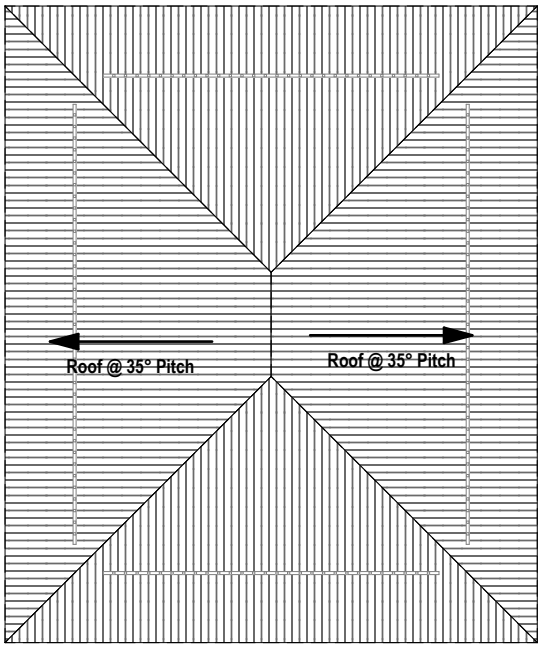
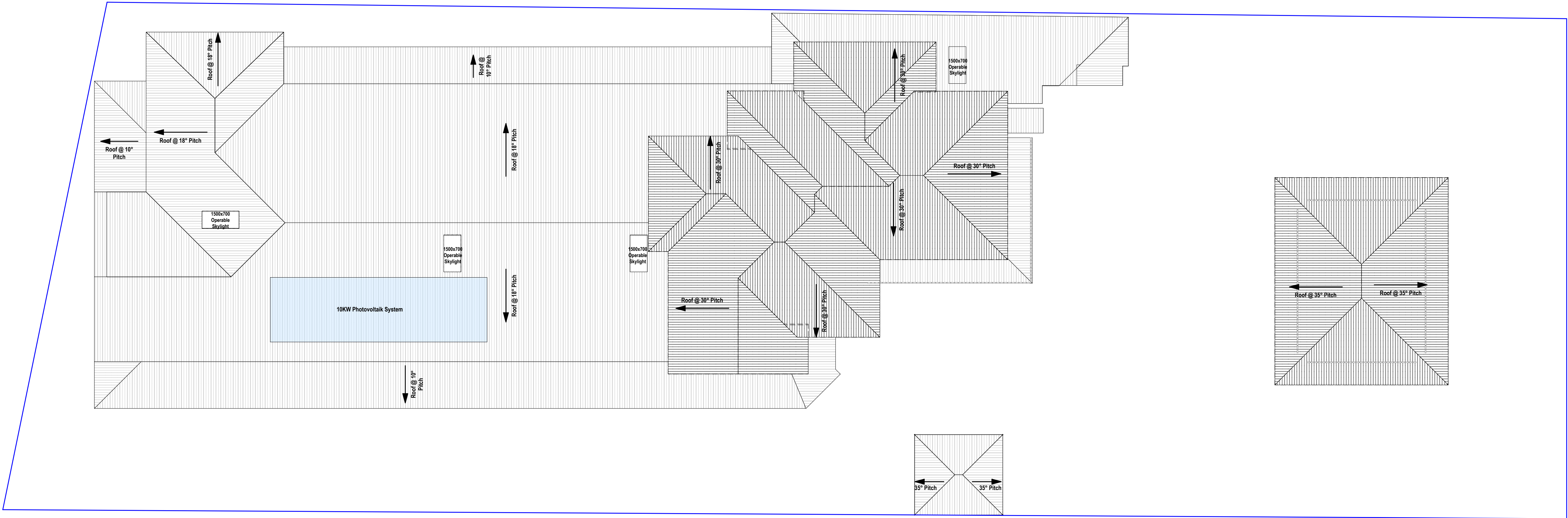
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Lot 85 (#94) West Road, Bassendean

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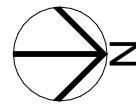
Project No: 23019 Revision Number: 19.00

Drawing No.: 08 of 10



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12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision	Description	19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Roof Plan

Scale: 1:100	Sheet Size: A1
Project No: 23019	Revision Number: 19.00

Drawing No.:
09 of 10



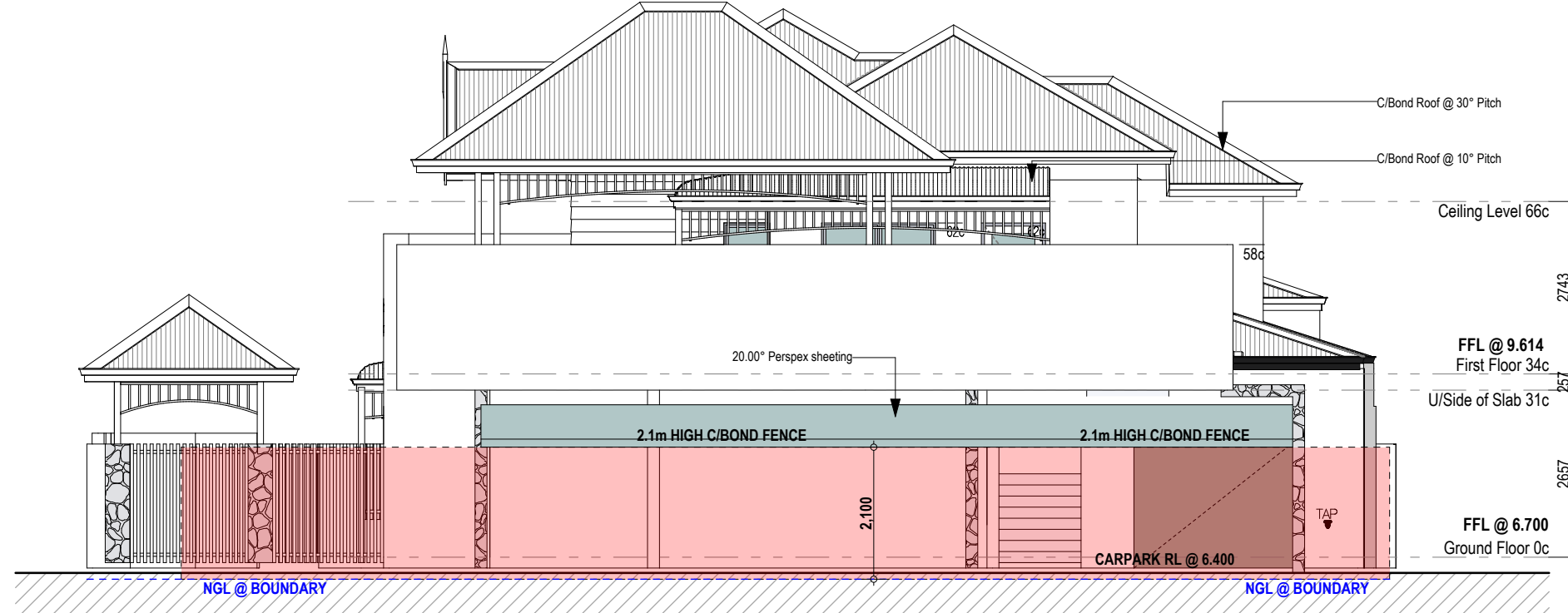
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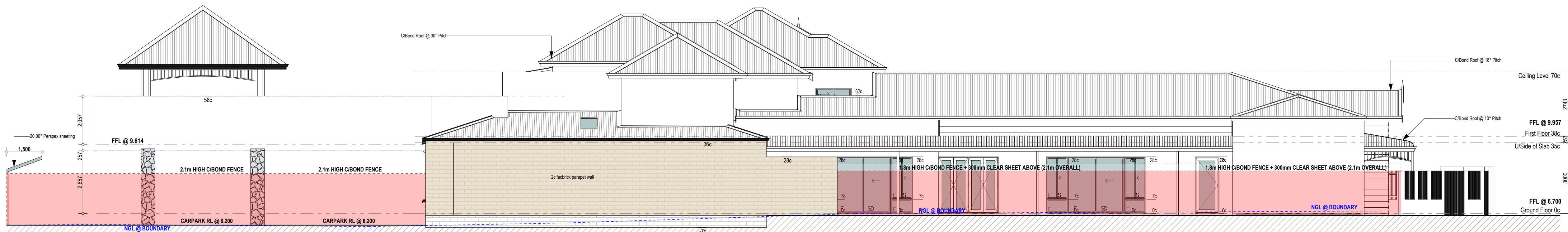
E1 East Elevation
1:100



E2 South Elevation
1:100



E4 North Elevation
1:100

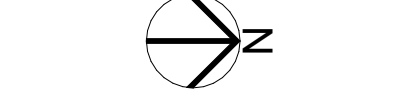


E3 West Elevation
1:100

19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	16.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	13.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24

Revision Description

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Elevations

Scale: 1:100
Project No: 23019
Revision Number: 19.00

Drawing No.:
10 of 10

MB
MACRI
BUILDERS

E: frank@macribuilders.com.au
P: 9444 9929
M: 0457 727 766

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Appendix 2

Transport Planning and Traffic Plans



PARKS AND RECREATION

WATERWAYS

PUBLIC PURPOSE

SHOPPING AREA

ROAD

Hay Street

STREET NAME

RAILWAY

ROAD BRIDGE

LOCATION BOUNDARY

DISTANCE FROM LOCATION

TOWN OF BASSENDEAN

BASSENDEAN

SUBURB NAME

LOCAL AUTHORITY BOUNDARY

LEGEND

			PROJECT: 94 WEST ROAD, BASSENDEAN	DRAWN BY:	<div>Civil & Traffic Engineering Consultants KCTT (Trading as KC Traffic and Transport Pty Ltd) PO Box 1456 Scarborough WA 6922</div> <div>PH: 08 9441 2700 WEB: www.kctt.com.au</div> <div><div>kctt</div><div>PART OF <div>Premise</div></div></div>
			TITLE: LOCALITY PLAN - 800M RADIUS		
A	19-12-2023	ISSUED FOR REVIEW	DRAWING NUMBER: KC01664.000_ S01	N.M.	
No	DATE	AMENDMENT			





PARKS AND RECREATION

WATERWAYS

PUBLIC PURPOSE

SHOPPING AREA

ROAD

Hay Street

STREET NAME

RAILWAY

ROAD BRIDGE

LOCATION BOUNDARY

DISTANCE FROM LOCATION

LOCAL GOVERNMENT NAME

SUBURB NAME

LOCAL AUTHORITY BOUNDARY

BUS ROUTES

103

BUS ROUTE NUMBER

BUS TERMINUS

TRAIN AND BUS TRANSFER

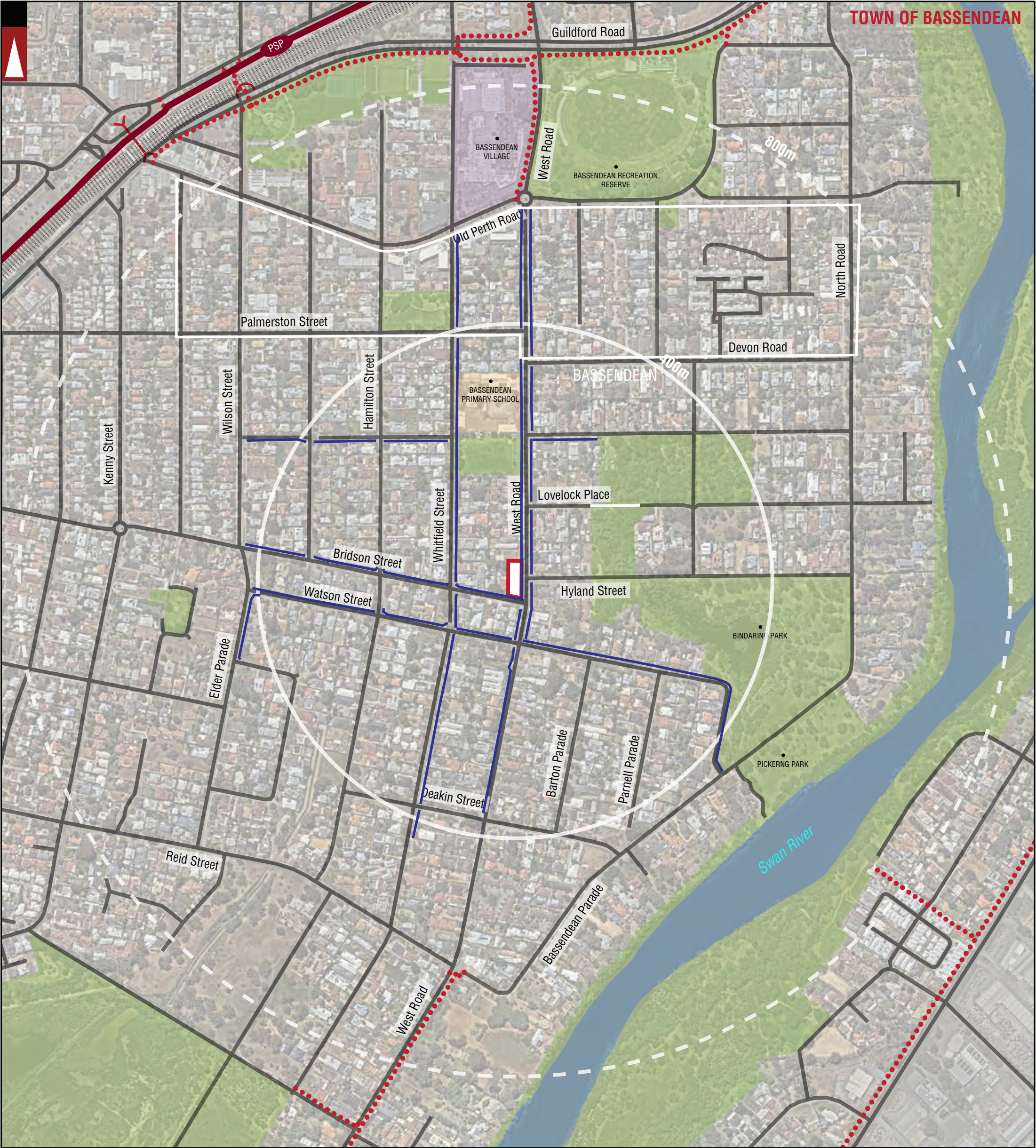
NOTE : FOR MORE INFORMATION REGARDING THE DESCRIPTION OF BUS ROUTES AND THEIR INDICATIVE PEAK AND OFF-PEAK FREQUENCIES REFER TO THE REPORT.

LEGEND

			PROJECT: 94 WEST ROAD, BASSENDEAN	DRAWN BY:	Civil & Traffic Engineering Consultants KCTT (Trading as KC Traffic and Transport Pty Ltd) PO Box 1456 Scarborough WA 6922
			TITLE: PUBLIC TRANSPORT PLAN - 800M RADIUS	N.M.	
A	19-12-2023	ISSUED FOR REVIEW	DRAWING NUMBER: KC01664.000_ S03		
No	DATE	AMENDMENT			

PH: 08 9441 2700
WEB: www.kctt.com.au





PARKS AND RECREATION

WATERWAYS

PUBLIC PURPOSE

SHOPPING AREA

ROAD

Hay Street

STREET NAME

RAILWAY

ROAD BRIDGE

LOCATION BOUNDARY

DISTANCE FROM LOCATION

TOWN OF BASSENDEAN

BASSENDEAN

SUBURB NAME

LOCAL GOVERNMENT NAME

SUBURB NAME

LOCAL AUTHORITY BOUNDARY

PSP

PRINCIPAL SHARED PATH (PSP)

OTHER SHARED PATH (SHARED BY PEDESTRIANS & CYCLISTS)

PEDESTRIAN PATH

WALKING TRAIL

LEGEND

			PROJECT: 94 WEST ROAD, BASSENDEAN	DRAWN BY:	Civil & Traffic Engineering Consultants KCTT (Trading as KC Traffic and Transport Pty Ltd) PO Box 1456 Scarborough WA 6922
			TITLE: PEDESTRIAN PATHS PLAN - 800M RADIUS	N.M.	
A	19-12-2023	ISSUED FOR REVIEW	DRAWING NUMBER: KC01664.000_ S04		
No	DATE	AMENDMENT			

PH: 08 9441 2700
WEB: www.kctt.com.au





PARKS AND RECREATION

WATERWAYS

PUBLIC PURPOSE

SHOPPING AREA

ROAD

Hay Street

STREET NAME

RAILWAY

ROAD BRIDGE

LOCATION BOUNDARY

DISTANCE FROM LOCATION

TOWN OF BASSENDEAN

BASSENDEAN

SUBURB NAME

LOCAL GOVERNMENT NAME

SUBURB NAME

LOCAL AUTHORITY BOUNDARY

5,512

NUMBER OF VEHICLES PER DAY

AM 1145 – 381
PM 1630 – 480

NUMBER OF VEHICLES PER AM PEAK HOUR
NUMBER OF VEHICLES PER PM PEAK HOUR

2014

YEAR

EAST OF HARLOW ROAD

LOCATION

LEGEND

			PROJECT: 94 WEST ROAD, BASSENDEAN	DRAWN BY:	Civil & Traffic Engineering Consultants KCTT (Trading as KC Traffic and Transport Pty Ltd) PO Box 1456 Scarborough WA 6922
			TITLE: EXISTING TRAFFIC COUNTS - 800M RADIUS	N.M.	
A	19-12-2023	ISSUED FOR REVIEW	DRAWING NUMBER: KC01664.000_ S05		
No	DATE	AMENDMENT			

PH: 08 9441 2700
WEB: www.kctt.com.au





LOCATION
BOUNDARY

ROAD
(VARIED WITH ROAD WIDTH)

Lewis Road

ROAD NAME

1,389

Total Expected Traffic Generation from the proposed development

503


Total Expected Traffic Generation from Subject Site on the specific section of road - IN and OUT direction

Traffic Flow IN Direction

Traffic Flow OUT Direction

NOTE: THE PLAN IS COURTESY OF MACRI BUILDERS

LEGEND

D	31-10-2024	PROPOSED LAYOUT AMENDED	PROJECT: 94 WEST ROAD, BASSENDEAN	DRAWN BY:	<div>Civil & Traffic Engineering Consultants KCTT (Trading as KC Traffic and Transport Pty Ltd) PO Box 1456 Scarborough WA 6922</div> <div>PH: 08 9441 2700 WEB: www.kctt.com.au</div> <div><div>kctt</div><div>PART OF </div></div>
C	05-06-2024	PROPOSED LAYOUT AMENDED			
B	30-05-2024	PROPOSED LAYOUT AMENDED	TITLE: TRAFFIC FLOW DIAGRAM	N.M.	
A	19-12-2023	ISSUED FOR REVIEW	DRAWING NUMBER: KC01664.000_ S06		
No	DATE	AMENDMENT			





LOCATION BOUNDARY

ROAD (VARIED WITH ROAD WIDTH)

Lewis Road

ROAD NAME

000

000

000

000

Traffic Flow IN Direction

000

000

000

000

Traffic Flow OUT Direction

1,389

Total Expected Traffic Generation from the proposed development - AM peak

NOTE: THE PLAN IS COURTESY OF MACRI BUILDERS

LEGEND

D	31-10-2024	PROPOSED LAYOUT AMENDED	PROJECT:	94 WEST ROAD, BASSENDEAN	DRAWN BY:	Civil & Traffic Engineering Consultants
C	05-06-2024	PROPOSED LAYOUT AMENDED	TITLE:	TRAFFIC FLOW DIAGRAM - AM PEAK	N.M.	KCTT (Trading as KC Traffic and Transport Pty Ltd)
B	30-05-2024	PROPOSED LAYOUT AMENDED	DRAWING NUMBER:	KC01664.000_ S07		PO Box 1456 Scarborough WA 6922
A	19-12-2023	ISSUED FOR REVIEW				PH: 08 9441 2700
No	DATE	AMENDMENT				WEB: www.kctt.com.au





LOCATION BOUNDARY

ROAD (VARIED WITH ROAD WIDTH)

Lewis Road

ROAD NAME

000

000

000

Traffic Flow IN Direction

000

000

000

Traffic Flow OUT Direction

1,389

Total Expected Traffic Generation from the proposed development - PM peak

NOTE: THE PLAN IS COURTESY OF MACRI BUILDERS

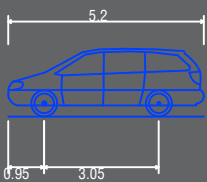
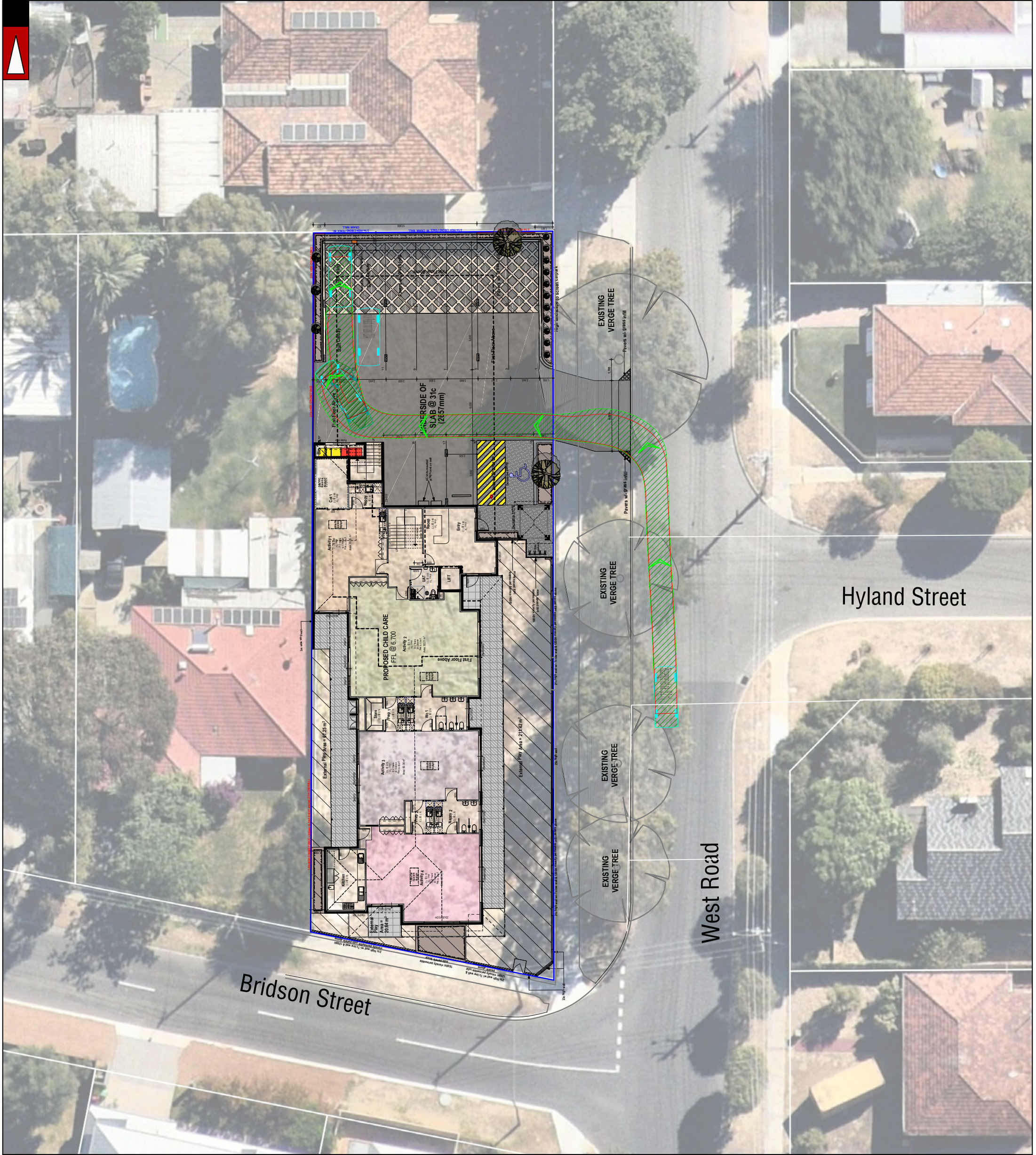
LEGEND

D	31-10-2024	PROPOSED LAYOUT AMENDED	PROJECT: 94 WEST ROAD, BASSENDEAN	DRAWN BY:	<div>Civil & Traffic Engineering Consultants KCTT (Trading as KC Traffic and Transport Pty Ltd) PO Box 1456 Scarborough WA 6922</div> <div>PH: 08 9441 2700 WEB: www.kctt.com.au</div> <div><div>kctt</div><div>PART OF <div>Premise</div></div></div>
C	05-06-2024	PROPOSED LAYOUT AMENDED			
B	30-05-2024	PROPOSED LAYOUT AMENDED	TITLE: TRAFFIC FLOW DIAGRAM - PM PEAK	N.M.	
A	19-12-2023	ISSUED FOR REVIEW	DRAWING NUMBER: KC01664.000_ S08		
No	DATE	AMENDMENT			



Appendix 3

Vehicle Turning Circle Plan

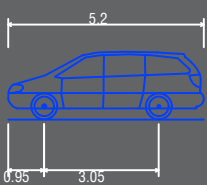


Passenger vehicle (5.2 m)
Overall Length 5.200m
Overall Width 1.940m
Overall Body Height 1.804m
Min Body Ground Clearance 0.295m
Track Width 1.840m
Lock to Lock Time 4.00s
Kerb to Kerb Turning Radius 6.300m

Lot boundary
Wheel Path (Forward Vehicle Motion)
Vehicle Chassis Envelope (Forward Vehicle Motion)
Wheel Path (Reverse Vehicle Motion)
Vehicle Chassis Envelope (Reverse Vehicle Motion)

LEGEND


E	01-11-2024	AMENDED ACCORDING TO RECEIVED COMMENTS	PROJECT: 94 West Road, Bassendean	DRAWN BY:	Civil & Traffic Engineering Consultants PO Box 1456 Scarborough WA 6922 PH: 08 9441 2700 WEB: www.kctt.com.au	
D	31-10-2024	PROPOSED LAYOUT AMENDED	TITLE: Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m)	A.M.		
C	05-06-2024	PROPOSED LAYOUT AMENDED				
B	30-05-2024	PROPOSED LAYOUT AMENDED				
NO	DATE	AMENDMENT	DRAWING NUMBER: KC01664.000_S20			

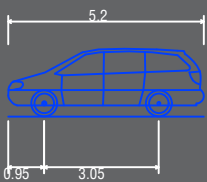


Passenger vehicle (5.2 m)
Overall Length 5.200m
Overall Width 1.940m
Overall Body Height 1.804m
Min Body Ground Clearance 0.295m
Track Width 1.840m
Lock to Lock Time 4.00s
Kerb to Kerb Turning Radius 6.300m

Lot boundary
Wheel Path (Forward Vehicle Motion)
Vehicle Chassis Envelope (Forward Vehicle Motion)
Wheel Path (Reverse Vehicle Motion)
Vehicle Chassis Envelope (Reverse Vehicle Motion)

LEGEND

D	31-10-2024	PROPOSED LAYOUT AMENDED	PROJECT: 94 West Road, Bassendean	DRAWN BY:	Civil & Traffic Engineering Consultants PO Box 1456 Scarborough WA 6922 PH: 08 9441 2700 WEB: www.kctt.com.au	 PART OF
C	05-06-2024	PROPOSED LAYOUT AMENDED	TITLE: Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m)	A.M.		
B	30-05-2024	PROPOSED LAYOUT AMENDED				
A	19-12-2023	ISSUED FOR REVIEW				
NO	DATE	AMENDMENT				



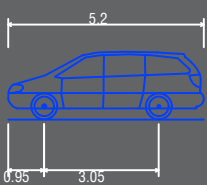
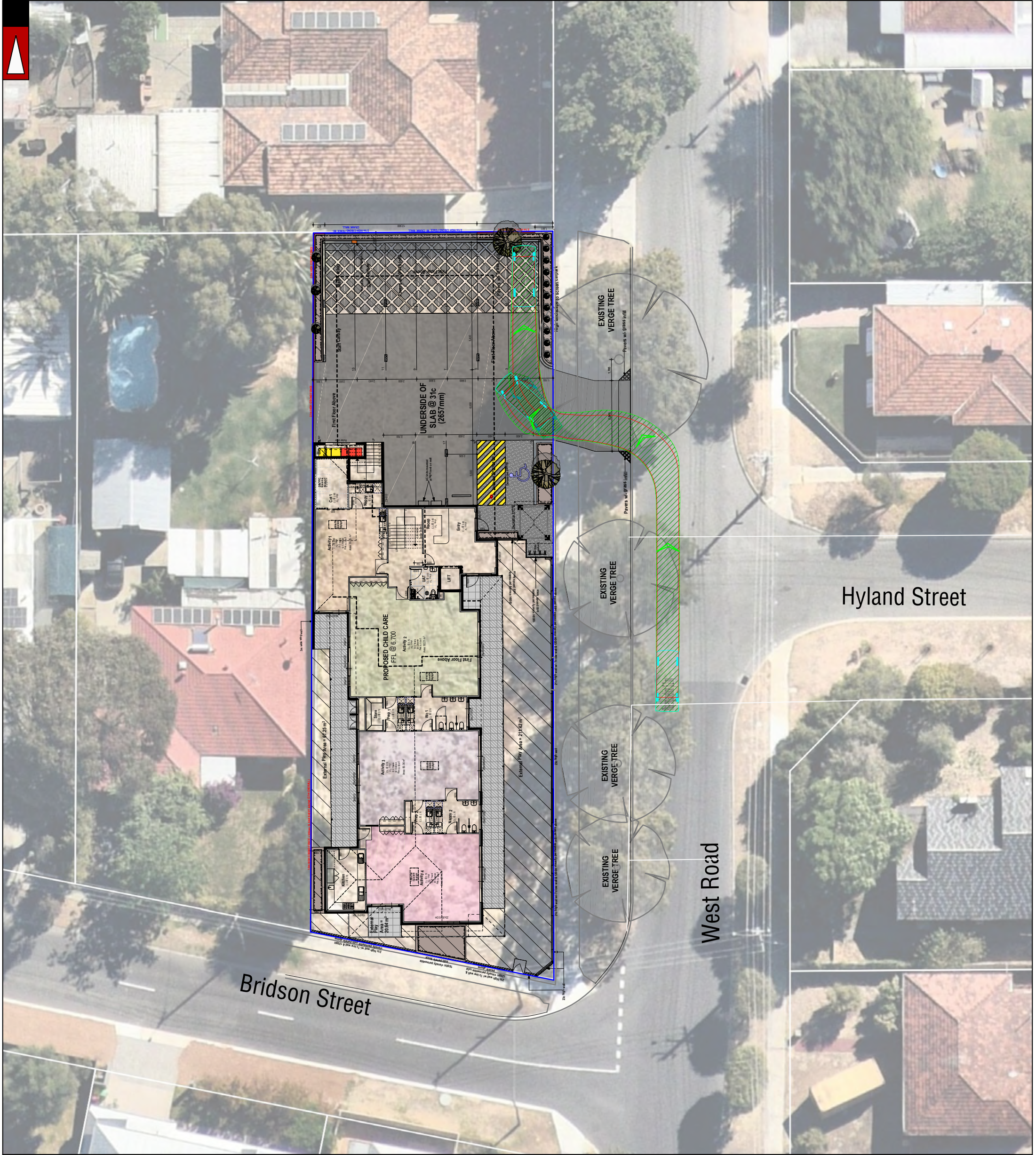
Passenger vehicle (5.2 m)
Overall Length 5.200m
Overall Width 1.940m
Overall Body Height 1.804m
Min Body Ground Clearance 0.295m
Track Width 1.840m
Lock to Lock Time 4.00s
Kerb to Kerb Turning Radius 6.300m

- Lot boundary
- Wheel Path (Forward Vehicle Motion)
- Vehicle Chassis Envelope (Forward Vehicle Motion)
- Wheel Path (Reverse Vehicle Motion)
- Vehicle Chassis Envelope (Reverse Vehicle Motion)

LEGEND

DRAWN BY:	Civil & Traffic Engineering Consultants PO Box 1456 Scarborough WA 6922 PH: 08 9441 2700 WEB: www.kctt.com.au	
A.M.		
kctt		
PART OF		

D	31-10-2024	PROPOSED LAYOUT AMENDED	PROJECT: 94 West Road, Bassendean	DRAWN BY: A.M.
C	05-06-2024	PROPOSED LAYOUT AMENDED	TITLE: Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m)	
B	30-05-2024	PROPOSED LAYOUT AMENDED	DRAWING NUMBER: KC01664.000_S22	
A	19-12-2023	ISSUED FOR REVIEW		
NO	DATE	AMENDMENT		



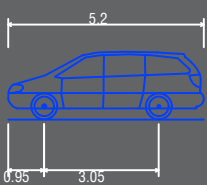
Passenger vehicle (5.2 m)
Overall Length 5.200m
Overall Width 1.940m
Overall Body Height 1.804m
Min Body Ground Clearance 0.295m
Track Width 1.840m
Lock to Lock Time 4.00s
Kerb to Kerb Turning Radius 6.300m

Lot boundary
Wheel Path (Forward Vehicle Motion)
Vehicle Chassis Envelope (Forward Vehicle Motion)
Wheel Path (Reverse Vehicle Motion)
Vehicle Chassis Envelope (Reverse Vehicle Motion)

LEGEND

D	31-10-2024	PROPOSED LAYOUT AMENDED	PROJECT:	DRAWN BY:	Civil & Traffic Engineering Consultants PO Box 1456 Scarborough WA 6922 PH: 08 9441 2700 WEB: www.kctt.com.au
C	05-06-2024	PROPOSED LAYOUT AMENDED	94 West Road, Bassendean		
B	30-05-2024	PROPOSED LAYOUT AMENDED	TITLE:		
A	19-12-2023	ISSUED FOR REVIEW	Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m)		
NO	DATE	AMENDMENT	DRAWING NUMBER:	A.M.	
			KC01664.000_S23		





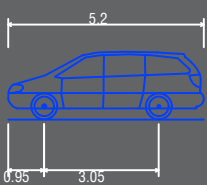
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Overall Length 5.200m
Overall Width 1.940m
Overall Body Height 1.804m
Min Body Ground Clearance 0.295m
Track Width 1.840m
Lock to Lock Time 4.00s
Kerb to Kerb Turning Radius 6.300m

Lot boundary
Wheel Path (Forward Vehicle Motion)
Vehicle Chassis Envelope (Forward Vehicle Motion)
Wheel Path (Reverse Vehicle Motion)
Vehicle Chassis Envelope (Reverse Vehicle Motion)

LEGEND

D	31-10-2024	PROPOSED LAYOUT AMENDED	PROJECT: 94 West Road, Bassendean	DRAWN BY: A.M.	Civil & Traffic Engineering Consultants PO Box 1456 Scarborough WA 6922 PH: 08 9441 2700 WEB: www.kctt.com.au	
C	05-06-2024	PROPOSED LAYOUT AMENDED				
B	30-05-2024	PROPOSED LAYOUT AMENDED				
A	19-12-2023	ISSUED FOR REVIEW				
NO	DATE	AMENDMENT				
			TITLE: Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m)			
			DRAWING NUMBER: KC01664.000_S24			

D	31-10-2024	PROPOSED LAYOUT AMENDED
C	05-06-2024	PROPOSED LAYOUT AMENDED
B	30-05-2024	PROPOSED LAYOUT AMENDED
A	19-12-2023	ISSUED FOR REVIEW
NO	DATE	AMENDMENT




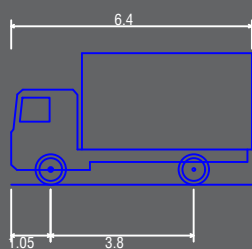
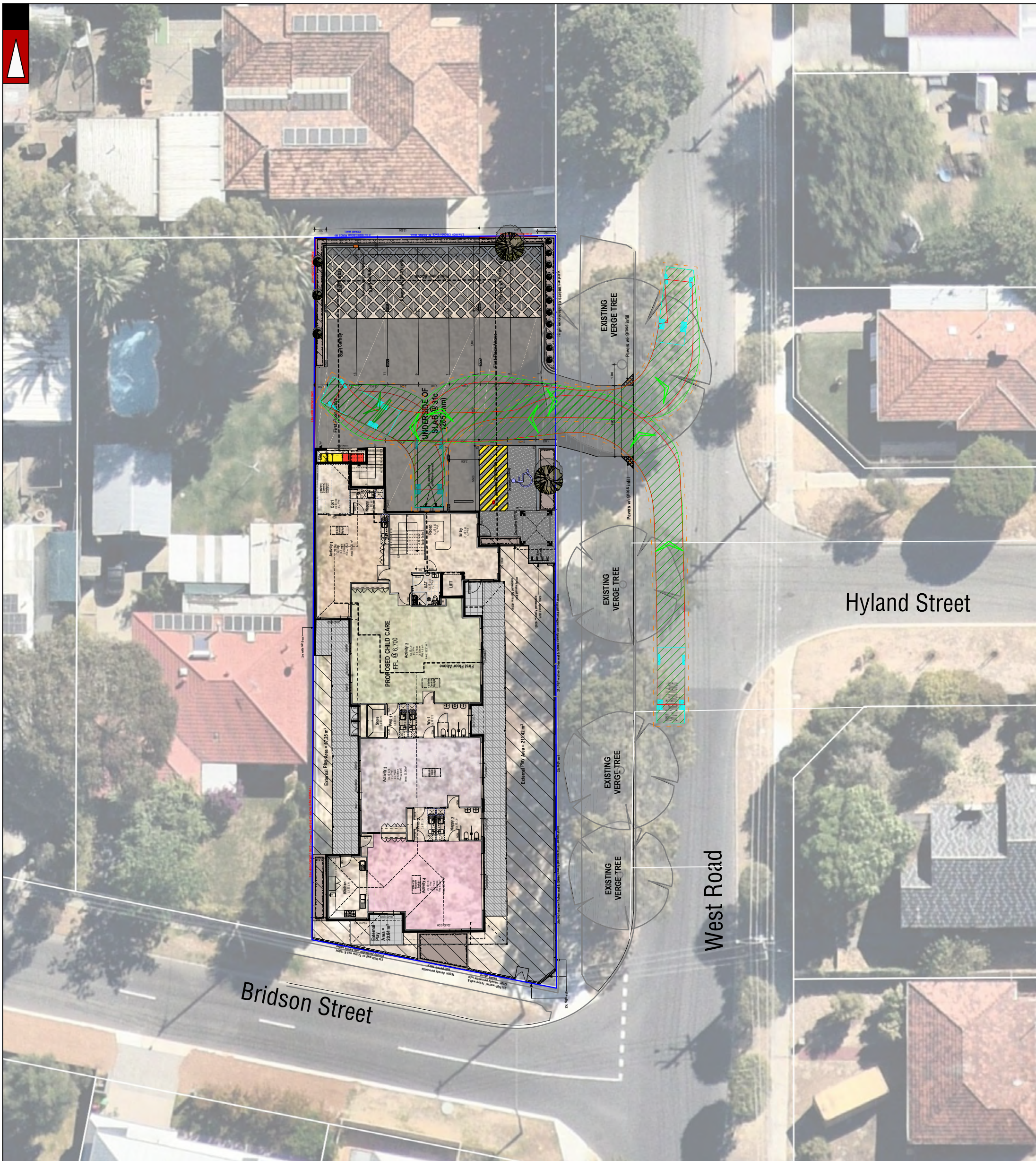
Passenger vehicle (5.2 m)
Overall Length 5.200m
Overall Width 1.940m
Overall Body Height 1.804m
Min Body Ground Clearance 0.295m
Track Width 1.840m
Lock to Lock Time 4.00s
Kerb to Kerb Turning Radius 6.300m

Lot boundary
Wheel Path (Forward Vehicle Motion)
Vehicle Chassis Envelope (Forward Vehicle Motion)
Wheel Path (Reverse Vehicle Motion)
Vehicle Chassis Envelope (Reverse Vehicle Motion)

300mm Body Path Clearance

LEGEND


D	31-10-2024	PROPOSED LAYOUT AMENDED	PROJECT: 94 West Road, Bassendean	DRAWN BY:	Civil & Traffic Engineering Consultants PO Box 1456 Scarborough WA 6922 PH: 08 9441 2700 WEB: www.kctt.com.au	 PART OF
C	05-06-2024	PROPOSED LAYOUT AMENDED	TITLE: Vehicle Turning Circle Plan - B99 Passenger Vehicle (5.2m)	A.M.		
B	30-05-2024	PROPOSED LAYOUT AMENDED				
A	19-12-2023	ISSUED FOR REVIEW				
NO	DATE	AMENDMENT				

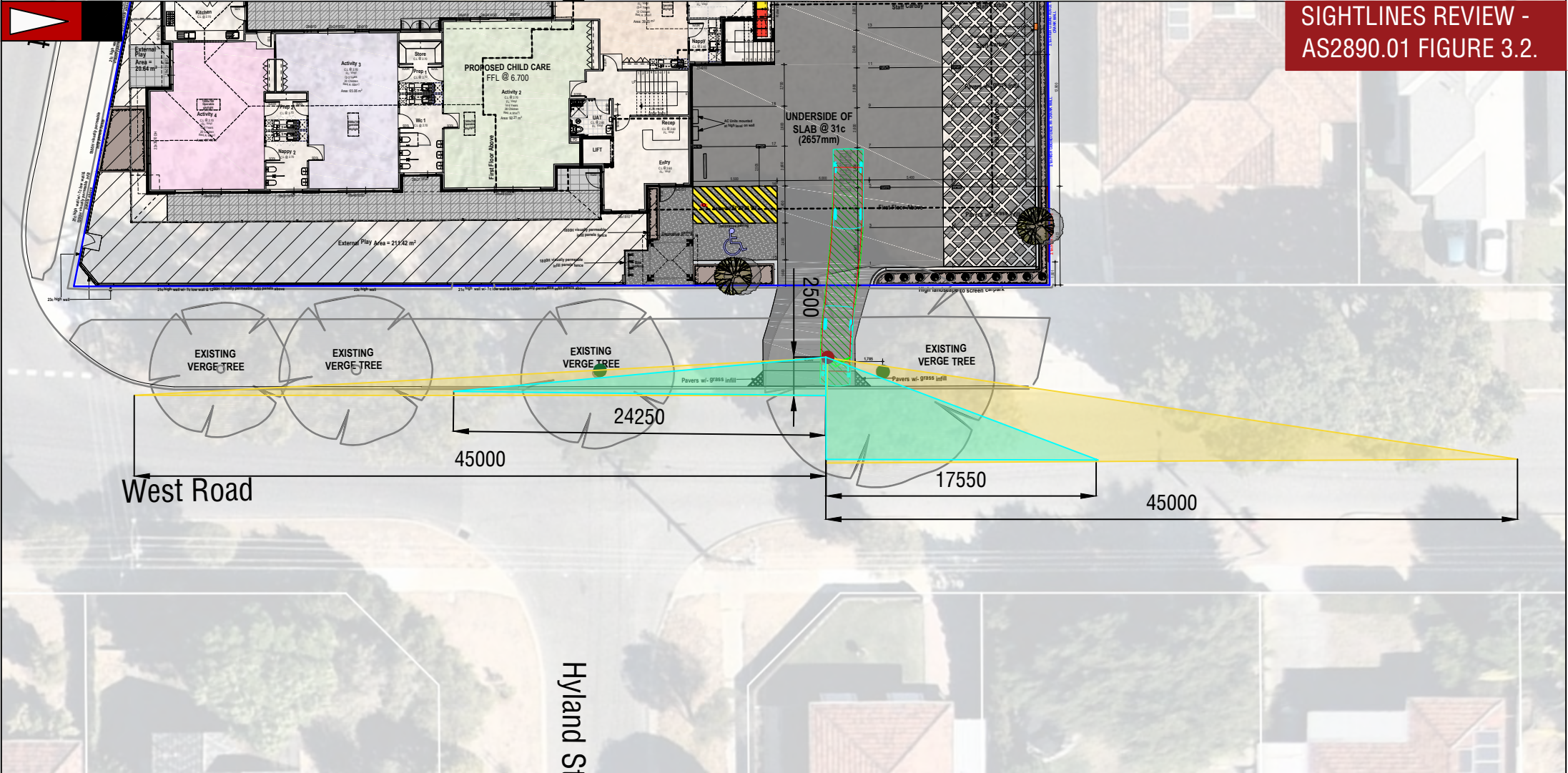
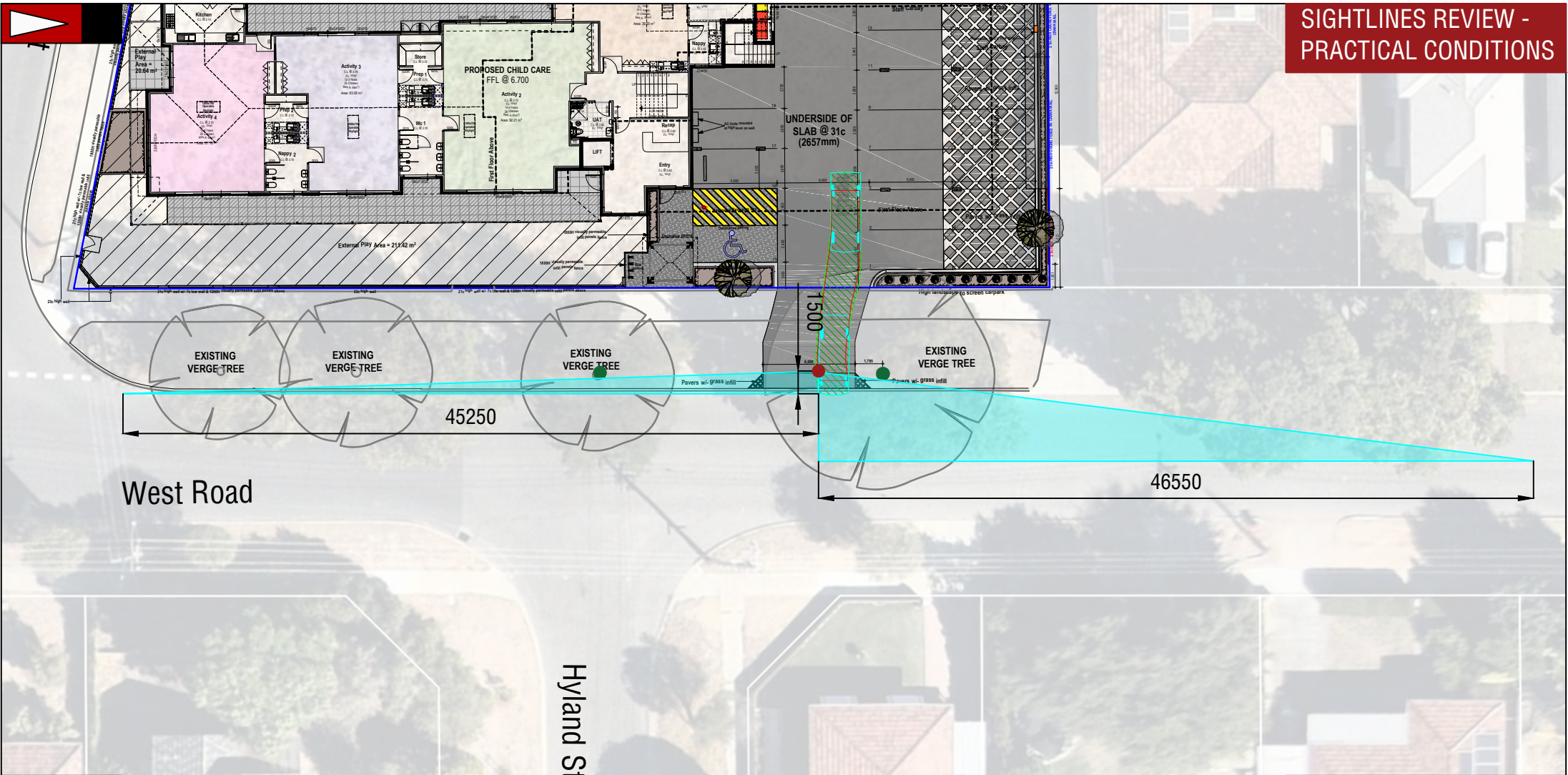


SRV - Small Rigid Vehicle	
Overall Length	6.400m
Overall Width	2.330m
Overall Body Height	3.500m
Min Body Ground Clearance	0.398m
Track Width	2.330m
Lock-to-lock time	4.00s
Curb to Curb Turning Radius	7.100m

- Lot boundary
- Wheel Path (Forward Vehicle Motion)
- Vehicle Chassis Envelope (Forward Vehicle Motion)
- Wheel Path (Reverse Vehicle Motion)
- Vehicle Chassis Envelope (Reverse Vehicle Motion)

LEGEND

E	01-11-2024	AMENDED ACCORDING TO RECEIVED COMMENTS	PROJECT: 94 West Road, Bassendean	DRAWN BY:	<div>Civil & Traffic Engineering Consultants PO Box 1456 Scarborough WA 6922</div> <div>PH: 08 9441 2700 WEB: www.kctt.com.au</div> <div> PART OF</div>
D	31-10-2024	PROPOSED LAYOUT AMENDED	TITLE: Vehicle Turning Circle Plan - Small Rigid Vehicle (6.4m)		
C	05-06-2024	PROPOSED LAYOUT AMENDED			
B	30-05-2024	PROPOSED LAYOUT AMENDED	DRAWING NUMBER: KC01664.000_S26		
NO	DATE	AMENDMENT			



The driveway does not meet AS2890.1 sightline standards due to a tree obstruction.

Practical review takes into account site-specific factors that may not be fully covered by the standards. Drivers typically position themselves optimally before entering the roadway, improving visibility despite the obstruction.

When the driver positions themselves properly, Stopping Sight Distance (SSD) values can still be achieved, ensuring adequate visibility and maintaining safety.

Since the obstruction is a tree, it's a fixed element that drivers can adapt to, unlike temporary obstructions such as parked cars.

Passenger vehicle (5.2 m)

- Overall Length 5.200m
- Overall Width 1.940m
- Overall Body Height 1.804m
- Min Body Ground Clearance 0.295m
- Track Width 1.840m
- Lock to Lock Time 4.00s
- Kerb to Kerb Turning Radius 6.300m

- Lot boundary
- Wheel Path (Forward Vehicle Motion)
- Vehicle Chassis Envelope (Forward Vehicle Motion)
- Wheel Path (Reverse Vehicle Motion)
- Vehicle Chassis Envelope (Reverse Vehicle Motion)

- Achievable sightlines
- Nominal sightlines
- Driver's position
- Tree trunk position

LEGEND

E	31-10-2024	PROPOSED LAYOUT AMENDED	PROJECT:	DRAWN BY:	<div>Civil & Traffic Engineering Consultants PO Box 1456 Scarborough WA 6922</div> <div>PH: 08 9441 2700 WEB: www.kctt.com.au</div> <div>PART OF</div>
D	25-09-2024	AMENDED ACCORDING TO RECEIVED COMMENTS	94 West Road, Bassendean		
C	05-06-2024	PROPOSED LAYOUT AMENDED	TITLE:		
B	30-05-2024	PROPOSED LAYOUT AMENDED	Sightlines Review		
NO	DATE	AMENDMENT	DRAWING NUMBER:	A.M.	
			KC01664.000_S40		

30-09-2024

Rpoint Properties | Rgate Property Group
172 Burswood Road
6100 Burswood WA

Attn: Trish Byrne

Re: 94 West Road, Bassendean – Sightlines review

Trish,

This letter has been prepared in order to examine the proposed crossover position and the achievable sight distances.

AS2890.01 Parking facilities Part 1: Off-street car parking standards prescribe the entering sight distance as per the below:

“Entering sight distance

Unsignalized access driveways shall be located so that the intersection sight distance along the frontage road available to drivers leaving the car park or domestic driveway is at least that shown in Figure 3.2.

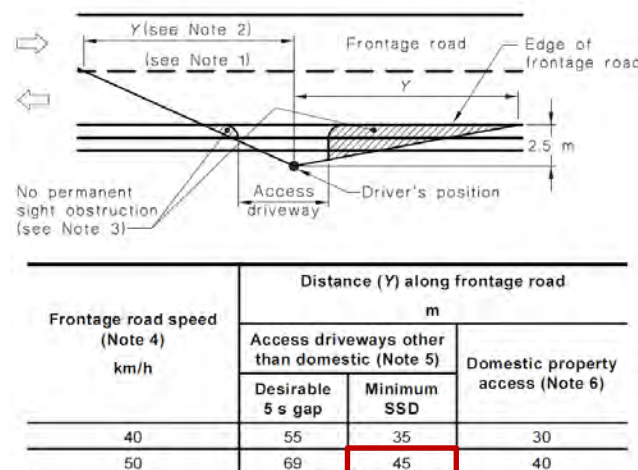


Figure 3.2 Sight Distance Requirements at Access Driveways”

The minimum stopping sight distance for the proposed crossover would be 45m based on a 50km/h speed on West Road. The driveway technically does not meet AS2890.1 sightline standards due to tree obstruction on both sides of the proposed crossover achieving 17.5m to the north and 24.2m to the south; however, this is a common situation in established suburbs with mature street trees.

AS2890.1 standards rely on theoretical models and generalised assumptions, which may not fully consider the specific characteristics of a site.

To provide a more nuanced approach a practical review of sight distances would allow for judgment based on actual site conditions while deviating from the standards if local or unique conditions justify them. Drivers typically position themselves optimally before entering the roadway, improving visibility despite the obstruction.

As mentioned above, this configuration is a commonality in suburbs blessed with mature vegetation. In these situations, the driver will slowly exit and will check the road for on-coming traffic as they are crossing the property

line – the sightline allows for that. As the driveway approaches the carriageway, and prepares to execute turning manoeuvre, they will be able to check the road again as the sightline is uninterrupted.

When the driver positions themselves properly, Stopping Sight Distance (SSD) values of 45m both to the north and the south can be achieved, ensuring adequate visibility and maintaining safety. The only location where the sightline is partially obstructed is when driver is positioned exactly 2.5m from the edge of the carriageway.

Since the obstruction is a tree, it's a fixed element that drivers can adapt to, unlike temporary obstructions such as parked cars, where different sections of sightline may be obstructed depending on another driver's parking skills.

Drawing S40 provides a detailed comparison between the practical sightline assessment and the standard AS2890.1 sightline review and demonstrates that with the rolling approach, the drivers can easily spot on coming traffic on West Road and are highly unlikely to be exposed to any undue safety risks.

Regards,

Marina Kleyweg

Director | Principal of Traffic and Transport

Ana Marijanovic

Senior Traffic Engineer

Prepared by:

ABN

Postal address:

Website:

KCTT (Trading as KC Traffic and Transport Pty Ltd)

35 148 970 727 |

PO Box 1456, Scarborough WA 6922 |

www.kctt.com.au |

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30-08-2024

Rpoint Properties | Rgate Property Group
172 Burswood Road
6100 Burswood WA

Attn: Trish Byrne

Re: 94 West Road, Bassendean - Crossover review in Safe System Framework

Trish,

This letter has been prepared in order to examine the proposed crossover position and its alternatives. The current position of the crossover does not fully comply with the AS2890.01 Parking facilities Part 1: Off-street car parking standards as the proposed location of the crossovers is encroaching into the restricted area from the intersection of West Road and Hyland.

Given its proximity to two intersections of West Road with Hyland Street and Bridson Street, there are limited options for placing of the crossover/driveway. KCTT believe that the proposed location is the best possible, having in mind the necessity of retaining existing trees, the position of nearby intersections and respective traffic volumes on these intersections.

The access point has been assessed through the Safe System Framework for three check cases identified through analysis:

- Currently Proposed Crossover Location – West Road north of Hyland Street (31 / 448)
- Alternative Crossover Location 1 – West Road north of Hyland Street abutting Lot 83 West Road (35 / 448)
- Alternative Crossover Location 2 – West Road north of Hyland Street between Option 1 & 2 (39 / 448)

The Safe System Matrix analysis yielded a score between 31 and 39 out of a possible 448, reflecting strong alignment with Safe System principle. The lower the score, the better the compliance with these principles, indicating that none of the alternative crossover positions would significantly impact safety.

Notably, the currently proposed crossover position achieved the lowest score, marking it as the safest and most optimal solution in terms of compliance with Safe System objectives, as the other two options directly oppose residential crossover and create unfavourable conditions for pedestrians (crossing two adjacent crossovers).

In summary, the slight departure from the AS2890.01 recommended positioning to retain a significant tree will not result in a situation that is less safe, per Safe System Framework, than other two compliant options that would necessitate tree removal.

On subsequent pages are details of our findings. If you have any queries, please don't hesitate to contact us.
Regards,

Marina Kleyweg

Director | Principal of Traffic and Transport

Ana Marijanovic

Senior Traffic Engineer

1. Traffic Engineering Letter

1.1 Proposal

Rpoint Properties engaged KCTT to prepare a Traffic Engineering Letter (TEL) for the proposed childcare centre at 94 West Road, Bassendean. The proposed development will have capacity for 80 children.

The subject site will have access to/from West Road, north of Hyland Street.

This letter will primarily address the proposed crossover position.

Given this is a traffic engineering letter addressing a specific matter, key elements regarding this subject will be discussed in detail (as available) while the other concern may be covered in a separate report as required.

1.2 Location

Street Number	94
Road Name	West Road
Suburb	Bassendean
Description of Site	The subject site is currently vacant. The proposed development is a Childcare centre with a capacity for 80 children and 13 staff members.

1.3 Traffic Volumes

Road Name	Location of Traffic Count	Vehicles Per Day (VPD)	Vehicles per Peak Hour (VPH)				Heavy Vehicle %	Date of Traffic Count	85% speed
			AM Peak Time	AM Peak VPH	PM Peak Time	PM Peak VPH	If HV count is Not Available, are HV likely to be in higher volumes than generally expected?		
			Monday - Friday Average						
West Road	North of Hyland Street	2,522	08:00 – 243		15:00 – 225		5%	July 2024	47.8
Hyland Street	East of West Street	415	07:00 – 30		16:00 – 37		9%	July 2024	43.5
Bridson Street	East of Elder Parade	2,673	07:45 – 280		15:00 – 250		5.2%	2021/2022	55.8
Monday - Sunday Average									
West Road	North of Hyland Street	2,438	08:00 – 207		16:00 – 214		4%	July 2024	47.5
Hyland Street	East of West Street	387	09:00 - 26		16:00 – 36		7%	July 2024	43.1

1.4 Vehicular Crash Information

Is Crash Data Available on Main Roads WA website? YES

Crash Reports

Area Details Centre: (-31.910, 115.954) (Shape)

Date Range 01/01/19 to 31/12/23

Severity	Date	Time	Nature	RUM Code	Crash Number
1110007 - WEST RD					
Hospital	2020-07-05	14:30	Non Collision	77	2020741374
Hospital	2020-07-05	15:55	Non Collision	75	2020728822
Total:2					

Date Sun 05/07/20 15:55

Severity **Hospital**

Number of Vehicles 1

Road 1110007 (WEST RD) (SLK: 1.26) S

Location Description West Rd 20m North of Bridson St

Crash Description REGOT travelling west on Hyland Rd turned left into West Rd and was travelling south at approximately 30km/h, at which point his front brake suddenly locked on causing his front wheel to lock up, which caused Driver T to fall onto the road surface from his bike. Driver T stated he had completed repairs to his brake system prior to the crash and was taking the bike for a test run to "bed the brakes in". Driver T conveyed by SJA to RPH however at material time injuries were BTB minor in severity. Follow up contact was finally able to be made on 02/08/20 at which point extent of injuries were known and retrospective 1-18 report completed.

Crash Nature Non Collision

RUM 75:Off Path On Straight: Lost Control On Cway

Light Condition Daylight

Road Condition Dry

Road Alignment Straight

Date Sun 05/07/20 14:30

Severity **Hospital**

Number of Vehicles 1

Road 1110007 (WEST RD) (SLK: 1.24) S

Location Description West Rd at Bridson St

Crash Description I have been working on my bike, I replaced the rear breaks and was taking it for a test drive. I was going for a gentle ride to test them and they worked perfect but I felt the front dragging. I was taking off from the corner going on ~20km/hr and my front wheel locked up causing my back wheel to keep spinning but not the front one. It caused my back tire to make the bike spin sideways and I fell off.

Crash Nature Non Collision

RUM 77:Loss Of Control: Right Turn - Intx

Light Condition Daylight

Road Condition Dry

Road Alignment Straight

Based on the crash description it seems the same crash has been represented twice with a different RUM code. The crash appears to be an isolated incident directly related to the mechanical state of the motorcycle rather than any deficiency or danger inherent to the road or location. Therefore, this incident alone does not suggest that there is a broader safety issue at this location.

1.5 Crossover location

Currently proposed crossover location

The current position of the crossover does not fully comply with the AS2890.01 Parking facilities Part 1: Off-street car parking standards as the proposed location of the crossovers is positioned within the 6m from the intersection of West Road and Hyland tangent point to the north.

The extract from AS2890.01 below outlines the required position of the crossover:

"3.2.3 Access driveway location

Driveway Categories 1 and 2 At unsignalized intersections of sub-arterial, collector or local streets with each other or with an arterial road, access driveways in Categories 1 and 2 (see Table 3.1) shall not be located in the sections of kerb shown by heavy lines in Figure 3.1. This requirement shall not apply to accesses to domestic driveways in the kerb section opposite the entering road at any intersection including signalized intersections. Furthermore, it shall not apply to any access driveway serving a property which would otherwise be denied access due to the physical impossibility of meeting the requirement."

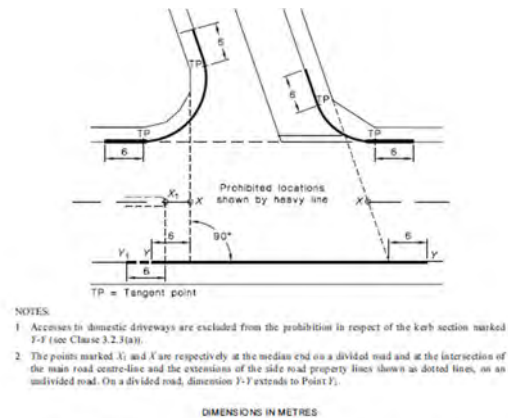


FIGURE 3.1 PROHIBITED LOCATIONS OF ACCESS DRIVEWAYS

However, it was understood that the council is aware of this non-compliance and prioritises the preservation of the existing trees along West Road.

Given its proximity to two intersections of West Road with Hyland Street and Bridson Street, there are limited options for placing of the crossover/driveway. KCTT believe that the proposed location is the best possible, having in mind the necessity of retaining existing trees, the position of nearby intersections and respective traffic volumes on these intersections.

Similar arrangement is provided at Bassendean Primary School carparking entrance at the intersection of West Road & Harcourt Street 200m north of the proposed development.

The figure (Figure 1) below illustrates the currently proposed crossover in relation to the existing trees that are aimed to be preserved as well as the limitations from the AS2890.01. The proposed crossover is located just north of the existing West Road & Hyland Street intersection encroaching into the restricted location within the 6m from the intersection tangent point.

Other parameters of the crossover are compliant to the AS2890.01.

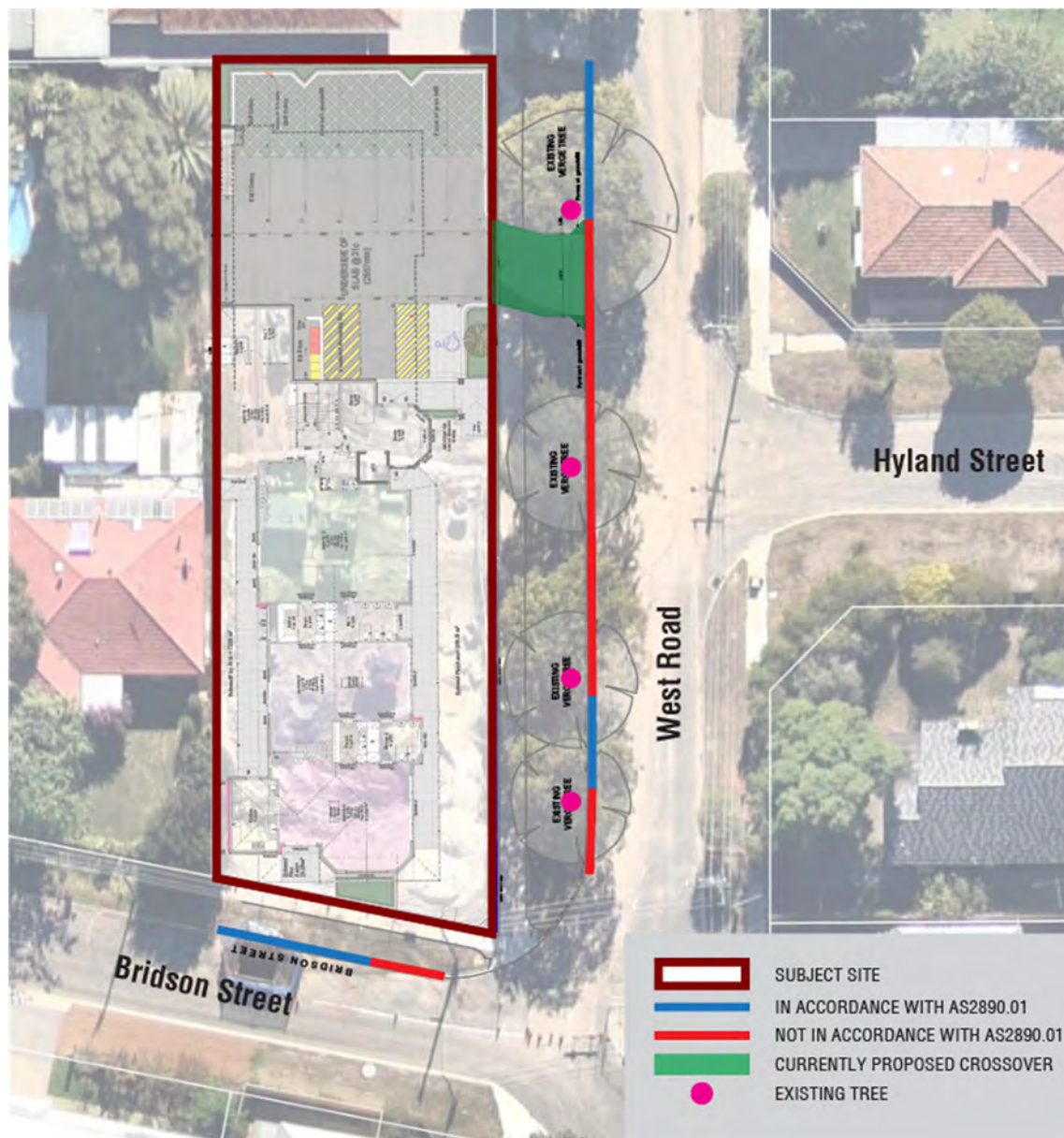


Figure 1 - Proposed crossover location (illustrative only)

Constraints

Figure below (Figure 2) highlights the possible crossover positions marked in blue.

The blue line south of Hyland Street represents the only other permissible location for a crossover on West Road. However, this alternative location also faces the same challenge—trees would need to be removed to accommodate the crossover.

Another option is to place the crossover on Bridson Street, which would meet the relevant standards. However, this location is close to the intersection of West Road and Bridson Street, where traffic is heavier (about 3,800 vehicles per day) compared to the intersection of West Road and Hyland Street (about 2,700 vehicles per day). This could potentially raise safety concerns.

Therefore, other crossover options north of Hyland Street have been assessed with Safe Systems Framework.



Figure 2 - Potential locations for crossover (illustrative only)

1.6 Safe Systems Framework Assessment

1.6.1 Setting the context

Prompt	Comment
What is the reason for the project ? Is there a specific crash type risk? Is it addressing specific issues such as poor speed limit compliance, road access, congestion, future traffic growth, freight movement, amenity concerns from the community, etc.	<ul style="list-style-type: none"> The proposed development crossover position is not compliant to relevant standards. As there are spatial constraints to providing the adequate position the safe system assessment will be undertaken to identify potential hazards. Currently no casualty crashes in the vicinity (1 hospital crash). 3 check cases will be provided as previously identified.
What is the function of the road? Consider location, roadside land use, area type, speed limit, intersection type, presence of parking, public transport services and vehicle flows. What traffic features exist nearby (e.g. upstream and downstream)?	<ul style="list-style-type: none"> An undivided one lane per direction Local Distributor Road with a 50km/h speed limit and medium flows (1,000 – 5,000 VPD) – West Road & Bridson Street. An undivided one lane per direction Access Road with a 50km/h speed limit and low flows (<1,000 VPD) – Hyland Street. Unsignalised full movement intersections of West Road with Hyland Street and Bridson Street, directly fronting the subject site. Urban residential land use surrounding the development. Bus service on West Road and Hyland Street, closest stop located 90m to south.
What is the speed environment? What is the current speed limit? Has it changed recently? Is it similar to other roads of this type? How does it compare to Safe System speeds? What is the acceptability of lowering the speed limit at this location?	<ul style="list-style-type: none"> The speed limit on West Road, Hyland Street and Bridson Street is 50km/h which is consistent with the other similar roads. 85% speed on West Road and Hyland Street is lower than the speed limit. 85% speed on Bridson Street is higher than the speed limit.
What road users are present? Consider the presence of elderly, school children and cyclists. Also note what facilities are available to vulnerable road users (e.g. signalised crossings, bicycle lanes, school zone speed limits, etc.).	<ul style="list-style-type: none"> Kindergarten - children to be driven to site. No cycling paths in the vicinity, Bridson Street is marked as good road riding environment. School Zone Speed Limit on West Road 100m north of the development site: 40km/h - Operating times 7:30 am to 9:00 am and 2:30 pm to 4:00 pm on School Days Safe crossing to the other side of the road located at both intersections.
What is the vehicle composition? Consider the presence of heavy vehicles (and what type), motorcyclists and other vehicles using the roadway.	<ul style="list-style-type: none"> 4-9% HV Light and medium sized vehicles (C1-C5)

1.6.2 Safe System matrix

Currently Proposed Crossover Location – West Road north of Hyland Street							
	Run-off-road	Head-on	Intersection	Other (Rear end)	Pedestrian	Cyclist	Motorcyclist
Exposure	Moderate AADT volume	Moderate AADT volume	Moderate AADT volume Low volume at crossover	Moderate AADT volume	Moderate pedestrian volume	Low cyclist volume	Low motorcyclist volume
	2/4	2/4	2/4	2/4	2/4	1/4	1/4
Likelihood	Presence of intersection Low clear zone One lane per direction	One lane per direction Movements/ conflict points minimal for HO crash	Low turning movements No turning lanes One lane per direction	Low turning movements No turning lanes One lane per direction	Pedestrian path across crossover 6.0m crossing Crossing facilities at intersection	No cycling lane No crossing facilities	No delineation Well surfaced Straight road
	2/4	1/4	2/4	2/4	2/4	1/4	1/4
Severity	85% speed under limit No barriers Trees and poles to hit	85% speed under limit	85% speed under limit Low speed at crossover	85% speed under limit Low speed at crossover	85% speed under limit Low speed at crossover Crossing pedestrian path Crossing facilities at intersection	85% speed under limit	85% speed under limit No barriers Trees and poles to hit
	2/4	1/4	1/4	1/4	2/4	2/4	3/4
Product	8/64	2/64	4/64	4/64	8/64	2/64	3/64
TOTAL				31/448			

Alternative Crossover Location 1 – West Road north of Hyland Street abutting Lot 83 West Road							
	Run-off-road	Head-on	Intersection	Other (Rear end)	Pedestrian	Cyclist	Motorcyclist
Exposure	Moderate AADT volume	Moderate AADT volume	Moderate AADT volume Low volume at crossover	Moderate AADT volume	Moderate pedestrian volume	Low cyclist volume	Low motorcyclist volume
	2/4	2/4	2/4	2/4	2/4	1/4	1/4
Likelihood	Presence of intersection Low clear zone One lane per direction	One lane per direction Movements/ conflict points minimal for HO crash	Low turning movements Adjacent to another crossover No turning lanes One lane per direction	Low turning movements Adjacent to another crossover No turning lanes One lane per direction	Pedestrian path across two crossovers 12.0m crossing Crossing facilities at intersection	No cycling lane No crossing facilities	No delineation Well surfaced Straight road
	2/4	1/4	3/4	3/4	3/4	1/4	1/4
Severity	85% speed under limit No barriers Trees and poles to hit	85% speed under limit	85% speed under limit Low speed at crossover	85% speed under limit Low speed at crossover	85% speed under limit Low speed at crossover Crossing pedestrian path Crossing facilities at intersection	85% speed under limit	85% speed under limit No barriers Trees and poles to hit
	2/4	1/4	1/4	1/4	2/4	2/4	3/4
Product	8/64	2/64	6/64	6/64	12/64	2/64	3/64
TOTAL				39/448			

Alternative Crossover Location 2 – West Road north of Hyland Street between Option 1 & 2							
	Run-off-road	Head-on	Intersection	Other (Rear end)	Pedestrian	Cyclist	Motorcyclist
Exposure	Moderate AADT volume	Moderate AADT volume	Moderate AADT volume Low volume at crossover	Moderate AADT volume	Moderate pedestrian volume	Low cyclist volume	Low motorcyclist volume
	2/4	2/4	2/4	2/4	2/4	1/4	1/4
Likelihood	Presence of intersection Low clear zone One lane per direction	One lane per direction Movements/ conflict points minimal for HO crash	Low turning movements One opposing crossover No turning lanes One lane per direction	Low turning movements One opposing crossover No turning lanes One lane per direction	Pedestrian path across crossover 6.0m crossing Crossing facilities at intersection	No cycling lane No crossing facilities	No delineation Well surfaced Straight road
	2/4	1/4	3/4	3/4	2/4	1/4	1/4
Severity	85% speed under limit No barriers Trees and poles to hit	85% speed under limit	85% speed under limit Low speed at crossover	85% speed under limit Low speed at crossover	85% speed under limit Low speed at crossover Crossing pedestrian path Crossing facilities at intersection	85% speed under limit	85% speed under limit No barriers Trees and poles to hit
	2/4	1/4	1/4	1/4	2/4	2/4	3/4
Product	8/64	2/64	6/64	6/64	8/64	2/64	3/64
TOTAL				35/448			

The total score of varies from 31 - 39 is achieved out of a possible 448 for the check cases and considers the safer speeds, safer roads and roadsides pillars. The closer the score is to zero, the more the project in question is in alignment with Safe System principles. The low score on the Safe System Matrix demonstrates strong compliance with Safe System objectives across all check cases, indicating that none of the alternative crossover positions would significantly compromise safety.

However, it's important to emphasise that the currently proposed crossover position achieves the lowest Safe System score, making it the optimal solution within this framework. This underscores the value of maintaining the proposed position, as it offers the best alignment with Safe System principles and ensures the highest level of safety for road users. Table below shows user exposure Safe System matrix scoring system.

Road user exposure						
	0 = there is no exposure to a certain crash type.	1 = volumes of vehicles that may be involved in a particular crash type are particularly low, and therefore exposure is low.	2 = volumes of vehicles that may be involved in a particular crash type are moderate, and therefore exposure is moderate.	3 = volumes of vehicles that may be involved in a particular crash type are high, and therefore exposure is high.	4 = volumes of vehicles that may be involved in a particular crash type are very high, or the road is very long, and therefore exposure is very high.	
	This might mean there is no side flow or intersecting roads, no cyclists, no pedestrians, or motorcyclists).	For run-of-road, head-on, intersection and 'other' crash types, AADT is < 1 000 per day. For cyclist, pedestrian and motorcycle crash types, volumes are < 10 units per day.	For run-of-road, head-on, intersection and 'other' crash types, AADT is between 1 000 and 5 000 per day. For cyclist, pedestrian and motorcycle crash types, volumes are 10–50 units per day.	For run-of-road, head-on, intersection and 'other' crash types, AADT is between 5 000 and 10 000 per day. For cyclist, pedestrian and motorcycle crash types, volumes are 50–100 units per day.	For run-of-road, head-on, intersection and 'other' crash types, AADT is > 10 000 per day. For cyclist, pedestrian and motorcycle crash types, volumes are >100 units per day.	

1.6.3 Additional Safe System components

Road user

Are road users likely to be alert and compliant, or are there factors that might influence this?

What are the expected compliance and enforcement levels (alcohol/drugs, speed, road rules, and driving hours) and what is the likelihood of driver fatigue? Can enforcement of these issues be conducted safely?

Are there special road uses (e.g. entertainment precincts, elderly, children, on-road activities), distraction by environmental factors (e.g. commerce, tourism), or risk-taking behaviours?

- Relatively young drivers driving children, expected to be alert
- Drivers expected to be compliant.
- Driver fatigue not expected to be a factor.
- Kindergarten children to be driven to site.
- No other special road uses expected.

Vehicle

What level of alignment is there with the ideal of safer vehicles?

Are there factors which might attract large numbers of unsafe vehicles? Is the percentage of heavy vehicles too high for the proposed/existing road design?

Are there enforcement resources in the area to detect non-roadworthy, overloaded or unregistered vehicles and thus remove them from the network? Can enforcement of these issues be conducted safely?

Has vehicle breakdown been catered for?

- The proposed development is expected to attract predominantly light passenger vehicles.
- Given that children are transported in passenger vehicles, there is no reason to believe that unsafe vehicles will be particularly attracted by this development.
- No known vehicle enforcement.

Post-crash care

Are there issues that might influence safe and efficient post-crash care in the event of a severe injury?

Do emergency and medical services operate as efficiently and rapidly as possible?

Are other road users and emergency response teams protected during a crash event? Are drivers provided the correct information to address travelling speeds on the approach and adjacent to the incident? Is there reliable information available via radio, VMS etc.?

Is there provision for e-safety (i.e. safety systems based on modern information and communication technologies, C-ITS)?

- Verge and crossovers may be used for emergency stops.
- St John of God Midland Public Hospital, located at approximately 6 kilometres away.

PROPOSED CHILD CARE CENTRE – 94 WEST RD, BASSENDEAN SITE ACCESS REVIEW

TECHNICAL NOTE 1

4.10.2024

1. INTRODUCTION

The Town of Bassendean (the Town) has received a development application for a Child Care Centre to be located at 94 West Road, Bassendean, which has raised some concerns with respect to sight distance and proposed driveway location.

Donald Veal Consultants (DVC) has conducted an independent review of the following issues:

1. whether the sightlines shown on page 50 of the Transport Impact Statement are correctly shown;
2. whether there is agreement with the suggestion that the location of the proposed crossover, whilst non-compliant with AS2890.01, represents a safe option for vehicular access to the site as demonstrated by the Safe System Assessment shown on page 7 of TIS addendum; and
3. whether an option to remove the tree and realign the crossover would provide a compliant outcome.

This technical note presents our findings and recommendations for consideration by the Town.

2. DOCUMENT REVIEW

DVC reviewed the documents provided, comprising:

- Transport Impact Statement - Revision C, dated 7.06.2024 and prepared by KCTT;
- TIS Addendum letter dated 30.08.2024; and
- Revised plans - Revision 16, dated 4.09.2024; and
- Sightlines Review letter dated 30.09.2024.

The TIS report (page 13) identifies compliance with achieving a minimum of 45m of Safe Stopping Distance (SSD). However, the Sightlines Review drawing KC01664.000_S40 revision C dated 5.06.2024, whilst showing compliant sight distances, has not taken the measurements from the proscribed setback from the kerb line. The measurements are taken from a position further forward to compensate for the trunks of trees on the verge.

The TIS Addendum letter accepts that the sight distance requirements of AS2890.01 are not met and presents a safe systems framework analysis to address the non-compliance, arguing that the three options identified by the traffic engineer for locating the crossover all have similar Safe System Matrix scores with the proposed location being no 'less safe' than the two compliant options that would require removal of the nearby tree.

The revised plans show a 6m wide proposed crossover located 1.785m from the centreline of the trunk of the existing verge tree.

The sightlines review letter provides dimensioned drawings demonstrating the required minimum sight distances are only met if measured 1.5m from the kerb and fall significantly short if measured from the proscribed setback of 2.5m.

3. SITE INVESTIGATION

DVC undertook a site visit on the afternoon of Tuesday 1st October 2024. Some site photos are attached.

Many of the trees along the verge of West Road in the vicinity of the site have plaques individually displaying their Town of Bassendean Significant Tree Registration Number and describing their history. These are English Oak trees planted around 1950, originally planted both sides of West Road but removed on one side in the 1970s to widen the road.

West Road has a carriageway width of approximately 8m adjacent the proposed development site with an urban default speed limit of 50 km/h. **Photos 1 and 2** show general views north and south of the proposed crossover.

To retain the oak tree whilst providing the required minimum sight distance, DVC investigated whether an engineering solution could be adopted. If nibs were provided to narrow the carriageway by 1.5m or 2m then both objectives would be met as a driver exiting the site would be well clear of the tree when 2.5m from the revised kerb line and have a clear line of sight in both directions.

When leaving the site, we noted that West Road is already narrowed by some 2.5m in the vicinity of Bassendean Primary School to accommodate embayed on street parking on both sides. See **Photo 3**.

The narrowing of West Road in the vicinity of the proposed development should have no detrimental impact on movements to and from Hyland Street as we note West Road is narrowed by some 2.5m by a painted edge line opposite Harcourt Street, adjacent the Primary School with no adverse impact on turning traffic. (See **Photo 4**).

4. SIGHT DISTANCE CALCULATIONS

The sight distance calculations shown in the Sightlines Review letter dated 30.09.2024 appear correct. Those described in the initial Transport Impact Statement report are misleading, claiming the appropriate standards are met when in fact they are not.

5. PROPOSED CROSSOVER COMPLIANCE

DVC does not agree with the arguments provided that the location of the proposed crossover, whilst non-compliant with AS2890.01, represents a safe option for vehicular access to the site. The tree poses a significant obstruction to the sight distance and drivers will need to pull into the carriageway to achieve adequate sight distance.

6. TREE REMOVAL OPTION

Clearly, removal of the tree would resolve the sight distance issues. However, given the significance of the tree that has been in place for over 70 years and will possibly live another 70 years or more, it is worth considering alternatives to preserve it.

7. RECOMMENDATIONS

DVC recommends consideration be given to localised narrowing of West Road by 1.5m or 2m so that exiting vehicles can pull further forward and achieve adequate sight distance.

Prepared: Ken Lak & Donald Veal

Approved: Donald Veal, Director

Date: 4/10/2024

PHOTOS



Photo 1: Oak Tree north of proposed crossover on West Road



Photo 2: View looking south on West Road from proposed crossover



Photo 3: Road narrowing on West Road near Bassendean Primary School



Photo 4: Truck turning right from Harcourt Street on narrowed portion of West Road

ACOUSTIC ASSESSMENT 2311111 **Revision 7**

CHILD CARE CENTRE

LOT 85, 94 WEST ROAD BASSENDEAN WA 6054



prepared for

RPOINT

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30 YEARS 1994 - 2024



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Attachment: 2311111 BASSENDEAN Supplementary Advice No 2, 6 pages, dated 21 OCT 24.

References:

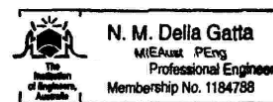
- A. Environmental Protection (Noise) Regulations 1997.
- B. Drawings: Macri Builders; Job 23019; Qty 10 sheets; Revision 16, Dated 240904.

Revisions:

Revision N°:	Date:	Issue / Comment	Status
0	09 FEB 24	DRP	Superseded
1	22 Feb 24	DA	
2	16 MAY 24	DA	
3	05 SEP 24	DA	
4	10 SEP 24	DA	
5	7 OCT 24	DA	
6	21 OCT 24	DA - AAAC Guideline CCC V3 data used for children modelling	Current
7	7 NOV 24	DA – Sup 2 added as an attachment, drawings updated to Rev 19, 1 st flr modelling updated	

Author:

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SUMMARY

- 0.1 ND Engineering's opinion is that the proposed Child Care Centre (CCC) for the daytime periods of 0700 - 1900 hours (7.00am – 7.00pm) Monday to Friday, excluding public holidays, the assessed noise emissions will comply with the Noise Regulations (Reference A) subject to implementation of the recommendations contained in Section 5 'Recommendations'.
- 0.2 Please note that the 7.00am start time and Crank Wall is based on including the car door closing impulsiveness as requested by the LGA. Carpark modelling within this report is based on non-impulsive car door closing and requiring management of the carpark rather than relying upon the built form. See the attachment to this report for noise modelling based on impulsiveness with and without the crank wall for carpark usage after 7.00am. The use of management rather than built form has typically allowed carparks to be used by staff prior to 7.00am thus allowing clientele to start arriving at 7.00am.

INTRODUCTION

- 1.1 ND Engineering was commissioned to provide an acoustic assessment of the potential noise from the proposed Child Care Centre (CCC) with regards to the other residential premises.

DESCRIPTION

- 2.1.1 The proposed CCC site, see Annex A, is located on West Road Bassendean on the corner of Bridson Street.
- 2.1.2 The nearest noise sensitive premises of interest are located:
- a. Adjoining residences to the: West at 4 Bridson St; and North at 92 West Rd.
 - b. Residences across to the East being: 85, 87, 93 & 95 West Rd; and 43 Hyland St cnr West Rd.
 - c. Residences across the road to the South being: 3 Bridson St; and 96 West Rd cnr Bridson St.
- 2.1.3 The residential assigned noise levels, see Annex A, of interest is an average maximum of LA10 = 45 dB(A) and a maximum of L_{Amax} = 65 dB(A) all during operating hours.
- 2.1.4 Refer to the following Annexes for detailed location and site descriptions:
- a. Annex A 'Location'; and
 - b. Annex B 'Plans'.
- 2.2 **Non-Equipment Noise** sources at the site will be:
- a.

Children:			
<u>Activity Room & Location</u>	<u>Description</u>	<u>Age Range</u>	<u>Qty</u>
Activity 1 Ground floor	Babies	0 - 2 yrs old.	12.
Activity 2 Ground floor	Kindy	4 - 5 yrs old.	28.
Activity 3 Ground floor	Toddler	2 - 3 yrs old.	20.
Activity 4 Ground floor	Pre-Kindy	3 - 4 yrs old.	20.
 - b. **Music** occasionally for children with the music being non-impulsive by nature. Refer Annex E 'Music' for more details.
 - c. **Carpark.** Refer Annex G 'Carpark' for more details.
- 2.3 **Equipment Noise** sources at the site are expected to comprise new Air-Conditioning systems and Mechanical Ventilation systems. Refer Annex F 'Mechanical Services' for more details.



ASSESSMENT

- 3.1 Noise emissions from the CCC are expected to occur Monday to Friday between 0700 to 1900 hours (7.00am – 7.00pm) mainly during outdoor play weather permitting. This means that for evenings, night time, public holidays and Sundays there is expected to be no noise emissions from the CCC at all. Anecdotal evidence indicates this is a desirable situation sought by some residences when purchasing properties adjacent to a CCC as their will be no afterhours (including Sunday and Public Holiday) noise thus negating a common source of complaint.
- 3.2 The relevant assigned noise levels at receiving premises, residential in the vicinity of the noise source, as allowed under Reference A are shown in the following Table 3.2. The assessments of the various noise sources emissions from the CCC are assessed against Table 3.2 as applicable.

Table 3.2 – ASSIGNED NOISE LEVELS					
Noise Sensitive Premises at locations a building directly associated with a noise sensitive use.	Time of day	Time of day	Assigned Noise Levels dB(A)		
			LA10	LA1	LAmx
. . within 15 m of . .	Day	0700 - 1900 hrs Monday to Saturday	45	55	65
		0900 - 1900 hrs Sunday, Public holidays	40	50	
	Evening	1900 - 2200 hrs all days			55
	Night	2200 - 0700 hrs Monday to Saturday	35	45	
		2200 - 0900 hrs Sunday, Public holidays			
. . greater than 15 m from . .	All hours	All hours	60	75	80
Commercial	All hours	All hours	60	75	80

- 3.3 Refer to the following annexes for the detailed assessments:
- a. Assigned Noise Levels. Refer Annex C 'Assigned Noise Levels'.
 - b. Children. Refer Annex D 'Children'.
 - c. Music. Refer Annex E 'Music'.
 - d. Mechanical Services. Refer Annex F 'Mechanical Services'.
 - e. Carpark. Refer Annex G 'Carpark'.
- 3.4 Recommendations arising from the assessments are collated and presented in Section 5 'Recommendations' in the main body of the report.



CONCLUSIONS

- 4.1 ND Engineering's opinion is that the proposed Child Care Centre for the daytime periods of 0700 - 1900 hours (7.00am - 7.00pm) Monday to Friday, excluding public holidays, the:
- a. Children's' noise emissions will comply with the Noise Regulations (Reference A) subject to implementation of the recommendations contained in Section 5 'Recommendations';
 - b. Non-children noise emissions will comply with the Noise Regulations (Reference A) subject to implementation of the recommendations contained in Section 5 'Recommendations'.



RECOMMENDATIONS

- 5.1 The recommendations presented in this report are in outline format only and require:
- a. Detailed final design of components by appropriately experienced persons in accordance with the current relevant editions of Australian Standards, Regulations, Gas Installation Code/s and the BCA.
 - b. Completion of minor details, including acoustic/vibration details, on site by competent and qualified tradesmen and technicians.
 - c. New materials and equipment to be installed in accordance with the manufacturer's and/or supplier's instructions.
 - d. New materials and equipment to comply with, and be installed in accordance with, the BCA.
 - e. Installer of materials and/or equipment to comply with:
 - (1) regulatory safety requirements.
 - (2) The safety procedures on the relevant Materials Safety Data Sheets (MSDS).
 - (3) The site safety requirements including the wearing of protective clothing such as safety boots, safety glasses, safety goggles and hard hats.
 - f. A site inspection to fully determine the extent of the work and the nature of the site.
- 5.2 The following recommendations are made:
- a. **Operational:**
 - (1) The CCC is to be operational, excluding public holidays, Monday to Friday only; and
 - (2) Staff arrivals/departures between 7.00am to 7.00pm (NB due to the inclusion of impulsiveness in the built form, rather than managing impulsiveness, parking is not permitted prior to 7.00am); and
 - (3) Customer arrivals/departures between 7.00am to 6.30pm; and
 - (4) External Play Areas, carpark excluded, accessed between 7.00am to 6.00pm.
 - b. **Children's play areas:**
 - (1) Children are not permitted outdoors, carpark excluded, prior to 7.00am.
 - (2) Practical considerations:
 - (a) Fixed play equipment should be non-metallic. If metal fixed play equipment is used then hollow metal sections shall be filled with expanding foam or sand.
 - (b) Concrete or brick paved areas, if any, should be minimised and where practicable covered with synthetic grass to minimise noise of play equipment on hard surfaces.
 - (3) There are no restrictions on neither Babies 0 to 2 years old nor Toddlers 2 to 3; and
 - (4) There are restrictions on the older children Pre-kindy 3 to 4 years old and Kindy 4 to 5 years old as follows for the Outdoor Play Areas (OPA):
 - Free play activity areas are on the 1st floor OPA ~262 m²; and
 - Quiet area on the ground floor South OPA ~20 m²; and



- Quiet area on the ground floor East OPA ~210 m²; and
- Not permitted in the ground floor West OPA ~67 m².

(5) See also **Figures 5.2 Noise Barriers and Acoustic Treatments:**

- c. **Music:** - Keep external windows and doors closed; and
- Do not play music outdoors.

d. **Mechanical Services:**

(1) Exhaust systems (requirements for any NEW Exhaust systems):

- (a) No specific external acoustic requirements for small non-kitchen exhaust systems.
- (b) No specific acoustic requirements for domestic kitchen canopy ducted to exterior when kitchen equipment inputs is less than either 8 kW electrical or 29 MJH gas.
- (c) Specific external acoustics requirements for a commercial kitchen canopy with an external fan when the kitchen equipment input is greater than either 8 kW electrical or 29 MJH gas then the exhaust fan shall be:
 - (i) Located more than 6.0 metres from residential boundary with a vertical discharge;
 - (ii) Operating at a speed not exceeding nominally 960 rpm with a Sound Pressure Level not exceeding 52 dB(A) @ 3.0 m at the operating speed.

(2) Air-Conditioning (AC) systems (requirements for any AC systems):

- (a) Evaporative AC units shall be of the centrifugal fan type and shall be sized to deliver the required air quantity on the low speed setting; and
- (b) Refrigerated AC Condenser Units (CU) shall be inverter type with a minimum - 5dB(A) low noise mode; and
- (c) AC units shall each have Sound Power Level (SWL or Lw) not exceeding the following at rated conditions (excluding silent or low noise mode) but inclusive of any silencers:
 - 3 units each 73 dB(A) each; or
 - 2 units each 75 dB(A) each; or
 - 1 unit 78 dB(A).
- (d) Locate the AC unit/s either within the carpark bin store as shown in the current drawings or on the 1st floor outdoor play area or at another location all subject to an acoustic assessment of the building Permit Application design by an Acoustic Engineer using SoundPlan noise modelling software.

e. **Carpark:**

- (1) Staff will be instructed not to arrive prior to 0700 hours and to be off site by 1900 hours; and
- (2) Staff parking between 7.00am to 7.00pm (NB due to the inclusion of impulsiveness in the built form, rather than managing impulsiveness, parking is not permitted prior to 7.00am); and
- (3) SIGNAGE is placed within the carpark asking parents/staff not to slam car doors/boots and not to play loud music.

f. **Noise Barriers and Acoustic Treatments:**

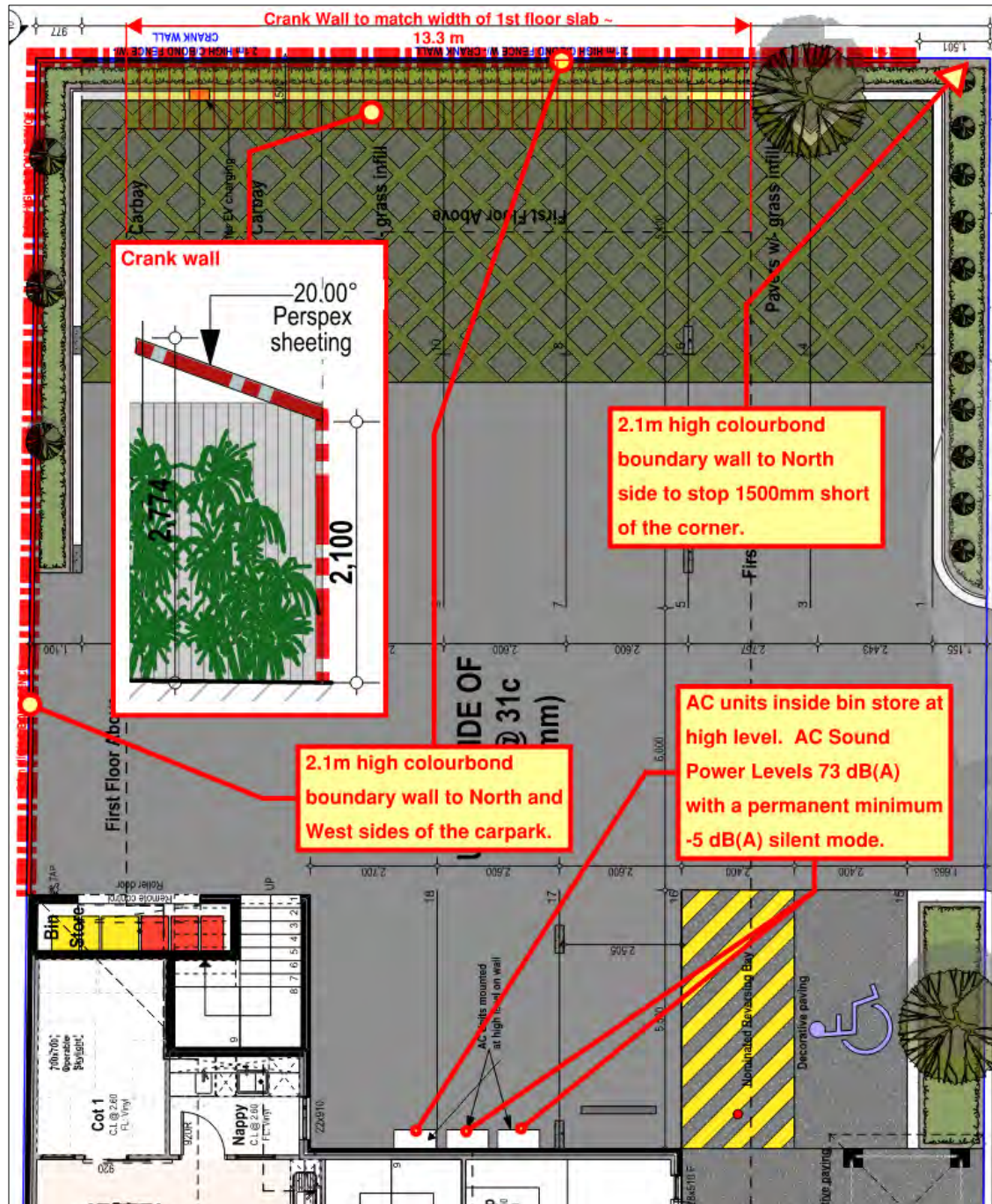


FIGURE 5.2a GROUND FLOOR NORTH – Carpark Noise Barriers

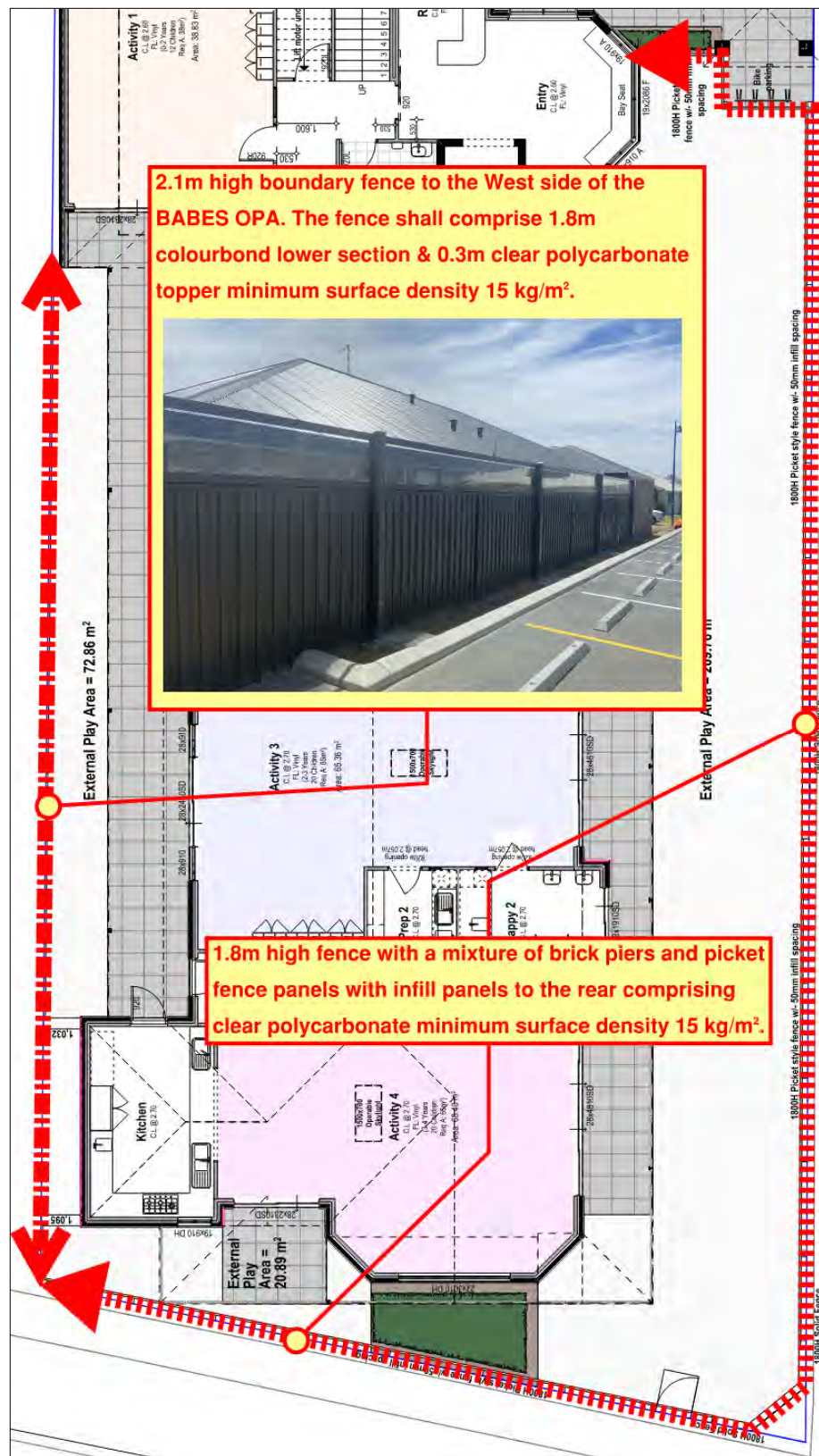


FIGURE 5.2b GROUND FLOOR SOUTH – Outdoor Play Areas Noise Barriers Babes/Toddlers

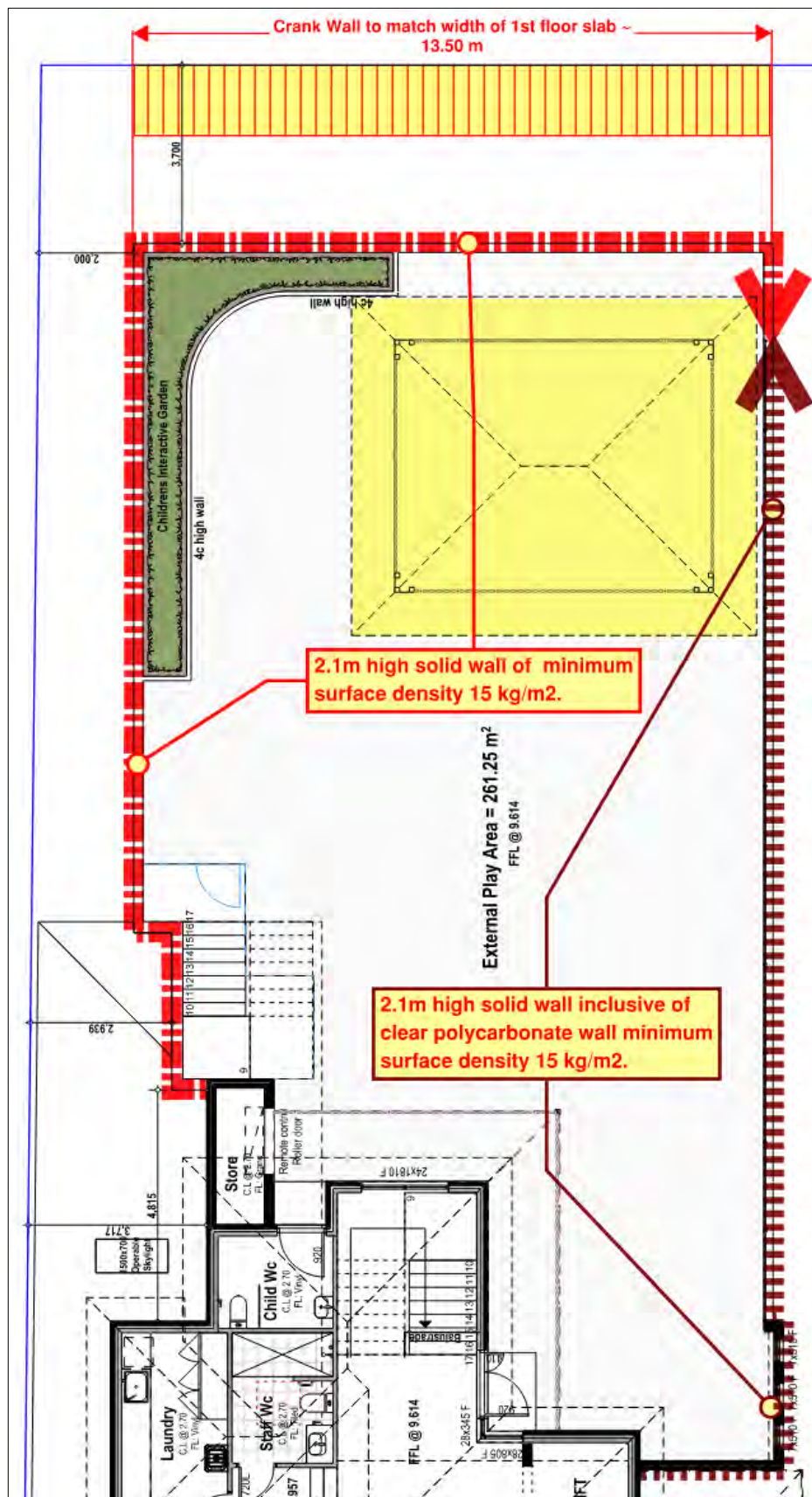


FIGURE 5.2c FIRST FLOOR NORTH - Noise Barriers



- ANNEXES:**
- A. Location.
 - B. Site Plans.
 - C. Assigned Noise Levels.
 - D. Children.
 - E. Music.
 - F. Mechanical Services.
 - G. Carpark.



Annex A – Location



FIGURE A1 – SITE LOCATION MAP

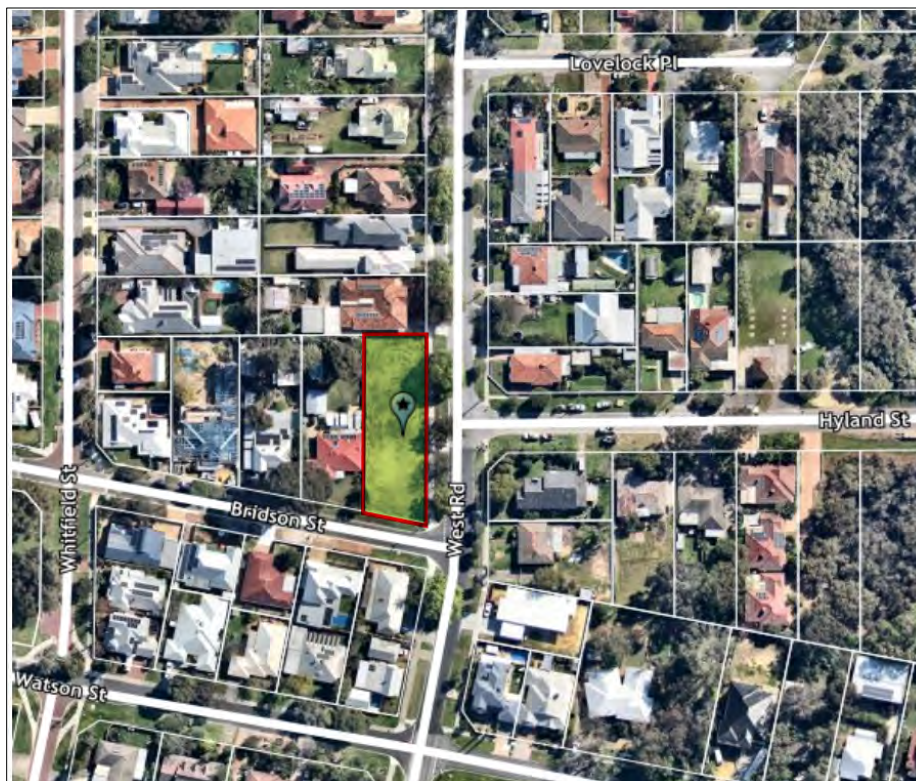


FIGURE A2 – SITE LOCATION AERIAL



FIGURE A3 – SITE LOCATION AERIAL DETAILED



Annex B - Plans



PERSPECTIVES

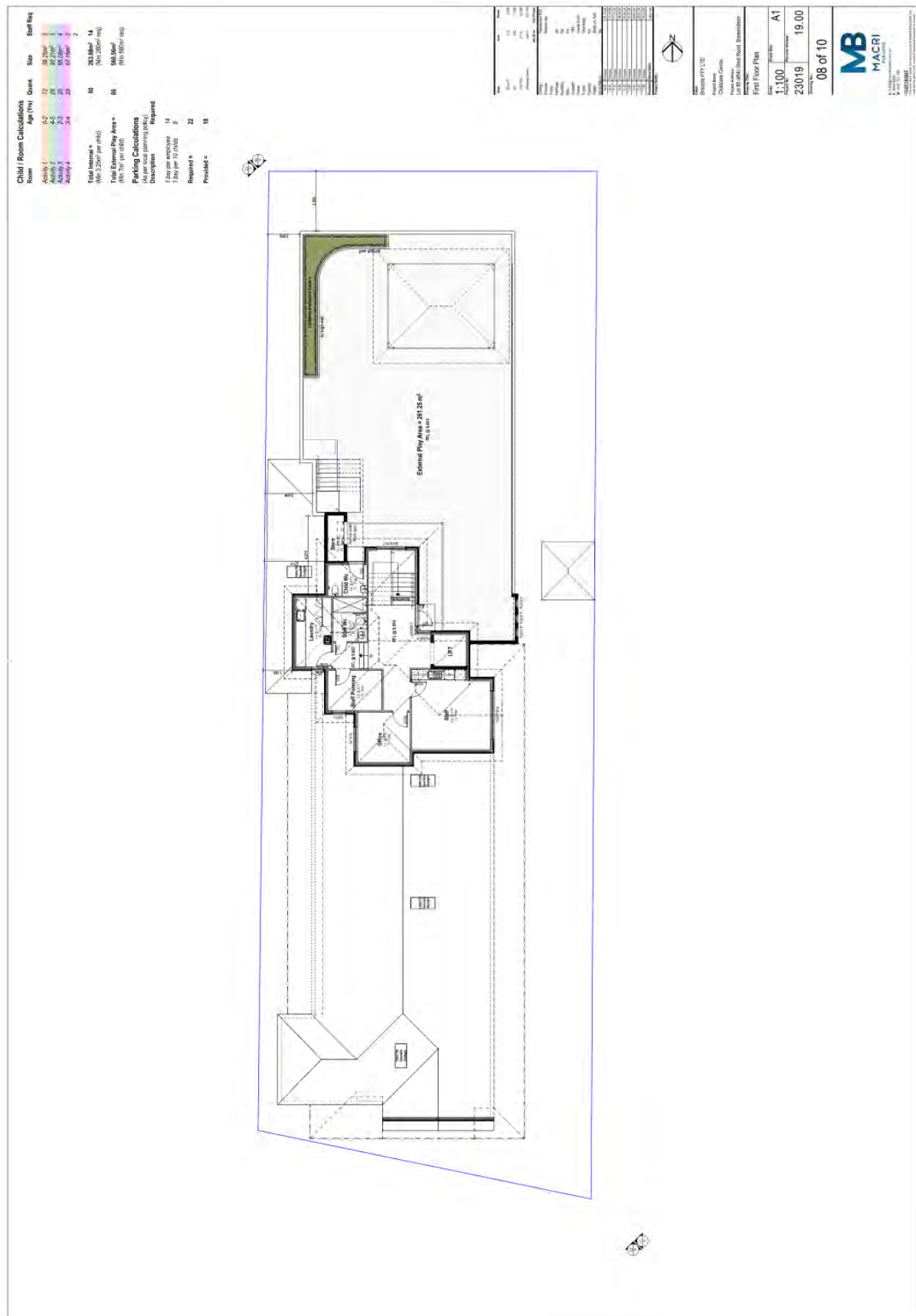


FIGURE B1 – FIRST FLOOR PLAN

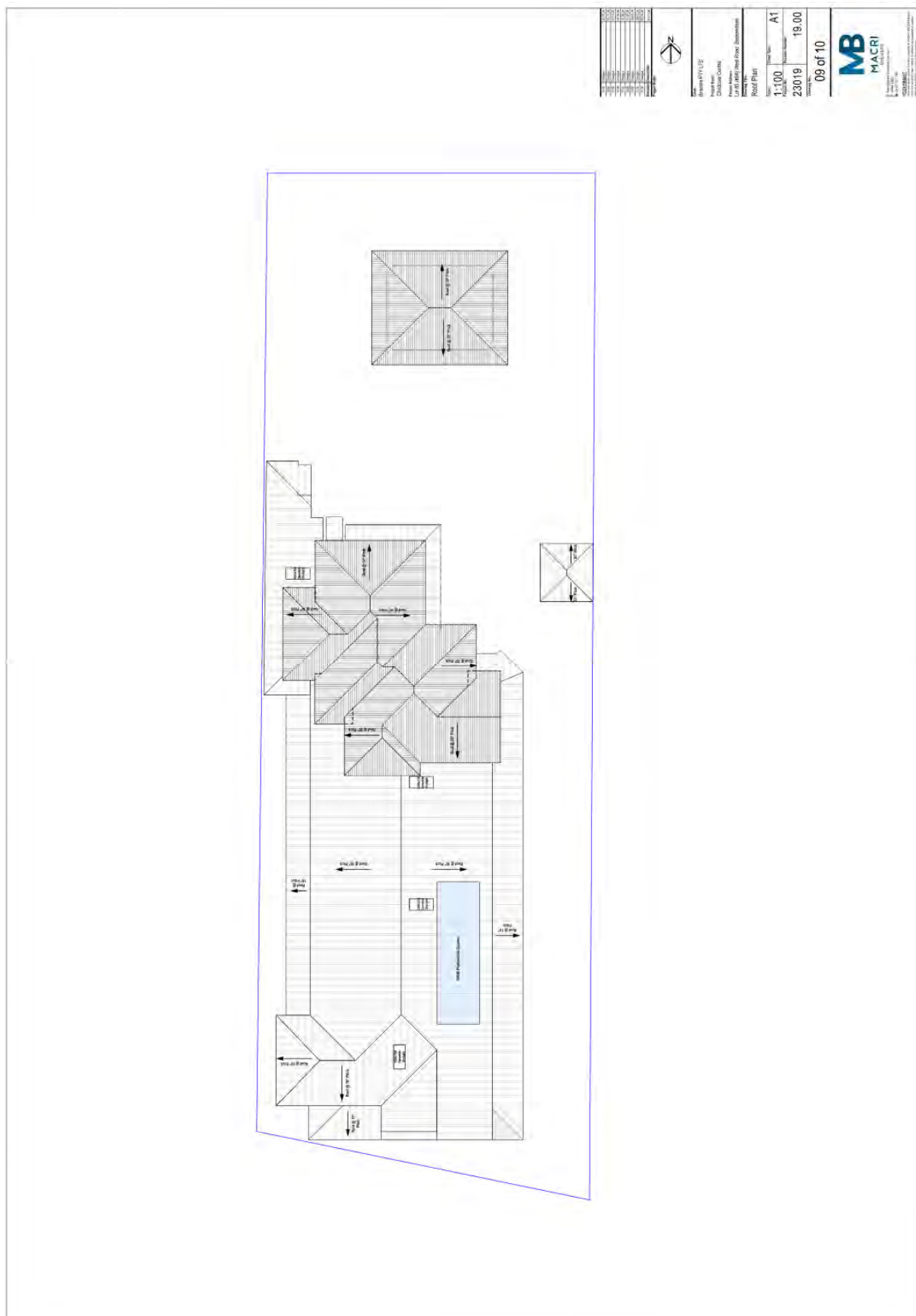


FIGURE B2 – ROOF PLAN

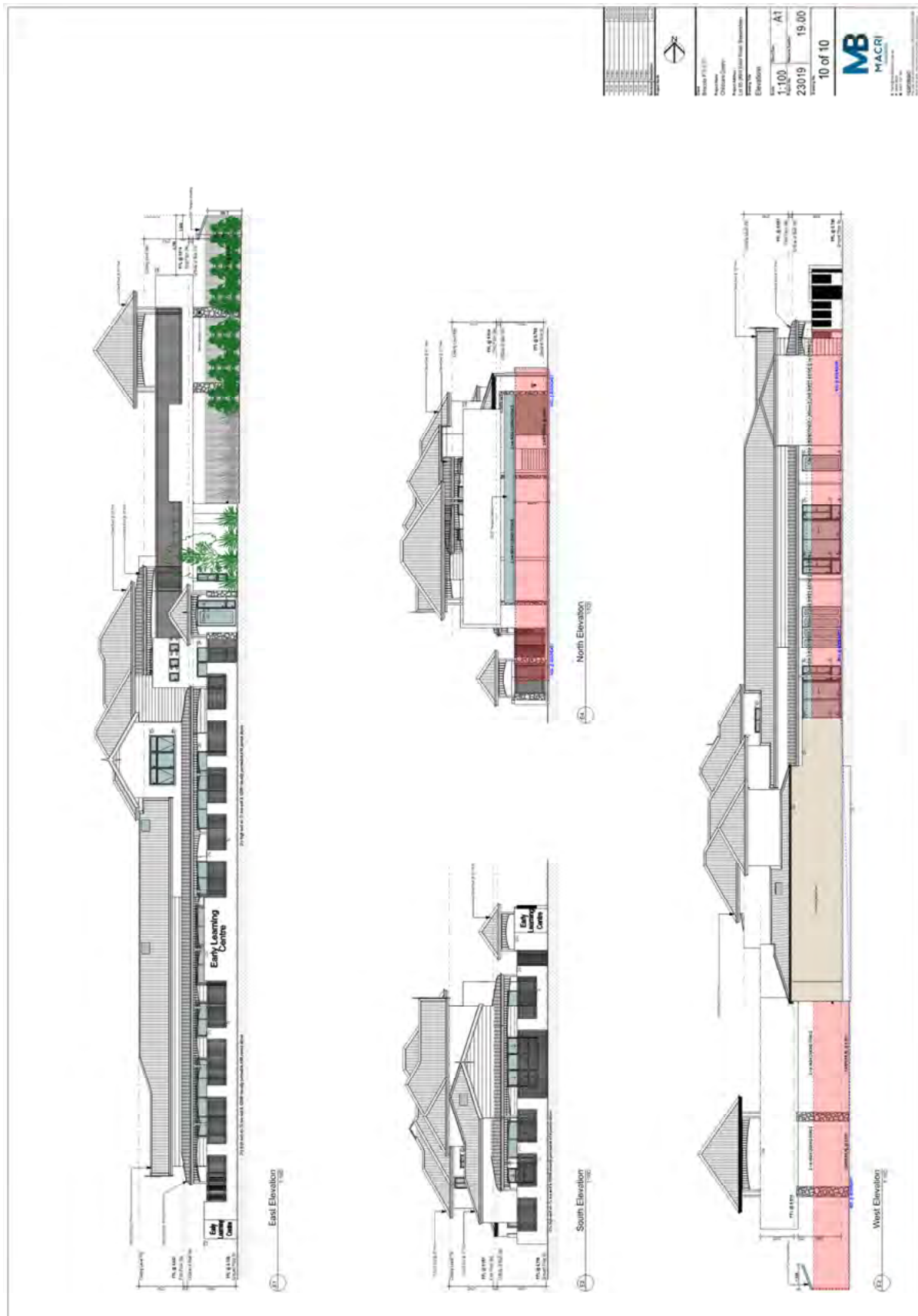


FIGURE 3 - ELEVATIONS



Annex C - Assigned Noise Levels

- C1. The assigned noise level, as determined by Reference A, comprises a Base Noise Level and an Influencing Factor adjustment to take into consideration noise from nearby features such as major roads, industrial and commercial premises. The assigned noise level comprises three criteria being the L_{Amax}, L_{A1} and L_{A10}.

- C2. L_{Amax} and L_{A1} represent respectively the single maximum noise event and the 1 percentile highest A weighted sound pressure levels over a representative measurement period.

The measurement criteria L_{A10} represents the 10 percentile highest A weighted sound pressure level over a representative measurement period of not less than 15 minutes and not more than 4 hours.

ND Engineering's understanding as a result of discussions with the DEP in March 2005 indicated that a representative measurement period for a CCC would be 4 hours.

- C3. Repeated attempts at obtaining statistical noise measurement data at various CCC without interference from traffic is difficult as most CCC are located on major and/or secondary roads with children playing outdoors when there is significant traffic noise in the morning and afternoon.

The L_{Amax} is fairly easy to obtain as it represents a single noise event such as a shout or scream. The other two criteria L_{A1} and L_{A10} are statistical measurements and traffic noise creates significant problems in acquiring the measurement in particular the L_{A1} measurement.

The L_{A10} measurement criteria provides a reasonable indication of the objectionable noise as any unwanted noise events such as traffic, wind induced vegetation noise and animal noise form a smaller and less significant component which can be partially edited out.

- C4. ND Engineering's assessment is based primarily on the L_{Amax} and L_{A10} criteria as obtaining a L_{A1} measurement that is 'legally' watertight is virtually impossible or not achievable when gathering noise data for the assessments. As a consequence, the assessments are based on the L_{Amax} and L_{A10} criteria. The L_{Amax} criteria is the most important criteria as this is the criteria associated with shouting that is most objectionable.

- C5. The base assigned noise levels are shown in the following table.

Table C5 – ASSIGNED ‘BASE’ NOISE LEVELS					
Noise sensitive premises at locations a building directly associated with a noise sensitive use.	Time of day	Time of day	Assigned Noise Levels dB(A)		
			LA10	LA1	LAmaz
. . within 15 m of . .	Day	0700-1900 hrs Monday to Saturday	45+IF	55+IF	65+IF
		0900-1900 hrs Sunday, Public holidays	40+IF	50+IF	
	Evening	1900-2200 hrs all days			55+IF
	Night	2200-0700 hrs Monday to Saturday	35+IF	45+IF	
		2200-0900 hrs Sunday, Public holidays			
. . greater than 15 m from . .	All hours	All hours	60	75	80
Commercial	All hours		60	75	80



- C6. The following table shows the Influencing Factor calculation for the adjustments to the base noise levels for the nearest residences to the childcare centre.

Table C6 – INFLUENCING FACTOR ASSESSMENT					
INFLUENCING FACTOR CRITERIA			ASSESSMENT		
Item	Criteria	Value	Criteria	Value	Totals
Major Road within the					0 (Transport Factor ≤ 6)
- 100 m radius inner circle	veh/w'day > 15000	6 dB		0	
- 450 m radius outer circle	veh / w'day > 15000	2 dB		0	
Minor Road within the					0 (≤ 30)
- 100 m radius inner circle	15k > veh/day > 6k	2 dB		0	
Type A 'Industrial and Utility premises' within the					
- 100 m radius inner circle	1/10 x Area%	≤ 10	0 %	0	0 (≤ 30)
- 450 m radius outer circle	1/10 x Area%	≤ 10	0 %	0	
Type B 'Commercial premises' within the					
- 100 m radius inner circle	1/20 x Area%	≤ 5	0 %	0	
- 450 m radius outer circle	1/20 x Area%	≤ 5	0 %	0	
INFLUENCING FACTOR = 0 dB(A)					

- C7. The assigned noise levels at receiving noise sensitive premises, residential in the vicinity of the noise source, as allowed under Reference A are shown in the following table.

Table C7 – ASSIGNED NOISE LEVELS					
Noise sensitive premises at locations a building directly associated with a noise sensitive use.	Time of day	Time of day	Assigned Noise Levels dB(A)		
			LA10	LA1	LAmx
. . within 15 m of . .	Day	0700-1900 hrs Monday to Saturday	45	55	65
		0900-1900 hrs Sunday, Public holidays	40	50	
	Evening	1900-2200 hrs all days			
	Night	2200-0700 hrs Monday to Saturday	35	45	55
		2200-0900 hrs Sunday, Public holidays			
. . greater than 15 m from . .	All hours	All hours	60	75	80
Commercial	All hours	All hours	60	75	80



Annex D - Children

- D1. Noise emissions from the child care centre are expected to occur Monday to Friday between 0700 - 1900 hours (7.00am – 7.00pm) mainly during the two hours of outdoor play per day weather permitting for the Kindy group. This means that for evenings, night time, public holidays and Sundays there is expected to be no noise emissions from the child care centre at all.
- D2. Anecdotal evidence indicates this is a desirable situation sought by some residences when purchasing properties adjacent to a child care centre as their will be no afterhours noise thus negating a common source of complaint.
- D3. The Children's voices categorised by age groups:
- a. **Junior & Kindy** (aka Pre-Kindy 3-4 yo & Kindy 4-6yo) all over 3 years old.

Measurements, observations and discussions with CCC staff since year 2000 indicates that this is the most significant noise producing group.
 - b. **Toddlers** 2 – 3 years old:

This is a very low noise producing group based on observations and discussions with CCC staff since year 2000. Their external play time is generally less than the Kindy group but more than the Babes group.

Attempts to obtain noise measurements suitable for use with Environmental Protection (Noise) Regulations 1997 "Reference A" have not been successful mainly due to traffic noise from nearby minor and/or major roads associated with the CCC's that ND Engineering has been reporting upon.
 - c. **Nursery** (aka Babes or Babies) 0 - 2 years old:

This is a very low noise producing group based on observations and discussions with CCC staff on previous assessments.

Attempts to obtain noise measurements suitable for use with Reference A have not been successful.
- D4. Children, weather permitting, are allowed outside to play for about 2 hours per day being typically about 0830 to 1000 hours and 1500 to 1800 hours with play typically being broken up into about 30 minute sessions at a time. Sometimes the afternoon outdoor play time is not utilised due to higher levels of sun exposure at this time of day. This low number of outdoor play hours is:
- (1) Consistent with information obtained from CCC operators since year 2005. There are some variations between CCC but it is generally consistent with ND Engineering experience with the CCC assessments undertaken since year 2005;
 - (2) Also due to current sun exposure policies as expressed by the Cancer Council's Sun Protection Policy which does not recommend outdoor play between 1000 to 1500 hours;
 - (3) Play groups are typically for 12 to 24 children depending upon supervision requirements, with play times being staggered with children being rotated between outdoor and indoor activities.



Children 0 to 3 years old - Assessment

- D5.1 **The Babes** 0 - 2 years old is a very low noise producing group based on observations and discussions with CCC staff since year 2000. Their external play time is typically about 30 minute sessions. Attempts to obtain noise measurements suitable for use with Reference A have not been successful due to the typically low noise output of this age group.
- D5.2 **The Toddlers** 2 – 3 years old age group is again a very low noise producing group based on observations and discussions with CCC staff since 2000. Their external play time is generally less than the Kindy group but more than the Babes group. Attempts to obtain noise measurements suitable for use with Reference A have not been successful mainly due to traffic noise from nearby secondary and/or major roads associated with the CCC's that ND Engineering has been reporting upon since 2000.
- D5.3 The noise levels created by small groups of children, in the Babes 0 to 2 years old and Toddlers 2 to 3 year old age groups, is unlikely to cause problems for the surrounding residences due to the:
- Low noise output of this age group; and
 - These age groups engage in parallel play, rather than group play, at this stage of their social development which is a low noise activity; and
 - Short duration outdoor play times, typically 30 minutes, especially if the weather is not mild.
- D5.4 The data utilised for this assessment is based on AAAC V3.0 CCC Acoustic Assessment Table 1.
- D5.5 The assessment is based on using sound data as shown in the notes to each noise model.
- D5.6 ND Engineering's assessment with regards to Residential Premises is that the noise emissions from the Babes and Toddlers Outdoor Play Areas (OPA) as currently presented see Reference B and Annex A, complies with the assigned noise levels, see Figures D6, subject to implementation of the recommendations including the following:
- (1) There are no restrictions on neither Babies 0 to 2 years old nor Toddlers 2 to 3; and
 - (2) See also **Figures 5.2 Noise Barriers and Acoustic Treatments:**
- D5.7 Refer to the Section 'Recommendations' in the main body of the report.



Children 3+ years old - Assessment

D6.1 The data utilised for this assessment is based on AAAC V3.0 CCC Acoustic Assessment Table 1.

D6.2 The assessment is based on using sound data as shown in the notes to each noise model.

D6.2 ND Engineering's assessment with regards to Residential Premises is that the noise emissions from the outdoor play areas (OPA) as currently presented see Reference B and Annex A, complies with the assigned noise levels, see Figures D6, subject to implementation of the recommendations including the following:

- (1) There are no restrictions on neither Babies 0 to 2 years old nor Toddlers 2 to 3; and
- (2) There are restrictions on the older children Pre-kindy 3 to 4 years old and Kindy 4 to 5 years old as follows for the Outdoor Play Areas (OPA):
 - Free play area on the 1st floor ~262m²;
 - Quiet area on the ground floor South ~20m²;
 - Quiet area on the ground floor East ~210m²;
 - Not permitted in the ground floor West ~67m² and
- (3) See also **Figures 5.2 Noise Barriers and Acoustic Treatments:**

D6.3 Refer to the Section 'Recommendations' in the main body of the report.

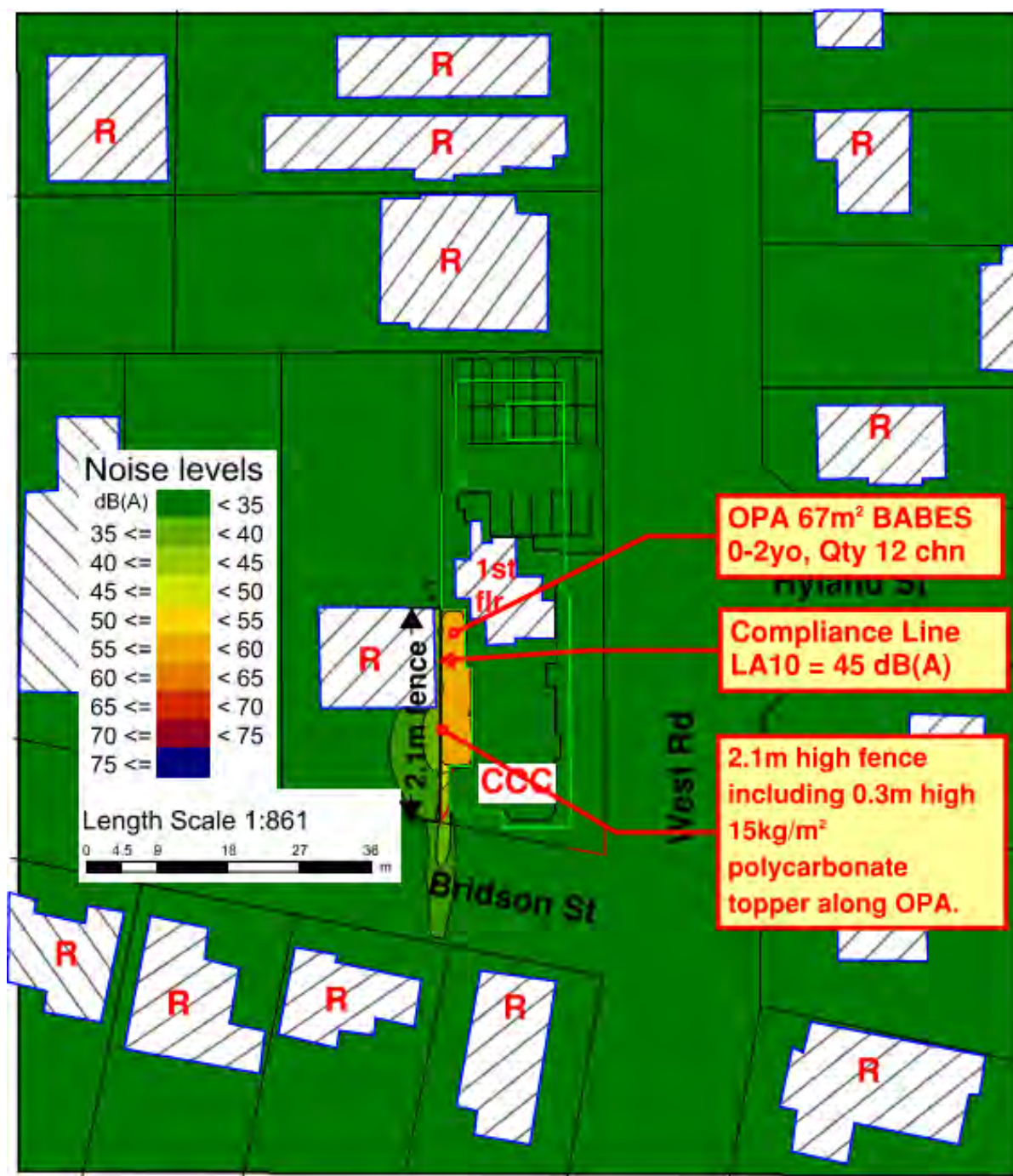


FIGURE D6.1a – GROUND FLOOR ‘BABES’ OUTDOOR PLAY AREA LA10

- Notes:**
0. North is top of page.
 1. Absenteeism is ignored.
 - 2.1 Residential receiver 1400mm high above residential ground floor level.
 - 2.2 Children (Chn) noise sources at 1000mm high above OPA ground level with a sound power level of 85 dB(A) per 10 children distributed over the OPA.
 3. Contour lines: 1400mm Above OPA GROUND Level.
 4. Concave conditions adopted.
 5. NDE's opinion is that the situation is compliant with 2.1m boundary fence.
 6. Western neighbours wall mounted ACU at high level under the eaves is unlikely to comply with the Noise Regulations in the Babes OPA regardless of boundary wall height.

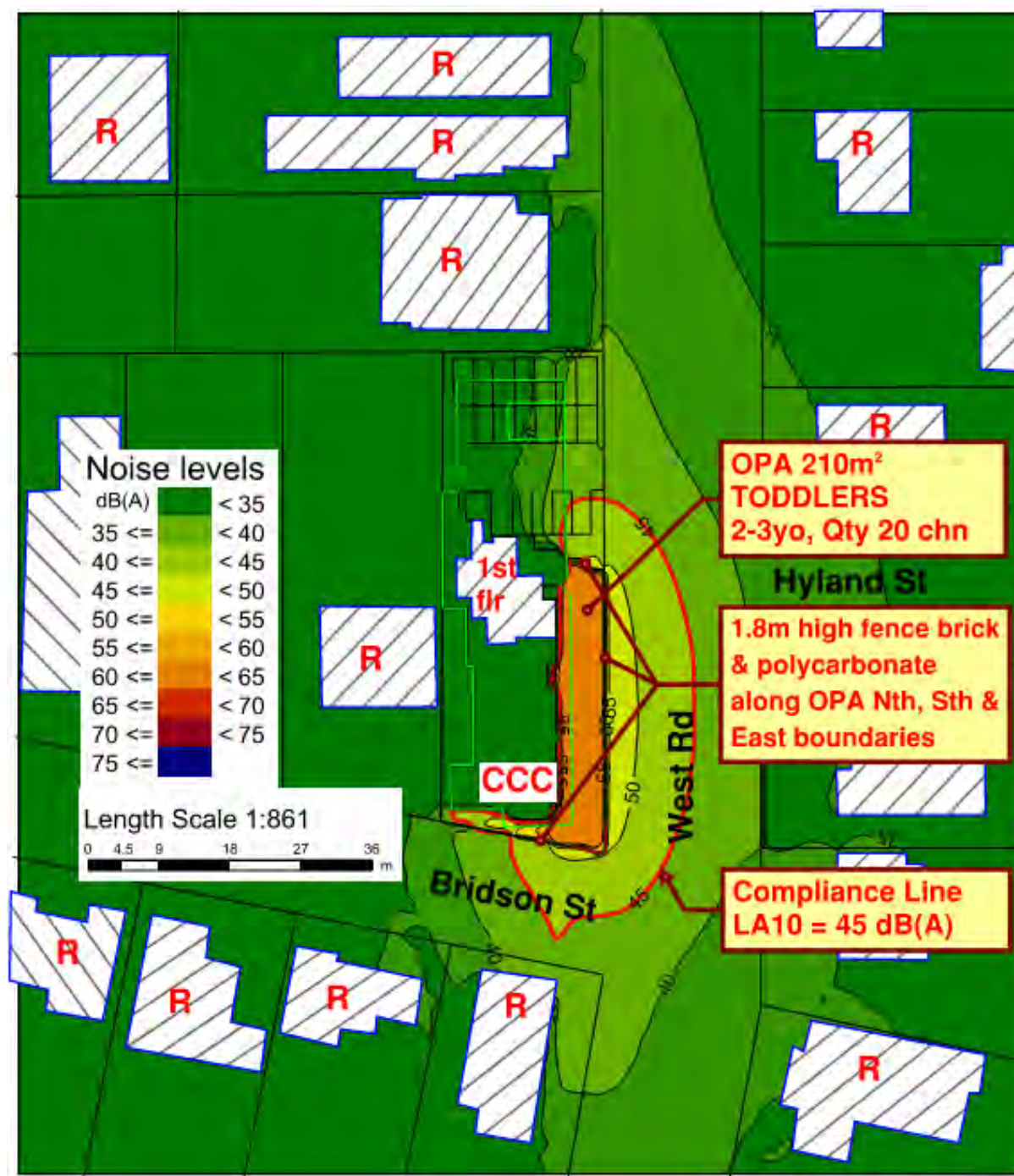


FIGURE D6.1a – GROUND FLOOR 'TODDLERS' OUTDOOR PLAY AREA LA10

- Notes:**
0. North is top of page.
 1. Absenteeism is ignored.
 - 2.1 Residential receiver 1400mm high above residential ground floor level.
 - 2.2 Children (Chn) noise sources at 1000mm high above OPA ground level with a sound power level of 85 dB(A) per 10 children distributed over the OPA.
 3. Contour lines: 1400mm Above OPA GROUND Level.
 4. Concave conditions adopted.
 5. NDE's opinion is that the situation is compliant with a 1.8 boundary solid fence.

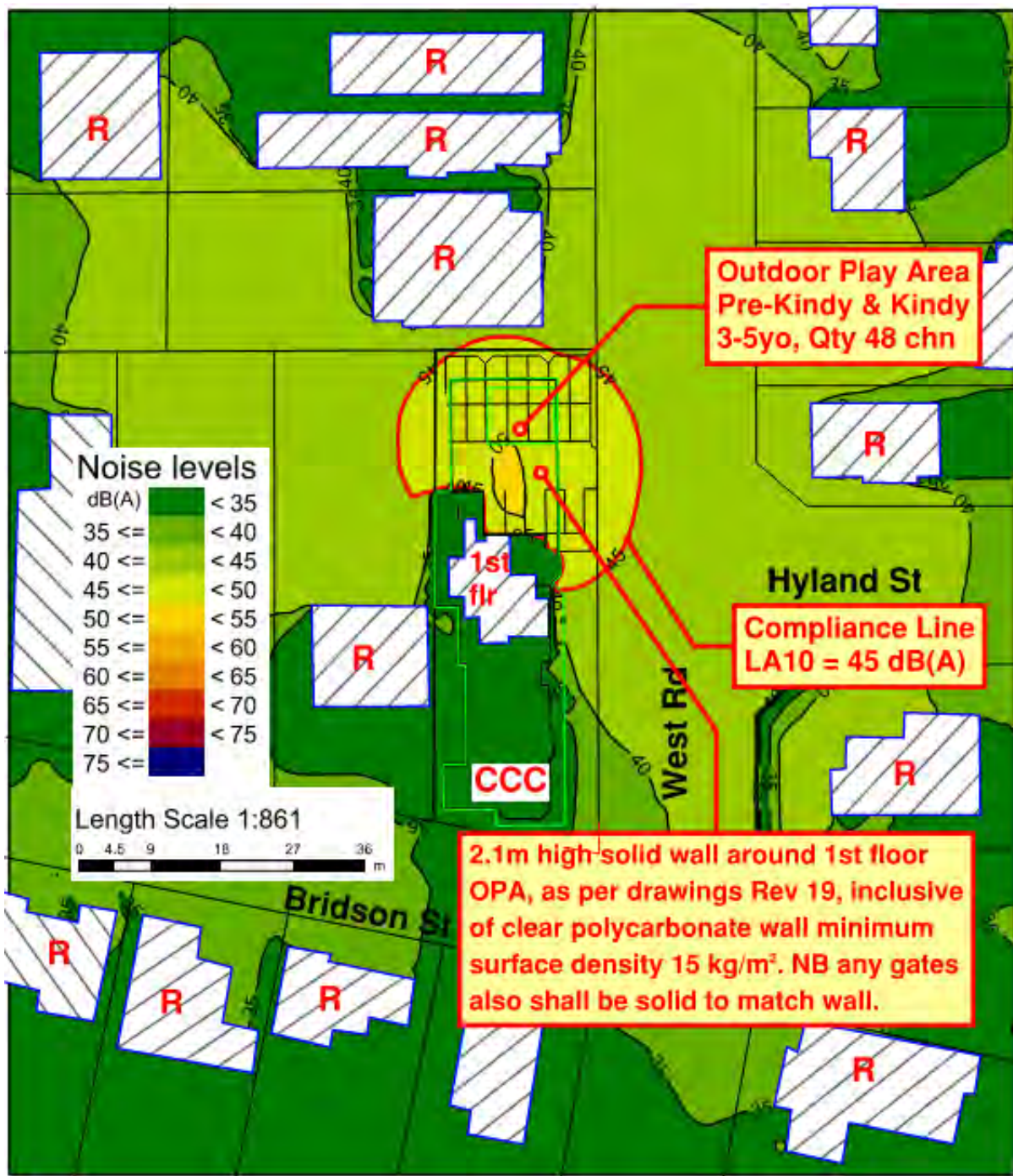


FIGURE D6.3a – 1st FLOOR OUTDOOR PLAY AREA LA10

- Notes:**
0. North is top of page.
 1. Absenteeism is ignored.
 - 2.1 Residential receiver 1400mm high above residential ground floor level.
 - 2.2 Children (Chn) noise sources at 1000mm high above 1st floor slab level with a sound power level of 87 dB(A) per 10 children distributed over the OPA.
 3. Contour lines: 1400mm Above GROUND Level.
 4. Concave conditions adopted.
 5. NDE's opinion is that the situation is compliant.



Annex E - Music

- E1. Typically, music produced within child care centres is for short durations as part of an activity and is played at a low volume as small children will typically not be able to follow instructions in rooms with a high noise background.

Basically, music levels will need to be kept at about 60 dB(A) or lower within the room which is equivalent to the noise level produced by a conversational adult male voice at 1 metre.

The music is typically non-impulsive, minimal bass, thus minimizing the main source of complaint typically associated with music.

- E2. The reduction in noise levels to the nearest residential boundary has been calculated to be at least 20 dB(A) as a result of attenuation due to the transmission loss of the glass.

Essentially with all external doors and windows closed the noise level due to music at the nearest residential boundary will be about 35 dB(A) which with all adjustments included is well below the daytime LA10 assigned noise levels.

- E3. Reductions due to distance and boundary fence reductions have not been included in the preceding calculation and are expected to be about 3 to 8 dB(A) with an average of 5 dB(A) therefore making the assessment fairly conservative.

- E4. Refer to the Section 'Recommendations' in the main body of the report.



Annex F – Mechanical Services

- F0. This section applies to any new Mechanical Services and not the existing Mechanical Services.
- F1. The main equipment noise sources at the site are expected to comprise:
- a. Air-conditioning being either:
 - (1) Evaporative ducted; or
 - (2) Refrigerated reverse cycle air conditioning systems configure possibly as a mixture of ducted and wall mounted systems;
 - b. Mechanical ventilation exhaust systems (for Bath, Kitchen, Laundry, WC's) being typically of two types for;
 - (1) Rooms with an external non-boundary wall having either window or wall mounted exhaust fans; and
 - (2) Rooms without an external non-boundary wall having either:
 - (i) Ceiling mounted exhaust fan ducted vertically to the exterior through the roof; or
 - (ii) Bulkhead/ceiling ducted exhaust system to a non-boundary external wall; and
- F2. The child care centre is expected to be operational, excluding public holidays, between 0700 to 1900 hours (7.00am – 7.00pm) Monday to Friday.
- F3.1 The main potential noise source is the Air-Conditioning condenser units and the detailed requirements for these AC condenser units are contained in the recommendations section of this report. Essentially the recommendations are the use of inverter AC condenser units with a minimum -5 dB(A) low noise mode and positioning of the AC condenser units either in the carpark bin store enclosure as shown in the current drawings) or on the 1st floor outdoor play area or at another location all subject to an acoustic assessment of the Building Permit Application design by an Acoustic Engineer using SoundPlan noise modelling software.
- F3.2 The toilet exhaust fans are unlikely to pose a problem and are not assessed in detail. In the unlikely event that these exhaust discharges through the roof do present some objectionable noise this can be easily overcome by the insertion of some additional acoustic flexible duct into the discharge line.
- F3.3 The kitchen exhaust fans will either be of a domestic kitchen canopy type or commercial kitchen canopy type depending upon the size of the kitchen equipment. If the kitchen equipment has inputs:
- a. Less than either 8 kW electrical or 29 MJH gas then a commercial kitchen canopy is not required, and a domestic kitchen canopy ducted to the exterior will suffice. In this situation, the exhaust system is unlikely to pose a problem and therefore is not assessed in detail.
 - b. Greater than either 8 kW electrical or 29 MJH gas then a commercial kitchen canopy is required with an external roof mounted fan. Essentially the exhaust fan will need to be located further than 6.0 metres from a residential boundary with a maximum speed of 960 rpm. Detailed requirements for these AC condenser units are contained in the recommendations section of this report.
- F4. Refer to the Section 'Recommendations' in the main body of the report.

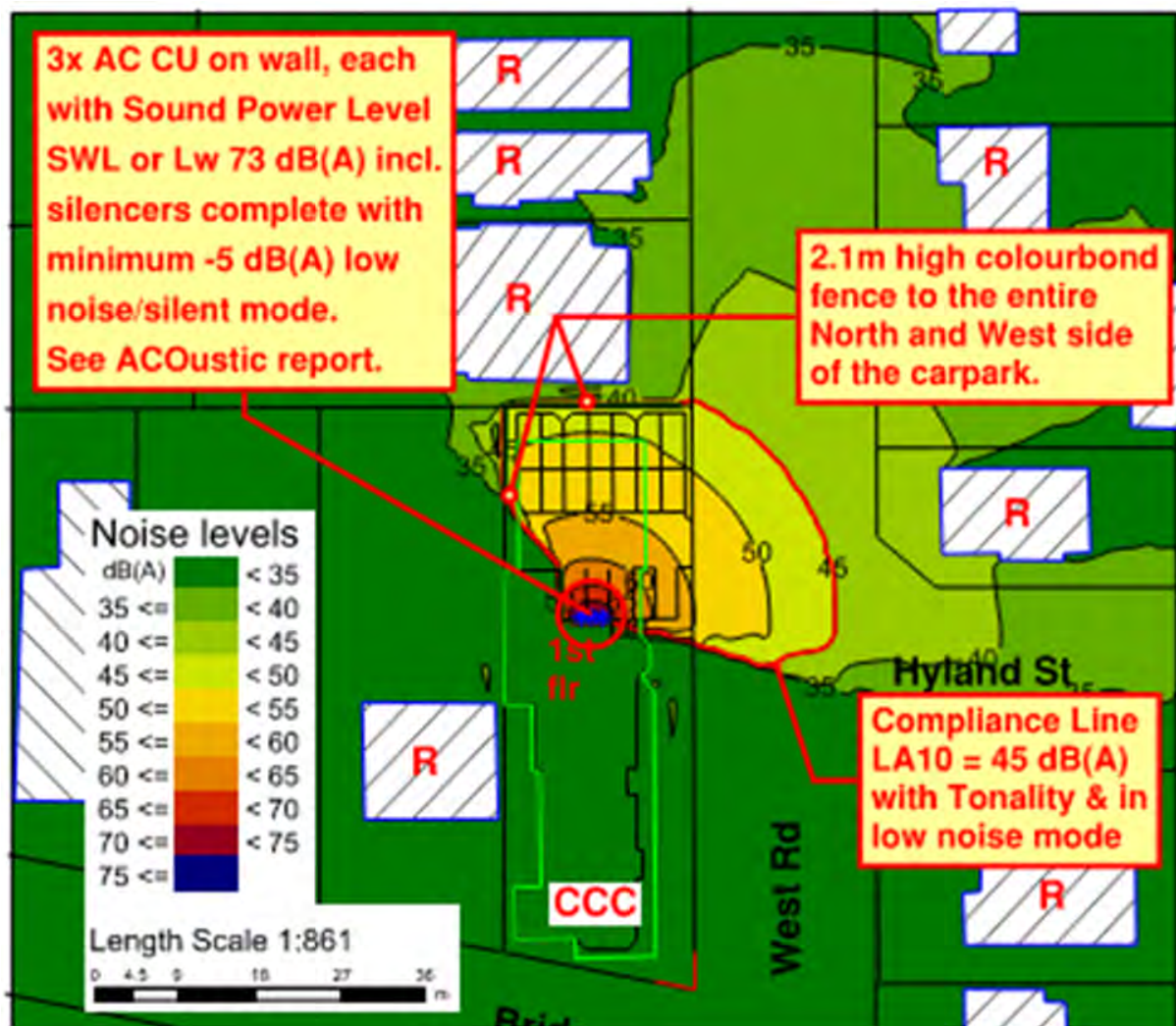


FIGURE F1 – AIR CONDITIONING - LA₁₀ inclusive of 5 dB(A) tonality penalty and 5 dB(A) low noise mode

- Notes:**
0. North is top of page.
 1. Compliance limit line is LA10 45 dB(A) after 0700 hrs.
 2. Receiver 1400mm high.
 3. Contour lines: AGFL = Above GROUND Floor Level of CCC.
 4. NDE's opinion is that the situation is compliant.



Annex G - Carpark

- G1. Carpark noises typically may comprise adults talking and children's voices, car radios and car doors.
- G2. Essentially the first and last persons on site are the CCC staff. The CCC staff parking should be restricted to car bays outside of the drop off zone in order to reduce parental stress by allowing them to park closer to the CCC doors.
- G3. Observations on various CCC site shows that pickup and drop offs are generally fairly quick especially in the morning. The morning drop offs tend to occur in several distinct groups being the trades/building/construction workers drop off at or prior to 0730 hours, the first school morning drop off at about 0815 hours (prior to older siblings being taken to school) and the second school morning drop off at about 0915 hours (when older siblings have been dropped off at school in the morning).
- G4.1. Measurements and observations were conducted at the Kids Campus CCC on 103 Canning Road Kalamunda on the morning of Wednesday 14 SEP 05 between 0730 to 0830 hours in order to obtain carpark noise data and discuss operational matters with the manager. This carpark contains about 21 car bays with about 15 on the residential side of the carpark and 6 on the CCC building side.
- G4.2. A series of three noise measurements on site at the Kids Campus CCC side of the residential boundary showed noise levels as follows: Cars doors closing L_{Amax} = 54 to 58 dB(A) at approximately 10 metres; and Children talking about L_{Amax} = 50 dB(A) at approximately 10 metres.
- ND Engineering measurement point near the residential boundary was located about 10 metres from the CCC entry doors. Parents were not made aware of ND Engineering's presence so that the behaviour was allowed to be as normal as possible. The entire carpark location was fairly reverberant. Parents were parking fairly close to either side of or in front of the CCC entry doors.
- The LA10 and LA1 measurements were meaningless as the noise from the nearby road heavily contaminated these two measurements however it would be safe to say that the LA1 and LA10 would be lower than the L_{Amax} measured values.
- G4.3. These L_{Amax} noise levels are not significant and given the short duration of the drop off the application of tonality and modulation penalties could not be applied to the measurements as the duration of the event was less than 10% of any representative measurement period. The only penalty that could be applied is if car doors are slammed resulting in the application of an impulsive penalty of +10 dB(A). The historical experience shows that for normal car door action the situation is one of compliance with the assigned noise levels however slamming of car doors would not be compliant prior to 7.00am and thus a noise management is required via signage.
- G5.1. ND Engineering's opinion is that the noise emissions within the carpark as currently presented, see Figures G5 and see Reference B and Annex A, would comply with the assigned noise levels subject to implementation of the recommendations.
- G5.2. ND Engineering recommendations are:
- Staff will be instructed not to arrive prior to 0600 hours and to be off site by 1900 hours; and
 - Staff parking can be located to be in the designated car bays noting that staff parking in bays 12 & 14 is not permitted prior to 7.00am; and
 - SIGNAGE is placed within the carpark asking parents/staff *not to slam car doors/boots and not to play loud music.*
- G6. Refer to the Section 'Recommendations' in the main body of the report.

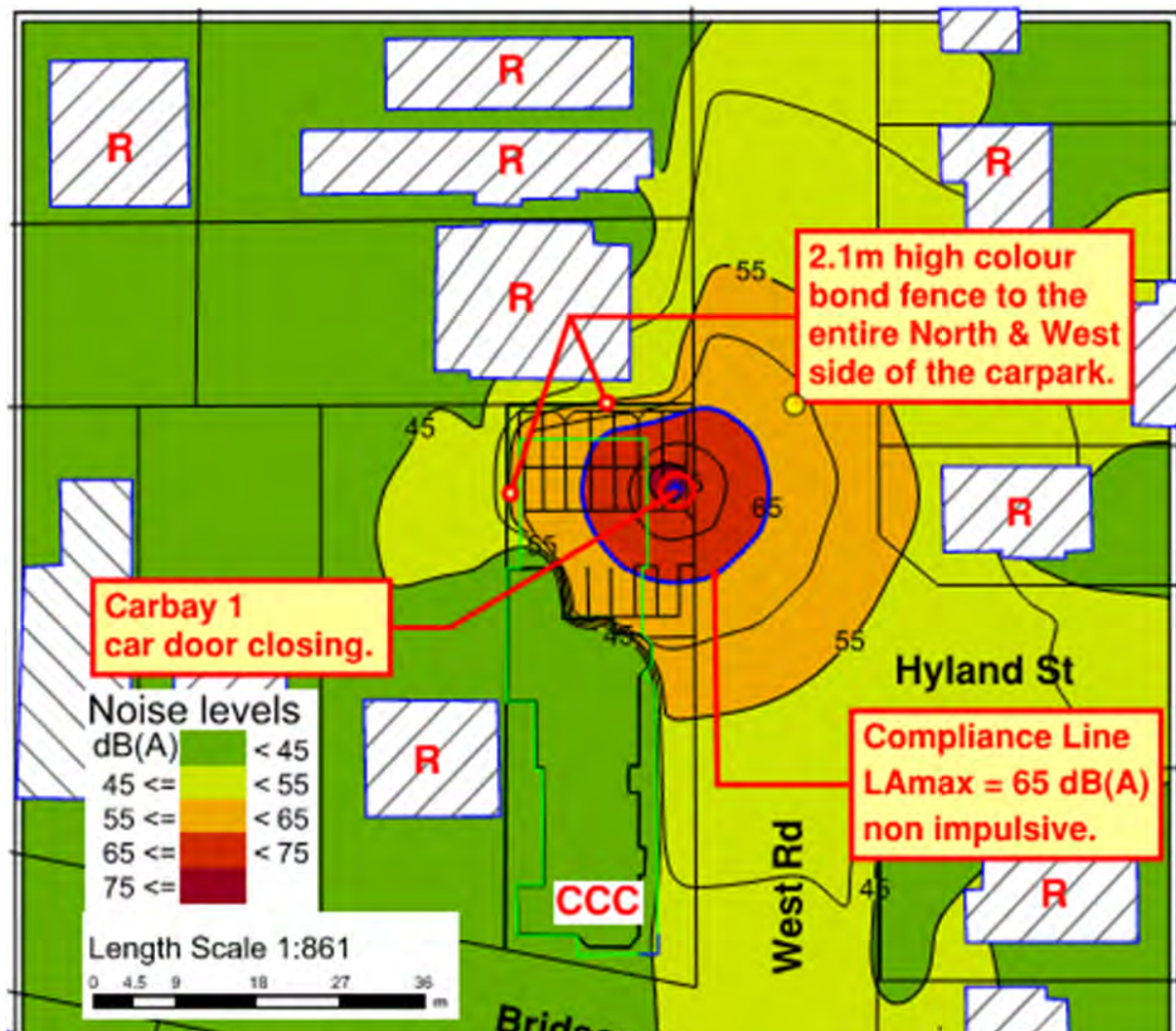


FIGURE G.1 – CARPARK BAY 1 - L_{Amax} NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is L_{Amax} 65 dB(A) after 0700 hrs, L_{Amax} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{WA} 87 dB(A).
 3. Contour lines: 1400 mm Above GROUND Level.
 4. NDE's opinion is that the situation is compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.

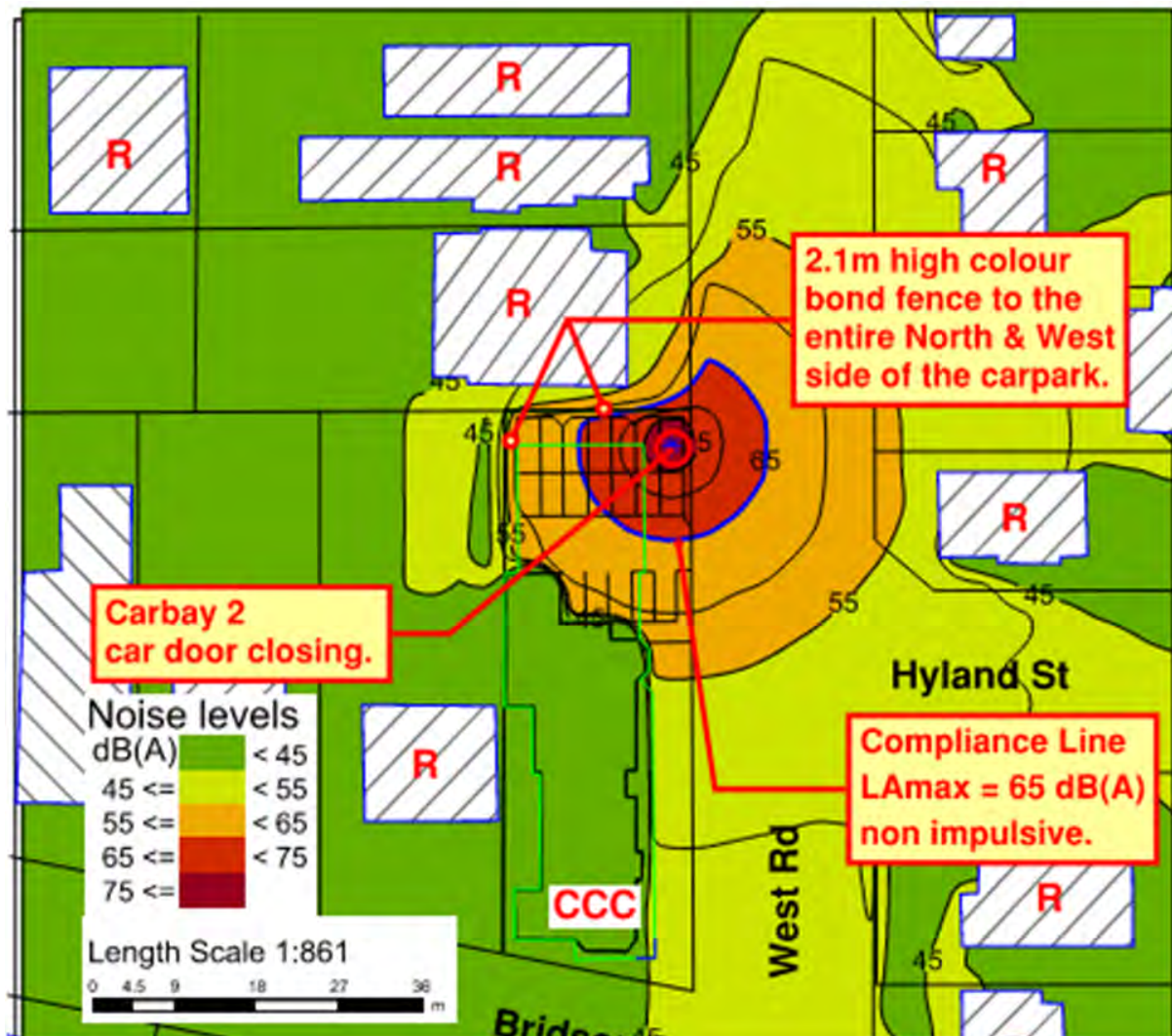


FIGURE G.2 – CARPARK BAY 2 - LA_{MAX} NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is LA_{max} 65 dB(A) after 0700 hrs, LA_{max} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{WA} 87 dB(A).
 3. Contour lines: 1400 mm Above GROUND Level.
 4. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.

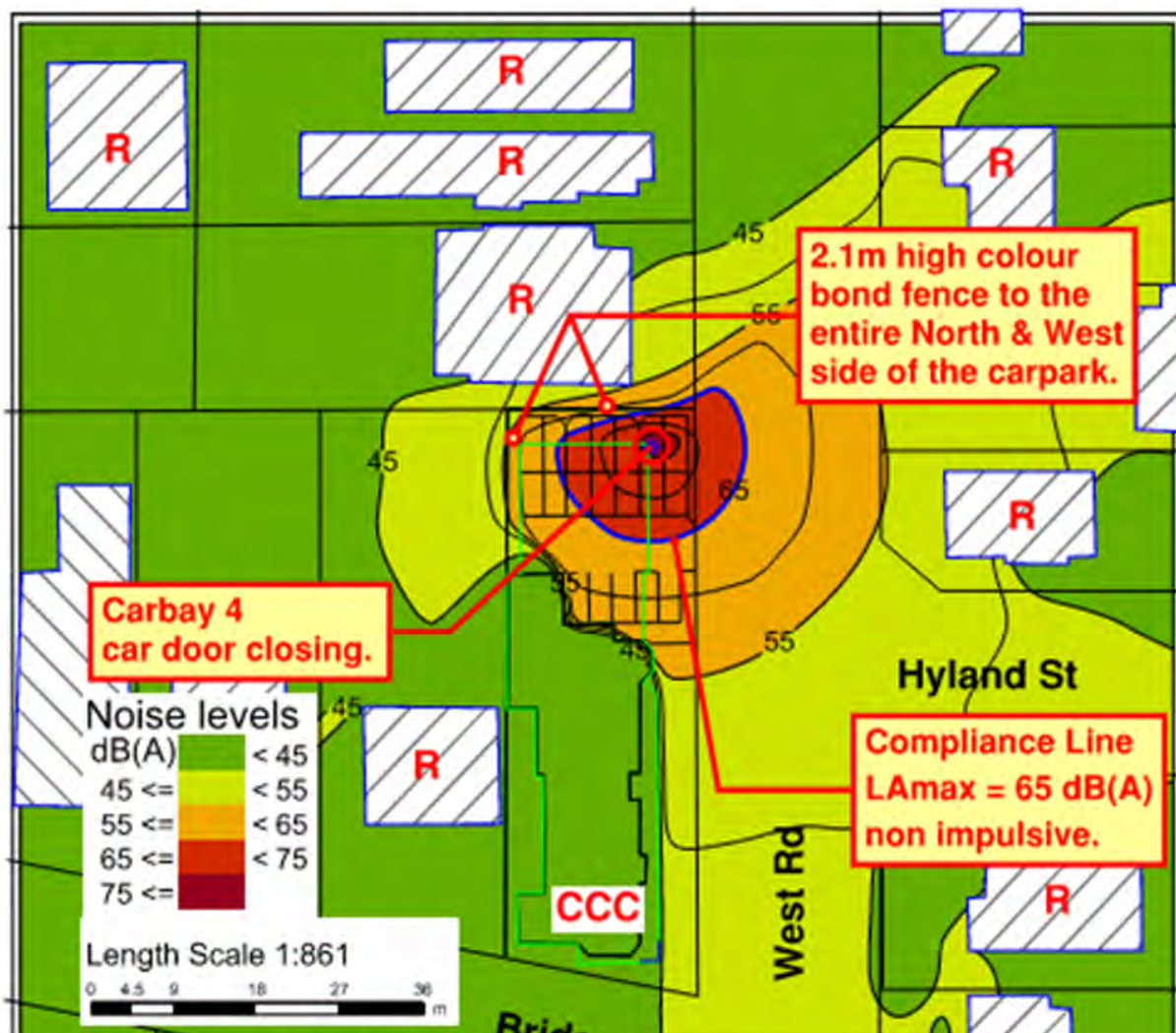


FIGURE G.4 – CARPARK BAY 4 - L_AMAX NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is L_{Amax} 65 dB(A) after 0700 hrs, L_{Amax} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{wA} 87 dB(A).
 5. Contour lines: 1400 mm Above GROUND Level.
 6. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.

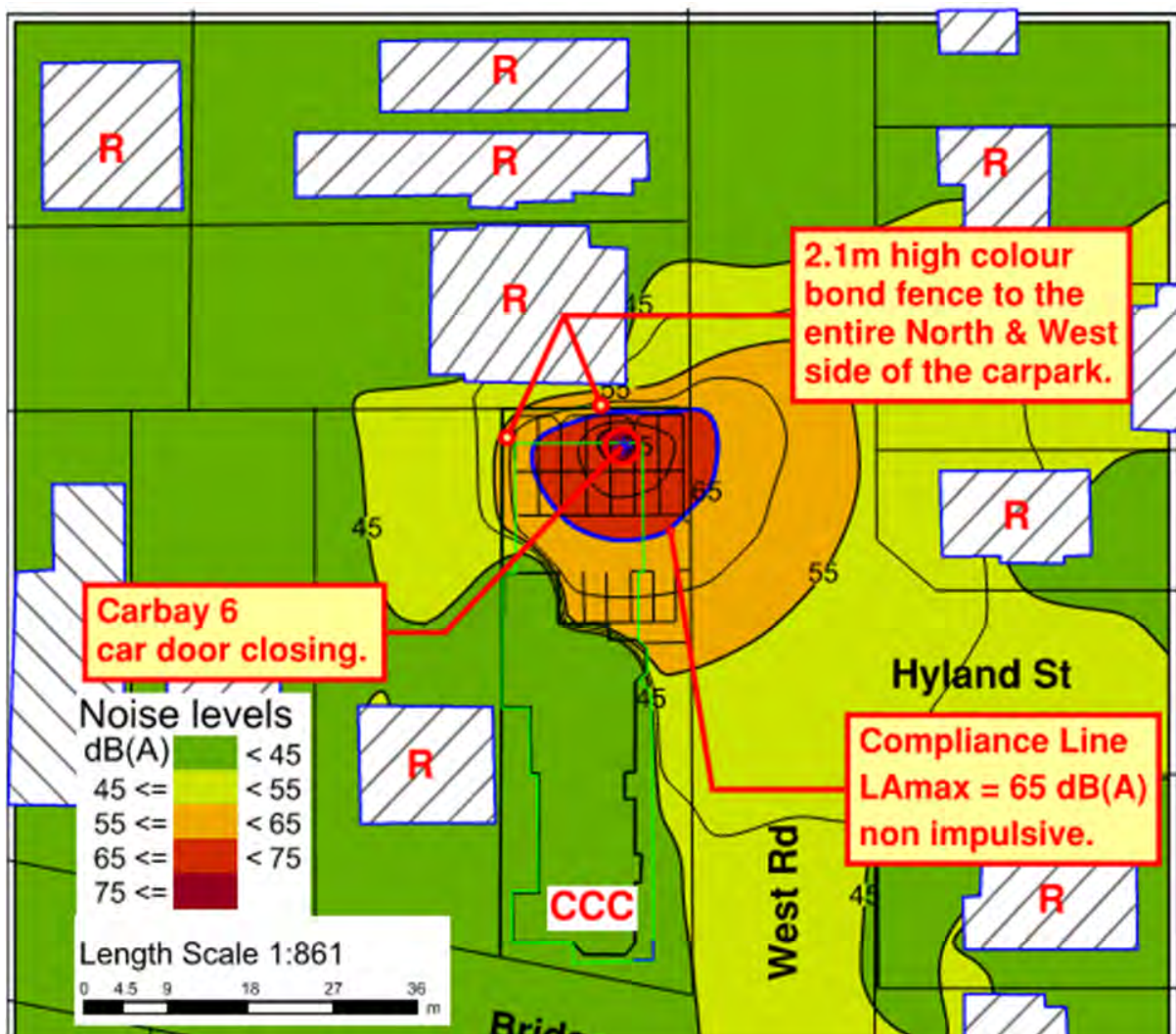


FIGURE G.6 – CARPARK BAY 6 - L_{MAX} NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is L_{Amax} 65 dB(A) after 0700 hrs, L_{Amax} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{WA} 87 dB(A).
 4. Contour lines: 1400 mm Above GROUND Level.
 2. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.

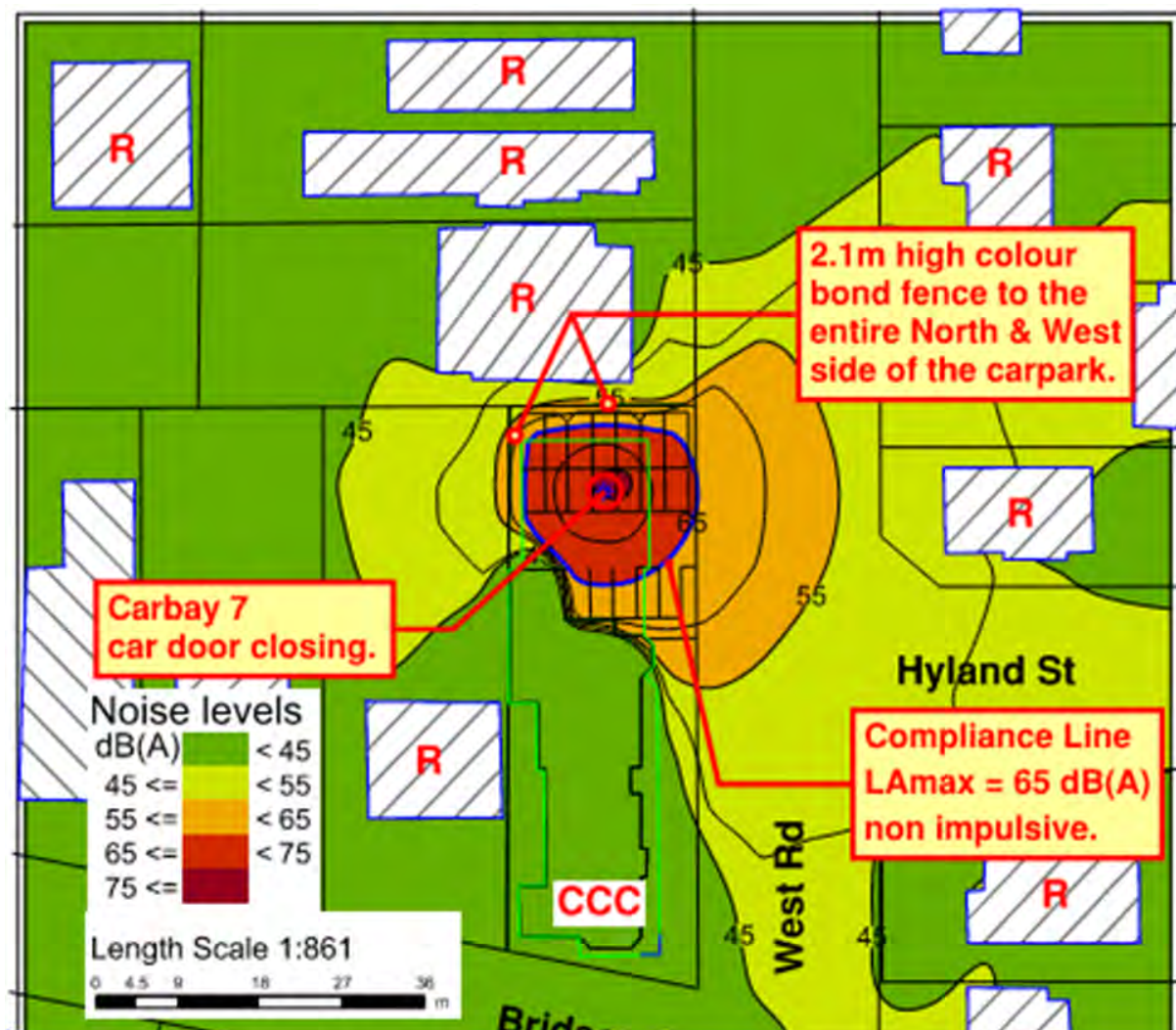


FIGURE G.7 – CARPARK BAY 7 - L_AMAX NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is L_{Amax} 65 dB(A) after 0700 hrs, L_{Amax} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{wA} 87 dB(A).
 3. Contour lines: 1400 mm Above GROUND Level.
 4. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.

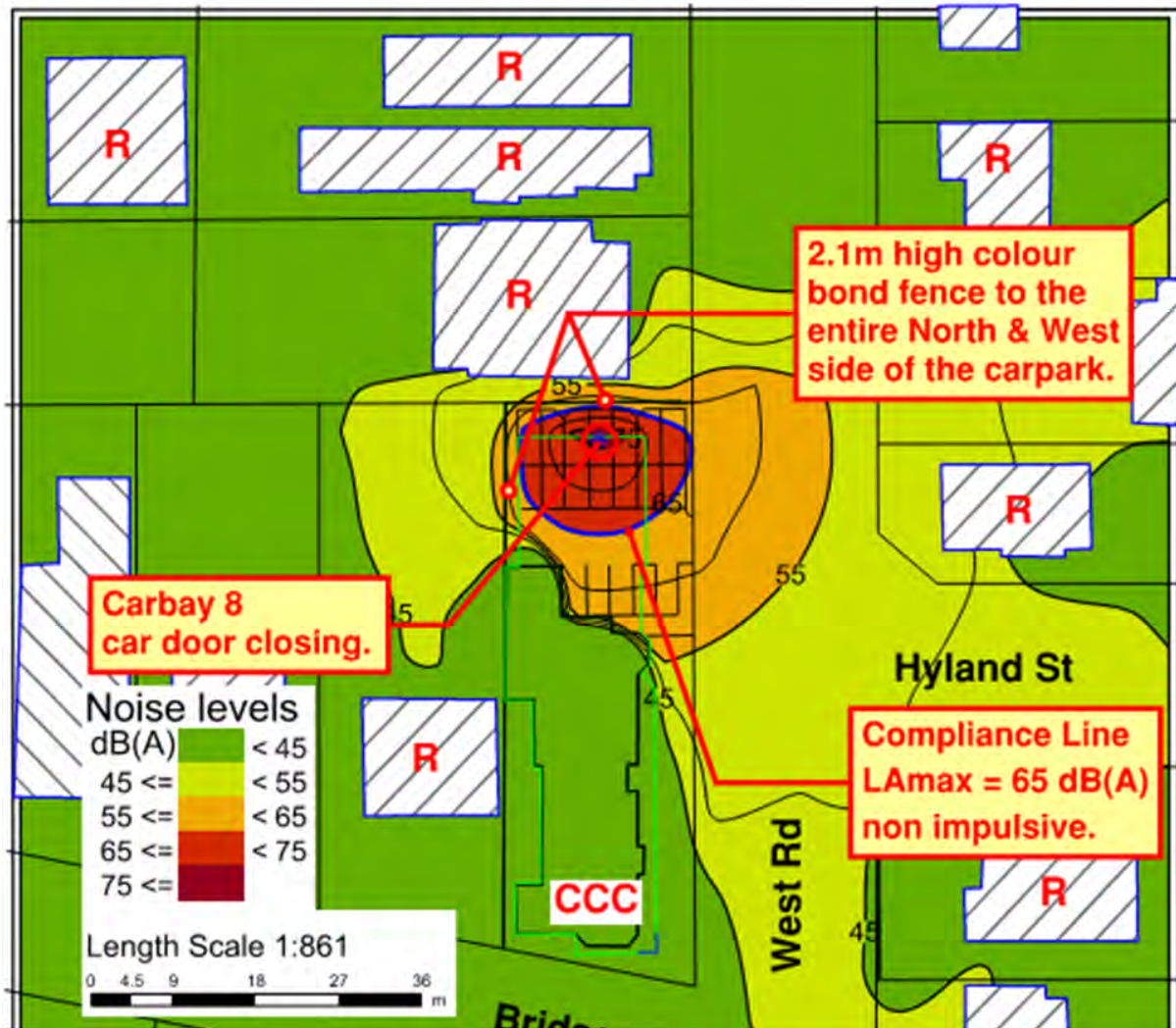


FIGURE G.8– CARPARK BAY 8 - LA_{MAX} NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is LA_{max} 65 dB(A) after 0700 hrs, LA_{max} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{WA} 87 dB(A).
 3. Contour lines: 1400 mm Above GROUND Level.
 4. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.

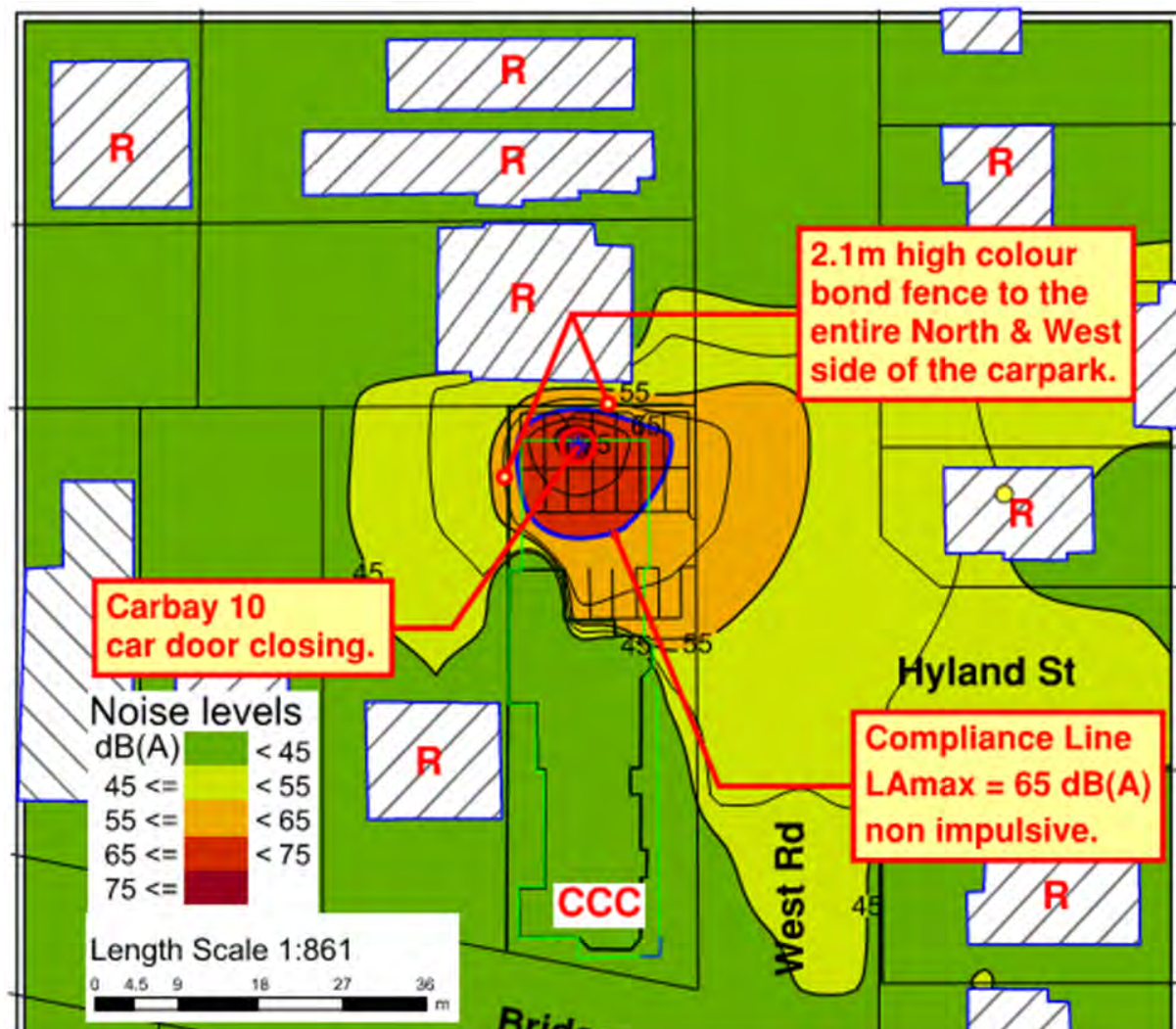


FIGURE G.10 – CARPARK BAY 10 - L_AMAX NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is L_{Amax} 65 dB(A) after 0700 hrs, L_{Amax} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{WA} 87 dB(A).
 3. Contour lines: 1400 mm Above GROUND Level.
 4. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.

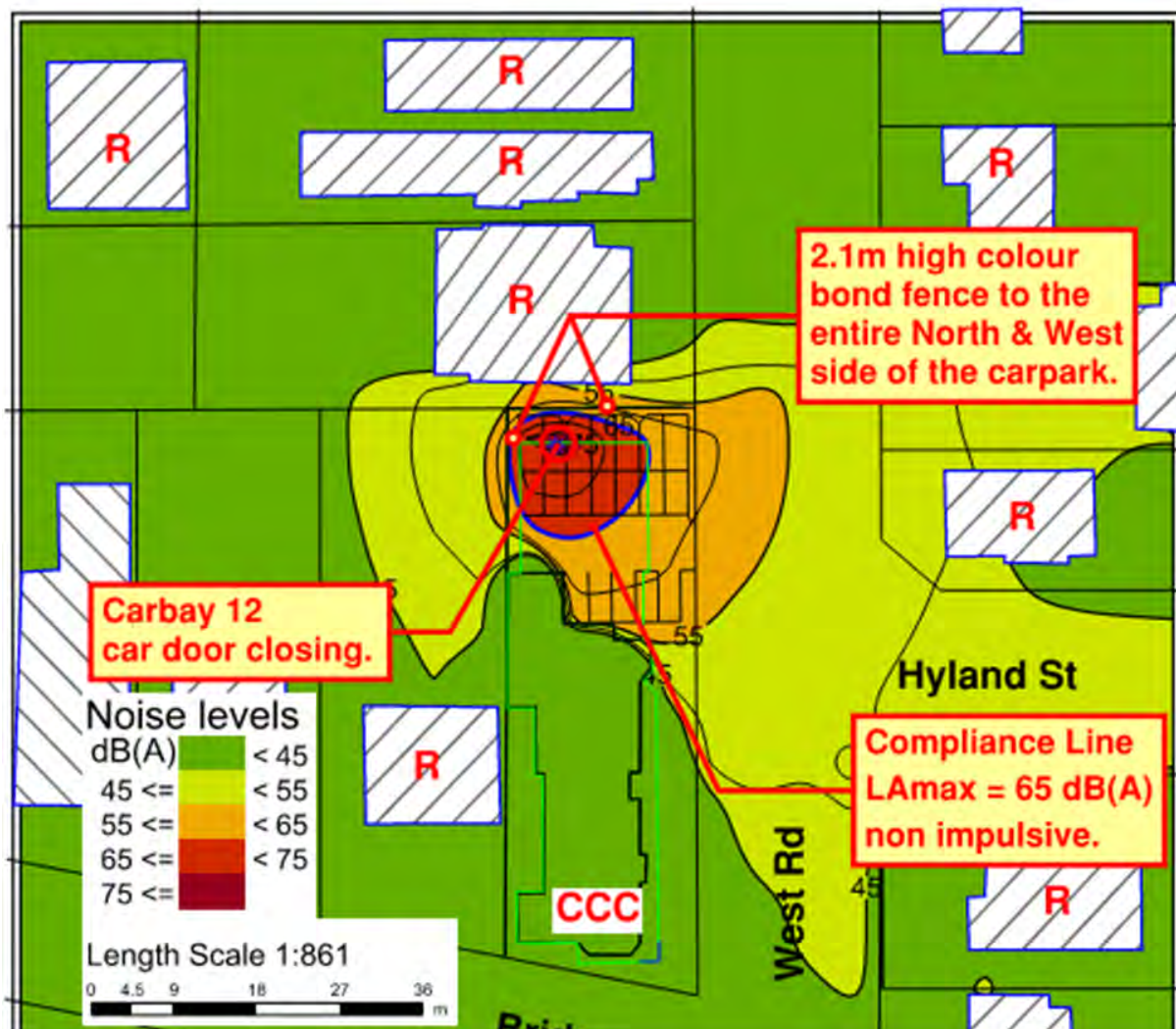


FIGURE G.12 – CARPARK BAY 12 Staff - LA_{MAX} NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is LA_{max} 65 dB(A) after 0700 hrs, LA_{max} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{WA} 87 dB(A).
 3. Contour lines: 1400 mm Above GROUND Level.
 4. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is non-compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.
 6. Carbay 12 use prior to 7.00am is not permitted.

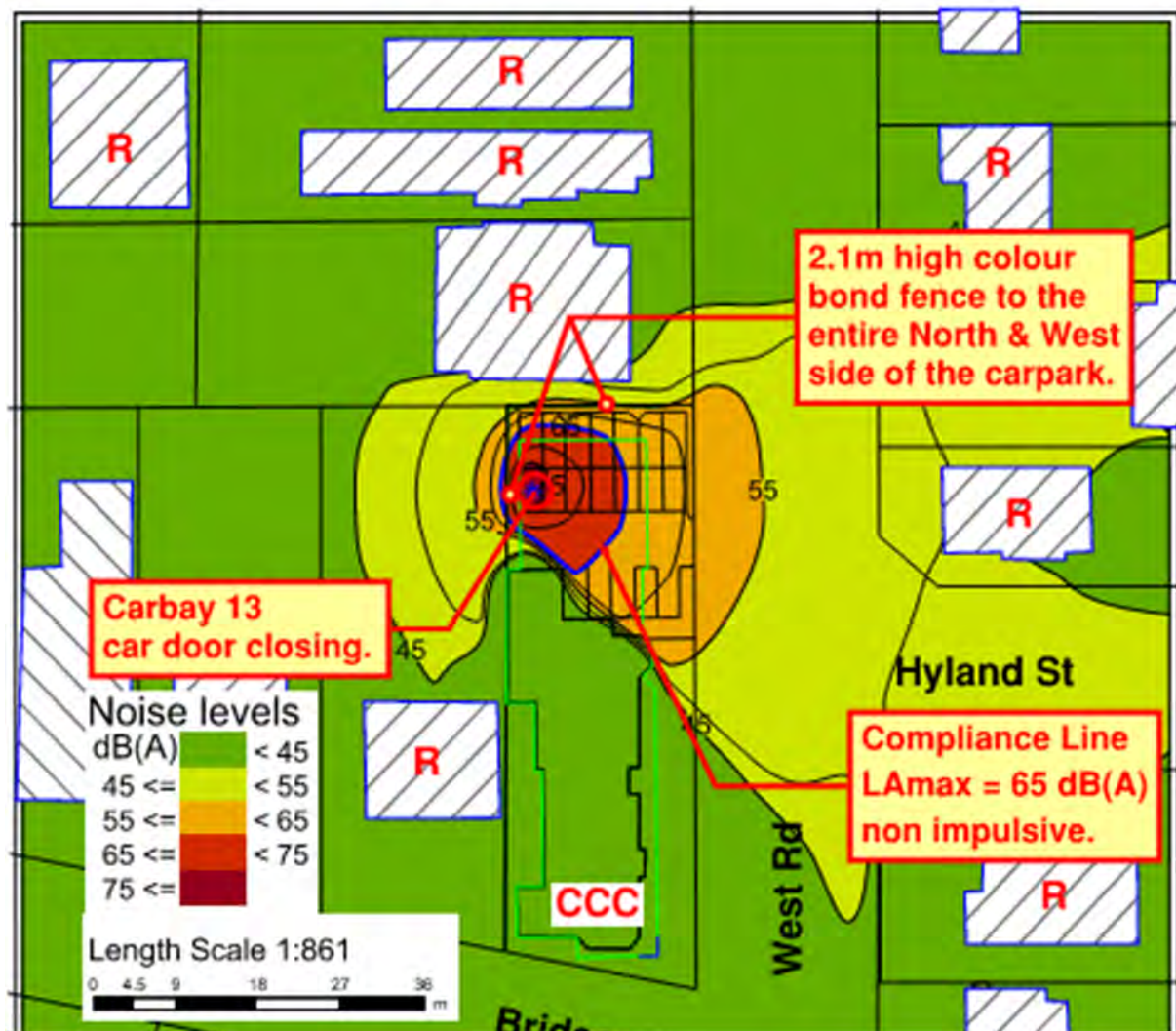


FIGURE G.13 – CARPARK BAY 13 Staff - LAMAX NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is LAmx 65 dB(A) after 0700 hrs, LAmx 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of LwA 87 dB(A).
 3. Contour lines: 1400 mm Above GROUND Level.
 4. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.

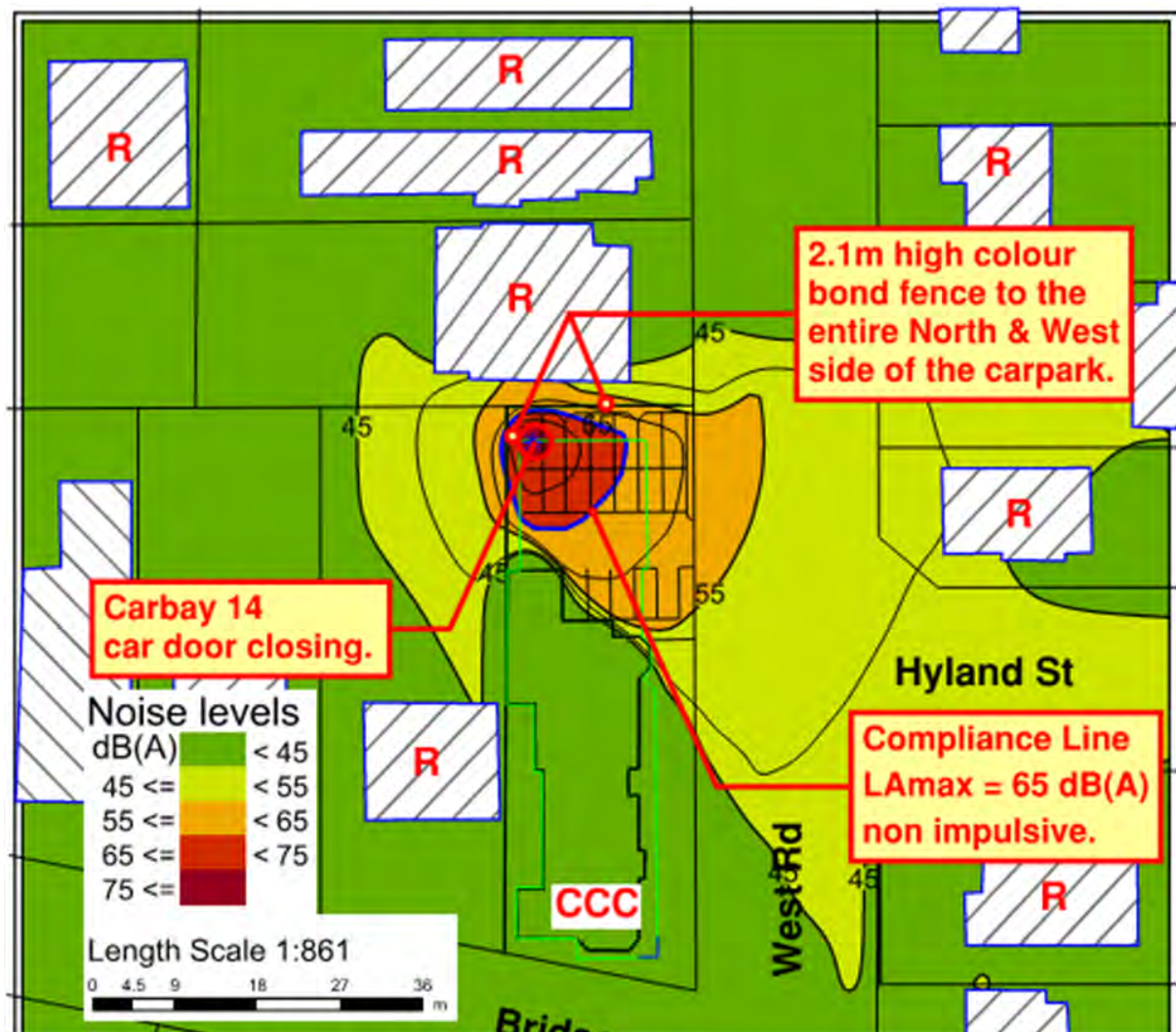


FIGURE G.14 – CARPARK BAY 14 Staff - LAMAX NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is L_{Amax} 65 dB(A) after 0700 hrs, L_{Amax} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{WA} 87 dB(A).
 3. Contour lines: 1400 mm Above GROUND Level.
 4. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is non-compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.
 6. Carbay 14 use prior to 7.00am is not permitted.

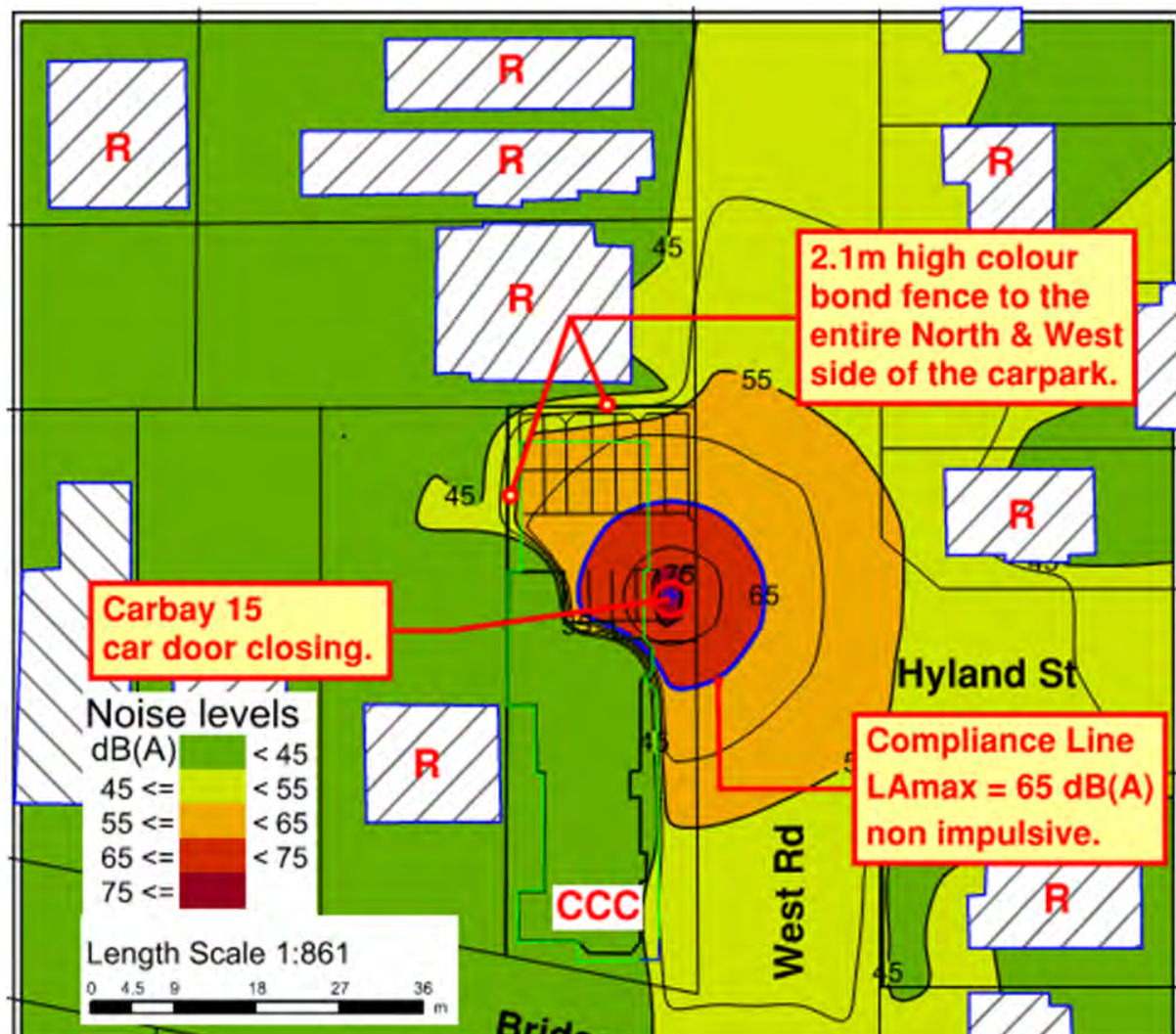


FIGURE G.15 – CARPARK BAY 15 Access - LAMAX NON-IMPULSIVE

- Notes:**
0. North is top of page.
 1. Compliance limit line is L_{Amax} 65 dB(A) after 0700 hrs, L_{Amax} 55 dB(A) before 0700 hrs.
 2. Receiver 1400mm high with Car Door closing noise sources at 1000mm high with a sound power level of L_{WA} 87 dB(A).
 3. Contour lines: 1400 mm Above GROUND Level.
 4. North fence to stop 1.5m short of NE corner boundary.
 5. NDE's opinion is that the situation is compliant for non-impulsive car door closing prior to 7.00am and impulsive car door closing after 7.00am.

PO Box 2124, Malaga WA 6944
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M: 0412 679 431

ND Engineering
Consulting Engineers

nde

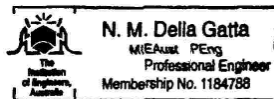
Ms Trish Byrne
General Manager – Property
Rpoint Properties Rgate Property Group
172 Burswood Road, BURSWOOD WA 6100

0402 600 378
trish@rpoint.com.au

2311111 BASSENDEAN SUPPLEMENTARY ADVICE No 2
CHILD CARE CENTRE - LOT 85, 93 WEST ROAD, BASSENDEAN WA 6054

1. This supplementary advice No 2 is to provide additional information on the carpark based on the CCC opening at 7am with an impulsiveness correction of +10 dB(A), added to the base sound power level of LwA 87 dB(A), for impulsive car doors closing and comparing with and without the crank wall.
2. The attachment on the following pages provides the following scenarios for two selected carbays 2 and 14:
 - a. 2.1m boundary fence (pages 2 to 3).
The 2.1m wall without the crank does not provide protection to the Northern neighbour at 92 West Road post 7am.
 - b. 2.1m boundary fence (pages 4 to 5) + a crank wall (sketch page 6)
The 2.1m wall with the crank provides protection to the Northern neighbour at 92 West Road post 7am.
3. If you have any queries please contact me.

Yours Sincerely



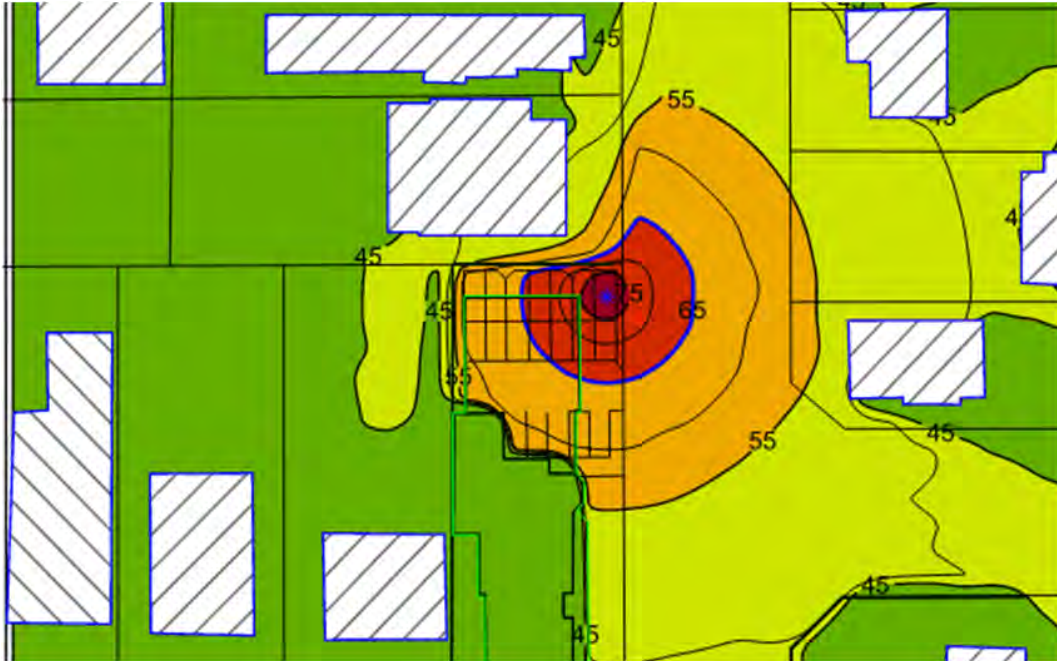
21 OCTOBER 2024

ATTACHMENTS 5x pages

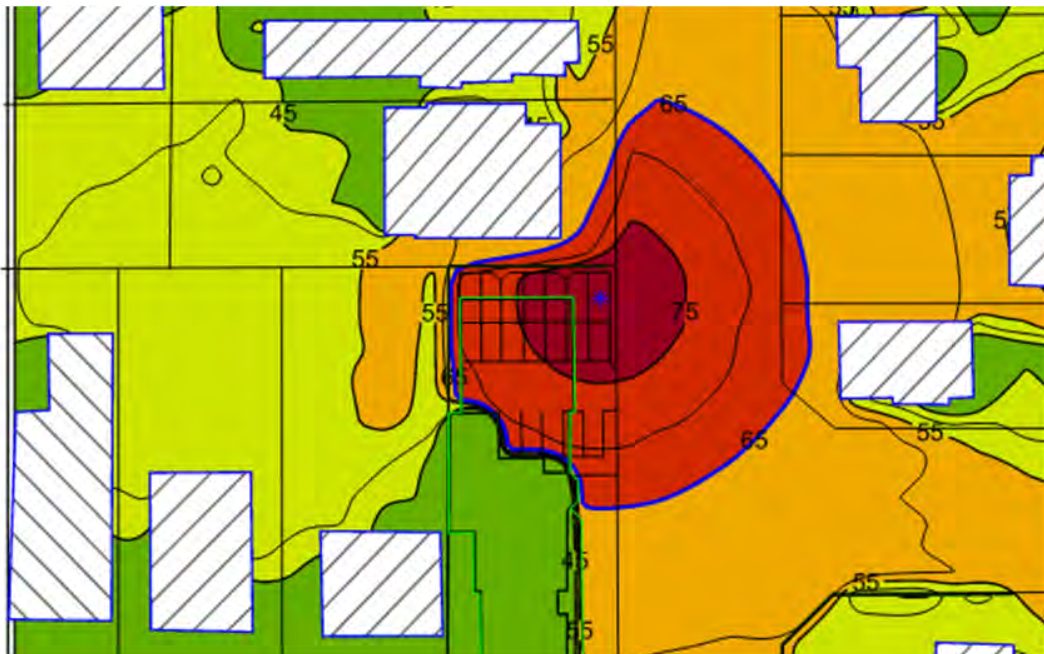
ATTACHMENT



CAR BAY 2 with 2.1m boundary fence



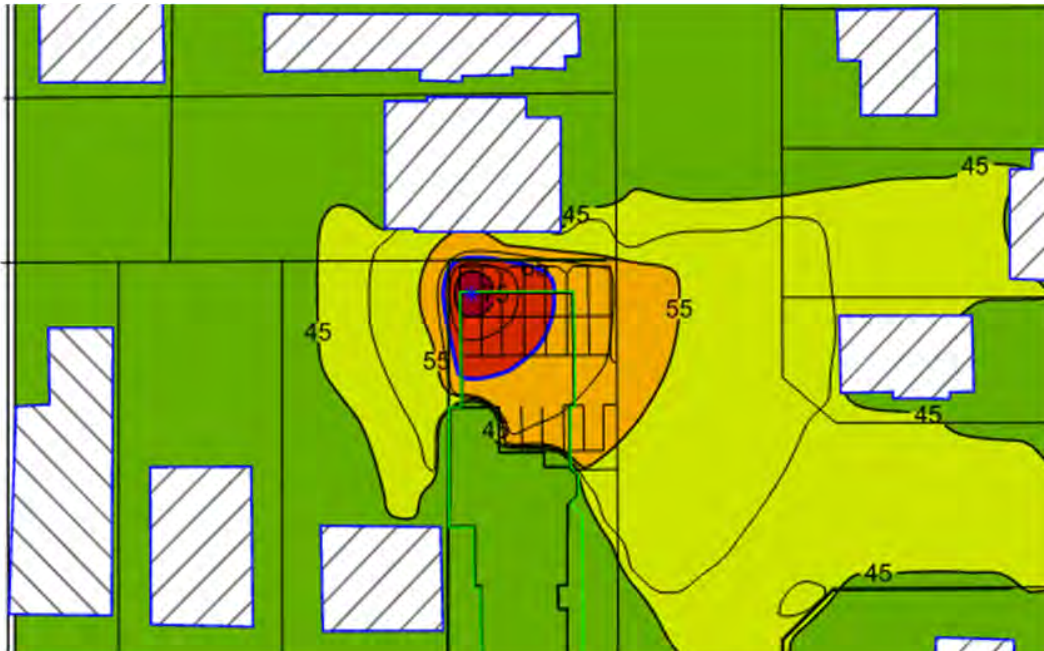
R502 veh_LAmax 65 Non-Imp_post 7am



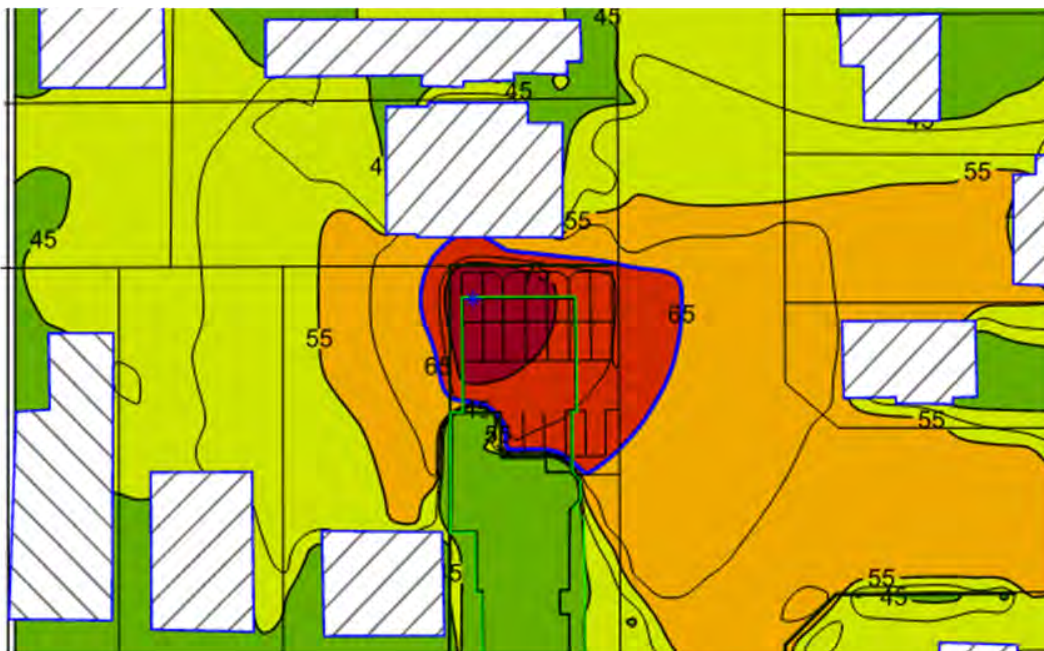
R502 veh_LAmax 65 Imp_post 7am



CAR BAY 14 with 2.1m boundary fence



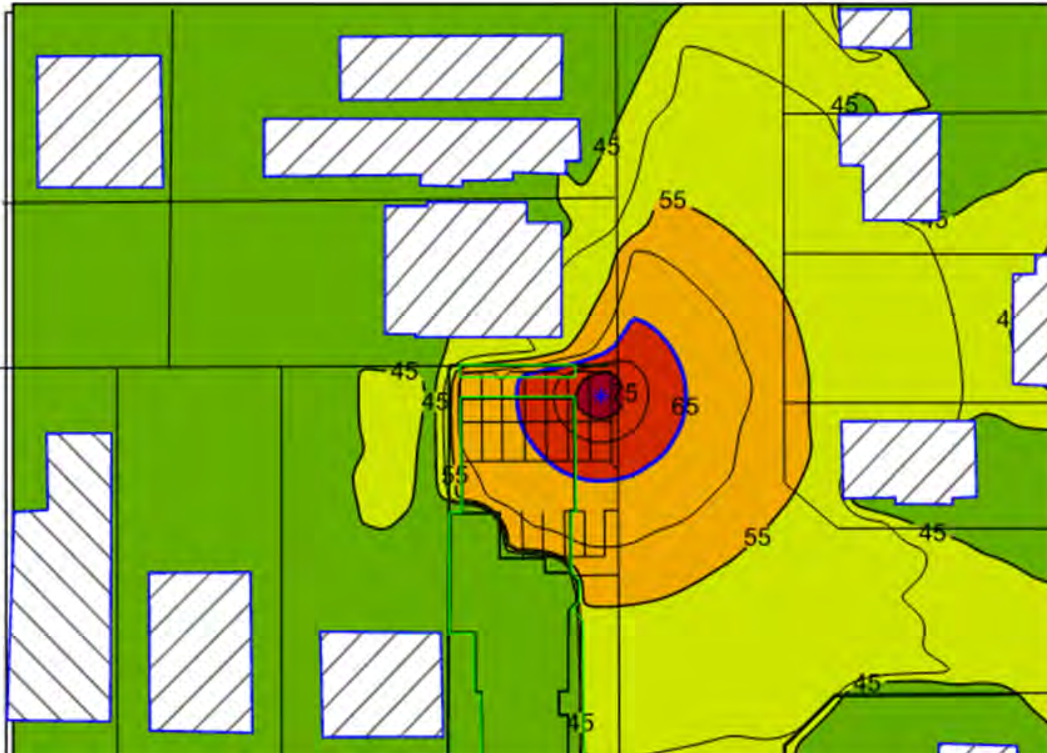
R514 veh_LAmax 65 Non-Imp_post 7am
compliant



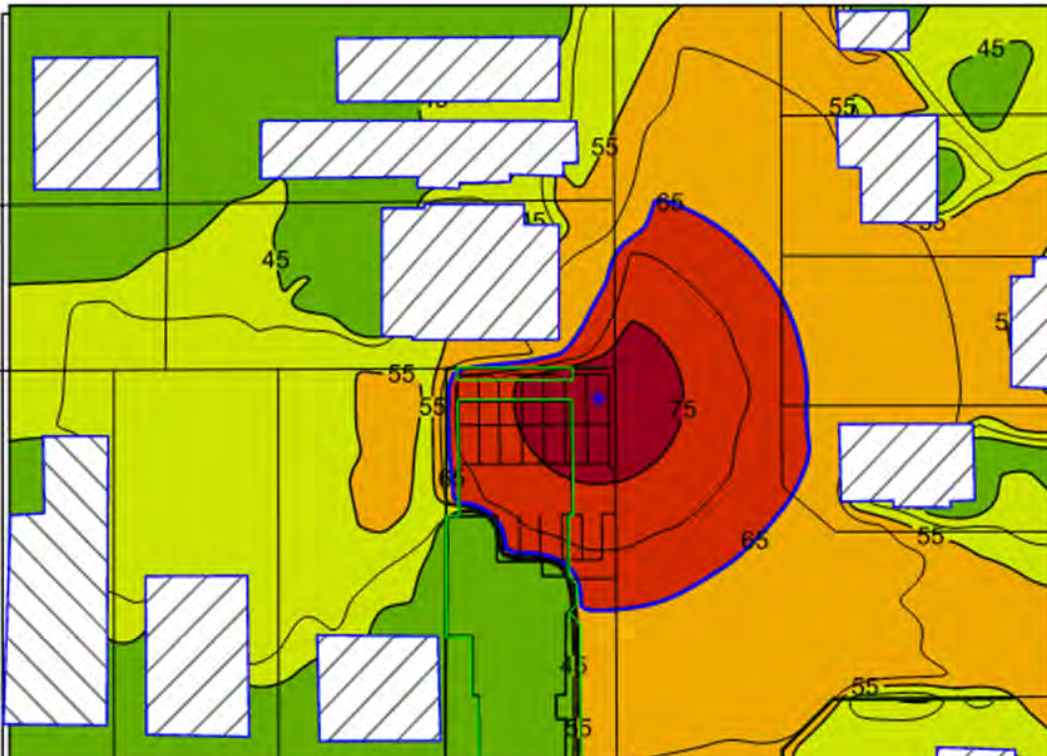
R514 veh_LAmax 65 Imp_post 7am
Non-compliant



CAR BAY 2 with 2.1m Boundary Fence + CRANK WALL



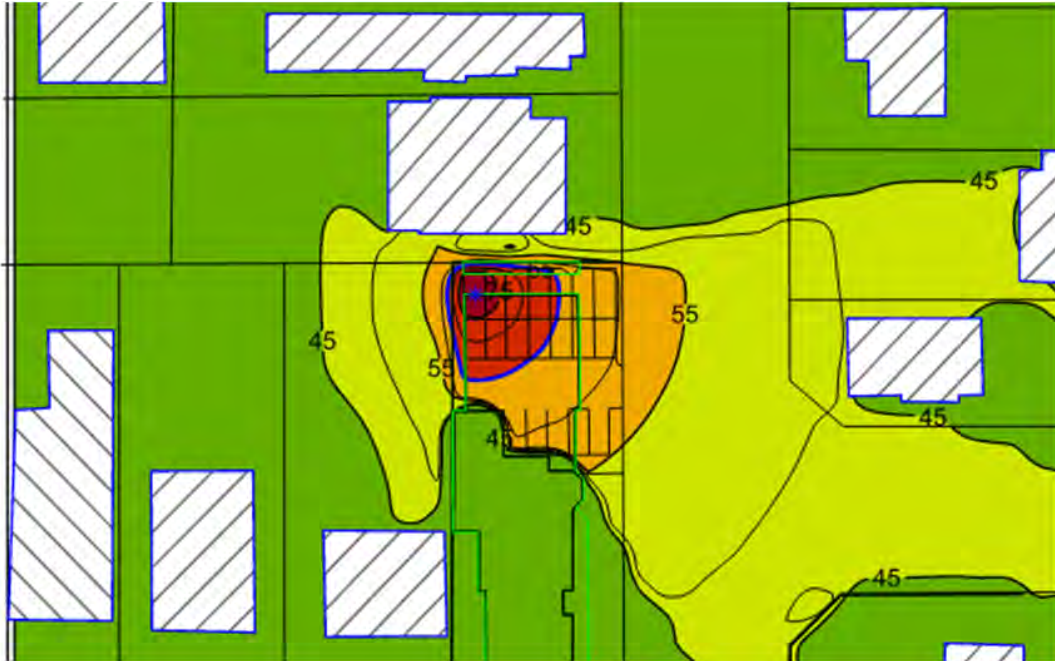
R502 veh_LAmax 65 Non-Imp_post 7am - with 2.1m Boundary Fence + CRANK WALL
compliant



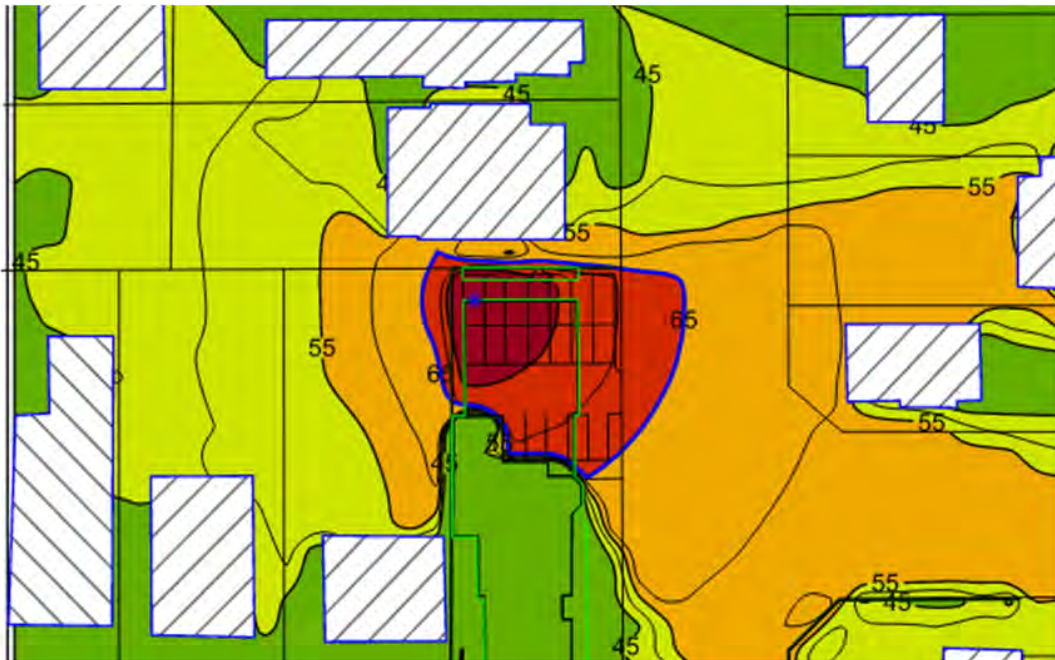
R502 veh_LAmax 65 Imp_post 7am - with 2.1m Boundary Fence + CRANK WALL
compliant



CAR BAY 14 with 2.1m Boundary Fence + CRANK WALL



R514 veh_LAmax 65 Non-Imp_post 7am + CRANK WALL
compliant



R514 veh_LAmax 65 Imp_post 7am + CRANK WALL
compliant



C/Bond Roof @ 35° Pitch

69c

61c

58c

30c

Stone cladding

Ceiling Level 66c

2743

3.9 m

First Floor 34c

U/Side of Slab 31c

2.7 m

20°

2657

1.5 m

2.1 m

Ground Floor 0c

bdy

Environmentally Sustainable Design (ESD) Report



Proposed Childcare Centre

Lot 85 (#94) West Road, Bassendean WA

Job No:	23019
Version:	4.0
Date:	31 Oct 2024

Executive Summary

This Environmentally Sustainable Design (ESD) report has been prepared for the proposed childcare centre located at Lot 85 (#94) West Road, Bassendean, in response to recommendations from the Town of Bassendean's Design Review Panel. The Panel encouraged the engagement of an ESD professional to consider sustainability measures, including material selection, electric vehicle charging points, heat pumps, water catchment, and optimal solar panel placement. In alignment with this, the Metropolitan Redevelopment Authority's (MRA) Sustainable Design Assessment Report (SDAR) methodology has been applied to guide the incorporation of sustainability initiatives into the development.

The project integrates the following key environmentally sustainable design elements:

- **Material Selection:** Sustainable materials with low embodied energy and high durability have been selected, contributing to reduced environmental impact and alignment with Green Star best practices.
- **Electric Vehicle Charging Points:** The design incorporates provisions for electric vehicle (EV) charging infrastructure, promoting low-carbon transport solutions and supporting future transitions to electric mobility.
- **Heat Pumps:** Energy-efficient heat pumps are specified to provide heating and cooling, significantly reducing greenhouse gas emissions compared to traditional systems and enhancing thermal comfort while minimising energy use.
- **Water Catchment:** A rainwater harvesting system is proposed to capture and store rainwater for irrigation and non-potable uses, contributing to water conservation efforts and reducing reliance on mains water.
- **Solar Panel Placement:** The optimal placement of solar panels has been determined to maximize energy generation on-site, supporting the project's commitment to renewable energy and reducing operational carbon emissions.

By adopting a Sustainable Design Assessment Report (SDAR) pathway, the project demonstrates a strong commitment to environmentally sustainable design, contributing to energy efficiency, water conservation, and lower emissions. These efforts align with the Town of Bassendean's planning policies and the Design Review Panel's recommendations, ensuring the Childcare Centre delivers long-term environmental, social, and economic benefits for the community.



Project Overview

The proposed development is a single storey childcare centre development. The project site is located at Lot 85 (#94) West Road, Bassendean.



Introduction

MRA Development Policy 1 – Green Building Requirements

- The MRA Development Policy 1 – Green Building policy identifies ten key Sustainable Building Categories that must be addressed using a holistic Environmental Sustainable Design (ESD) review:
 - Indoor Environment Quality (IEQ)
 - Energy Efficiency
 - Water Efficiency
 - Stormwater Management
 - Building Materials
 - Transport
 - Waste Management
 - Urban Ecology
 - Innovation
 - Construction and Building Management

Objectives

- Provide a sustainability pathway that demonstrates sustainability initiatives have been incorporated into the development in accordance with the Town of Bassendean's local planning policies.

Proposed Sustainability Pathway

- The ten key Sustainable Design Categories specified in MRA Development Policy 1 – Green Building provide a holistic rating framework for the design and construction of new buildings and major refurbishments, whereby the sustainability attributes of a building are assessed through ten categories.
- The ten key Sustainable Design Categories specified in MRA Development Policy 1 – Green Building closely align with the 9 categories that comprise the Green Star Design & As Built rating tool.
- Therefore, the proposed sustainability pathway is to achieve an equivalent rating, using the Green Star Design & As Built rating tool as a guide.

Applicable Performance Standards

- 4 Star Green Star Design & As Built V1.3 equivalency



Indoor Environment Quality (IEQ)

Objectives

- To achieve a healthy indoor environment quality for the wellbeing of building occupants; and
- To provide a naturally comfortable indoor environment which will lower the need for building services, such as artificial lighting, mechanical ventilation and cooling and heating devices.

Relevant Standards

- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response

- Indoor Air Quality Provision of Outdoor Air.
- Indoor Air Quality Exhaust or Elimination of Pollutants.
- Acoustic Comfort Internal Noise Levels.
- Acoustic Comfort Reverberation.
- Lighting Comfort Minimum Lighting Comfort.
- Lighting Comfort General Illuminance and Glare Reduction.
- Lighting Comfort Localised Lighting Control.
- Visual Comfort Glare Reduction.
- Visual Comfort Daylight.
- Indoor Pollutants Paints, Adhesives, Sealants and Carpets.
- Indoor Pollutants Engineered Wood Products.
- Thermal Comfort.

Energy Efficiency

Objectives

- To ensure the efficient use of energy; and
- To reduce total operating greenhouse emissions; and
- To reduce energy peak demand.

Relevant Standards

- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response

- High efficiency LED lighting.
- High efficiency HVAC systems.
- High efficiency HWS.
- High efficiency façade design.
- Provision of 10kW photovoltaic system to be installed on the roof.
- No fossil fuels burnt on site to generate electricity, heating or cooling.



Water Efficiency

Objectives

- To ensure the efficient and sustainable use of water resources; and
- To minimise total potable water use; and
- To maximise the use of alternative water sources.

Relevant Standards

- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response

- High efficiency fittings and fixtures.
- Drip irrigation with moisture sensors for landscaping.
- Low water use landscape design.

Stormwater Management

Objectives

- To reduce the impact of stormwater run-off; and
- To improve the water quality of stormwater run-off such as erosion and pollution of waterways; and
- To achieve best practice stormwater quality outcomes; and
- To incorporate water sensitive urban design principles.

Relevant Standards

- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response

- Reduction peak discharge and pollution to stormwater system

Building Materials

Objectives

To minimise the environmental impacts of materials used by encouraging the use of materials with a favourable lifecycle assessment based on the following factors:

- Fate of material
- Recycling/reuse
- Embodied energy
- Impact on biodiversity
- Human health
- Environmental toxicity including carbon emissions
- Environmental responsibility

Relevant Standards

- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response



- Minimise mass of steel framing.
- Sourcing of responsible structural and reinforcing steel
- Sourcing of responsible PVC products including permanent formwork, pipes, flooring, blinds and cables
- Target 90% recycling rate for construction and demolition waste.

Transport

Objectives

- To minimise car dependency; and
- To ensure that the built environment is designed to promote the use of public transport, walking and cycling.

Relevant Standards

- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response

- Pram locks to promote walkability for parents.
- Secure bicycle parking and end of trip facilities.
- Connection with local bike and pedestrian pathways.
- Access to public transport.

Waste Management

Objectives

- To ensure waste avoidance, reuse and recycling during the design, construction and operation stages of development; and
- To ensure long term reusability of building materials.

Relevant Standards

- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response

- Waste streams separated.
- Dedicated waste storage areas with appropriate access.

Urban Ecology

Objectives

- To protect and enhance biodiversity; and
- To provide sustainable landscaping such as low water use, low fertiliser requirements and local native plant species selection; and
- To protect and manage all remnant indigenous plant communities; and
- To encourage the planting of indigenous vegetation.

Relevant Standards



- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response

- Light coloured roof to reduce Heat Island Effect.
- Maximise use of native planting throughout all proposed landscaping.

Innovation

Objectives

To encourage innovative technology, design and processes in all development, which positively influence the sustainability of buildings.

Relevant Standards

- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response

- Electric vehicle charging station.
- Onsite renewable energy (10kW PV system).

Construction and Building Management

Objectives

To encourage a holistic and integrated design and construction process and ongoing high performance.

Relevant Standards

- Green Star Design and As-Built V1.3 equivalent rating system.

Proposed Design Response

- Definition of environmental targets.
- Service and maintainability review to assist handover processes.
- Commissioning & tuning of building systems, to ensure the building optimises the investment into energy efficiency that has occurred.
- Building information to be provided to all relevant stakeholders.
- Environmental Management System for construction.

Conclusion

The proposed sustainability pathway strategy will achieve compliance with a 4-star Green Star equivalency rating. The final initiatives will be specified on the construction drawings and include the relevant obligations for As-Built deliverables.



Briscola PTY LTD

Address:Lot 85 (#94) West Road, Bassendean

Childcare Centre

Job Number: 23019

Drawing No	Description
01	Cover Page
02	3D
03	Existing Site Survey
04	Site Plan
05	Context Plan
06	Aerial Map
07	Ground Floor Plan
08	First Floor Plan
09	Roof Plan
10	Elevations

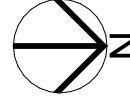




19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	10.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	17.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24

Revision Description 19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
3D

Scale: Sheet Size: A1

Project No: 23019 Revision Number: 19.00

Drawing No.: 02 of 10

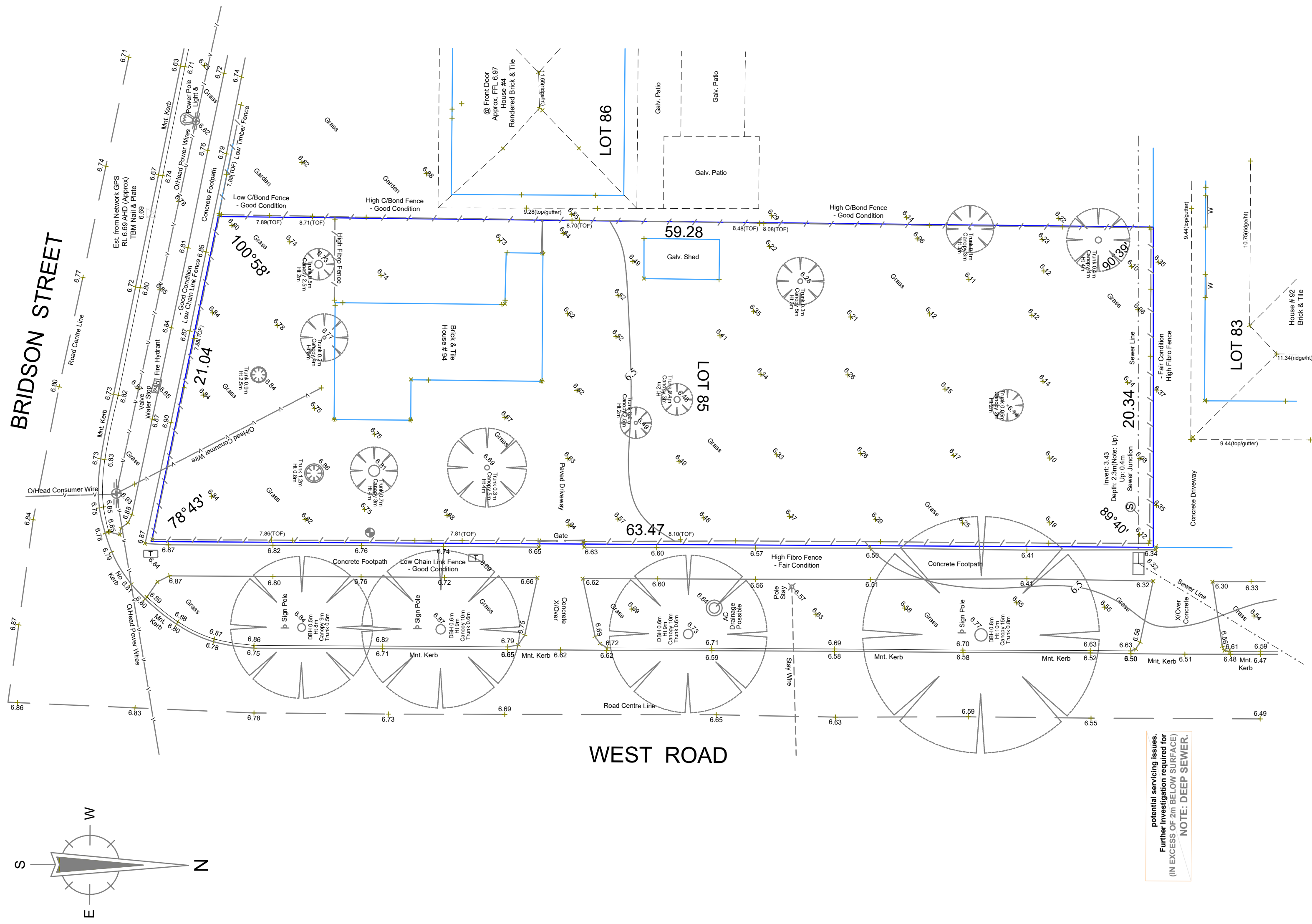
MB
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E: frank@macribuilders.com.au
P: 9444 9929
M: 0457 727 766

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FEATURE SURVEY		STREET NAME : West Road		LOT # : 85	HOUSE # : 94	SUBURB : Bassendean	LOCAL AUTHORITY : Town of Bassendean	LOT AREA : 1239m ²	R CODE : R20	SURVEY DATE : 21/11/2022	SCALE AT A3 SIZE : 1:250
CLIENTS DETAILS:											
Briscola Pty Ltd											
LEVEL DATUM : AHD (Approx.)		DWG REF : West 94 F - v1.0									
LEGEND											
WATER METER		SEWER AC		TELSTRANBN PIT		POWER POLE		TREE (TO SCALE)		W WINDOW	
SEWER CONN.		(TOF) TOP OF FENCE		TEMP. BENCHMARK							



19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	15.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	31.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
10.00	Changes	19.01.24

Revision Description

Project North:

Client

Briscola PTY LTD

Project Name

Childcare Centre

Project Address

Lot 85 (#94) West Road, Bassendean

Drawing Title:

Existing Site Survey

Scale:

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Sheet Size:

A1

Project No:

23019

Revision Number:

19.00

Drawing No.:

03 of 10

Client

Briscola PTY LTD

Project Name

Childcare Centre

Project Address

Lot 85 (#94) West Road, Bassendean

Drawing Title:

Existing Site Survey

Scale:

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Sheet Size:

A1

Project No:

23019

Revision Number:

19.00

Drawing No.:

03 of 10

Client

Briscola PTY LTD

Project Name

Childcare Centre

Project Address

Lot 85 (#94) West Road, Bassendean

Drawing Title:

Existing Site Survey

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Project No:

23019

Revision Number:

19.00

Drawing No.:

03 of 10

Client

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Project Name

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Project Address

Lot 85 (#94) West Road, Bassendean

Drawing Title:

Existing Site Survey

Scale:

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Project No:

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Revision Number:

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Drawing No.:

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Client

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Drawing Title:

Existing Site Survey

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Project No:

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Drawing No.:

03 of 10

Client

Briscola PTY LTD

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Drawing No.:

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Drawing No.:

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Project No:

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Drawing No.:

03 of 10

Client

Briscola PTY LTD

Project Name

Childcare Centre

Project Address

Lot 85 (#94) West Road, Bassendean

Drawing Title:

Existing Site Survey

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Project No:

23019

Revision Number:

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Drawing No.:

03 of 10

Client

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Project Address

Lot 85 (#94) West Road, Bassendean

Drawing Title:

Existing Site Survey

Scale:

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Sheet Size:

A1

Project No:

23019

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19.00

Drawing No.:

03 of 10

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Drawing Title:

Existing Site Survey

Scale:

1:200

Sheet Size:

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Project No:

23019

Revision Number:

19.00

Drawing No.:

03 of 10

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Hawaiian Bassendean



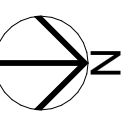
Commerical Development



Bassendean Primary School

19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	10.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	11.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision Description		19.01.24

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Context Plan

Scale: Sheet Size: A1

Project No: 23019 Revision Number: 19.00

Drawing No.: 05 of 10



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17.00	Changes	10.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	17.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
10.00	Changes	19.01.24

Revision	Description	19.01.24
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Client
Briscola PTY LTD

Project Name
Childcare Centre

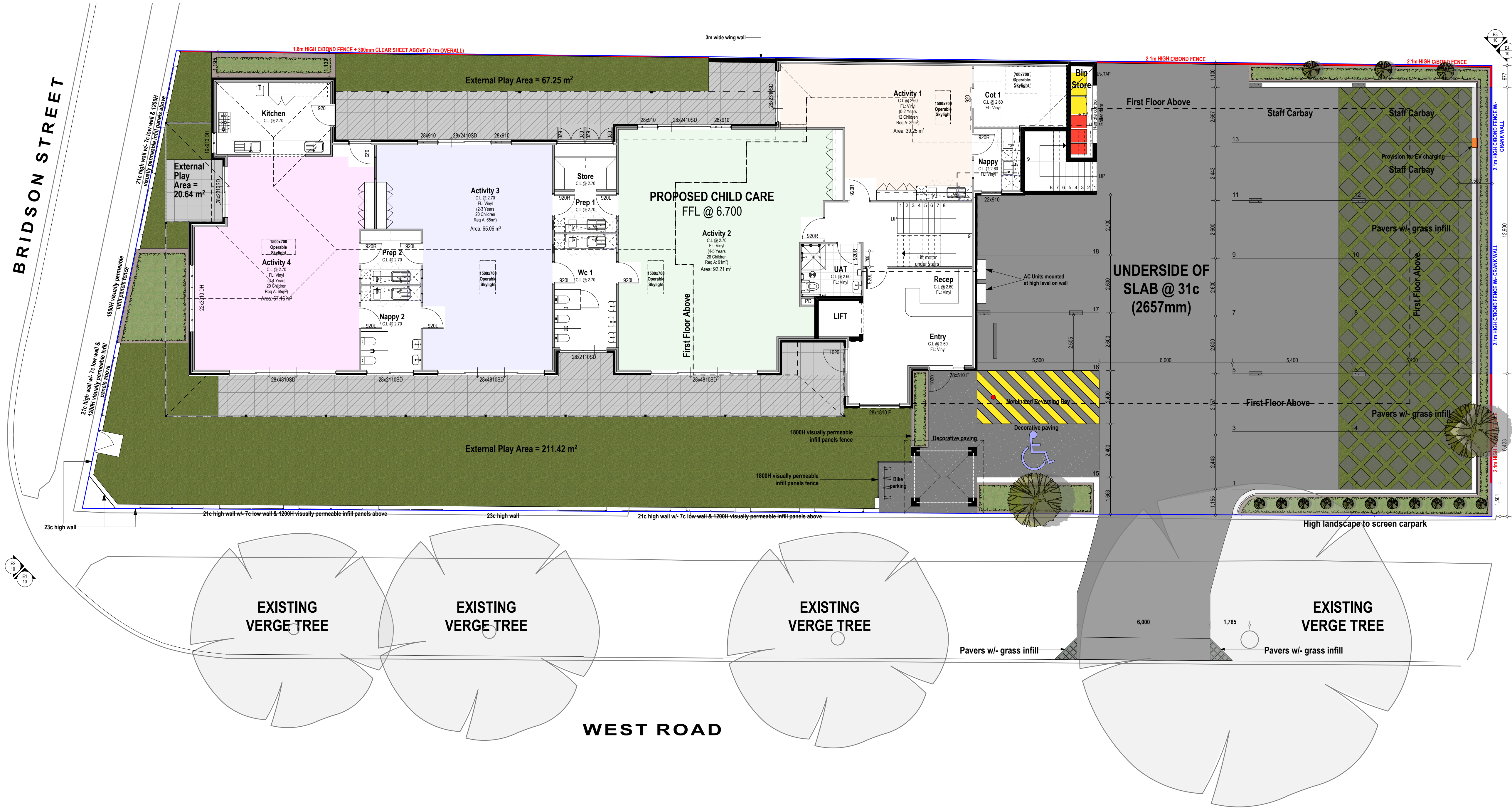
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Drawing Title:
Aerial Map

Scale:	Sheet Size:
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Project No:	Revision Number:
23019	19.00

Drawing No.:
06 of 10

Child / Room Calculations				
Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	39.25m ²	3
Activity 2	4-5	28	92.21m ²	3
Activity 3	2-3	20	65.06m ²	4
Activity 4	3-4	20	67.16m ²	2
Total Internal =		80	263.68m ²	14
(Min 3.25m ² per child)				(Min 260m ² req)
Total External Play Area =		80	560.56m ²	
(Min 7m ² per child)				(Min 560m ² req)
Parking Calculations				
(As per local planning policy)				
Description	Required			
1 bay per employee	14			
1 bay per 10 childs	8			
Required =	22			
Provided =	18			



Zone	Area	Perim
Store FF	4.22	8.850
Bin	5.62	11.755
First Floor	111.61	52.380
Childcare Centre	440.77	522.780
	562.22 m ²	195.774 m

Site Calculations		
Site Area:	1,258m ²	
Building Footprint:	457m ²	
Site Coverage:	36.32%	
Allowable Site Coverage:	50%	

Zoning:	Residential R20
Policy:	Residential
Heritage:	No
Bushfire:	No
BAL:	TBA
Acoustic:	No
Sewer:	Inside North
Power:	Overhead
Coastal:	No
Water:	Meter on Site
Wind Rating:	No

19.00	Changes	28.10.24
18.00	Changes	18.10.24
17.00	Changes	10.10.24
16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	11.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision	Description	19.01.24

Project North:	
Client	
Briscola PTY LTD	
Project Name	
Childcare Centre	
Project Address	
Lot 85 (#94) West Road, Bassendean	
Drawing Title:	
Ground Floor Plan	
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Project No:	Revision Number:
23019	19.00
Drawing No.:	
07 of 10	

Child / Room Calculations

Room	Age (Yrs)	Quant.	Size	Staff Req
Activity 1	0-2	12	39.25m ²	3
Activity 2	4-5	28	92.21m ²	3
Activity 3	2-3	20	65.06m ²	4
Activity 4	3-4	20	67.16m ²	2

Total Internal = 80 263.68m² 14
(Min 3.25m² per child) (Min 260m² req)

Total External Play Area = 80 560.56m²
(Min 7m² per child) (Min 560m² req)

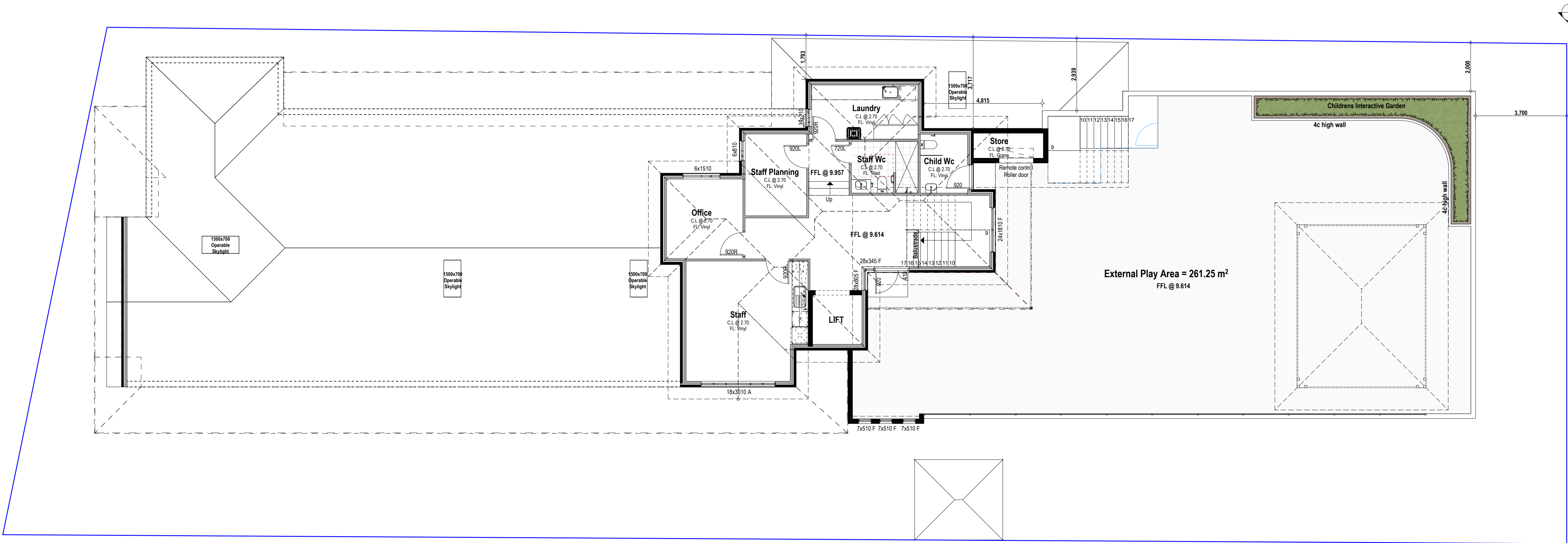
Parking Calculations

(As per local planning policy)

Description	Required
1 bay per employee	14
1 bay per 10 child	8

Required = 22

Provided = 18



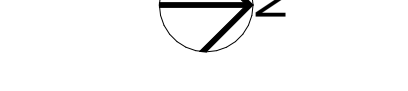
Zone	Area	Perim
Stone FF	4.22	8.80
Bin	5.62	11.76
First Floor	111.61	52.30
Childcare Centre	440.77	122.78

562.22 m ²	195,774 mm
Zoning:	Residential R20
Policies:	Residential
Heritage:	No
Bushfire:	No
BAL:	TBA
Acoustic:	No
Sewer:	Inside North
Power:	Overhead
Coastal:	No
Water:	Meter on Site
Wind Rating:	No

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16.00	Changes	04.09.24
15.00	Changes	30.08.24
14.00	Changes	31.07.24
13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24

Revision	Description	Date
19.01	24	

Project North:



Client
Briscola PTY LTD

Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
First Floor Plan

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Project No:
23019

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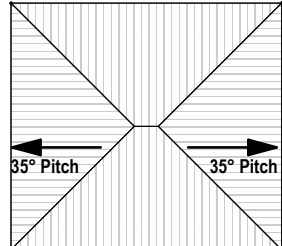
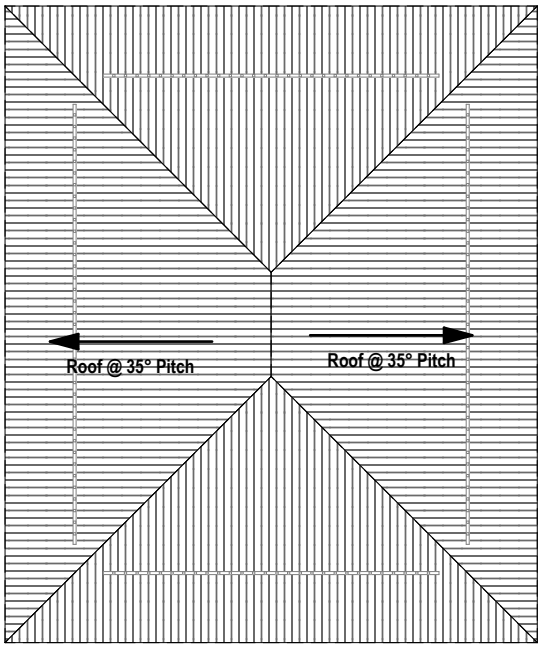
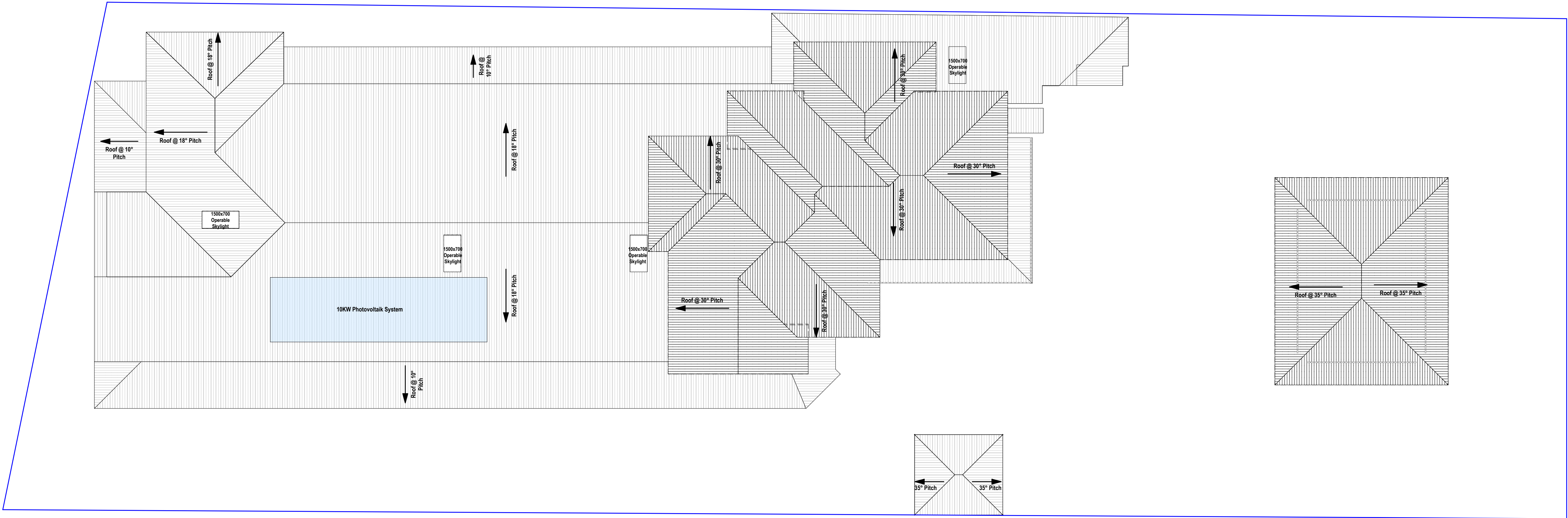
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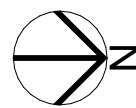
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13.00	Changes	26.05.24
12.00	Changes	09.05.24
11.00	Changes	25.04.24
Revision	Description	19.01.24

Project North:



Client
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Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:

Roof Plan

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Project No:

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Revision Number:

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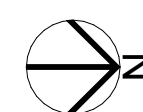
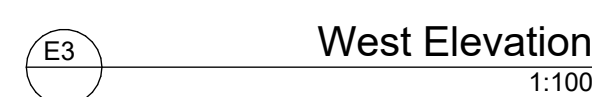
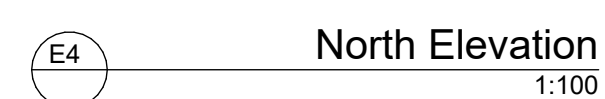
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Project Name
Childcare Centre

Project Address
Lot 85 (#94) West Road, Bassendean

Drawing Title:
Elevations

Scale: 1:100	Sheet Size: A1
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SUBJECT:

PRELIMINARY ARBORICULTURAL REPORT (PAR) UPON 1No OF
OAK TREE LOCATED WITHIN ROADSIDE VERGE AT 94 WEST
ROAD BASSENDEAN

PREPARED FOR:

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PREPARED BY:

ArborCulture Australia Pty Ltd
ABN: 55650934286
Arborist/ assessor: Rick Thomas
ISA Tree Risk Assessment Qualified #SG-0176A
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DATED AT:

24th June 2024

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1.0 INTRODUCTION

The subject site located at Lot 85 (No 94) West Road Bassendean is proposed for land development and the build of a Childcare Centre.

This Preliminary Arborists Report (PAR) is the result of a tree & site assessment of a visual nature performed on the 17th June 2024 upon 1No's of Quercus robur (English Oak) tree located within the roadside verge at 94 West Road Bassendean and, the study of proposed development plans issued to the arborist-assessor for the assessment and reporting of construction impacts and the necessary impact mitigations and tree protection measures as recommendations to apply.

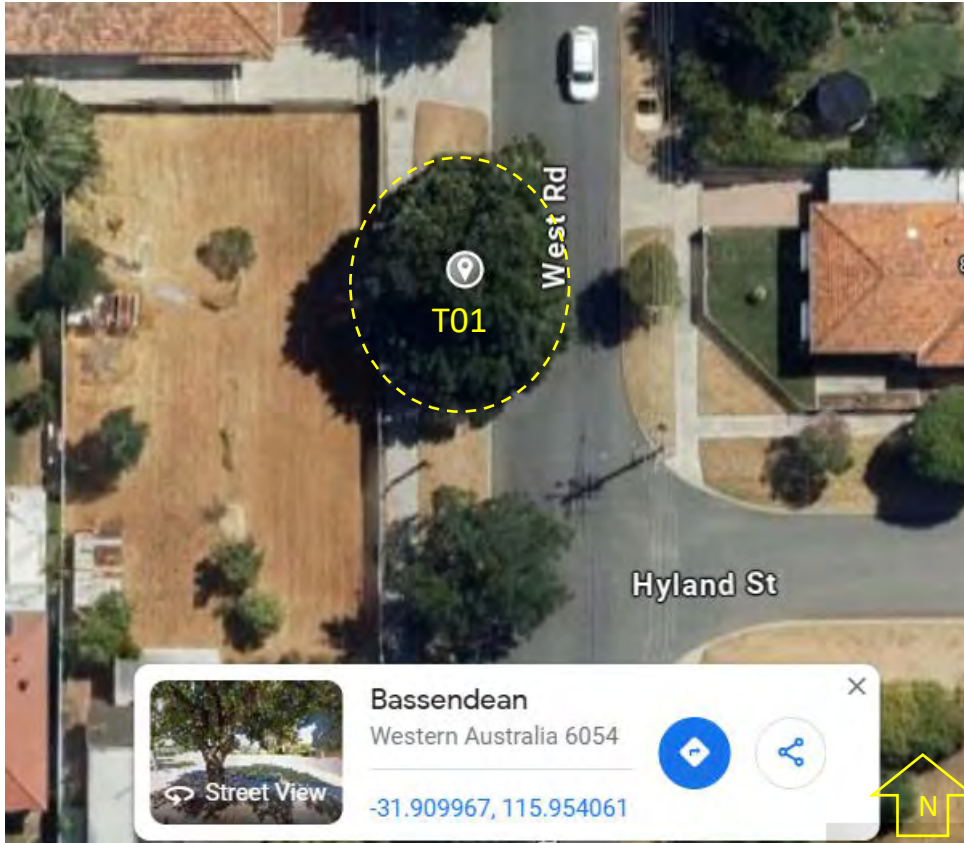
In the absence of a tree identification number attached to the tree (tag-ID) or annotated on plan drawings and for the purpose of this report, the arborist-assessor has allocated the ID# as T01 to the subject Oak tree.

The Oak tree (T01) together with approximately 31No other Oak trees along West Road, are listed on the City of Bassendean's Significant Tree Register

2.0 LIMITATIONS

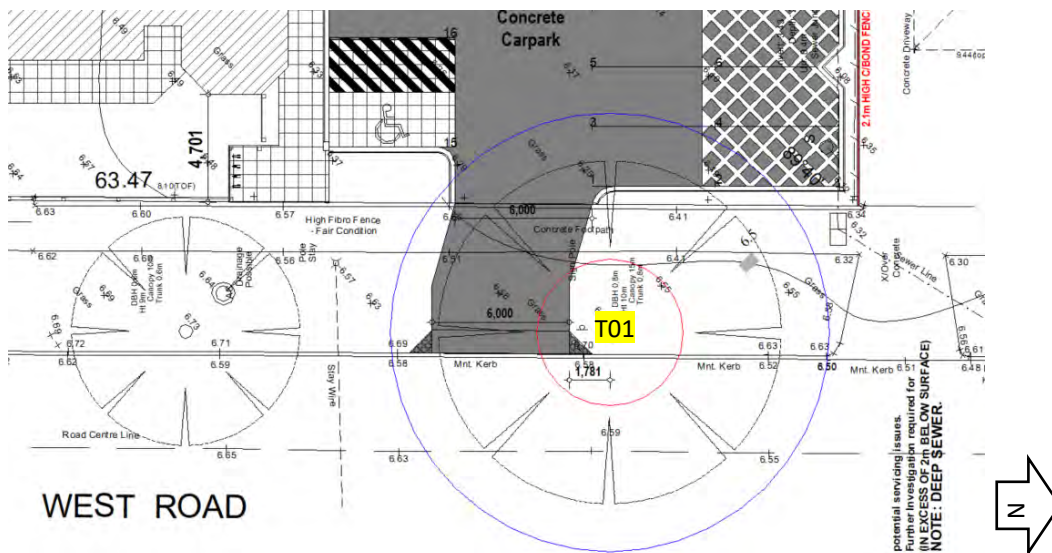
At the time of writing this report, the arborist-assessor was not aware of any sub-level utilities being proposed for demolition, diversion, or installation within the TPZ of T01.

3.0 SITE PLAN (PART PLANS)



Aerial – Existing Plan View





T01 = the subject Oak tree, located on the northern side of the public verge of Lot 85, (No.94) West Road, Bassendean



Part Plan - Proposed Layout View

- Tree Protection Zone (TPZ) = 9.6m Radius from trunk centre
- Structural Root Zone (SRZ) = 3.2m Radius from trunk centre

4.0 – VISUAL TREE ASSESSMENT (VTA)

T01 - Baseline biodata (summary)		
 <p>Img-4481 (Whole tree - facing west direction)</p>  <p>Img-4480 (Whole tree - facing north direction)</p>	TREE ID#	T01
	SPECIES (Common name)	Quercus robur (English Oak)
	HEIGHT Est. (m)	10.0
	TRUNK DBH Est. (m) Measured at 1.4m from EL	0.8
	TREE PROTECTION ZONE (TPZ) Radius from trunk centre at base (m)	9.6
	TRUNK DRC Est. (m) Measured above root collar/ trunk flare	0.9
	STRUCTURAL ROOT ZONE (SRZ) Radius from trunk outer at base (m)	3.2
	CROWN WIDTH Est. (m) Along widest axis north/ south	15.0
	TREE HEALTH CONDITION	Good
	CROWN STRUCTURAL/ ARCHITECTURAL CONDITION	Good
	CROWN FORM	Fair/ good
	VERTICAL DISTANCE TO FIRST BRANCH North side, over proposed crossover (m)	3.0
 <p>Img-4476 (Trunk collar, facing west direction)</p>	T01 – Observations (summary) Tree is growing within the public green-verge. The trunk centre is located 0.9m from existing road kerb on the east side and 3.5m from edge of 2.0m wide footpath on the west side Soil conditions appeared as a brown sandy loam as organic layer, conditions were moist and clean. There were no visible structural surface roots present. Tree rises off a single main stem to the first main unions at approx. 2.5m above existing ground level. Main unions appeared sound, no visible defects observed. Tree crown is stout, compact and slightly asymmetrical, broader across the north-south axis. Leaf size, density and colour appeared seasonally normal, there was no evidence of pests or diseases observed. Amounts of deadwood >8cmØ observed as present in the crown, especially on the East and North sides of tree	
 <p>Img-4394 (Trunk collar, facing south-west direction)</p>		

5.0 – CONSTRUCTION IMPACT ASSESSMENT



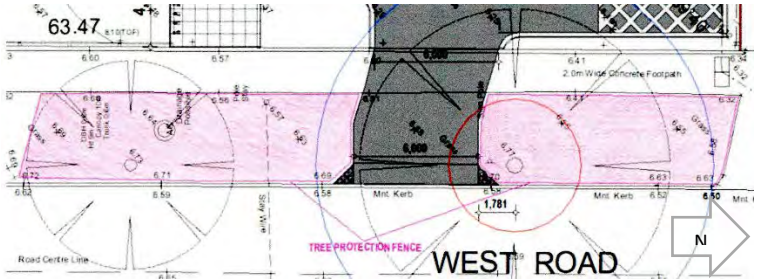

5.1 IDENTIFIED CONSTRUCTION IMPACTS & MITIGATION METHODS

SN #	Identified Construction Impacts	Impact Mitigation(s) – Method summary	Pictorial description (in support, if and where necessary)
5.11	Disturbances to the green- verge ground areas within the TPZ	<ul style="list-style-type: none"> Contractor to install a Tree Protection Fence (TPF), prior to mobilizing any heavy machinery, equipment, or materials to site (Refer Tree Protection Plan under item 6.0 below) 	Refer Tree Protection Plan under item 6.0 below
5.12	Establishment and utilization of site access into the site, during constructions (Note: T01 crown vertical height clearance over proposed crossover/ existing footpath is est. 3.0m)	<ul style="list-style-type: none"> Building contractors to establish & utilize access for heavy vehicles (eg: concrete trucks/ track mounted and other heavy machines) from Bridson Street. There are no trees on the verge fronting the property on Bridson Street. Alternatively, existing crossover south of T01 and outside the TPZ of T01 on West Road may be used, subject to the relevant Authorities approval. 	

SN #	Identified Construction Impacts	Impact Mitigation(s) – Method summary	Pictorial description (in support, if and where necessary)
5.13	Establishment and site utilizations generally, during constructions	<ul style="list-style-type: none"> Building contractor is advised to coordinate the works to commence and complete from the carpark and crossover to the North of site and, progress and work their way out southwards to the child-care buildings and out from the proposed established site access on Bridson Street. The builders site office (if applicable) may be situated upon the completed carpark surface. 	
5.14	Any proposed sub-level utilities demolitions, diversions, or installations	<ul style="list-style-type: none"> At the time of assessment, the arborist-assessor was not aware of any sub-level utility proposals within TPZ of T01, at this DA stage. (TBC) 	
5.15	Proposed new 6.0m wide crossover on south side of tree, edge is 1.78m from trunk centre and approx. 1.38m from trunk outer. Closest edge of crossover truncated splay-corner is 1.2m from trunk centre and approx. 0.8m from trunk outer	<p>Refer img-5.15a</p> <ul style="list-style-type: none"> Contractor to engage a project arborist to provide standing supervision during the proposed crossover excavation works. Commence by trenching using hand-tools along the north edge / tree side of crossover alignment to the depth of the proposed construction layers, exposing any tree roots with care. (eg: approx. 200mm comprising of 100mm compacted base course + 100mm reinforced concrete, according to construction details - City of Bassendean Asset Services 'Specification for the construction of crossovers') Progress the excavations to the specified depth within the main body of proposed crossover using light mini-excavator machine and a spotter person applying manual method; project arborist severing tree roots cleanly if and as they occur and; pictorially recording/ documenting the process. 	 <p>Img-5.15a: Proposed crossover</p>

SN #	Identified Construction Impacts	Impact Mitigation(s) – Method summary	Pictorial description (in support, if and where necessary)
	Proposed demolition of the existing non-mountable kerb and proposed construction of mountable kerb and construction of crossover truncated/ splayed corner, at the proposed crossover abutting roadside.	<p>Refer img-5.15b</p> <ul style="list-style-type: none"> ○ Retain kerb 'A' ○ Demo kerb 'B' breaking and removing using hand-held breaker machine with care. ○ Construct crossover and truncated/ splayed corner according to construction details - City of Bassendean Asset Services '<i>Specification for the construction of crossovers</i>' 	 <p>Img-5.15b: demo existing kerb, construct truncated/ splay corner</p>  <p>Example only: truncated/ splayed corner at existing neighbouring property along West Street</p>
5.16	Proposed construction of concrete carpark and car bays, within the TPZ and site boundary	<p>Refer Img-5.16</p> <ul style="list-style-type: none"> ○ Contractor to engage a project arborist to provide standing supervision during the proposed trenching works. ○ Trench along the edge (site side/ west side) of the existing footpath using hand-tools to the depth of the proposed construction layers, exposing any tree roots with care. ○ Progress the excavations to the specified depth within the main body of proposed carpark/ car-bays within the TPZ using light mini-excavator machine and a spotter person; project arborist severing tree roots cleanly if and as they occur and; pictorially recording/ documenting the process. 	 <p>Img-5.16: trench to depth of proposed constructions along the edge of existing footpath, site side (red-dashed line)</p>

6.0 TREE PROTECTION PLAN

SN #	Item description	Pictorial description (in support, if and where necessary)
6.1	<p><u>Installing the Tree Protection Fence (TPF).</u></p> <p>Contractor to install the (TPF) prior to mobilizing any heavy equipment or machines to site or, the movements or storage of heavy materials to or on site respectively.</p>	 <p>Img-6.1: Install TPF along perimeter of conservation zone (shaded in magenta); the TPF to be installed via two separate enclosures, 1x south of proposed crossover and, 1x north of proposed crossover.</p>
6.2	<p><u>TPF materials & installation</u></p> <p>TPF should comprise a minimum of 2.0m vertical height and constructed of sturdy galvanised weld-mesh panels, coupled together, and supported by sleeved base-weights; to ensure the TPF stability. Cross-bracing with steel tubes to support the TPF structure may also be necessary.</p> <p>Man-access gate(s) should be installed to each TPF enclosure, to facilitate tree maintenance & inspection activities.</p>	 <p>Img-6.2: Example of TPF materials; subject to the relevant Authorities approval for erection on street verges</p>
6.3	<p><u>Mulching the conserved soil areas.</u></p> <p>Contractor to install recycled tree green-waste (wood-chip & mulched twigs and leaves) material to the total area(s) within the perimeter of TPF(s) to a thickness of 10cm (excluding 50cm lateral distance from tree trunk outer)</p> <p>Mulch material and its composition, to be sourced from the City of Bassendean approved supplier/ source and, replenish mulch periodically to maintain a minimum of 5cm thickness cover</p>	
6.4	<p><u>Maintenance watering & weeding</u></p> <p>a. Watering (over duration of construction period)</p> <p>Apply potable water evenly distributed over the total conserved soil areas within the perimeter(s) of TPF(s)</p> <p>Watering should be seasonally applied and in the absence of soaking rain,</p>	

SN #	Item description	Pictorial description (in support, if and where necessary)
	<p>commencing September through to May.</p> <p>During dry periods apply 2m³ at minimum of twice weekly, to each of the north and south TPF enclosures. Monitor conditions and, increase the water volumes and regularities as necessary, to ensure that conserved soils are always moist, to a depth of 60cm</p> <p>b. Weeding</p> <p>Remove weeds by hand, as they emerge.</p>	
6.5	<p><u>Tree crown management</u></p> <p>A quantity of deadwood material >8cmØ was observed as present within tree crown of T01.</p> <p>It is recommended the developer or contractor submit application to the City of Bassendean to have the deadwood removed since, deadwood is in the process of decaying and will inevitably, break out and fall from the tree</p>	
6.6	<p><u>TPZ & site cleanliness (general)</u></p> <p>The contractor shall not store any materials or equipment within the TPZ(s) enclosures nor, store or place any materials or equipment against any TPF structure.</p> <p>The contractor shall ensure that cement trucks and other vessels used to carry and or work with cement-phytotoxins, are not washed or slurries discharged within the TPZ(s) and/ or; the entire green-verge along West Street.</p>	

7.0 CONCLUSION

- a. Tree T01 can be successfully retained, with acknowledgment and the subsequent method considerations contained under item 5.1 above 'Identified Construction Impacts & Mitigation Methods' and; the timely and continual actioning of the 'Tree Protection Plan' under item 6.0 above.

8.0 RECOMMENDATIONS

- a. Consider and adopt the impact mitigating measures under item 5.1 above and, apply the tree protection measures under item 6.0 above.
- b. The land-developer or building contractor to engage a project arborist to advise and supervise, tree related works including any excavation(s) within the TPZ of T01, mulching conserved soil areas and installation of TPF and, the ongoing tree maintenance operations.

---End Report---

Yours sincerely;



Rick Thomas
ArborCulture Australia Pty Ltd

9.0 LITERATURE CITED & DISCLAIMER

LITERATURE CITED:

- o AS 4970-2009 "Protection of Trees on Development Sites"
- o AS 4373-2007 "Pruning of Amenity Trees"
- o The Body Language of Trees – A handbook for Failure Analysis [Mattheck & Breloer 2003; edited by D Lonsdale from translation by R Strouts]
- o The Face of Failure in Nature and Engineering – 1st edition [Mattheck 2004]
- o The Landscape Below Ground (G. Watson 1st ed 1994)
- o Illustrated Guide to Pruning; 3rd edition [Ed Gilman]
- o Urban Soils – Applications and Practices [Craul]
- o Files and records held by ArborCulture Australia Pty Ltd

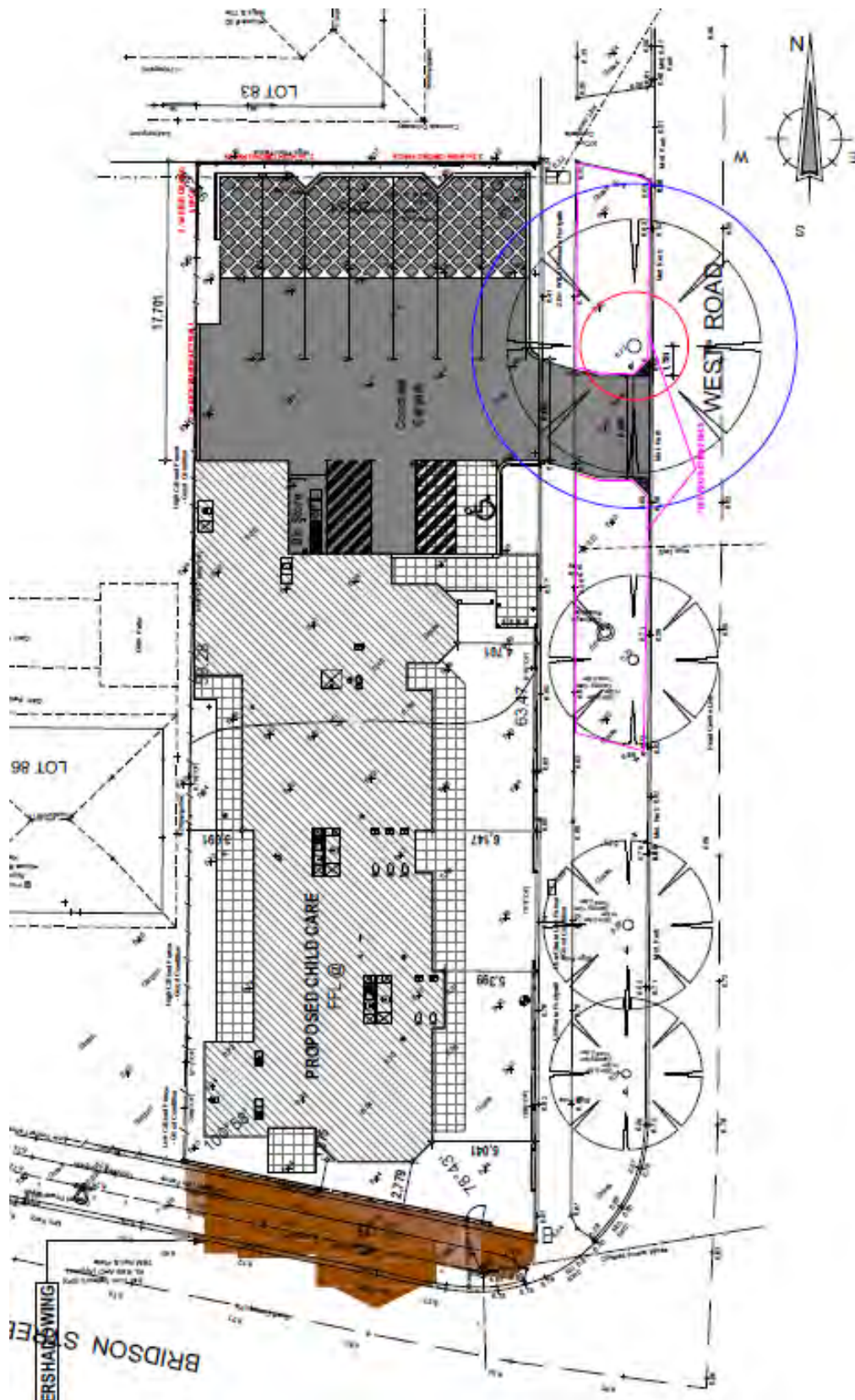
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10.0 SITE LAYOUT PLAN



11.0 GLOSSARY

GLOSSARY OF ARBORICULTURAL TERMS

Abscission. The shedding of a leaf or other short-lived part of a woody plant, involving the formation of a corky layer across its base; in some tree species twigs can be shed in this way

Abiotic. Pertaining to non-living agents; e.g. environmental factors

Absorptive roots. Non-woody, short-lived roots, generally having a diameter of less than one millimetre, the primary function of which is uptake of water and nutrients

Adaptive growth. In tree biomechanics, the process whereby the rate of wood formation in the cambial zone, as well as wood quality, responds to gravity and other forces acting on the cambium. This helps to maintain a uniform distribution of mechanical stress

Adaptive roots. The adaptive growth of existing roots; or the production of new roots in response to damage, decay or altered mechanical loading

Adventitious shoots. Shoots that develop other than from apical, axillary or dormant buds; see also 'epicormic'

Age Class. The age of the tree is represented as Juvenile, Semi-mature, Mature or Senescent.

Juvenile. A young tree, given normal environmental conditions for that tree it will not yet flower or fruit.

Semi-mature. Able to reproduce but not yet nearly the size of a mature specimen in that location.

Mature. Has reached or nearly reached full size and spread for that species in the given location.

Senescent. Has passed maturity, tree health in a state of decline.

Anchorage. The system whereby a tree is fixed within the soil, involving cohesion between roots and soil and the development of a branched system of roots which withstands wind and gravitational forces transmitted from the aerial parts of the tree

Architecture. In a tree, a term describing the pattern of branching of the crown or root system

Axil. The place where a bud is borne between a leaf and its parent shoot

Bacteria. Microscopic single-celled organisms, many species of which break down dead organic matter, and some of which cause diseases in other organisms

Bark. A term usually applied to all the tissues of a woody plant lying outside the vascular cambium, thus including the phloem, cortex and periderm; occasionally applied only to the periderm or the phellem

Basidiomycotina (Basidiomycetes). One of the major taxonomic groups of fungi; their spores are borne on microscopic peg-like structures (basidia), which in many types are in turn borne on or within conspicuous fruit bodies, such as brackets or toadstools. Most of the principal decay fungi in standing trees are basidiomycetes

Bolling. A term sometimes used to describe pollard heads

Bottle-butt. A broadening of the stem base and buttresses of a tree, in excess of normal and sometimes denoting a growth response to weakening in that region, especially due to decay involving selective delignification

Bracing. The use of rods or cables to restrain the movement between parts of a tree

Branch:

Primary. A first order branch arising from a stem

Lateral. A second order branch, subordinate to a primary branch or stem and bearing sub-lateral branches

Sub-lateral. A third order branch, subordinate to a lateral or primary branch, or stem and usually bearing only twigs

Branch bark ridge. The raised arc of bark tissues that forms within the acute angle between a branch and its parent stem

Branch collar. A visible swelling formed at the base of a branch whose diameter growth has been disproportionately slow compared to that of the parent stem; a term sometimes applied also to the pattern of growth of the cells of the parent stem around the branch base

Brown-rot. A type of wood decay in which cellulose is degraded, while lignin is only modified

Buckling. An irreversible deformation of a structure subjected to a bending load

Buttress zone. The region at the base of a tree where the major lateral roots join the stem, with buttress-like formations on the upper side of the junctions

Cambium. Layer of dividing cells producing xylem (woody) tissue internally and phloem (bark) tissue externally

Canker. A persistent lesion formed by the death of bark and cambium due to colonisation by fungi or bacteria

Canopy species. Tree species that mature to form a closed forest canopy

Cleaning out. The removal of dead, crossing, weak, and damaged branches, where this will not damage or spoil the overall appearance of the tree

Compartmentalization. The confinement of disease, decay or other dysfunction within an anatomically discrete region of plant tissue, due to passive and/or active defences operating at the boundaries of the affected region

Compression strength. The ability of a material or structure to resist failure when subjected to compressive loading; measurable in trees with special drilling devices

Compressive loading. Mechanical loading which exerts a positive pressure; the opposite to tensile loading

Condition. An indication of the physiological vitality of the tree. Where the term 'condition' is used in a report, it should not be taken as an indication of the stability of the tree

Crown/Canopy. The main foliage bearing section of the tree

Crown lifting. The removal of limbs and small branches to a specified height above ground level

Crown thinning. The removal of a proportion of secondary branch growth throughout the crown to produce an even density of foliage around a well-balanced branch structure

Crown reduction/shaping. A specified reduction in crown size whilst preserving, as far as possible, the natural tree shape

Crown reduction/thinning. Reduction of the canopy volume by thinning to remove dominant branches whilst preserving, as far as possible the natural tree shape

Deadwood. Branch or stem wood bearing no live tissues. Retention of deadwood provides valuable habitat for a wide range of species and seldom represents a threat to the health of the tree. Removal of deadwood can result in the ingress of decay to otherwise sound tissues and climbing operations to access deadwood can cause significant damage to a tree. Removal of deadwood is generally recommended only where it represents an unacceptable level of hazard

Decurrent. In trees, a system of branching in which the crown is borne on a number of major widely-spreading limbs of similar size (cf. excurrent). In fungi with toadstools as fruit bodies, the description of gills which run some distance down the stem, rather than terminating abruptly

Defect. In relation to tree hazards, any feature of a tree which detracts from the uniform distribution of mechanical stress, or which makes the tree mechanically unsuited to its environment

Delamination. The separation of wood layers along their length, visible as longitudinal splitting

Dieback. The death of parts of a woody plant, starting at shoot-tips or root-tips

Disease. A malfunction in or destruction of tissues within a living organism, usually excluding mechanical damage; in trees, usually caused by pathogenic micro-organisms

Distal. In the direction away from the main body of a tree or subject organism (cf. proximal)

Dominance. In trees, the tendency for a leading shoot to grow faster or more vigorously than the lateral shoots; also the tendency of a tree to maintain a taller crown than its neighbours

Dormant bud. An axial bud which does not develop into a shoot until after the formation of two or more annual wood increments; many such buds persist through the life of a tree and develop only if stimulated to do so

Dysfunction. In woody tissues, the loss of physiological function, especially water conduction, in sapwood

DBH (Diameter at Breast Height). Stem diameter measured at a height of 1.5 metres or the nearest measurable point. Where measurement at a height of 1.5 metres is not possible, another height may be specified

Endophytes. Micro-organisms which live inside plant tissues without causing overt disease, but in some cases capable of causing disease if the tissues become physiologically stressed, for example by lack of moisture

Epicormic shoot. (sprout) A shoot/ sprout having developed from a dormant or adventitious bud activated from beneath the bark

Excessive end-weight. Branch foliage biomass excessively loaded towards branch ends is a common cause of stem/branch union failure in trees; particularly when combined with environmental forces such as wind and rain loading

Excrescence. Any abnormal outgrowth on the surface of tree or other organism

Excurrent. In trees, a system of branching in which there is a well-defined central main stem, bearing branches which are limited in their length, diameter and secondary branching (cf. decurrent)

Flush-cut. A pruning cut which removes part of the branch bark ridge and or branch-collar

Form. relates to main trunk and main stem formation from a structural perspective & general stem architecture as well as what is deemed as being typical of a well-managed specimen. Crown structure and stem capacity to bear dead-weight loads are subjects of significance and often cannot be confirmed without aerial examination of main branch unions.

Aerial inspections are usually confined to large mature trees, and/or when the presence of epiphytes is suspected of masking hidden defects within the tree and/or in high-risk category areas [impact potential, target value etc]. For the purpose of this report ratings are given based on inspection from ground level by the arborist. The rating 'Good' being the highest or best 'Fair' refers to the presence of an abnormality whilst crown structure is usually still sustainable. In this instance an aerial inspection is recommended unless stated otherwise; 'Poor' refers to disorder(s) of stem/branch structure with potential to cause failure in the future and 'Hazardous' refers to notable disorder(s) with high potential for failure.

Girdling root. A root which circles and constricts the stem or roots possibly causing death of phloem and/or cambial tissue

Girth. is the tree main trunk circumference measured at 500mm above existing soil level

Guying. A form of artificial support with cables for trees with a temporarily inadequate anchorage

Habit (see form). The overall growth characteristics, shape of the tree and branch structure

Hazard beam. An upwardly curved part of a tree in which strong internal stresses may occur without being reduced by adaptive growth; prone to longitudinal splitting

Health. is an aspect requiring in-depth analysis to properly determine and encompasses tree vitality. However, for this report health condition is based upon visual analysis from ground level and what is deemed as being a general comparison to tree of same species growing under similar conditions. The ratings provided are 'Good' being good result and reflecting species typical in Australia and, 'Fair' refers to one or more minor health defects being noted yet generally sustainable and; 'Poor' being the lowest rating and used when disorder(s) have had or are having a significant adverse effect on tree health condition

Heartwood/false-heartwood/ripewood. Sapwood that has become dysfunctional as part of the natural aging processes

Heave. A term mainly applicable to a shrinkable clay soil which expands due to re-wetting after the felling of a tree which was previously extracting moisture from the deeper layers; also the lifting of pavements and other structures by root diameter expansion; also the lifting of one side of a wind-rocked root-plate

Height. - refers to the overall estimated height of total tree mass from existing ground level to the apex or peripheral extent of canopy

High canopy tree species. Tree species having potential to contribute to the closed canopy of a mature forest

Image numbers. are the image(s) recorded by the assessor(s) at the time inspection(s) were conducted for this report. Images that refer most descriptively and relative to the topic addressed are included if & where necessary.

Incipient failure. In wood tissues, a mechanical failure which results only in deformation or cracking, and not in the fall or detachment of the affected part

Included bark (ingrown bark). Bark of adjacent parts of a tree (usually forks, acutely joined branches or basal flutes) which is in face-to-face contact

Increment borer. A hollow auger, which can be used for the extraction of wood cores for counting or measuring wood increments or for inspecting the condition of the wood

Infection. The establishment of a parasitic micro-organism in the tissues of a tree or other organism

Internode. The part of a stem between two nodes; not to be confused with a length of stem which bear nodes but no branches

Lever arm. A mechanical term denoting the length of the lever represented by a structure that is free to move at one end, such as a tree or an individual branch

Lignin. The hard, cement-like constituent of wood cells; deposition of lignin within the matrix of cellulose microfibrils in the cell wall is termed Lignification

Lions tailing. A term applied to a branch of a tree that has few if any side-branches except at its end, and is thus liable to snap due to end-loading

Loading. A mechanical term describing the force acting on a structure from a particular source; e.g. the weight of the structure itself or wind pressure

Longitudinal. Along the length (of a stem, root or branch)

Lopping. A term often used to describe the removal of large branches from a tree, but also used to describe other forms of cutting

Microdrill. An electronic rotating steel probe, which when inserted into woody tissue provides a measure of tissue density

Minor deadwood. Deadwood of a diameter less than 25mm and or unlikely to cause significant harm or damage upon impact with a target beneath the tree

Mulch. Material laid down over the rooting area of a tree or other plant to help conserve moisture; a mulch may consist of organic matter or a sheet of plastic or other artificial material

Mycelium. The body of a fungus, consisting of branched filaments (hyphae)

Occluding tissues. A general term for the roll of wood, cambium and bark that forms around a wound on a woody plant (cf. woundwood)

Occlusion. The process whereby a wound is progressively closed by the formation of new wood and bark around it

Pathogen. A micro-organism which causes disease in another organism

Photosynthesis. The process whereby plants use light energy to split hydrogen from water molecules, and combine it with carbon dioxide to form the molecular building blocks for synthesizing carbohydrates and other biochemical products

Phytotoxic. Toxic to plants

PICUS. A device which uses sound-waves to measure decay in wood.

Pollarding. The removal of the tree canopy, back to the stem or primary branches. Pollarding may involve the removal of the entire canopy in one operation, or may be phased over several years. The period of safe retention of trees having been pollarded varies with species and individuals. It is usually necessary to re-pollard on a regular basis, annually in the case of some species

Scaffold or Primary branch. A major branch, generally having a basal diameter greater than 0.25 x stem diameter

Probability. A statistical measure of the likelihood that a particular event occurring

Proximal. In the direction towards from the main body of a tree or other living organism (cf. distal)

Pruning. The removal or cutting back of twigs or branches, sometimes applied to twigs or small branches only, but often used to describe most activities involving the cutting of trees or shrubs

Radial. In the plane or direction of the radius of a circular object such as a tree stem

Rams-horn. In connection with wounds on trees, a roll of occluding tissues which has a spiral structure as seen in cross-section

Rays. Strips of radially elongated parenchyma cells within wood and bark. The functions of rays include food storage, radial translocation and contributing to the strength of wood

Reactive Growth/Reaction Wood (also response wood). Production of woody tissue in response to altered mechanical loading; often in response to internal defect or decay and associated strength loss (cf. adaptive growth)

Removal of dead wood. Unless otherwise specified, this refers to the removal of all accessible dead, dying and diseased branch-wood and broken snags

Removal of major dead wood. The removal of, dead, dying and diseased branch-wood above a specified size

Respacing. Selective removal of trees from a group to provide space and resources for the development of retained trees.

Residual wall. The wall of non-decayed wood remaining following decay of internal stem, branch or root tissues

RET. See Root Exploratory Trench

Root-collar. The transitional area between the stem/s and roots

Root-collar examination. Excavation of surfacing and soils around the root-collar to assess the structural integrity of roots and/or stem

Root Exploratory Trench. A trench excavated using hand-tools only for the purpose of investigating root presence within soils proposed for disturbance. RET are typically excavated to a depth of 800mm – 1000mm and 400mm wide

Root injury. A tree with a root disease or decay [i.e. *Armillaria ostoyae* or *Heterobasidion annosus*] is at an increased risk of failing. Advanced infections may result in whole tree failure. Root loss through physical injury or severance, heightens whole tree failure risk considerably. Scaffold roots are primary in providing tree stability

Root protection Zone (RPZ). An area of ground surrounding a tree that contains sufficient rooting volume to ensure the tree's survival. (see TPZ)

Root zone. Area of soils containing absorptive roots of the tree/s described. The **Primary** root zone is that which we consider of primary importance to the physiological well-being of the tree

RPZ. See 'Root Protection Zone'

Sapwood. Living xylem tissues

Scaffold roots. are the larger diameter roots that are primary in providing stability to the tree. Most mature tree species commonly possess between 5 and 7 scaffold roots [although this is species and environmentally specific]. Scaffold roots are usually within the top 0.6m of soil surface and in addition to stability they also provide the functions of energy storage and water and mineral absorption [transportation] from the fine roots arising from them

Secondary branch. A branch, generally having a basal diameter of less than 0.25 x stem diameter

Selective delignification. A kind of wood decay (white-rot) in which lignin is degraded faster than cellulose

Shedding. In woody plants, the normal abscission, rotting off or sloughing of leaves, floral parts, twigs, fine roots and bark scales

Simultaneous white-rot. A kind of wood decay in which lignin and cellulose are degraded at about the same rate

Snag. In woody plants, a portion of a cut or broken stem, branch or root which extends beyond any growing-point or dormant bud; a snag usually tends to die back to the nearest growing point.

Soft-rot. A kind of wood decay in which a fungus degrades cellulose within the cell walls, without any general degradation of the wall as a whole

Species. is stated as botanical name [Genus and species] in each case

Spores. Propagules of fungi and many other life-forms; most spores are microscopic and dispersed in air or water

Shrub species. Woody perennial species forming the lowest level of woody plants in a forest or woodland and not normally considered to be trees

Sporophore. The spore bearing structure of fungi

Sprouts. Adventitious shoot growth erupting from beneath the bark

Stem/s. The main supporting structure/s, from ground level up to the first major division into branches

Stress. In plant physiology, a condition under which one or more physiological functions are not operating within their optimum range, for example due to lack of water, inadequate nutrition or extremes of temperature

Stress. In mechanics, the application of a force to an object

Stringy white-rot. The kind of wood decay produced by selective delignification

Structure. The structure rating takes into consideration above and below ground architecture to give an overall rating for tree structural integrity and potential for failure

Structural roots. Roots, generally having a diameter greater than ten millimetres, and contributing significantly to the structural support and stability of the tree

Structural Root Zone (SRZ). An area given in radius metres from trunk outer that is deemed necessary for protection and sustainable stability of a tree. (Calculated in accordance with AS 4970-2009 *Protection of Trees on Development Sites*)

Subsidence. In relation to soil or structures resting in or on soil, a sinking due breakdown of organics or settlement due to soil pore-space reduction or to shrinkage when certain types of clay soil dry out, sometimes due to extraction of moisture by tree roots

Subsidence. In relation to branches of trees, a term that can be used to describe a progressive downward bending due to increasing weight

Taper. In trunks and stems, the incremental degree of change in girth size along a given trunk, or stem length

Target canker. A kind of perennial canker, containing concentric rings of dead occluding tissues

Targets. In tree risk assessment (with slight misuse of normal meaning) persons or property or other things of value which might be harmed by mechanical failure of the tree or by objects falling from it

Topping. In arboriculture, the removal of the crown of a tree, or of a major proportion of it

Torsional stress. Mechanical stress applied by a twisting force

Translocation. In plant physiology, the movement of water and dissolved materials through the body of the plant

Transpiration. The evaporation of moisture from the surface of a plant, especially via the stomata of leaves; it exerts a suction which draws water up from the roots and through the intervening xylem cells

Tree Protection Zone (TPZ). An area given in radius metres from trunk outer that is deemed necessary for protection and sustainability of tree health condition. (Calculated in accordance with AS 4970-2009 *Protection of Trees on Development Sites*)

Understorey tree species. Tree species not having potential to attain a size at which they can contribute to the closed high canopy of a forest.

Vascular wilt. A type of plant disease in which water-conducting cells become dysfunctional

Vessels. Water-conducting cells in plants, usually wide and long for hydraulic efficiency; generally not present in coniferous trees

White-rot. A range of kinds of wood decay in which lignin, usually together with cellulose and other wood constituents, is degraded

Wind exposure. The degree to which a tree or other object is exposed to wind, both in terms of duration and velocity

Wind force. The force exerted by a wind on tree parts

Windthrow. The uprooting of a tree caused by wind force.

Wound dressing. A general term for sealants and other materials used to cover wounds in the hope of protecting them against desiccation and infection; only of proven value against fresh wound parasites, not typically recommended in urban/ amenity tree settings.



PART D – OTHER BUSINESS

- 1. State Administrative Tribunal Applications and Supreme Court Appeals**
- 2. Meeting Closure**