



PART D – CITY OF MANDURAH

1. Declarations of Due Consideration

2. Disclosure of Interests

3. Form 1 DAP Applications

- 3.1 Lot 502 (No.400) Pinjarra Road, Greenfields - Proposed Bulky Goods
Showroom – DAP/25/02919

4. Form 2 DAP Applications

Nil

5. Section 31 SAT Reconsiderations

Nil

Part D – Item 3.1 – Lot 502 (No. 400) Pinjarra Road GREENFIELDS – Proposed Bulky Goods Showroom

Form 1 – Responsible Authority Report (Regulation 12)

DAP Name:	Metro Outer
Local Government Area:	City of Mandurah
Applicant:	Hidding Urban Planning
Owner:	400 Pinjarra Road Pty Ltd
Value of Development:	\$14 million
Responsible Authority:	City of Mandurah
Authorising Officer:	Casey Mihovilovich Chief Executive Officer
LG Reference:	DA-11686
DAP File No:	DAP/25/02919
Application Received Date:	29 May 2025
Report Due Date:	4 September 2025
Application Statutory Process Timeframe:	60 Days
Attachment(s):	<ol style="list-style-type: none"> 1. Development Plans 2. Design Review Panel Report 3. SPP 7.0 Design Principles Report 4. Planning Report 5. Landscape Concept Plan 6. Traffic Impact Assessment

Responsible Authority Recommendation

That the Metro Outer DAP resolves to:

1. **Approve** DAP Application reference and accompanying plans (Plan No 9477, Revision SK025) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015* and the provisions of the City of Mandurah Local Planning Scheme No. 12, subject to the following conditions:

Conditions

1. This decision constitutes planning approval only and is valid for a period of 4 years from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect.
2. The development shall be carried out and fully implemented in accordance with the details indicated on the stamped approved plan(s) unless otherwise required or agreed in writing by the City of Mandurah, including the following modifications illustrated in red ink:

- a. The setback of the “back of house” portion of Building 1 shall be increased to a minimum of 6m from all boundaries.
 - b. The Motor Vehicle Repair tenancy (“Tenancy 9”) is to be replaced with Bulky Goods Showroom tenancy.
 - c. The proposed crossover via Lakes Road to the east shall be identified as left in / left out movement only.
 - d. Incorporate window openings to the facades addressing Pinjarra and Lakes Road and to the eastern facade in Building 1 to the satisfaction of the City of Mandurah.
3. All uncovered car parking bays to be in accordance with Australian Standard AS2890.1. Any bays adjacent to kerbs or for those bays that are to be used for disabled parking, shall be in accordance with Australian Standards AS1428.1

Prior to Construction

4. Prior to the commencement of site works, a detailed stormwater plan must be submitted to and approved by the City of Mandurah showing all stormwater from roofed and paved areas being collected and disposed of on-site in accordance with water sensitive design principles.
5. Prior to the commencement of site works, a Construction Management Plan shall be submitted to and approved by the City of Mandurah. The Plan must detail how the site will be managed during and after works are completed, in order to minimise issues associated with dust/sand, erosion, noise, vibration, traffic and general construction issues. The approved plan must thereafter be implemented to the satisfaction of the City of Mandurah.

Prior to Occupation

6. Prior to occupancy, the landowner shall enter into a deed of agreement with the City of Mandurah and adjoining Lots 105, 9003 and 9100 for the creation of a reciprocal car parking and access easement. The easement shall ensure reciprocal rights of access and will be placed on the title as an encumbrance which will facilitate the movement of vehicles and pedestrians over the subject lots to the satisfaction of the City of Mandurah. All costs associated with the preparation and registration of the encumbrance shall be borne by the applicant.
7. Prior to the commencement of site works, a detailed landscaping plan for the subject site and road verge(s) must be submitted to, and approved to the satisfaction of the City of Mandurah, and must include the following:
 - The location, number and type of proposed trees and shrubs;
 - Any existing vegetation and/or landscaped areas to be retained;
 - Pedestrian, paving and lighting treatments;
 - An implementation schedule; and,
 - Maintenance/management responsibilities.

The plan, once approved, shall be implemented and maintained to the satisfaction of the City of Mandurah.

8. Prior to the commencement of the use, vehicle parking, manoeuvring and circulation areas shall be suitably constructed, sealed, drained, kerbed, marked (including loading and disabled bays), and thereafter maintained to the specification and satisfaction of the City of Mandurah.
9. The HVAC and services shall be appropriately screened from public view to the specifications and satisfaction of the City of Mandurah.
10. Bicycle parking facilities shall be provided within the precinct to the satisfaction of the City of Mandurah.

Ongoing Conditions

11. All delivery and service vehicles must be located entirely on the site, and have their engines and refrigeration units turned off during loading and unloading of goods associated with the use of the site, and shall only access the site during the 'day period' of 7:00am to 7:00pm Monday to Saturday, and 9:00am to 7:00pm Sunday to the satisfaction of the City of Mandurah.
12. Rubbish collection vehicles, forklifts and similar equipment shall not enter the site or be operated on site outside of 7am to 7pm Monday to Saturday and 9am and 7pm Sundays and Public Holidays, unless otherwise approved by the City of Mandurah.
13. Security, building, signage and carpark lighting must be located, designed and installed to prevent excess light spillage from the development and must comply with AS4282 Control of the Obtrusive Effects of Outdoor Lighting and other relevant lighting standards.
14. Window signage, heavy tinting and imagery are limited to 25% of the total glass area of the elevation for the entire development unless otherwise approved by the City of Mandurah.
15. Painting, heavy tinting or otherwise obscuring of the shop front is not permitted to ensure interaction between the car parking area / street and shop floor is maintained unless otherwise approved by the City of Mandurah.

Advice Notes

1. Prior to any work commencing, an application for a building permit is to be submitted to and approved by the City of Mandurah.
2. In relation to Condition 6, the City of Mandurah's solicitors are required to check any deed prepared, at the applicant's cost. Alternatively, the City's solicitors (at the applicant's cost) may prepare the required deed by completing an authority form with the City. A survey quality sketch, showing the required access easement on the subject lot, is required and shall be prepared by a practicing land surveyor.

Details: outline of development application

Region Scheme	Peel Region Scheme
Region Scheme - Zone/Reserve	Urban
Local Planning Scheme	Local Planning Scheme 12
Local Planning Scheme - Zone/Reserve	Urban Development
Structure Plan/Precinct Plan	Lakes Rd and Pinjarra Rd Structure Plan
Structure Plan/Precinct Plan - Land Use Designation	Service Commercial
Use Class and permissibility:	Bulky Goods Showroom – P Motor Vehicle Repair – X
Lot Size:	2.2ha
Existing Land Use:	Vacant site
State Heritage Register	No
Local Heritage	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Heritage List <input type="checkbox"/> Heritage Area
Design Review	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other
Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

Development approval is sought for a large format retail development consisting of 11 x Bulky Good Showroom (“Showroom”) tenancies and 1 x Motor Vehicle Repair tenancy, which are intended to operate during normal day time business hours. The development has been designed with an internal presence facing the parking areas, rather than directly addressing public roads. The applicant has attempted to respond to Lakes Road and Pinjarra Road through the use of wall materials, varying height, and glazing treatments.

Proposed building heights range from 7.5m to 8.5m, extending up to 12m for signage elements.

Access to the site is primarily achieved via a new left in / left out crossover from Lakes Road at the eastern side of the property, together with a centralised accessway connecting with the adjacent property to the west. Access across the site will be formalised via easements and enforced via condition of approval, which has been discussed between surrounding landowners.

Service and bin storage areas adjacent to the northern boundary will ultimately be screened by future development on the lot to the north, however service areas located adjacent to the Pinjarra Road boundary are screened by the building, signage structure, and solid wall. Whilst these elements screen the service areas and restrict access, the result is a solid wall abutting Pinjarra Road.

Proposed Land Use	Bulky Goods Showroom Motor Vehicle Repair
Proposed Net Lettable Area	10,712m ²
Proposed No. Storeys	1
Proposed No. Dwellings	N/A

Background:

The subject site is located at the corner of Pinjarra Road and Lakes Road, and is 2.23ha in area which comprises of a single house and outbuildings. The site is bound by development sites to the north and west. Existing showroom development exists on the opposite side of Lakes Road to the east.



The subject site is zoned Urban under the Peel Region Scheme, the land fronts Pinjarra Road which is a Primary Regional Roads Reserve. No direct access is proposed from Pinjarra Road.

The subject site is zoned Urban Development under the City's Local Planning Scheme 12 (Scheme 12). Scheme 12 does not set out any land use permissibility for the Urban Development zone, Clause 3.3.6 states that if the zoning table does not identify any permissible uses for land in a zone, the local government may have due regard to any applicable Structure Plan and/or Local Development Plan.

The Mandurah East District Structure Plan was approved in 2009 to coordinate the development of the landholdings within Mandurah East and designates the site as Mixed Business / Employment Node. Under this framework, landowners within this precinct are required to prepare individual structure plans as this plan functions as a district structure plan.

The Lakes Road and Pinjarra Road Outline Development Plan ("Structure Plan") was approved in 2014 and applies to the subject site, adjacent Lots 9003, 9100 and 105, as well as the existing Showroom development to the eastern side of Lakes Road. The Structure Plan also has specific design requirements which seek development fronting Pinjarra Road.

Adjacent Lot 9003 has development approval in place for Showroom development which has been coordinated with the subject site.

The eastern part of this Structure Plan area (east of Lakes Road) has been normalised in terms of zoning through the gazettal of Scheme 12, zoning the land Service Commercial with Restricted Use provisions which effectively retain the original intent of the Structure Plan in terms of land use permissibility.

Given the pending expiry of this Structure Plan together with others in the vicinity, the City has progressed the preparation and acceptance for advertising of the Mandurah Health Precinct Structure Plan. The Mandurah Health Precinct Structure Plan, once in operation, will replace all current Structure Plans that exist for the landholdings within the Structure Plan Boundary.

Legislation and Policy:

Legislation

Planning and Development Act 2005

Planning and Development (Local Planning Schemes) Regulations 2015

Local Planning Scheme 12

State Government Policies

State Planning Policy 7 - Design of the Built Environment

Structure Plans/Activity Centre Plans

Lakes Road and Pinjarra Road Structure Plan

Local Policies

Local Planning Policy No 2 – Signage

Consultation:

Public Consultation

Nil

Referrals/consultation with Government/Service Agencies

Nil

Design Review Panel Advice

The proposal was presented to a Design Review Panel due to inconsistency with Scheme (and Structure Plan) building design and orientation requirements. The Panel noted the constraints of working with this use and typology in an urban context. However, the proponent was invited to consider the Panel's commentary under Principles 1-10 and make some modest amendments to improve the design outcome for the community, visitors, staff and customers. The Panel noted the following key considerations:

- The management of built form and scale at the intersection of Pinjarra and Lakes Road with an enhanced setback and a modified angle of geometry to the east facing elevation of Building 1.

Officer comment:

Building alignment, setbacks and parapet heights remain as designed. The City recommends that the “back of house” of Building 1 be decreased in area or relocated west in order to increase the setback to the truncation. This recommendation is reflected in the conditions and is discussed within this report.

- Consideration of window openings at the intersection of Pinjarra and Lakes Road and along the east elevation of Building 1.

Officer comment:

It is acknowledged that colourback glazing has been added to the Pinjarra Road and Lakes Road facades, introducing the perception of passive surveillance.

- Consider additional north south pedestrian access through the car park.

Officer Comment

It is noted that updated plans include two north south linkages through the site.

- Enhanced and amplified landscape design with a focus on a high-quality landscape buffer to Pinjarra and Lakes Road and more resolution of the landscape breakout space demonstrating its quality and amenity.

Officer comment:

It is acknowledged that the proponent has increased tree canopy size and further detail has been provided for the landscape breakout spaces to Lake Road has been provided.

- A consolidated, measurable and holistic Environmentally Sustainable Design (ESD) report with initiatives clearly reflected on the drawings.

Officer comment:

It was acknowledged that an ESD professional and report were produced but not submitted in time for the Panel review.

Other Advice

The City is satisfied that with an appropriate stormwater management plan for the site prior to site works stormwater can be effectively managed.

Access is proposed via left in / left out access point to Lakes Road, as well as coordinated access from surrounding development, which is consistent with the approved Structure Plan – on this basis a referral to Main Roads WA was not undertaken given direct access is not proposed and represents access which is contemplated by the approved Structure Plan.

Planning Assessment:

Development Standards

The subject site is zoned Urban Development as per Scheme 12.

Clause 3.3.6 of the Scheme states that if a zoning table does not identify any permissible uses for land in a zone the local government may, in considering an application for development approval, have due regard to an approved Structure Plan.

In this instance, the Lakes Road and Pinjarra Road Structure Plan identifies the site as Service Commercial noting that the predominant land use shall be Showroom assessed against the provisions of the Structure Plan and Town Planning Scheme 3, with the exception of Motor Vehicle Repair which is a use that is not permitted (X).

Therefore, the Bulky Goods Showroom has been assessed against the provisions of the Scheme (as per the below).

Local Planning Scheme 12 Development Standards and Requirements			
Provision	Requirement	Proposed	Assessment
Schedule 1 Additional requirements that apply to land in Scheme area	Buildings shall be required to front the primary street and shall be of high quality. Elevations are to include articulation through a variety of materials, height, setbacks and awnings covering pedestrian walkways around the building. Walls with no articulation and no variety of materials will not be accepted.	Buildings have been designed to face internally and essentially turn their back on the street. Notwithstanding this, the internal elevations include quality facades, using a range of colours, materials and feature elements.	<i>Discretion sought.</i>
	Wherever possible buildings are to address all public spaces, through the provision of windows and control of signage on windows to provide for	Lack of glazing facing external streets.	<i>Discretion sought.</i>

	<p>surveillance and security. The blanking out of windows with signage is not acceptable.</p> <p>Landscaping is to be of a high quality through the use of trees within parking areas, the street verge and around buildings, complimented by extensive low-rise native landscaping.</p> <p>A variety of paving is to be provided adjacent to the building and within the car parking area to demarcate pedestrian crossings and driveway access, ensuring that the extent of bitumen is reduced.</p> <p>Car parking areas are to be designed to wrap around the site and/or buildings, to form internal access ways. Car parking areas are to be articulated using high quality landscaping, tree plantings, variety of pavements, pedestrian refuge islands and access ways.</p>	<p>A comprehensive landscape scheme has been provided by a landscape architect, providing softening of the built form.</p> <p>Pedestrian connections have been proposed and these will be constructed using alternate materials and colours to demarcate these areas separate to driveway and access areas.</p> <p>Car parking areas are proposed centrally to the development but also wrap around the frontage to Lakes Road in the north-eastern part of the site.</p>	<p>Complies</p> <p>Complies</p> <p>Complies</p> <p>Complies</p>
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	<p>The layout and location of access ways, parking, loading bay and service areas are to be designed to permit vehicles to enter streets in a forward gear.</p>	<p>The development forms a series of internal accessways. Connectivity to development sites to the north and west are also proposed. Car parking areas will be supplemented with landscaping zones and tree planting and key pedestrian crossings are also proposed.</p> <p>The design of the development, as well as consideration for linking in with adjacent development sites, enables cross access by all vehicles and ensuring all vehicles can enter surrounding streets in a forward gear.</p>	
	<p>Service and loading areas are to be screened from street view and residential land uses.</p>	<p>Service and loading areas adjacent to the northern boundary will ultimately be screened by future development. However, service areas abut Pinjarra Road which are screened by the building, fencing and signage resulting in a blank façade facing Pinjarra Road.</p>	<p>Discretion sought.</p>
	<p>The extent and amount of signage</p>	<p>Formalised signage zones</p>	<p>Complies</p>

	<p>shall be controlled and minimised and shall have regard to the following:</p> <p>(i) An area on the building's facade shall be designed to incorporate the appropriate signage for individual tenancies, in a manner that does not detract from the building's design;</p> <p>(ii) The painting of the building in a tenant's corporate colours is considered a form of signage and should not detract from the building's design; and,</p> <p>(iii) Use of pylon and free-standing signs is to be restricted and kept to a minimum having reference to the applicable Precinct Structure Plan, Local Development Plan or Local Planning Policy.</p>	<p>have been proposed on buildings to ensure that they are incorporated into the façade.</p> <p>Colours are proposed to align with the intended corporate colours of businesses, resulting in excessive use of corporate colours.</p> <p>Freestanding signage is kept to a minimum, with only two pylon signs proposed allowing for multiple business advertising (one proposed along Pinjarra Road and one along Lakes Road). However, the height is well in excess of the height permitted under Local Planning Policy (12m in lieu of 6m).</p>	<p><i>Discretion sought.</i></p> <p><i>Discretion sought.</i></p>
Service Commercial Zone - Setbacks	<p>Minimum setbacks</p> <p>Primary = 6m</p> <p>Secondary = 3m</p> <p>Side & Rear = Nil</p>	<p>Pinjarra Rd = 7.5m</p> <p>Lakes Rd = 21.8m (Building 2)</p> <p>Lakes Rd = 8m (Building 1)</p> <p>Setback to rear (north) = 3m</p> <p>Setback to neighbouring Lot 8 = 3m</p> <p>Setback to side (west) boundary = 4m</p>	<p><i>Discretion sought - reduced setback at intersection.</i></p>

Service Commercial Zone - Landscaping	Minimum of 10% of the site shall be set aside for landscaping. Minimum of 3m landscaping strip shall be provided with the front setbacks.	10.15% Varying width.	Complies <i>Discretion sought - reduced width at the truncation and northern end.</i>
Schedule 2 Parking Requirements	Bulky Goods Showroom: 1 bay per 50m ² NLA. 10,312m ² = 206 bays Motor Vehicle Repair: 1 bay per 50m ² NLA. 400m ² = 8 bays Total required = 214 bays.	Total provided = 215 bays	Complies

Land Use

The Pinjarra Road and Lakes Road Structure Plan identifies that Motor Vehicle Repair is not permitted, whilst the previous Town Planning Scheme No 3 also identified Motor Vehicle Repair as not permitted.

The subject site is zoned Urban Development and therefore the land use permissibility of the Service Commercial zone as per the current Local Planning Scheme 12 does not apply to this site.

Clause 3.3.6 of Scheme 12 specifies that due regard shall be given to an approved Structure Plan, which in this instance identifies Motor Vehicle Repair as a use that is not permitted.

The zoning of developed lots also within the Pinjarra Road and Lakes Road Structure Plan area was normalised through the gazettal of Scheme 12 in 2023 resulting in a zoning of Service Commercial. Notwithstanding this, the intent of the approved Structure Plan in terms of land use permissibility is enforced through Clause 3.5 Restricted Uses of the Scheme, which does not specify Motor Vehicle Repair as a use which can be contemplated on the land identified, despite them being zoned Service Commercial.

The subject site is undeveloped and therefore the Urban Development zoning was retained through the gazettal of Scheme 12, on the basis that the approved Structure Plan provided the relevant local planning framework (i.e. Motor Vehicle Repair is not permitted).

It is expected that once the subject site, and immediately surrounding lots are developed, the City would progress to normalise the zoning to Service Commercial including identification of the site as per Clause 3.5 Restricted Uses via a Scheme Amendment.

Built Form

The City notes the use of varying colours and materials, as well as design features, to enhance what is a difficult development typology. This was a view shared by the DRP.

The applicant introduced colorback glazing to back of house portions of those buildings facing Pinjarra Road to introduce the perception of surveillance, and in response to the comments made by the DRP.

Notwithstanding this, the City has concerns over the impact of the building essentially turning its back on the street and the major intersection, as well as the impact of building bulk on the corner truncation particularly when viewed at the pedestrian / streetscape level.

The built form, orientation and internal car parking layout arguably creates a pleasant experience for visitors to the site, within a central shopping space that is landscaped, provides good pedestrian connection, and benefits from shopfront glazing providing interaction and surveillance – however, this results in the building turning its back of the streets. The development is considered to be car orientated, and the likely number of visitors arriving to the site on foot is expected to be low given the nature of the use and locational context – the site is not within an activity centre and is surrounded by relatively low density resulting in decreased pedestrian activity.

To alleviate some of these concerns, it is recommended that the back of house portion of Building 1 be amended to achieve the required 6m setback from the boundary to lessen this impact and to allow for a greater extent of landscaping.

Access and Traffic

Vehicular access to the site will be via a new 10m wide left in / left out crossover from Lakes Road to the east of the site. The location of the proposed crossover from Lakes Road is consistent with the approved Structure Plan. To ensure clarity, it is appropriate to condition the left in / left out crossover.

Access across the adjacent sites has been discussed with the adjacent landowners and developments have been integrated accordingly. The access will be formalised via condition of development approval in the form of easements to allow for reciprocal rights of access.

Given access will be available across the surrounding sites, visitors to the site will be able to access the site from multiple directions.

The City is progressing the Mandurah Health Precinct Structure Plan which has been approved by Council for the purpose of advertising. The traffic analysis undertaken as part of this process verified that Lakes Road will be able to accommodate the forecast growth in traffic, with safe and functional access provided to lots on both sides (i.e. restricting the subject site to left in / left out access onto Lakes Road) and a series of

interventions including a roundabout at the intersection of Lakes Road and Stratford Road. Given the Mandurah Health Precinct Structure Plan is in the earlier planning stages, the approved document is likely to require developer contributions for road network upgrades with the trigger being the development of land.

Landscaping

The most extensive landscape zones are with the Pinjarra Road and Lakes Road setback areas, which assist in softening and compliment the built form. It is noted that small landscape zones and trees are proposed throughout the centralised parking area.

The landscape scheme incorporates a range of species, feature planting mixes within street setback area to accentuate the building and development entry point, and dense planting to provide a buffer to the adjacent residential lot.

Signage

Consolidated signage areas are proposed inclusive of wall signage on external facades as well as pylon signs. There are two pylon signs proposed, with one on each road frontage.

Overall, signage is considered to vary the height and dimensional requirements of the City's Local Planning Policy 2: Signage which in the case of pylon / blade signage is considered to be significantly in excess of policy provisions. However, the type and size of signage is consistent and compatible with the scale and character of this style of development, and more specifically those considered more recently by the DAP within the Mandurah locality.

Conclusion:

It is recommended that the proposal be approved subject to modifications relating to the building interface with Pinjarra Road and Lakes Road, and the removal of Motor Vehicle Repair as a use as per the requirements of the approved Structure Plan.

The building does not address Pinjarra Road and Lakes Road and essentially turns its back on the street. Alternatively, the buildings have been orientated and designed to face internally, creating a central space for parking and pedestrians. The City notes that this approach is inconsistent with the design provisions of the Scheme and the approved Structure Plan, however through the use of building treatments and landscaping may be considered to be acceptable given the form of development proposed as well as the sites locational context.

The removal of Motor Vehicle Repair is considered to be appropriate, and consistent with the intentions of the relevant planning framework. The approved Structure Plan specifically identifies Motor Vehicle Repair as a use which is not permitted.





END
PROPERTY

LOCATION PLAN

SCALE: 1:500

meyer
shircore
architects

PROPOSED LARGE FORMAT RETAIL DEVELOPMENT

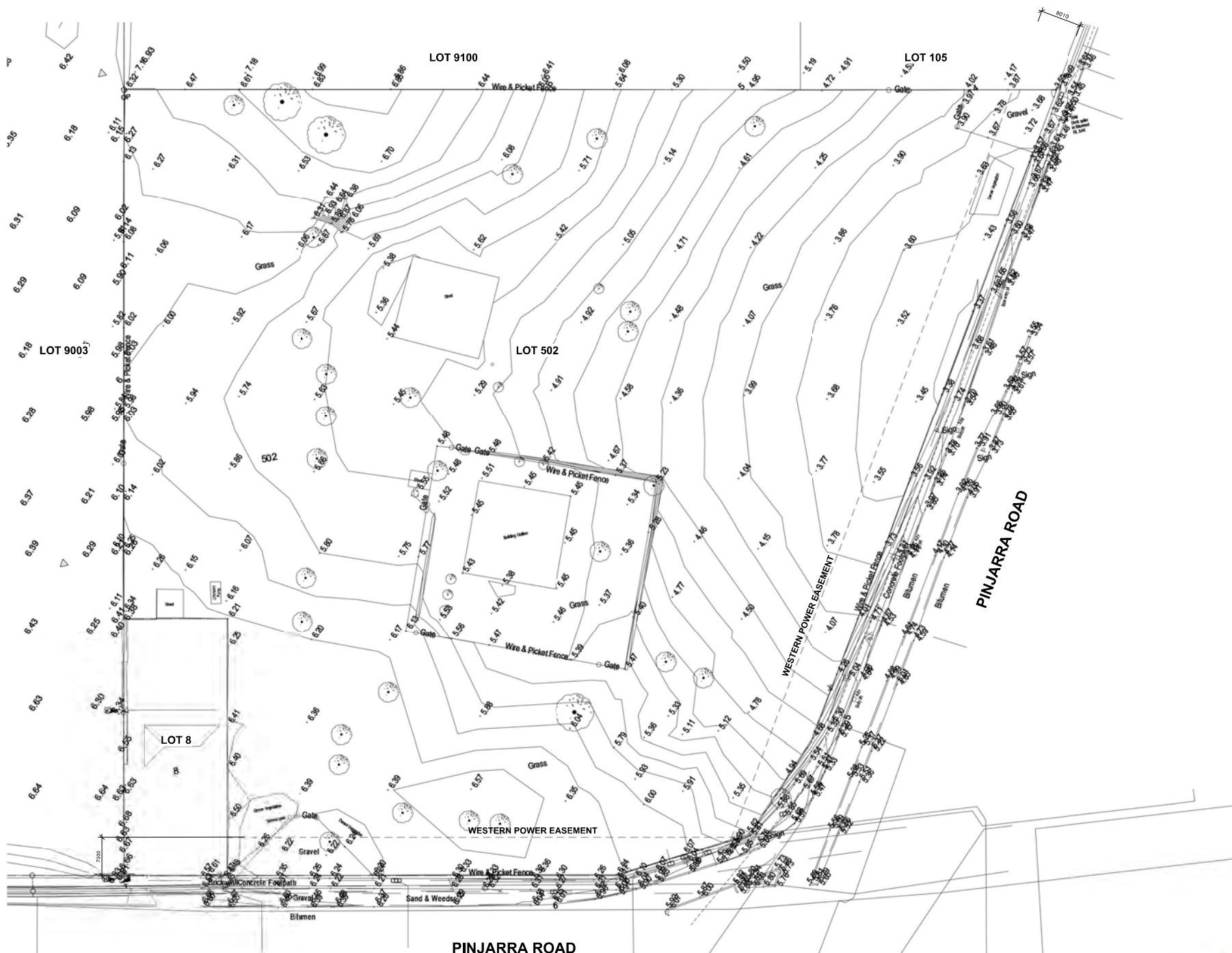
LOCATION : LOT 502, PINJARRA ROAD, GREENFIELDS, WA
FOR : PREMIER COMMERCIAL PTY LTD BY: VEND PROPERTY



DATE: AUG 2025 PROJECT NUMBER
REVISION: 5K025 9477
SHEET: 1
SCALE: 1:500 @B1

VEND
PROPERTY

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14/15, Greenfield Road, 427th Street, Lakeside WA 6008
Ph: 08 9451 0515
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SITE SURVEY

SCALE: 1:300

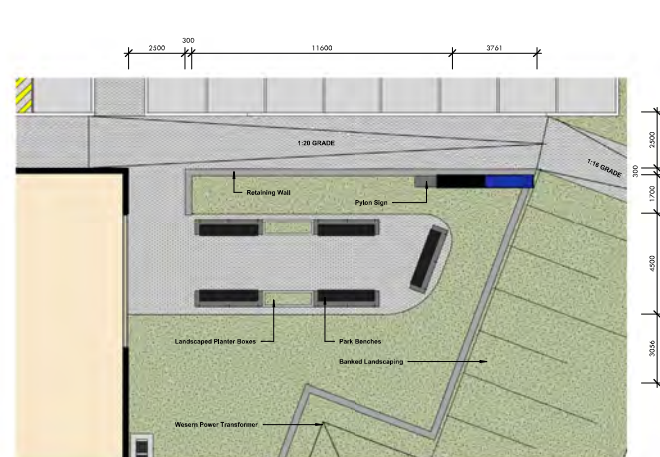
PROPOSED LARGE FORMAT RETAIL DEVELOPMENT

LOCATION : LOT 502, PINJARRA ROAD, GREENFIELDS, WA
FOR : PREMIER COMMERCIAL PTY LTD BY: VEND PROPERTY



	DATE: AUG 2025		PROJECT NUMBER
	REVISION: SK025		9477
	SHEET: 3		@ Mr. Naveed & Associates FCN 111 and 116 1-A-2, Green Park III Phase II Plot, Lahore WAPDA 100 Yards Suburb Lahore WAPDA © 2018/2019/2021
	SCALE: As indicated @B1		@ Mr. Naveed & Associates 100 Yards Suburb Lahore WAPDA © 2018/2019/2021





2 **POCKET PARK**
6 SCALE: 1 : 100

SCALE: 1:200

meyer
shircore
architects

LOCATION : LOT 502, PINJARRA ROAD, GREENFIELDS, WA
FOR : PREMIER COMMERCIAL PTY LTD BY: VEND PROPERTY



SOUTH ELEVATION (PINJARRA ROAD)

SCALE: 1 : 200



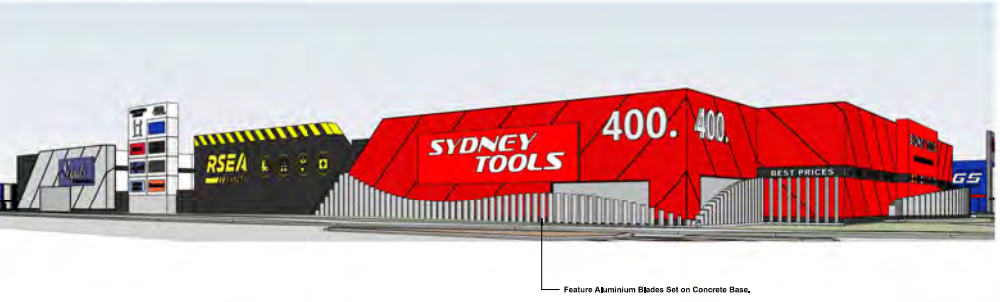
EAST ELEVATION (LAKES ROAD)

SCALE: 1 : 200



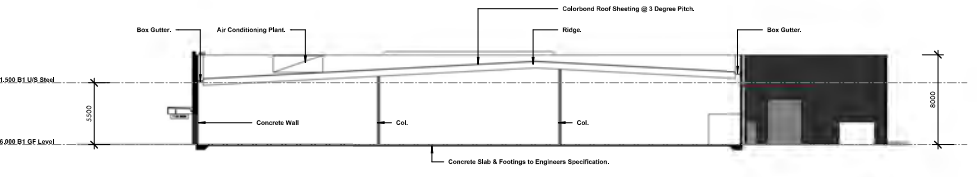
NORTH ELEVATION

SCALE: 1 : 200



WEST ELEVATION

SCALE: 1 : 200



SECTION A

SCALE: 1 : 200



BUILDING 1

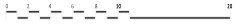


PROPOSED LARGE FORMAT RETAIL DEVELOPMENT

LOCATION : LOT 502, PINJARRA ROAD, GREENFIELDS, WA
FOR : PREMIER COMMERCIAL PTY LTD BY: VEND PROPERTY



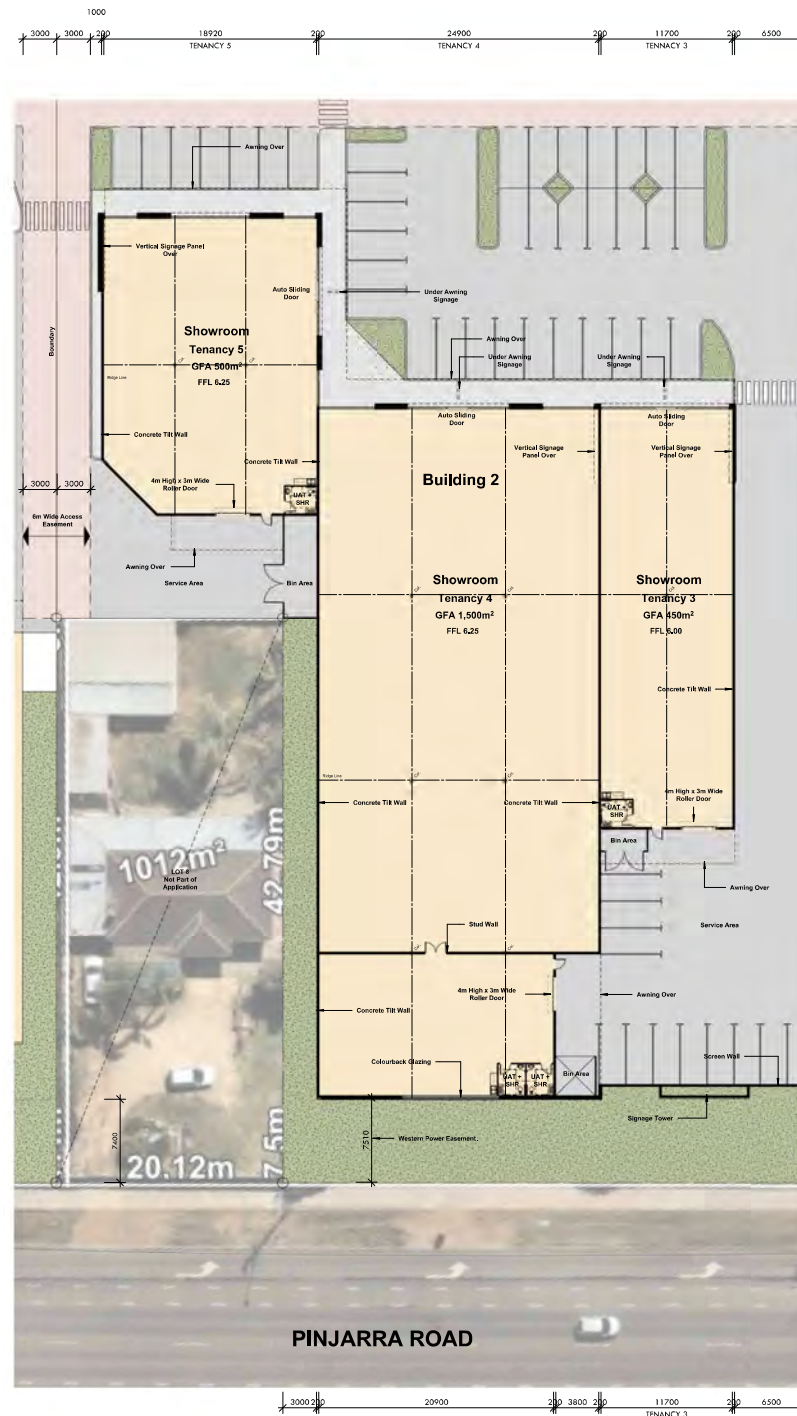
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REVISION:	SK025	9477
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SCALE:	1 : 200 @B1	





DATE: AUG 2025 PROJECT NUMBER
REVISION: SK025 **9477**
SHEET: 8
SCALE: 1 : 200 @B1

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PO Box 1204 Subiaco WA 6004
t: 08 9481 4613 e: info@meyerskidmore.com.au



BUILDING 2 GROUND FLOOR PLAN
SCALE: 1:200

BUILDING 2

meyer shircore architects

PROPOSED LARGE FORMAT RETAIL DEVELOPMENT

LOCATION : LOT 502, PINJARRA ROAD, GREENFIELDS, WA
FOR : PREMIER COMMERCIAL PTY LTD BY: VEND PROPERTY



1 5 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280 290 300 310 320 330 340 350 360 370 380 390 400 410 420 430 440 450 460 470 480 490 500 510 520 530 540 550 560 570 580 590 600 610 620 630 640 650 660 670 680 690 700 710 720 730 740 750 760 770 780 790 800 810 820 830 840 850 860 870 880 890 900 910 920 930 940 950 960 970 980 990 1000

DATE: AUG 2025 PROJECT NUMBER: 9477
REVISION: SK025
SHEET: 9
SCALE: 1:200 @B1

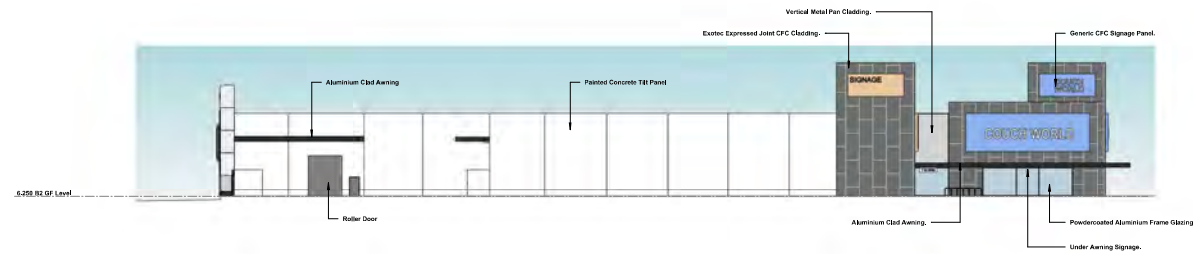
VEND PROPERTY

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G/L 1/2, Greenfield Road, 427 Pinjarra Road, Greenfields, WA 6008
Pty Ltd 100 Merges Way 2000
C/O 100 Merges Way 2000



B2 SOUTH ELEVATION (PINJARRA ROAD)

SCALE: 1:200



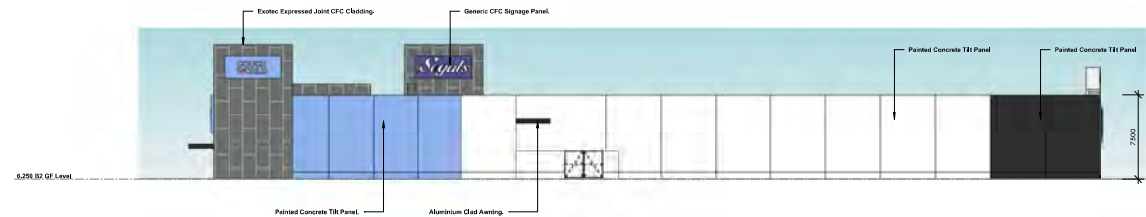
B2 EAST ELEVATION

SCALE: 1:200



B2 NORTH ELEVATION

SCALE: 1:200



B2 WEST ELEVATION

SCALE: 1:200



VIEW 1

SCALE



VIEW 2

SCALE



VIEW 3

SCALE

BUILDING 2

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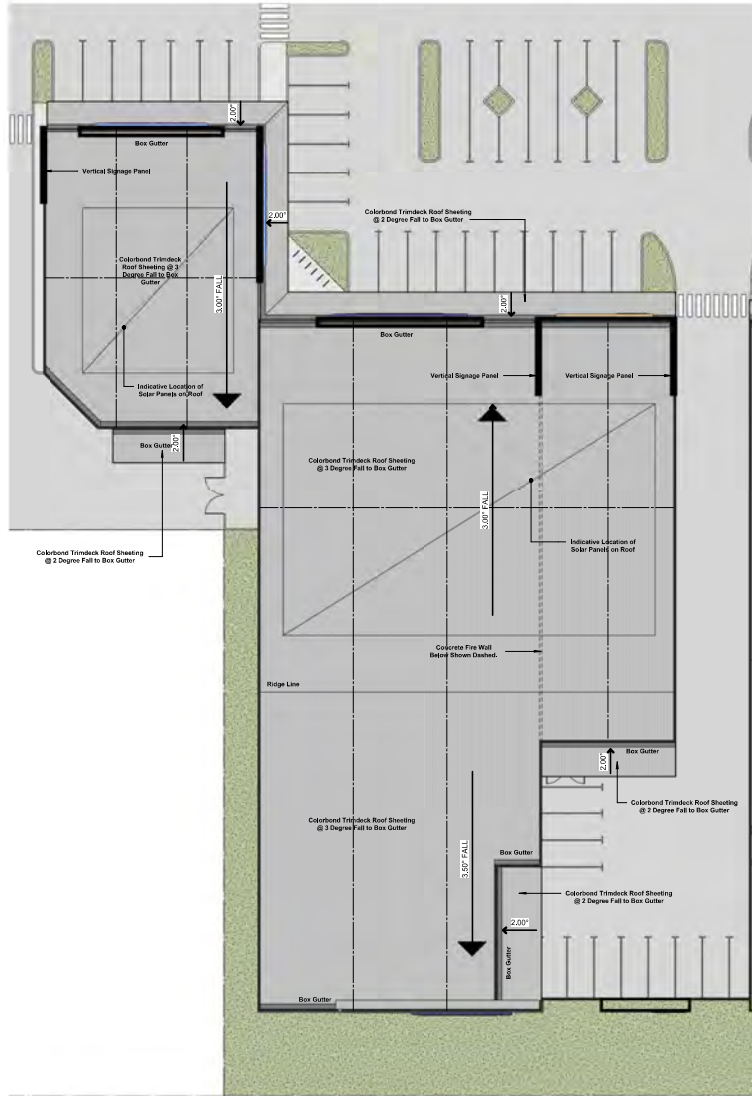
PROPOSED LARGE FORMAT RETAIL DEVELOPMENT

LOCATION : LOT 502, PINJARRA ROAD, GREENFIELDS, WA
FOR : PREMIER COMMERCIAL PTY LTD BY: VEND PROPERTY

VEND
PROPERTY

DATE: AUG 2025 PROJECT NUMBER
REVISION: 5K025 9477
SHEET: 10
SCALE: 1:200 @B1
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10/101 Greenfield Road 107th Floor, Greenfield, WA 6008
P.O. Box 100, Merges WA 6002
C/O 600 0011

1 2 3 4 5 10



BUILDING 2 ROOF PLAN

SCALE: 1:250

BUILDING 2



PROPOSED LARGE FORMAT RETAIL DEVELOPMENT

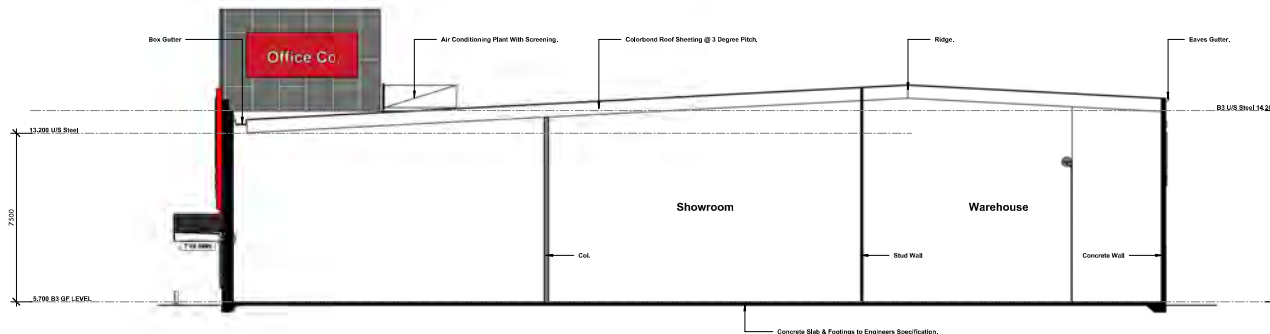
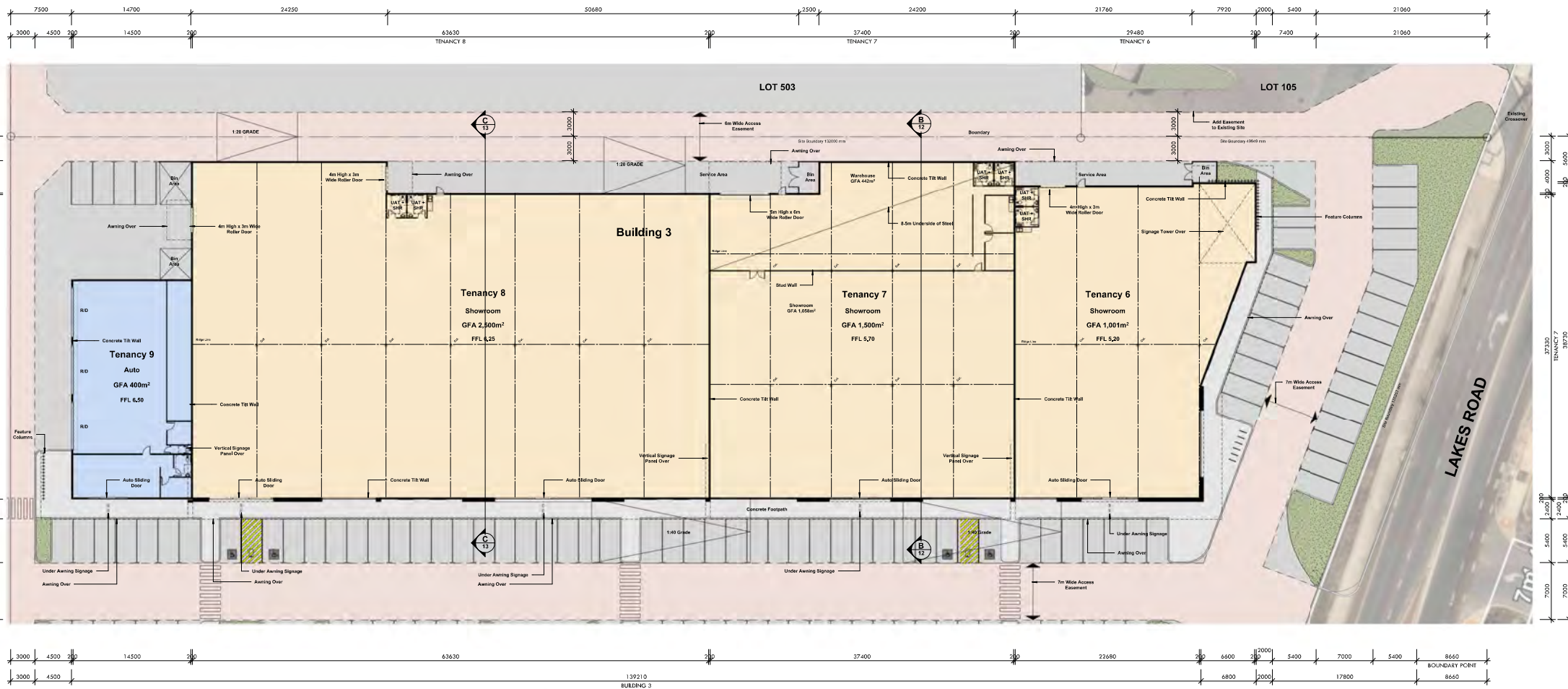
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DATE:	AUG 2025	PROJECT NUMBER
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SHEET:	11	
SCALE:	1:200 @B1	



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 1/100-1/101 Pinjarra Road, Greenfields, WA 6008
 PO Box 100, Melville WA 6009
 C/O 100-101
 C/O 100-101



SECTION B
SCALE: 1:100

BUILDING 3 GROUND FLOOR PLAN
SCALE: 1:200

BUILDING 3

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PROPOSED LARGE FORMAT RETAIL DEVELOPMENT

LOCATION : LOT 502, PINJARRA ROAD, GREENFIELDS, WA
FOR : PREMIER COMMERCIAL PTY LTD BY: VEND PROPERTY



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BUILDING 3

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PROPOSED LARGE FORMAT RETAIL DEVELOPMENT

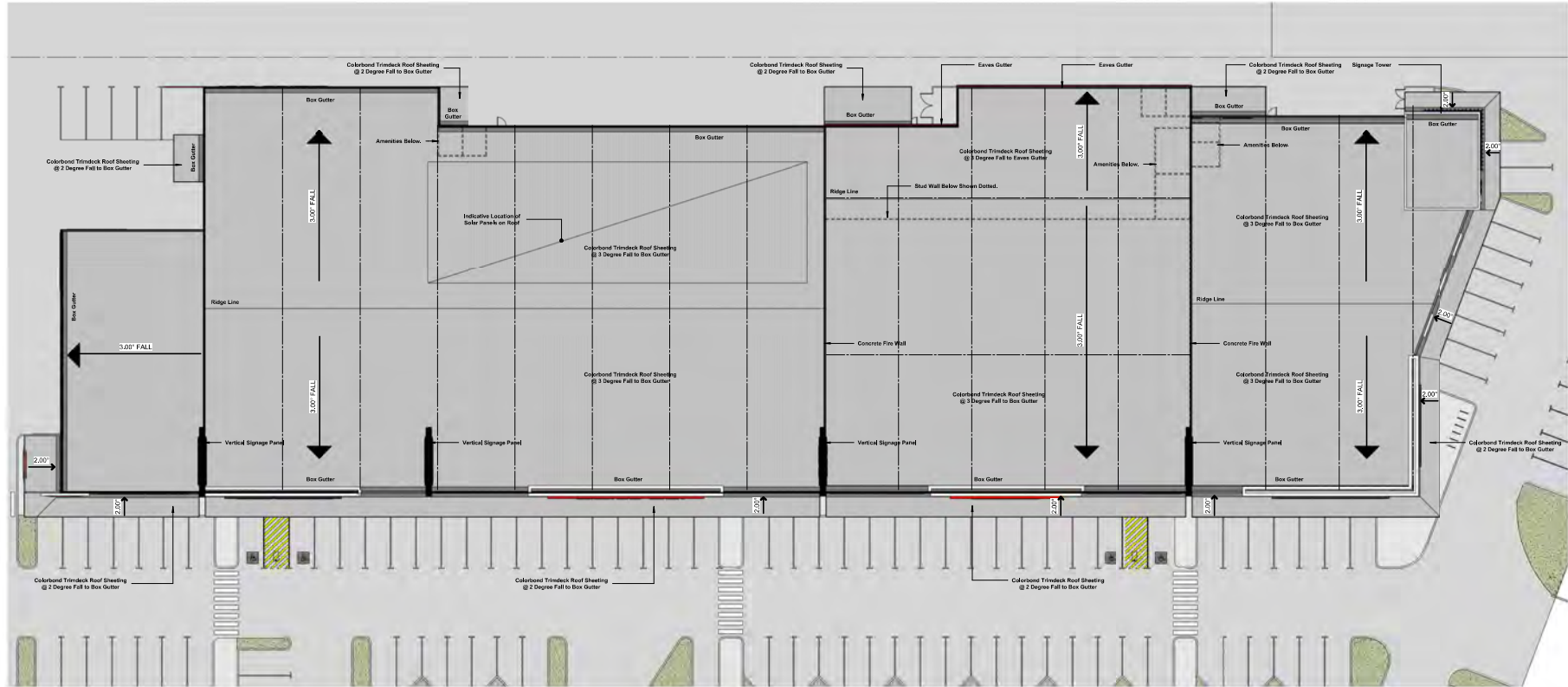
LOCATION : LOT 502, PINJARRA ROAD, GREENFIELDS, WA
FOR : PREMIER COMMERCIAL PTY LTD BY: VEND PROPERTY

VEND
PROPERTY

DATE: AUG 2025 PROJECT NUMBER
REVISION: 5K025 9477
SHEET: 14
SCALE: @81

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P.O. Box 100, Mergo WA 6006
E: info@mshircore.com.au



BUILDING 3 ROOF PLAN

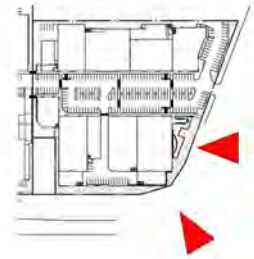
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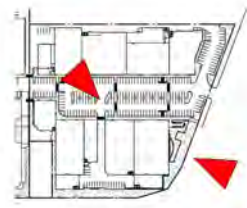
BUILDING 3

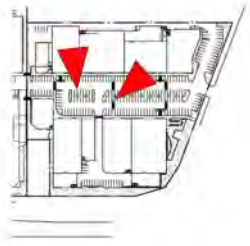
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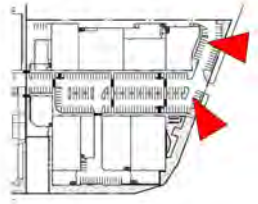
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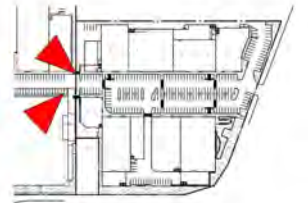


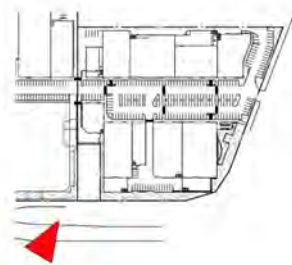








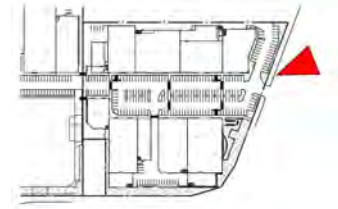






PROPOSED LARGE FORMAT RETAIL DEVELOPMENT

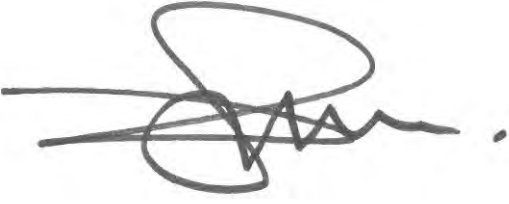
LOCATION : LOT 502, PINJARRA ROAD, GREENFIELDS, WA
FOR : PREMIER COMMERCIAL PTY LTD BY: VEND PROPERTY



Design Review Report

Lot 502 Pinjarra Road Greenfields-DR01 Report

01 July 2025

Design Review Report		
Subject	2025-07-01-Lot 502 Pinjarra Road Greenfields-DR01 Report	
Date	01.07.2025	
Time	11.30am	
Location	City of South Perth	
Design Reviewers	Name Simon Venturi Dominic Snellgrove Brett Woodgush	Panel Member Chair Panel Member
Proponent	Nik Hidding	Hidding Urban Planning
Project Team	Stephen Shircore Max Fabre	Meyer Shircore Meyer Shircore
Planning Authority	Tom Foulds	City of Mandurah
Stakeholders	N/A	
Declarations	None	
Briefings		
Relevant Authorities Project Team		
Design Review Report endorsement		
Reviewer's signature	 (Name) Dominic Snellgrove	

Introductory Comments	
Design quality evaluation	
	Supported
	Pending further attention
	Not yet supported
	Yet to be addressed
Strengths of the Proposal	<ul style="list-style-type: none"> • The appointment of a landscape professional and a preliminary landscape design and the confirmation of a minimum 10% site deep soil planting provision. • Predominantly native plant species selection • Landscaped breakout space provision • ESD professional appointed and a preliminary report produced for the meeting. (report not submitted prior) • Well-arranged presentation organised around the 10 design principles. • Very functional, efficient & competent commercial retail planning • Generous tree canopy proposed • 2.5M wide footpaths • Car parking largely screened from the public realm on Pinjarra and Lakes Road • Loading dock screened from Pinjarra Road • North south car park pedestrian designated walkways • Sustainability preliminary brief with stated principles and targeted initiatives including waterwise planting, low flow water fixtures and fittings, low VOC materials, low carbon construction, bike bays, passive shade, high performance glazing, daylight harvesting and all electric power. • Bulk and scale broken down with varied colours, parapet heights, built form mediation and blade structures • Legible, linked and direct vehicular access
Principle 1 Context and character	<i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i>
	<p>a) One of the most important aspects of responding to context and character in design reviews is delivering an appropriate streetscape. Different locations will demand varying levels of engagement: some streetscapes require significant activation, passive surveillance, a pedestrian-friendly experience, and strong interaction with the public domain; others call for a more passive and subdued relationship with pedestrians and the community.</p> <p>b) In addressing Principle 1, it is essential to analyse both the existing and the anticipated future streetscape architecture. By deriving cues from this analysis, designers can craft an appropriate and measured response that respects and enhances the unique context of each place.</p> <p>c) Whilst the car parking is successfully screened from the Pinjarra Road public domain the screening is achieved with blank and inactive facades raising concerns around streetscape activation and viability on the Pinjarra Road interface</p> <p>d) The intersection of Pinjarra Road and Lakes Road is particularly constrained without any generous landscape set back to match the main elevational conditions.</p> <p>e) Whilst Building 1 has a generous setback in relation to Lakes Road, the plan</p>

	alignment and elevational treatment of Building 1 does not respond to the geometry of Lakes Road.
Recommendations	<ol style="list-style-type: none"> 1. Consider creating additional setbacks at the corner intersection of Pinjarra and Lakes Road to allow for a more generous landscape buffer. 2. Consider incorporating window openings at the façade at the revised intersection of Pinjarra and Lakes Road where the back of house is located. This might create the impression of an active and engaged corner treatment. 3. Consider incorporating windows in the east façade of Building 1 to create the impression of passive surveillance over Lakes Road. 4. Consider varying the parapet heights at the corner intersection to mediate the built form and scale and signal the intersection more prominently. 5. Consider aligning/angling a portion, (not all) the building 1 east facing elevation to match the alignment of Lakes Road. This might include the top 30-40% of the plan footprint and incorporate the aluminum blades reoriented to align with Lakes Road. 6. In this way some level of streetscape interface and the perception of passive surveillance might be achieved
Principle 2 Landscape quality	<i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i>
	<ol style="list-style-type: none"> a) The Proponent is commended for engaging a landscape design professional at the early stages of concept development. b) The inclusion of shade tree canopy throughout the car park and a landscaped break out space is commended. c) However, there are some inconsistencies between the landscape plan and the architectural plans. d) The landscape drawings are preliminary in detail and would benefit from more consideration and detail.
Recommendations	<ol style="list-style-type: none"> 1. Ensure the landscape plans are reflected in the architectural plans 2. Ensure tree canopy size is maximised 3. Consider additional areas of landscape within the car park to mitigate urban heat island effect. 4. Continue to develop the landscape design and drawings to better illustrate the intent. Whilst the breakout space in the landscape area is strongly supported there is little by way of detail to describe it.
Principle 3 Built form and scale	<i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i>
	<ol style="list-style-type: none"> a) The built form and scale by way of height is considered appropriate and supported.
Recommendations	<ol style="list-style-type: none"> 1. See comments under Principle 1 context and character for the treatment of built form at the intersection.
Principle 4 Functionality and build quality	<i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.</i>
	<ol style="list-style-type: none"> a) The design represents a functional, efficient & competent commercial retail proposal. b) The inclusion of roof plans is appreciated c) However, the roof plans do not illustrate the location of plant equipment and are not accompanied by any sections illustrating plant location and screening. d) A materials palette illustrating durability and quality was not part of the submission.
Recommendations	<ol style="list-style-type: none"> 1. Provide roof plans and sections illustrating the location of HVAC

	<p>equipment and how it is fully screened from the public realm.</p> <p>2. Confirm materials to be used.</p>
Principle 5 Sustainability	<i>Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.</i>
	<p>a) The Proponent is commended for including an ESD brief with initiatives tabled across energy, water, materials, bike racks and passive design.</p> <p>b) Although not available prior to the review it is understood that an ESD professional has been appointed and a preliminary sustainable design report commissioned.</p>
Recommendations	<p>1. The Panel looks forward to seeing the ESD report and a comprehensive and holistic approach to sustainable design including measurable commitments including:</p> <ul style="list-style-type: none"> • Solar PV performance • EV charging • All electric power • Mitigating urban heat island effect • Low carbon construction • Rainwater harvesting <p>2. Illustrate the location of referenced bike racks on the drawings</p> <p>3. Consider lighting to avoid excessive light pollution with up lighting.</p>
Principle 6 Amenity	<i>Good design optimises internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.</i>
	<p>a) The landscape design proposal has the most capacity to deliver amenity for staff, service providers patrons and visitors.</p> <p>b) However, the landscape drawings are preliminary in detail and would benefit from more consideration and detail</p>
Recommendations	<p>1. Continue to develop the landscape design and drawings to better illustrate the intent. Whilst the breakout space in the landscape area is strongly supported there is little by way of detail to describe it. This area would benefit from a more generous approach to fixed seating and shade etc.</p> <p>2. Illustrate the location of bike racks for visitors and staff.</p> <p>3. Confirm the location of EOT facilities for staff.</p>
Principle 7 Legibility	<i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i>
	<p>a. The master plan approach with an inward looking centre results in a legible outcome for both pedestrians, vehicles and service vehicles.</p> <p>b. The north to south dedicated pedestrian links across the car park provide for safe and intuitive movement.</p> <p>c. The single vehicular cross over and through site vehicular link minimise vehicular impact on the public realm foot path and facilitate safe and convenient car movement.</p> <p>d. There are inconsistencies between landscape and architecture in the way the north to south pedestrian links is illustrated.</p>
Recommendations	<p>1. Consider an additional north to south pedestrian link through the car park.</p> <p>2. Consider maintaining pedestrian paving on the road way north to south pedestrian links and at the loading dock access vehicle points to signal pedestrian priority.</p>
Principle 8 Safety	<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>

	<ul style="list-style-type: none"> a. The proposal offers little or no passive surveillance over the public realm interface with Pinjarra and Lakes Road. b. Sightlines at the roadway between T9 & T8 and T3 & T2 are constrained and may result in unsafe pedestrian movements.
Recommendations	<ol style="list-style-type: none"> 1. Consider incorporating window openings at the façade at the revised intersection of Pinjarra and Lakes Road where the back of house is located. This might create the impression of an active and engaged corner treatment. 2. Consider incorporating windows in the east façade of Building 1 to create the impression of passive surveillance over Lakes Road and the proposed break out space. 3. Consider truncations in T9 & T8 and T3 & T2 to allow for safer sight lines.
Principle 9 Community	<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
	<ul style="list-style-type: none"> a) In a project of this scope and scale the most important outcome for the community is an active, engaged and passively surveilled ground plane and public domain. b) The proposal offers little or no streetscape interaction or passive surveillance over the public realm interface with Pinjarra and Lakes Road.
Recommendations	1. See comments under Principle 1 context and character in relation to the intersection of Pinjarra and Lakes Road and the east elevation of Building 1.
Principle 10 Aesthetics	<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
	<ul style="list-style-type: none"> a) The Panel appreciate the limited capacity for aesthetic considerations within the context of this project typology. b) The landscape design can be developed further to create an attractive landscape frame for the built form as well as a hard paving strategy. c) The intersection of Lakes and Pinjarra Road could be modified and improved as per comments under Principle 1 context and character. d) Further detail design resolution of the blade device as well as confirmation of the materials palette would be beneficial.
Recommendations	1. See above

Concluding Remarks

The Panel understands the constraints of working with this use and typology in an urban context. However, the Proponent is invited to consider commentary under Principles 1-10 above and make some modest amendments to improve the design outcome for the community, visitors, staff and customers. Particular attention should be paid to the following:

1. The management of built form and scale at the intersection of Pinjarra and Lakes Road with an enhanced set back and a modified angle of geometry to the east facing elevation of Building 1.
2. Consideration of window openings at the intersection of Pinjarra and Lakes Road and along the east elevation of building 1.
3. Enhanced and amplified landscape design with a focus on a high quality landscape buffer to Pinjarra and Lakes Road and more detail resolution of the landscape breakout space demonstrating its quality and amenity. (Consistency between the architecture and the landscape drawings should also be established.)
4. A consolidated, measurable and holistic ESD report with initiatives clearly reflected on the drawings where appropriate. (It is acknowledged that an ESD professional and report were produced but not submitted in time for the review)

Design Review progress.					
		Supported			
		Pending further attention			
		Not yet supported			
		Yet to be addressed			
		DR1	DR2	DR3	
Principle 1 - Context and character					
Principle 2 - Landscape quality					
Principle 3 - Built form and scale					
Principle 4 - Functionality and build quality					
Principle 5 - Sustainability					
Principle 6 - Amenity					
Principle 7 - Legibility					
Principle 8 - Safety					
Principle 9 - Community					
Principle 10 - Aesthetics					

10 PRINCIPLES DESIGN REPORT

Lot 502 Pinjarra Road
Greenfields WA

Prepared for
Vend Property
Design Review Panel

Project Number 9477
May 2025 Rev A

Contents & Register

ADDRESS	LOT 502 PINJARRA ROAD, GREENFIELDS
LOCAL COUNCIL	CITY OF MANDURAH
SITE AREA	2.2316ha

Project Overview

SPP7 DESIGN OF THE BUILT ENVIRONMENT

- 1 Context & Character
- 2 Landscape Quality
- 3 Built Form & Scale
- 4 Functionality & Build Quality
- 5 Sustainability
- 6 Amenity
- 7 Legibility
- 8 Safety
- 9 Community
- 10 Aesthetics

PRINCIPLE 1

CONTEXT & CHARACTER

Responds to and enhances
the unique characteristics of
a local area, contributing to a
sense of place

Locality Plan

{Insert Location Information - example below}

- Lot 502 is located at the corner of Pinjarra Road and Lakes Road in the City of Mandurah, 70km south of Perth CBD.
- The site is 2.2321 ha in size, bordered by Pinjarra Road (south), Lakes Road (east), and Stratford Road (north and west).
- Currently zoned ‘Urban Development’ under the Metropolitan Region Scheme (MRS).
- Lot 900 has direct frontage to both Pinjarra and Lakes Roads.
- The site forms part of a larger development parcel zoned Service Commercial with the predominant land use being ‘showroom’.



Local Context

- Future development sites to the immediate west and north zoned Service Commercial
- Large format retail to the east
- Mixed Use and Residential to the wider surrounds
- Peel Health Campus to the north west
- Pinjarra Road along the southern boundary is classified as a Primary Distributor, and Lakes Road to the East is a Distributor B road.



PRINCIPLE 2

LANDSCAPE QUALITY

Operates as an integrated
and sustainable system,
within a broader ecological
context

Landscape Quality

The proposed landscaping is designed to enhance and complement the built form, with a strong focus on human scale and comfort.

It aims to strengthen the development’s identity, walkability, connectivity, and sense of place.

The design also responds to the broader context, drawing on existing landscape features to ensure integration with the surrounding area.

The design response incorporates the following landscape objectives:

- A range of tree species throughout car park to preserve sight lines.
- Feature planting mixes and single species within setbacks to accentuate buildings and development entry point.
- Dense native and endemic planting species to provide buffer to adjacent residential lot.

SELECTED TREE SPECIES



SELECTED GROUNDCOVERS & CASCADING



SELECTED STRAPPY



SELECTED SHRUBS

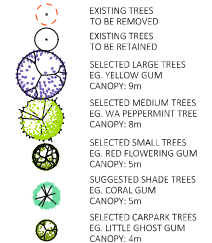


GENERAL NOTES:
1. THIS IS A CONCEPT PLAN ONLY
2. ALL STRUCTURES SUBJECT TO ENGINEERING
AND CONSTRUCTION
3. ALL MEASUREMENTS TO BE CHECKED PRIOR
TO CONSTRUCTION



REV	DATE	DWN	APP	DESCRIPTION
A	14.04.25	ALC	KD	LANDSCAPE CONCEPT PLAN
B	16.04.25	KD	KD	UPDATED FRONT SET BACK
C	01.05.25	ALC	KD	LANDSCAPE CONCEPT PLAN - UPDATED

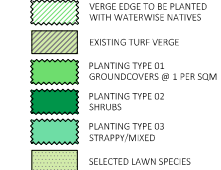
LEGEND TREES



FEATURE PLANTS



PLANTING



MISC



LANDSCAPE AREA CALCULATIONS

SITE AREA
Total site area = 22,021m²
LANDSCAPE AREAS
Total landscape areas = 2217m² (10% of site)
CAR BAYS
Total car bays provided = 214
Total trees per car bay = 38
Total = 1:6
CANOPY COVER
Proposed Large Trees (8m canopy) x 5 = 518m²
Proposed Medium Trees (6m canopy) x 4 = 201.2m²
Proposed Small Trees (5m canopy) x 36 = 705.6m²
Proposed Small Trees (4m canopy) x 11 = 158.6m²
Total Canopy Cover = 1383.4m² (63.3% of site)

THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION UNLESS
APPROVED BY CLIENT AND REVISED 'O' ISSUED FOR CONSTRUCTION

GRASS TREES ON SITE TO BE RETAINED & RELOCATED ON SITE



PRINCIPLE 3

BUILT FORM & SCALE

Shapes, massing and height that is approximate to the setting, and negotiates existing and future context

Built Form & Scale

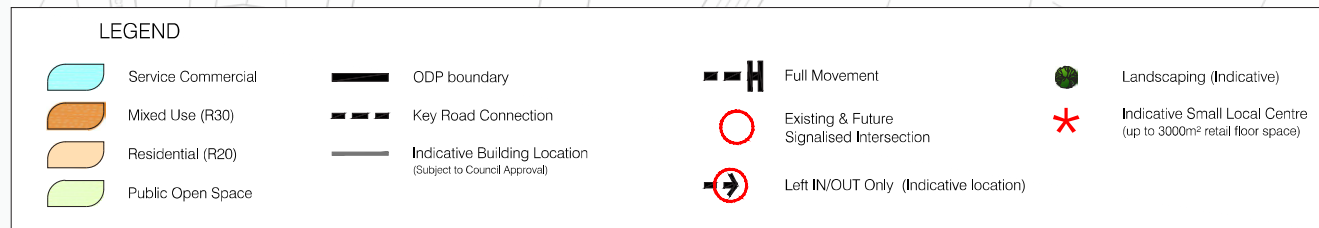
The proposed development is a high quality architectural design that serves as a gateway development along Pinjarra Road into Lakelands and complements the existing and future development.

The proposed development intends to continue an architectural language that responds to the natural landscape, topography and desired land uses in the local area.

The proposed development has been designed in a manner that balances the car-oriented nature of the land uses, having regard to the local context of the locality, with built form that appropriately addresses the key frontages.

The proposed buildings are situated to allow important N/S and E/W connections with the adjoining development sites, which once developed, will integrate seamlessly with the subject site and create a cohesive retail destination, as demonstrated in the Outline Development Plan.





1. The developer is responsible for the full construction of the intersection between the site and Pinjarra Road including construction of traffic signals to the satisfaction of Main Roads Western Australia

2. Subdivision, land use and development standards shall generally be in accordance with the Residential Zone of TPS 3 and Part 7 of the Residential Design Codes. The following land uses are also permitted ('AA'):

- Office
- Medical Centre
- Car Park
- Child Care Premises
- Consulting Room

3. Subdivision and Development Standards shall generally be in accordance with the Service Commercial Zone of TPS 3 with the exception of the following:-

4. The predominant land use within the ODP will be 'Showroom' as defined in Town Planning Scheme No.3. However, up to 3000m² of retail floor space is permitted within the location of the indicative Small Local Centre, within which land use permissibility shall be assessed against the provisions of Table 2 of Town Planning Scheme No.3. All other uses will be assessed in accordance with Table 4 of Town Planning Scheme No.3 with the exception of 'Motor Vehicle Repair and/or Motor Vehicle Hire' which is not permitted.

5. Minimum Floorspace permitted for each tenancy is 300 square metres;

6. Notwithstanding the wording of Note 1, Car parking to be provided on a ratio of 1:40 on the basis of reciprocal rights of access and parking between the subject lots;

7. Additional Building Design and Site Layout requirement are to comply with the following principles:

- Where proposed buildings front Pinjarra Road and/or Lakes Road, such buildings shall articulate elevations through a variety of materials, height, setbacks and awnings covering pedestrian walkways around the buildings, providing activated facades to both streets;
- Landscaping is to be of a high quality with the use of mature trees within parking areas, the street verge and around buildings. The retention of significant trees is encouraged where possible.
- Demarcation of pedestrian crossings and driveway access points through a variety of paving.
- The extent and amount of signage within the ODP site shall be controlled and minimised and will be subject to a signage Masterplan approved by the City for each development.
- Loading areas are to be located away from Pinjarra Road and Lakes Road.

2020-36M-01 (21.02.2014), nts

PRINCIPLE 4

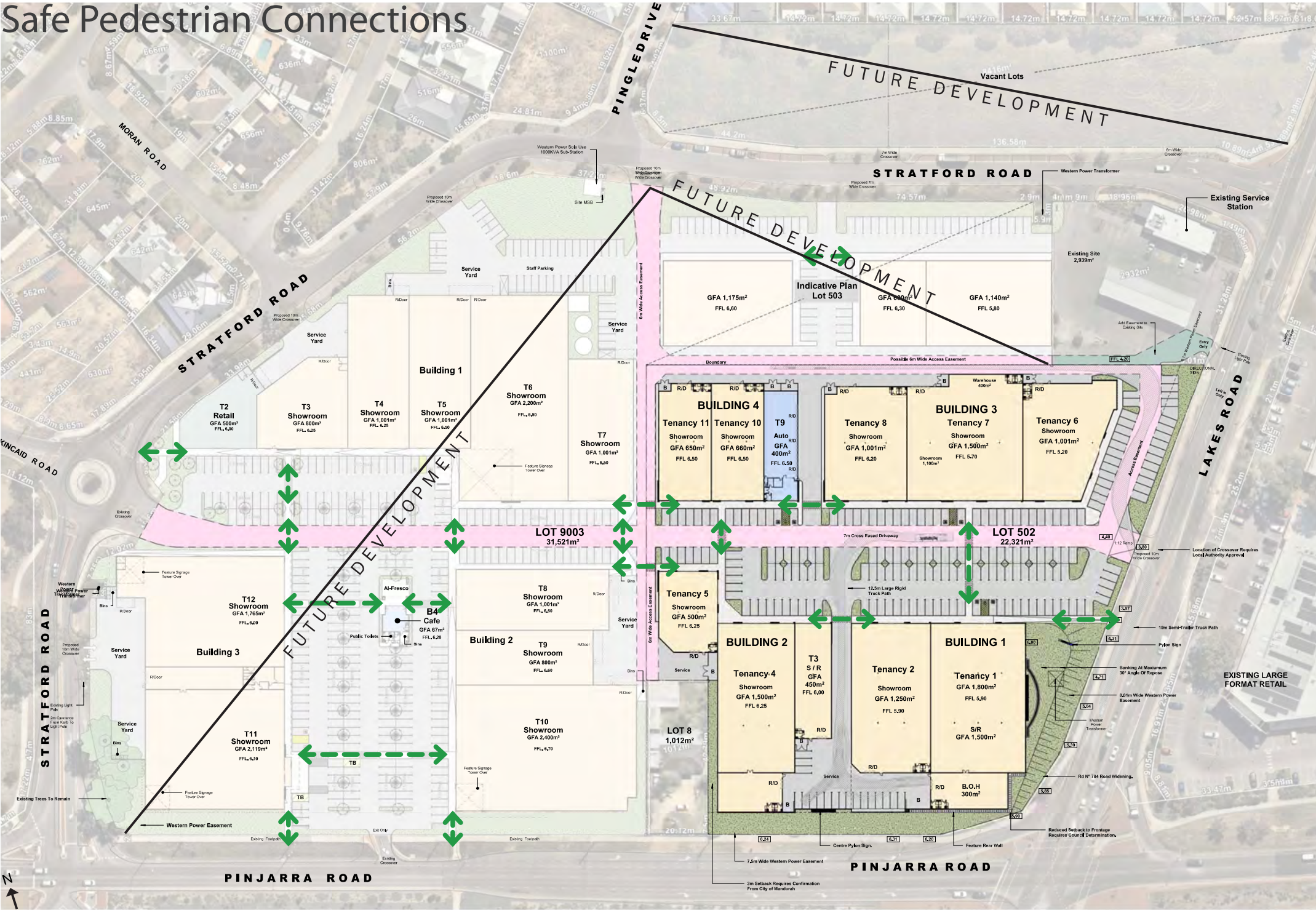
FUNCTIONALITY & BUILD QUALITY

Meets the needs of users
while efficiently and
effectively balancing
functional requirements

[illegible]

[illegible]

Safe Pedestrian Connections



Build Quality

The materials selected for the development are proposed to be highly durable to withstand harsh environmental conditions, minimise maintenance and shall respond to the desired context and character of the site.



Build Quality



PRINCIPLE 5

SUSTAINABILITY

Optimises sustainability
to deliver positive
environmental, social and
economic outcomes

Sustainability

ENERGY



- Solar arrays to all buildings to maximise solar gain
- Provision for future EV charging stations

WATER



- Water-wise and drought resistant planting
- Permeable paving zones in parking areas for passive reticulation
- Efficient tapware and fixtures specifications

MATERIALS



- Sustainable, low carbon and low VOC material selection
- Robust materiality
- Efficient construction framework
- Waste reduction and recycling
- 90% construction waste diverted from landfill

FACILITIES



- Visitor bicycle racks provided throughout

PASSIVE DESIGN



- Sun shading provided over all glazed areas.
- High performance building fabric
- High level of internal daylight



PRINCIPLE 6

AMENITY

Contributes to comfortable
and productive environments
through internal and external
amenity

Amenity

The proposal promotes high levels of amenity for the wider community, providing substantial opportunities for gathering.

The development also includes bicycle parking, end of trip facilities and EV charging (shared car park) to ensure that all forms of transport are accommodated for persons travelling to and from the site.



PRINCIPLE 7

LEGIBILITY

Contains clear connections
and memorable elements to
help people find their way
around

Legibility

- Access easements cut through the site, creating service corridors within and connecting to the adjacent development lots.
- Access driveway from Lakes Road. Future through access points to Stratford Road (upon development of adjoining sites).
- Discreet servicing access.
- Pedestrian movement prioritised internally.
- Clear sight lines, well-designed car parking, and pedestrian connections.

- 1

Development identification sign
- All Access
- Primary Circulation
- Secondary Circulation
- Servicing Circulation



PRINCIPLE 8

SAFETY

Optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use

Safety

The proposed development creates and contributes to an activated and effective street and public space. The development provides passive surveillance and has been designed in consideration of the CPTED principles to improve safety and amenity.

Sightlines have been considered through the design process, with the buildings designed to allow clear vision to the car parking areas and street frontages.

Proposed landscaping has been designed to provide shade and amenity, but not to impact on sight lines across the site, minimising opportunity for visual barriers or hidden areas.

An effective lighting scheme shall be incorporated to enhance visibility and result in a well-lit site and streetscape environment.

Clearly defined car bays, line markings, landscape strips, buildings and footpaths identify the use of specific areas of the site.

Bollards are incorporated to prevent vehicle access into 'safe' pedestrian areas on the footpaths adjoining buildings.



PRINCIPLE 9

COMMUNITY

Responds to community needs as well as the wider social context, supporting diversity and social interaction

Community

The proposed development is consistent with the intent for the sub-region given it will directly support the envisioned growth by providing services and employment opportunities to residents, whilst being supported by a series of technical inputs demonstrating the proposed development will have a net positive impact on the surrounding community.



PRINCIPLE 10

AESTHETICS

Creates attractive and
inviting spaces and places
that engage the senses

Aesthetics

Although several of the proposed tenants or owners have a generic aesthetic, the development has been thoughtfully designed to be well-planned, visually coherent, and appealing with robust materials surrounded by an enduring landscape. The buildings have been designed to a high visual standard, with facades articulated with contrasting elements which create visual interest, and is of a size and scale appropriate to the surrounding area.



PLANNING REPORT

LARGE FORMAT RETAIL DEVELOPMENT

LOT 502 (#400) PINJARRA ROAD,
GREENFIELDS

APPLICATION TO CITY OF MANDURAH / METRO OUTER DAP

18 MAY 2025



Hidding.
URBAN PLANNING

This Planning Report has been prepared by **Hidding Urban Planning** for a proposed
Large Format Retail Development at
Lot 502 (#400) Pinjarra Road, Greenfields

No part of this report may be reproduced, electronic or otherwise, without the prior written
consent of **Hidding Urban Planning**



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DOCUMENT HISTORY & DETAILS

AUTHOR	REVISION	DATE	REVISION TYPE
Nik Hidding	R01	18/05/2025	Final

File No. C2594
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Project: Greenfields
File Name: C2594appln01
Document Revision: R01

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ANNEXURES:

- Annexure 1:** Certificate of Title
 - Annexure 2:** Development Plans
 - Annexure 3:** 10 Design Principles Design Report
 - Annexure 4:** Transport Impact & Car Parking Assessment
 - Annexure 5:** Landscape Plan
 - Annexure 6:** Waste Management Plan
 - Annexure 7:** Civil Design & Certification
-

APPLICATION DETAILS

Table 1: Application Details

Property Location	Lot 502 (#400) Pinjarra Road, Greenfields
Applicant	Hidding Urban Planning
Landowner	400 Pinjarra Road Pty Ltd
Local Government	City of Mandurah
Local Planning Scheme	City of Mandurah Local Planning Scheme No. 12 (LPS12)
City of Mandurah LPS12 Zoning	Urban Development
Local Structure Plan	Outline Development Plan – Corner Lakes Road & Pinjarra Road, Mandurah Zoned - Service Commercial
Proposed Land Uses	Bulky Goods Showroom (10 tenancies) Motor Vehicle Repair (1 tenancy)
Existing Use	Vacant Land
Total Site Area	2.2316ha
Estimated Construction Value	\$14 million
Project Team:	
Architect	Meyer Shircore Architects
Town Planning	Hidding Urban Planning
Traffic & Waste Management	Move Consultants
Landscape Design	Kelsie Davies Landscape Architecture (KDLA)
Civil Design	Colliers

1.0 INTRODUCTION

Hidding Urban Planning has prepared this Planning Report on behalf of 400 Pinjarra Road Pty Ltd as part of an Application for Development Approval for a proposed Large Format Retail Development comprising 10 Bulky Goods Showroom tenancies and one (1) Motor Vehicle Repair tenancy at Lot 502 (#400) Pinjarra Road, Greenfields (**Subject Land**).

This report provides a detailed Town Planning assessment of the proposed development against the relevant State and local Planning framework. The information contained in this report confirms that the proposed development is appropriate for the site and reflects the applicable planning framework.

1.1 DEVELOPMENT ASSESSMENT PANEL (DAP) DETERMINATION

As the anticipated construction cost of the project is **\$14 million**, this applicant has “opted in” to the Development Assessment Panel (DAP) process.

Accordingly, please find **attached** the completed City of Mandurah Application for Development Approval Form, Peel Region Scheme Form 1 and DAP Form 1, all signed and authorised by the landowners.

1.2 PRE-LODGEMENT DISCUSSIONS

The applicant has met with the City of Mandurah planning team to discuss the project on two occasions prior to lodging the application, with the latest meeting occurring on 8 May 2025. Designs of the proposed development were tabled at the May 2025 meeting.

2.0 SITE DETAILS

2.1 LEGAL DESCRIPTION OF LAND

This Development Application is made in respect of Lot 502 (#400) Pinjarra Road, Greenfields. The Title details for the subject site is provided in **Table 2** below.

Table 2: Legal Description of Land

Lot	Plan	Vol/Folio	Area	Address	Proprietors
502	89972	2065/8	2.2316ha	400 Pinjarra Road, Greenfields	400 Pinjarra Road Pty Ltd

The Certificate of Title is attached at **Annexure 1**. An electricity easement is located along the sites' frontage to Lakes Road and Pinjarra Road (refer Sketch at Annexure 1). Development has been located outside of this easement.

2.2 SITE DETAILS

The subject land is located at the corner of Pinjarra Road and Lakes Road, within the City of Mandurah, 70kms south of the Perth CBD. The subject land has a total land area of 2.2316 hectares and currently comprises a single dwelling and outbuildings. All existing structures will be removed to make way for the proposed development.

The subject land is bound by Pinjarra Road to the south, Lakes Road to the east and development sites to the north and west. Existing commercial development exists on the opposite side of Lakes Road, to the east, comprising similar Large Format Retail development. A 7-Eleven service station exists to the north-east, accessed from Lakes Road.

An Aerial Photograph of the subject land and its surrounds is included at **Figure 1** below.



Figure 1: Aerial Photograph (PlanWA)

3.0 THE PROPOSAL

Development Approval is sought for a new Large Format Retail Development on the subject land, comprising 10 Bulky Goods Showroom tenancies and one (1) Motor Vehicle Repair (auto service) tenancy.

The proposed development is depicted on the attached plans prepared by Meyer Shircore Architects at **Annexure 2**. The plans include Feature Survey, Site Plan, Overall Site Plan, Building Floor Plans, Elevations, Roof Plans, and 3D Perspectives.

3.1 SUPPORTING PLANS & REPORTS

Plans, consultant reports and other supporting information have been prepared to assist in the assessment of this application, and these are detailed in **Table 3** below.

Table 3: Supporting Plans & Reports

Company	Plan/Document	Annexure
Meyer Shircore Architects	Development Plans	2
Meyer Shircore Architects	10 Design Principles Design Report	3
Move Consultants	Transport Impact & Car Parking Assessment	4
Kelsie Davies Landscape Architecture (KDLA)	Landscape Plan	5
Move Consultants	Waste Management Plan	6
Colliers	Civil Design & Certification	7

3.2 PROPOSED DEVELOPMENT COMPONENTS

3.2.1 Building & Design

The proposed Large Format Retail development will include 11 commercial tenancies with 10,712m² of total building area, representing 48% of the site area.

The development has been designed with an internal presence, facing a main internal roadway and core car parking area. The development also presents very well to Pinjarra Road and Lakes Road, with quality facades, consolidated signage areas, feature walls and design elements such as a curved aluminium blade feature.

Proposed building heights range from 7.50m-8.50m, extending up to 12.00m for signage feature elements. Building will have awnings over the main shopfront areas providing cover over adjacent pedestrian footpaths.

The development is proposed to be set back 7.51m from the Pinjarra Road boundary.

The development has been set back from Lakes Road to ensure it is outside of the existing electricity easement and to enable a quality landscape zone.

The proposed development is a high-quality architectural design that serves as a gateway development along Pinjarra Road into Greenfields and complements the existing and future development in the area.

The proposed development intends to continue an architectural language that responds to the natural landscape, topography and desired land uses in the local area.

The proposed development has been designed in a manner that balances the car-oriented nature of the land uses, having regard to the local context of the locality, with built form that appropriately addresses the key frontages.

The proposed buildings are situated to allow important N/S and E/W connections with the adjoining development sites, which once developed, will integrate seamlessly with the subject site and create a cohesive retail destination, as demonstrated in the Outline Development Plan. We understand that the adjacent development to the west has recently been approved (amended Development Approval).

The buildings have been designed to a high visual standard, with facades articulated with contrasting elements which create visual interest and are of a size and scale appropriate to the surrounding area.

The proposed development is supported by a Design Report addressing the 10 Design Principles of State Planning Policy 7.0 – Design of the Built Environment – refer **Annexure 3**.

3.2.2 Operation

It is anticipated that the tenancies of the Large Format Retail development will trade during normal business hours 8:00am – 6:00pm Monday to Saturday and 11:00am to 5:00pm on Sundays and Public Holidays. However, given the type of development proposed, individual businesses may trade beyond these times at their discretion, and it is not considered necessary to restrict the business hours of the development through a condition of development approval.

3.2.3 Vehicular Access, Car Parking & Pedestrian Access

Vehicular access to the site will be derived from a new 10m wide crossover from Lakes Road at the eastern side of the property, together with a centralised cross accessway connecting with the adjacent property to the west. The location of the proposed crossover from Lakes Road accords with the planned location for site access considered through previous structure planning processes.

Cross access easements have been discussed and agreed with adjacent landowners and can be formalised prior to occupation of development (and imposed through appropriately-worded Conditions of Development Approval).

There are 215 car parking bays proposed on the subject site (inclusive of disabled bays), with the main parking zone proposed in the central part of the site and adjacent to Lakes Road in the north-eastern portion of the site. Some car parking is also proposed at the rear of Tenancies 1-4 adjacent to back-of-house areas for those tenancies. It is anticipated that this parking area will be for staff only.

A Transport Impact & Car Parking Assessment has been prepared by Move Consultants in support of the proposal – refer **Annexure 4**.

Pedestrian access has been fully considered, with footpaths proposed connecting each of the proposed tenancies along with pedestrian crossings over vehicular driveways in key locations. The footpaths will connect to footpaths associated with the proposed development on the adjacent site to the west, as well connecting to the Lakes Road pedestrian footpath.

Service and loading areas are discrete, being proposed at rear locations, generally outside of main public zones. These service areas will be accessed by service vehicles as well as the general public collecting goods from service pick-up locations. Service/loading areas have awnings extending out from the main building over key roller door locations, providing weather cover for loading operations and public goods collection (refer Roof Plans).

3.2.4 Landscaping

Comprehensive landscape zones have been proposed across the site, with an area of 2265m² representing 10% of the site area. The main landscape zones are along the Pinjarra Road and Lakes Road setback areas. Smaller landscape zones and tree diamonds are proposed in the centralised car parking area.

In support of the proposal, a Landscape Plan has been prepared by Kelsie Davies Landscape Architecture (KDLA) – refer **Annexure 5**.

The proposed landscaping is designed to enhance and complement the built form, with a strong focus on human scale and comfort.

It aims to strengthen the development's identity, walkability, connectivity, and sense of place.

The design also responds to the broader context, drawing on existing landscape features to ensure integration with the surrounding area.

The design response incorporates the following landscape objectives:

- A range of tree species throughout car park to preserve sight lines.
- Feature planting mixes and single species within setbacks to accentuate buildings and development entry point.
- Dense native and endemic planting species to provide buffer to adjacent residential lot.

3.2.5 Signage

Consolidated signage areas are proposed inclusive of wall signage on external facades as well as pylon signs. There are two pylon sign proposed; one along Lakes Road and one along Pinjarra Road.

The type and size of signage is conducive to the type of development proposed and represents an aesthetic and cohesive outcome.

3.2.6 Waste Management

A Waste Management Plan has been prepared by Move Consultants in support of the proposal – refer **Annexure 6**.

3.2.7 Civil Design

Colliers has prepared a Civil Design for the proposal – refer **Annexure 7**.

The stormwater drainage design for the development has been undertaken in accordance with the City of Mandurah requirements to store a 100 ARI (1% AEP) of 24 hour duration storm event and will comprise the use of 27 soakwells in combination with 1,056 GRAF cells.

4.0 PLANNING ASSESSMENT

4.1 PEEL REGION SCHEME

The subject land is zoned “Urban” under the Peel Region Scheme (**PRS**) as shown in **Figure 2** below. The subject land fronts Pinjarra Road which is a “Primary Regional Roads” Reserve. No direct access is proposed from Pinjarra Road.

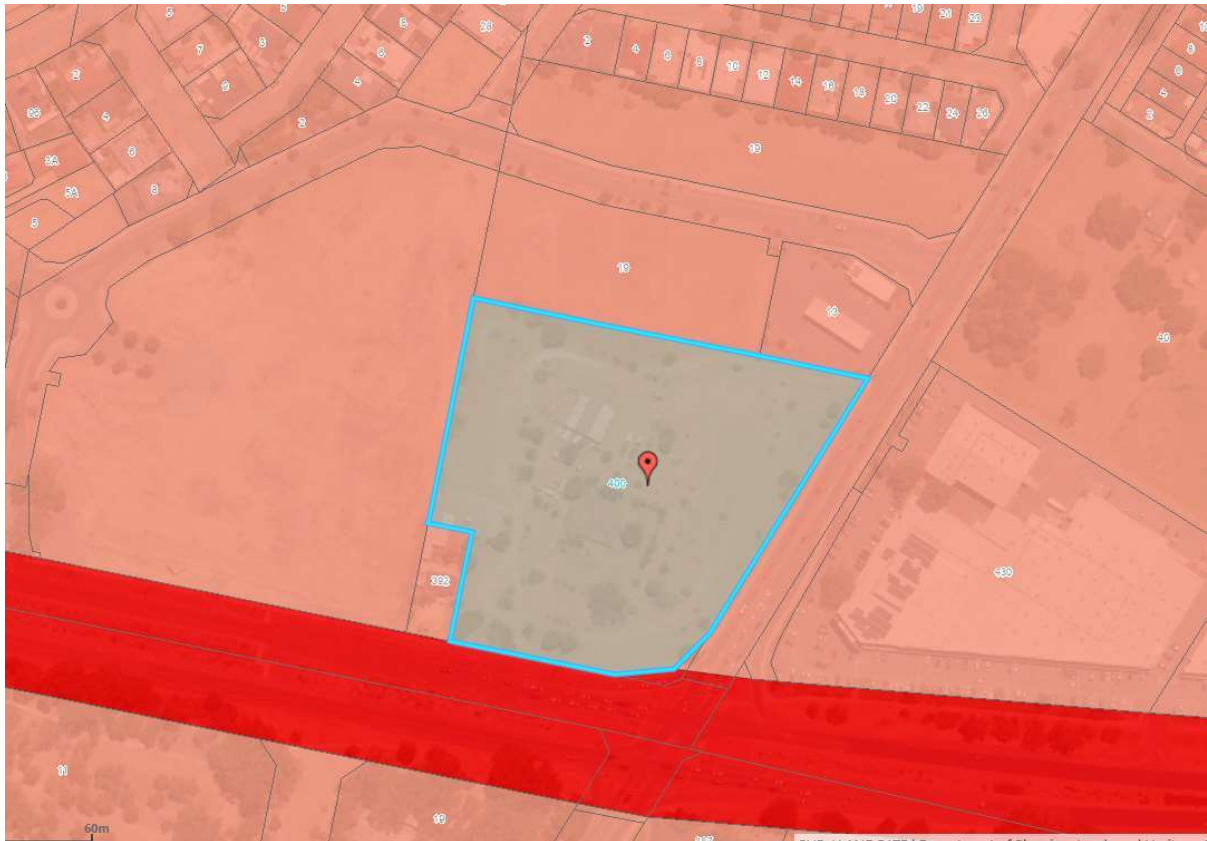


Figure 2: Peel Region Scheme Map Extract (PlanWA)

4.2 CITY OF MANDURAH LOCAL PLANNING SCHEME NO. 12

4.2.1 Zoning & Land Use

The subject land and immediately adjacent lots are zoned “Urban Development” in the City of Mandurah Local Planning Scheme No. 12 (**LPS12**) as shown in **Figure 3**.

Table 2 of LPS12 sets out the objectives of the Urban Development zone, which are —

- *To provide an intention of future land use and a basis for more detailed structure planning in accordance with the provisions of this Scheme.*
- *To provide for a range of residential densities to encourage a variety of residential accommodation.*

- To provide for the progressive and planned development of future urban areas for residential purposes and for commercial and other uses normally associated with residential development.

The proposed development is considered to accord with the Objectives by providing new development and land uses which accord with the Structure Planning processes that have occurred over the years.

Detailed planning has occurred with the approval of the Mandurah East Structure Plan (approved 2009) as well as the Outline Development Plan 142 (ODP142) – Corner of Lakes Road and Pinjarra Road (approved 2014). The subject land is identified as a “Service Commercial” zone in ODP142. An assessment of the proposed development against the relevant provisions of the ODP142 are set out in Part 4.3 of this Report.

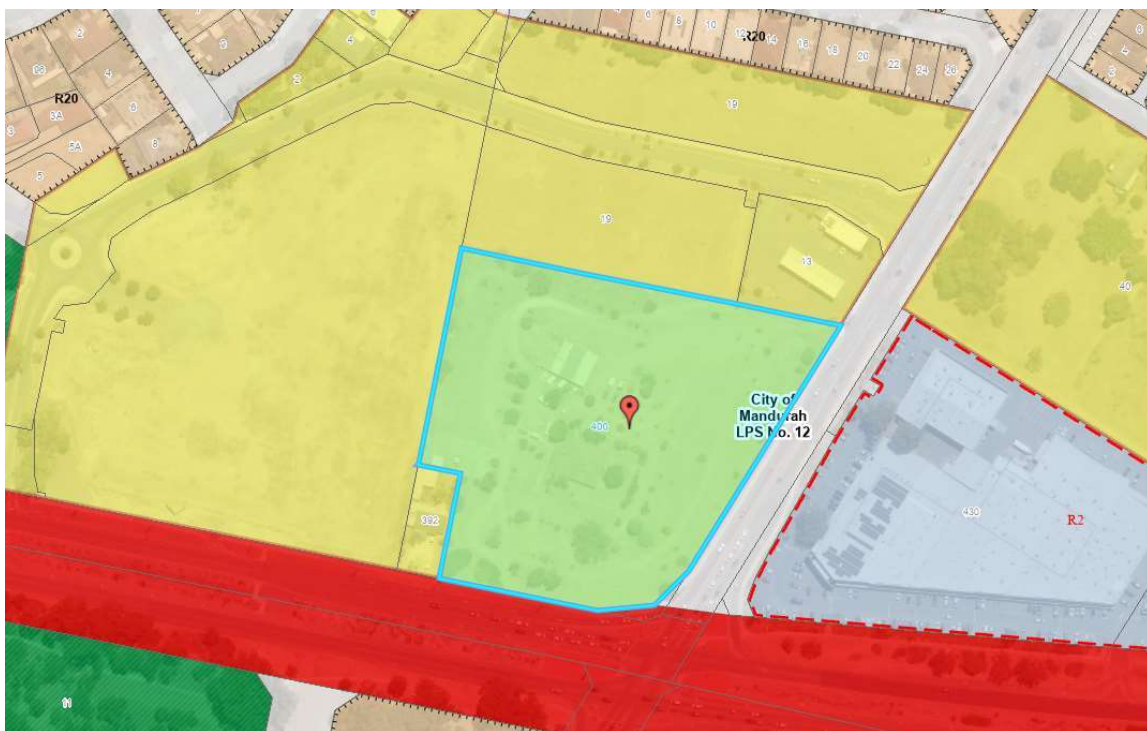


Figure 3: LPS12 Scheme Map Extract (PlanWA)

Table 3 ‘Zoning Table’ of LPS12 does not set out any land use permissibility for the Urban Development zone. Clause 3.3.6 of LPS12 sets out:

If the zoning table does not identify any permissible uses for land in a zone the local government may, in considering an application for development approval for land within the zone, have due regard to any of the following plans that apply to the land:

- (a) a structure plan;
- (b) a local development plan.

Accordingly, a Structure Plan is required to provide guidance on the permissibility of uses within the “Urban Development” zone.

Part 4.3 of this Report details how the proposed development accords with the ODP142 requirements.

The land uses associated with the proposed development are the Bulky Goods Showroom use (for 10 tenancies) and the Motor Vehicle Repair use (for one tenancy). These land uses are defined in Clause 6.2 of LPS12 as follows:

“Bulky Goods Showroom” means premises

(a) used to sell by retail any of the goods and accessories of the following types that are principally used for domestic purposes:

- (i) automotive parts and accessories;*
- (ii) camping, outdoor and recreation goods.*
- (iii) electric light fittings;*
- (iv) animal supplies including equestrian and pet goods;*
- (v) floor and window coverings;*
- (vi) furniture, bedding, furnishings, fabrics, manchester and homewares;*
- (vii) household appliances, electrical goods and home entertainment goods;*
- (viii) party supplies;*
- (ix) office equipment and supplies;*
- (x) babies’ and children’s goods, including play equipment and accessories;*
- (xi) sporting, cycling, leisure, fitness goods and accessories;*
- (xii) swimming pools;*

or

(b) used to sell by retail goods and accessories by retail if:

- (i) a large area is required for the handling, display or storage of the goods; or*
- (ii) vehicular access to the premises is required for the purpose of collection of purchased goods.*


“Motor Vehicle Repair” means premises used for or in connection with –

- (a) electrical and mechanical repairs, or overhauls, to vehicles other than panel beating, spray painting or chassis reshaping of vehicles; or*
- (b) repairs to tyres other than recapping or retreading of tyres;*

4.2.2 Development Standards & Requirements

Schedule 1 of LPS12 sets out a range of development standards and requirements. The relevant standards and requirements of LPS12 are considered in **Table 4** below.

Table 4: LPS12 Development Standards & Requirements

Development Standard or Requirement	Response
Schedule 1 Additional Requirements that apply to Land in Scheme Area	
General Requirements	
2. Where the R-Codes do not apply in all Centre Zones and the Service Commercial zone.	
<p>Notwithstanding the requirements relating to development that are set out in a Precinct Structure Plan, Local Development Plans or Local Planning Policies the following provisions shall be provided for development where the R-Codes do not apply in all Centre zones and the Service Commercial Zone:</p> <p>(a) Buildings shall be required to front the primary street and shall be of high quality. Elevations are to include articulation a through variety of materials, height, setbacks and awnings covering pedestrian walkways around the building. Walls with no articulation and no variety of materials will not be accepted.</p>	<p>Buildings have been designed with high quality facades, using a range of colours, materials, feature elements and varying height forms. At the corner of Pinjarra Road and Lakes Road, the building steps in, creating an interesting building form, see below.</p> 
(b) Wherever possible buildings are to address all public spaces, through the provision of windows and control of signage on windows to provide for surveillance and security. The blanking out of windows with signage is not acceptable.	It is not always possible to have windows on all sides of buildings, given the nature of the intended use (bulky good showrooms), however the facades which don't have windows are designed with quality materials and interesting colour schemes and feature elements, enabling a quality outcome.
(c) Landscaping is to be of a high quality through the use of trees within parking areas, the street verge and around buildings, complimented by extensive low-rise native landscaping.	High quality landscaping areas are proposed, refer Landscaping Plan.
(d) A variety of paving is to be provided adjacent to the building and within the car parking area to demarcate pedestrian crossings and driveway access, ensuring that the extent of bitumen is reduced.	Pedestrian connections have been proposed and these will be constructed using alternate materials and colours to demarcate these areas separate to driveway and access areas.
(e) Car parking areas are to be designed to wrap around the site and/or buildings,	Car parking areas are proposed centrally to the development but also wrap around the frontage to Lakes Road in the north-eastern part of the site.

Development Standard or Requirement	Response
<i>to form internal access ways. Car parking areas are to be articulated using high quality landscaping, tree plantings, variety of pavements, pedestrian refuge islands and access ways.</i>	The development forms a series of internal accessways. Connectivity to development sites to the north and west are also proposed. Car parking areas will be supplemented with landscaping zones and tree planting and key pedestrian crossings are also proposed.
<i>(f) The layout and location of access ways, parking, loading bay and service areas are to be designed to permit vehicles to enter streets in a forward gear.</i>	The design of the development, as well as consideration for linking in with adjacent development sites, enables cross access by all vehicles and ensuring all vehicles can enter surrounding streets in a forward gear.
<i>(g) Service and loading areas are to be screened from street view and residential land uses.</i>	Service and loading areas are proposed at rear areas, away from street and predominantly away from normal public access.
<i>(h) The extent and amount of signage shall be controlled and minimised and shall have regard to the following:</i> <i>(i) An area on the building's facade shall be designed to incorporate the appropriate signage for individual tenancies, in a manner that does not detract from the building's design;</i> <i>(ii) The painting of the building in a tenant's corporate colours is considered a form of signage and should not detract from the building's design; and,</i> <i>(iii) Use of pylon and free-standing signs is to be restricted and kept to a minimum having reference to the applicable Precinct Structure Plan, Local Development Plan or Local Planning Policy.</i>	<p>Formalised signage zones have been proposed on buildings to ensure that they are incorporated into the façade.</p> <p>Colours are proposed to align with intended the corporate colours of businesses, however this provides variety and interest.</p> <p>Freestanding signage is kept to a minimum, with only two pylon signs proposed allowing for multiple business advertising (one proposed along Pinjarra Road and one along Lakes Road).</p>
Zone Requirements	
Service Commercial Zone	
<i>Within the Service Commercial and General Industry zones, the following shall apply:</i> <i>(a) Minimum setbacks:</i> <i>6m Primary Street</i> <i>3m Secondary Street</i> <i>Nil Side and Rear</i>	Pinjarra Road setback: 7.51m Lakes Road setback 21.8m (northern) Lakes Road setback 8m (southern) Reduced setback at intersection – requires City discretion Setback to rear (north) boundary: 3m Setback to neighbouring Lot 8: 3m Setback to side (west) boundary: 4m
<i>(b) Open space and Landscaping:</i> <i>(i) a minimum of 10 percent of site shall be set aside for open space and landscaping, excluding car parking and manoeuvring areas;</i>	2265m ² of landscaping comprising 10.15% of the site area has been set aside for landscaping.

Development Standard or Requirement	Response
<i>(ii) a minimum of 3m landscaping strip shall be provided within the front setback.</i>	Landscaping zones along setback areas varies in width, with a large 7.5m wide landscaping zone along Pinjarra Road, a large landscaping area along Lakes Road (southern end), with a narrower landscaping area along Lakes Road at the northern end. Overall, the landscaping proposed is considered to be good and warrants support.
Schedule 2 Parking Requirements	
<u>Bulky Goods Showroom:</u> 1 parking bay per 50m ² of net lettable floor area	<u>Bulky Good Showroom:</u> 10,312m ² @ 1/50m ² = 206 bays
<u>Motor Vehicle Repair:</u> 1 parking bay per 50m ² of net lettable floor area	<u>Motor Vehicle Repair:</u> 400m ² @ 1/50m ² = 8 bays Total Bays Required: 214 bays Total Provided: 215 bays Car Parking is compliant with LPS12.

As set out in Table 4 above, the proposal is generally compliant with the Development Standards and Requirements of LPS12.

4.2.3 Matters to be Considered

Clause 67 - Part 2 - Schedule 2 (**Deemed Provisions**) of the *Planning and Development (Local Planning Schemes) Regulations 2015* (Regulations) outlines matters to be given due regard by local government when assessing development applications.

Table 5 below provides an assessment against matters relevant to this proposal.

Table 5: Clause 67 Matters to be Considered

Relevant Matters to be Considered	Response
<i>(a) The aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area;</i>	The proposed use and development is consistent with the aims and provisions of the City's LPS12 for the following reasons: <ul style="list-style-type: none"> The proposal seeks approval for land uses which are capable of approval in the Urban Development zone inclusive of the Outline Development Plan applicable to the site. The proposed development is in general compliance with the provisions of LPS12.
<i>(b) The requirements of orderly and proper planning including any proposed local</i>	This report demonstrates the proposed development is in general compliance with the

Relevant Matters to be Considered	Response
<i>planning scheme or amendment to this Scheme that has been advertised under the Planning and Development (Local Planning Schemes) Regulations 2015 or any other proposed planning instrument that the local government is seriously considering adopting or approving;</i>	local planning framework applicable to the subject site.
(c) <i>Any approved State Planning Policy</i>	This report demonstrates the proposed development is in compliance with the relevant State Planning Policies.
(d) <i>Any environmental protection policy approved under the Environmental Protection Act 1986 section 31 (d);</i>	This matter is not relevant to this proposal.
(e) <i>Any policy of the Commission</i>	This matter is not relevant to this proposal.
(f) <i>Any policy of the State</i>	This matter is not relevant to this proposal.
(fa) <i>Any local planning strategy for this Scheme endorsed by the Commission</i>	The City of Mandurah Local Planning Strategy formulated the preparation of the recent LPS12. Accordingly, it is considered that the proposed development aligns with the objectives of the Local Planning Strategy.
(g) <i>Any local planning policy for the Scheme area;</i>	This report demonstrates the proposed development is in general compliance with the local planning policies applicable to the subject site.
(h) <i>Any structure plan or local development plan that relates to the development.</i>	The Mandurah East Structure Plan (2009) and the Outline Development Plan 142 (2014) have been prepared and approved. The proposed development has been considered against the provisions of ODP142 – refer Part 4.3 of this Report.
(i) <i>Any report of the review of the local planning scheme that has been published under the Planning and Development (Local Planning Schemes) Regulations 2015.</i>	This matter is not relevant to this proposal.
(j) <i>In the case of land reserved under this Scheme, the objectives for the reserve and the additional and permitted uses identified in this Scheme for the reserve.</i>	This matter is not relevant to this proposal as the subject land is not reserved under the Scheme.
(k) <i>The built heritage conservation of any place that is of cultural significance.</i>	This matter is not relevant to this proposal.
(l) <i>The effect of the proposal on the cultural heritage significance of the area in which the development is located.</i>	This matter is not relevant to this proposal.
(m) <i>The compatibility of the development with its setting, including - (i) the compatibility of the development with the desired future character of its setting; and</i>	The proposed development is entirely compatible with its setting for the following reasons: <ul style="list-style-type: none"> • The proposed development is suitably located within a service commercial precinct and aligns with approved development to the

Relevant Matters to be Considered	Response
(ii) <i>the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development;</i>	<p>west which is of a similar nature, and existing commercial development to the east.</p> <ul style="list-style-type: none"> The scale of the development does not impact any adjacent properties and is similar in its scale to other developments (existing and proposed) in the precinct. <p>Having regard to the above, the nature of the proposed development is entirely compatible with its surroundings.</p>
<p>(n) <i>The amenity of the locality including the following –</i></p> <p>(i) <i>Environmental impacts of the development;</i></p> <p>(ii) <i>The character of the locality;</i></p> <p>(iii) <i>Social impact of the development;</i></p>	<p><u>Environmental Impacts:</u></p> <p>The proposed development is not anticipated to result in any adverse environmental impacts.</p> <p><u>Character of the Locality:</u></p> <p>The character of the area, particularly along the northern side of Pinjarra Road, is a mix of commercial development which the proposed development is similar to. The proposed development is considered to align with the future character of the area, having been planned for the type of development proposed for over 10 years.</p> <p><u>Social Impacts:</u></p> <p>The proposed development will not have any adverse social impacts on the surrounding locality, but will provide a positive social impact through the creation of jobs through construction and operation of the facility which is intended to cater for the growing population in the area.</p>
(o) <i>The likely effect of the development on the natural environment or water resources and any means that are proposed to protect or to mitigate impacts on the natural environment or the water resource.</i>	This matter is not relevant to this proposal.
(p) <i>whether adequate provision has been made for the landscaping of the land to which the application relates and whether any trees or other vegetation on the land should be preserved.</i>	The development proposal provides large areas of landscaping, allowing the planting of trees to beautify the site. Refer Landscaping Plan at Annexure 5 .
(q) <i>the suitability of the land for the development taking into account the possible risk of flooding, tidal inundation, subsidence landslip, bush fire, soil erosion, land degradation or any other risk.</i>	A stormwater management plan has been prepared – refer Annexure 7 .
(r) <i>The suitability of the land for the development taking into account the possible risk to human health or safety.</i>	This matter is not relevant to this proposal.
<p>(s) <i>The adequacy of –</i></p> <p>(i) <i>the proposed means of access to and egress from the site; and</i></p> <p>(ii) <i>arrangements for the loading, unloading, manoeuvring and parking of vehicles;</i></p>	As outlined in this Report and the supporting Transport Impact Assessment (TIA) prepared by Move Consultants (Annexure 4), the proposed access arrangements to and from the site are satisfactory.

Relevant Matters to be Considered	Response
	The development has been appropriately designed, including appropriate arrangements for manoeuvring and parking of vehicles.
(t) <i>The amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system in the locality and the probable effect on traffic flow and safety;</i>	The Transport Impact Assessment (Annexure 4) indicates that the proposed development will not have an unacceptable level of impact on the surrounding road network.
(u) <i>the availability and adequacy for the development of the following —</i> <i>(i) public transport services;</i> <i>(ii) public utility services;</i> <i>(iii) storage, management and collection of waste;</i> <i>(iv) access for pedestrians and cyclists (including end of trip storage, toilet and shower facilities);</i> <i>(v) access by older people and people with disability;</i>	The Transport Impact Assessment (Annexure 4) addresses these matters. The Waste Management Plan (Annexure 6) addresses the storage, management and collection of waste.
(v) <i>The potential loss of any community service or benefit resulting from the development other than potential loss that may result from economic competition between new and existing businesses.</i>	No loss of any community service or benefit is expected to occur as a result of the proposed development.
(w) <i>The history of the site where the development is to be located.</i>	No significant historical matter is relevant to this proposal.
(x) <i>The impact of the development on the community as a whole notwithstanding the impact of the development on particular individuals.</i>	The proposed development is not anticipated to impact the community.
(y) <i>Any submissions received on the application.</i>	The City can advertise the application for public comment and assess any relevant submission made.
(za) <i>The comments or submissions received from any authority consulted under clause 66.</i>	The City can refer the application to any authority it considers appropriate, and to assess any relevant submissions/comments made. The application is likely to be referred to Main Roads WA.
(zb) <i>Any other planning consideration the local government considers appropriate.</i>	The City can determine whether there are any other planning considerations that are relevant.

Having regard to **Table 5** above, the proposal appropriately addresses the relevant matters to be given due regard as set out in the Deemed Provisions.

4.3 OUTLINE DEVELOPMENT PLAN 142

The subject land is subject to Outline Development Plan 142 (**ODP142**) and sits within the “Service Commercial” as shown in **Figure 4** below.

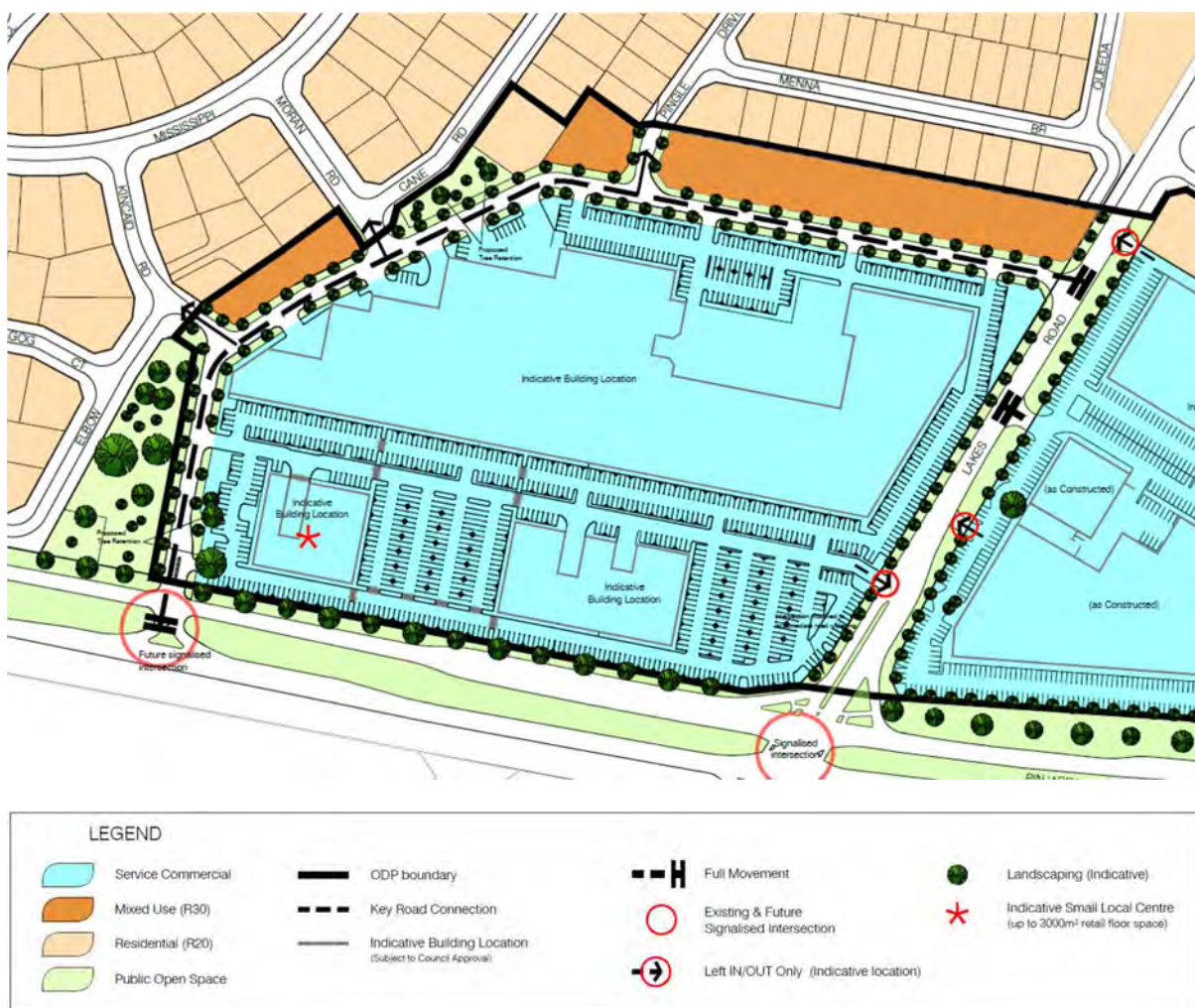


Figure 4: ODP142 Extract

ODP142 is a document that is to be given due regard in the assessment of the application but is not ultimately binding on a decision-maker.

Land use permissibility is to be guided the “Service Commercial” zoning of ODP142 and the zoning table of LPS12.

A “Bulky Goods Showroom” use is a “P” Permitted Use in the “Service Commercial” zone under LPS12 and a “Motor Vehicle Repair” use is a “D” Discretionary Use in the zone and is capable of approval at the discretion of the decision-maker.

Table 6 below provides an assessment against the relevant development standards of ODP142.

Table 6: Development Standards of ODP142

Development Standard	Response
<i>The developer is responsible for the full construction of the intersection between the site and Pinjarra Road including construction of traffic signals to the satisfaction of Main Roads WA.</i>	This work has already occurred. Complies

Development Standard	Response
<p>Service Commercial</p> <p>3. Subdivision and Development Standards shall generally be in accordance with the Service Commercial Zone of TPS3 with the exception of the following:</p>	<p>This report addresses the various development standards of the City's LPS12 which supersedes TPS3.</p> <p style="text-align: right;">Complies</p>
<p>4. The predominant land use within the ODP will be 'Showroom' as defined in Town Planning Scheme No. 3. However, up to 3000m² of retail floor space is permitted within the location of the indicative Small Local Centre, within which land use permissibility shall be assessed against the provisions of Table 2 of Town Planning Scheme No. 3. All other uses will be assessed in accordance with Table 4 of Town Planning Scheme No. 3 with the exception of 'Motor Vehicle Repair and/or Motor Vehicle Hire' which is not permitted.</p>	<p>The new land use under LPS12 is "Bulky Goods Showroom" which the proposal is predominately for.</p> <p>Notwithstanding the ODP provision advising that Motor Vehicle Repair is not permitted, the Service Commercial zoning and land use permissibility under LPS12 indicates it is capable of approval. Further, the provisions of an Outline Development Plan or Structure Plan are not binding on a decision-maker. Accordingly, the land uses are capable of approval.</p>
<p>5. Minimum floorspace permitted for each tenancy is 300 square metres.</p>	<p>All proposed tenancies exceed the minimum prescribed tenancy floor area.</p> <p style="text-align: right;">Complies</p>
<p>6. Notwithstanding the wording of Note 1, Car parking to be provided on a ratio of 1:40 on the basis of reciprocal rights of access and parking between the lots.</p>	<p>Car parking has been provided in accordance with the parking ratios of Schedule 2 of LPS12, using the ratio of 1:50.</p> <p>Requiring parking at a rate of 1:40 as per the ODP would increase the parking requirement and this is considered to be unnecessary.</p> <p>Given the ODP requirements are in conflict with LPS12, the requirements LPS12 should prevail.</p> <p style="text-align: right;">Discretion (Parking complies with LPS12)</p>
<p>7. Additional Building Design and Site Layout requirement are to comply with the following principles:</p> <ul style="list-style-type: none"> Where proposed buildings front Pinjarra Road and/or Lakes Road, such buildings shall articulate elevations through a variety of materials, height, setbacks and awnings covering pedestrian walkways around the buildings, providing activated facades to both streets; Landscaping is to be of a high quality with the use of mature trees within parking areas, the street verge and\ around buildings. The retention of significant trees is encouraged where possible. Demarcation of pedestrian crossings and driveway access points through a variety of paving. The extent and amount of signage within the ODP site shall be controlled and minimised and will be subject to a signage Masterplan approved by the City for each development. 	<ul style="list-style-type: none"> As per response in part 4.2.2 of this report, the proposed buildings have been designed with high quality facades, using a range of colours, materials, feature elements and varying height forms. At the corner of Pinjarra Road and Lakes Road, the building steps in, creating an interesting building form. High quality landscaping areas proposed including opportunity for tree planting within car parking areas and around buildings, particularly within the street setback zones. Key pedestrian crossings have been clearly marked out in the plans. Signage has been proposed on main facades of buildings as is normal for this development type. Loading areas are located away from Lakes Road, however there are loading areas in proximity to Pinjarra

Development Standard	Response
<ul style="list-style-type: none"> Loading areas are to be located away from Pinjarra Road and Lakes Road. 	Road, however these are screened from view through screen walls and the main Centre Pylon. Loading areas have roof cover as shown on Roof Plans.

4.4 LOCAL PLANNING POLICIES

4.4.1 Local Planning Policy No. 2 - Signage

Approval is required under the City's Local Planning Policy No. 2 – Signage (**LPP2**) for the type of signage proposed.

Part 3.2 of LPP2 sets out the following Assessment Criteria for signage:

- The scale and design of the proposed signage is compatible with the character of the locality and the general land use of the area.*
- The scale and design of the proposed signage is in keeping with the scale and design of the building and does not obscure architectural features.*
- The proposed signage does not block views, does not interfere with the safe movement of pedestrian traffic, or affect the amenity of adjoining properties.*
- The proposed signage represents a cohesive and attractive visual statement.*
- The proposed signage ensures that visual surveillance of the street is maintained or enhanced.*

The proposal complies with the Assessment Criteria, as follows:

- The scale, design and layout of the signage proposed is compatible with the character of the locality, for example the existing large format retail development to the east which has other similar land uses. The type of signage proposed is typical for large format retail developments.
- The scale and design of signage is in keeping with the scale and design of proposed buildings, being integrated into the facades as shown in the Elevation Plans and 3D Perspectives.
- Proposed signage does not block views or interfere with the safe movement of pedestrian traffic and does not affect the amenity of adjoining premises. Adjoining premises will be developed for similar development/land use.
- Overall, the proposed signage strategy presents a cohesive and attractive visual statement.
- Visual surveillance of streets, principally Lakes Road, is maintained.

Accordingly, it is considered that the proposed signs comply with the assessment criteria of the Signage Policy and are capable of approval.

4.5 STATE PLANNING POLICIES

4.5.1 WAPC SPP 7.0 – Design of the Built Environment

The Design Report attached at **Annexure 3** provides a response to the 10 Design Principles of State Planning Policy 7.0 Design of the Built Environment (**SPP7.0**) and can be used for the purposes of presentation to the Design Review Panel.

5.0 CONCLUSION

Hidding Urban Planning seeks Development Approval for a proposed Large Format Retail Development at Lot 502 (#400) Pinjarra Road, Greenfields.

In summary, the proposed development warrants approval for the following reasons:

- The proposed development is generally compliant with the development standards and requirements of the City of Mandurah Local Planning Scheme No. 12.
- The proposed Bulky Goods Showroom use and Motor Vehicle Repair use is capable of approval under the Service Commercial zoning of Outline Development Plan 142 as Permitted and Discretionary Uses, respectively.
- The proposed development provides for the new development of the existing site, which is currently vacant.
- The proposal considers surrounding development and enables cross access between adjacent development sites to ensure a cohesive and connected outcome.
- The proposed development is supported with an expert traffic assessment, landscape design, stormwater design, and waste management plan demonstrating compliance with the City's relevant technical requirements.

Having regard to the above, the proposed development should be supported and approved.

For these reasons, and considering the assessment contained within this report, we respectfully request that the City of Mandurah have regard to the merits and broader benefits of the proposal when undertaking its assessment of the application and to recommend approval to the Metro Outer DAP, subject to reasonable conditions.

Hidding.
URBAN PLANNING

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PO Box 920 Subiaco WA 6904
hidding.com.au

ANNEXURES

ANNEXURE 1

CERTIFICATE OF TITLE

ANNEXURE 2

DEVELOPMENT PLANS

ANNEXURE 3

10 DESIGN PRINCIPLES REPORT

ANNEXURE 4

TRANSPORT IMPACT & CAR PARKING ASSESSMENT

ANNEXURE 5

LANDSCAPE PLAN

ANNEXURE 6

WASTE MANAGEMENT PLAN

ANNEXURE 7

CIVIL DESIGN & CERTIFICATION

SELECTED TREE SPECIES



AGOfle



CORfic



EUCvic



EUCtor



EUCleu



HAKlau

SELECTED GROUNDCOVERS & CASCADING



ACAsal



ADEcun



MYOpar



EREblu



CRegin



HIBsca



SCAaem

SELECTED STRAPPY



CONcan



DIAeme



LOMTan



LOMnya



DIABla



ANIfila

SELECTED SHRUBS



WESaus



OLEaxi



MELLit



WESgre



CORDus



BANniv



XANpre

REV	DATE	DWN	APP	DESCRIPTION
A	14.04.25	ALC	KD	LANDSCAPE CONCEPT IMAGES

PLANTING PALETTE

Symbol	Species	Common Name	Quantities	Size
Trees				
ADOfle	Agave flexilis	Wk Peppermint	As Shown	100L
CORfic	Corymbia ficifolia (grafted)	Wk Red Flowering Gum	As Shown	100L
EUCvic	Eucalyptus victrix	Yuhua Gum	As Shown	100L
EUCtor	Eucalyptus torquata	Coral Gum	As Shown	100L
EUCleu	Eucalyptus leucopylla ssp. megalocarpa	Yellow Gum	As Shown	100L
EUCvic	Eucalyptus victrix	Little Ghost Gum	As Shown	100L
HAKlau	Hakea laurifolia	Wk Cullen Hakea	As Shown	100L
LACind	Lagerstroemia indica	White Grape Myrtle	As Shown	100L
MELqui	Melaleuca quinquenervia	Broad-leaved Paperbark	As Shown	100L
Groundcovers and Cascading				
ACAsal	Acaesalpinx saligna	Springtime Cascade Wattle	3/m2	140mm
ADCan	Adiantum canaliculatum	Coral Carpet	2/m2	140mm
COFle	Corymbia ficifolia	Fire Bells	2/m2	140mm
DIOfle	Dichondra 'Silver Falls'	Silver Falls	2/m2	140mm
IBREta	Brembsia 'Blue Horizon'	Blue Horizon	3/m2	140mm
GRGin	Grevillea 'Gin Gin Gem'	Gin Gin Gem	2/m2	140mm
HIBsca	Hibbertia scandens	Snake Vine	2/m2	140mm
MYOpar	Myoporum parvifolium 'Variental'	Variental	2/m2	140mm
ROSEar	Rosemaria officinalis 'Prostratus'	Creeping Rosemary	2/m2	140mm
SCAem	Scaevola humilis 'Purple Fusion'	Pan Flower	2/m2	140mm
Strappy				
CONcan	Conoclinium canaliculatum	Grey Cottonheads	3/m2	140mm
DIAeme	Dianella aemula	Emerald Arch	3/m2	140mm
DIABla	Dianella blanda	Emerald Blazer	3/m2	140mm
FIKnob	Fiona nodosa	Knobby Cuckoo Bush	3/m2	140mm
LOMTan	Lomandra tanika	Tanika	3/m2	200mm
LOMnya	Lomandra nymphaea	Nyalia	3/m2	200mm
BANniv	Banksia nivalis	Honey-pot dryandra	2/m2	140mm
MELlit	Melaleuca 'Little Nessel'	Dwarf Honey Myrtle	2/m2	140mm
MURpan	Murphya paniculata	Orange Jasmine	3/m2	200mm
DIABla	Dianella blanda	Coral Carpet	3/m2	140mm
PIFmuf	Phyllostegia 'Miss Muffet'	Miss Muffet	3/m2	200mm
RIHawn	Rhipidolobos 'Oriental Pearl'	Dwarf Indian Hawthorn	2/m2	200mm
SYZora	Syzygium 'Orange Tassel'	Lilly Pilly	2/m2	200mm
WESbox	Wesleya 'Aussie Box'	Aussie Box	3/m2	200mm
WESbox	Wesleya 'Grey Box'	Grey Box	3/m2	200mm
Shade Planting				
ALRepl	Agave reptans	Bungle weed	3/m2	140mm
OLmin	Olea minor	Burn Lily	3/m2	200mm
CORbus	Corymbia 'Buddy Bells'	Buddy Bells	3/m2	140mm
DIORep	Dichondra repens	Kidney plant	3/m2	140mm
LIRus	Liriodendron 'Just Right'	Just Right	3/m2	140mm
PHIXan	Philodendron Xanadu	Xanadu	3/m2	200mm
SARtong	Sarcocaulis trifasciata laurentii	Mother-in-law's Tongue	3/m2	200mm
VIOned	Viola hederacea	Native Violet	3/m2	200mm
Feature Plants				
ANIfila	Anigozanthos flavidus	Kangaroo Paw	2/m2	12L
XANpre	Xanthorrhoea preissii	Grass Tree	As Shown	MTP

NOTES

- GENERAL
 - 1.1. ALL NOTES ARE AS NOTED AND TO SUIT ALL PAPER SIZE
 - 1.2. THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION UNLESS REVISED BY ISSUED FOR CONSTRUCTION AND SIGNED AND APPROVED BY PROJECT MANAGER/ENGINEER/DESIGNER
 - 1.3. PLANTING LIST SHOULD BE CHECKED BY SUPERINTENDENT BEFORE INSTALLATION BEGINS
- SOIL PREPARATION
 - 2.1. ALL AREAS ARE TO BE FINE GRADED EXCEPT TO CONFORM TO KNEE LEVELS AND SURROUNDING FINISHES
 - 2.2. SURFACES SHALL BE FREE FROM OBSTRUCTIONS, IRREGULARITIES AND POTENTIAL DRAINAGE ISSUES. ANY AREAS, AREAS SHALL BE BROUGHT TO A MINIMUM OF 100mm DEPTH OF 200mm
 - 2.3. PLANTING AREAS SHALL BE SPREAD WITH 100mm SOIL OF APPROVED STANDARD SOIL. CONSIDER THAT SHALL BE APPLIED TO RELEVANT AREAS (TERRACES, ETC)
- PLANTING
 - 3.1. PLANTED AREAS SHALL BE BLENDED WITH AN ORANGE (WOODCHIPS) MULCH UNLESS OTHERWISE STATED TO A MINIMUM DEPTH OF 75mm
 - 3.2. ADVANCED TREES SHALL BE SITED 100mm FROM CURB OR POOL OR 500mm FROM THE WATER CONNECTION POINT OR AS STIPULATED
 - 3.3. TREES PLANTED WITH A DRAINAGE OR BARRIER WALLS AND/OR BARRIERS SHALL BE INSTALLED WITHIN 500mm DEPTH MULCH ROOT BARRIER
 - 3.4. PLANTING SHALL BE SITED TO THE RIGHT OF THE ROAD OR AS STIPULATED
 - 3.5. PLANTS TO BE SITED IN OPEN SPACES TO THE RIGHT OF THE ROAD OR AS STIPULATED
 - 3.6. PLANTS TO BE SITED IN OPEN SPACES TO THE RIGHT OF THE ROAD OR AS STIPULATED
 - 3.7. PLANTS SHALL BE SUPPLIED FROM AN INDUSTRY ACCREDITED NURSERY/PLANTER. PLANTS SHALL BE IN APPROPRIATE SIZE FOR THE LISTED POT SIZE AND IN GOOD HEALTH
- IRRIGATION
 - 4.1. PLANTING TO GROUND LEVEL TO BE IRRIGATED WITH A FULLY AUTOMATIC SYSTEM FROM MAINS
 - 4.2. WATER PRESSURE TO HAVE A MINIMUM FLOW RATE OF 10LPM AT 100mm FROM THE WATER CONNECTION POINT OR AS STIPULATED
 - 4.3. CONTROLLER TO BE LOCATED IN SERVICE ROOM OR AS STIPULATED
 - 4.4. USING SENSITIVE PRESSURE SENSING TO MONITOR PLANTING AREAS TO BE PROVIDED BY OTHERS
 - 4.5. IRRIGATION TO BE SITED TO THE RIGHT OF THE ROAD OR AS STIPULATED
 - 4.6. IRRIGATION TO BE SITED TO THE RIGHT OF THE ROAD OR AS STIPULATED
 - 4.7. IRRIGATION TO BE SITED TO THE RIGHT OF THE ROAD OR AS STIPULATED
 - 4.8. IRRIGATION TO BE SITED TO THE RIGHT OF THE ROAD OR AS STIPULATED
 - 4.9. IRRIGATION TO BE SITED TO THE RIGHT OF THE ROAD OR AS STIPULATED
 - 4.10. IRRIGATION TO BE SITED TO THE RIGHT OF THE ROAD OR AS STIPULATED



REV	DATE	DWN	APP	DESCRIPTION
A	14.04.25	ALC	KD	LANDSCAPE CONCEPT PLAN
B	16.04.25	KD	KD	UPDATED FRONT SET BACK
C	01.05.25	ALC	KD	LANDSCAPE CONCEPT PLAN - UPDATED
D	01.08.25	JB	KD	LANDSCAPE CONCEPT PLAN - UPDATED
E	12.08.25	KD	KD	LANDSCAPE CONCEPT PLAN - UPDATED

LEGEND

TREES

- EXISTING TREES TO BE REMOVED
- EXISTING TREES TO BE RETAINED
- SELECTED LARGE TREES
EG. YELLOW GUM
CANOPY: 8m
- SELECTED MEDIUM TREES
EG. WA PEPPERMINT TREE
CANOPY: 8m
- SELECTED SMALL TREES
EG. RED FLOWERING GUM
CANOPY: 5m
- SUGGESTED SHADE TREES
EG. CORAL GUM
CANOPY: 5m
- SELECTED CARPARK TREES
EG. LITTLE GHOST GUM
CANOPY: 4m
- SELECTED DENSE BUFFER TREES
EG. PIN CUSHION HAKEA
CANOPY: 3m

FEATURE PLANTS

- FEATURE PLANTING
EG. GRASS TREES
- SELECTED FEATURE PLANTING
EG. KANGAROO PAWS

PLANTING

- VERGE EDGE TO BE PLANTED WITH WATERWISE NATIVES
- EXISTING TURF VERGE
- PLANTING TYPE 01
GROUNDCOVERS @ 1 PER SQM
- PLANTING TYPE 02
SHRUBS
- PLANTING TYPE 03
STRAPPY/MIXED

MISC

- PAVING (BY OTHERS)
- BLUE GRAVEL
- SELECTED EDGE RESTRAINT
- FEATURE ROCKS

LANDSCAPE AREA CALCULATIONS

SITE AREA
Total site area = 22,321m²
LANDSCAPE AREAS
Total Landscape areas = 2,115m² (9.4% of site)
CAR BAYS
Total car bays provided = 210
Total trees per car bays = 40
Total = 1:5
CANOPY COVER
Proposed Large Trees (5m canopy) x 5 = 381.5m²
Proposed Medium Trees (8m canopy) x 6 = 301.8m²
Proposed Small Trees (5m canopy) x 34 = 666.4m²
Proposed Small Trees (4m canopy) x 13 = 165.8m²
Proposed Small Trees (3m canopy) x 5 = 50m²
Total Canopy Cover = 1,505.5m² (7% of site)

THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION UNLESS APPROVED BY CLIENT AND REVISED '0' ISSUED FOR CONSTRUCTION

GRASS TREES ON SITE TO BE RETAINED & RELOCATED ON SITE



LARGE FORMAT RETAIL DEVELOPMENT LANDSCAPE CONCEPT PLAN

MEYER SHIRCORE
LOT 502, PINJARRA ROAD, GREENFIELDS



Project: **Transport Impact and Car Parking
Assessment – V3**
Lot 502 (No. 400), Pinjarra Road, Greenfields
Proposed Large Format Retail Development

Owners: **400 Pinjarra Road Pty. Ltd.**

Author: Heidi Herget

Signature: 

Date: 21st May 2025

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2	HH	HH	20/05/2025	REV		20/05/2025
3	HH	HH	21/05/2025	REV		21/05/2025

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1. INTRODUCTION

Move Consultants has been commissioned to prepare a Transport Impact and Parking Assessment for a large format retail development, inclusive of ten (1) showroom tenancies and an auto centre, to be located on Lot 502 (No. 400), Pinjarra Road within the north-west quadrant of the signalised intersection of Pinjarra Road/Lakes Road/Wanjeep Road, Greenfields in the City of Mandurah. This assessment has been prepared in accordance with the City of Mandurah's *Town Planning Scheme No. 3*, the *Mandurah East Structure Plan* and the *WAPC Transport Impact Assessment Guidelines – Vol. 4: Individual Developments*. Background information has also been distilled from the previous application lodged for the amalgamated Lot 501, Pinjarra Road Greenfields TIA prepared by Transcore in 2014, where relevant and appropriate.

1.1 Location

The site is located within the north-west quadrant of the signalised intersection of Pinjarra Road/Lakes Road/Wanjeep Road, Greenfields within the Mandurah East Commercial Area and is shown in **Figure 1**. The site is located approximately 1.2km east of the Mandurah Forum Shopping Centre and 1.6km due south-east of the Mandurah Railway Station.



Figure 1 – Local Context

The location of the site in a metropolitan context is shown in **Figure 2**.

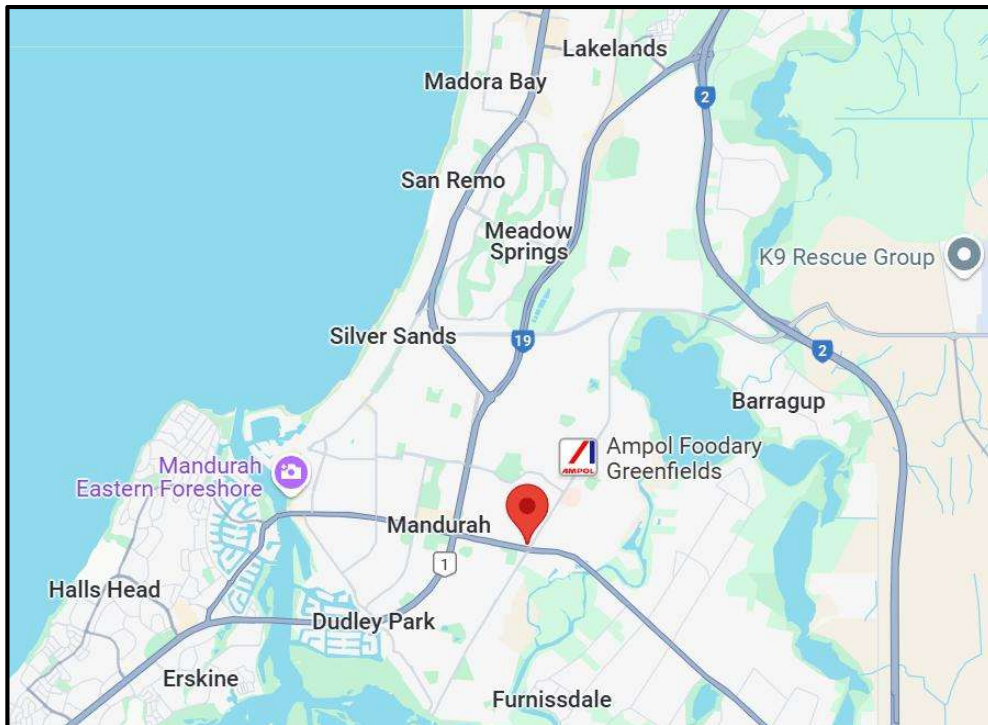


Figure 2: Metropolitan Context

The proposal is located within the *Mandurah East Local Structure Plan* area as shown in **Figure 3**.

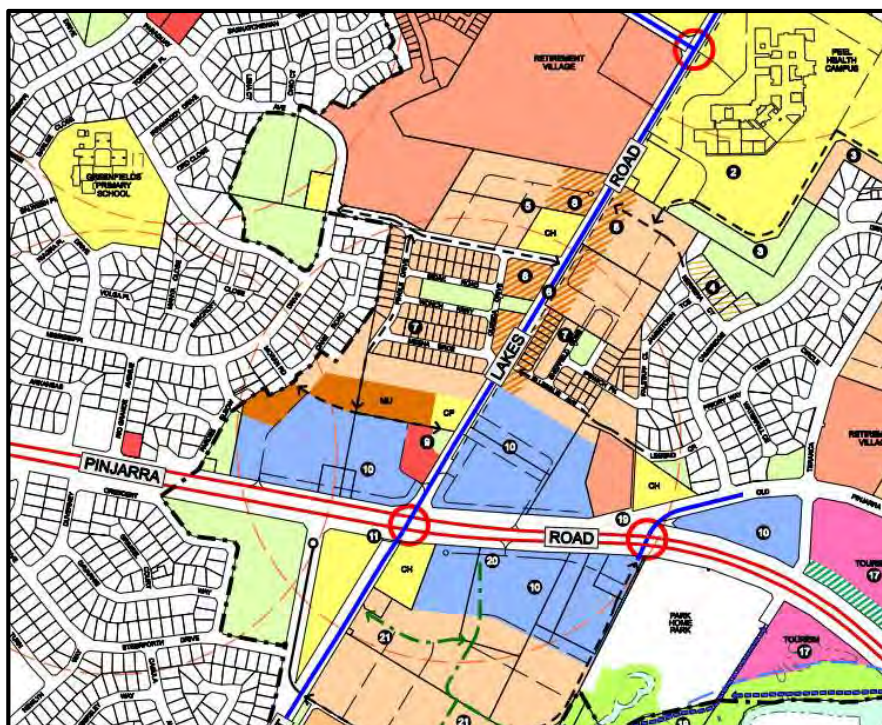


Figure 3: Mandurah East Structure Plan Area

The proposal consists of the development of a large format retail development with 11 tenancies, inclusive of ten (10) showroom tenancies and an auto centre, with access proposed to the west side of Lakes Road, north of Pinjarra Road and shared access with 372 Pinjarra Road, located immediately to the west of the site to the south side of Stratford Road. In addition, reciprocal east-west access across the subject site and 372 Pinjarra Road has been allowed for in both site plans. A copy of the proposed site plan is attached in in **Appendix A**.

Two (2) site visits were conducted on 3rd April and 1st May 2025 to identify any potential traffic-related issues associated with the proposal and to undertake ‘spot’ traffic surveys in the vicinity of the intersection of Pinjarra Road and Lakes Road to ascertain latent level of traffic travelling along Lakes Road and in the vicinity of the Lakes Road/Stratford Road intersection.

1.2 Scope of Assessment

This report has been prepared in accordance with the Western Australian Planning Commission’s *Transport Assessment Guidelines for Developments: Volume 4 – Individual Developments* (2006).

Specifically, this report aims to assess the impacts of the proposed development on the boundary road network in the vicinity of the site to identify any modifications, to site or road layout, which may be required to serve the proposed site. In addition, the assessment considers the proposed access, circulation, and egress arrangements to and from the site.

For this purpose, the traffic operations on the adjacent and broader local road network have been assessed under both existing and future proposed traffic conditions with regard to the potential impacts from additional traffic generated by the proposed development of the site.

2. EXISTING MOVEMENT NETWORK

2.1 Road Infrastructure

Pinjarra Road is a primary east-west connecting road serving a broad catchment of users between the Mandurah Foreshore area to the west and the Forrest Highway interchange and Pinjarra Town Centre to the east. It functions as the major east-west road within the broader Mandurah area and has been constructed as a dual divided carriageway in the vicinity of the site with a fixed raised central median. It has been designated as a *Primary Distributor* road under the Main Roads WA *Functional Road Hierarchy*, which is defined as a road which “...provides for major regional and interregional traffic movement and carries large volumes of generally fast-

moving traffic with some roads designated as strategic freight routes, with all designated as National or State Roads. These roads are managed by Main Roads.” It operates under a posted speed limit of 60kph and is owned, operated, and maintained by Main Roads Western Australia. Pinjarra Road is also designated as a PBS Tandem Drive Quad Axle Concessional Level 2 PBSQ2.2 under the Main Roads WA *Restricted Access Vehicle Network*. Future road planning for Pinjarra Road, as documented in the 2014 Transcore TIA, indicates that the road will potentially be widened to a triple divided carriageway in the future, subject to capital works funding and traffic demands.

Lakes Road, to the east of the site, functions as a north-south link between Pinjarra Road and the Gordon Road Industrial Area to the north and north-west of the site as well as primary access to the Greenfields residential urban cell. It has been constructed as a single divided carriageway in the vicinity of the site with dedicated turning lanes in place in the fixed central median to serve the Mandurah Home City development located opposite the subject site within the north-east quadrant of the signalised intersection of Pinjarra Road/Lakes Road. It has been designated as a *District Distributor B* road, north of Pinjarra Road, which is defined as a road which “...*performs a similar function to type A district distributors but with reduced capacity due to flow restrictions from access to and roadside parking alongside adjoining property. These are often older roads with a traffic demand in excess of that originally intended. District Distributor A and B roads run between land-use cells and generally not through them, forming a grid which would ideally space them around 1.5 kilometres apart and are managed by Local Government.*” It operates under a posted speed limit of 60kph in the vicinity of the site and is owned, operated, and maintained by the City of Mandurah. Lakes Road has also been designated as a PBS Tandem Drive Concessional Drive Level 3 PBS1B.3 on the Main Roads WA *Restricted Access Vehicle Network*.

Wanjeep Street, located to the south-east of the site connecting to the south side of Pinjarra Road, is a secondary north-south road providing direct access into Coodanup urban cell and connects to Pinjarra Road opposite Lakes Road at a signalised 4-way intersection. It has been constructed as a wide single divided carriageway. It has been classified as a *Local Distributor* road under the Main Roads *Functional Road Hierarchy* which is defined as a road which “...*carries traffic within a cell and link District Distributors at the boundary to access roads. The route of the Local Distributor discourages through traffic so that the cell formed by the grid of District Distributors only carries traffic belonging to or serving the area. These roads should accommodate buses but discourage truck and are managed by Local Government.*” Wanjeep

Street operates under a speed limit of 50kph and is owned, operated and maintained by the City of Mandurah.

Stratford Road, located along the northern boundary of the site, is a local road providing direct access into abutting properties as well as access to Pinjarra Road at its south-western terminus via a signalised T-intersection. It has been constructed as a single undivided carriageway in the vicinity of the site. It has been designated as *Access Roads* which are defined as roads which “...provide access to abutting properties with amenity, safety and aesthetic aspects having priority over the vehicle movement function. These roads are bicycle and pedestrian friendly and are managed by Local Government.” Stratford Road operates under a speed limit of 50kph and is owned, operated, and maintained by the City of Mandurah.

An extract from the MRWA’s *Functional Road Hierarchy* is shown in **Figure 4**.



Figure 4 – MRWA Functional Road Hierarchy

Table 1 shows the indicative existing daily traffic volumes for the boundary road network in the vicinity of the site.

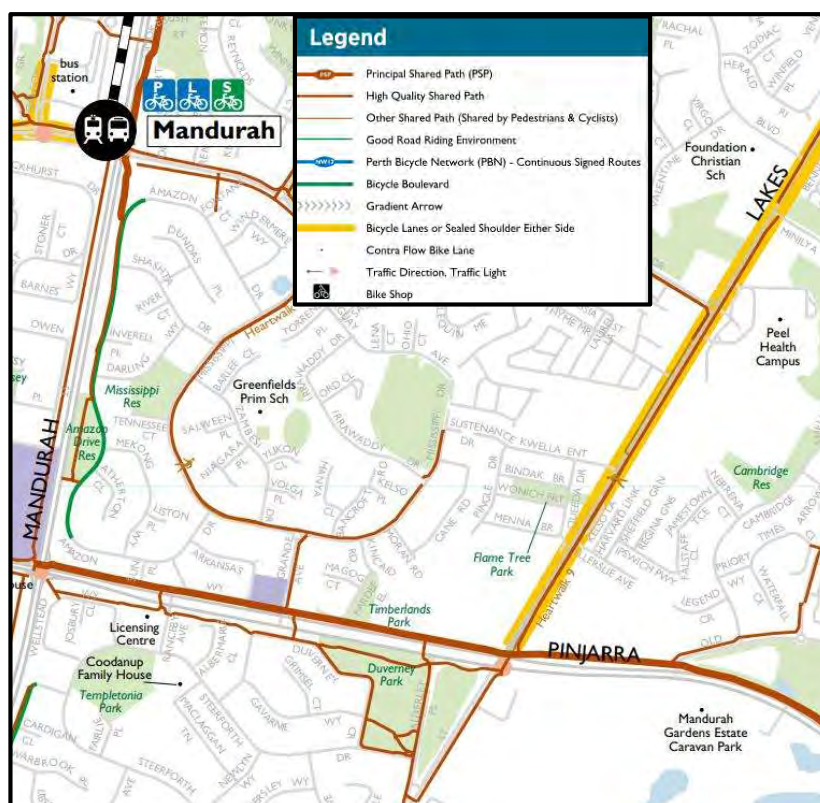
Table 1: Existing Traffic Volumes

Road Name	Existing Daily Volume (vpd)	Practical Capacity (vpd)
Pinjarra Road (East of Stratford Road)	20,950 vpd (MRWA, 2023)	35,000 to 40,000 vpd
Pinjarra Road (East of Lakes Road)	20,400 vpd (MRWA, 2023)	35,000 to 40,000 vpd
Wanjeep Street (South of Pinjarra Road)	9,300 vpd (MRWA, 2022)	10,000 to 15,000 vpd
Stratford Road (North of Pinjarra Road)	1,000 vpd (MRWA, 2022)	3,000 vpd

2.2 Pedestrian and Cycling Infrastructure

There are dual use paths in place on both sides of Pinjarra Road adjacent to and opposite the southern boundary of the site. There are on-road cycle lanes/sealed shoulders in place on both sides of Lakes Road, adjacent to the eastern boundary of the site with a footpath in place on the west side and a dual use path on the east side. Recently completed works on Stratford Road have delivered a dual use path on the west and north side of the road between Pinjarra Road and Lakes Road and on the south side of the ride immediately west of Lakes Road adjacent to the existing 7-11 petrol station.

Figure 5 shows the existing pedestrian and cycling infrastructure in the vicinity of the site.

**Figure 5: Pedestrian and Cycling Infrastructure**

The City of Mandurah's Integrated Transport Strategy has designated Pinjarra Road as a Primary Route and Lakes Road as a Secondary Route in its Long-Term Cycle Network as shown in **Figure 6**.

6.

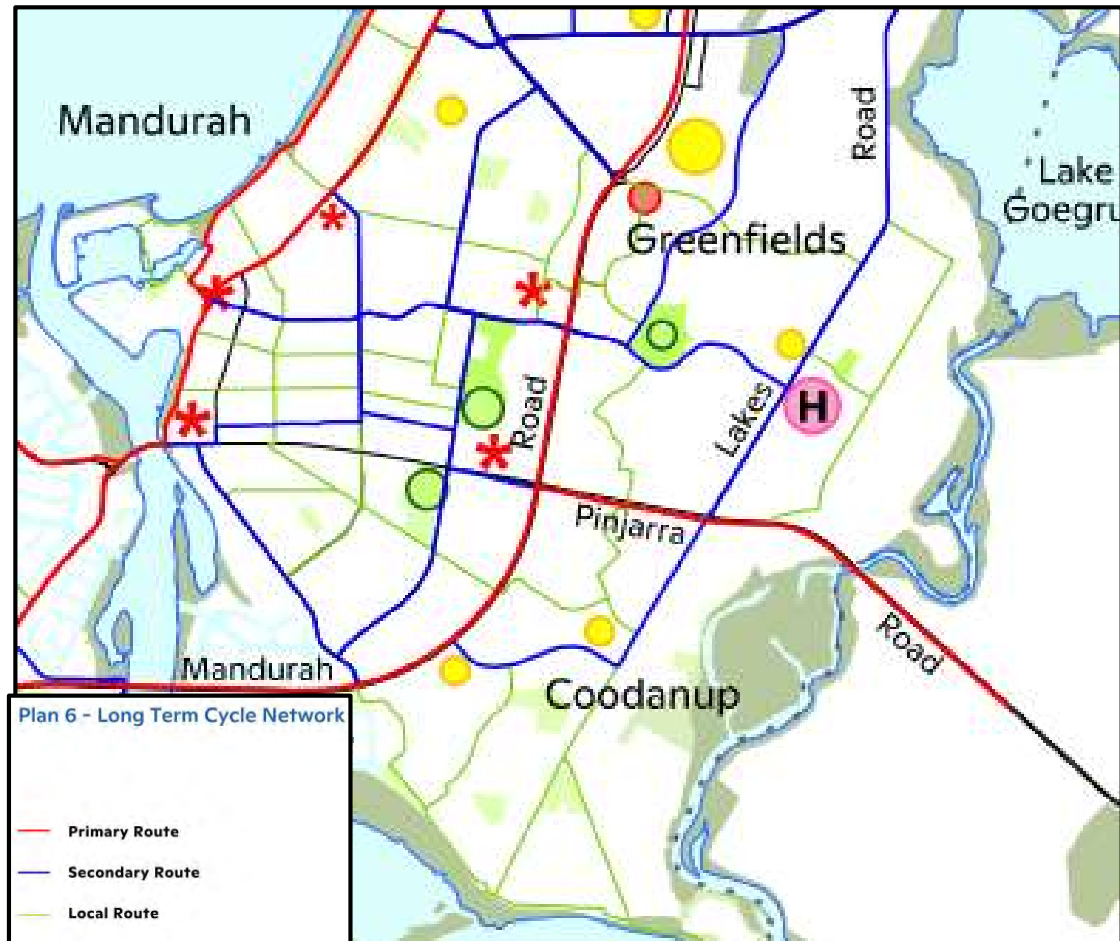


Figure 6: City of Mandurah Long-Term Cycle Network (Excerpt)

2.3 Public Transport Infrastructure

The subject site is served line haul bus services along Pinjarra Road to the south of the site with bus stops in place on both sides of the road to the west of Stratford Road. Bus services operating along Pinjarra Road include the following:

- Route 600: Mandurah Station to Pinjarra via Pinjarra Road and Ravenswood
- Route 604: Mandurah Station to South Yunderup via North Yunderup

These services provide direct access to the Mandurah Forum Shopping Centre and the Mandurah Railway Station to the west and north-west of the site, respectively, and provide service frequencies of 30-minutes during weekday peak hours and hourly to 2-hourly service outside these times.

Figure 7 show the public transport infrastructure in the vicinity of the site.

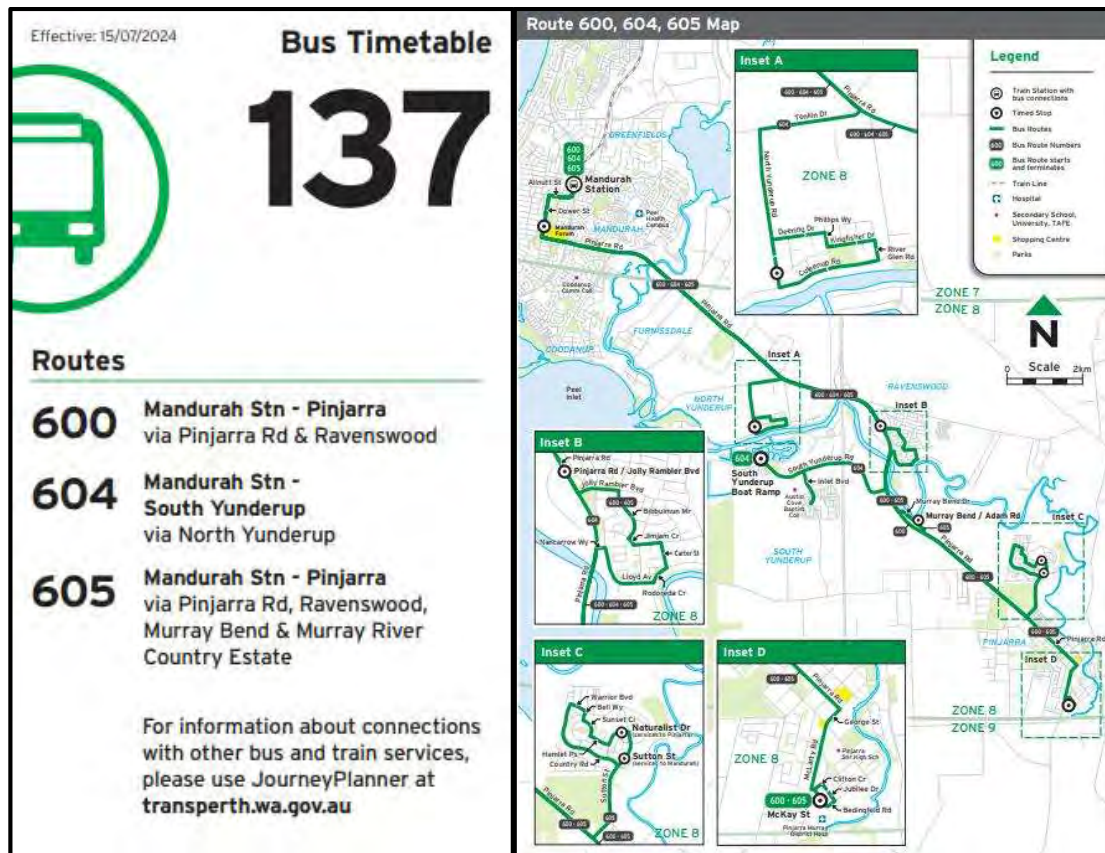


Figure 7: Public Transport Infrastructure Route 115

3. DEVELOPMENT PROPOSAL

3.1 Development Details

The site is proposed to be developed as a large format retail development catering to ten (10) showroom tenancies and an auto centre with primary access afforded via the west side of Lakes Road along the eastern boundary of the site and secondary access via a shared access to the south side of Stratford Road via the adjacent property located to the immediate west at 372 Pinjarra Road. Reciprocal access across both 372 Pinjarra Road and 400 Pinjarra Road will be provided as part of the development of both properties via a central east-west connection as shown in **Figure 8**.

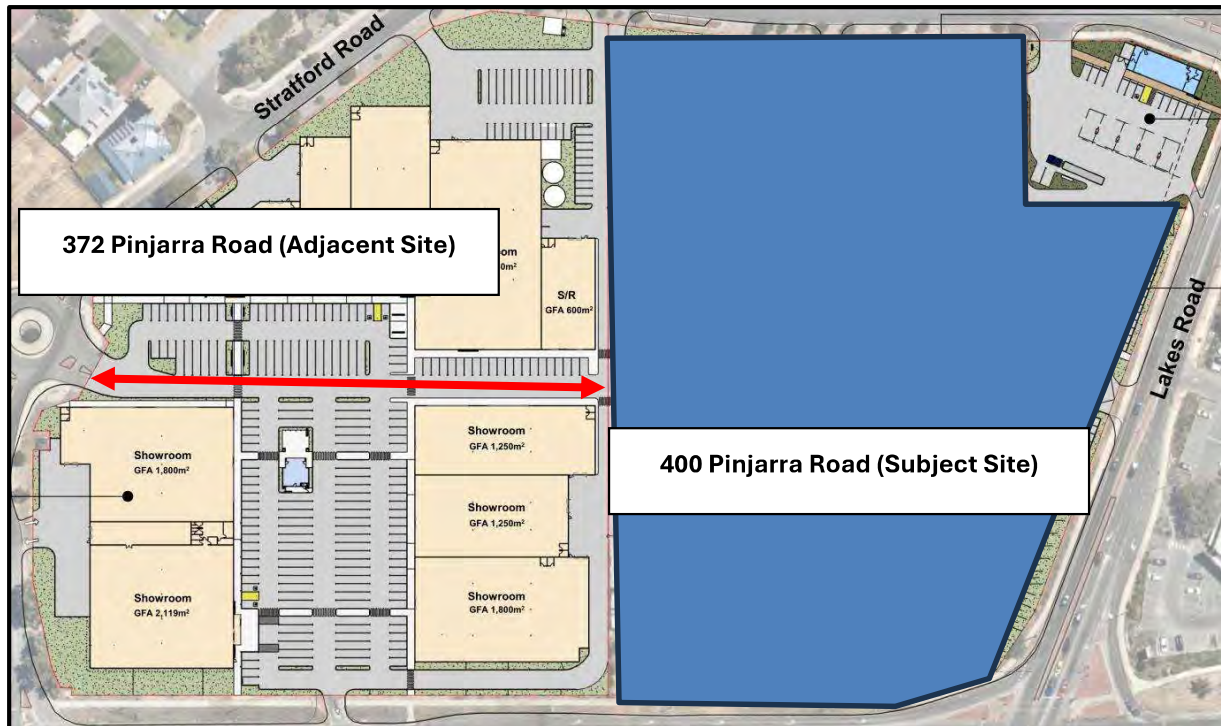


Figure 8: Proposed East-West Reciprocal Access

3.2 Access and Car Parking

Vehicular access to the site has been proposed via a full movements crossover to the west side of Lakes Road between Pinjarra Road and the existing 7-11 petrol station to the north of the site with secondary access afforded via a shared crossover with the adjacent property at 372 Pinjarra Road to the south side of Stratford Road. Reciprocal east-west access across the site extending from the Lakes Road crossover to the primary crossover serving 372 Pinjarra Road to the west at the roundabout with Stratford Road has been negotiated with the adjacent landowner to the immediate west in order to allow for efficient and effective distribution of traffic across both sites into and out of the sites. An additional crossover to the south side of Stratford Road along the northern boundary of the site, east of the shared main crossover, is also proposed to serve the site.

Proposed car parking on the site consists of 215 at-grade bays

3.3 Bicycle Parking

Bicycle parking is proposed to be provided on in accordance with Council and Austroads requirements.

4. TRAFFIC ANALYSIS

A traffic generation and distribution exercise has been undertaken to assess the potential traffic impacts associated with the proposed development. The aim of this exercise was to establish the traffic volumes which would be generated from the proposed development and to quantify the effect that the additional traffic has on the surrounding road network, specifically on the local road network including Pinjarra Road, Lakes Road, Stratford Road and Wanjeep Street. Also, the volume and functionality of traffic at the crossover locations to the west side of Lakes Road and the south side of Stratford Road was also assessed.

4.1. Trip Generation and Distribution

The average daily traffic volume generated by the site was estimated based derived traffic generation rates sourced from the Institute of Transportation Engineers' *ITE Trip Generation Manual, 10th Edition*.

The anticipated traffic generation has been adjusted for 15% reduction to account for multi-purpose trip making or 'self-containment'/synergy within the site (i.e. tenancies catering to more than one tenancy during an average trip to the site). Traffic generation has thereby been estimated to be 2,866 daily vehicle trips (1433 in/1433 out) and 220 vehicle trips (161 in/59 out) and 222 vehicle trips (109 in/113 out) during the weekday a.m. and p.m. peak hours, respectively. Based upon the existing traffic patterns in the area and the spatial distribution of adjacent land uses, the following distribution for the proposed development generated traffic has been assume broadly:

- 30% to and from the east via Pinjarra Road via signalised intersection with Lakes Road/Wanjeep Street.
- 30% to and from the west via Pinjarra Road via signalised intersection with Lakes Road/Wanjeep Street.
- 25% to and from the north via the internal reciprocal road via 372 Pinjarra Road.
- 15% to and from the north via Lakes Road.

The number of trips entering / exiting the site via the proposed site crossover has been assigned based upon the most logical route for vehicles to take given their origin / destination.

The anticipated site-generated traffic was then assigned to the respective crossovers to the boundary road network upon the existing proportions for both the weekday a.m. and p.m. peak hours. The resultant increases in weekday daily and a.m. and p.m. peak hour-generated traffic under the 'worst case' scenario for the boundary road network would be as follows:

- Pinjarra Road (East) of Lakes Road:
 - Daily: +860 vehicular trips
 - A.M. Peak Hour: +66 vehicular trips
 - P.M. Peak Hour: +67 vehicular trips
- Pinjarra Road (West) of Lakes Road:
 - Daily: +860 vehicular trips
 - A.M. Peak Hour: +66 vehicular trips
 - P.M. Peak Hour: +67 vehicular trips
- Stratford Road (North and East/West):
 - Daily: +717 vehicular trips
 - A.M. Peak Hour: +55 vehicular trips
 - P.M. Peak Hour: +56 vehicular trips
- Lakes Road (North):
 - Daily: +429 vehicular trips
 - A.M. Peak Hour: +33 vehicular trips
 - P.M. Peak Hour: +32 vehicular trips

These increases in daily and a.m./p.m. peak hour volumes will have a minimal impact on existing traffic operations in the area and can be comfortably accommodated within the practical capacities of the respective links on the boundary road network. The results of this analysis indicate that it is consistent with the original modelling results documented in the Transcore report. Upgrades to the local road network include the construction of Stratford Road and the signalisation of the Pinjarra Road/Stratford Road intersection in 2020.

4.2. Intersection and Crossover Assessment

4.2.1 SIDRA Intersection Analysis

The operational performance of the signalised intersection of Pinjarra Road/Lakes Road/Wanjeep Street to the south of the site and primary site crossover to Lakes Road and the shared crossover to Stratford Road have been assessed using the software program SIDRA Intersection for future road conditions. It has been assumed that under future road conditions

that full build-out of the property situated at 372 Pinjarra Road to the immediate west and the proposed commercial development located at 395 and 411 Pinjarra Road to the south-east of the site under a 10+ year plus horizon scenario

SIDRA is a commonly used intersection modelling tool used by traffic engineers for all types of intersections. Outputs for four standard measures of operation performance can be obtained, being Degree of Saturation (DoS), Average Delay, Queue Length, and Level of Service (LoS).

- **Degree of Saturation** is a measure of how much physical capacity is being used with reference to the full capability of the particular movement, approach, or overall intersection. A DoS of 1.0 equates to full theoretical capacity although in some instances this level is exceeded in practice. SIDRA uses maximum acceptable DoS of 0.90 for signalised intersections for its Design Life analysis. Design engineers typically set a maximum DoS threshold of 0.95 for new intersection layouts or modifications.
- **Average Delay** reports the average delay per vehicle in seconds experienced by all vehicles in a particular lane, approach, or for the intersection as a whole. For severely congested intersections the average delay begins to climb exponentially.
- **Queue Length** measures the length of approach queues. In this document we have reported queue length in terms of the length of queue at the 95th percentile (the maximum queue length that will not be exceeded for 95 percent of the time). Queue lengths provide a useful indication of the impact of signals on network performance. It also enables the traffic engineer to consider the likely impact of queues blocking back and impacting on upstream intersections and accesses.
- **Level of Service** is a combined appreciation of queuing incidence and delay time incurred, producing an alphanumeric ranking of A through F. A LoS of A indicates an excellent level of service whereby drivers delay is at a minimum and they clear the intersection at each change of signals or soon after arrival with little if any queuing. Values of B through D are acceptable in normal traffic conditions. Whilst values of E and F are typically considered undesirable, within central business district areas with significant vehicular and pedestrian numbers, corresponding delays/queues are unavoidable and hence, are generally accepted by road users.

4.2.2 Results of SIDRA Analysis

The results of the SIDRA analysis under existing and proposed weekday peak hour conditions indicate that the signalised intersection at Pinjarra Road/Lakes Road/Wanjeep Street intersection would continue to operate at acceptable levels of service during both the weekday a.m. and p.m. peak periods with the additional of site-generated traffic having a minimal impact on the operations at this location.

The proposed crossover on the west side of Lakes Road and south side of Stratford Road will operate at good Levels of Service during both the respective weekday a.m. and p.m. peak hours.

The results of the SIDRA analysis confirm the results documented in the 2014 Transcore TIA.

5. TRAFFIC ENGINEERING REVIEW AND CAR PARKING ASSESSMENT

5.1. Review of Site Layout and On-Site Circulation

A review of the proposed on-site layout indicates that it has been designed in compliance with Australian Standards *AS 2890.1: Off-Street Parking* with all vehicles entering and exiting the site in forward gear inclusive of the waste collection and loading, service, and delivery vehicles. A Loading, Service and Delivery Management Plan can be prepared and will be submitted under separate cover, if required. The site will cater to and accommodate vehicles up to and including a MRV 12.5m inclusive of both service/delivery and waste collection vehicles.

Appendix B outlines the swept path analysis for these vehicles accessing the site.

5.2. Crash History and Sightlines

A review of the crash history on the boundary road network in the vicinity of the site crossover during the 5-year reporting period 2020-2024 indicates that there have been no driveway or rear end crashes between Pinjarra Road and Stratford Road or on Stratford Road between Pingle Drive and Lakes Road along the eastern and northern frontages of the site, respectively,

This lack of crash history along the respective frontages of the site would indicate that there would be a minimal risk associated with entering and exiting the crossovers to the site.

A review of the sightlines at the proposed crossovers to both Lakes Road and Stratford Road indicate that there are sufficient sightlines in place to accommodate minimum AGSD, SISD and MGSD requirements under a design speed of 60kph with sufficient and clear sightlines to the intersection with Pinjarra Road along Lakes Road and along Stratford Road to the east and west of the main crossover along the northern boundary of the site. No conflict with the vehicles entering and exiting the 7-11 petrol station to the north-east of the site or with vehicles entering and/exiting the Mandurah Home City opposite the site is expected.

5.3. Rubbish Collection and Loading, Servicing and Delivery

Rubbish collection arrangements would be undertaken in consultation with the City of Mandurah. A separate Waste Management Plan have been prepared and submitted under separate cover.

5.4. Car Parking

The proposed car parking supply for the site is 215 on-site bays inclusive of six (6) ACROD bays plus service/delivery bays at the rear of the respective tenancies. The overall proposed on-site supply is consistent with the City of Mandurah's *Town Planning Scheme No. 3*.

A review of the proposed layout on the site indicates that it is compliant with Australian Standard *AS2890.1: Off-Street Parking* and all vehicles will enter and exit in forward gear.

6. CONCLUSIONS

Move Consultants has been commissioned to prepare a Transport Impact and Parking Assessment for a proposed for a large format retail development on Lot 502 (No. 400), Pinjarra Road, to be located within the north-west quadrant of the signalised intersection of Pinjarra Road/Lakes Road/Wanjeep Road, Greenfields in the City of Mandurah. This assessment has been prepared in accordance with the City of Mandurah's *Town Planning Scheme No. 3*, the *Mandurah East Structure Plan* and the *WAPC Transport Impact Assessment Guidelines – Vol. 4: Individual Developments*. Background information has also been distilled from the previous application lodged for the amalgamated Lot 501, Pinjarra Road Greenfields TIA prepared by Transcore in 2014, where relevant and appropriate.

The anticipated site-generated traffic associated with the proposal on a daily and a.m./p.m. peak hour basis will have a minimal impact on existing traffic operations in the area and can be comfortably accommodated within the practical capacities of the respective links on the boundary road network. The results of this analysis indicate that it is consistent with the original modelling results documented in the Transcore report. Recent upgrades to the local road network include the construction of Stratford Road and the signalised of the Pinjarra Road/Stratford Road intersection in 2020.

The results of the SIDRA analysis under existing and proposed weekday peak hour conditions and are consistent with those previously documented in the 2014 Transcore TIA report. The results indicate that the signalised intersection at Pinjarra Road/Lakes Road/Wanjeep Street intersection would continue to operate at acceptable levels of service during both the weekday a.m. and p.m. peak periods with the additional of site-generated traffic having a minimal impact on the operations at this location. The proposed crossover on the west side of Lakes Road and south side of Stratford Road will operate at good Levels of Service during both the respective weekday a.m. and p.m. peak hours.

This lack of crash history along the respective frontages of the site would indicate that there would be a minimal risk associated with entering and exiting the crossovers to the site.

A review of the sightlines at the proposed crossovers to both Lakes Road and Stratford Road indicate that there are sufficient sightlines in place to accommodate minimum AGSD, SISD and MGSD requirements under a design speed of 60kph with sufficient and clear sightlines to the intersection with Pinjarra Road along Lakes Road and along Stratford Road to the east and west of the main crossover along the northern boundary of the site. No conflict with the vehicles entering and exiting the 7-11 petrol station to the north-east of the site or with vehicles entering and exiting the Mandurah Home City opposite the site is expected.

The proposed crossover arrangements as shown on the plan and are consistent with Austroads and City of Mandurah standards.

The proposed 215 bays are more than sufficient to accommodate the demands associated with the proposal and is consistent with Council requirements. Proposed on-site bicycle parking for the site will be provided consistent with Council policy.

In conclusion, based upon the results of this transport analysis, traffic engineering review and car parking assessment, there are not anticipated to be any safety, or operational concerns associated with the proposal and the proposed on-site car parking is appropriate and are therefore supported from a traffic perspective.



7. APPENDIX A – SITE PLAN





8. APPENDIX B – SWEPT PATH ANALYSIS

