



PART B – CITY OF STIRLING

1. Declarations of Due Consideration

2. Disclosure of Interests

3. Form 1 DAP Applications

- 3.1 Lots 28 and 29 (House Number 322A and 322B) Grand Promenade,
Dianella - 13 Grouped Dwellings and 6 Ancillary Dwellings –
DAP/25/03027

4. Form 2 DAP Applications

Nil.

5. Section 31 SAT Reconsiderations

Nil.

**Part B - Item 3.1 - LOT NO. 28 AND 29 (HOUSE NUMBER 322A AND 322B)
GRAND PROMENADE DIANELLA – 13 GROUPED DWELLINGS AND SIX ANCILLARY DWELLINGS**

**Form 1 – Responsible Authority Report
(Regulation 12)**

DAP Name:	Metro Inner DAP
Local Government Area:	City of Stirling
Applicant:	PTS Town Planning Pty Ltd
Owner:	The Roman Catholic Archbishop of Perth
Value of Development:	\$8 million
Responsible Authority:	City of Stirling
Authorising Officer:	Amanda Sheers, Director Planning and Development
LG Reference:	DA25/1830
DAP File No:	DAP/25/03027
Application Received Date:	16 December 2025
Report Due Date:	29 April 2026
Application Statutory Process Timeframe:	90 Days, with an additional 49 days agreed between the Applicant and City of Stirling.
Attachment(s):	<ol style="list-style-type: none"> 1. Development Application Plans (received 18 March 2026) 2. Aerial Location Plan 3. Metropolitan Region Scheme Zoning Map 4. City of Stirling Local Planning Scheme No.3 Zoning Map 5. Applicant's Original Planning Report received 3 December 2025 6. City of Stirling Planning Assessment 7. Applicant's Final Submission: <ol style="list-style-type: none"> a. Applicant's Response to Request for Information received 18 March 2026 b. Landscaping Plans prepared by Kdla Kelsie Davies Landscape Architecture received 3 December 2025 c. Transport Impact Statement prepared by Urbii received 3 December 2025 d. Transport Noise Assessment by Lloyd George Acoustics received 3 December 2025

	<p>e. Waste Management Plan prepared by Urbii received 3 December 2025</p> <p>8. External Agency Referral Responses</p> <p>a. Water Corporation Referral received 23 March 2026</p> <p>b. Public Transport Authority of Western Australia received 23 April 2026</p>
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Responsible Authority Recommendation

That the Metro Inner Development Assessment Panel resolves to:

Approve DAP Application reference DAP/25/03027 and accompanying plans as listed in Condition 2 in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the Planning and Development (Local Planning Schemes) Regulations 2015, and the provisions of the City of Stirling Local Planning Scheme No. 3, for the proposed development for 10 Grouped Dwellings, Three Small Dwellings and Six Ancillary Dwellings on Lots 28 and 29, House Number 322A and 322B, Grand Promenade, Dianella subject to the following conditions:

Conditions

1. This decision constitutes planning approval only and is valid for a period of four (4) years from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect.
2. The development is to comply in all respects with the attached approved plans, as dated, marked and stamped, together with any requirements and annotations detailed thereon by the Development Assessment Panel. The plans approved as part of this application form part of the planning approval issued (including any modifications required by conditions of approval).

PLAN TITLE	DATE	REVISION	DRAWING No.	DRAWN BY
Feature Survey	11/3/26	E	A001	MDC Architects
Site Plan	11/3/26	E	A002	MDC Architects
Ground Floor Plan	11/3/26	E	A010	MDC Architects
First Floor Plan	11/3/26	E	A011	MDC Architects
Roof Plan	11/3/26	E	A012	MDC Architects
Individual Lot Plan	11/03/26	E	A013	MDC Architects
Individual Lot Plan	11/3/26	E	A014	MDC Architects

PLAN TITLE	DATE	REVISION	DRAWING No.	DRAWN BY
Elevations	11/3/26	E	A030	MDC Architects
Elevations	11/3/26	E	A031	MDC Architects
Elevations	11/3/26	E	A032	MDC Architects
Elevations	11/3/26	E	A033	MDC Architects
Elevations	11/3/26	E	A034	MDC Architects
Landscaping Plan	03/12/25	Rev D	N/A	Kdla Kelsie Davies Landscape Architects
Transport Impact Statement	03/12/25	U25.090.r0 1a	N/A	Urbii
Transport Noise Assessment	03/12/25	0	N/A	Lloyd George Acoustics
Waste Management Plan	03/12/25	U25.0990.r 02a	N/A	Urbii

Building Design

3. The colours, materials and finishes of the development shall be in accordance with the details and annotations as indicated on the approved plans which form part of this approval, to the satisfaction of the City of Stirling.
4. Prior to occupation of the development, the external finish of the boundary walls shall be to the same standard as the rest of the development, to the satisfaction of the City of Stirling.

Construction Management

5. Prior to the submission of a Building Permit application, a Construction Management Plan (CMP) shall be submitted to the City of Stirling for approval. The CMP shall include specific details on the management of aspects including but not limited to, dust, noise, vibration, waste management, parking, traffic, street tree protection zones, storage of materials, site safety / security and any other relevant matters to the satisfaction of the City of Stirling. The CMP is to be complied with for the duration of the construction of the development, to the satisfaction of the City of Stirling.

Landscaping

6. Prior to occupation of the development, all landscaped areas are to be planted, reticulated and mulched in accordance with the landscaping plan to be approved, including:
 - a. The planting of 25 small trees.

All landscaped areas should be maintained thereafter for the duration of the development, to the satisfaction of the City of Stirling.

Lighting

7. Prior to the submission of a Building Permit application, a Lighting Strategy detailing lighting of the building exterior, communal areas and vehicle access leg is to be provided, to the satisfaction of the City of Stirling.

Noise

8. Prior to the submission of the Building Permit application, the landowner/applicant is to provide written confirmation that all recommendations in the Transport Noise Assessment have been incorporated into the building design, with the certified Building Permit application, to the satisfaction of the City of Stirling.
9. All recommendations provided in the Transport Noise Assessment prepared by Lloyd George Acoustics received 3 December 2025, are to be implemented, to the satisfaction of the City of Stirling.
10. Prior to the commencement of use, the landowner shall provide consent to a Notification, pursuant to Section 70A of the Transfer of Lands Act 1893 being placed on the Certificate(s) of Title of Lots No.28 and 29 (No 322A and 322B) Grand Promenade, Dianella:

'This lot is in the vicinity of a transport corridor and is affected, or may in the future be affected, by road and rail transport noise. Road and rail transport noise levels may rise or fall over time depending on the type and volume of traffic.'

11. Unless otherwise agreed with the City of Stirling within three months of the commencement of the development, a Noise Report prepared by an accredited noise consultant shall be submitted to and approved in writing by the City of Stirling. The noise report confirming noise from the development complies with the *Environmental Protection (Noise) Regulations 1997*, to the satisfaction of the City of Stirling.

Parking and Vehicle Access

12. Prior to occupation of the development, a Parking Management Plan (PMP) shall be submitted to and approved by the City of Stirling. The PMP shall detail the allocation of car parking bays, signage and ongoing management of resident and visitor parking bays, including the identification of bays allocated for exclusive use. Specific allocation of car parking bays provided on site is to be as follows:
 - a. Allocation of Lots 7, 8, 9 and 10 residential permanent car parking bays; and
 - b. Five residential visitor car parking bays.

The development shall be carried out and maintained in accordance with the approved Parking Management Plan, to the satisfaction of the City of Stirling.

13. Prior to occupation of the development, all driveways, parking and manoeuvring areas shall be hard surfaced, drained and maintained to the satisfaction of the City of Stirling.
14. Prior to the occupation of the development, directional signage shall be provided at the entry to the site. The signage shall direct visitors to the visitor car parking, motorcycle, scooter and bicycle spaces. The signage shall thereafter be retained for the life of the development.
15. Prior to the occupation of the development, any redundant crossover shall be removed and the kerbing and road verge reinstated to the satisfaction of the City of Stirling.
16. The crossover shall be designed, constructed, and installed prior to occupation of the development to the satisfaction of the City of Stirling.
17. All on-site car parking bays are to be compliant with Australian/New Zealand Standard AS/NZS 2890.1 and Australian/New Zealand Standard AS/NZS 2890.6, to the satisfaction of the City of Stirling.
18. All car parking bays, manoeuvring and circulation areas are to comply with Australian Standards AS/NZS2890.1:2004 Amendment 1 and AS2890.2:2018. The number of ACROD car parking bays and their design and layout are to comply with Australian Standards AS/NZS2890.6:2009 (Off-street Parking for People with Disabilities) and the Building Code of Australia (Volume 1 section D3.5), to the satisfaction of the City of Stirling.
19. Unless otherwise approved by the City of Stirling, no walls, fences, letterboxes or other structures above 0.75 metres in height to be constructed within the 1.5 metre width x 1.5 metre depth triangular area of where:
 - a. Walls, letterboxes or fences adjoin vehicular access points to the site, or
 - b. A driveway meets a public or private street, or
 - c. Two streets intersect, or
 - d. A driveway meets a right of way.

Visual Privacy

20. Prior to occupation of the development, all privacy screening shall be visually impermeable and is to comply in all respects with the requirements of Part C of the Residential Design Codes Volume 1 (Clause 3.10 Visual Privacy), to the satisfaction of the City of Stirling.

Waste Management

21. The development is to comply with the Waste Management Plan prepared by Urbii received 3 December 2025.

General

22. Prior to the occupation or use of the development, a restrictive covenant, to the benefit of City of Stirling pursuant to Section 129BA of the *Transfer of land Act 1893* is to be placed on the certificate(s) of title of the proposed lots 4, 5 and 6 advising of the existence of a restriction on the use of the land. Notice of this restriction is to be included on the survey strata plan. The restrictive covenant is to state as follows:

"This lot is not to be developed for any purposes other than a small Dwelling as defined in the Western Australian Planning Commission's Residential Design Codes Volume 1."

23. Stormwater from all roofed and paved areas shall be collected and contained on site. Stormwater must not affect or be allowed to flow onto or into any other property or road reserve. There shall be no connection to the City's drainage infrastructure without the written consent of the City of Stirling.
24. All external fixtures, building services and utilities of the development are to be integrated into the building, landscape and/or fencing such that they are accessible for servicing requirements but not visually obtrusive, to the satisfaction of the City of Stirling.
25. All air conditioning units, plant and roof equipment and other external fixtures are to be screened from view from the surrounding streets and adjoining properties to the satisfaction of the City of Stirling.

Advice NotesGeneral

1. Where an approval has so lapsed, no development shall be carried out without the further approval of the Development Assessment Panel or City of Stirling having first been sought and obtained.
2. If an applicant is aggrieved by this determination there is a right of appeal under Part 14 of the *Planning and Development Act 2005*. An appeal must be lodged within 28 days of the determination with the State Administrative Tribunal.
3. This is a Development Approval under the City of Stirling Local Planning Scheme and related policies. It is not a Building Permit or an approval to commence or carry out development under any other law. It is the responsibility of the applicant to obtain any other necessary approvals, consents and licences required under any other law, and to commence and carry out development in accordance with all relevant laws.
4. This approval is not an authority to ignore any constraint to development on the land, which may exist through statute, regulation, contract or on title, such as an easement or restrictive covenant. It is the responsibility of the applicant and not the Development Assessment Panel or City of Stirling to investigate any such constraints before commencing development. This approval will not necessarily

have regard to any such constraint to development, regardless of whether or not it has been drawn to the Development Assessment Panel or City of Stirling's attention.

5. The development is to be connected to the sewer.
6. Noisy Construction Work outside the period 7.00am to 7.00pm Monday to Saturday and at any time on Sundays and Public Holidays is not permitted unless a Noise Management Plan for the construction site has been approved in writing by the City of Stirling.
7. The applicant is responsible for ensuring that all lot boundaries as shown on the approved plans are correct.
8. Development is to comply in all respects with the attached approved plans which have been stamped accordingly.
9. Where, in the opinion of the City of Stirling, achieving compliance with the conditions of approval requires significant modifications to the approved plans, a Form 2 application will be required for consideration in accordance with Clause 17 or 17A of the *Planning and Development (Development Assessment Panels) Regulations 2011*.
10. All construction works to comply with the requirements of the *Environmental Protection Act 1986* and the *Environmental Protection (Noise) Regulations 1997*. Noisy construction work outside the period 7:00am to 7:00pm Monday to Saturday and at any time on Sundays and Public Holidays is not permitted unless an approved Noise Management Plan for the construction site has been issued.

Landscaping

11. In reference to Condition 6, the small trees indicated on the approved plans for retention must be retained on site with a minimum nine square metres of deep soil area with a minimum dimension of 1.5 metres at ground level free of intrusions.
12. All street trees adjoining the subject land that are not conditioned for removal are to be retained and protected throughout the duration of the development. Should these trees die, decline, be damaged or are removed, charges will apply as per the City of Stirling's Street and Reserve Tree Policy. Part of or all of the Verge Bond may be retained to cover the associated costs and further penalties and/or legal action may occur. It is the responsibility of the applicant to ensure that the site survey plan is correct as any inaccuracy of the plans will not be considered justification for removal of the trees in the event that their positions are incorrectly shown. The removal of street trees without the written approval of the City is an offence under the City of Stirling Public Places and Thoroughfare Law 2009.
13. The proposed crossover configuration is subject to the approval of the City of Stirling's Verge Control team. A "Crossover Installation Application" is required to be submitted and approved prior to the commencement of the crossover installation.

Lighting

14. Any outside lighting is to comply with Australian Standard AS 4282-2023 - Control of the Obtrusive Effects of Outdoor Lighting (as amended) for the control of obtrusive effects of outdoor lighting and must not spill into any adjacent premises. Details of any outside lighting are to be submitted at the Building Permit application stage, demonstrating compliance with Australian Standard AS 4282-2023 (as amended).

Noise

15. All noise generated by plant and equipment including air conditioners shall comply with the *Environmental (Noise) Regulations 1997*.

Waste Management

16. The bin enclosure is required to comply with the requirements of the City of Stirling's Waste Management Local Law 2010.

Details: outline of development application

Region Scheme	Metropolitan Region Scheme (MRS)
Region Scheme - Zone/Reserve	Urban
Local Planning Scheme	City of Stirling Local Planning Scheme No.3 (LPS3)
Local Planning Scheme - Zone/Reserve	Residential
Structure Plan/Precinct Plan	No
Structure Plan/Precinct Plan - Land Use Designation	No
Use Class and permissibility:	Grouped Dwelling – 'P' (permitted) use
Lot Size:	4,970m ²
Existing Land Use:	Vacant
State Heritage Register	No
Local Heritage	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Heritage List <input type="checkbox"/> Heritage Area
Design Review	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other
Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

The key components of the proposed development are summarised as follows:

- The construction of 10 single and two-storey Grouped Dwellings, three single storey Small Dwellings and six single storey Ancillary Dwellings.
- Associated car parking and amenities with high quality landscaping including the retention of two mature trees.
- Vehicle access from Grand Promenade and Chester Avenue.
- Eight residential occupant and six visitor car parking bays.

Proposed Land Use	Grouped Dwellings
Proposed No. Storeys	Two Storeys
Proposed No. Dwellings	13 Grouped Dwellings and six ancillary dwellings

Background

The subject site consists of two adjoining vacant lots with a combined site area of 4,970m². The site has two street frontages to Grand Promenade to the southwest and Chester Avenue to the northeast. A Public Transport Authority (PTA) bus stop directly adjoins the Grand Promenade frontage of the subject site but is not impacted by the proposal.

Vehicle access is provided from Grand Promenade and Chester Avenue. Under the City's Functional Road Hierarchy, Grand Promenade is classified as a District Distributor A road and Chester Avenue is classified as a Local Road.

The surrounding locality predominantly consists of one and two-storey Grouped Dwellings and Multiple Dwellings and is residential in nature at a density of R30.



Figure 1 Aerial Image with Subject Site Highlighted in blue

Legislation and Policy:

The following legislation is applicable to the proposed development.

Legislation

- Planning and Development Act 2005
- Planning and Development (Development Assessment Panels) Regulations 2011
- Planning and Development (Local Planning Schemes) Regulations 2015
- Metropolitan Region Scheme (MRS)
- City of Stirling Local Planning Scheme No. 3 (LPS3)

State Planning Policies

- State Planning Policy 5.4 – Road and Rail Noise (SPP 5.4)
- State Planning Policy 7.0 – Design of the Built Environment (SPP7.0)

Planning Codes

- Residential Design Codes Volume 1 – Part C (R-Codes)

Local Planning Policies

- Local Planning Policy 6.7 – Parking and Access (LPP6.7)
- Local Planning Policy 6.18 – Public Consultation (LPP6.18)

Draft Planning Instrument

- Draft City of Stirling Local Planning Scheme No. 4 (LPS4)

Consultation:

Consultation with Government/Service Agencies

The proposed development was referred to the following:

- Water Corporation; and
- Public Transport Authority of Western Australia

In principle, no objections to the development proposal were received by the City, with the relevant responses incorporated into the recommended conditions of approval and advice notes.

Public Consultation

Having regard to the City's Local Planning Policy 6.18 - Public Consultation (LPP 6.18), the proposed development is considered a standard application. The City's Local Planning Scheme No.3 (LPS3) and the Residential Design Codes Volume 1 (R-Codes) allow for advertising to be waived in these circumstances where it is satisfied that in its opinion the proposed development will not result in a significant impact on the amenity of the adjoining property or the street. The City's assessment concluded the development satisfied the planning framework, and therefore public consultation has not been undertaken.

Planning Assessment:

The development has been assessed against the relevant planning framework including LPS3, State Planning Policies and Local Planning Policies. The following matters have been identified as the relevant key considerations in the assessment of this application:

1. Land Use
2. Housing Typology
3. Built Form
4. Parking
5. Vehicular Access
6. Waste and Services Access
7. City of Stirling Design Review Panel

The City of Stirling Planning Assessment contained in Attachment 6 evaluates the proposal against the key considerations of the relevant planning framework, including reference to where discretion is sought.

1. Land Use

The table below summarises the permissibility of the proposed land uses, in accordance with *Table 1 – Zoning Table* in the *Local Planning Scheme No.3*.

Land Use Permissibility	
Land Use	
Grouped Dwelling	'P' Use Grouped Dwelling is permitted by the Scheme within the Residential zone providing the use complies with the relevant development standards and the requirements of the Scheme.

The proposed development is consistent with the objectives of the Residential zone, the key aim of which is to provide for residential development at a range of densities with a variety of housing type and size, to meet the current and future needs of the community.

The proposal incorporates a range of housing typologies and well-designed communal open space, providing improved amenity, safety and high-quality landscaping across both private and communal areas. Further the proposed dwellings will cater to current and future needs of the community and demand for housing. Accordingly, the City considers the proposed land use appropriate and addresses the aim of the City's LPS3.

2. Housing Typology

The development proposes a mix of grouped dwellings, small dwellings, and ancillary dwellings. This provides a diversity of housing typologies within the site while maintaining a clear and logical hierarchy between the primary and subordinate dwellings.

In accordance with Part D of the R-Codes, the site area requirements specify the following area:

Housing Typologies Proposed	
Grouped Dwelling	
Provision	Proposed
Grouped Dwelling - <i>a dwelling that is one of a group of two or more dwellings on the same lot such that no dwelling is placed wholly or partly vertically above or below another, except where special conditions of landscape or topography dictate otherwise, and includes a dwelling in a strata titles scheme with common property.</i>	<ul style="list-style-type: none"> • Lots 1 - 3 and Lots 7-13 • Meets the R-Codes definition of Grouped Dwelling.

<p>Part D of the R-Codes – Site Area</p> <ul style="list-style-type: none"> • <i>C1.1.1 Development which complies with the dwelling type and site area requirements set out in Table D</i> <p>R30:</p> <ul style="list-style-type: none"> • Minimum 260m² • Average 300m 	<ul style="list-style-type: none"> • Proposed minimum 260m². Complies. • Average of 382.3m². Complies.
Small Dwelling	
Provision	Proposed
<p>Small Dwelling - a single house or grouped dwelling with an internal floor area no greater than 70m².</p> <p>Part D of the R-Codes – Site Area Concessions</p> <ul style="list-style-type: none"> • <i>C1.1.6 In areas coded R30 to R40; for an accessible dwelling designed to gold level universal design (in accordance with Part B, 5.5.4 C4 or Part C, C2.7.2), or a small dwelling (in accordance with Part B, 5.5.5 C5 or Part C, C2.9.1) that is the subject of a development proposal, the minimum and average site area of Table D may be reduced by up to 35 per cent.</i> • Minimum area 260m² less of 35% concession permitted: 169m² 	<ul style="list-style-type: none"> • Lots 4 – 6. • Meets the R-Codes definition of Small Dwelling. • Proposed 170m². Complies
Ancillary Dwelling	
Provision	Proposed
<p>Ancillary Dwelling - self-contained dwelling on the same site as a dwelling which may be attached to, integrated with or detached from the dwelling.</p>	<ul style="list-style-type: none"> • Lots 1 to 3 and 11 to 13. • Meets the R-Codes definition of Ancillary Dwelling.

The 10 Grouped Dwellings represent the primary residential form across the site and are not seeking any site area concessions. These dwellings achieve compliance with the applicable minimum and average site area requirements and are designed as self-contained dwellings with appropriate levels of private open space, parking and internal amenity.

The ancillary dwellings are incidental to the grouped dwellings. The ancillary dwellings have been designed to satisfy the relevant requirements of R-Codes, including those relating to scale, location and relationship to the primary dwelling. The ancillary dwellings are not considered to result in any increase in density beyond that afforded under the R-Codes and contemplated by the zoning and coding of the site. The design ensures the ancillary dwellings remain clearly incidental to the primary grouped dwellings through their reduced size, integrated built form and shared pedestrian access arrangements. This approach is consistent with the intent of the ancillary dwelling provisions, which seek to provide flexible, subordinate accommodation without undermining the overall residential form or amenity of the development. The City considers the proposed ancillary dwellings satisfy the requirements of the planning framework.

The proposed three Small Dwellings (Lots 4 to 6) are subject to the 35% small dwelling concession in accordance with Part D of the R-Codes in terms of site area. The total site area for these dwellings includes the allocation of associated parking bays for their exclusive private use, which contribute to meeting the minimum site area requirement. This matter is discussed further later in the report.

The proposed mix of housing typologies allows for the provision of quality communal open space that, while not a requirement for grouped dwellings, the development has been deliberately designed to incorporate generous shared areas centrally located within the site. This design is accessible, provides surveillance of communal areas enhances amenity, safety as well as opportunities for social interaction, and supports positive living outcomes for future residents. The proposed configuration and housing typology mix are supported and considered to represent an appropriate and well resolved planning outcome.

3. Built Form

The table below outlines the key provisions of the relevant planning framework, relating to built form.

Provision	Proposed Building Height
Maximum two storeys permitted	One and two storeys proposed. Complies
Street Setbacks	
Minimum 3m required	Proposed 4.4m from Grand Promenade. Complies Proposed 4.5m from Chester Avenue. Complies

The City has reviewed the proposed built form and is satisfied that it is compliant and acceptable.

4. Parking

The table below provides a summary of the City's car parking assessment against table 2.3a of the R-Codes.

Table 2.3a – Car Parking			
Parking Type	Acceptable Outcomes Rate	Proposed Dwellings	Bays Required / Permitted
Ancillary Dwelling	Minimum 0 bays per dwelling Maximum 1 bay per ancillary dwelling	6	Minimum 0 required Maximum 6 permitted
1 Bedroom Dwellings	Minimum 0 bays per dwelling Maximum 1 bay per dwelling	2	Minimum 0 required Maximum 2 permitted
2+ Bedroom Dwellings	Minimum 0 bay per dwelling Maximum 2 bay per dwelling	11	Minimum 0 required Maximum 22 permitted
Visitors	13 or more dwellings 3, plus 1 additional space per four dwellings or part thereof	19	Minimum 5 bays required
Total Required (Minimum):			5 visitor bays
Total Permitted (Maximum):			30 resident bays
Total Provided:			14 bays total <ul style="list-style-type: none"> - 6 visitor bays - 8 resident bays <ul style="list-style-type: none"> o 5 exclusive bays o 3 unallocated resident parking

The parking arrangement has been structured to support both compliance with the applicable requirements and the functional needs of the development. In this regard, four occupant parking bays are exclusively allocated to Lots 7 to 10 and are permanently assigned for the private use of the relevant dwellings. These bays contribute an additional 13m² toward the site area of those lots, enabling compliance with the minimum site area requirement under Part D of the R-Codes. This exclusive allocation is supported under the *Strata Titles Act 1985*. The remaining occupant parking bays are available for shared resident use across the development, providing flexibility while maintaining adequate parking provision. Visitor parking bays are provided separately and remain available exclusively for visitor use.

To support the effective operation of this arrangement, a condition is recommended requiring a Parking Management Plan (PMP) be submitted. The PMP will detail the allocation, operation and ongoing management of occupant and visitor parking bays, ensuring the intended arrangement is clearly established and maintained over the life of the development. In addition, a condition has been recommended requiring the permanent signposting and marking of all parking bays, including the identification of visitor and resident bays, to ensure clarity, legibility and ongoing compliance with the approved parking arrangement.

5. Vehicle Access

The subject site has dual frontage to Grand Promenade and extends to Chester Avenue. Vehicle access to the site is proposed via existing crossover on Grand Promenade and Chester Avenue.

A Transport Impact Statement (TIS) prepared by Urbii has been submitted by the applicant and the following considerations have been detailed:

- The proposed vehicle access arrangements and driveway widths are considered to sufficiently accommodate the low traffic volumes generated by the development.
- The dimensions of car parking aisles and bays are compliant with Australian Standards AS2890.1. On-site bays are 2.4m wide by 5.4m long and an aisle width of 5.8m has been provided.
- The proposed development is estimated to generate a total of 95 vehicles per day with 10 vehicles per hour generated during the AM and PM peak hours. The TIS notes in its reference to the Western Australian Planning Commission Transport Impact Assessment Guidelines for Development that an increase in traffic of less than 10 percent of capacity could not normally be likely to have a material impact on any particular section of road. The proposed development will not increase traffic flows on any roads adjacent to the site by the quoted WAPC threshold of +100vph to warrant further analysis.

The City considers the impact on the surrounding road network is acceptable.

Passing Bay

To allow for adequate vehicle passing, the crossover on Grand Promenade should at least be 5.5m wide to allow for vehicles to enter and exit simultaneously. Further, the crossover is required to also be setback 1.15m from the eastern lot boundary to provide sufficient sightlines due to an existing masonry fence that obstructs sightlines for vehicles exiting the property, noting the adjacent footpath.



Figure 2 Grand Promenade frontage demonstrating 1.8m high fence

To address this requirement, amended plans and further information were provided by the Applicant to provide a side setback of 1.15 metres to the eastern side boundary from the Grand Promenade crossover. With regards to the width of the crossover proposed at 5.31m, the traffic consultant has demonstrated that a reduced crossover width of 5.31m is acceptable in this instance. The assessment has had regard to Australian Standards AS/NZS 2890.1, which specifies that for a Class 1A parking facility serving fewer than 25 parking spaces on an arterial road, an Access Facility Category B driveway applies, with a corresponding combined driveway width range of 3.0m to 5.5m. The City considers the proposed crossover arrangement to be acceptable.

6. Waste and Services Access

Waste collection will be facilitated by the City's Waste Collection Service. The submitted Waste Management Plan (WMP) has been reviewed and considered appropriate. A rear loader waste truck will be used to collect waste from the site car park on scheduled waste collection days.

The submitted TIA confirms that Chester Avenue is a local road carrying low traffic volumes and that no issues are anticipated with waste collection vehicles reversing into the site. The arrangement has been reviewed by the City and is considered acceptable, and a condition is recommended in relation to the submitted WMP.

7. City of Stirling Design Review Panel (DRP)

The City's Design Review Panel (DRP) provides design advice that is supplementary to the City's planning assessment. The proposed development was reviewed by the City's DRP on 9 October 2025. The revised development plans received on 18 March 2025, together with the applicant's response, address most of the matters raised by the DRP. As such, further consideration of the proposal by the DRP was not considered necessary.

Conclusion

The proposed Grouped Dwelling development at 322A and 322B Grand Promenade is considered to represent an appropriate planning outcome, providing a mix of grouped dwellings, small dwellings and ancillary dwellings. The land use is consistent within the Residential zone and aligns with the relevant objectives. The proposal provides a diversity of housing types to meet the needs of the community while maintaining residential amenity. The grouped dwellings comply with the applicable development standards and are supported subject to appropriate conditions. The ancillary dwellings are clearly incidental to the grouped dwellings, comply with the applicable provisions of the R-Codes, and remain subordinate in scale, form and function, ensuring no unintended increase in density or amenity impacts.

The development has been deliberately designed to incorporate centrally located communal open space, which enhances amenity, safety and opportunities for social interaction for future residents. Car parking, vehicle access and waste servicing arrangements have been demonstrated as compliant and functional, with appropriate conditions recommended to ensure the ongoing management, clarity and operation of parking and waste collection.

On balance, the proposed development responds positively to its context, achieves compliance with the relevant planning controls, and provides a quality residential outcome. Accordingly, the application is supported and recommended for approval subject to appropriate conditions.

Attachment 1 - Development Application Plans



322 GRAND PROMENADE, DIANELLA -
ISSUE FOR PLANNING - DA UPDATES

DRAWING REGISTER	
Sheet Name	Sheet Number
OVERALL EXISTING FEATURE SURVEY	A001
OVERALL SITE PLAN	A002
OVERALL GROUND FLOOR PLAN	A010
OVERALL FIRST FLOOR PLAN	A011
INDIVIDUAL LOT PLANS	A013
INDIVIDUAL LOT PLANS	A014
ELEVATIONS	A030
STREET ELEVATIONS	A031
STREET ELEVATIONS	A032
COURTYARD ELEVATIONS	A033
COURTYARD ELEVATIONS	A034

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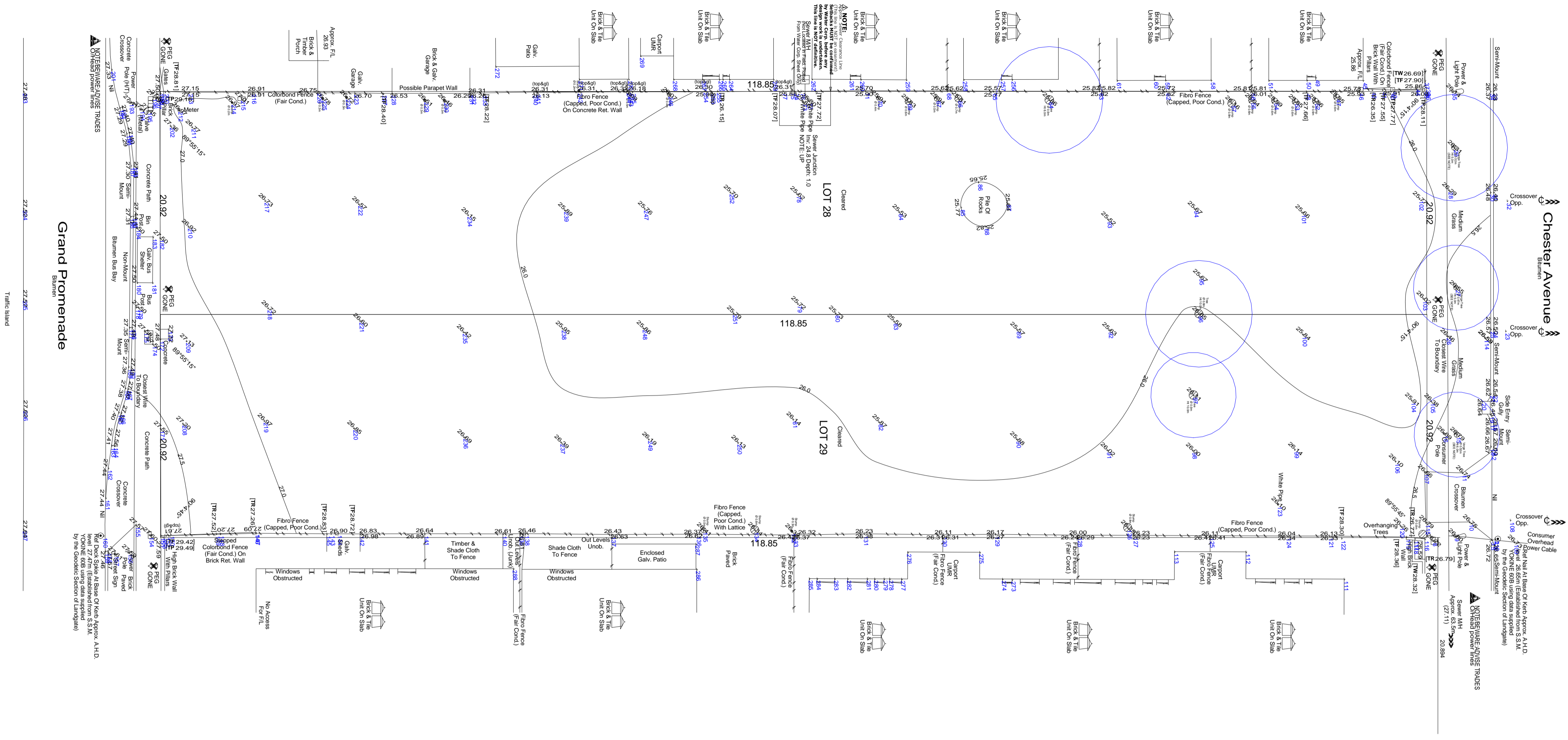
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322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING

OVERALL COVER SHEET

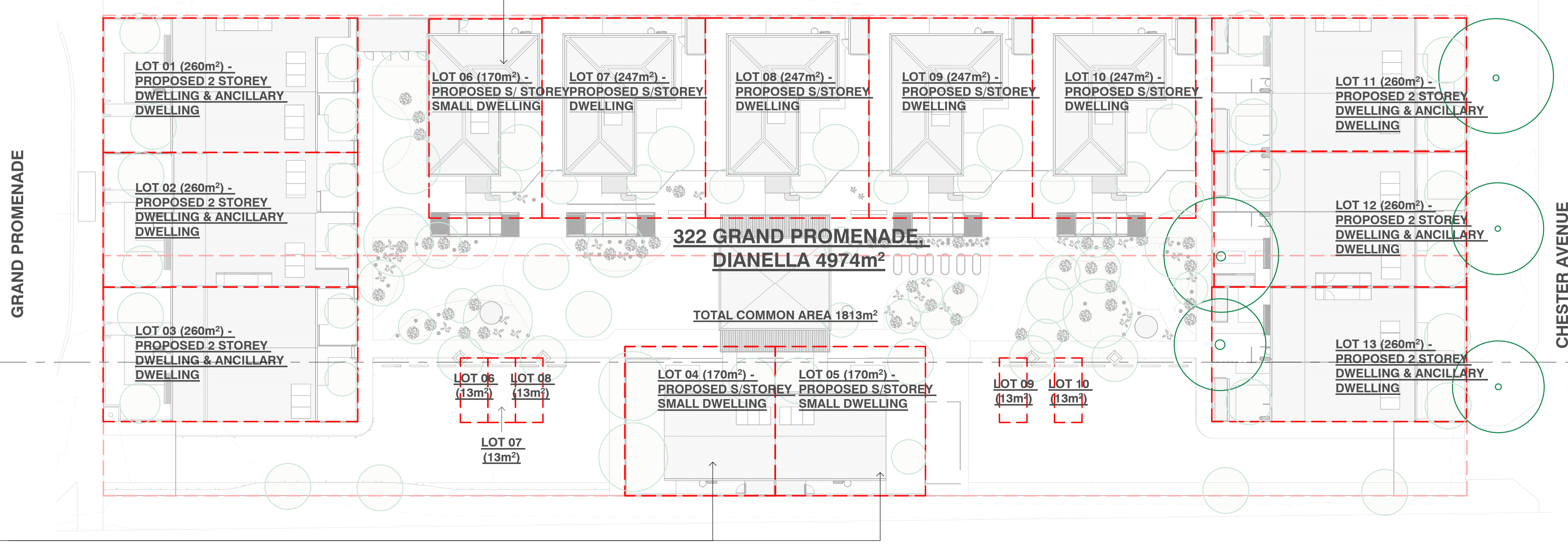
CLIENT: CATHOLIC ARCHDIOCESE OF PERTH	SCALE AT A1:
STATUS: IFP-DA	DRAWING NO: A000



EXISTING FEATURE SURVEY
SCALE: 1 : 250

AS PER R-CODES, SMALL DWELLING
MINIMUM SITE AREA MAY BE REDUCED
BY UP TO 35%.

MIN. SITE AREA FOR R30 = 260m²
MIN. SITE AREA FOR R30 SMALL
DWELLING = 169m²



MIN. SITE AREA FOR R30 = 260m²
5% REDUCTION = 247m²

LOT 04 (SMALL DWELLING) : 170m² + 13m² = 183m²
LOT 05 (SMALL DWELLING) : 170m² + 13m² = 183m²
LOT 06 (SMALL DWELLING) : 170m² + 13m² = 183m²
LOT 07 : 247m² + 13m² = 260m²
LOT 08 : 247m² + 13m² = 260m²
LOT 09 : 247m² + 13m² = 260m²
LOT 10 : 247m² + 13m² = 260m²

AS PER R-CODES, SMALL DWELLING
MINIMUM SITE AREA MAY BE
REDUCED BY UP TO 35%.

MIN. SITE AREA FOR R30 = 260m²
MIN. SITE AREA FOR R30 SMALL
DWELLING = 169m²

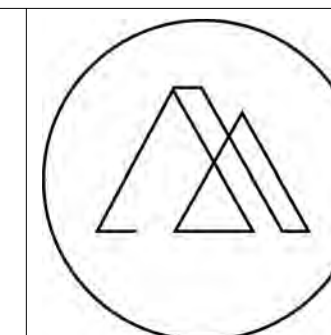
OVERALL LOT AREAS
SCALE: 1 : 250

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**322 GRAND PROMENADE,
DIANELLA (4955m²),
R30**



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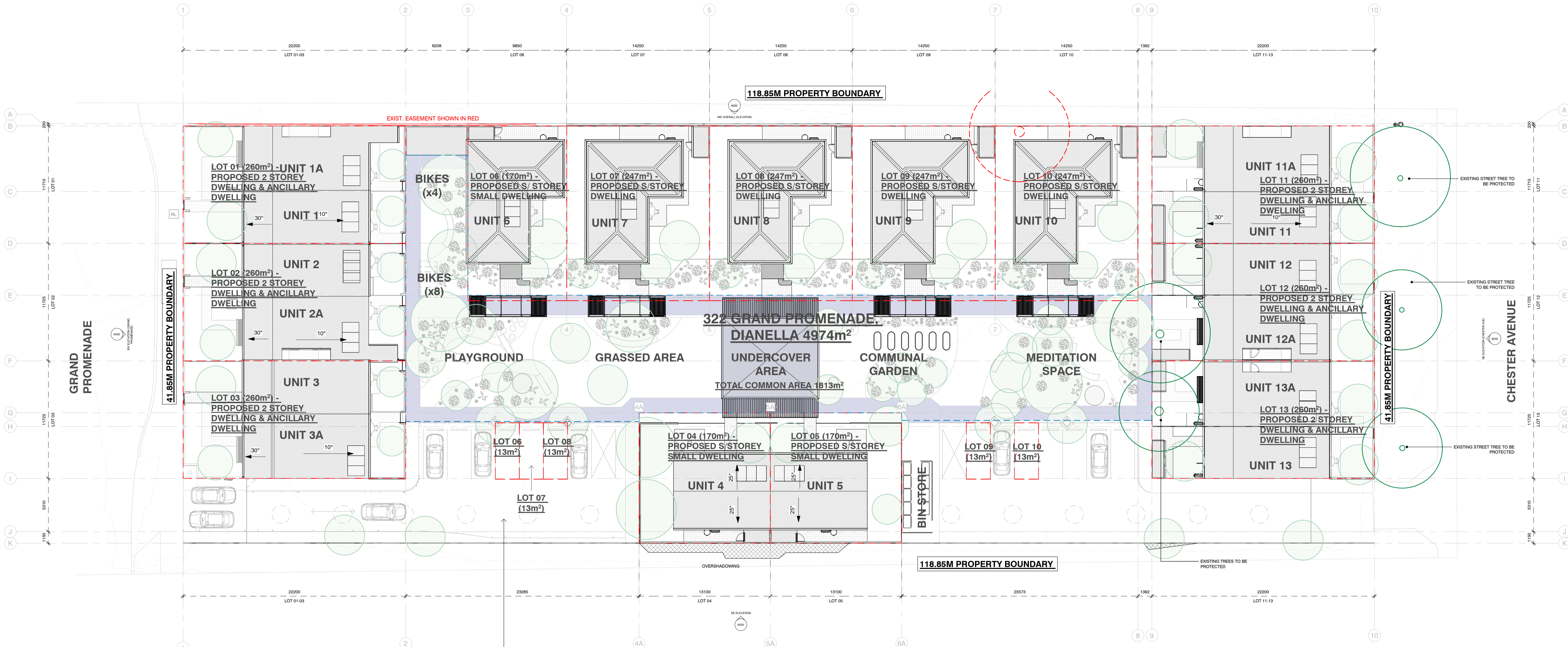
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322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING

OVERALL EXISTING FEATURE SURVEY

CLIENT	CATHOLIC ARCHDIOCESE OF PERTH	SCALE AT A1:	1 : 250
STATUS:	IFP-DA	DRAWING NO:	A001



SITE COVER
 LOT AREA = 4974m²
 MAX SITE COVER R30 = 60%
 TOTAL DEVELOPMENT AREA = 1546m² OR 31%

LOT AREA = 4974m²
 COMMUNAL OPEN SPACE AREA SHOWN AS BLUE DASHED
 MIN. COMMUNAL OPEN SPACE REQ = 114m²
 COMMUNAL OPEN SPACE ACHIEVED = 979m²

ACCESSIBLE/HARD LANDSCAPE AREA SHOWN AS PURPLE INFILL
 MIN. ACCESSIBLE/HARD LANDSCAPE AREA REQ = 38m²
 ACCESSIBLE/HARD LANDSCAPE AREA ACHIEVED = 400m²

MAX. COVERED ROOF AREA = 25% OF EACH COMMUNAL OPEN SPACE
 PERMANENT ROOF COVER PROPOSED = 80m²

**322 GRAND PROMENADE,
 DIANELLA (4955m²),
 R30**

PARKING - CAR

USERS	CAR BAYS REQUIRED (LOC A)	CAR BAYS PROVIDED
RESIDENTS	0	5 UNALLOCATED
		3 ALLOCATED
VISITORS	5	6
TOTAL	5	14

PARKING - OTHER

USERS	BICYCLE PARKING REQ.	BICYCLE PARKING PROVIDED
RESIDENTS	0	12

USERS	MC PARKING REQ.	MC PARKING PROVIDED
RESIDENTS	0	0

STORMWATER CALCS

	AREA (sqm)
ROOF AREA	1630sqm
PAVED AREA	700sqm
TOTAL	2330sqm
CoS SOAKWELL REQ = AREA x 0.015	35.7m ³
1800 x 1200 SOAKWELL CAPACITY (APPROX)	3m ³

12 REQ.

OVERALL SITE PLAN
 SCALE: 1 : 200

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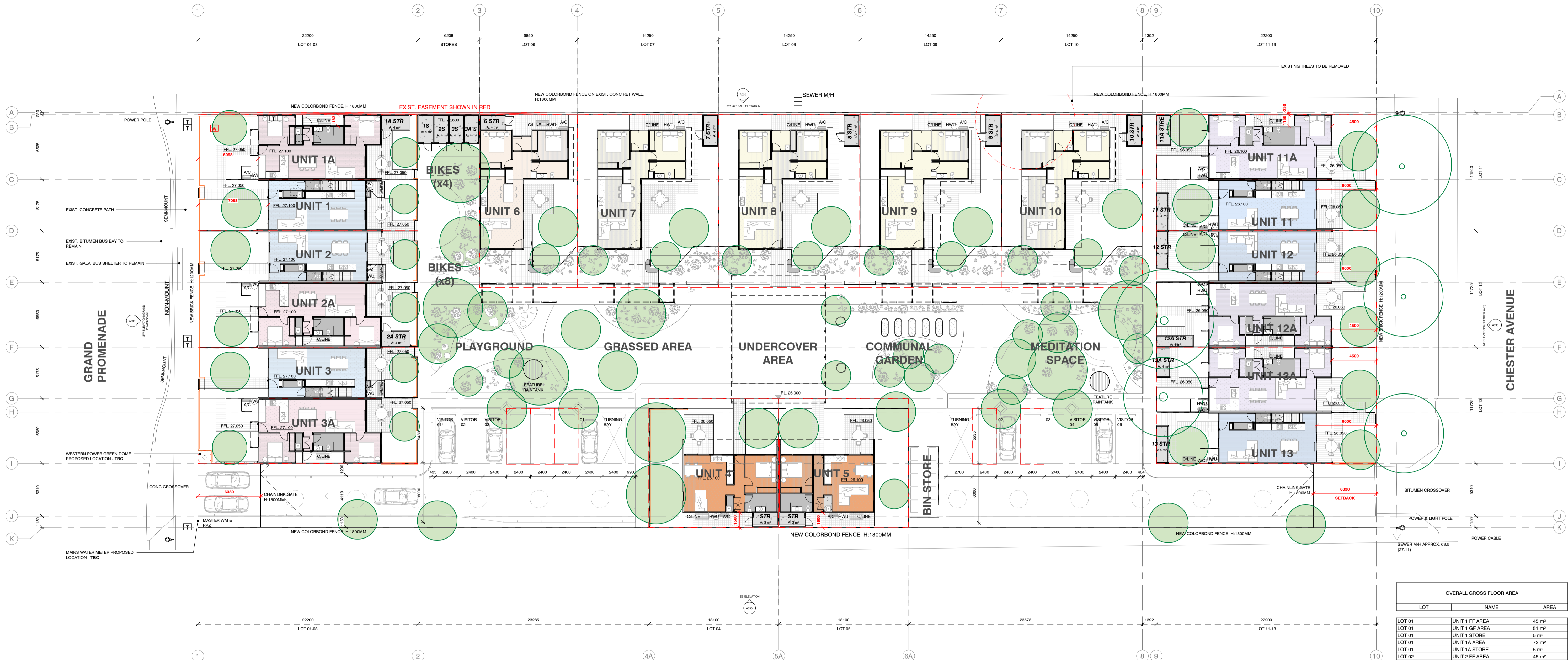
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322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING

OVERALL SITE PLAN

CLIENT	CATHOLIC ARCHDIOCESE OF PERTH	SCALE AT A1:	1 : 200
STATUS:	IFP-DA	DRAWING NO:	A002



AMENDED PLAN City of Stirling
18 Mar 2026
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OVERALL GROSS FLOOR AREA		
LOT	NAME	AREA
LOT 01	UNIT 1 FF AREA	45 m ²
LOT 01	UNIT 1 GF AREA	51 m ²
LOT 01	UNIT 1 STORE	5 m ²
LOT 01	UNIT 1A AREA	72 m ²
LOT 01	UNIT 1A STORE	5 m ²
LOT 02	UNIT 2 FF AREA	45 m ²
LOT 02	UNIT 2 GF AREA	51 m ²
LOT 02	UNIT 2 STORE	5 m ²
LOT 02	UNIT 2A AREA	72 m ²
LOT 02	UNIT 2A STORE	5 m ²
LOT 03	UNIT 3 FF AREA	44 m ²
LOT 03	UNIT 3 GF AREA	51 m ²
LOT 03	UNIT 3 STORE	5 m ²
LOT 03	UNIT 3A AREA	72 m ²
LOT 03	UNIT 3A STORE	5 m ²
LOT 04	UNIT 4 AREA	57 m ²
LOT 04	UNIT 4 STORE	4 m ²
LOT 05	UNIT 5 AREA	57 m ²
LOT 05	UNIT 5 STORE	4 m ²
LOT 06	UNIT 6 AREA	75 m ²
LOT 06	UNIT 6 STORE	4 m ²
LOT 07	UNIT 7 AREA	76 m ²
LOT 07	UNIT 7 STORE	5 m ²
LOT 08	UNIT 8 AREA	76 m ²
LOT 08	UNIT 8 STORE	5 m ²
LOT 09	UNIT 9 AREA	76 m ²
LOT 09	UNIT 9 STORE	5 m ²
LOT 10	UNIT 10 AREA	76 m ²
LOT 10	UNIT 10 STORE	5 m ²
LOT 11	UNIT 11 FF AREA	45 m ²
LOT 11	UNIT 11 GF AREA	51 m ²
LOT 11	UNIT 11 STORE	4 m ²
LOT 11	UNIT 11A AREA	72 m ²
LOT 11	UNIT 11A STORE	4 m ²
LOT 12	UNIT 12 FF AREA	45 m ²
LOT 12	UNIT 12 GF AREA	51 m ²
LOT 12	UNIT 12 STORE	4 m ²
LOT 12	UNIT 12A AREA	72 m ²
LOT 12	UNIT 12A STORE	4 m ²
LOT 13	UNIT 13 FF AREA	45 m ²
LOT 13	UNIT 13 GF AREA	51 m ²
LOT 13	UNIT 13 STORE	4 m ²
LOT 13	UNIT 13A AREA	72 m ²
LOT 13	UNIT 13A STORE	4 m ²

TYPOLOGIES LEGEND
SCALE: 1:200

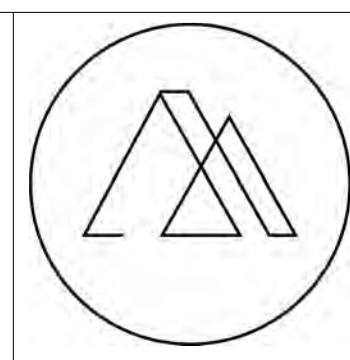
- TYPE 1 (2x1)
- TYPE 1A (2x1)
- TYPE 2 (2x1.5)
- TYPE 2A (2x1.5)
- TYPE 3 (2x1)
- TYPE 3A (2x1)
- TYPE 4 (1x1)

CITY OF STIRLING
ZONE: R30

OVERALL GROUND FLOOR PLAN
SCALE: 1:200

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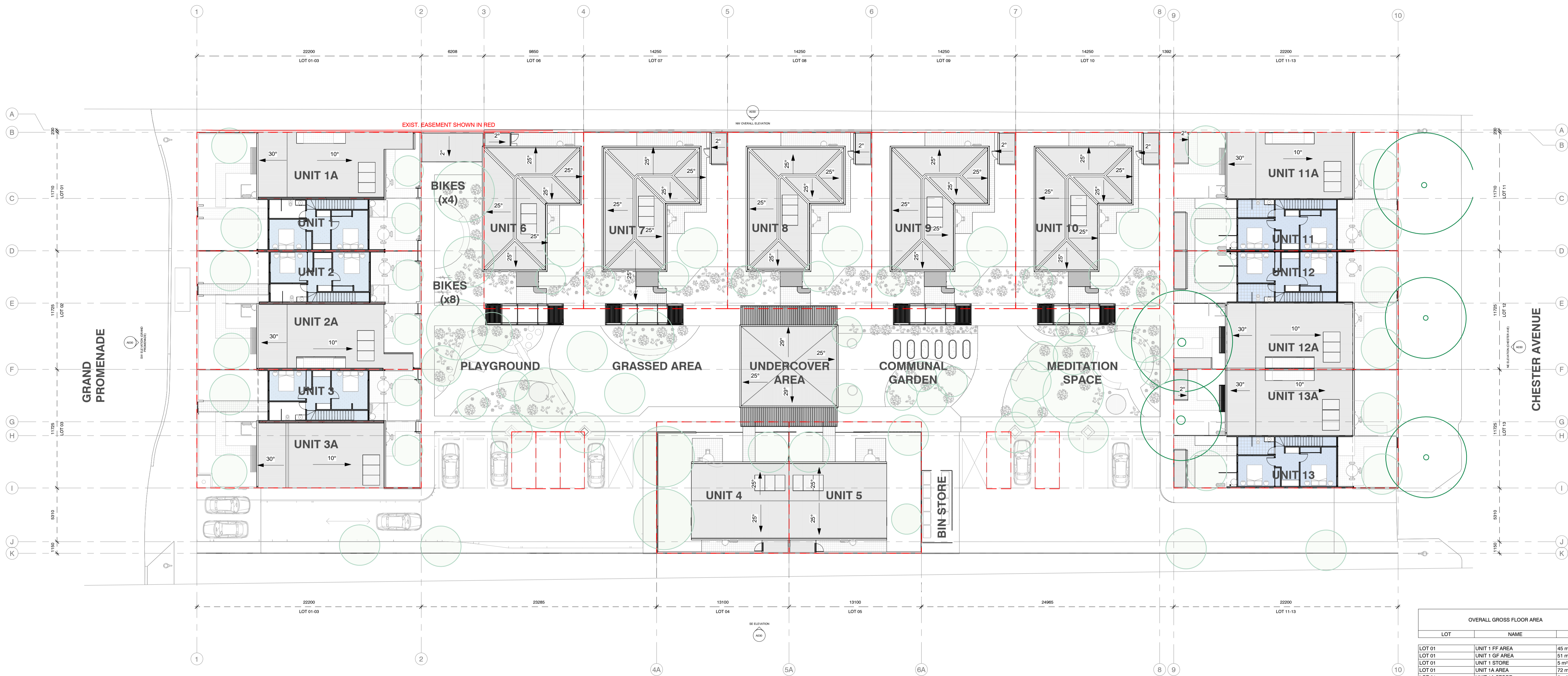
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322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING

OVERALL GROUND FLOOR PLAN

CLIENT	SCALE AT A1:
CATHOLIC ARCHDIOCESE OF PERTH	1:200
IFP-DA	A010



AMENDED PLAN

City of Stirling
18 Mar 2026
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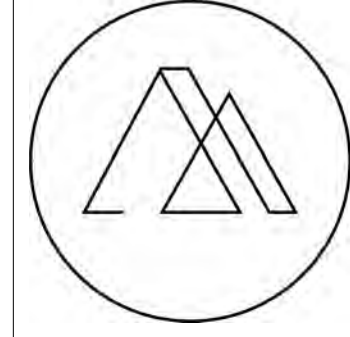
OVERALL GROSS FLOOR AREA		
LOT	NAME	AREA
LOT 01	UNIT 1 FF AREA	45 m ²
LOT 01	UNIT 1 GF AREA	51 m ²
LOT 01	UNIT 1 STORE	5 m ²
LOT 01	UNIT 1A AREA	72 m ²
LOT 01	UNIT 1A STORE	5 m ²
LOT 02	UNIT 2 FF AREA	45 m ²
LOT 02	UNIT 2 GF AREA	51 m ²
LOT 02	UNIT 2 STORE	5 m ²
LOT 02	UNIT 2A AREA	72 m ²
LOT 02	UNIT 2A STORE	5 m ²
LOT 03	UNIT 3 FF AREA	44 m ²
LOT 03	UNIT 3 GF AREA	51 m ²
LOT 03	UNIT 3 STORE	5 m ²
LOT 03	UNIT 3A AREA	72 m ²
LOT 03	UNIT 3A STORE	5 m ²
LOT 04	UNIT 4 AREA	57 m ²
LOT 04	UNIT 4 STORE	4 m ²
LOT 05	UNIT 5 AREA	57 m ²
LOT 05	UNIT 5 STORE	4 m ²
LOT 06	UNIT 6 AREA	75 m ²
LOT 06	UNIT 6 STORE	4 m ²
LOT 07	UNIT 7 AREA	76 m ²
LOT 07	UNIT 7 STORE	5 m ²
LOT 08	UNIT 8 AREA	76 m ²
LOT 08	UNIT 8 STORE	5 m ²
LOT 09	UNIT 9 AREA	76 m ²
LOT 09	UNIT 9 STORE	5 m ²
LOT 10	UNIT 10 AREA	76 m ²
LOT 10	UNIT 10 STORE	5 m ²
LOT 11	UNIT 11 FF AREA	45 m ²
LOT 11	UNIT 11 GF AREA	51 m ²
LOT 11	UNIT 11 STORE	4 m ²
LOT 11	UNIT 11A AREA	72 m ²
LOT 11	UNIT 11A STORE	4 m ²
LOT 12	UNIT 12 FF AREA	45 m ²
LOT 12	UNIT 12 GF AREA	51 m ²
LOT 12	UNIT 12 STORE	4 m ²
LOT 12	UNIT 12A AREA	72 m ²
LOT 12	UNIT 12A STORE	4 m ²
LOT 13	UNIT 13 FF AREA	45 m ²
LOT 13	UNIT 13 GF AREA	51 m ²
LOT 13	UNIT 13 STORE	4 m ²
LOT 13	UNIT 13A AREA	72 m ²
LOT 13	UNIT 13A STORE	4 m ²

OVERALL FIRST FLOOR PLAN
SCALE: 1 : 200

CITY OF STIRLING
ZONE: R30

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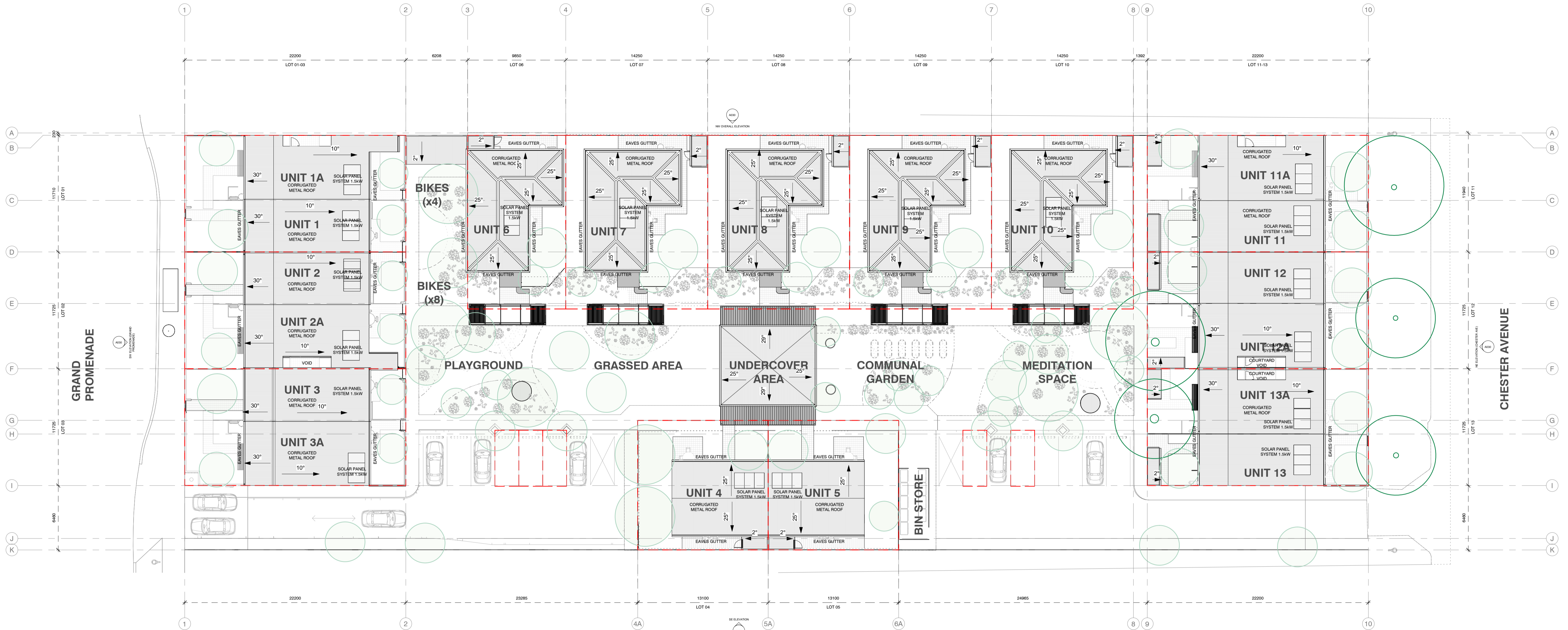
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322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING

OVERALL FIRST FLOOR PLAN

CLIENT	CATHOLIC ARCHDIOCESE OF PERTH	SCALE AT A1	1 : 200
STATUS	IFP-DA	DRAWING NO.	A011



AMENDED PLAN City of Stirling
18 Mar 2026
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OVERALL ROOF PLAN
SCALE: 1 : 200

OVERALL GROSS FLOOR AREA		
LOT	NAME	AREA
LOT 01	UNIT 1 FF AREA	45 m ²
LOT 01	UNIT 1 GF AREA	51 m ²
LOT 01	UNIT 1 STORE	5 m ²
LOT 01	UNIT 1A AREA	72 m ²
LOT 01	UNIT 1A STORE	5 m ²
LOT 02	UNIT 2 FF AREA	45 m ²
LOT 02	UNIT 2 GF AREA	51 m ²
LOT 02	UNIT 2 STORE	5 m ²
LOT 02	UNIT 2A AREA	72 m ²
LOT 02	UNIT 2A STORE	5 m ²
LOT 03	UNIT 3 FF AREA	44 m ²
LOT 03	UNIT 3 GF AREA	51 m ²
LOT 03	UNIT 3 STORE	5 m ²
LOT 03	UNIT 3A AREA	72 m ²
LOT 03	UNIT 3A STORE	5 m ²
LOT 04	UNIT 4 AREA	57 m ²
LOT 04	UNIT 4 STORE	4 m ²
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LOT 05	UNIT 5 STORE	4 m ²
LOT 06	UNIT 6 AREA	75 m ²
LOT 06	UNIT 6 STORE	4 m ²
LOT 07	UNIT 7 AREA	76 m ²
LOT 07	UNIT 7 STORE	5 m ²
LOT 08	UNIT 8 AREA	76 m ²
LOT 08	UNIT 8 STORE	5 m ²
LOT 09	UNIT 9 AREA	76 m ²
LOT 09	UNIT 9 STORE	5 m ²
LOT 10	UNIT 10 AREA	76 m ²
LOT 10	UNIT 10 STORE	5 m ²
LOT 11	UNIT 11 FF AREA	45 m ²
LOT 11	UNIT 11 GF AREA	51 m ²
LOT 11	UNIT 11 STORE	4 m ²
LOT 11	UNIT 11A AREA	72 m ²
LOT 11	UNIT 11A STORE	4 m ²
LOT 12	UNIT 12 FF AREA	45 m ²
LOT 12	UNIT 12 GF AREA	51 m ²
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LOT 12	UNIT 12A AREA	72 m ²
LOT 12	UNIT 12A STORE	4 m ²
LOT 13	UNIT 13 FF AREA	45 m ²
LOT 13	UNIT 13 GF AREA	51 m ²
LOT 13	UNIT 13 STORE	4 m ²
LOT 13	UNIT 13A AREA	72 m ²
LOT 13	UNIT 13A STORE	4 m ²

CITY OF STIRLING
ZONE: R30

322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING

OVERALL ROOF PLAN

CLIENT: CATHOLIC ARCHDIOCESE OF PERTH

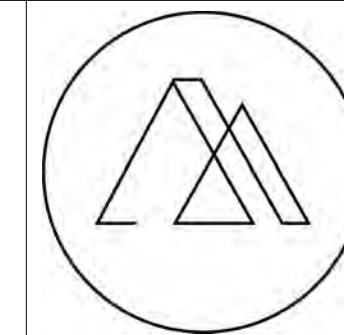
STATUS: IFF-DA

SCALE: 1 : 200

DRAWING NO: A012

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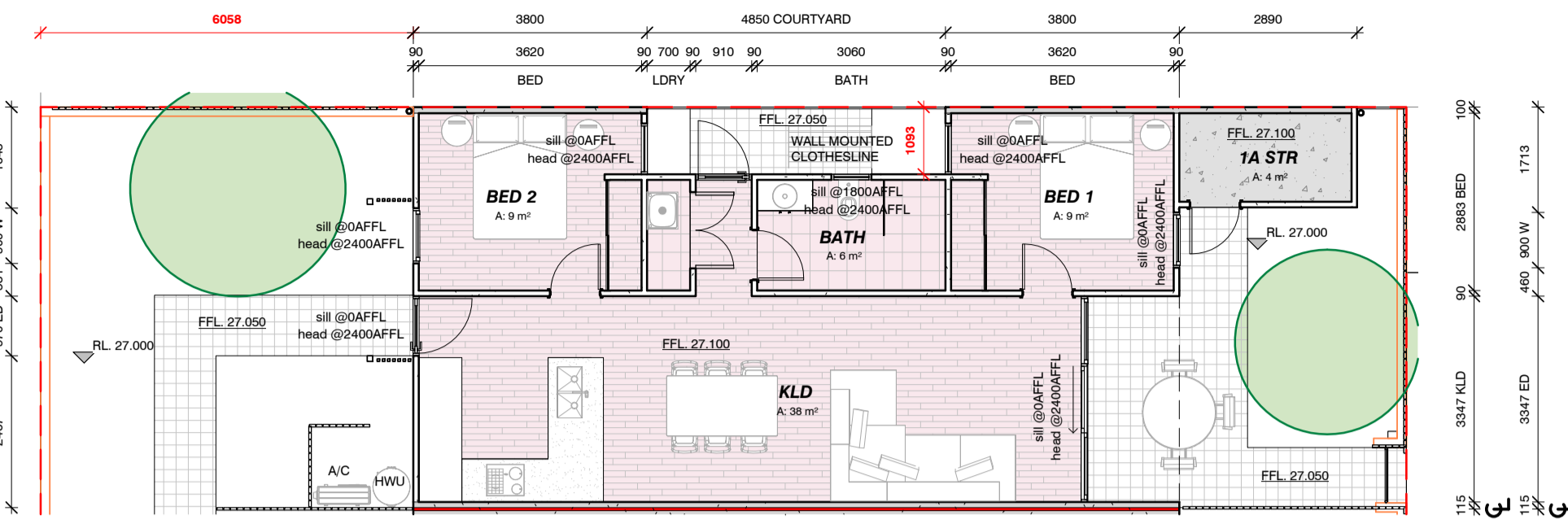
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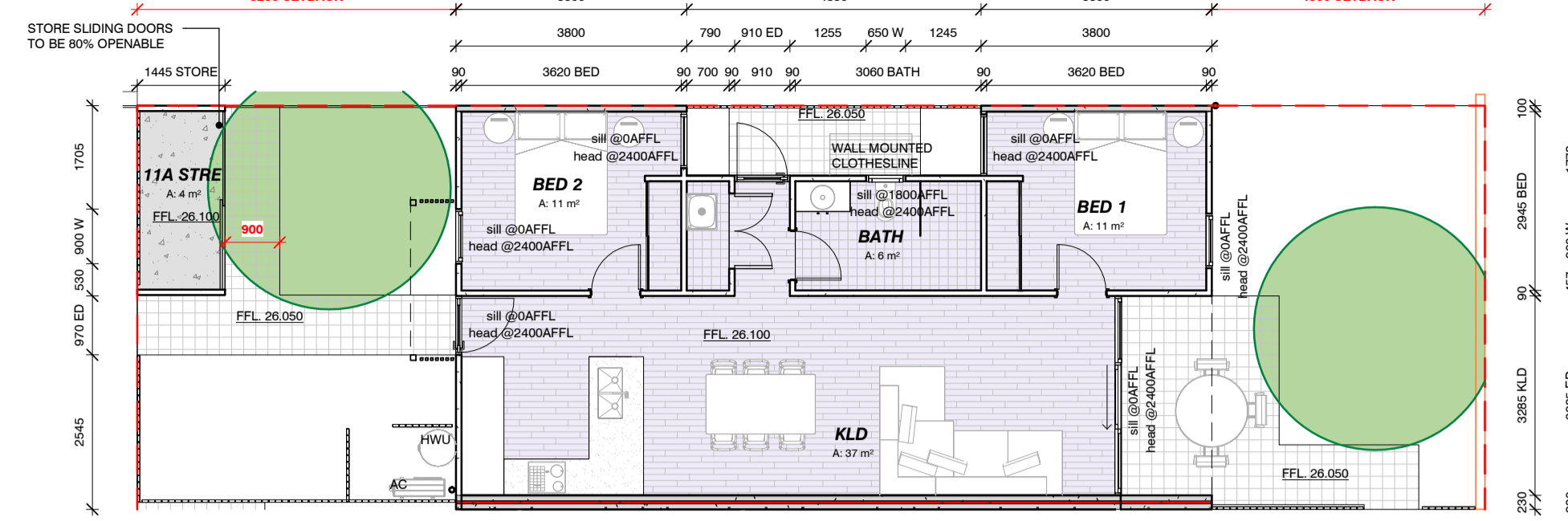
CITY OF STIRLING
ZONE: R30

TYPE 1 (2x1)
GROSS FLOOR AREA = 72m²
INTERNAL FLOOR AREA = 67m²



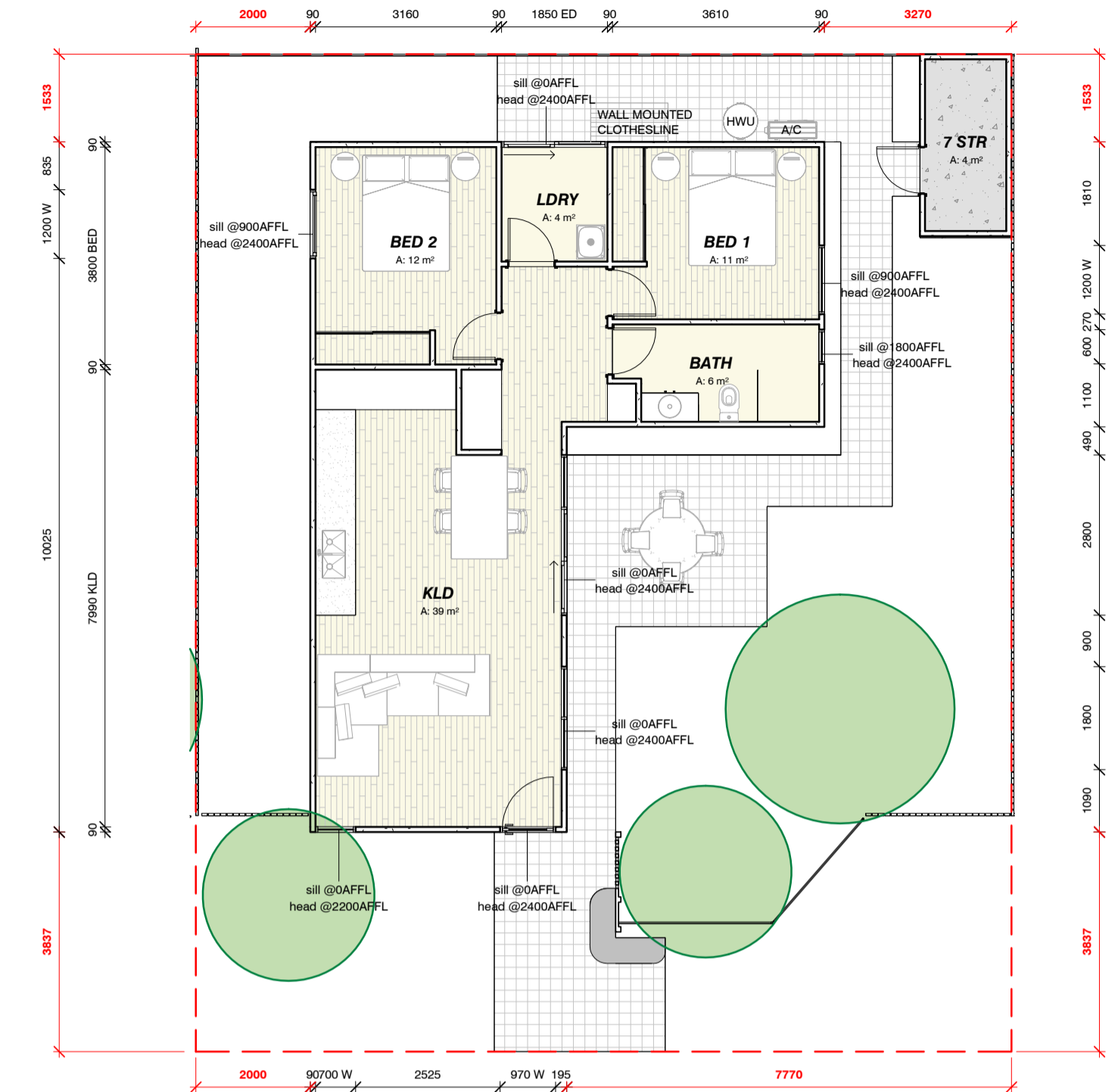
TYPE 1 FLOOR PLAN
 SCALE: 1:100

TYPE 1A (2x1)
GROSS FLOOR AREA = 72m²
INTERNAL FLOOR AREA = 67m²



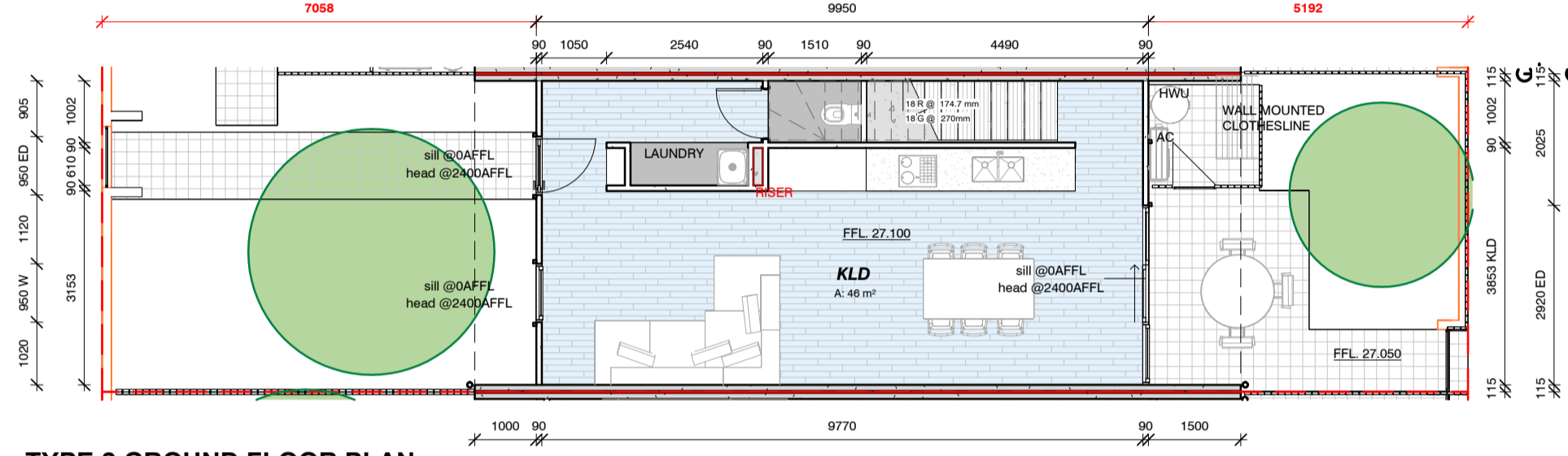
TYPE 1A FLOOR PLAN
 SCALE: 1:100

TYPE 3 (2x1)
GROSS FLOOR AREA = 77m²



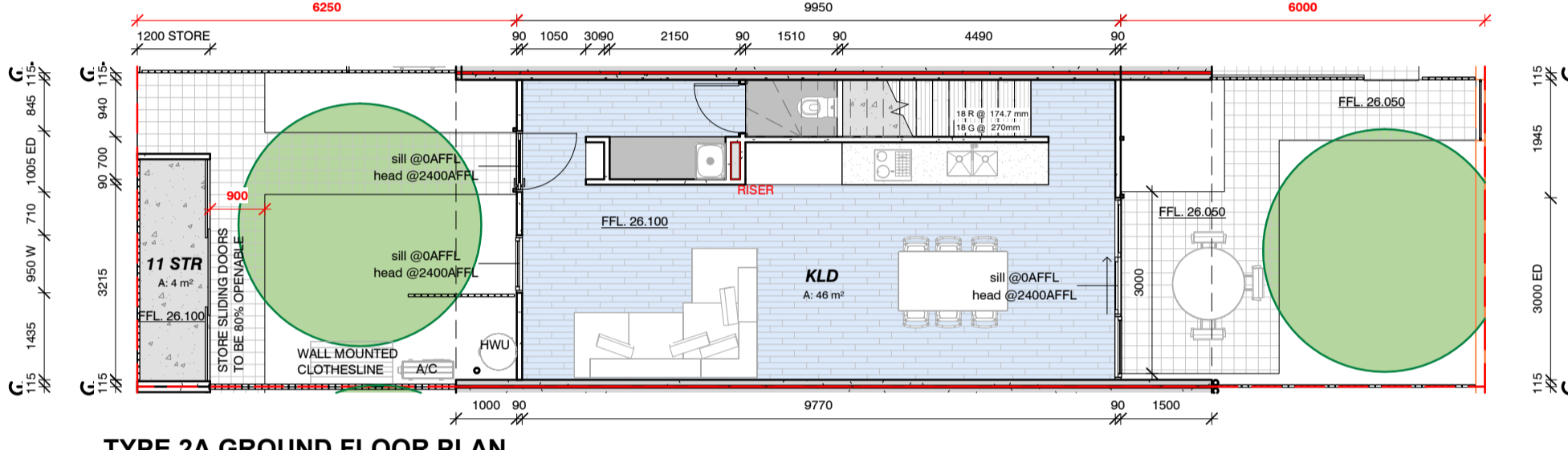
TYPE 3 FLOOR PLAN
 SCALE: 1:100

TYPE 2 (2x1.5)
GROSS FLOOR AREA = 103m²



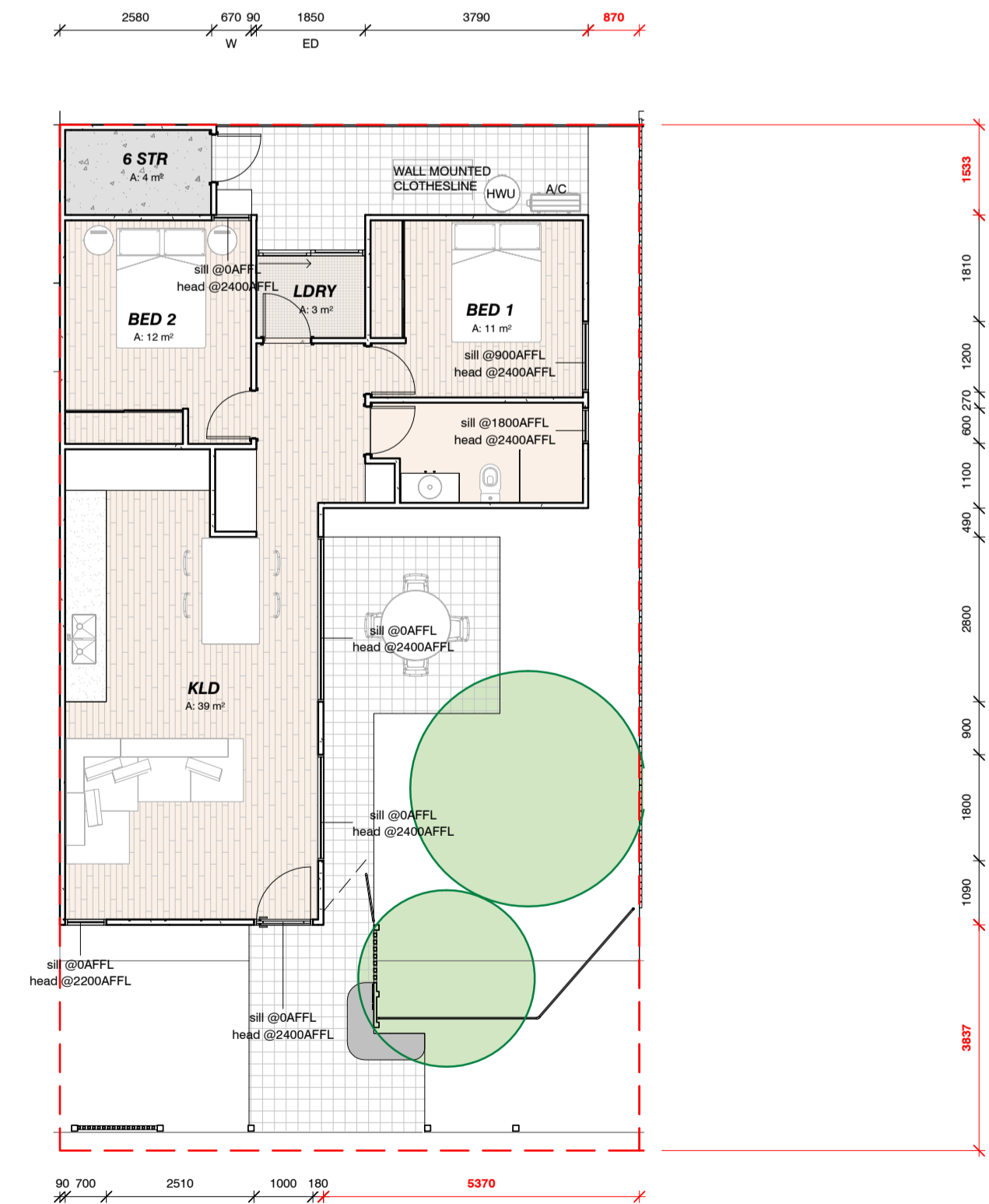
TYPE 2 GROUND FLOOR PLAN
 SCALE: 1:100

TYPE 2 (2x1.5)
GROSS FLOOR AREA = 103m²



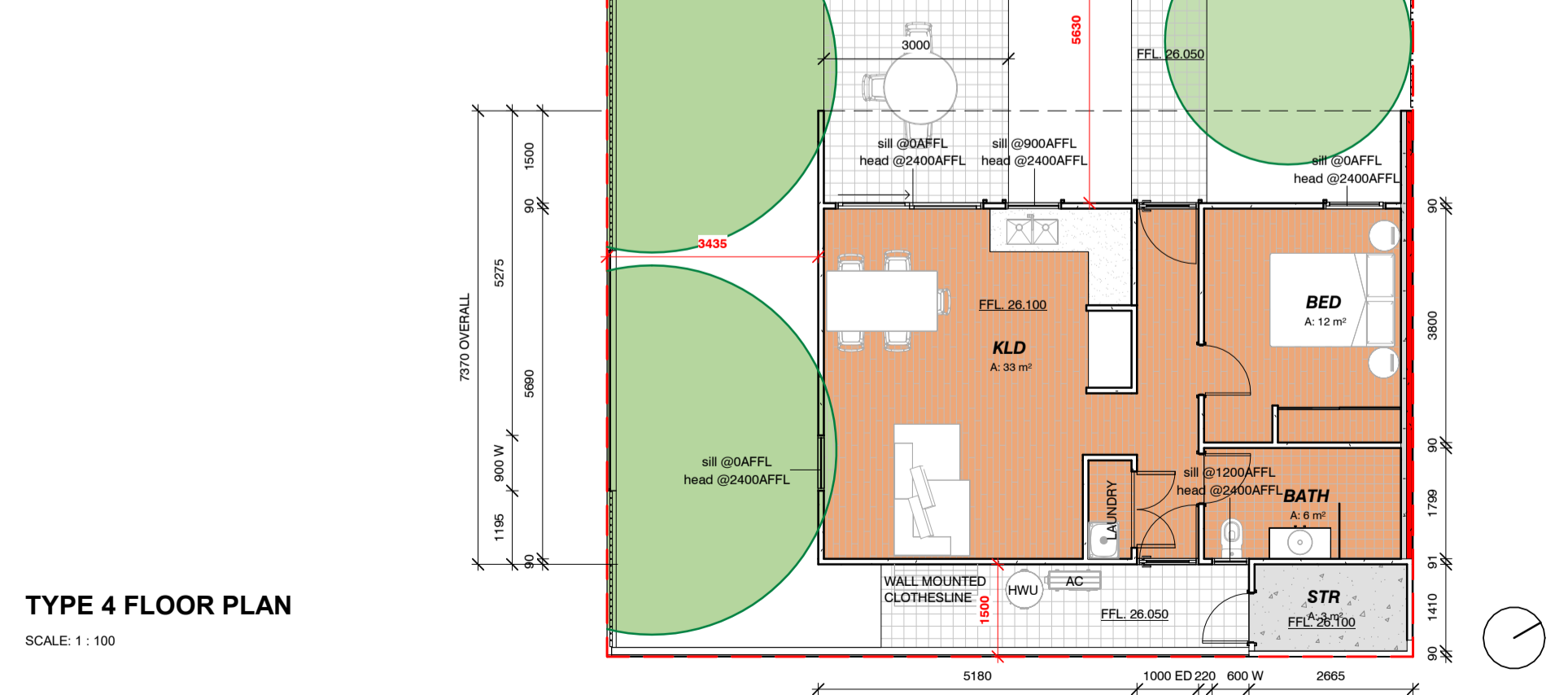
TYPE 2A GROUND FLOOR PLAN
 SCALE: 1:100

TYPE 3A (2x1) SMALL DWELLING
GROSS FLOOR AREA = 75m²
INTERNAL FLOOR AREA = 70m²



TYPE 3A FLOOR PLAN
 SCALE: 1:100

TYPE 4 (1x1) SMALL DWELLING
GROSS FLOOR AREA = 56m²



TYPE 4 FLOOR PLAN
 SCALE: 1:100

City of Stirling
18 Mar 2026
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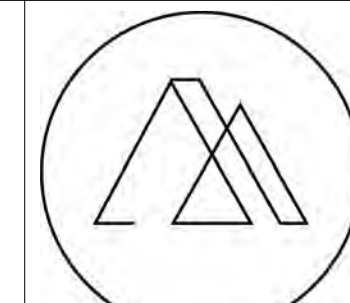
AMENDED PLAN

- GENERAL NOTES:**
- ALL HABITABLE ROOM WINDOWS TO EXCEED 10% OF THE HABITABLE ROOM INTERNAL FLOOR AREA.
 - ALL HABITABLE ROOM WINDOWS TO COMPRISE A MINIMUM OF 50% OF CLEAR GLAZING.
 - STORES WITH DIMENSION REDUCED TO 1M PROPOSED TO INCLUDE DOORS ON THE GREATER DIMENSIONS THAT OPEN A MIN. OF 80% OF THE LENGTH.

- TYPOLOGIES LEGEND**
 SCALE: 1:200
- TYPE 1 (2x1)
 - TYPE 1A (2x1)
 - TYPE 2 (2x1.5)
 - TYPE 2A (2x1.5)
 - TYPE 3 (2x1)
 - TYPE 3A (2x1)
 - TYPE 4 (1x1)

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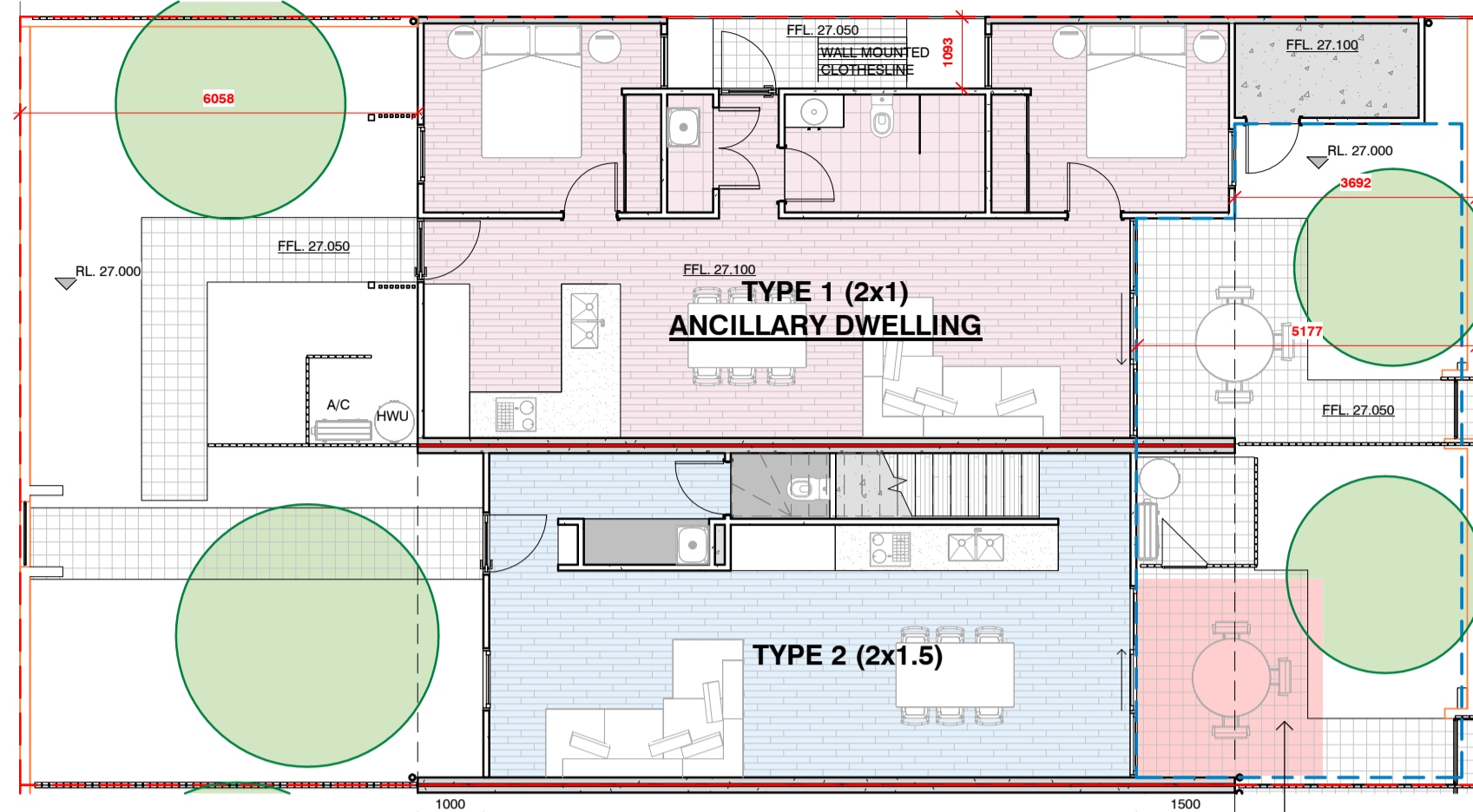


322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING

INDIVIDUAL LOT PLANS

CLIENT:	CATHOLIC ARCHDIOCESE OF PERTH	SCALE: 1:100
STATUS:	IFP-DA	As indicated
DRAWING NO:		A013

TYPE 1 (2x1) & TYPE 2 (2x1.5)



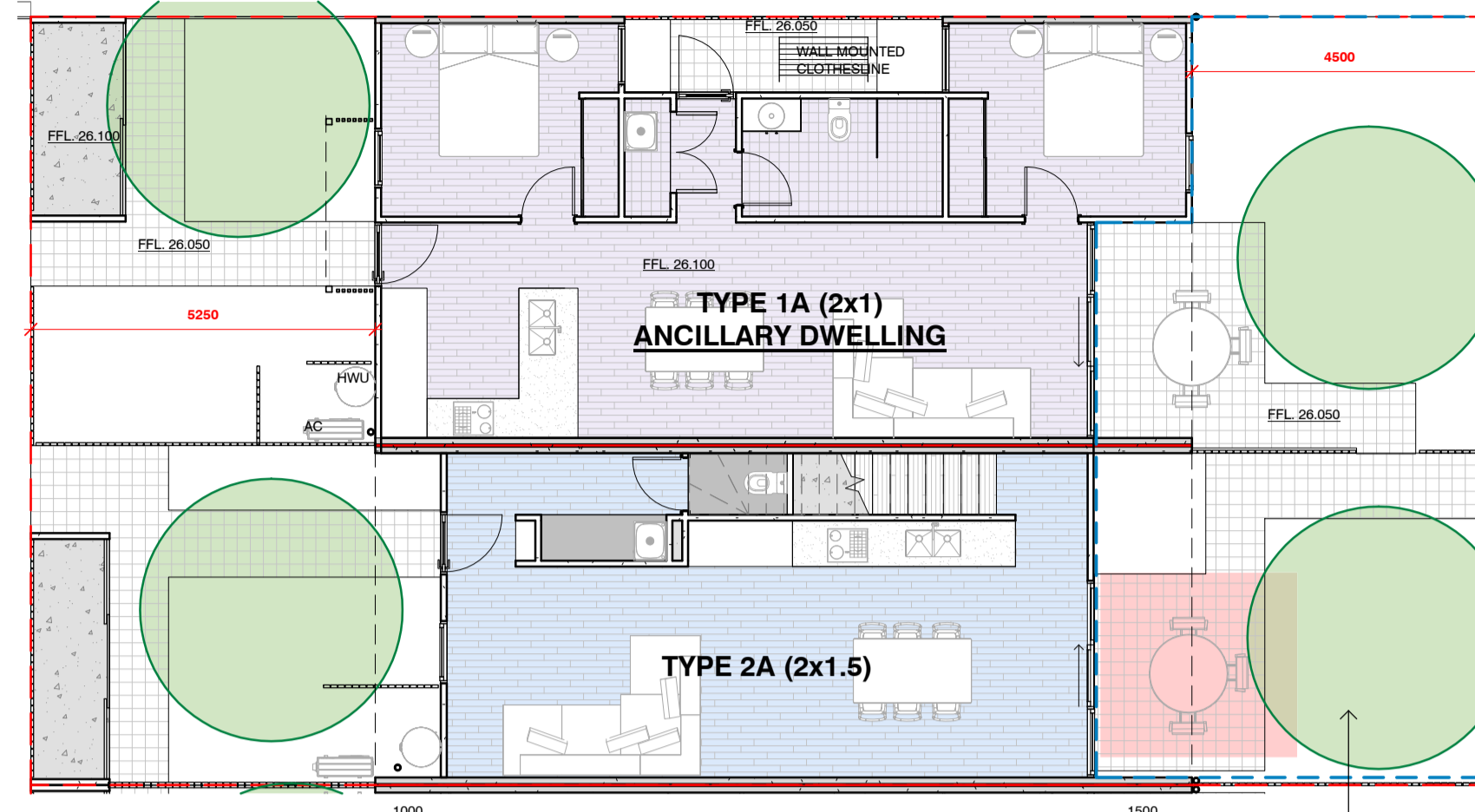
SITE COVER
 LOT AREA = 260m²
 MAX SITE COVER R30 = 60%
 TOTAL DEVELOPMENT AREA = 130 m² OR 50%

LOT AREA = 260m²
 PRIMARY GARDEN AREA SHOWN AS BLUE DASHED
 MIN. PRIMARY GARDEN AREA REQ = 40m²
 PRIMARY GARDEN AREA ACHIEVED = 47m²
 MAX. PERMANENT ROOF COVER = 1/3 OF PRIMARY GARDEN AREA
 PERMANENT ROOF COVER PROPOSED = 12m²

TYPE 1 & 2 FLOOR PLAN

SCALE: 1 : 100

TYPE 1A (2x1) & TYPE 2A (2x1.5)



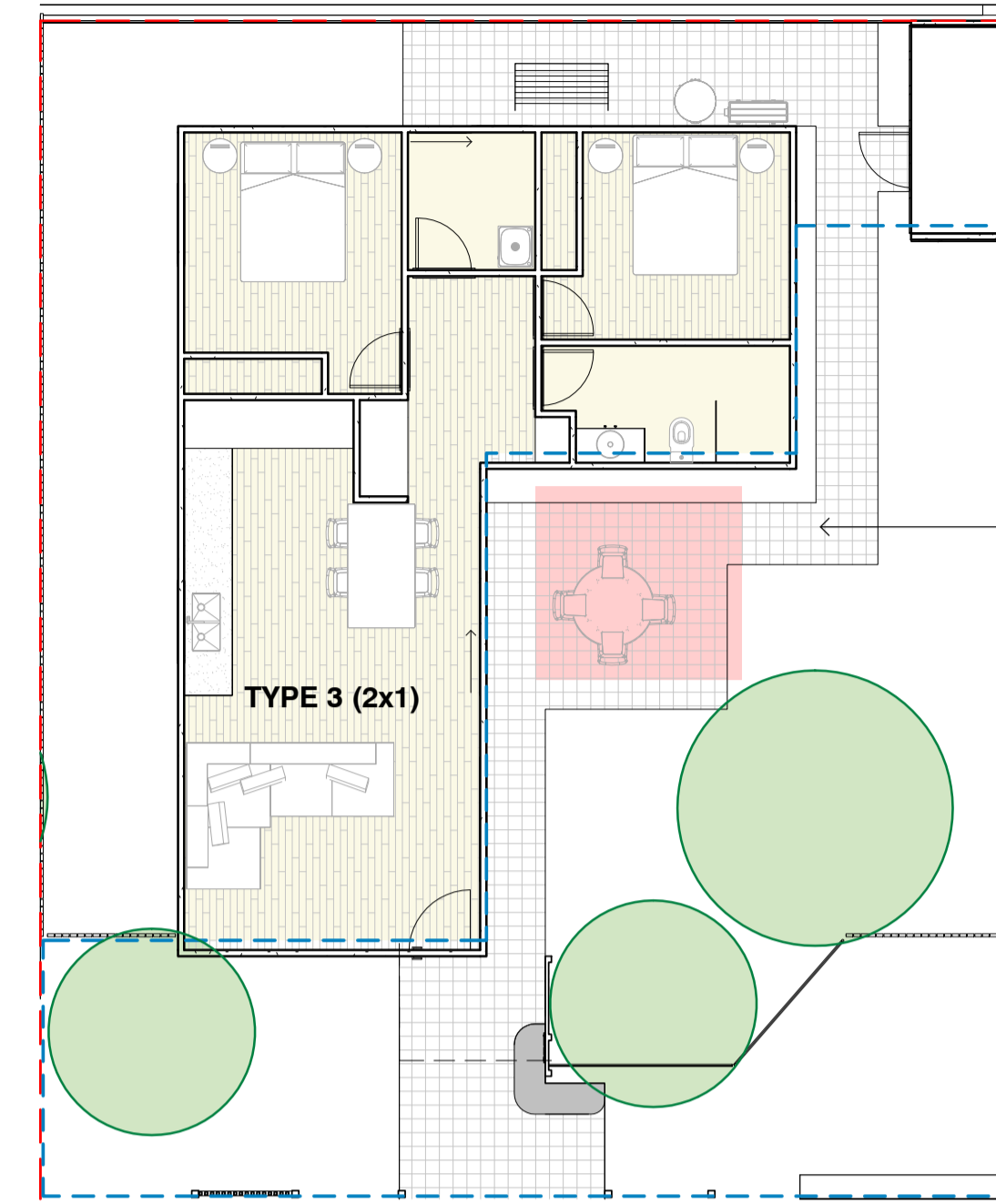
SITE COVER
 LOT AREA = 260m²
 MAX SITE COVER R30 = 60%
 TOTAL DEVELOPMENT AREA = 132m² OR 50%

LOT AREA = 260m²
 PRIMARY GARDEN AREA SHOWN AS BLUE DASHED
 MIN. PRIMARY GARDEN AREA REQ = 40m²
 PRIMARY GARDEN AREA ACHIEVED = 65m²
 MAX. PERMANENT ROOF COVER = 1/3 OF PRIMARY GARDEN AREA
 PERMANENT ROOF COVER PROPOSED = 12m²

TYPE 1A & 2A FLOOR PLAN

SCALE: 1 : 100

TYPE 3 (2x1)



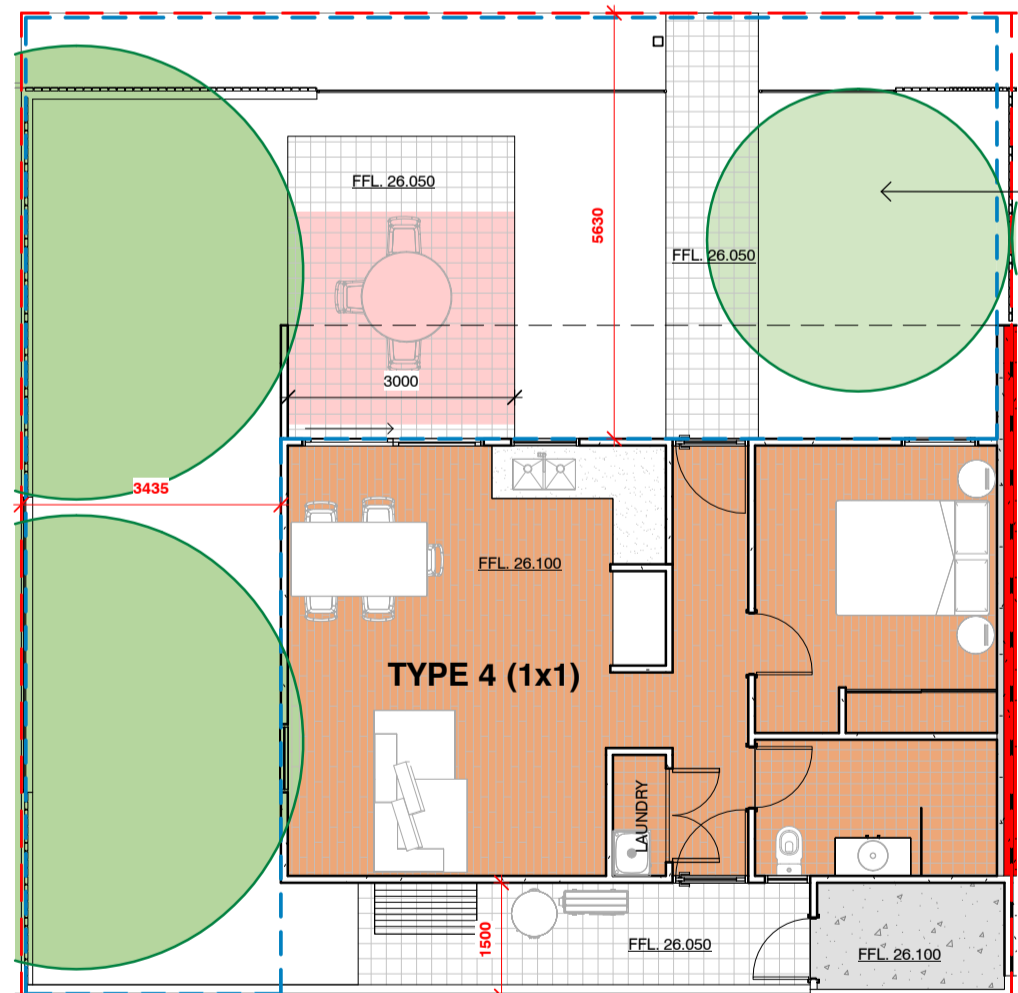
SITE COVER
 LOT AREA = 260m²
 MAX SITE COVER R30 = 60%
 TOTAL DEVELOPMENT AREA = 82m² OR 32%

LOT AREA = 260m²
 PRIMARY GARDEN AREA SHOWN AS BLUE DASHED
 MIN. PRIMARY GARDEN AREA REQ = 40m²
 PRIMARY GARDEN AREA ACHIEVED = 118m²
 MAX. PERMANENT ROOF COVER = 1/3 OF PRIMARY GARDEN AREA
 PERMANENT ROOF COVER PROPOSED = 13m²

TYPE 3 FLOOR PLAN

SCALE: 1 : 100

TYPE 4 (1x1) SMALL DWELLING



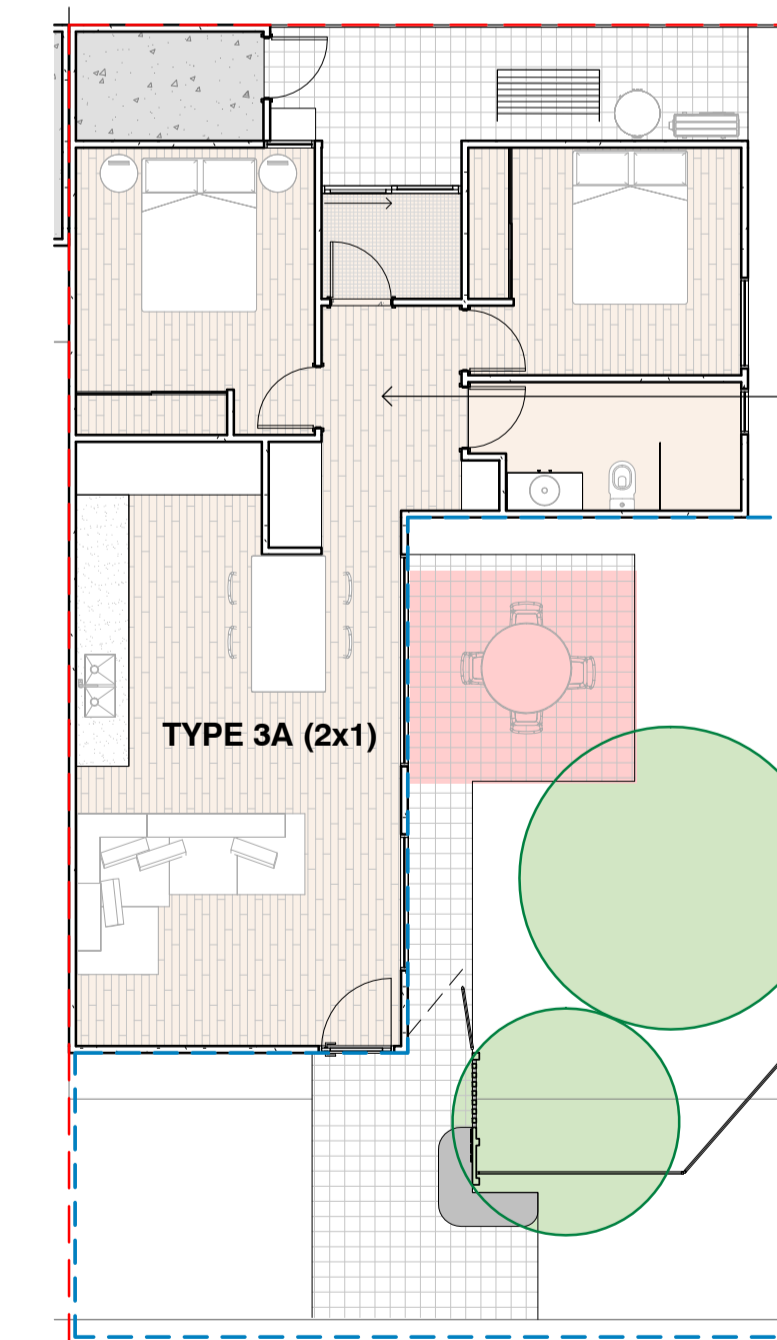
LOT AREA = 170m² (SMALL DWELLING)
 PRIMARY GARDEN AREA SHOWN AS BLUE DASHED
 MIN. PRIMARY GARDEN AREA REQ = 40m²
 PRIMARY GARDEN AREA ACHIEVED = 96m²
 MAX. PERMANENT ROOF COVER = 1/3 OF PRIMARY GARDEN AREA
 PERMANENT ROOF COVER PROPOSED = 14m²

SITE COVER
 LOT AREA = 170m²
 MAX SITE COVER R30 = 60%
 TOTAL DEVELOPMENT AREA = 61m² OR 36%

TYPE 4 FLOOR PLAN

SCALE: 1 : 100

TYPE 3A (2x1) SMALL DWELLING



LOT AREA = 183m²
 PRIMARY GARDEN AREA SHOWN AS BLUE DASHED
 MIN. PRIMARY GARDEN AREA REQ = 40m²
 PRIMARY GARDEN AREA ACHIEVED = 73m²
 MAX. PERMANENT ROOF COVER = 1/3 OF PRIMARY GARDEN AREA
 PERMANENT ROOF COVER PROPOSED = 13m²

SITE COVER
 LOT AREA = 183m²
 MAX SITE COVER R30 = 60%
 TOTAL DEVELOPMENT AREA = 80m² OR 44%

TYPE 3A FLOOR PLAN

SCALE: 1 : 100

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- TYPE 1 (2x1)
- TYPE 1A (2x1)
- TYPE 2 (2x1.5)
- TYPE 2A (2x1.5)
- TYPE 3 (2x1)
- TYPE 3A (2x1)
- TYPE 4 (1x1)

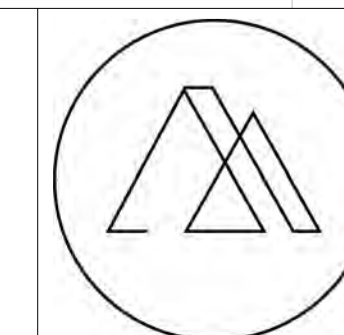
TYPLOGIES LEGEND

SCALE: 1 : 200

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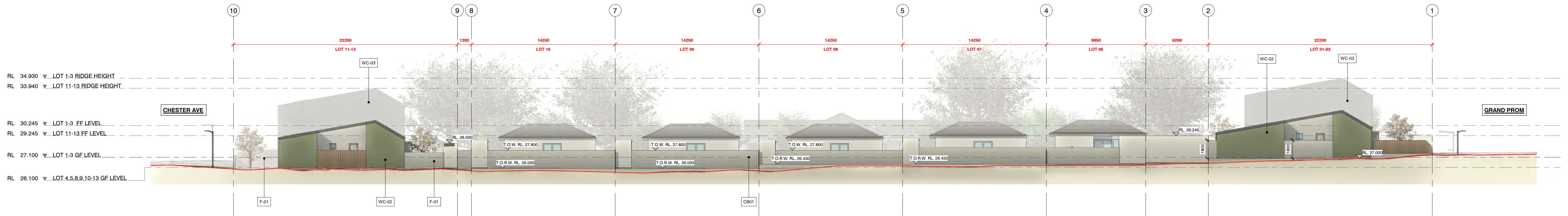
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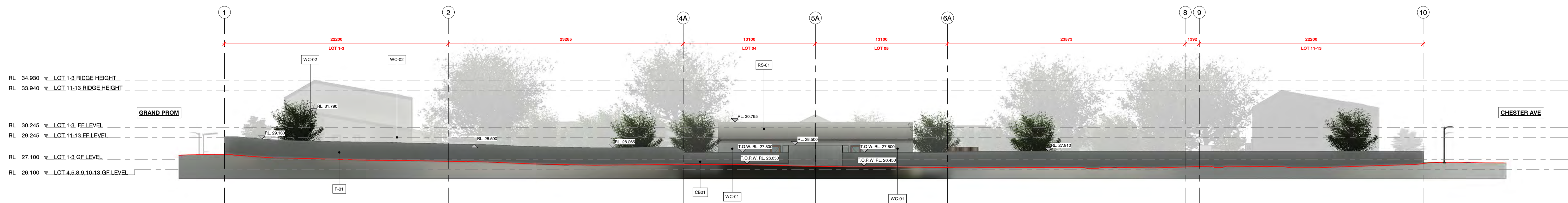
322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING

INDIVIDUAL LOT PLANS

CLIENT:	CATHOLIC ARCHDIOCESE OF PERTH	SCALE: 1:100	As indicated
STATUS:	IFP-DA	DRAWING NO:	A014



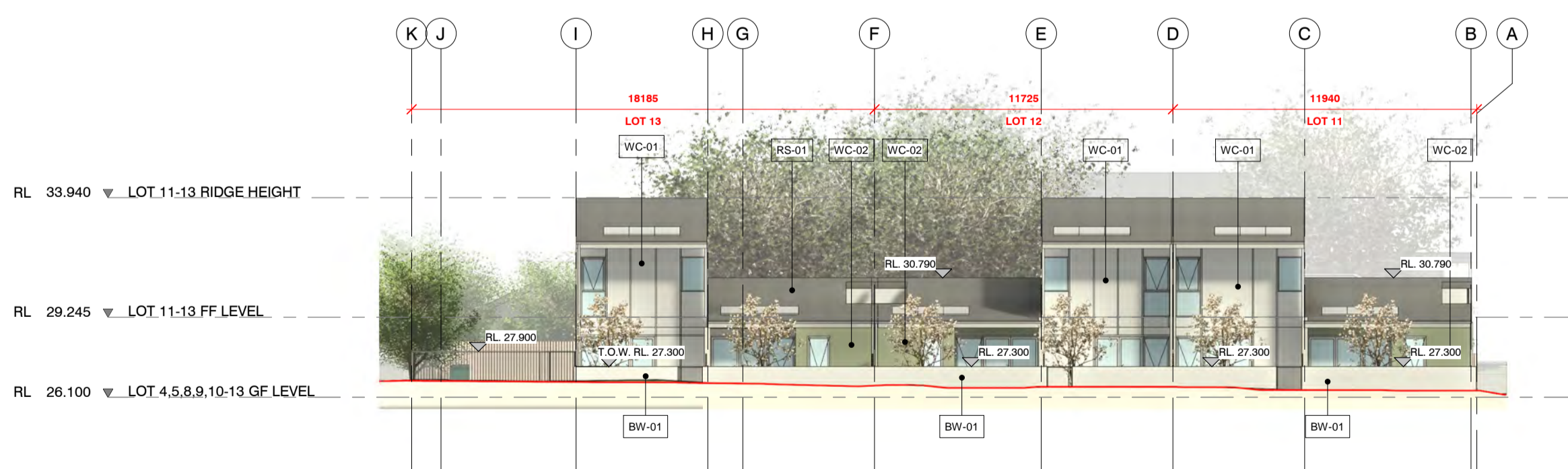
NW OVERALL ELEVATION
SCALE: 1 : 200



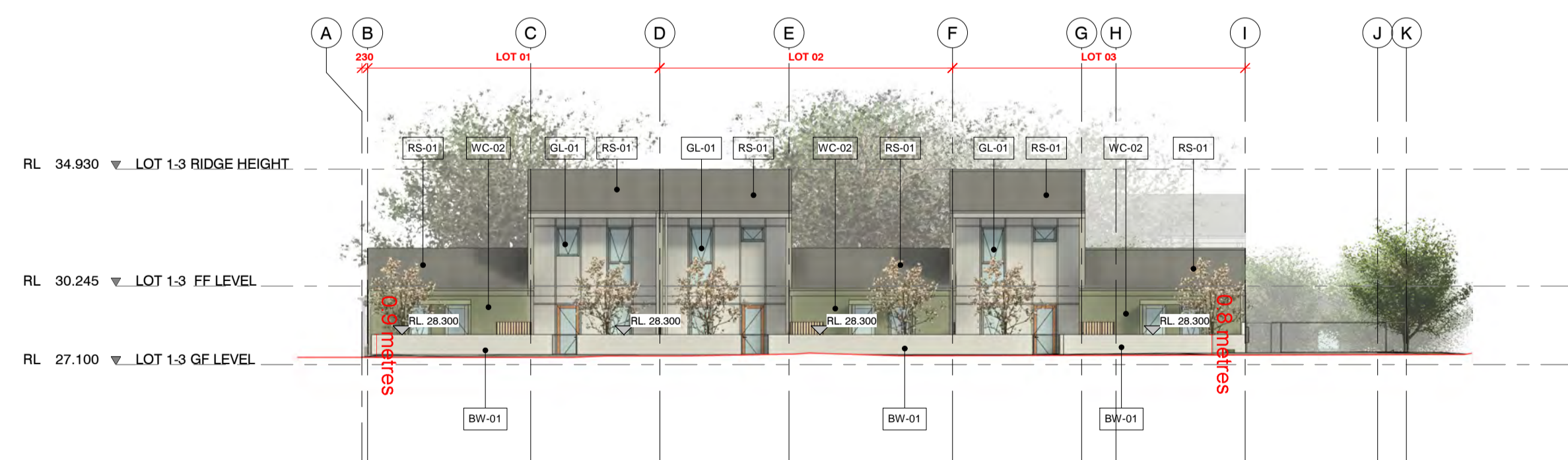
SE ELEVATION
SCALE: 1 : 200

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NE ELEVATION (CHESTER AVE)
SCALE: 1 : 200



SW ELEVATION (GRAND PROMENADE)
SCALE: 1 : 200

EXTERNAL MATERIAL LEGEND	
WC-01	FIBRE CEMENT (BOARD & BATTEN) - PAINT FINISH, WHITE
WC-02	FIBRE CEMENT - PAINT FINISH, GREEN
WC-03	CORRUGATED METAL ZINCALUME
RS-01	ROOF SHEETING - CORRUGATED METAL ZINCALUME
BW-01	BRICKWORK - 2C BRICK FENCING, PAINT FINISH
F-01	FENCING - TIMBER
F-02	FENCING - CHAINLINK

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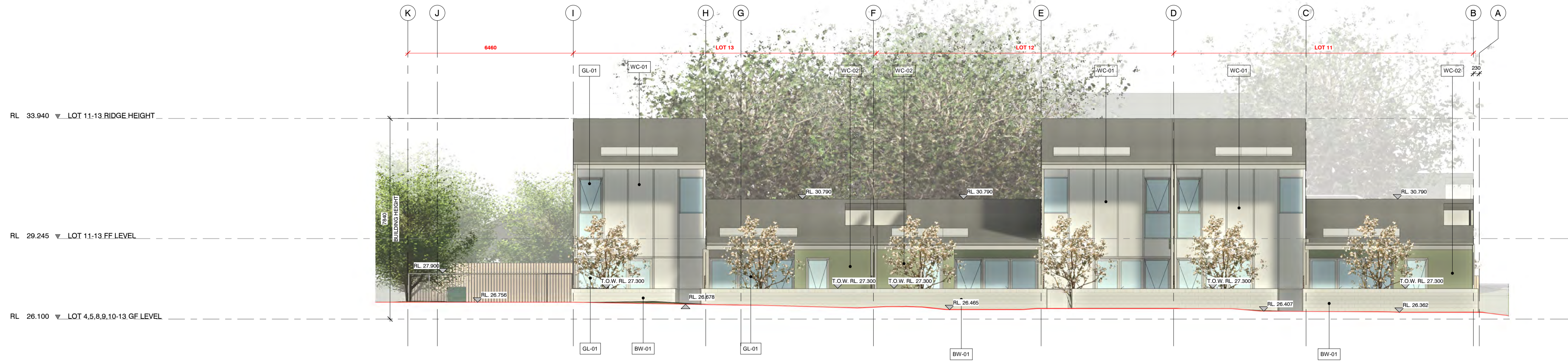
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322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING	
ELEVATIONS	
CLIENT CATHOLIC ARCHDIOCESE OF PERTH	SCALE AT A1: As indicated
STATUS: IFP-DA	DRAWING NO: A030

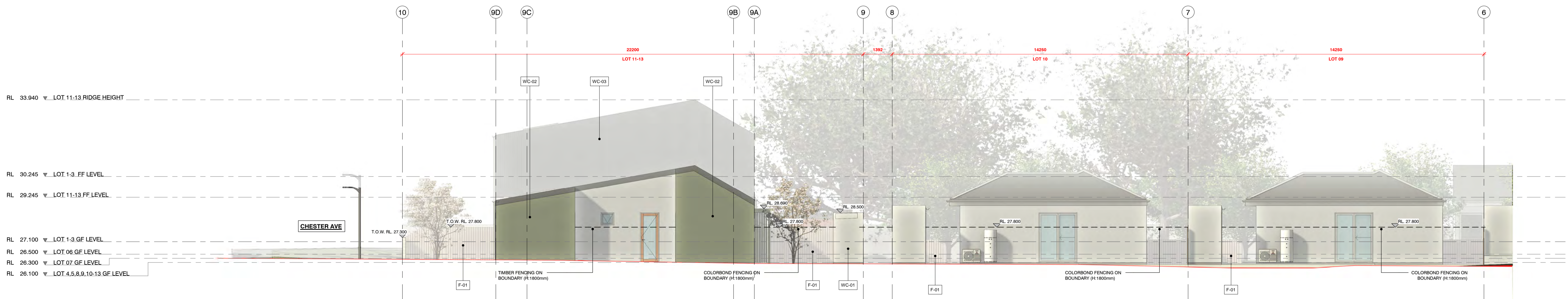


NORTH EAST ELEVATION (CHESTER AVE)
SCALE: 1:100

EXTERNAL MATERIAL LEGEND	
WC-01	FIBRE CEMENT (BOARD & BATTEN) - PAINT FINISH, WHITE
WC-02	FIBRE CEMENT - PAINT FINISH, GREEN
WC-03	CORRUGATED METAL ZINCALUME
RS-01	ROOF SHEETING - CORRUGATED METAL ZINCALUME
BW-01	BRICKWORK - 2C BRICK FENCING, PAINT FINISH
F-01	FENCING - TIMBER
F-02	FENCING - CHAINLINK



SOUTH WEST ELEVATION (GRAND PROMENADE)
SCALE: 1:100



NORTH WEST ELEVATION LOT 09-13
SCALE: 1:100

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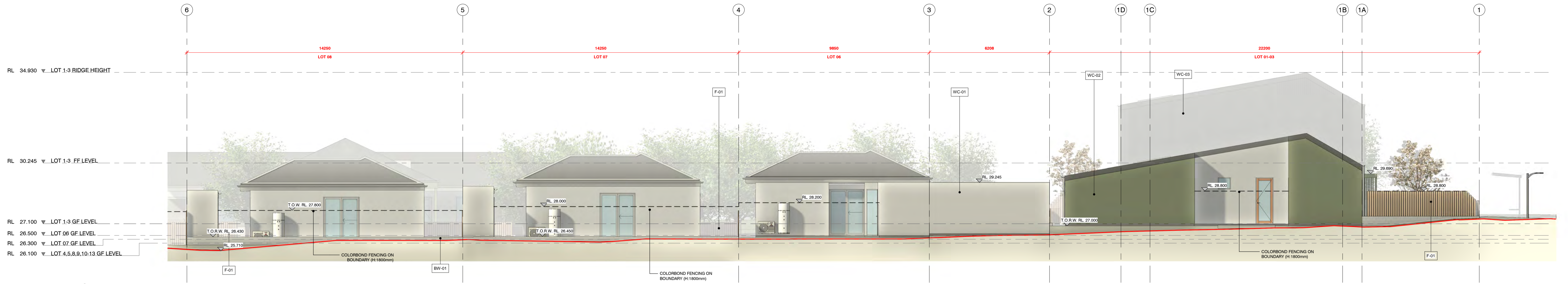
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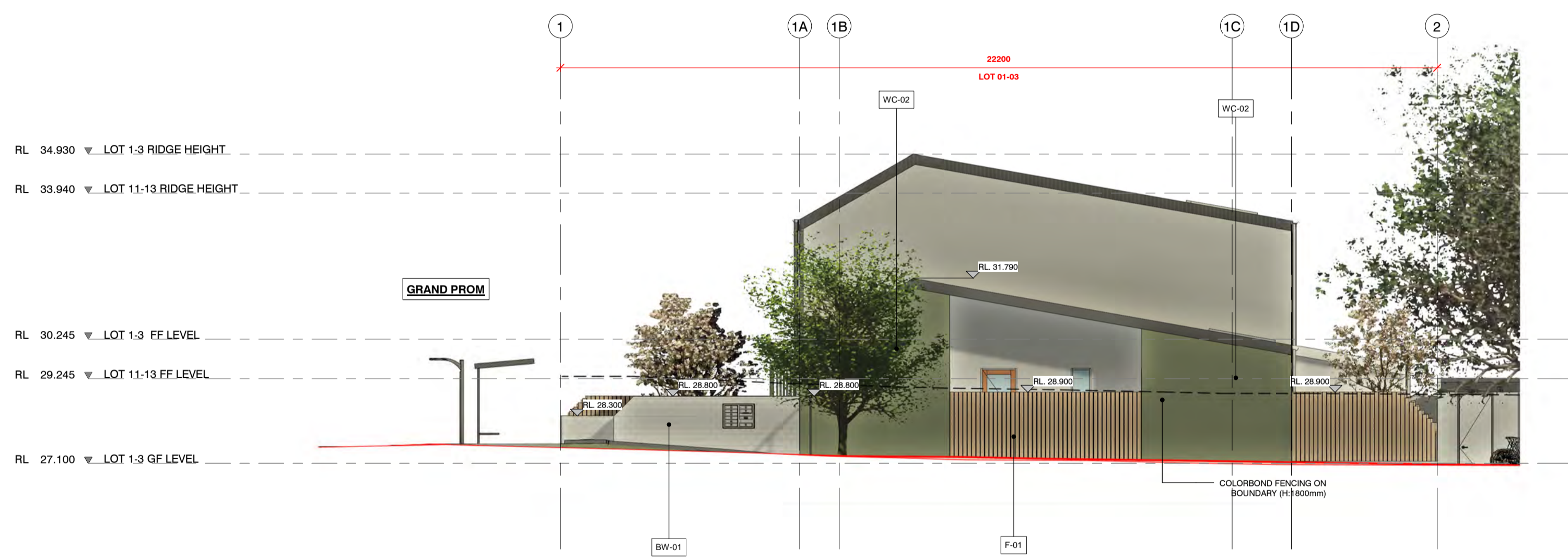
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DALE ALCOCK PROJECTS

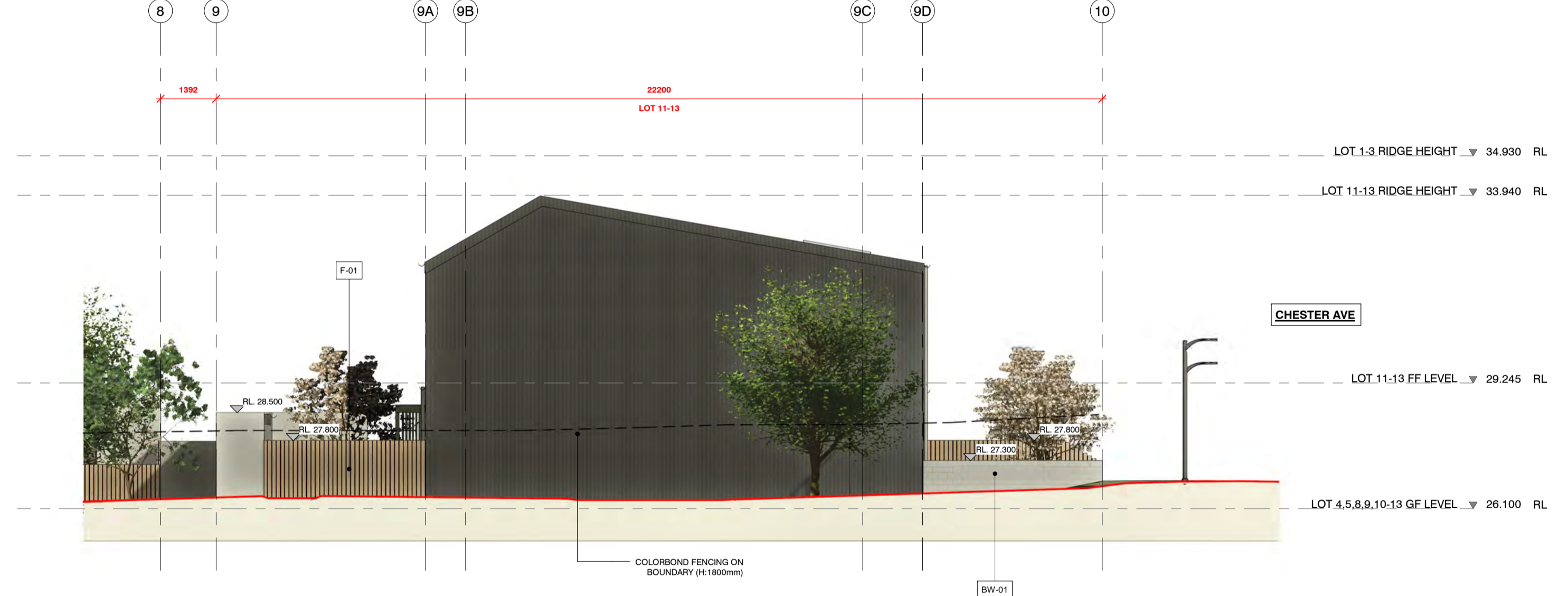
322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING	
STREET ELEVATIONS	
CLIENT: CATHOLIC ARCHDIOCESE OF PERTH	SCALE: As indicated
STATUS: IFP-DA	DRAWING NO: A031



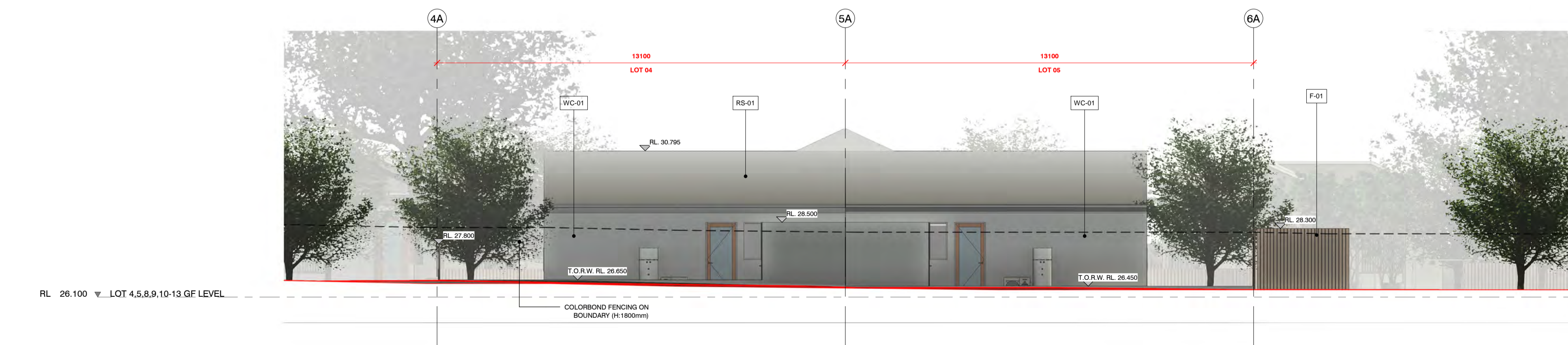
NORTH WEST ELEVATION LOT 01-08
SCALE: 1 : 100



SOUTH EAST ELEVATION LOT 01-03
SCALE: 1 : 100



SOUTH EAST ELEVATION LOT 11-13
SCALE: 1 : 100



SOUTH EAST ELEVATION LOT 04-05
SCALE: 1 : 100

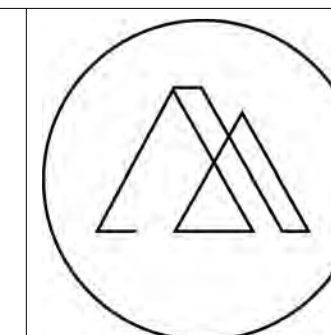
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EXTERNAL MATERIAL LEGEND	
WC-01	FIBRE CEMENT (BOARD & BATTEN) - PAINT FINISH, WHITE
WC-02	FIBRE CEMENT - PAINT FINISH, GREEN
WC-03	CORRUGATED METAL ZINCALUME
RS-01	ROOF SHEETING - CORRUGATED METAL ZINCALUME
BW-01	BRICKWORK - 2C BRICK FENCING, PAINT FINISH
F-01	FENCING - TIMBER
F-02	FENCING - CHAINLINK

NO.	DATE	ISSUE
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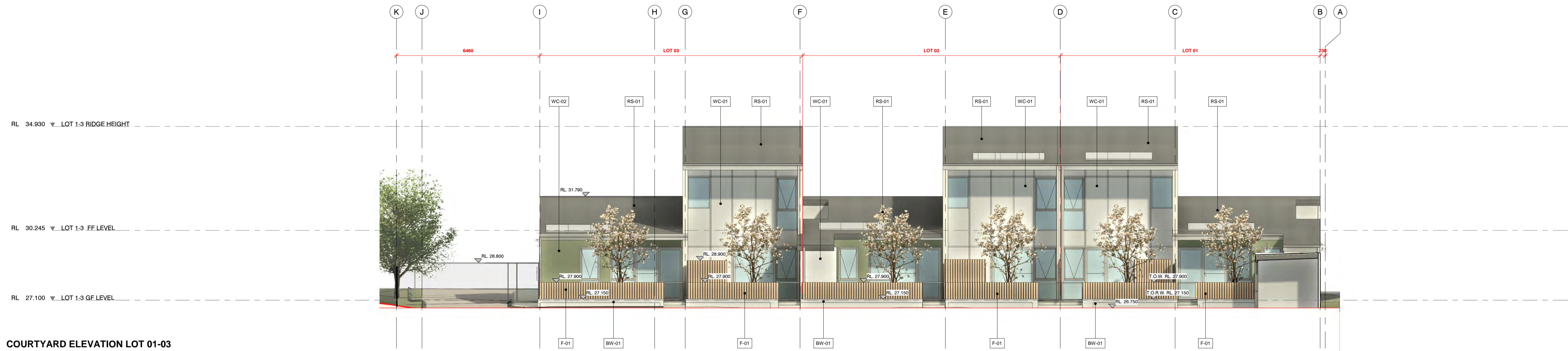
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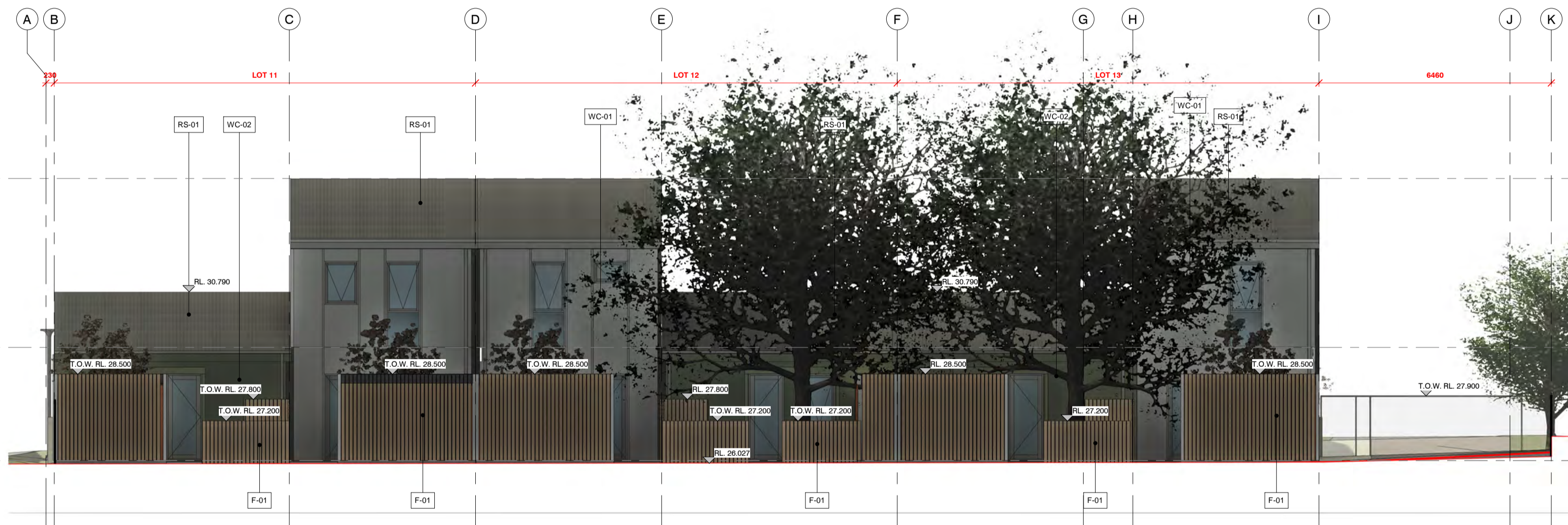
322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING

STREET ELEVATIONS

CLIENT	SCALE AT A1:
CATHOLIC ARCHDIOCESE OF PERTH	As indicated
STATUS:	DRAWING NO:
IFP-DA	A032



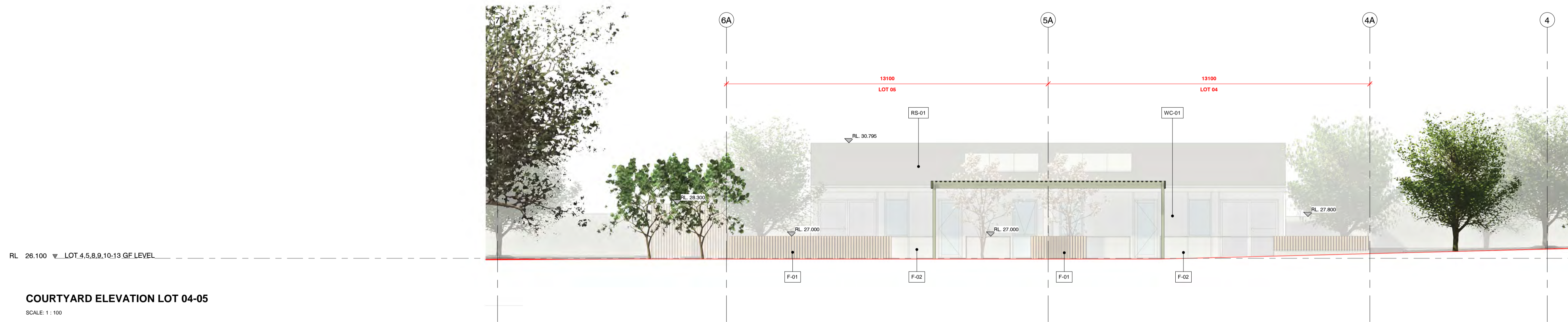
COURTYARD ELEVATION LOT 01-03
SCALE: 1 : 100



COURTYARD ELEVATION LOT 11-13
SCALE: 1 : 100

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COURTYARD ELEVATION LOT 04-05
SCALE: 1 : 100

EXTERNAL MATERIAL LEGEND	
WC-01	FIBRE CEMENT (BOARD & BATTEN) - PAINT FINISH, WHITE
WC-02	FIBRE CEMENT - PAINT FINISH, GREEN
WC-03	CORRUGATED METAL ZINCALUME
RS-01	ROOF SHEETING - CORRUGATED METAL ZINCALUME
BW-01	BRICKWORK - 2C BRICK FENCING, PAINT FINISH
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F-02	FENCING - CHAINLINK

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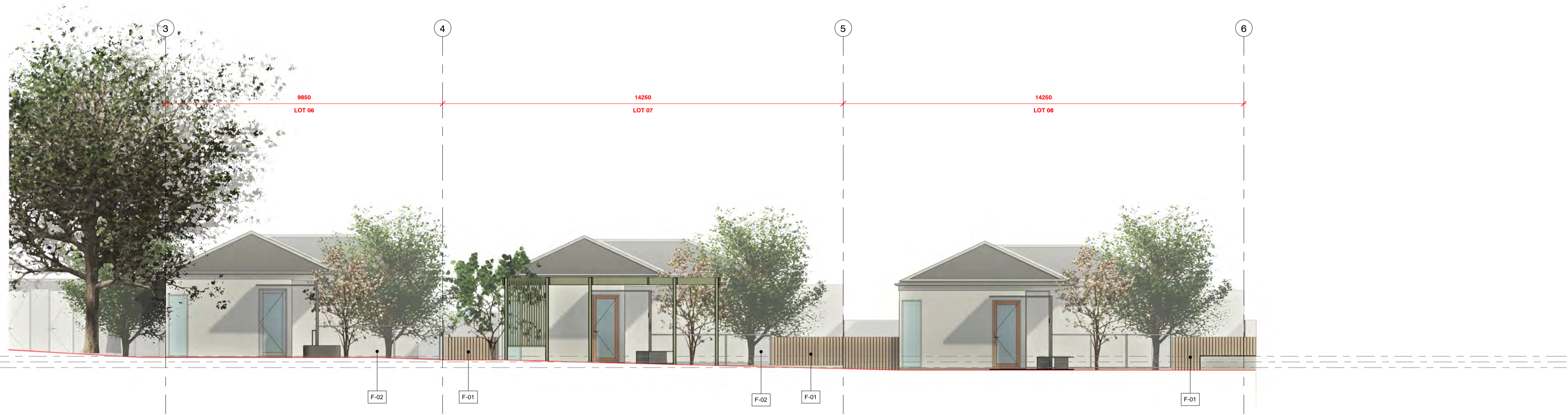
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322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING	
COURTYARD ELEVATIONS	
CLIENT: CATHOLIC ARCHDIOCESE OF PERTH	SCALE: AT A1: As indicated
STATUS: IFP-DA	DRAWING NO: A033

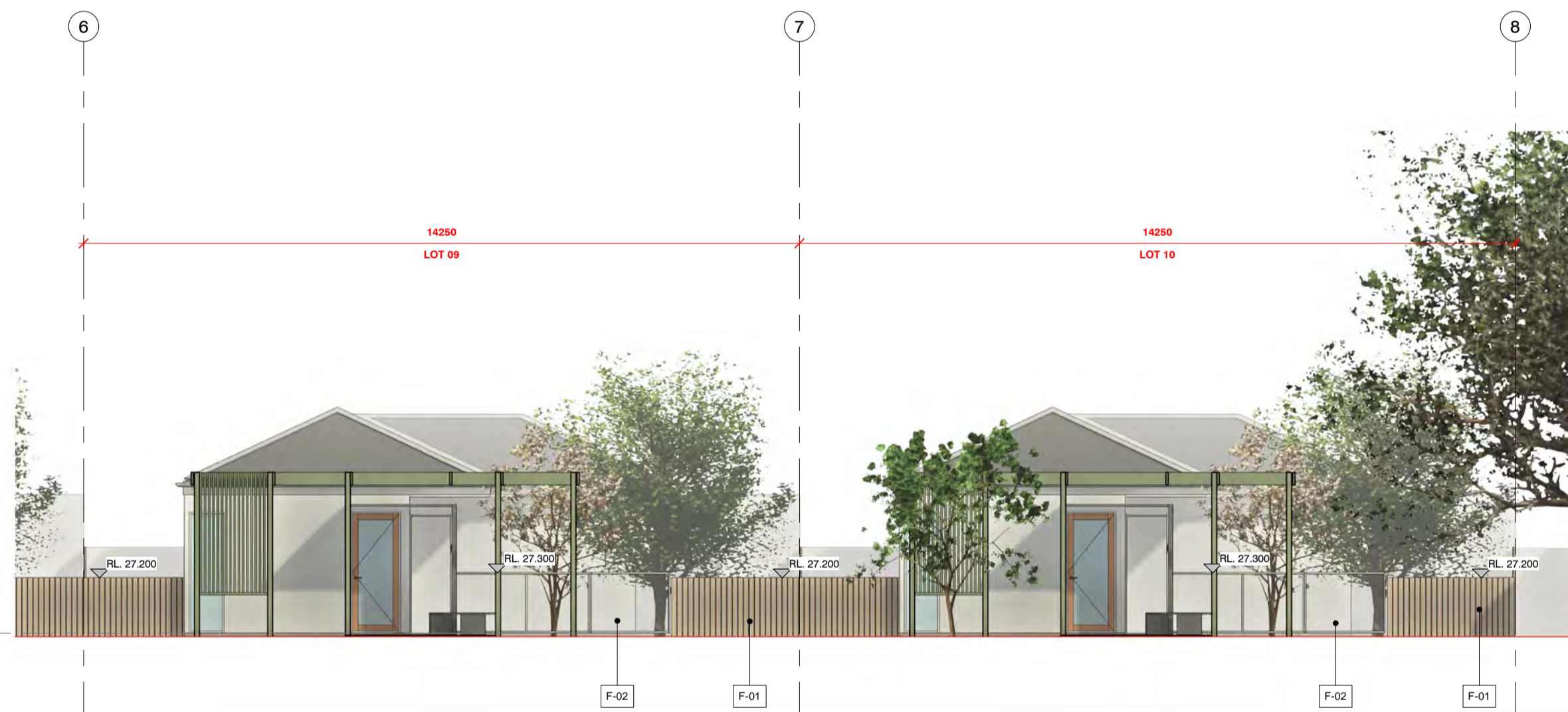
RL 26.500 ▾ LOT 06 GF LEVEL
 RL 26.300 ▾ LOT 07 GF LEVEL
 RL 26.100 ▾ LOT 4,5,8,9,10-13 GF LEVEL



COURTYARD ELEVATION LOT 06-08

SCALE: 1 : 100

RL 26.100 ▾ LOT 4,5,8,9,10-13 GF LEVEL



COURTYARD ELEVATION LOT 09-10

SCALE: 1 : 100

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EXTERNAL MATERIAL LEGEND	
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WC-02	FIBRE CEMENT - PAINT FINISH, GREEN
WC-03	CORRUGATED METAL ZINCALUME
RS-01	ROOF SHEETING - CORRUGATED METAL ZINCALUME
BW-01	BRICKWORK - 2C BRICK FENCING, PAINT FINISH
F-01	FENCING - TIMBER
F-02	FENCING - CHAINLINK

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E	11/03/2026	ISSUE FOR APPROVAL, DA UPDATES

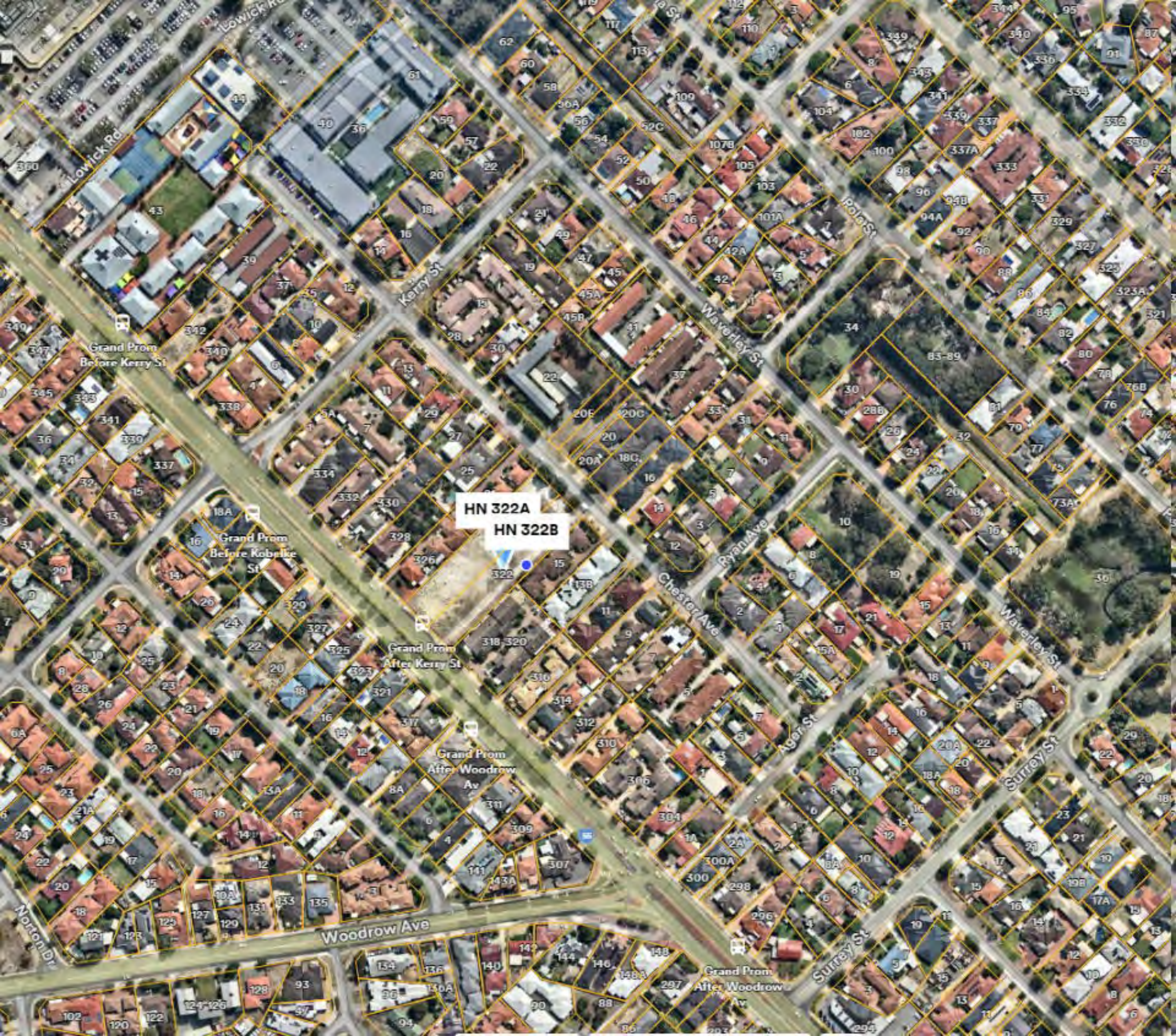
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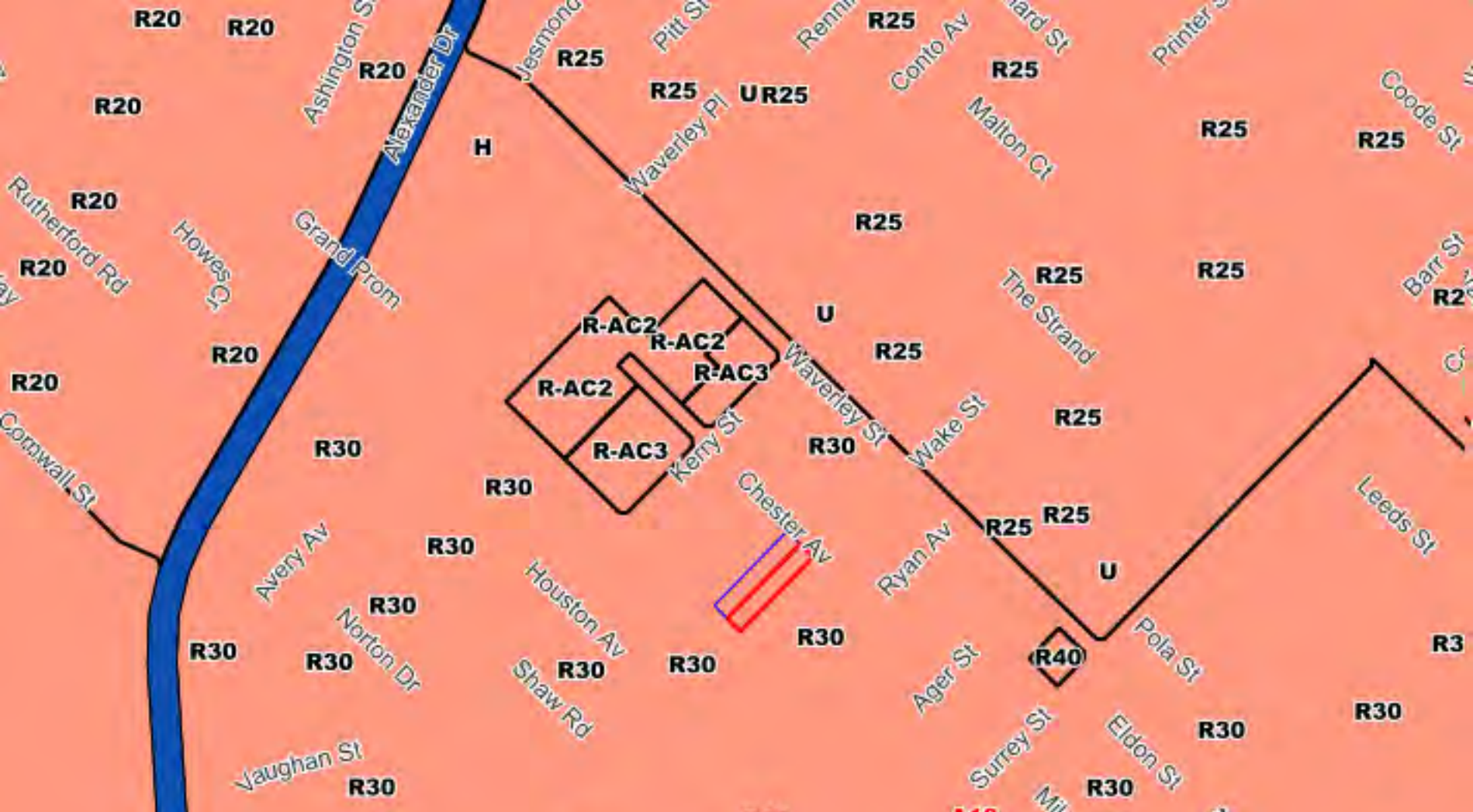

322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING	
COURTYARD ELEVATIONS	
CLIENT: CATHOLIC ARCHDIOCESE OF PERTH	SCALE: AT A1: As indicated
STATUS: IFP-DA	DRAWING NO: A034

Attachment 2 - Aerial Location Plan



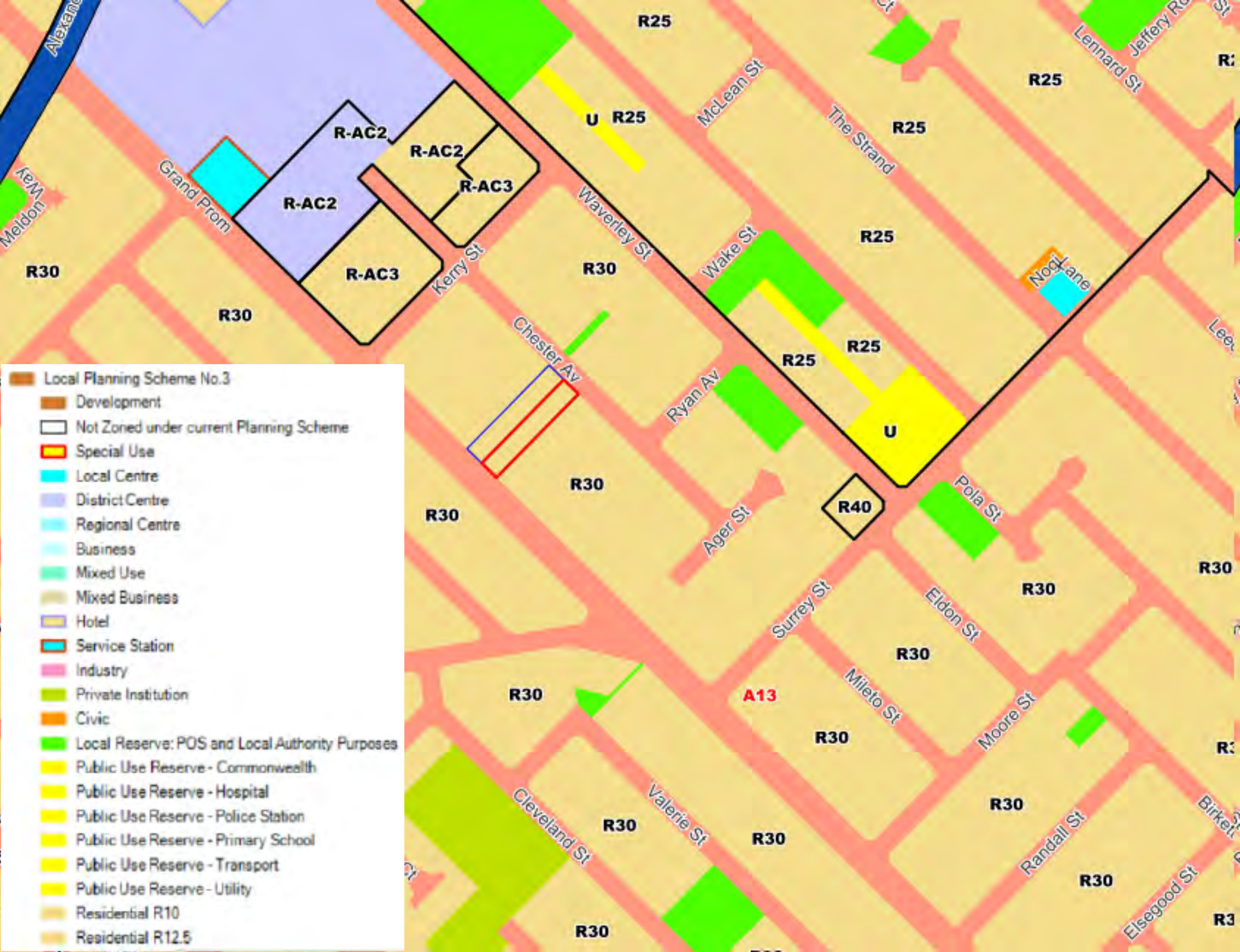
HN 322A
HN 322B

Attachment 3 - Metropolitan Region Scheme Zoning Map



- Region Scheme Zones (MRS)
- Central city area
- Industrial
- Private recreation
- Urban
- Urban deferred

Attachment 4 - City of Stirling Local Planning Scheme No.3 Zoning Map



- Local Planning Scheme No.3**
- Development
 - Not Zoned under current Planning Scheme
 - Special Use
 - Local Centre
 - District Centre
 - Regional Centre
 - Business
 - Mixed Use
 - Mixed Business
 - Hotel
 - Service Station
 - Industry
 - Private Institution
 - Civic
 - Local Reserve: POS and Local Authority Purposes
 - Public Use Reserve - Commonwealth
 - Public Use Reserve - Hospital
 - Public Use Reserve - Police Station
 - Public Use Reserve - Primary School
 - Public Use Reserve - Transport
 - Public Use Reserve - Utility
 - Residential R10
 - Residential R12.5

Attachment 5 - Applicant's Planning Report

322 (LOTS 28 AND 29) GRAND PROMENADE, DIANELLA

13 GROUPED DWELLINGS AND 6 ANCILLARY DWELLINGS

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2 DECEMBER 2025



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PLANNING

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Project Team	1		
Subject Site	2	Appendix B	
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Site Improvements	2		
Surrounding Development	2	Appendix C	
Heritage	2	Architectural Design Report	
Contamination	2		
Bushfire Prone Area	3	Appendix D	
		Landscape Concept Plan C	
Planning Framework	4	Appendix E	
Planning and Development (Local Planning Schemes) Regulations 2015 (Regs)	4	DRP Response	
Metropolitan Region Scheme	4		
SPP 5.4 Road and Rail Noise	4	Appendix F	
SPP 7.0 Design of the Built Environment	4	Acoustic Impact Statement	
SPP 7.3 Residential Design Codes Volume 1	4		
Local Planning Scheme No.3	4	Appendix G	
Draft Local Planning Scheme No. 4	4	R-Codes Assessment	
Planning Policies	4		
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Proposed Development	5	Transport Impact Assessment	
Architectural Design	5		
Landscape Design	5	Appendix I	
Design Review	5	Waste Management Plan	
Planning Assessment	6		
Metropolitan Region Scheme	6		
SPP 5.4 Road and Rail Noise	6		
SPP 7.0 Design of the Built Environment	6		
SPP 7.3 Residential Design Codes Volume 1	6		
Draft Local Planning Scheme No. 4	6		
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Planning and Development (Local Planning Schemes) Regulations 2015 (Regs)	7		
Conclusion	8		

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Introduction

This Development Application Report has been prepared for The Roman Catholic Archbishop of Perth in support of an application for 13 grouped dwellings and 6 ancillary dwellings at Lots 28 and 29 (No. 322) Grand Promenade, Dianella (the subject site).

The Catholic Archdiocese of Perth and SSJG Ministries Inc are seeking to construct 19 homes designed for single mothers with young children and older women at risk of homelessness.

This report will address the relevant matters pertinent to the proposal, including:

- The site details.
- The planning framework.
- The proposed development.
- An assessment under the planning framework.
- Consideration of other factors including, but not limited to acoustic, landscaping, traffic and waste.

Planning Approvals Required

The proposed development has an estimated cost of \$8 million and the Applicant has elected for the application to be determined by the Development Assessment Panel (DAP). This application requires the determination of the Metro Inner Development Assessment Panel.

Key Project Parameters

Address	322 Grand Promenade, Dianella
Lot Numbers	Multiple lot title Lots 28 and 29
MRS Zone	Urban
LPS3 Zone	Residential R30
Draft LPS4 Zone	Residential R30
Number of Dwellings	13 grouped dwellings and 6 ancillary dwellings
Development Value	\$8 million

Project Team

Landowner	The Roman Catholic Archbishop of Perth
Project Director	Adras Pty Ltd
Architect	MDC Architects and Dale Alcock Projects
Landscape Architect	KDLA
Town Planning	PTS Town Planning Pty Ltd
Traffic	Urbii
Acoustic	Lloyd George Acoustics
Waste Management	Urbii

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Subject Site

Site Location and Property Description

The subject site is described as Lot 28 and 29 (No. 322) Grand Promenade, Dianella contained on Certificate of Title Volume 1187 Folio 965 and Diagram 18940. The Certificate of title is a multi-lot title.

An easement burden applies to the site for the purposes of supporting the building erected on Lot 145 on Diagram 36999. This applies to the south-western boundary of Lot 28 and is identified on the site survey.

The site has a total land area of 4970m² and maintains a 41.82 metre frontage to Grand Promenade and a 41.82 metre frontage to Chester Avenue.

The site is generally flat with an AHD height of 27.36 metres AHD in the south-west corner, 27.5 metres AHD in the south-east corner, 26 metres AHD in the north-west corner and 26.79 metres in the north-east corner of the site.

A copy of the current Certificate of Title, Diagram and Easement Burden is included in Appendix A.

Site Improvements

The site is currently vacant with two mature trees located towards the Chester Avenue frontage.

The Grand Promenade frontage includes a bus stop and no street trees. An existing crossover is located on the eastern side of the frontage, where the proposed crossover is to be located.

The Chester Avenue frontage includes three mature street trees and an existing crossover on the eastern side of the frontage, where the proposed crossover is to be located.

Refer to following Aerial Plan.

Surrounding Development

The surrounding area is characterized by residential development with a mix of single and grouped dwellings surrounding the site. There are some multiple dwelling developments also within proximity of the site.

The Dianella Plaza is located approximately 500 metres from the site. Dianella Plaza includes a range of services including supermarkets.

The bus stop (11598 and 11399) on Grand Promenade in front of the site operates bus services 67 and 998/999. Bus 67 connects Mirrabooka to Perth Station. Bus 998 is a circle route connecting Innaloo, Claremont, Fremantle, Bullcreek, Belmont, Morley and the site.

The subject site has good access public open space with several reserves within 200-300 metres of the site.

Heritage

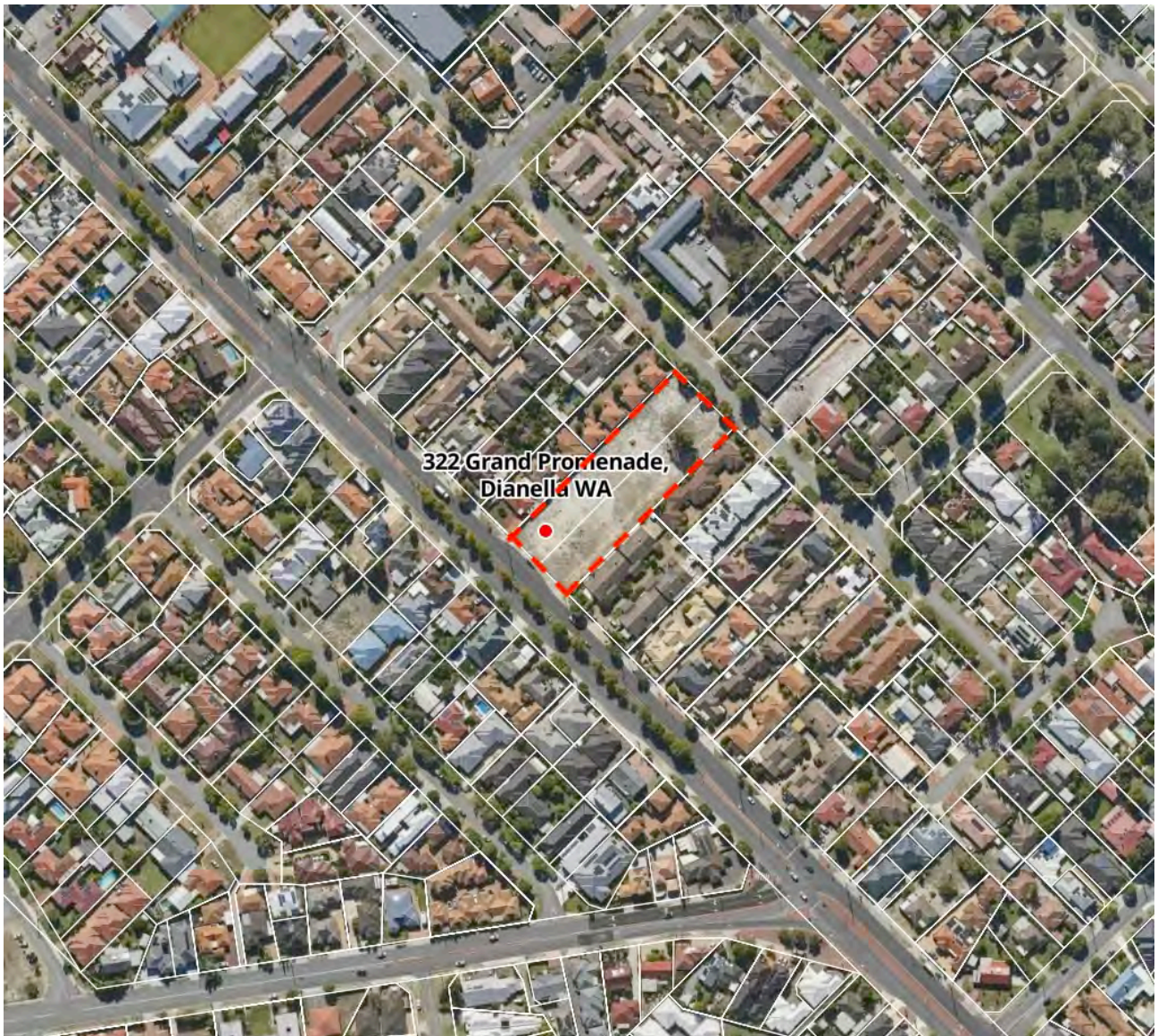
The subject site is not listed on the State Heritage Register and is not listed as a Heritage Place under the City of Stirling Local Planning Scheme No. 3 (LPS3).

Contamination

A search of the contaminated site database does not identify the site as a contaminated site.

Bushfire Prone Planning

The subject site is not located within a bushfire prone area.



Planning Framework

Planning and Development (Local Planning Schemes) Regulations 2015 (Regs)

Clause 67 of Schedule 2 of the Planning Regulations outlines the matters to which the determining authority is to have due regard when considering an application for development approval.

Metropolitan Region Scheme

The subject site is zoned 'Urban' under the Metropolitan Region Scheme (MRS). The site is not affected by any reservations under the MRS.

State Planning Policy 5.4 Road and Rail Noise

SPP 5.4 seeks to minimise the adverse impact of road and rail noise on noise-sensitive land-use and/or development within the specified trigger distance of strategic freight and major traffic routes and other significant freight and traffic routes. Grand Promenade is identified as a road that creates noise.

State Planning Policy 7.0 Design of the Built Environment

The policy identifies the principles to address design quality and built form outcomes. The principles include: Context and character, Landscape quality, Built form and scale, Functionality and build quality, Sustainability, Amenity, Legibility, Safety, Community and Aesthetics.

State Planning Policy 7.3 Residential Design Codes Volume 1

State Planning Policy 7.3 – R-Codes Volume 1 (R-Codes) applies to the site and specifically Parts C and D given the R30 grouped dwellings proposed.

City of Stirling Local Planning Scheme No. 3

The subject site is zoned Residential with a density of R30.

City of Stirling Draft Local Planning Scheme No. 4

Draft LPS4, which is with the WAPC and Minister for Final Approval, maintains the Residential Zoning and Residential Density of R30. Draft LPS4 is a seriously entertained planning consideration.

Planning Policies

The following planning policies are relevant to the proposed development.

- LPP 6.3 Bin Storage Areas.
- LPP 6.12 Public Art on Private Land – Not applicable to grouped dwelling developments.

Proposed Development

This application seeks approval for 13 grouped dwellings and 6 ancillary dwellings around a centrally landscaped communal area.

The three dwellings fronting Grand Promenade (units 1-3) and the three dwellings fronting Chester Avenue (units 11-13) include a two storey two bedroom dwelling and a single storey two bedroom ancillary dwelling. The internal dwellings include five two bedroom dwellings and two single bedroom dwellings. A large communal open space area is located centrally on the site.

Vehicle access is provided from both Grand Promenade and Chester Avenue and use the existing crossover locations. Eight car parking bays are located in the car parking area accessed from Grand Promenade and eight car parking bays are accessed from Chester Avenue. The two car parking areas are secured by gates which are recessed within the driveway to enable cars to be located on the driveway while the gate opens. The driveway enables two way access.

A bin store is located internally with access from Chester Avenue.

The two existing mature trees located on site are retained as part of the proposed development and the proposed development does not impact the three mature street trees along Chester Avenue.

Refer to Appendix B – Development Plans

Architectural Design

The Architectural Design Report is included in Appendix C.

Landscape Design

A Landscape Design Report is included in Appendix D.

City of Stirling Design Review Panel

The design was presented to the City of Stirling Design Review Panel on 9 October 2025.

Appendix E includes a table that provides the DRP's comments and the Applicant's response.

Planning Assessment

Metropolitan Region Scheme (MRS)

The subject site is appropriately zoned Urban under the MRS for the proposed residential development.

State Planning Policy 5.4 Road and Rail Noise

Appendix F provides an Acoustic Assessment against the provisions of SPP5.4.

State Planning Policy 7.0 Design of the Built Environment (SPP7.0)

Appendix C includes the architectural design report.

State Planning Policy 7.2 – Residential Design Codes Volume 1

An assessment of the proposed development under State Planning Policy 7.3 – R-Codes Volume 1 (R-Codes) is included in Appendix G.

The proposed development is generally compliant with the Deemed-to-Comply provisions and where not, have been assessed against the Design Principles.

City of Stirling Draft Local Planning Scheme No. 4

As Draft LPS4 is with the WAPC and Minister for Final Approval, LPS4 may be gazetted during consideration of this application. Draft LPS4 maintains the same Residential Zoning and same Residential Density of R30. Draft LPS4 is a seriously entertained planning consideration.

The proposed development is consistent with the objectives of the Residential Zone given that the development provides for residential development at a density consistent with the R30 density and a range of dwelling sizes to meets the needs of the community and provides a high quality design, built form and streetscape outcome to Grand Promenade and Chester Avenue.

City of Stirling Local Planning Scheme No. 3

The proposed development is consistent with the objectives of the Residential Zone given that the development provides for residential development at a density consistent with the R30 density and a range of dwelling sizes to meets the needs of the community.

Land Use

Grouped dwellings are a 'P' permitted land use in the Residential Zone.

Trees and Development

The proposed development retains the two matures trees on the site and does not impact the mature street trees on the verge.

Local Planning Policies

LPP 6.3 Bin Storage Areas

Urbii has prepared a Waste Management Plan for the proposed development which is contained in Appendix I.

Planning and Development (Local Planning Schemes) Regulations 2015 (Regs)

Clause 67 of Schedule 2 of the Planning Regulations outlines the matters to which the determining authority is to have due regard when considering an application for development approval.

- (a) The proposed development has considered the objectives of the Residential zone in which the site is located and is consistent with the objectives.
- (b) The requirements of orderly and proper planning including built form, access and amenity of surrounding properties has been considered. Draft Local Planning Scheme No. 4 has also been considered.
- (c) The application has considered the relevant State Planning Policies.
- (g) An assessment has been undertaken of the local planning policies
- (m) The assessment considered the compatibility the compatibility of the development with its setting, including the desired character and locality.
- (n) The assessment has considered the amenity of the locality
- (p) The provision of landscaping on site is extensive including retaining mature trees and street trees.
- (s) The site is well located to public transport and the traffic assessment consider the impact of the development on traffic access and servicing.
- (t) The Traffic assessment consider the impact of the amount of traffic likely to be generated by the development.
- (u) A bus stop is located in front of the site that services multiple bus routes, the site can be adequately serviced and good bicycle and pedestrian access is provided to the site.

In giving due regard to the matters under Clause 67 of Schedule 2 of the Planning Regulations the proposed development warrants approval.

Conclusion

The Catholic Archdiocese of Perth and SSJG Ministries Inc are seeking to construct 13 grouped dwellings and 6 ancillary dwellings designed for single mothers with young children and older women at risk of homelessness.

The dwellings have been designed taking into account the proposed tenants and includes a large central communal area.

This report has considered the planning framework that applies to the site and other matters such as built form, car parking, traffic, noise and waste management.

The proposed redevelopment is consistent with the objective of the residential zone and consistent with the planning framework.

The proposed development is an important outcome for the site, locality and community and we respectfully seek support from the City of Stirling and approval by the Metro Inner DAP.

Appendix A

Certificates of Title, Deposited Plan and Easement Burden

Appendix B

Development Plans

Appendix C

Architectural Design Report

Appendix D

Landscape Concept Plan C

Appendix E

DRP Response

Appendix F

Acoustic Impact Statement

Appendix G

R-Codes Assessment

Appendix H

Transport Impact Assessment

Appendix I

Waste Management Plan

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Design Review Panel

Principle	DRP 1 Comments 9/10/25	Applicant Response
Context and Character	The Applicant was encouraged to consider the presentation of the side wall to the street. It was suggested that breaking up the wall and making it appear softer would benefit the design.	This wall must be treated as a party wall/wall on boundary and thus must meet FRL requirements. The two storey component of the walls are located internal to the site with the built form articulated from the single storey to two storey portions of the buildings.
Landscape	The Panel stated it is positive that a Landscape Architect has been engaged to assist with the project, although the central space could be improved, and there is an opportunity for the landscaping to become more creative.	The design has been amended to relocate to the two internal dwellings to the east side of the site, providing a large central communal landscape area that will be viewed from all dwellings. The central space has been redesigned as per below notes, ensuring a diverse and interesting use of space with cost effective elements.
	Comment was made that retention of the tree is supported. It was suggested that the Applicant should consider the detail design and possibly an arborist report to ensure the trees can be protected during construction.	Agreed. This shall be reviewed in Detailed Design Stage.
	The soft landscape area would benefit from a program and increased diversity of spaces. Some areas appear cut up with paths, leaving leftover spaces without a clear function.	The revised plans have focused on braking down the communal landscape area into more clearly defined zones. The Centrally located entertaining area splits the length of the space with active programmed space on one side (an informal nature plan and open kick space) and more passive programmed space on the other (productive garden and an enclosed quiet sitting area).
	It was recommended by the Panel that the solid pavement should be reduced wherever possible.	Permeable paving has been introduced within the landscape areas, bike parking and to some walkways.
	Given the limited car parking, there is an opportunity to adopt a pedestrian-focused, mews-style design. The Panel stated this approach prioritises pedestrian movement and the sense of arrival, creating a safer and more legible circulation network while still accommodating necessary vehicle access in a controlled and minimal manner.	The reposition of the two central units has allowed the majority of the proposed dwellings to connect directly to the central landscape area. The connection of the central space to pedestrian walkways and the carpark allows for an easily navigated/legible entry sequence.

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	It was suggested that breaking up the front garden wall and making it appear softer to the edge of the street would benefit the design outcome.	The design prioritises the amenity of the occupants, planting has been considered, along with the height of street fencing, such that it will engage with the public realm and soften the overall appearance of the development.
	It was noted by the Panel there were multiple plant species which are on the Polyphagous Shot-hole borer list. It is suggested an alternative should be provided for these species.	The suggested palette has been reconciled against the latest PSHB list and updated accordingly.
Built Form and Scale	It was suggested it would be beneficial to provide elevation drawings with detail on material finishes to assist the Panel in reviewing this project.	Noted, the design has been further resolved with elevations now provided in the revised submission.
	The Panel recommended incorporating balconies on the two-storey units which would contribute to delivering a better sense of streetscape activation.	At the client's request, balconies were intentionally left off. We have ensured that widows to upper-level bedrooms are generous and provide ample passive surveillance to the street.
	It was noted by the Panel there could be opportunity to consider moving the play space to the north which may be a better outcome as it is currently overshadowed.	With reference to the revised design, the landscape and play spaces have been re-configured in response to the panel's comments.
	It was suggested by the Panel there is potential to add articulation and improve the streetscape elevations by considering the addition of windows to the upper-floor units which face the ancillary dwellings.	As noted, the upper-level 'blank' walls adjacent to ancillary dwellings will be required to meet FRL requirements for party wall/boundary wall construction.
Functionality and build quality	The Panel noted consideration has been given to the servicing of the development; however, this requires further interrogation to ensure functionality and long-term operational efficiency, including provisions for on-site waste collection.	Refer to Waste Management Plan.
	It was suggested active cooling and heating strategies must be identified, particularly regarding the placement and visual integration of condensers to minimise impact on the built form and public realm.	Revised plans have considered the proposed location of external condenser units, visually screened from public view.
	The Applicant was encouraged to consider alternative layouts as the current layout exposes a lot of backs to paths.	Noted, with revised plans indicating a new site layout.
	It was noted by the Panel that the entry for units 2, 3, 17 and 19 could be flipped to avoid entry through the laundry. Entry directly into the living area would be more welcoming.	These have been amended.
Sustainability	The design appears to integrate multiple passive strategies, such as optimal solar access and cross-ventilation. A consolidated sustainability strategy would be beneficial. Commitments on the inclusion of PV systems, EV infrastructure, energy-efficient fixtures, and other readily implementable sustainability measures should be provided.	The revised proposal includes a PV array per unit, as well as an inclusion of site rainwater collection as well as visible rainwater collection/play features to engage with the communal area.

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	Comment was made by the Panel the Applicant is strongly encouraged to commit to solar energy and consider battery power supply to reduce the cost to residents.	A 1.5kW solar array has been added to each dwelling.
	The Panel stated there is an opportunity to optimise cross ventilation throughout the development.	We consider this has been well captured in the current dwelling siting and layouts.
Amenity	It was recommended by the Panel to consider the presentation of back fences as these could be a detractor.	As noted previously, the revised site layout considers this and ensures there are no 'back fences' presenting to the central communal area.
	It was suggested that the location of the seat located at the front fences could be reviewed and relocated slightly away from the fence.	The small seat at the entry point of units 6-10 has been retained to provide a place to sit and take shoes off, meet neighbours, have a coffee as we feel this enhances the connection to community within the site.
Legibility	It was mentioned by the Panel the paths around units 8 and 13 undermine legibility and could be improved.	Noted, refer above commentary around site reconfiguration.
Safety	It was noted by the Panel the paths behind units 8 and 13 do not support Crime Prevention Through Environmental Design (CPTED).	Noted, refer above commentary around site reconfiguration.
Community	Nil	
Aesthetics	It was noted by the Panel it is essential the material and finishes palette should be developed to reference and complement the existing site character and broader context.	Materials and finishes have been chosen for their longevity, limiting ongoing maintenance to the Church/occupants and providing a quality of housing that does not suggest it is 'social or affordable'
	Comment was made it is essential for a good outcome the blank side walls receive careful attention in their material selection.	Our proposal for these walls is to provide a homogenous appearance to the design, one around form and volume as opposed to individual surface treatment.

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Attachment 6 - City of Stirling Officers Assessment

Attachment 6 – City of Stirling Planning Assessment

1. State Planning Policy 7.0 - Design of the Built Environment (SPP7.0)

SPP 7.0 applies to all development in Western Australia. The purpose of SPP 7.0 is to inform and guide landowners, proponents, designers, reviewers and decision-makers to achieve good design outcomes in the built environment.

The City's summary of the proposed development against SPP7.0 is as follows:

SPP7.0 Design of the Built Environment	
Design Principle	Officer Comment
<p>1.Context and Character</p> <p><i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i></p>	<p>The proposed development demonstrates an appropriate response to both the existing and intended future development context. The provision of one and two storey built form addressing Grand Promenade and Chester Avenue reflects the evolving character of the area and reinforces an urban residential streetscape.</p> <p>The selected colour palette and material finishes are compatible with surrounding development and contribute positively to the locality, supporting a cohesive and integrated sense of place.</p>
<p>2.Landscape Quality</p> <p><i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i></p>	<p>The development incorporates a well considered landscape strategy that enhances the visual amenity and environmental performance of the site.</p> <p>Each dwelling is provided with two small trees, supplemented by additional soft landscaping within street setbacks and communal open spaces to improve streetscape character and resident amenity.</p> <p>The retention of two mature trees, together with the protection of existing street trees, further contributes to the urban canopy and demonstrates an integrated landscape and built form outcome.</p>
<p>3. Built form and scale</p> <p><i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i></p>	<p>The massing and scale of the proposed development are considered appropriate and responsive to the surrounding context. Building are well-articulated and proportioned, reducing perceived bulk and ensuring compatibility with both the existing built environment and the anticipated future character of the area.</p>
<p>4. Functionality and build quality</p> <p><i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life cycle.</i></p>	<p>The development provides safe, legible and efficient access arrangements for pedestrians and vehicles, supported by adequate parking provision.</p>

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SPP7.0 Design of the Built Environment	
Design Principle	Officer Comment
	The layout facilitates intuitive movement throughout the site and supports functional, durable outcomes consistent with long term residential use.
<p>5. Sustainability</p> <p><i>Good design optimises the sustainability of the built environment, delivering positive environmental, social and economic outcomes.</i></p>	<p>The development incorporates sustainable design measures that deliver positive environmental and social outcomes.</p> <p>Passive design responses, energy efficient fixtures and water-wise landscaping reduce resource consumption and enhance occupant comfort.</p>
<p>6. Amenity</p> <p><i>Good design provides successful places that offer a variety of uses and activities while optimising internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable, productive and healthy.</i></p>	<p>The development delivers a high level of internal and external amenity for residents through the provision of private courtyards and well designed communal areas.</p> <p>Adequate natural light and ventilation are achieved, and the scale and bulk of the development are managed to reduce unreasonable impacts on neighbouring properties.</p>
<p>7. Legibility</p> <p><i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i></p>	<p>The development exhibits a clear and legible layout, with well defined pathways, entrances and circulation routes both internally and externally. A clearly identifiable entry is accessible from Grand Promenade and Chester Avenue, supported by a pedestrian connection through the site that facilitates safe and intuitive movement.</p>
<p>8. Safety</p> <p><i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i></p>	<p>The development incorporates passive surveillance opportunities and minimises potential safety risks associated with vehicle movement. Vehicle circulation areas are designed to allow forward exit to the street, with unobstructed sightlines to reduce conflict between vehicles, pedestrians and cyclists. Subject to the recommended conditions, no significant safety issues have been identified.</p>
<p>9. Community</p> <p><i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i></p>	<p>The development contributes positively to housing diversity within the locality through the provision of a mix of dwelling types.</p> <p>Passive surveillance from habitable rooms and active interfaces with communal spaces encourage social interaction and support a safe, inclusive residential environment.</p>

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SPP7.0 Design of the Built Environment	
Design Principle	Officer Comment
<p>10. Aesthetics</p> <p><i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i></p>	<p>The architectural design incorporates articulated façades, varied materials and complementary colour treatments to reduce visual bulk and enhance streetscape appeal. Large openings and alternating finishes provide visual interest and a contemporary architectural expression aligned with the precinct's character.</p> <p>High quality landscaping further softens the built form and enhances overall visual presentation.</p>

7. Planning and Development (Local Planning Schemes) Regulations 2015

The proposed development is required to be considered against the relevant matters listed under *Clause 67 (2) of the Deemed Provisions of the Planning and Development (Local Planning Schemes) Regulations 2015*.

Commentary on the relevant considerations is provided below:

Deemed Provisions Clause 67 – Consideration of application by local government	
Provision	Officer Comment
(a) <i>the aims and provisions of this Scheme and Scheme and any other local planning scheme operating within the Scheme area;</i>	The proposed Residential (Grouped Dwelling development) is consistent with the objectives of the Residential zoning under the LPS3.
(b) <i>the requirements of orderly and proper planning including scheme or amendment to this Scheme that has been advertised under the Planning and Development (Local Planning Schemes) Regulations 2015 or any other proposed planning instrument that the local government is seriously considering adopting or approving;</i>	<p>The City's assessment concludes that the development is consistent with LPS3, and the relevant SPPs and LPPs.</p> <p>Under the City's Draft LPS4 the subject site is proposed to remain Residential R30.</p>
(c) <i>any approved State planning policy;</i>	<p>The development is consistent with SPP 7.0. This assessment is contained in this report.</p> <p>The development is impacted by SPP 5.4 and a Transport Noise Assessment has been prepared by a qualified acoustic consultant outlining the subject to the necessary measures being undertaken, the development should meet compliance with SPP 5.4. Conditions have been recommended to ensure the development satisfies this requirement.</p>
(g) <i>any local planning policy for the Scheme area;</i>	The development is generally consistent with the requirements of the City's Local Planning Policies.
(m) <i>the compatibility of the development with its setting, including –</i>	The massing and scale of the development are considered appropriate for its context.

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Deemed Provisions Clause 67 – Consideration of application by local government	
Provision	Officer Comment
<p>(i) <i>the compatibility of the development with the desired future character of its setting; and</i></p> <p>(ii) <i>the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development.</i></p>	<p>The proposal is limited to one to two storeys, which is compliant. The built form is well articulated and proportioned, ensuring compatibility with both the existing and intended future character of the area.</p>
<p>(n) <i>the amenity of the locality including the following –</i></p> <p>(i) <i>environmental impacts of the development;</i></p> <p>(ii) <i>the character of the locality;</i></p> <p>(iii) <i>social impacts of the development;</i></p>	<p>The proposed development will not have a significant adverse impact on the character of the locality as the built form is in keeping with emerging character of the locality.</p> <p>The proposed development is appropriate within its setting and positively contributes to the area.</p>
<p>(p) <i>Whether adequate provision has been made for the landscaping of the land to which the application relates and whether any trees or other vegetation on the land should be preserved.</i></p>	<p>The proposed development exceeds the required proportion of landscaping and trees on site.</p> <p>The City has recommended appropriate condition setting ensuring this is implemented.</p>
<p>(s) <i>the adequacy of –</i></p> <p>(i) <i>the proposed means of access to and egress from the site; and</i></p> <p>(ii) <i>arrangements for the loading, unloading, manoeuvring and parking of vehicles;</i></p>	<p>Subject to the recommended conditions, the development provides safe and efficient vehicle access and egress. Access is proposed from both Grand Promenade and Chester Avenue.</p> <p>The vehicle access way has been designed to allow vehicles to enter and exit the site in forward gear, with adequate manoeuvring space provided in accordance with Australian Standards.</p> <p>In relation to parking, the development provides sufficient parking to service the proposed development.</p>
<p>(t) <i>the amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system in the locality and the probable effect on traffic flow and safety;</i></p>	<p>The development comprises 13 grouped dwellings and six ancillary dwellings is residential in nature; therefore, high traffic volumes are not anticipated. The Transport Impact Assessment (Urbii, November 2025) estimates the traffic increase is well below the Western Australia Planning Commission's threshold for further analysis and will not affect traffic flow or safety. Sightline truncations and internal design measures ensure safe access and egress.</p>

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Deemed Provisions Clause 67 – Consideration of application by local government	
Provision	Officer Comment
	The proposal is considered acceptable in relation to traffic generation and road network capacity.
(u) <i>the availability and adequacy for the development of the following –</i> (i) <i>public transport services;</i> (ii) <i>public utility services;</i> (iii) <i>storage, management and collection of waste;</i> (iv) <i>access for pedestrians and cyclists (including end of trip</i> (v) <i>access by older people and people with disability;</i>	<p>The proposed development is accessible via a high frequency public transport bus routes (998 and 999) on Grand Promenade, with the closest bus stop located abutting the subject site.</p> <p>The development provides communal vehicle parking bays.</p> <p>Utility services are available to the development.</p> <p>A Waste Management Plan has been provided and meets the City's requirements.</p> <p>The development provides pedestrian access from the entrance of the site. The development accommodates access. The development provides 'at grade' car parking and universal access design features throughout the site.</p>
(y) <i>any submissions received on the application;</i>	The proposal does not trigger the need for public consultation.
(za) <i>the comments or submissions received from any authority consulted under clause 66;</i>	Consultation to relevant authorities such as Water Corporation has been undertaken as detailed earlier in the report under 'External Referral Agencies'.

Draft Local Planning Scheme No. 4 (LPS4)

The City has prepared new Local Planning Scheme No.4 (LPS4) to replace Local Planning Scheme No.3 (LPS3). On 10 June 2025 at its Ordinary Meeting, Council considered a report on the outcomes of advertising of draft LPS4 and resolved to support it, subject to modifications. On 18 June 2025, draft LPS4 was accepted by the Department of Planning, Lands and Heritage for final assessment and determination.

In relation to draft LPS4, a decision-maker can give due regard to a seriously entertained planning proposal when determining an application, in accordance with Schedule 2 Clause 67(2)(b) of the *Planning and Development (Local Planning Schemes) Regulations 2015*. The City acknowledges the subject site is proposed to remain Residential R30 (see Figure 1).

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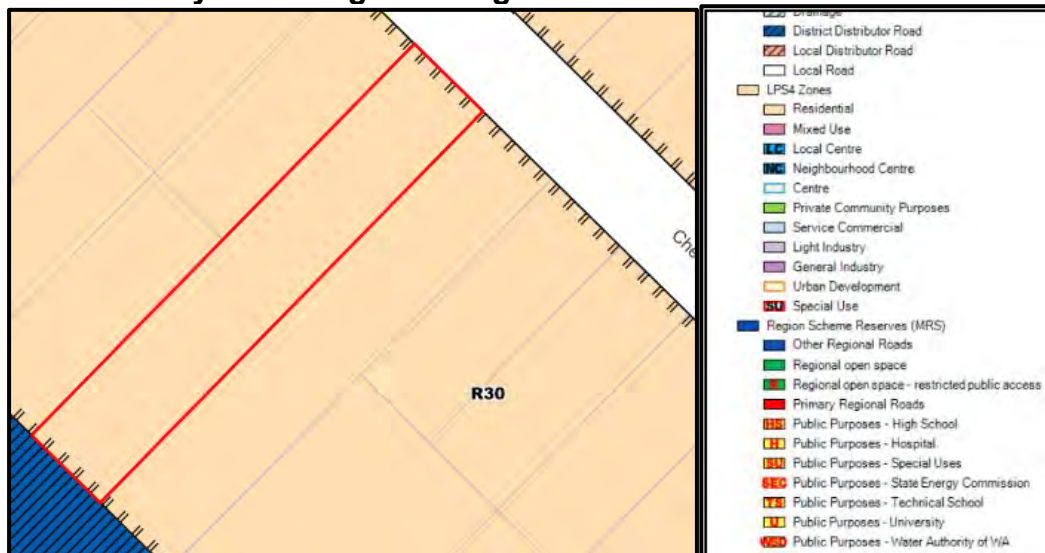


Figure 1 Draft LPS4 Scheme Map and Zoning Legend

Part C – Residential Design Codes (R-Codes)

The proposed development integrates substantial trees and landscaping throughout the site in accordance with the requirements ensuring the delivery of an enhanced landscape outcome and increased canopy across the development.

Section 1.0 - The Garden

1.2 Trees and Landscaping

C1.2.4 A minimum number of trees to be planted in accordance with Tables 1.2a and provided with the required deep soil area per tree in accordance with Table 1.2b.

Dwelling Type	Minimum tree requirements	
Single house (per dwelling)	1 small tree	Where the primary street setback is 1.5m or greater: - frontages less than 20m: 1 small tree in the primary street setback area; or - frontages 20m or greater: 1 small tree in the primary street setback area per 10m frontage ² .
Grouped dwellings (per dwelling)	1 small tree OR 2 small trees where primary garden area is reduced in accordance with C1.1.2	

Figure 2 Minimum tree requirements as per R-Codes

- Minimum number of trees for the whole development = 25 small trees
 - Four small trees at each street frontage, totalling eight small trees.
 - An additional small tree per dwelling, 13 small trees
 - Four small trees in the car parking area
- Proposed 80 trees, including the retention of two mature trees which fall under the category of medium trees has per table 1.2b tree size and deep soil area.

Section 3 – Neighbourliness

3.3 Street Setbacks.

C3.3.1 Buildings are set back from the street boundary in accordance with Table 3.3a.

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Street type	R30	R35	R40	R50	R60	R80
Primary street	4m	4m	3m	2m	2m	2m
Secondary street	1.5m	1.5m	1m	1m	1m	1m
Adjoining communal street	0.5m					
Adjoining laneway or right-of way where it is the primary street to the dwelling ¹	2m					
Adjoining laneway or right-of way	0.5m					

R80 Code standards apply to single houses and grouped dwellings in areas coded R100, R160 and RAC.

Where road widening is required, street setbacks are to be calculated from the adjusted street boundary.

¹ Does not apply to ancillary dwellings.

Figure 3 Minimum street setback requirements as per R-Codes

Dwelling 3A and 13 have nil setbacks from the communal street in lieu of the required 0.5m.

Although Dwellings 3A and 13 propose a nil setback to the communal street (in lieu of the required 0.5m), the variation is considered acceptable having regard to the lot configuration and the function and character of the internal street. The communal street operates as a low-speed residential environment with a pedestrian path and landscape treatment along the street edge, and the nil setback assists in defining a clear and consistent edge to the internal street while retaining functional private outdoor areas behind the wall. The nil setback does not compromise pedestrian access, safety clearances, servicing or essential infrastructure requirements.

Given the nil setback, the design response focuses on minimising visual impact to that portion of the communal street and pedestrian route. The wall is to be finished to an equivalent standard to the remainder of the development. On balance, the proposal provides an appropriate response to the internal street context and achieves the intent of Design Principles P3.3.1 and P3.3.2, notwithstanding the variation to the minimum 0.5m communal street setback.

3.4 - Lot boundary setbacks

C3.4.4 Boundary walls may be built in accordance with Table 3.4b provided:

- i. boundary walls are located behind the street setback;
- ii. overshadowing does not exceed the limits of C3.9.1, C3.9.2 and C3.9.3; and
- iii. they are finished to an equivalent standard to the rest of the development.

R-Coding	Maximum boundary wall height	Maximum boundary wall length
R30 – R35	3.5m	Maximum two-thirds the length of the lot boundary the wall abuts, measured from behind the street setback line. Applicable up to two lot boundaries.
R40 and above	3.5m	Maximum two-thirds the length of the lot boundary the wall abuts, measured from behind the street setback line. Applicable to all lot boundaries.

Figure 4 Lot boundary setback requirements as per R-Codes

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Variation proposed:

- Unit 1A and 11A have a portion of their boundary wall exceeding the maximum 3.5m (see Figure 5).

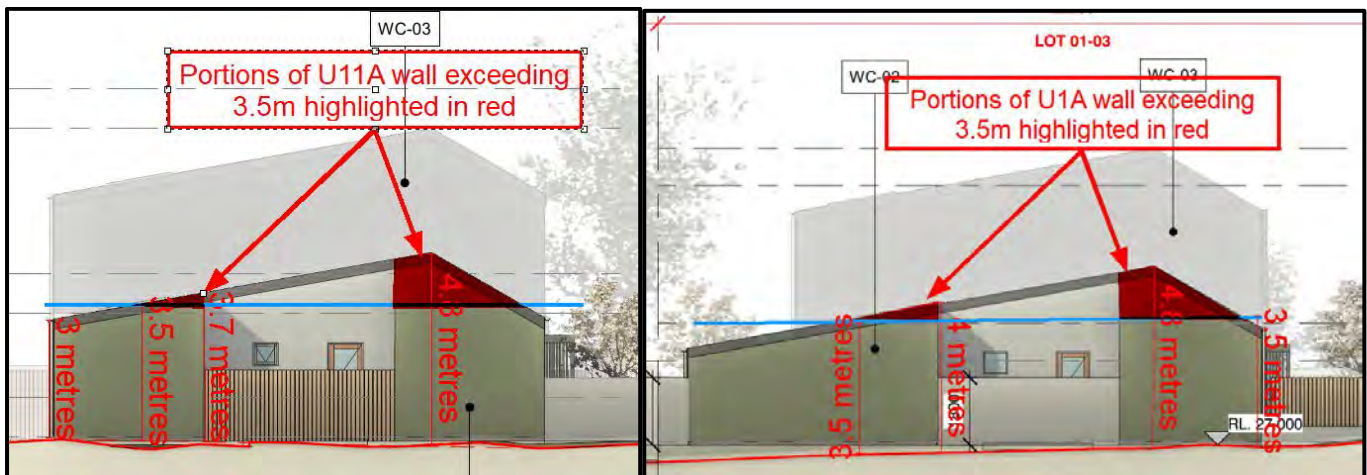


Figure 5 portions of wall exceeding the permitted boundary wall height

Notwithstanding the proposed variation, the development is considered to satisfy the intent of Design Principles P3.4.1 to P3.4.4. The varied portions of boundary wall are limited in extent and location, are positioned behind the street setback and do not adversely present to the street, reinforcing the established streetscape character and remaining consistent with the existing and desired built form local character.

The proposal also maintains an appropriate transition to adjoining residential properties, noting the variation relates only to a portion of the boundary wall rather than the whole boundary wall. Unit 1A’s boundary wall abuts the neighbour’s driveway and existing boundary wall as depicted in Figure 6, where the adjoining interface is already characterised by hardstand and boundary treatment rather than active outdoor living areas, noting this the extent of neighbour impact can be considered acceptable.

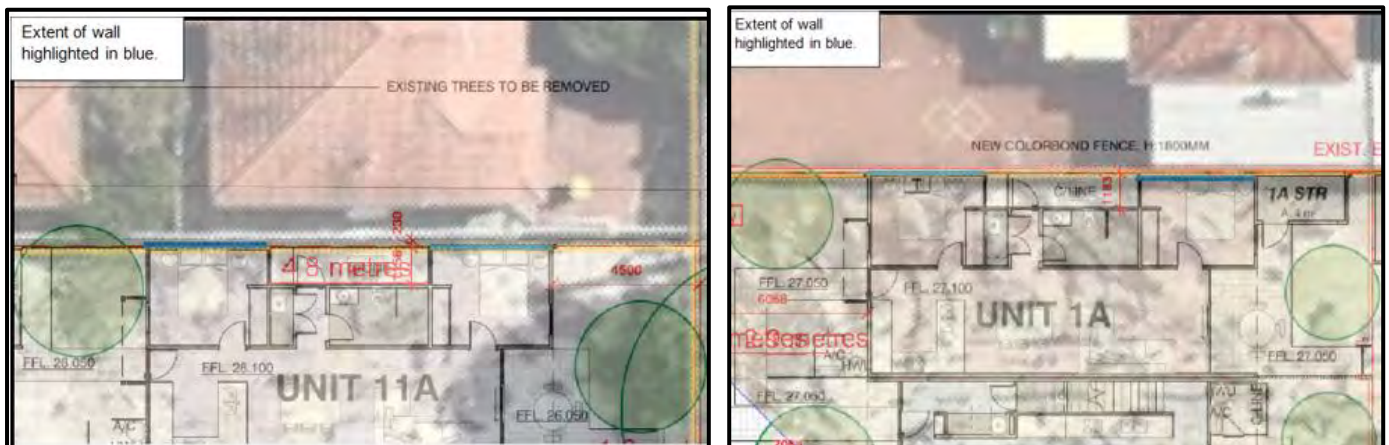


Figure 6 portions of wall abutting adjoining properties (326 Grand Promenade and 21 Chester Avenue)

In terms of amenity, the varied boundary wall elements do not result in unreasonable impacts to solar access, natural ventilation or privacy, with the relevant portions arranged to avoid overlooking. The sections of wall where the variation is proposed for Unit 11A does not abut active habitable spaces as depicted in Figure 6, further reducing potential amenity impacts. Additionally, the wall is located along the north-western boundary and the resulting impact to solar access on neighbouring properties is considered acceptable.

The limited boundary wall elements enable more efficient site planning outcomes by consolidating built form and retaining functional primary garden areas, and importantly the wall has been broken up such that only a

Attachment 6 – City of Stirling Planning Assessment

portion as depicted in Figures 5 and 6, exceeds the 3.5m maximum length, which reduces perceived bulk and assists in limiting impacts on neighbouring properties. On balance, the proposal achieves an appropriate built form outcome and satisfies the intent of the design principles.

3.6 - Streetscape

C3.6.3 *For single houses and grouped dwellings, front doors to be protected from the weather (for example by a porch, verandah, building over or similar), with a minimum dimension of 1.2m (refer Figure 3.6a).*

Variation proposed:

- Dwellings 1A, 2A and 3A - porch with 1m x 2.7m
- Dwellings 4 & 5 have no porch
- Dwellings 11A, 12A & 13A – porch with 1m x 2.7m

Whilst some dwellings do not meet the deemed to comply requirements of C3.6.3, the proposal is considered to satisfy the intent of the Design Principles. Dwellings 1A, 2A, 3A, 11A, 12A and 13A provide covered porch structures which, while having a reduced minimum dimension of 1.0m in depth, offer effective weather protection to dwelling entries and clearly define the building frontage. Dwellings 4 and 5 do not incorporate traditional porch structures; however, the pedestrian access to the dwelling entry is positioned directly to a communal undercover area which allows for weather protection.

Attachment 7a - Applicant's Response to Request for Further Information

City of Stirling
18 Mar 2026
RECEIVED

Our Ref: J000533
Your Ref: DA25/1830
DAP Ref: DAP/25/03027



18 March 2025

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ABN - 32 603 168 850

Dear Meron

322A AND 322B (LOTS 28 AND 29) GRAND PROMENADE, DIANELLA – RESPONSE TO REQUEST FOR FURTHER INFORMATION

We refer to your letter dated 20 February 2026 and have enclosed amended plans to address the matters raised and our responses below.

Restrictions on Title

The plans have been amended to include the easement burden which has resulted in the building and storerooms being moved off the boundary to reflect the easement burden. It is noted that the easement is very small and reflects the brick encroachment on the adjoining property. The amendments to the plans has been enabled by an alternate party wall design which reduces the width of the party wall enabling the building to be moved off the boundary to reflect the easement.

In relation to the easement and the construction methodology, this will be further developed during detailed design, and it is noted that there is an obligation to both maintain the easement and to protect the adjoining wall during construction.

Part C of the Residential Design Codes

3.2.1 - Clause 1.2 Trees and landscaping

We acknowledge the provision for lighting to pathways and vehicle access areas within the communal street to ensure safe and legible movement throughout the site. The detailed lighting plan for the site will form part of the next stage of design being the detailed design stage and therefore we consider it appropriate to condition any approval to require a lighting plan.

3.2.2 - Clause 2.1 Size and layout of dwellings

The plans have been amended to show circulation spaces of 900mm in front of the storerooms. The storeroom doors open on the greater dimension, and a note has been added to the individual lot plans to state that the storerooms with a reduced dimension are to include doors for a minimum 80 per cent of the length.

3.2.3 - Clause 2.2 Solar access and natural ventilation

The plans have been amended to include the sill height and head height of the windows and doors to show the size of the windows. The elevations show the size of the windows. Additionally, as some of the windows are obscured by fencing or other structures on the elevations and a note has been added to the individual lot plans to state that all habitable rooms windows to exceed 10% of the habitable internal room floor area.

3.2.4 - Clause 2.4 Parking: Visitor parking

The R-Codes do not require any occupant parking and 5 visitor bays. The development plans provide 6 visitor bays. Of the occupant bays, 5 are allocated to specific units and 3 bays will be unallocated occupant bays. The various bays will be signposted.

Given the compliance with the R-Codes, we do not see a need for a car parking management plan.

3.2.5 - Clause 3.7 Access

The plans have been amended to provide a turnaround bay on each side of the car parking areas.

4.1 Engineering Services

The plans have been amended to provide a side setback of 1.15 metres to the eastern side boundary from the Grand Promenade crossover.

With respect to the width of the crossover, the plans have been amended to provide a 5.31 metre width. We have obtained advice from the project traffic consultant who advises that:

"I acknowledge the City's advice regarding the need to offset the crossover from the side boundary to address sightline constraints associated with the boundary wall. I also acknowledge that, under the R-Codes, a minimum driveway width of 5.5 m at the street boundary is typically sought where access is provided to a Primary Distributor or Integrator Arterial Road to enable a passing opportunity at the frontage.

From an operational perspective, I consider a crossover width of 5.325 m to be acceptable in this instance.

For reference, AS/NZS 2890.1 indicates that, for a Class 1A parking facility serving fewer than 25 parking spaces on an arterial frontage road, an Access Facility Category 1 driveway applies. Under Table 3.2, the corresponding combined driveway width is 3.0 m to 5.5 m. On that basis, a 5.31 m crossover remains within the AS2890.1 range for a compliant combined access driveway.

Accordingly, while the proposal does not achieve the 5.5 m width typically sought under the R-Codes at the street boundary, it is my view that a 5.31 m crossover should still be sufficient to allow two passenger vehicles to pass one another at low speed within the access throat, particularly given the shortfall is minor.

Attachment 7b - Landscaping Plans prepared by Kdla Kelsie
Davies Landscape Architecture

GENERAL NOTES:
 1. THIS IS A CONCEPT PLAN ONLY
 2. ALL STRUCTURES SUBJECT TO ENGINEERING AND COUNCIL APPROVAL
 3. ALL MEASUREMENTS TO BE CHECKED PRIOR TO CONSTRUCTION

REV	DATE	DWN	APP	DESCRIPTION
A	08.09.25	ALC	KD	LANDSCAPE CONCEPT DESIGN
B	16.09.25	ALC	KD	LANDSCAPE CONCEPT DESIGN
C	07.11.25	ALC/KD	KD	LANDSCAPE CONCEPT DESIGN - UPDATED
D	14.11.25	AC	KD	LANDSCAPE CONCEPT DESIGN - UPDATED



LEGEND TREES

- EXISTING TREES TO BE REMOVED
- EXISTING TREES TO BE RETAINED
- SELECTED LARGE NATIVE TREES
 EG. COASTAL BLACKBUTT
 CANOPY: 8m
- SELECTED MEDIUM FEATURE TREES
 EG. SILVER PRINCESS
 CANOPY: 6m
- SELECTED MEDIUM FEATURE TREES
 EG. CORAL GUM
 CANOPY: 5m
- SELECTED MEDIUM TREES
 EG. FUSHIA GUM
 CANOPY: 4m
- SELECTED SMALL TREES
 EG. BANKSIA INTERFOLIA
 CANOPY: 4m
- SELECTED SMALL TREES
 EG. NATIVE FRANGIPANI
 CANOPY: 4m
- SMALL NATIVE CARPARK TREES
 EG. LITTLE GHOST GUM
 CANOPY: 4m
- SELECTED SMALL TREES
 EG. CITRUS TREE / FEIOA
 CANOPY: 3m
- SELECTED SMALL TREES
 EG. HAKEA
 CANOPY: 3-4m

FEATURE PLANTS

- SELECTED FEATURE PLANTING
 EG. GRASS TREE, ROO PAWS, BANKSIA MENZI DWARF
- SELECTED CLIMBER ON TRELLIS

PLANTING

- VERGE PLANTING (3/M2)
 LOW NATIVE GROUNDCOVERS
- PLANTING TYPE 01
 GROUNDCOVERS
- PLANTING TYPE 02
 SHRUBS
- PLANTING TYPE 03
 STRAPPY/MIXED
- PLANTING TYPE 04
 SHADE PLANTING
- PLANTING TYPE 05
 VEGETABLE PLANTING
- SELECTED LAWN SPECIES
 EG. SOFT LEAF BUFFALO
- ORGANIC MULCH
 EG. WOODCHIP MULCH

PAVEMENTS

- DRIVEWAY (BY BUILDER)
 TYPE TBC
- PEDESTRAIN PAVING (BY BUILDER)
- PERMEABLE PAVING (BY BUILDER)
- INFORMAL PATHS AGGREGATE MULCH
 EG. S.W. RIVERSTONE (BY BUILDER)
- SELECTED STEPPERS
 EG. ORGANIC ROUNDS

LEGEND MISCELLANEOUS

- SELECTED COMMUNAL FURNITURE
 TIMBER/ RAMMED EARTH BENCH SEATS
- INFORMAL NATURE PLAY ITEMS
 EG. LOGS, STEPPERS, TEE PEE
- BOULDERS
 EG. GRANITE / LIMESTONE
- RAISED CORRUGATED ALUMINUM VEGETABLE PLANTERS (BY BUILDER)
- SELECTED TURF EDGE RESTRAINT (BY BUILDER)
- RAINTANKS WITH AUTOTAPS
 EG. PIONEER WATERTANKS GT10: 12,200L

LANDSCAPE CALCULATIONS

Lot/Unit	Lot Area	Landscaped Area	FSA	FSA Landscaping	No. of Trees
1A	146m ²	36m ² (25%)	40m ²	29m ² (73%)	2
1	114m ²	26m ² (23%)	35m ²	25m ² (71%)	2
2	114m ²	36m ² (32%)	35m ²	25m ² (71%)	2
2A	146m ²	39m ² (27%)	40m ²	34m ² (85%)	2
3	114m ²	37m ² (32%)	35m ²	25m ² (71%)	2
3A	146m ²	44m ² (30%)	40m ²	28m ² (70%)	2
4	170m ²	77m ² (45%)	N/A	N/A	3
5	170m ²	77m ² (45%)	N/A	N/A	3
6	170m ²	50m ² (29%)	N/A	N/A	2
7	247m ²	108m ² (44%)	N/A	N/A	3
8	247m ²	108m ² (44%)	N/A	N/A	3
9	247m ²	108m ² (44%)	N/A	N/A	3
10	247m ²	108m ² (44%)	N/A	N/A	4
11A	146m ²	42m ² (29%)	29m ²	15m ² (50%)	2
11	114m ²	26m ² (23%)	34m ²	21m ² (61%)	2
12A	114m ²	26m ² (23%)	29m ²	15m ² (50%)	2
12	146m ²	43m ² (29%)	34m ²	21m ² (61%)	2
13A	146m ²	42m ² (29%)	29m ²	15m ² (51%)	3
13	114m ²	25m ² (22%)	34m ²	21m ² (61%)	2
Common Area	1846m ²	703m ² (38%)	N/A	N/A	34
Total:	4955m²	1773m² (35%)	414m²	(274m²) 66%	80

REFER TO PAGE 100 FOR PLANTING SCHEDULE, IMAGES AND NOTES

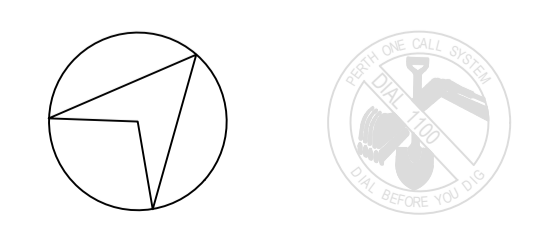
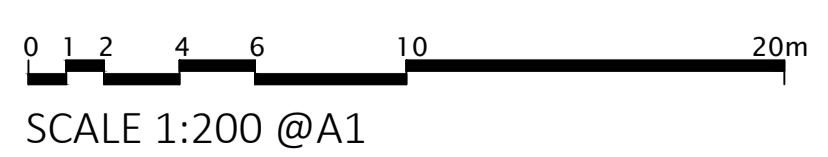
THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION UNLESS APPROVED BY CLIENT AND REVISED 'O' ISSUED FOR CONSTRUCTION

City of Stirling
 3 December 2025
 RECEIVED



DALE ALCOCK PROJECTS
 Karrynup WA 6018
 mob: 0450 965 569
 email: kelsie@kda.com.au

JOB No. 0590 PAGE 101 REV D



DIANELLA WOMENS GROUPED HOUSING LANDSCAPE CONCEPT PLAN

DALE ALCOCK PROJECTS
 322 GRAND PROMENADE, DIANELLA

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SELECT PLANTING IMAGES
TREES



SHRUBS



NATIVE GRASSES



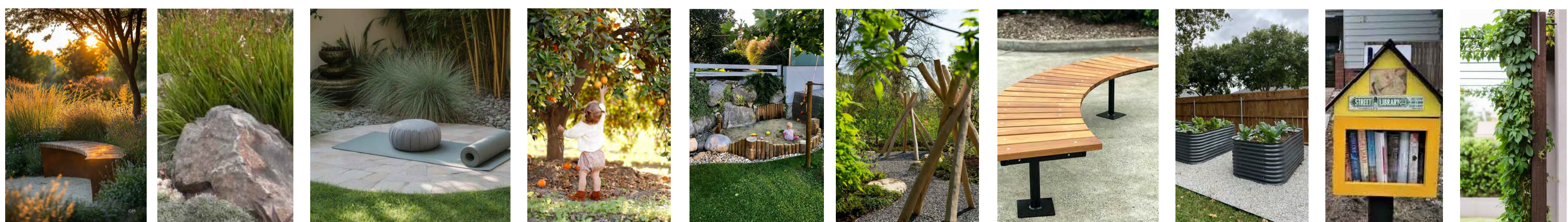
SHADE PLANTING



GROUNDCOVERS & CASCADING



LANDSCAPE INSPIRATION AND ELEMENTS



REV	DATE	DWN	APP	DESCRIPTION
A	08.09.25	ALC	KD	LANDSCAPE CONCEPT IMAGES
B	07.11.25	AC	KD	LANDSCAPE CONCEPT IMAGES

PLANTING PALETTE

Symbol	Species	Common Name	Quantities	Size
Trees:				
ACAsal	Acacia saligna	Peppa	As Shown	100L
BANint	Banksia integrifolia	Coast Banksia	As Shown	100L
CITlim	Citrus limon	Lemon	As Shown	100L
CITsin	Citrus sinensis	Washington Navel	As Shown	100L
EUCcae	Eucalyptus caesia	Silver Princess	As Shown	100L
EUCfor	Eucalyptus forrestiana	Fuchsia Gum	As Shown	100L
EUCtod	Eucalyptus teretifolia	Coastal Blackbutt	As Shown	100L
EUCvic	Eucalyptus vicifolia	Little Ghost Gum	As Shown	100L
HAKlau	Hakea laurina	Pin Cushion Hakea	As Shown	100L
HYMfla	Hymenopospermum flavum	Native Frangipani	As Shown	100L
Groundcovers and Cascading:				
ACAsal	Acacia saligna prostrate	Springtime Cascade Wattle	2/m ²	140mm
ADEcun	Adenanthos cuneolatus	Coral Carpet	2/m ²	140mm
CAScou	Casuarina glauca 'Cousin It'	Cousin It	2/m ²	140mm
CITlim	Citrus limon	Silver Falls	2/m ²	140mm
EREblu	Eremophila 'Blue Horizon'	Blue Horizon	2/m ²	140mm
GREgin	Grevillea 'Gin Gin Gem'	Gin Gin Gem	2/m ²	140mm
HEMpun	Hemandra pungens	Snake Bush	2/m ²	140mm
KENpro	Kennedia prostrata	Running Postman	2/m ²	140mm
HIBsca	Hibbertia scandens	Snake Vine	2/m ²	140mm
MYOpar	Myoporum parvifolium 'Yareena'	Yareena	2/m ²	140mm
ROSOFF	Rosmarinus officinalis 'Prostratus'	Creeping Rosemary	2/m ²	140mm
VIOhed	Viola hederacea	Native Violet	3/m ²	200mm
Grasses, Herbs, Rushes & Sedges:				
CONcan	Conostylis candidans	Grey Cottonheads	3/m ²	140mm
DIAeme	Dianella tasmanica 'Emerald Arch'	Emerald Arch	3/m ²	140mm
DIArev	Dianella revoluta	Blue flax lily	3/m ²	140mm
LIRjus	Liriodendron taylorii	Just Right	3/m ²	200mm
LOMTan	Lomandra 'Tanika'	Tanika	3/m ²	200mm
LOMYa	Lomandra Nyssia	Nyssia	3/m ²	200mm
Shade Planting:				
ALJrep	Ajuga reptans	Bungle weed	3/m ²	140mm
OREB	Oreocarya 'Dusky Bell'	Dusky Bell	3/m ²	140mm
DARpat	Darwinia chinodora	Darwinia	2/m ²	140mm
DICrep	Dicentra repens	Kidney plant	3/m ²	140mm
LIRjus	Liriodendron taylorii	Just Right	3/m ²	140mm
MYOpar	Myoporum parvifolium 'Yareena'	Yareena	3/m ²	200mm
OPHjap	Ophiopogon japonicus	Mondo Grass	3/m ²	200mm
VIOhed	Viola hederacea	Native Violet	3/m ²	200mm
Feature Plants:				
ANifla	Anemone flava	Kangaroo Paw	As Shown	12L
BANmen	Banksia menziesii 'dwarf'	Menzies Banksia Dwarf	As Shown	12L
STRreg	Struthio reginae	Bird of Paradise	As Shown	12L
XANpre	Xanthorrhoea preissii	Grass Tree	As Shown	MTP
ZAMfur	Zamia furfuracea	Cardboard Palm	As Shown	12L
Climbing Plants:				
HARwhi	Hardenbergia violacea 'White Out'	White Native Wisteria	As Shown	140mm
TRVas	Trachelospermum jasminoides	Sar Jasmine	As Shown	140mm

NOTES

- GENERAL
 - ALL SCALES ARE AS NOTED AND TO SUIT A1 PAPER SIZE
 - THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION UNLESS REVISED 'D' ISSUED FOR CONSTRUCTION AND SIGNED AND APPROVED BY PROJECT MANAGER/SUPERINTENDENT.
 - PLANTING SETOUT SHOULD BE CHECKED BY SUPERINTENDENT BEFORE INSTALLATION BEGINS.
- SOIL PREPARATION
 - ALL AREAS ARE TO BE FINE GRADED EVENLY TO CONFORM TO KERB LEVELS AND SURROUNDING FINISHES.
 - SURFACES SHALL BE FREE FROM DEPRESSIONS, IRREGULARITIES AND NOTICEABLE CHANGES IN GRADE. GENERALLY, GRADES SHALL DEVIATE IN LEVEL NO GREATER THAN 20mm IN ONE LINEAR METRE.
 - PLANTED AREAS SHALL BE SPREAD WITH MIN. 50mm OF APPROVED STANDARD SOIL CONDITIONER THAT SHALL BE RIPPED INTO EXISTING SOIL TO A MIN. DEPTH OF 200mm.
 - RAISED PLANTER AREAS AND POTS SHALL BE INSTALLED WITH APPROPRIATE DRAINAGE CELL, AGGREGATE AND GEOTEXTILE MEMBRANE BELOW SOIL.
 - ALL SITE AND IMPORTED SOILS, POTTING MIX, SOIL CONDITIONERS AND MULCHES TO BE IN ACCORDANCE TO RELEVANT AUSTRALIAN STANDARDS.
- PLANTING
 - PLANTED AREAS SHALL BE MULCHED WITH AN ORGANIC (WOODCHIP) MULCH UNLESS OTHERWISE STATED TO A MINIMUM DEPTH OF 75mm.
 - ADVANCED TREES SHALL BE STAKED W/ 50x50mm DIA HARDWOOD POSTS. POSTS SHALL BE PAINTED BLACK AND INSTALLED TO A MIN DEPTH OF 500mm. TREES SHALL BE SECURED TO POLES W/ RUBBER TIES IN FIGURE 8.
 - TREES PLANTED WITH IN 1000mm OF BOUNDARY WALLS AND/OR PARKING AREAS SHALL BE INSTALLED WITHIN 600mm DEPTH NYLEX ROOT BARRIER MEMBRANE. MEMBRANE SHALL BE INSTALLED AS PER MANUFACTURERS RECOMMENDATIONS.
 - FINAL PLANTING SHALL BE SELECTED FROM PLANTING PALETTE SCHEDULE.
 - PLANTS TO BE SET OUT IN EVEN SPACING TO FILL THE DESIGNATED AREAS.
 - IN AREAS OF MIXED PLANTING, SPECIES TO BE SPREAD OUT AT RANDOM, IN GROUPINGS OF 2 OR 3.
 - PLANTS SHALL BE SUPPLIED FROM AN INDUSTRY ACCREDITED WHOLESALE NURSERY. PLANTS SHALL BE IN APPROPRIATE SIZE FOR THE LISTED POT SIZE AND IN GOOD HEALTH.
- IRRIGATION
 - PLANTING TO GROUND LEVEL TO BE IRRIGATED VIA A FULLY AUTOMATIC SYSTEM FROM MAINS.
 - WATER PRESSURE TO HAVE A MINIMUM FLOW RATE OF 30L/m² AT 300kPa FROM THE WATER CONNECTION POINT (OR AS STIPULATED)
 - CONTROLLER TO BE LOCATED IN SERVICE ROOM (OR AS SHOWN ON IRRIGATION DETAILS).
 - SLEEVES BENEATH PAVED SURFACES AND TO RAISED PLANTING AREAS TO BE PROVIDED BY OTHERS.
 - IRRIGATION TO GARDEN BEDS TO BE NETAFIM TECHLINE, SUB SURFACE IRRIGATION. INSTALLED TO MANUFACTURERS SPECIFICATION. IRRIGATION TO TURF TO BE POP UP SPRINKLERS; MP ROTATORS OR SIMILAR. IRRIGATION TO TREES TO BE BUBBLERS; TORO FLOOD BUBBLERS OR SIMILAR.
 - ASCON DRAWINGS, MANUALS AND 12 MONTH WARRANTY SHALL BE SUPPLIED BY THE IRRIGATION CONTRACTOR TO THE CLIENT UPON PRACTICAL COMPLETION.
 - PLEASE REFER TO IRRIGATION DRAWING SET FOR FINAL LAYOUT AND SCHEDULE (TO FUTURE DETAIL).



Attachment 7c - Transport Impact Statment prepared by Urbii



urbii

Sustainable Transport. Safe Solutions

City of Stirling
3 Dec 2025
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322 Grand Promenade, Dianella

Proposed Residential Development

TRANSPORT IMPACT STATEMENT



Prepared for:
Dale Alcock Projects

November 2025

322 Grand Promenade, Dianella

Prepared for: Dale Alcock Projects
Prepared by: Paul Ghantous
Date: 20 November 2025
Project number: U25.090

City of Stirling
3 Dec 2025
RECEIVED

Version control

Version No.	Date	Prepared by	Revision description	Issued to
U25.090.r01	18/09/25	Paul Ghantous	FINAL	Dale Alcock Projects
U25.090.r01a	20/11/25	Paul Ghantous	REVISED FINAL	Dale Alcock Projects



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1 Introduction

This Transport Impact Statement has been prepared by **Urbii** on behalf of **Dale Alcock Projects** with regards to the proposed residential development, located at **322 Grand Promenade, Dianella**.

The subject site is situated on the north-eastern side of Grand Promenade and extends to Chester Avenue, as shown in Figure 1. The site is presently vacant (Figure 2) and is surrounded by a mix of residential, education, commercial and retail land uses. Dianella Plaza Shopping Centre is accessible within 500m walking distance to the north.

It is proposed to develop the site into a residential development, delivering 19 grouped dwellings.

The key issues that will be addressed in this report include the traffic generation and distribution of the proposed development, access and egress movement patterns, car parking and access to the site for alternative modes of transport.



Figure 1: Subject site



Figure 2: Existing site

Source: Google Streetview Image

2 Scope of work

The WAPC *Transport Assessment Guidelines 2016* identifies the proposed development as being “moderate impact” (Figure 3). A Transport Impact Statement (TIS) has been prepared to support a robust Development Application and to assist the LGA with demonstration of traffic impact.

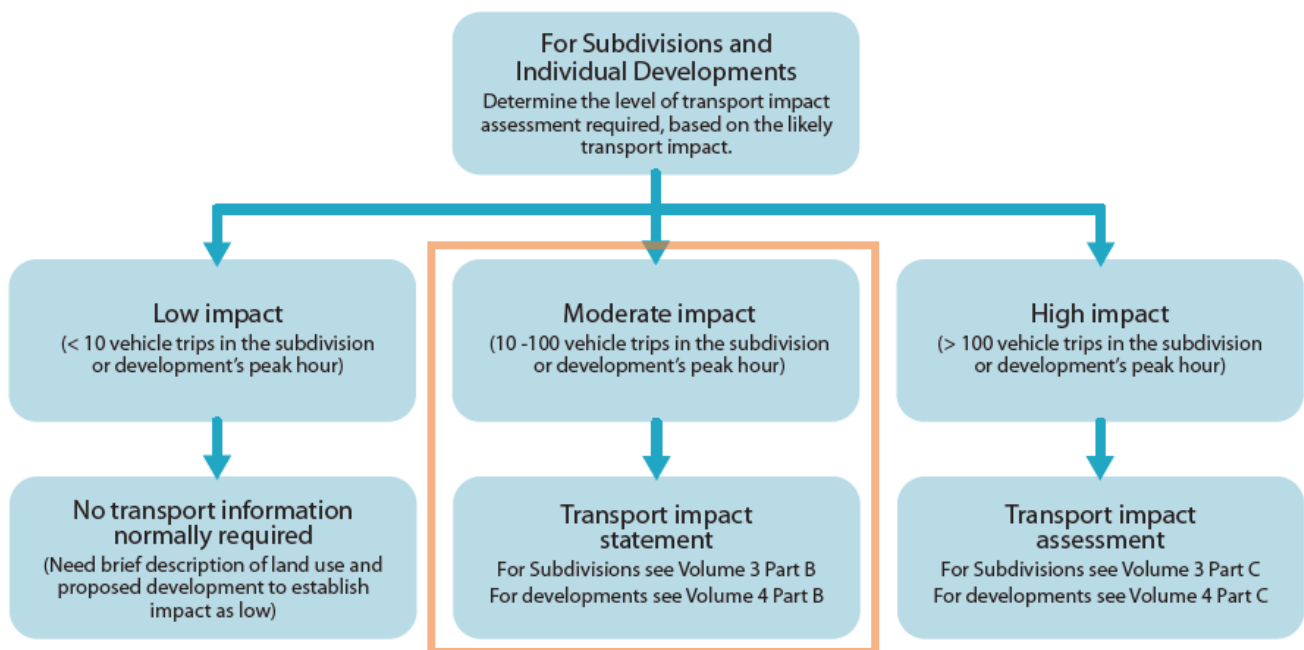


Figure 3: WAPC Transport Assessment Guidelines – reporting requirements

3 Proposed development

The proposal for the subject site is for a grouped dwelling residential development, comprising:

- A total of 19 residential dwellings;
- 10 resident car parking bays;
- 6 visitor car parking bays;
- 12 bicycle parking spaces for the shared use of residents and visitors;
- Communal resident facilities including playground, undercover recreation area, barbeque and garden; and,
- A centralised bin store for the development.

Vehicle access to the site is proposed via one crossover on Grand Promenade and one crossover on Chester Avenue. Waste collection will be undertaken onsite within the car park.

People walking and cycling will access the development from the external path network abutting the site.

The proposed development plans are included for reference in Appendix A.

4 Vehicle access and parking

4.1 Existing vehicle access

Existing vehicle access to the site is via one left-in/left-out crossover on Grand Promenade and one full-movement crossover on Chester Avenue (Figure 4).



Figure 4: Existing vehicle access

4.2 Proposed vehicle access

As detailed in the proposed development plans and in Figure 5, it is proposed to retain and use the existing crossovers on Grand Promenade and Chester Avenue for vehicle access to the site.

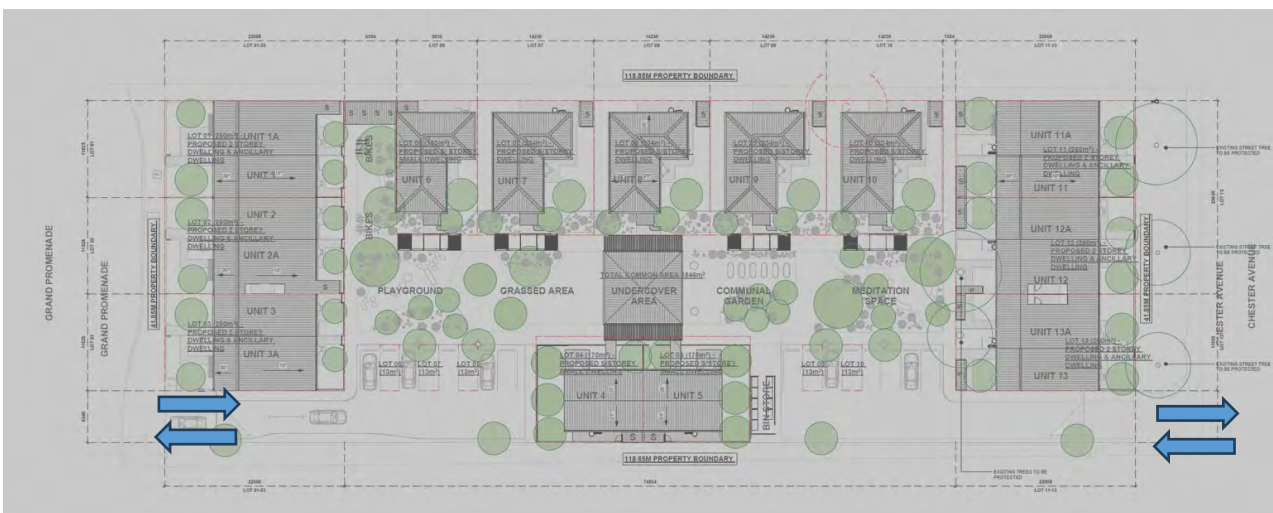


Figure 5: Proposed vehicle access



Each crossover will provide access to a car parking area delivering 8 bays. No internal connection is provided between the crossovers, to reduce the risk of rat running external traffic through the site.

The development driveways employ a variety of road surface textures and colours to calm the speed of motorised traffic. Flush walking paths are integrated into the driveways. Space for walking and cycling is shared with motorised traffic, to make the internal roadway easy to cross and to create opportunities for flexible use of the space by residents.

There is a functional driveway and parking aisle width varies between a maximum of 5.8m, narrowing to 4.5m wide in some locations. Each driveway is expected to carry a maximum of 5 vehicles per hour during peak hours, with less traffic during off-peak periods.

The proposed vehicle access arrangements and driveway widths are considered sufficient to accommodate the low traffic volumes generated by the development.

4.3 Car parking layout

Dimensions of car parking aisles and bays are compliant with AS2890.1. Onsite bays are 2.4m wide by 5.4m long and an aisle width of 5.8m has been provided. A 1m blind aisle extension is provided at the end of the car park.

4.4 Parking requirements

Reference was made to Residential Design Codes for applicable parking requirements. Advice provided to Urbii indicates the site is in 'Location A,' with the applicable parking rates set out in Table 1.

Table 1: R-Codes Volume 1 – Part C: Residential parking rates

	Location A	Minimum parking space(s) (per dwelling)	Maximum garage and carport parking (per dwelling)
Occupant car parking	Ancillary dwelling	0	1
	Studio and 1 bedroom dwelling	0	1
	2 bedroom dwelling	0	2
	3+ bedroom dwelling	1	2
	Location B	Minimum parking space(s) (per dwelling)	Maximum garage and carport parking (per dwelling)
	Ancillary dwelling	0	1
	Studio and 1 bedroom dwelling	1	1
	2 bedroom dwelling	1	2
3+ bedroom dwelling	1	2	
Visitor carparking	Number of dwellings	Minimum Parking	
	0-4 dwellings	No visitor car parking required	
	5-8 dwellings	1	
	9-12 dwellings	2	
	13 or more dwellings	3, plus 1 additional space per four dwellings or part thereof	
Motorcycle/scooter parking (multiple dwellings only)	0-19 dwellings	No motorcycle/scooter parking required	
	20 or more dwellings	One motorcycle/scooter space for every 10 car parking spaces	
<i>Minimum parking applies to all types of parking on site including (but not limited to) garages, carports, uncovered spaces, undercroft and basement parking.</i> <i>Maximum carparking applies to garages and carports. Additional parking may be provided as uncovered spaces, undercroft or basement parking.</i> LOCATION A – includes all land located within: <ul style="list-style-type: none"> - 800m walkable catchment of a train station on a high-frequency rail route; - 250m walkable catchment of a transit stop: <ul style="list-style-type: none"> o on a high-frequency transit route; or o that has multiple transit routes, that when combined stop every 15 minutes during weekday peak periods (7am – 9am and 5pm – 7pm); or - the defined boundaries of an activity centre. LOCATION B – includes all land that is not within Location A.			

The parking requirements for the development are presented in Table 2.

Table 2: Development parking requirements

Parking	Min	Max
Ancillary dwelling	0	0
Studio and 1 bedroom dwelling	0	2
2-bedroom dwelling	0	34
3+ bedroom dwelling	0	0
Total occupant car parking	0	36
Visitor car parking	5	
Total car parking	5	41

4.5 Parking supply

The proposed development provides the following parking allocation:

- 10 car parking spaces for residents.
- 6 car parking spaces provided onsite for visitors.

The proposed car parking supply is sufficient to meet the needs of the development and is compliant with the requirements of the R-Codes.



5 Provision for service vehicles

The proposed development is residential in nature and will not generate significant delivery and other service vehicle traffic.

Waste collection will be facilitated internally within the car park accessed from Chester Avenue. The City's waste department has advised they are able to provide an onsite collection service. Waste vehicles will reverse into the car park from Chester Avenue. Chester Avenue is a minor access road carrying local traffic only. Therefore, no issues are anticipated with reversing waste trucks into the site.

Australian Standard AS2890.2 *Off-street commercial vehicle facilities*, permits a maximum of one reversing manoeuvre (entry or exit) from a minor road, subject to permission from the relevant authority:

3.2.3.2 Minor road access

Where providing regular service from a minor road, manoeuvring on-street, if permitted by the relevant authority, shall be **strictly limited to one reverse movement either onto or off the street, and be subject to determination of both safety and obstruction to other on-street traffic.**

NOTE The AV vehicle class is the largest vehicle to be considered for reverse manoeuvres.

The swept path of the maximum size design vehicle using the facility may be allowed to occupy the entire width (less specified clearances) of a two-way access driveway when the vehicle is entering or leaving the minor road.

Swept path analysis was undertaken using the City's waste vehicle template, detailed in Figure 6. Swept path analysis confirms satisfactory service vehicle movements and is presented in Appendix B.

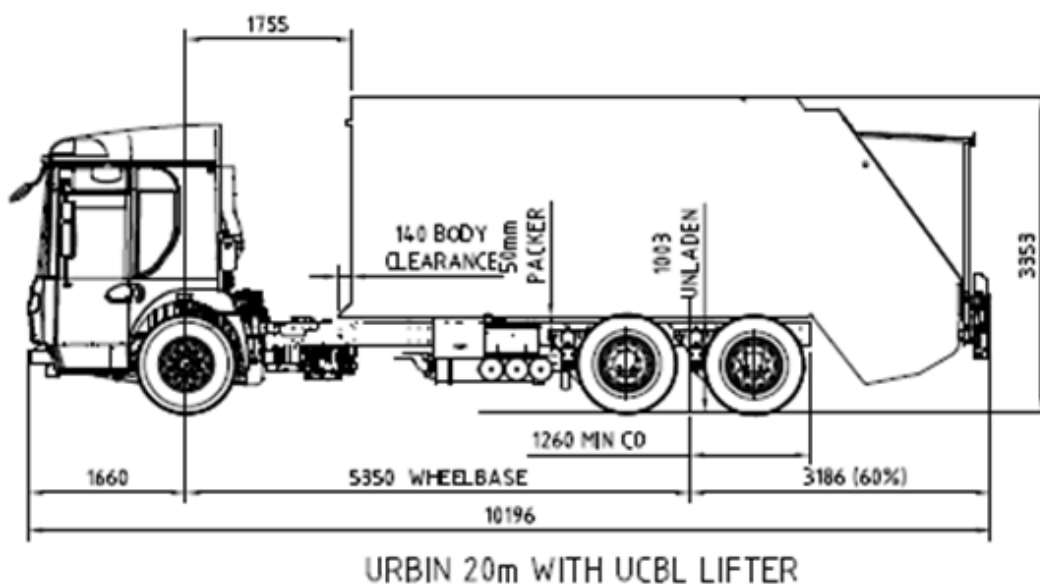


Figure 6: City of Stirling waste truck

6 Hours of operation

For most residential developments, the peak traffic hours typically coincide with the weekday commuter AM and PM peak hours on the surrounding road network.

The weekday AM peak hour in the Perth Metropolitan Area usually occurs between 7am to 9am and the weekday PM peak hour occurs between 4pm to 6pm. The peak hours for the proposed development are expected to coincide with these times.



7 Daily traffic volumes and vehicle types

7.1 Traffic generation

The traffic volume that will be generated by the proposed development has been estimated using trip generation rates derived with reference to the following sources:

- RTA *Guide to Traffic Generating Developments (2002)*;
- RTA TDT 2013/04a; and
- RTA NSW *Guide to Transport Impact Assessment 2024*.

The trip generation rates adopted are detailed in Table 3.

Table 3: Adopted trip rates for traffic generation

Land use	Trip rate source	Daily rate	AM rate	PM rate	AM-in	AM-out	PM-in	PM-out
Residential	TfNSW - Medium density residential building	5	0.5	0.5	25%	75%	65%	35%

The estimated traffic generation of the proposed development is detailed in Table 4. The proposed development is estimated to generate a total of 95 vehicles per day (vpd), with 10 vehicles per hour (vph) generated during the AM and PM peak hours, respectively.

These trips include both inbound and outbound vehicle movements. It is anticipated that most of the vehicle types would be passenger cars and SUVs.

Table 4: Traffic generation – Weekday AM and PM peak hours

Land use	Quantity	Daily Trips	AM Trips	PM Trips	AM Peak Trips		PM Peak Trips	
					IN	OUT	IN	OUT
Residential	19	95	10	10	3	7	7	3

7.2 Impact on surrounding roads

The WAPC Transport Impact Assessment Guidelines for Developments (2016) provides the following guidance on the assessment of traffic impacts:

“As a general guide, an increase in traffic of less than 10 percent of capacity would not normally be likely to have a material impact on any particular section of road but increases over 10 percent may. All sections of road with an increase greater than 10 percent of capacity should therefore be included in the analysis. For ease of assessment, an increase of 100 vehicles per hour for any lane can be considered as equating to around 10 percent of capacity. Therefore, any section of road where development traffic would increase flows by more than 100 vehicles per hour for any lane should be included in the analysis.”

The proposed development will not increase traffic flows on any roads adjacent to the site by the quoted WAPC threshold of +100vph to warrant further analysis. Therefore, the impact on the surrounding road network is acceptable.



8 Traffic management on the frontage roads

Information from online mapping services, Main Roads WA, Local Government, and/or site visits was collected to assess the existing traffic management on frontage roads.

8.1.1 Grand Promenade

Grand Promenade near the subject site is an approximately 20m wide, four-lane divided road. A 5m wide raised and kerbed median is provided, with integrated tree planting and street lighting. Paths for walking and cycling are provided on both sides of the road.

Grand Promenade is classified as a *Distributor A* road in the Main Roads WA road hierarchy (Figure 7) and operates under a speed limit of 60km/h (Figure 8). Distributor A roads are the responsibility of Local Government and are primarily for high capacity traffic movements between industrial, commercial and residential areas (Figure 9).

Traffic data obtained from Main Roads WA indicates that Grand Promenade carries 16,500 vehicles per day.

8.1.2 Chester Avenue

Chester Avenue near the subject site is an approximately 7m wide, two-lane undivided road. No footpath is provided within the road reservation.

Chester Avenue is classified as an *Access* road in the Main Roads WA road hierarchy (Figure 7) and operates under a speed limit of 50km/h (Figure 8). Access roads are the responsibility of Local Government and are for the provision of vehicle access to abutting properties. (Figure 9).

No traffic data was available at the time of preparation of this report. However, access roads typically carry under 3,000 vehicles per day.

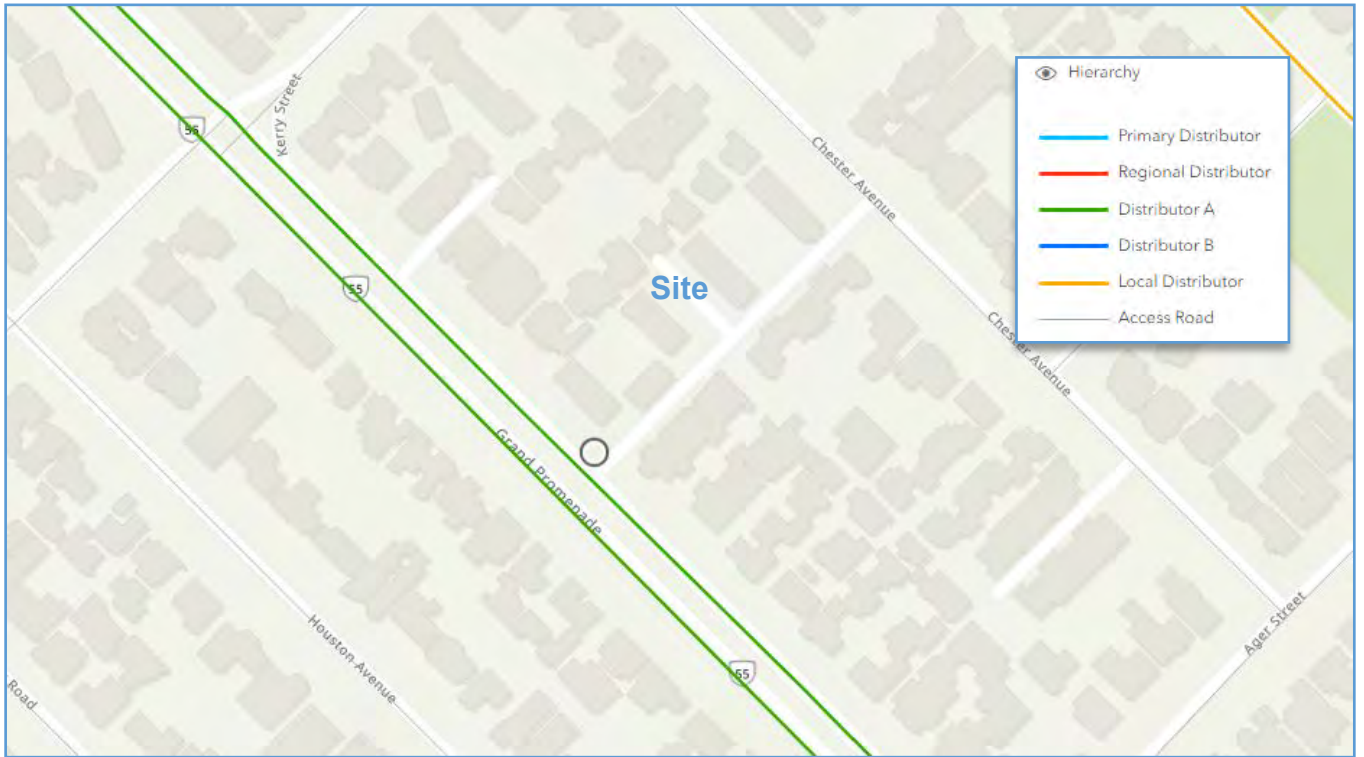


Figure 7: Main Roads WA road hierarchy plan

Source: Main Roads WA Road Information Mapping System (RIM)

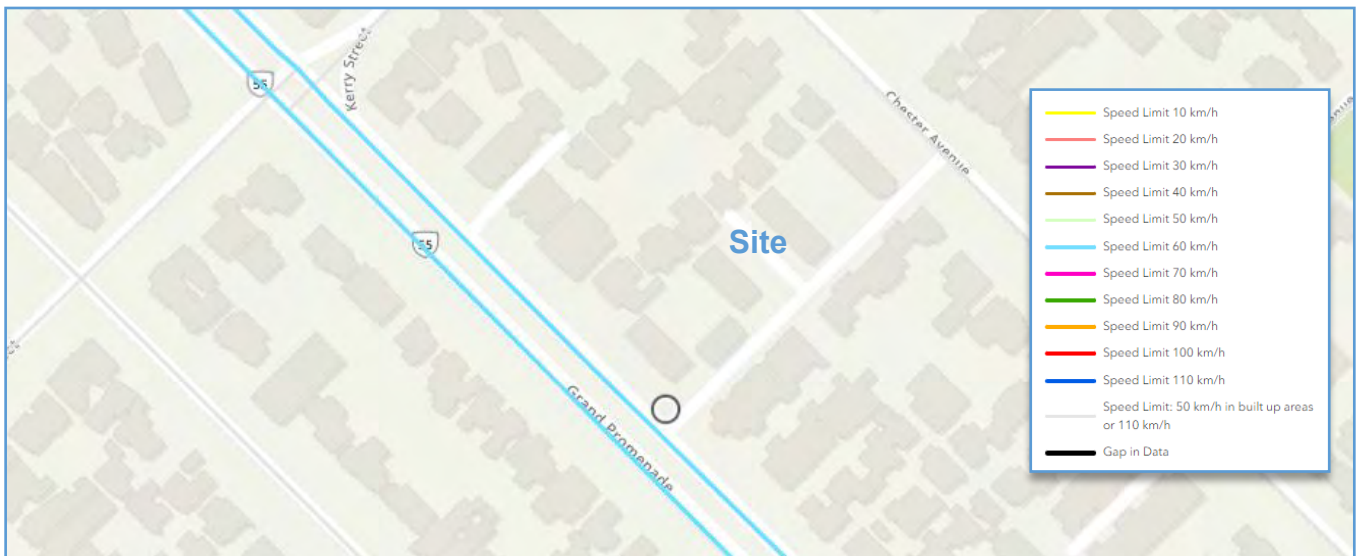


Figure 8: Main Roads WA road speed zoning plan

Source: Main Roads WA Road Information Mapping System (RIM)

**ROAD HIERARCHY FOR WESTERN AUSTRALIA
ROAD TYPES AND CRITERIA (see Note 1)**

CRITERIA	PRIMARY DISTRIBUTOR (PD) (see Note 2)	DISTRICT DISTRIBUTOR A (DA)	DISTRICT DISTRIBUTOR B (DB)	REGIONAL DISTRIBUTOR (RD)	LOCAL DISTRIBUTOR (LD)	ACCESS ROAD (A)
<i>Primary Criteria</i>						
1. Location (see Note 3)	All of WA incl. BUA	Only Built Up Area.	Only Built Up Area.	Only Non Built Up Area. (see Note 4)	All of WA incl. BUA	All of WA incl. BUA
2. Responsibility	Main Roads Western Australia.	Local Government.	Local Government.	Local Government.	Local Government.	Local Government.
3. Degree of Connectivity	High. Connects to other Primary and Distributor roads.	High. Connects to Primary and/or other Distributor roads.	High. Connects to Primary and/or other Distributor roads.	High. Connects to Primary and/or other Distributor roads.	Medium. Minor Network Role Connects to Distributors and Access Roads.	Low. Provides mainly for property access.
4. Predominant Purpose	Movement of inter regional and/or cross town/city traffic, e.g. freeways, highways and main roads.	High capacity traffic movements between industrial, commercial and residential areas.	Reduced capacity but high traffic volumes travelling between industrial, commercial and residential areas.	Roads linking significant destinations and designed for efficient movement of people and goods between and within regions.	Movement of traffic within local areas and connect access roads to higher order Distributors.	Provision of vehicle access to abutting properties
<i>Secondary Criteria</i>						
5. Indicative Traffic Volume (AADT)	In accordance with Classification Assessment Guidelines.	Above 8 000 vpd	Above 6 000 vpd.	Greater than 100 vpd	Built Up Area - Maximum desirable volume 6 000 vpd. Non Built Up Area – up to 100 vpd.	Built Up Area - Maximum desirable volume 3 000 vpd. Non Built Up Area – up to 75 vpd.
6. Recommended Operating Speed	60 – 110 km/h (depending on design characteristics).	60 – 80 km/h.	60 – 70 km/h.	50 – 110 km/h (depending on design characteristics).	Built Up Area 50 - 60 km/h (desired speed) Non Built Up Area 60 – 110 km/h (depending on design characteristics).	Built Up Area 50 km/h (desired speed). Non Built Up Area 50 – 110 km/h (depending on design characteristics).
7. Heavy Vehicles permitted	Yes.	Yes.	Yes.	Yes.	Yes, but preferably only to service properties.	Only to service properties.
8. Intersection treatments	Controlled with appropriate measures e.g. high speed traffic management, signing, line marking, grade separation.	Controlled with appropriate measures e.g. traffic signals.	Controlled with appropriate Local Area Traffic Management.	Controlled with measures such as signing and line marking of intersections.	Controlled with minor Local Area Traffic Management or measures such as signing.	Self controlling with minor measures.
9. Frontage Access	None on Controlled Access Roads. On other routes, preferably none, but limited access is acceptable to service individual properties.	Prefer not to have residential access. Limited commercial access, generally via service roads.	Residential and commercial access due to its historic status. Prefer to limit when and where possible.	Prefer not to have property access. Limited commercial access, generally via lesser roads.	Yes, for property and commercial access due to its historic status. Prefer to limit whenever possible. Side entry is preferred.	Yes.
10. Pedestrians	Preferably none. Crossing should be controlled where possible.	With positive measures for control and safety e.g. pedestrian signals.	With appropriate measures for control and safety e.g. median/islands refuges.	Measures for control and safety such as careful siting of school bus stops and rest areas.	Yes, with minor safety measures where necessary.	Yes.
11. Buses	Yes.	Yes.	Yes.	Yes.	Yes.	If necessary (see Note 5)
12. On-Road Parking	No (emergency parking on shoulders only).	Generally no. Clearways where necessary.	Not preferred. Clearways where necessary.	No – emergency parking on shoulders – encourage parking in off road rest areas where possible.	Built Up Area – yes, where sufficient width and sight distance allow safe passing. Non Built Up Area – no. Emergency parking on shoulders.	Yes, where sufficient width and sight distance allow safe passing.
13. Signs & Linemarking	Centrelines, speed signs, guide and service signs to highway standard.	Centrelines, speed signs, guide and service signs.	Centrelines, speed signs, guide and service signs.	Centrelines, speed signs and guide signs.	Speed and guide signs.	Urban areas – generally not applicable. Rural areas - Guide signs.
14. Rest Areas/Parking Bays	In accordance with Main Roads' Roadside Stopping Places Policy.	Not Applicable.	Not Applicable.	Parking Bays/Rest Areas. Desired at 60km spacing.	Not Applicable.	Not Applicable.

Figure 9: Road types and criteria for Western Australia

Source: Main Roads Western Australia D10#10992

9 Public transport access

Information was collected from Transperth, PTA and site visits to assess the existing public transport access to and from the site.

The subject site has access to multiple bus services within walking distance, as detailed in Table 5.

Table 5: Bus routes servicing the subject site

Route Number	Route Description
67	Perth - Mirrabooka Bus Stn via Beaufort St & Grand Promenade (Figure 11)
998 (High Freq)	CircleRoute – Clockwise (Figure 12)
999 (High Freq)	CircleRoute - Anti-Clockwise (Figure 12)

Public transport services provide a viable alternative mode of transport for residents and visitors to the proposed development.

The closest bus stops are located on Grand Promenade, less than 100m walk from the site (Figure 10). Bus services provide excellent coverage and connectivity to the rail network.

Access to the high frequency circle routes is available within walking distance of the site. The existing public transport network plans are shown in Figures 11 & 12.

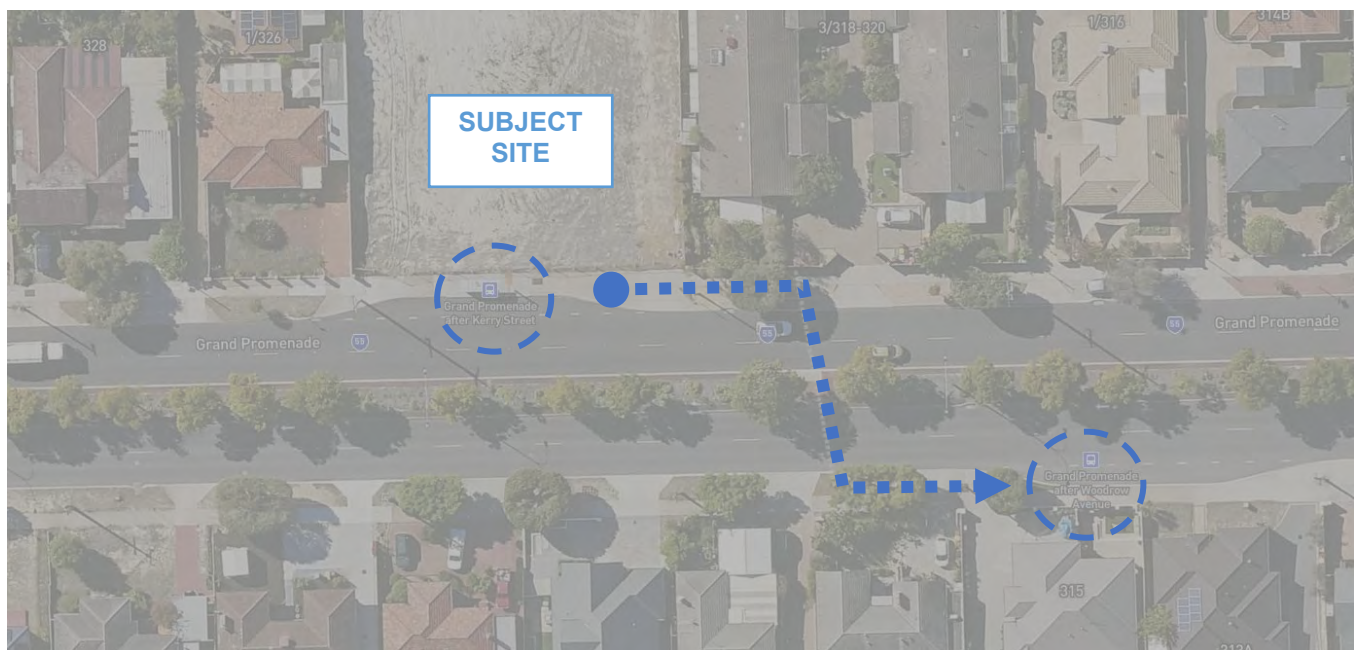


Figure 10: Closest bus stops serving the proposed development

Route 67, 68 Map

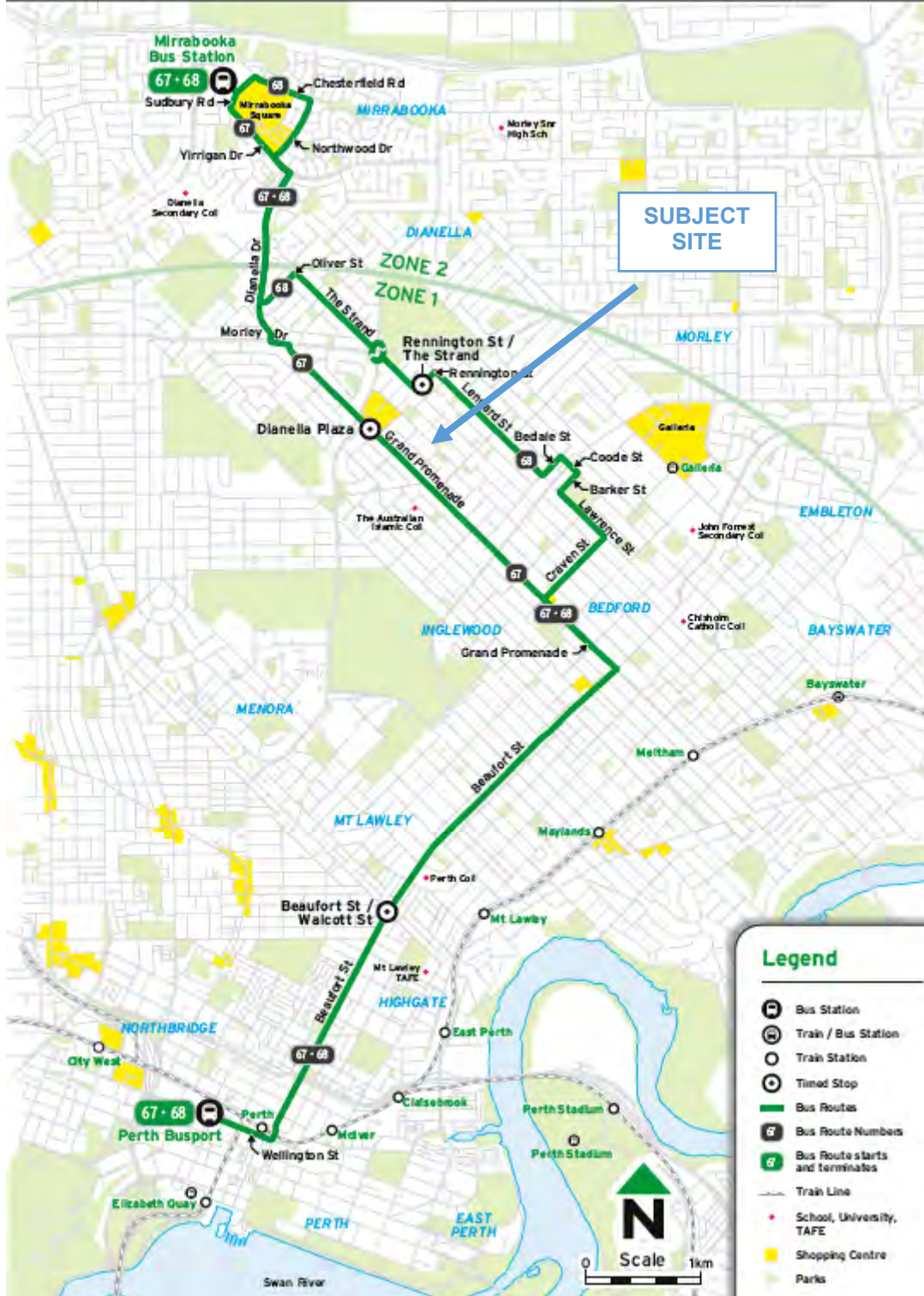


Figure 11: Transperth public transport plan (routes 67)

Source: Transperth

10 Pedestrian access

Information from online mapping services, Main Roads WA, Local Government, and site visits was collected to assess the pedestrian access for the proposed development.

10.1 Pedestrian facilities and level of service

A footpath is provided on both sides of Grand Promenade adjacent to the subject site. Pedestrian crossing facilities, including kerb ramps are provided at nearby intersections, which promote improved access for bicycles, wheelchairs, and prams.

The WAPC Transport Impact Assessment Guidelines for Developments (2016) provide warrants for installing pedestrian priority crossing facilities. This is based on the volume of traffic as the key factor determining if pedestrians can safely cross a road. The guidelines recommend pedestrian priority crossing facilities be considered once the peak hour traffic exceeds the volumes detailed in Table 6.

The traffic volumes in this table are based on a maximum delay of 45 seconds for pedestrians, equivalent to Level of Service E. The pedestrian crossing facilities on adjacent roads near the site are sufficient and within the traffic volume thresholds.

Table 6: Traffic volume thresholds for pedestrian crossings

Road cross-section	Maximum traffic volumes providing safe pedestrian gap
2-lane undivided	1,100 vehicles per hour
2-lane divided (with refuge)	2,800 vehicles per hour
4-lane undivided*	700 vehicles per hour
4-lane divided (with refuge)*	1,600 vehicles per hour

The proposed development provides an extensive internal path network which connects between the dwellings, communal facilities, car parking, and external road reservations.

11 Bicycle access

Information from online mapping services, Department of Transport, Local Government, and/or site visits was collected to assess bicycle access for the proposed development.

11.1 Bicycle network

The Perth and Peel Long Term Cycle Network (LTCN) designates routes by their function, rather than built form. Function considers the type of activities that take place along a route, and the level of demand (existing and potential). The built form of a route is based on the characteristics of the environment, including space availability, topography, traffic conditions (speed, volumes), and primary users. The cycling network hierarchy is described in Figure 13.

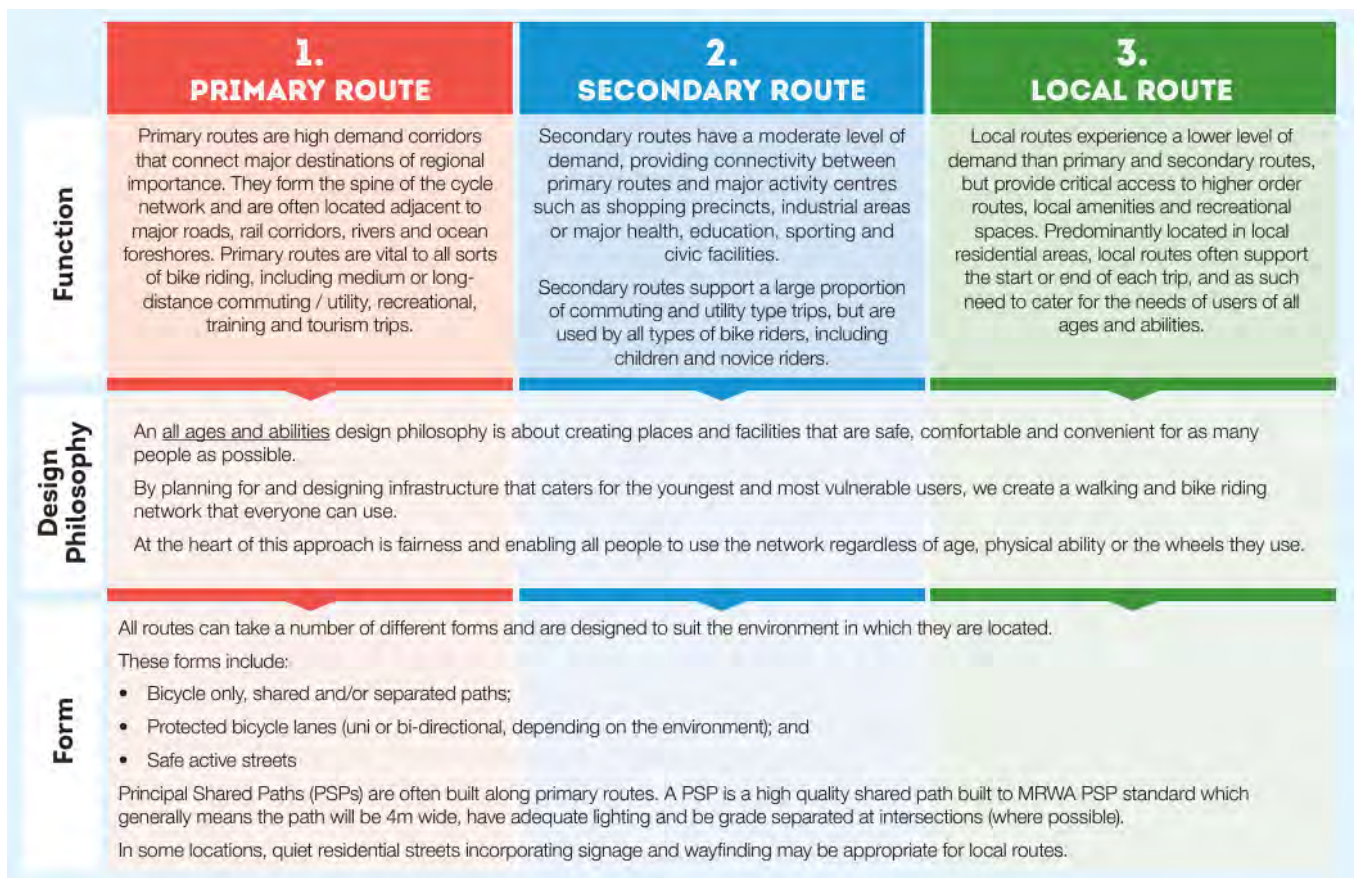


Figure 13: Western Australian Cycling Network Hierarchy

The Long-Term Cycle Network plan is detailed in Figure 14. Grand Promenade and Chester Avenue do not form part of the LTCN. People can cycle on the road and less confident people can legally cycle on the footpath.



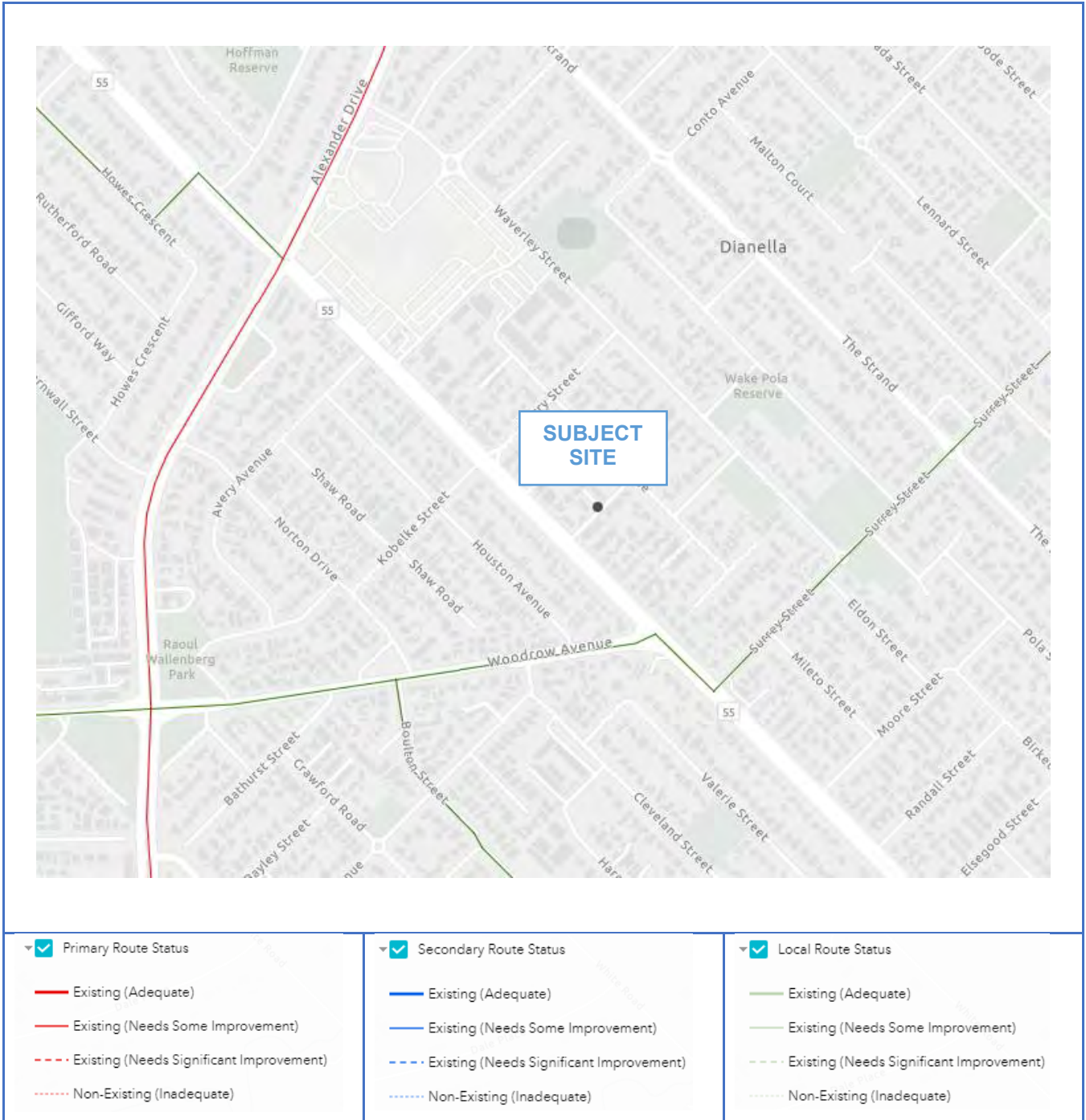


Figure 14: Perth and Peel Long Term Cycle Network plan (LTCN)

The Strava cycling heatmap tool shows that Grand Promenade and Alexander Drive are relatively popular cycling routes in the area (Figure 15).

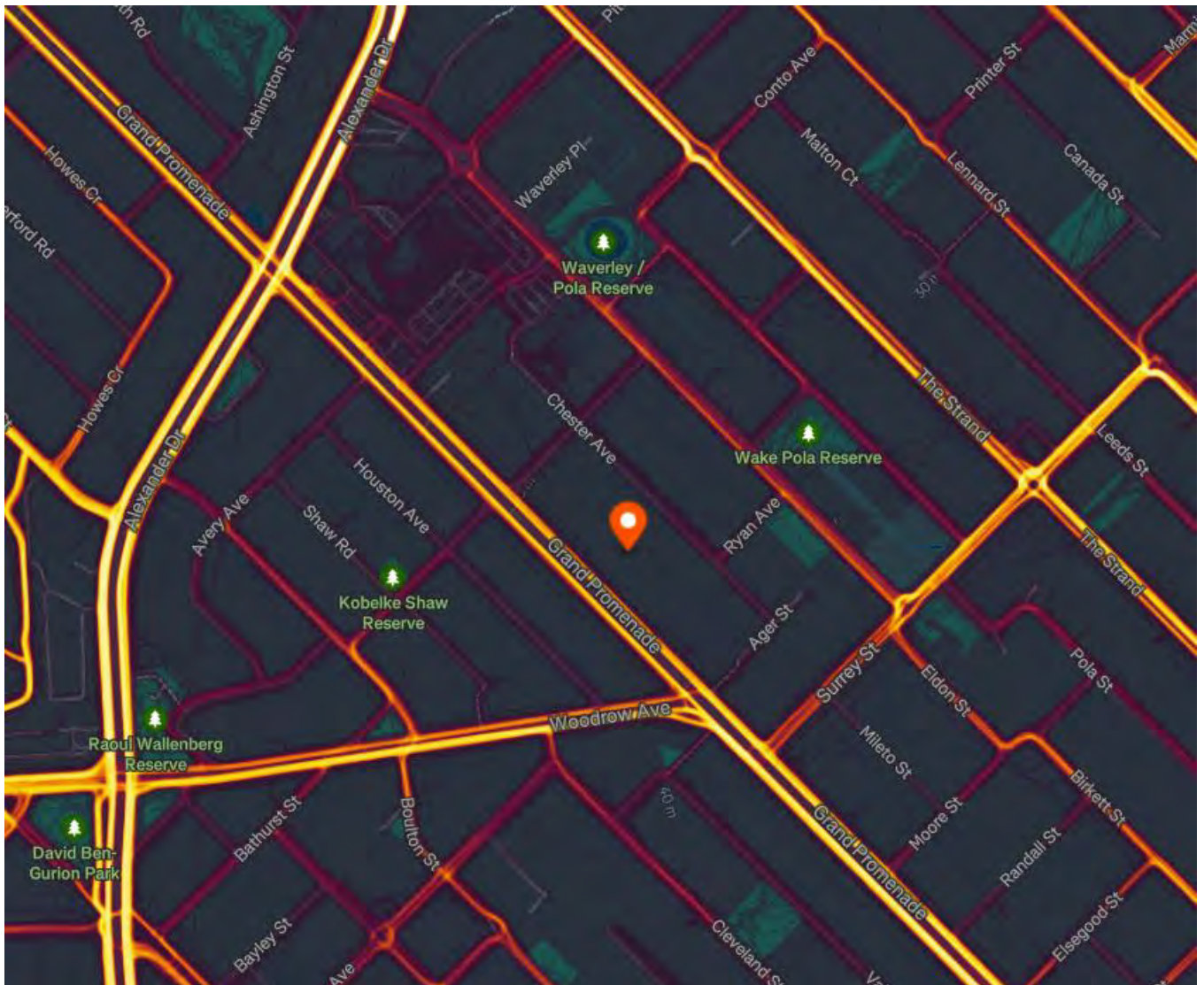


Figure 15: Strava cycling heatmap

11.2 Bicycle parking and end of trip facilities

Parking for 12 bicycles is provided for the development, which will be for the shared use of residents and visitors.



11.3 Sustainable transport catchment

As detailed in Figure 16, the subject site is well placed for residents and visitors to travel by sustainable modes of transport. A comfortable 8km or 20-25min cycle will provide the development with a large catchment.

This range can be further increased through a combination of micro-mobility and train travel with close access to train stations.

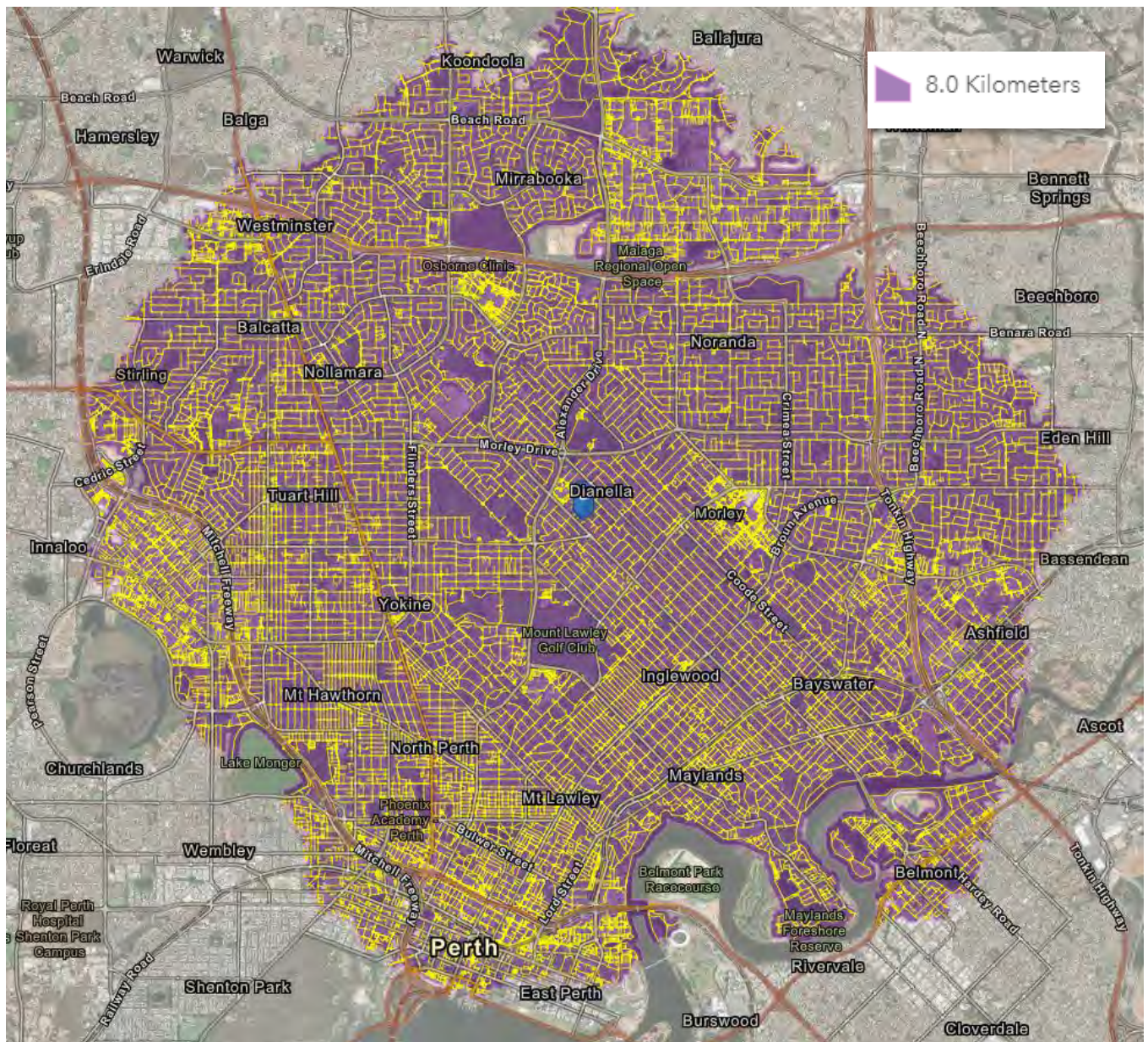


Figure 16: Cycling and micro-mobility catchment

12 Site specific issues

No additional site-specific issues were identified within the scope of this assessment.



13 Safety issues

The five-year crash history in the vicinity of the site was obtained from Main Roads WA. As detailed in Figure 17, 3 crashes were recorded in the locality in the last five years. The detailed crash history is presented in Table 7.

The low traffic generation of the proposed development is unlikely to impact traffic safety in the area.



Figure 17: 5-year crash map in the locality (2020-2024)

Source: MRWA crash mapping tool

Table 7: 5-year crash history in the locality (2020-2024)

Severity	No.	%
Fatal	0	0
Hospital	0	0
Medical	1	33.33
PDO Major	2	66.67
PDO Minor	0	0

Year	No.	%
2020	1	33.33
2021	1	33.33
2022	1	33.33

Nature	No.	%
Head On	0	0
Hit Animal	0	0
Hit Object	0	0
Hit Pedestrian	0	0
Non Collision	0	0
Not Known	0	0
Rear End	1	33.33
Right Angle	1	33.33
Right Turn Thru	0	0
Sideswipe Opposite Dirn	0	0
Sideswipe Same Dirn	1	33.33

Light	No.	%
Dark - Street Lights Not Provided	0	0
Dark - Street Lights Off	0	0
Dark - Street Lights On	1	33.33
Dawn Or Dusk	0	0
Daylight	2	66.67
Not Known	0	0

Conditions	No.	%
Dry	2	66.67
Not Known	0	0
Other / Unknown	1	33.33
Wet	0	0

Alignment	No.	%
Curve	0	0
Not Known	0	0
Straight	3	100.00

Total	No.	%
Total	3	



14 Conclusion

This Transport Impact Statement has been prepared by Urbii on behalf of Dale Alcock Projects with regards to the proposed residential development, located at 322 Grand Promenade, Dianella.

The subject site is situated on the north-eastern side of Grand Promenade and extends to Chester Avenue. The site is presently vacant and is surrounded by a mix of residential, education, commercial and retail land uses. Dianella Plaza Shopping Centre is accessible within 500m walking distance to the north.

It is proposed to develop the site into a residential development, delivering 19 grouped dwellings.

The site features good connectivity with the existing road, walking and cycling network. There is good public transport coverage through nearby bus services which connect to the rail network.

The car parking supply is satisfactory and can accommodate the car parking demand of the proposed development.

The traffic analysis undertaken in this report shows that the traffic generation of the proposed development is minor (less than 100vph on any lane) and as such would have a minor impact on the surrounding road network.

It is concluded that the findings of this Transport Impact Statement are supportive of the proposed development.

15 Appendices

Appendix A: Proposed development plans





OVERALL SITE PLAN
SCALE: 1:500

**322 GRAND PROMENADE,
DIANELLA (4965m²),
R30**

A.	PREPARED BY	MDC ARCHITECTS
B.	DATE OF PREPARATION	27/05/2025
C.	PROJECT NO.	322 GRAND PROMENADE
D.	PROJECT NAME	322 GRAND PROMENADE
E.	PROJECT ADDRESS	322 GRAND PROMENADE, DIANELLA, VIC 3086
F.	PROJECT TYPE	RESIDENTIAL DEVELOPMENT
G.	PROJECT STATUS	CONCEPT
H.	PROJECT PHASE	CONCEPT

Use Metric dimensions in all drawings. Do not scale drawings. All measurements are to be taken from the center of the lot unless otherwise specified. All dimensions are to be taken from the center of the lot unless otherwise specified. The design is subject to the approval of the relevant authorities and is not to be used for any other purpose without the written consent of MDC ARCHITECTS.



**MDC
ARCHITECTS**
272/280/300/302/304/306/308/310/312/314/316/318/320/322/324/326/328/330/332/334/336/338/340/342/344/346/348/350/352/354/356/358/360/362/364/366/368/370/372/374/376/378/380/382/384/386/388/390/392/394/396/398/400/402/404/406/408/410/412/414/416/418/420/422/424/426/428/430/432/434/436/438/440/442/444/446/448/450/452/454/456/458/460/462/464/466/468/470/472/474/476/478/480/482/484/486/488/490/492/494/496/498/500/502/504/506/508/510/512/514/516/518/520/522/524/526/528/530/532/534/536/538/540/542/544/546/548/550/552/554/556/558/560/562/564/566/568/570/572/574/576/578/580/582/584/586/588/590/592/594/596/598/600/602/604/606/608/610/612/614/616/618/620/622/624/626/628/630/632/634/636/638/640/642/644/646/648/650/652/654/656/658/660/662/664/666/668/670/672/674/676/678/680/682/684/686/688/690/692/694/696/698/700/702/704/706/708/710/712/714/716/718/720/722/724/726/728/730/732/734/736/738/740/742/744/746/748/750/752/754/756/758/760/762/764/766/768/770/772/774/776/778/780/782/784/786/788/790/792/794/796/798/800/802/804/806/808/810/812/814/816/818/820/822/824/826/828/830/832/834/836/838/840/842/844/846/848/850/852/854/856/858/860/862/864/866/868/870/872/874/876/878/880/882/884/886/888/890/892/894/896/898/900/902/904/906/908/910/912/914/916/918/920/922/924/926/928/930/932/934/936/938/940/942/944/946/948/950/952/954/956/958/960/962/964/966/968/970/972/974/976/978/980/982/984/986/988/990/992/994/996/998/1000



**DALE
ALCOCK
PROJECTS**

322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING
OVERALL SITE PLAN
SCALE: 1:500
DATE: 27/05/2025
PROJECT NO: 322 GRAND PROMENADE



OVERALL GROUND FLOOR PLAN
SCALE: 1:1000

Use Shaded Dimensions as Indicated. Do not make any changes to the plan without the approval of MDC Architects and Council. The client warrants the accuracy of the information and content of this plan. The client warrants the accuracy of the information and content of this plan. The client warrants the accuracy of the information and content of this plan.

DATE	11/2024
PROJECT	322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING
CLIENT	DALE ALCOCK PROJECTS
ARCHITECT	MDC ARCHITECTS
ADDRESS	272 CALVERTON ROAD, BUNN FERRISBURG WA 6108
CONTACT	PH: 08 9419 3905

322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING
OVERALL GROUND FLOOR PLAN
DATE: 11/2024
CONCEPT



OVERALL FIRST FLOOR PLAN
SCALE: 1:200

A.	11824	11825	11826	11827	11828	11829	11830	11831	11832	11833	11834
B.	20604	20605	20606	20607	20608	20609	20610	20611	20612	20613	20614

Use Metric dimensions in all drawings. Do not scale or measure off drawings. All dimensions are in millimeters. All dimensions are to be taken from the center of the line unless otherwise specified. All dimensions are to be taken from the center of the line unless otherwise specified. The design is subject to the approval of the relevant authorities and conditions may vary without prior approval.

322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING
OVERALL FIRST FLOOR PLAN

MDC ARCHITECTS
272 GARDENS ROAD, #01-01 GARDENS FOREVER
SINGAPORE 238461
ARCH: CA 193285

DALE ALCOCK PROJECTS

DATE: 11/2024
DRAWN BY: ANIT
CHECKED BY: ANIT
STATUS: CONCEPT



OVERALL ROOF PLAN

NO.	REVISION	DATE	BY

Use Metric dimensions in all drawings. Do not scale or measure off drawings. All dimensions are to be taken from the centerline of the building unless otherwise specified. All dimensions are to be taken from the centerline of the building unless otherwise specified. The client warrants the accuracy of all dimensions and coordinates. Reproduction without the approval of MDC Architects and Dale Alcock Projects is prohibited.

322 GRAND PROMENADE, DIABELLA - GROUPED HOUSING

OVERALL ROOF PLAN

DATE: 11/2024

PROJECT: CONCEPT

MDC ARCHITECTS
272 GARDENS ROAD, SUITE 101/102, GARDENS FOREST, SINGAPORE 263227

DALE ALCOCK PROJECTS

1:1000

11/2024

AWT

Appendix B: Swept path diagrams

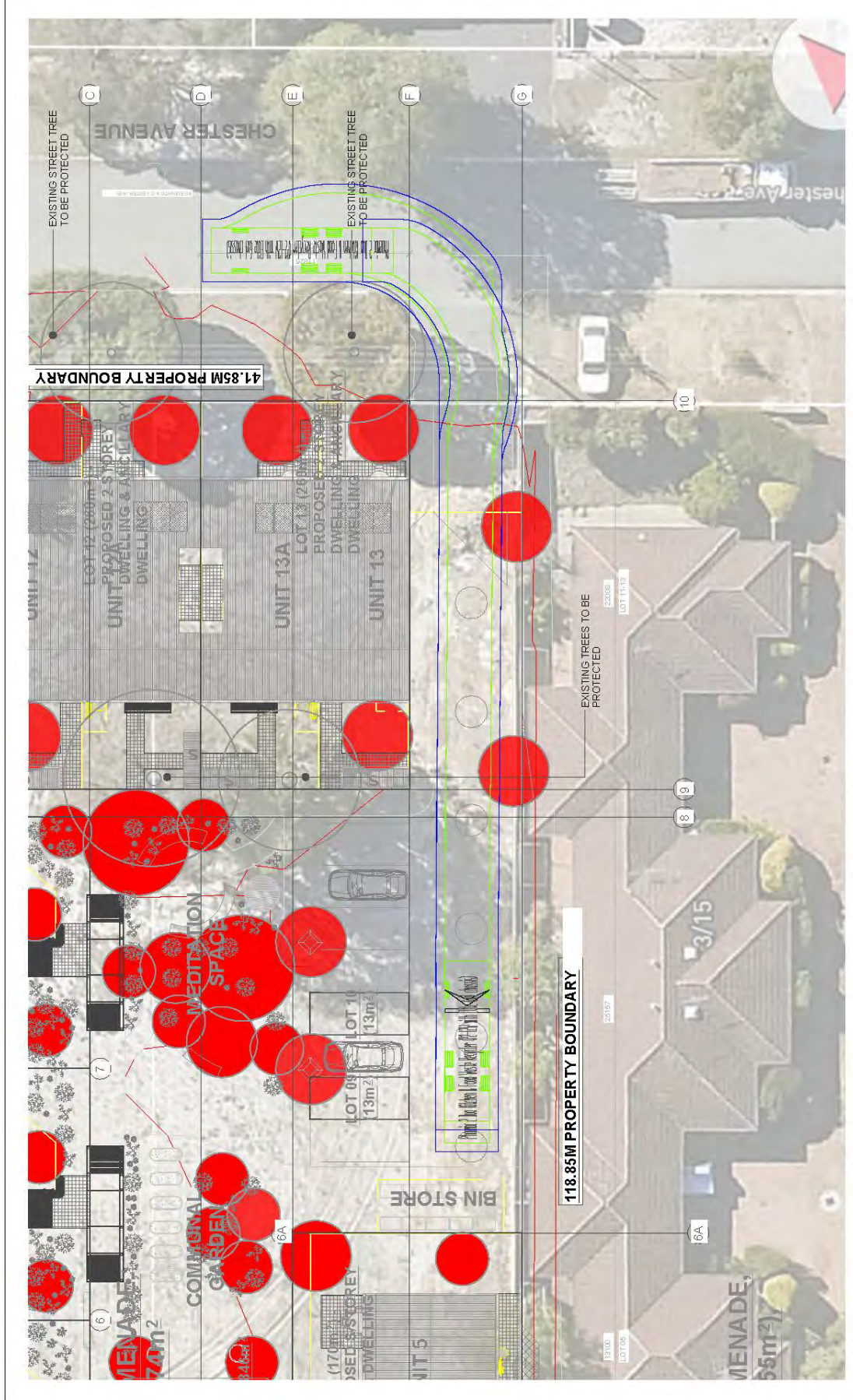
Swept path diagrams are included in this section of the report. Different coloured lines are employed to represent the various envelopes of the vehicle swept path, as described below:

Cyan represents the wheel path of the vehicle

Green represents the vehicle body envelope

Blue represents a 500mm buffer line, offset from the vehicle swept path

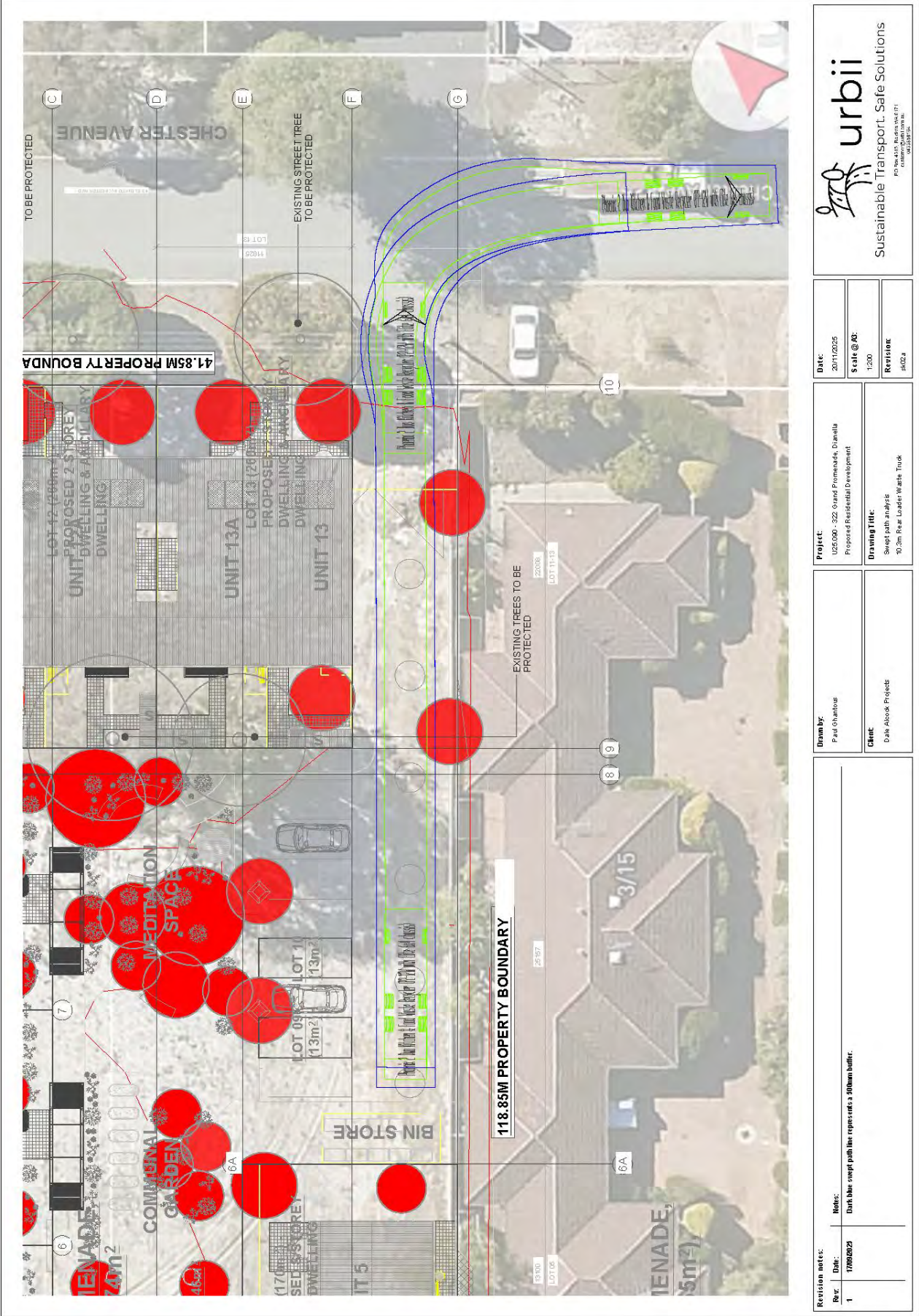
The swept path diagrams are also provided separately in high-quality, A3 PDF format.



Revision notes:		Project:	
Rev:	Date:	1025000 - 322 Grand Promenades, Danella	201710205
1	17/09/2023	Proposed Residential Development	Scale @ A0:
		Drawing Title:	1:2000
		Sweet path analysis	Revision:
		10.2m Rear Loader Waste Truck	1011a
Drawn by:		Date:	
Paul Ghantous		201710205	
Client:		Scale @ A0:	
Dale Alcock Projects		1:2000	

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Sustainable Transport. Safe Solutions

PO Box 4115, Riverside Park CT1
01100, Warrington, UK
01925 832222



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Sustainable Transport. Safe Solutions.
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Melbourne, VIC 3008, Australia
www.urbii.com.au

Date:	20/11/2025
Scale @ A0:	1:200
Revision:	1/02.2

Project:	U25.090 - 322 Grand Promenade, Dianella Proposed Residential Development
Drawing Title:	Swept path analysis 10.2m Rear Loader Waste Truck

Drawn by:	Paul Chantous
Client:	Dale Alcock Projects

Revision notes:	
Rev:	Date:
1	17/09/2025
Notes: Dark blue swept path line represents a 500mm buffer.	

Attachment 7d - Transport Noise Assessment by Lloyd George Acoustics

City of Stirling
3 Dec 2025
RECEIVED

Transportation Noise Assessment

322A and 322B Grand Promenade, Dianella

Reference: 250910720-01

Prepared for:
Dale Alcock

City of Stirling
3 Dec 2025
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Reference: 250910720-01

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12-Sep-25	0	Issued to Client	Terry George	-

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1. INTRODUCTION

Dale Alcock is proposing to construct eleven (11) dwellings at 322A and 322B Grand Promenade, Dianella - refer *Figure 1-1*. The proposed dwellings are a mix of single and double storey, with the plans provided in *Appendix A*.

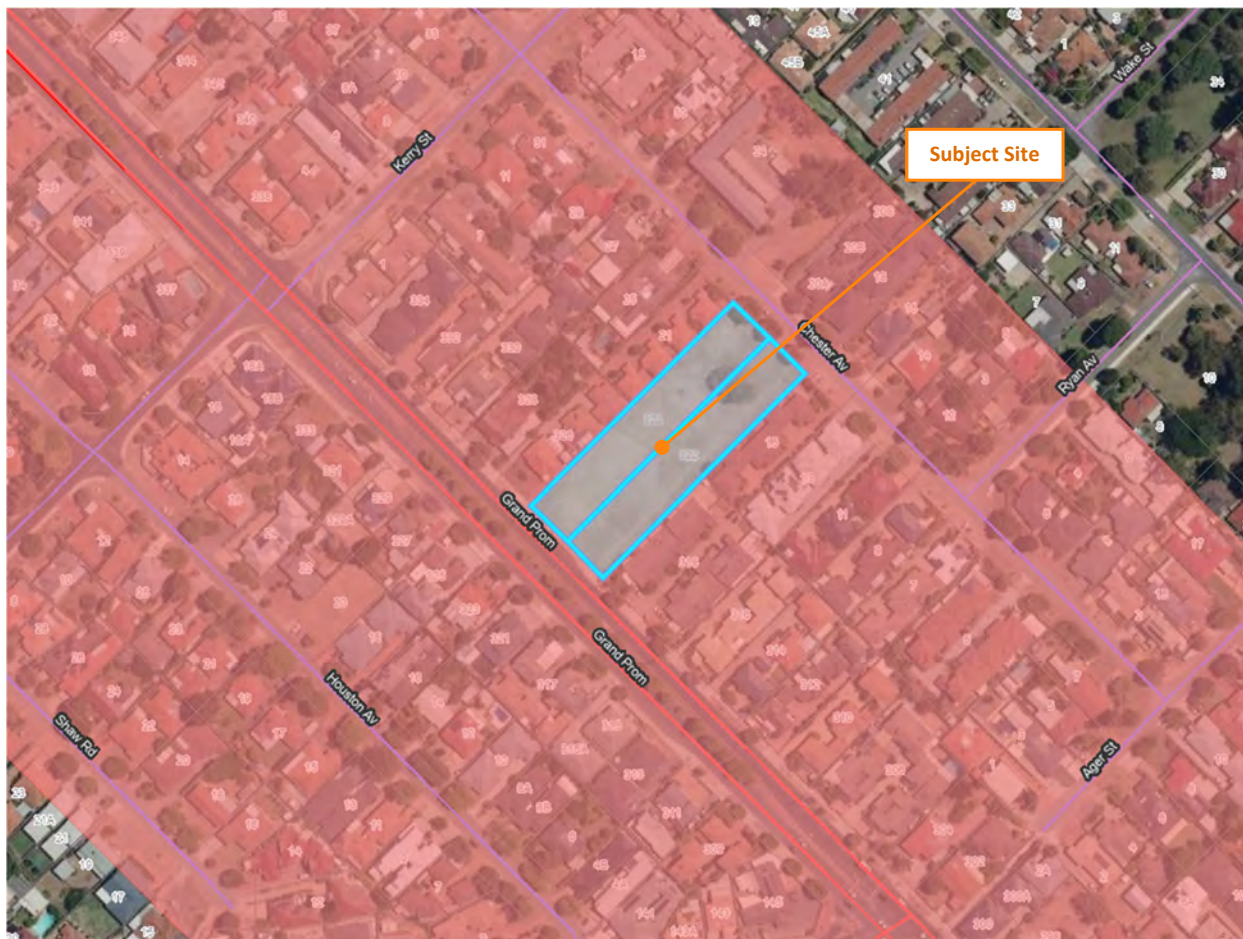


Figure 1-1: Subject Site Location (Source: DPLH PlanWA)

The site adjoins Grand Promenade, which is considered an 'Other Significant Freight / Traffic Route' as shown on the PlanWA Maps and the subject site is within the trigger distance (refer *Table 1-1* and *Figure 1-2*) and therefore a noise assessment is required in accordance with *State Planning Policy No. 5.4 Road and Rail Noise*, being the subject of this report.

Table 1-1: Transport Corridor Classification and Trigger Distances

Transport Road Classification	Trigger Distance	Distance Measured From
Strategic Freight and Major Traffic Routes Roads as defined by Perth and Peel Planning Frameworks and/or roads with either 500 or more Class 7 to 12 Austroads vehicles per day, and/or 50,000 per day traffic volume	300 metres	Road carriageway edge
Other Significant Freight/Traffic Routes These are generally any State administered road and/or local government road identified as being a future State administered road (red road) and other roads that meets the criteria of either ≥ 100 Class 7 to 12 Austroads vehicles daily or $\geq 23,000$ daily traffic count (averaged equivalent to 25,000 vehicles passenger car units under region schemes)	200 metres	Road carriageway edge

**Figure 1-2: Subject Site Locality in Relation to Road (Source: DPLH PlanWA)**

Appendix B contains a description of some of the terminology used throughout this report.

2. CRITERIA

The criteria relevant to this project is provided in *State Planning Policy No. 5.4 Road and Rail Noise* (hereafter referred to as SPP 5.4) produced by the Western Australian Planning Commission (WAPC). SPP 5.4 is supported by the *Road and Rail Noise Guidelines* (the Guidelines) and the Department of Planning, Lands and Heritage mapping. The objectives of SPP 5.4 are to:

- Protect the community from unreasonable levels of transport noise;
- Protect strategic and other significant freight transport corridors from incompatible urban encroachment;
- Ensure transport infrastructure and land-use can mutually exist within urban corridors;
- Ensure that noise impacts are addressed as early as possible in the planning process; and
- Encourage best practice noise mitigation design and construction standards.

Table 2-1 sets out noise targets that are to be achieved by proposals under which SPP 5.4 applies. Where the targets are exceeded, an assessment is required to determine the likely level of transport noise and management/mitigation required.

Table 2-1: Noise Targets for Noise Sensitive Land-Use

Scenario	Outdoor Noise Target		Indoor Noise Target	
	Noise-sensitive land-use and/or development	55 dB L _{Aeq} (Day)	50 dB L _{Aeq} (Night)	40 dB L _{Aeq} (Day) (Living and Work Areas)

Notes:

- Day period is from 6am to 10pm and night period from 10pm to 6am.
- The outdoor noise target is to be measured at 1-metre from the most exposed, habitable¹ facade of a noise sensitive building.
- For all noise-sensitive land-use and/or development, indoor noise targets for other room usages may be reasonably drawn from Table 1 of Australian Standard/New Zealand Standard AS/NZS 2107:2016 *Acoustics – Recommended Design Sound Levels and Reverberation Times for Building Interiors* (as amended) for each relevant time period.
- Outdoor targets are to be met at all outdoor areas as far as is reasonable and practicable to do so using the various noise mitigation measures outlined in the Guidelines.

The application of SPP 5.4 is to consider anticipated traffic volumes for the next 20 years from when the noise assessment has been undertaken.

In the application of the noise targets, the objective is to achieve:

- Indoor noise levels as specified in *Table 2-1* in noise-sensitive areas (e.g. bedrooms and living rooms of houses); and
- A reasonable degree of acoustic amenity for outdoor living areas on each residential lot.

¹ A habitable room is defined in *State Planning Policy 3.1* as a room used for normal domestic activities that includes a bedroom, living room, lounge room, music room, sitting room, television room, kitchen, dining room, sewing room, study, playroom, sunroom, gymnasium, fully enclosed swimming pool or patio.

3. METHODOLOGY

Noise measurements and modelling have been undertaken in accordance with the requirements of SPP 5.4 and associated Guidelines, as described in *Section 3.1* and *Section 3.2*.

3.1. Site Measurements

Noise monitoring was undertaken at a nearby site (#335) using a Brüel & Kjær 2250 (S/N: 3011946) sound level meter (refer *Figure 3-1*). This meter complies with the instrumentation requirements of *Australian Standard 2702-1984 Acoustics – Methods for the Measurement of Road Traffic Noise*. The meter was field calibrated before and after the measurement session and found to be accurate to within ± 1 dB. Lloyd George Acoustics holds a current laboratory calibration certificate for the meter.

The microphone was approximately 1.4 metres above existing ground level and approximately 18 metres from the edge of Grand Promenade main carriageway. The measurements were recorded on 9 December 2021, between 1.00pm and 2.00pm. Whilst the measurement is relatively old, it is still applicable for model calibration purposes.



Figure 3-1: Photograph of Sound Level Meter on Site

From the one-hour measurement, a relationship between noise levels and the hourly traffic volumes can then be derived to determine the existing $L_{Aeq(\text{Day})}$ and $L_{Aeq(\text{Night})}$ at the measurement location.

3.2. Noise Modelling

The computer program *SoundPLAN 8.2* was utilised incorporating the *Calculation of Road Traffic Noise* (CoRTN) algorithms, modified to reflect Australian conditions. The modifications included the following:

- Vehicles were separated into heavy (Austroads Class 3 upwards) and non-heavy (Austroads Class 1 and 2) with non-heavy vehicles having a source height of 0.5 metres above road level and heavy vehicles having two source heights at 1.5 metres and 3.6 metres above road level;
- A -0.8 dB correction has been applied to the lower level heavy vehicle noise source and -8.0 dB to the higher level noise source based on the *Transportation Noise Reference Book*; Paul Nelson (1987), so as to provide consistent results with the CoRTN algorithms.

Predictions are made at heights of 1.4 metres above floor level and at 1-metre from various rooms of the proposed dwelling, resulting in a + 2.5 dB correction due to reflected noise.

Various input data are included in the modelling and these are discussed in *Section 3.2.1* to *Section 3.2.5*.

3.2.1. Ground Topography

Topographical data was taken from publicly available sources (e.g. *Google*) in the form of spot heights. This was combined with the proposed dwellings and existing neighbouring dwellings to create a 3D noise model.

3.2.2. Road Surface

The corrections applied for different road surface finishes are provided in *Table 3-1*.

Table 3-1: Noise Relationship Between Different Road Surfaces

Chip Seal				Asphalt			
14mm	10mm	5mm	Slurry	Dense Graded	Novachip	Stone Mastic	Open Graded
+3.5 dB	+2.5 dB	+1.5 dB	+1.0 dB	0.0 dB	-0.2 dB	-1.5 dB	-2.5 dB

The existing road surface is dense graded asphalt and assumed to remain unchanged into the future.

3.2.3. Vehicle Speed

The existing posted speed is 60 km/hr and assumed to remain unchanged into the future.

3.2.4. Traffic Volumes

Existing traffic volumes were obtained from Main Roads WA Traffic Map. A modelled Validation Plot and Forecast 2041 traffic volumes were obtained from Main Roads WA (Thomas Ng, Traffic Modelling Analyst, Reference: #42112). The 2041 volumes have then been increased to 2046 using the growth rate from 2021 to 2041. The validation plot allows the forecast volumes to be calibrated with *Table 3-2* providing the traffic volumes used in the noise modelling. Note that the percentage heavy vehicles are assumed to be the same in the future as existing.

Table 3-2: Traffic Information Used in Noise Modelling

Parameter	Scenario			
	Existing – 2021/22		Future – 2046	
	Eastbound	Westbound	Northbound	Southbound
24-hour Volume	8,971	9,021	13,700	12,800
% Heavy	6.1	6.1	6.1	6.1

3.2.5. Ground Attenuation

The ground attenuation has been assumed to be 0.0 (0%) for the roads and 0.5 (50%) outside of the roads, noting that 0.0 represents hard reflective surfaces such as water and 1.0 represents absorptive surfaces such as grass.

4. RESULTS

4.1. Noise Monitoring

The results of the hourly noise level measurements, in free-field conditions, were:

- 9 December 2021: 1.00pm and 2.00pm – 63.3 dB $L_{Aeq,1hour}$.

Combining the measured noise level with the corresponding hourly traffic volume, as shown in *Figure 4-1*, results in 63.2 $L_{Aeq(Day)}$ and 55.4 $L_{Aeq(Night)}$. Based on these results, the $L_{Aeq(Day)}$ is more critical than the $L_{Aeq(Night)}$ since their difference is greater than 5 dB (refer *Section 2*).

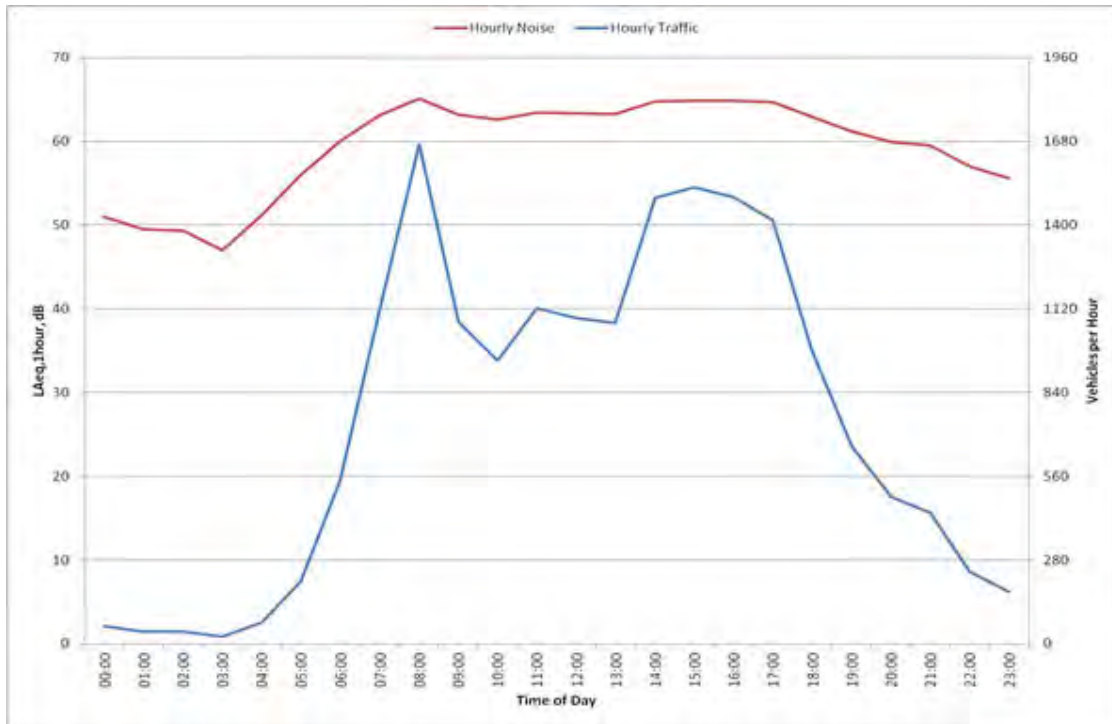


Figure 4-1: Noise Level Relationship to Hourly Traffic Volumes

4.2. Noise Modelling

The noise model was initially set-up for existing conditions and calibrated to the noise measurement location. The model is then updated to include the proposed building and future traffic volumes, maintaining the same model calibration. *Table 4-1* provides the predicted outdoor L_{Aeq(Day)} noise levels to the façade of each habitable room.

Table 4-1: Predicted Future (2046) $L_{Aeq(Day)}$ Outdoor Noise Levels

Unit	Room	$L_{Aeq(Day)}$, dB
1	Bed 2	69
	Bed 1	53
	Entry	69
	Living	53
2	Bed 1	58
	Bed 2	71
	Dining	49
	Entry	69
	Living	69
3	Bed 1	57
	Bed 2	71
	Dining	49
	Entry	69
	Living	69
4	Bed 2	70
	Bed 1	52
	Dining	51
	Entry	70
5	Bed 1	57
	Bed 2	71
	Dining	49
	Entry	69
	Living	69
6	Bed 2	68
	Bed 1	53
	Entry	69
	Living	52

Unit	Room	L _{Aeq} (Day), dB
7	Bed 1	52
	Bed 2	54
	Dining	52
	Entry	53
	Kitchen	54
	Living	54
8	Bed 1	54
	Dining	52
	Entry	55
	Living	52
9	Bed 1	51
	Bed 2	53
	Dining	51
	Entry	54
	Kitchen	54
	Living	54
10	Bed 1	50
	Bed 2	52
	Dining	50
	Entry	54
	Kitchen	54
	Living	55
11	Bed 1	49
	Bed 2	52
	Dining	49
	Entry	54
	Kitchen	54
	Living	55

Unit	Room	L _{Aeq} (Day), dB
12	Bed 1	49
	Bed 2	52
	Dining	49
	Entry	53
	Kitchen	54
	Living	54
13	Bed 1	55
	Dining	49
	Entry	53
	Living	49
14	Bed 1	47
	Bed 2	52
	Dining	47
	Entry	52
15	Bed 1	47
	Bed 2	59
	Dining	43
	Entry	52
16	Bed 1	52
	Dining	46
	Entry	52
17	Bed 1	47
	Bed 2	59
	Dining	43
	Entry	53
	Living	52

Unit	Room	L _{Aeq} (Day), dB
18	Bed 1	46
	Bed 2	52
	Entry	53
	Living	46
19	Bed 1	47
	Bed 2	59
	Dining	43
	Entry	52

5. ASSESSMENT

Where the outdoor noise targets of *Table 2-1* are achieved, no further controls are necessary. As such, *Table 5-1* provides the minimum construction recommended for those areas requiring upgrades.

Table 5-1: Minimum Construction Requirements: Unit 1

Element	Room	Minimum Construction
External Glazing	Units 1, 4, 6 Bedrooms Facing Grand Promenade	Windows to be no more than 20% of floor area. Windows to achieve minimum $R_w + C_{tr} \geq 34$, likely achievable using 10.5mm thick VLam Hush glass in fixed/awning style frame with acoustic seals.
	Units 2, 3, 5 Bedrooms Facing Grand Promenade	Windows to be no more than 20% of floor area. Windows to achieve minimum $R_w + C_{tr} \geq 36$, likely requiring double glazing such as Thermaglaze 6.5/16/6.5 in fixed/awning style frame with acoustic seals.
	Units 2, 3, 5 Living Facing Grand Promenade	Windows to be no more than 20% of floor area. Windows to achieve minimum $R_w + C_{tr} \geq 34$, likely achievable using 10.5mm thick VLam Hush glass in fixed/awning style frame with acoustic seals.
	Units 15, 17, 19 Bedrooms Facing Grand Promenade	Windows to be no more than 20% of floor area. Windows to achieve minimum $R_w + C_{tr} \geq 26$, likely achievable using 6mm thick glass in fixed/awning style frame with acoustic seals.
External Doors	Entry	Door to be minimum 40mm thick solid, timber core door with full perimeter acoustic seals. Any glass inserts to be minimum 10.38mm thick laminated. Alternative door to be $R_w + C_{tr} \geq 31$.
External Walls	Units 1 to 6 Façade Facing Grand Promenade Units 1 & 6 Side on to Grand Promenade Units 2, 3 and 5 Side on to Grand Promenade	Cladding to be min. 8.5mm thick fibre cement to 90mm stud and 90mm thick, 14 kg/m ³ fibrous insulation in the cavity. 2x 13mm thick sound-rated plasterboard on furring channels and resilient mounts to stud.
	Units 15, 17, 19 Bedrooms Facing Grand Promenade Units 7, 9 & 10 Bedrooms	Cladding to be min. 8.5mm thick fibre cement to 90mm stud and 90mm thick, 14 kg/m ³ fibrous insulation in the cavity. 1x 13mm thick sound-rated plasterboard to stud.
	All Others	Cladding to be min. 8.5mm thick fibre cement to 90mm stud and 90mm thick, 14 kg/m ³ fibrous insulation in the cavity. 1x 13mm thick plasterboard to stud.

Element	Room	Minimum Construction
Roof / Ceiling	Units 1 to 6 Bedrooms Facing Grand Promenade	Metal roof with <i>Anticon 50</i> . Ceiling to be 13mm thick sound-rated plasterboard on furring channels (or 2x13mm thick sound-rated plasterboard direct fixed) with R4 fibrous insulation above.
	Units 1 to 6 All Other Rooms with Roof Above	Metal roof with <i>Anticon 50</i> . Ceiling to be 13mm thick sound-rated plasterboard with R4 fibrous insulation above.
Ventilation	Rooms Requiring Upgraded Glazing	Fresh air requirements to be satisfied on the basis of windows closed to affected rooms when occupied. Builder to confirm how this will be achieved. Any ducted fresh air intakes are to be on the side of the house opposite the corridor.
Notification	Lot	As per SPP 5.4 requirements.

By implementing the construction recommendations, noise levels are calculated to comply with the indoor noise target of SPP 5.4. Alternative constructions can be accepted provided these are supported by a laboratory certificate.

It should be noted that the recommendations are to comply with the minimum standard of SPP 5.4. Compliance with the indoor noise target does not result in all residents considering the noise level as acceptable, since this is a subjective response. Where a resident is particularly sensitive to noise, they may wish to consider additional upgrades.

Appendix A – Proposed House Plans

Appendix B – Terminology

The following is an explanation of the terminology used throughout this report:

- **Decibel (dB)**

The decibel is the unit that describes the sound pressure levels of a noise source. It is a logarithmic scale referenced to the threshold of hearing.

- **A-Weighting**

An A-weighted noise level has been filtered in such a way as to represent the way in which the human ear perceives sound. This weighting reflects the fact that the human ear is not as sensitive to lower frequencies as it is to higher frequencies. An A-weighted sound level is described as L_A , dB.

- **L_{eq}**

The L_{eq} level represents the average noise energy during a measurement period.

- **$L_{Aeq(Day)}$**

The $L_{Aeq(Day)}$ level is the logarithmic average of the L_{Aeq} levels from 6.00am to 10.00pm.

- **$L_{Aeq(Night)}$**

The $L_{Aeq(Night)}$ level is the logarithmic average of the L_{Aeq} levels from 10.00pm to 6.00am.

- **Noise-sensitive land use and/or development**

Land-uses or development occupied or designed for occupation or use for residential purposes (including dwellings, residential buildings or short-stay accommodation), caravan park, camping ground, educational establishment, child care premises, hospital, nursing home, corrective institution or place of worship.

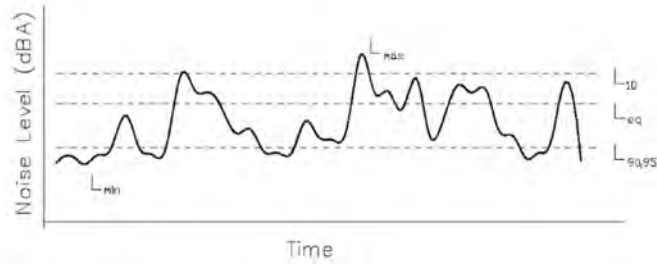
- **R_w**

This is the weighted sound reduction index. It is a single number rating determined by moving a grading curve in integral steps against the laboratory measured transmission loss until the sum of the deficiencies at each one-third-octave band, between 100 Hz and 3.15 kHz, does not exceed 32 dB. The higher the R_w value, the better the acoustic performance.

- **C_{tr}**

This is a spectrum adaptation term for airborne noise and provides a correction to the R_w value to suit source sounds with significant low frequency content such as road traffic or home theatre systems. A wall that provides a relatively high level of low frequency attenuation (i.e. masonry) may have a value in the order of -4 dB, whilst a wall with relatively poor attenuation at low frequencies (i.e. stud wall) may have a value in the order of -12 dB.

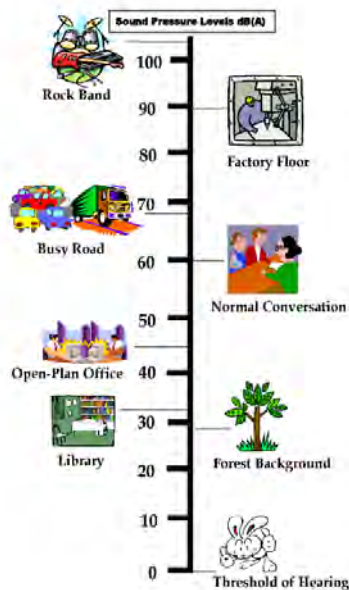
- **Chart of Noise Level Descriptors**



- **Austrroads Vehicle Class**

VEHICLE CLASSIFICATION SYSTEM AUSTRROADS		
CLASS 1: LIGHT VEHICLES		
1	Small cars, vans, mopeds, motorbikes, motorcycles	
2	Small trucks, coaches, buses	
CLASS 3: HEAVY VEHICLES		
3	Two axle trucks or bus (2 axles)	
4	Three axle trucks or bus (3 axles, 2 axle groups)	
5	Four axle trucks or bus (4 axles, 2 axle groups)	
6	Five axle articulated (5 axles, 3 axle groups)	
7	Six axle articulated (6 axles, 3 or 4 axle groups)	
8	Five axle articulated (5 axles, 3 or 4 axle groups)	
9	Six axle articulated (6 axles, 3 or 4 axle groups)	
CLASS 10: SPECIAL VEHICLES AND ROAD TRAINS		
10	Articulated heavy truck (10 axles, 4 axle groups)	
11	Double road train (11 axles, 5 or 6 axle groups)	
12	Triple road train (12 axles, 5 or 6 axle groups)	

- **Typical Noise Levels**



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Attachment 7e - Waste Management Plan prepared by Urbii

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322 Grand Promenade, Dianella

Proposed Residential Development

Waste Management Plan



Prepared for:
Dale Alcock Projects

November 2025

322 Grand Promenade, Dianella

Prepared for: Dale Alcock Projects
Prepared by: Paul Ghantous
Date: 23 November 2025
Project number: U25.090

City of Stirling
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Version control

Version No.	Date	Prepared by	Revision description	Issued to
U25.090.r02	18/09/25	PG	FINAL	Dale Alcock Projects
U25.090.r02a	23/11/25	PG	REVISED FINAL	Dale Alcock Projects



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1 Introduction

This Waste Management Plan has been prepared by **Urbii** on behalf of **Dale Alcock Projects** with regards to the proposed residential development, located at **322 Grand Promenade, Dianella**.

The subject site is situated on the north-eastern side of Grand Promenade and extends to Chester Avenue, as shown in Figure 1. The site is presently vacant and is surrounded by a mix of residential, education, commercial and retail land uses.

It is proposed to develop the site into a residential development, delivering 19 grouped dwellings.

The key issues that will be addressed in this WMP include calculation of the waste generation of the site, assessment of waste storage provisions and documentation of the waste collection arrangements.

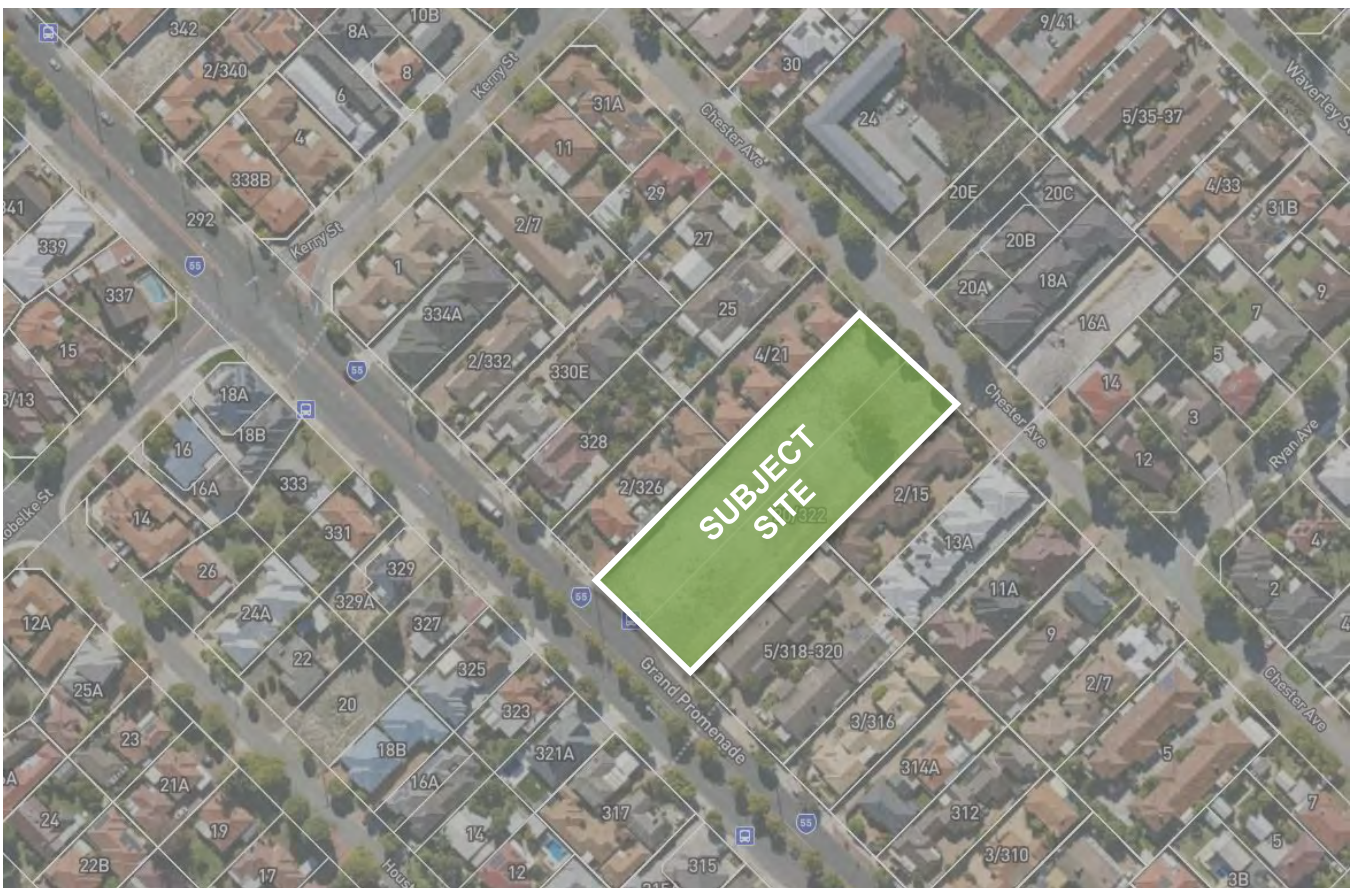


Figure 1: Subject site



2 Objectives

The objectives of this WMP are adapted from WALGA:

- Ensure that the long-term waste management needs for the development are met in an efficient and sustainable manner.
- Minimise the impact of waste services and facilities on the streetscape and surrounds, in relation to both the footpath/public realm and the frontage of the development.
- Reduce the impact of waste collection services and facilities on the amenity of the locality particularly in terms of noise and odour.
- Maximise safety for both waste collection staff and the public.
- Minimise traffic and footpath obstruction.

3 Referenced documents

The documents referenced in preparing this WMP may include, but are not limited to:

- City of Melbourne *Guidelines for Waste Management Plans 2021*;
- City of Perth *Waste Guidelines for all Developments 2019*;
- WALGA *Multiple Dwelling Waste Management Plan Guidelines*;
- WALGA *Subdivision Waste Management Plan Guidelines*; and,
- Waste Authority WA *Waste Avoidance and Resource Recovery Strategy for 2030*.



4 Guiding concepts

Urbii adopts the guiding concepts of the State's Waste Strategy and encourages these concepts to be considered in all developments to the furthest extent feasible.

4.1 Waste hierarchy

The *Waste Avoidance and Resource Recovery Strategy 2030* applies the waste hierarchy (Figure 2), which is a widely accepted decision-making tool. The waste hierarchy ranks waste management options in order of their general environmental desirability. Waste avoidance is the most preferred option in the hierarchy.

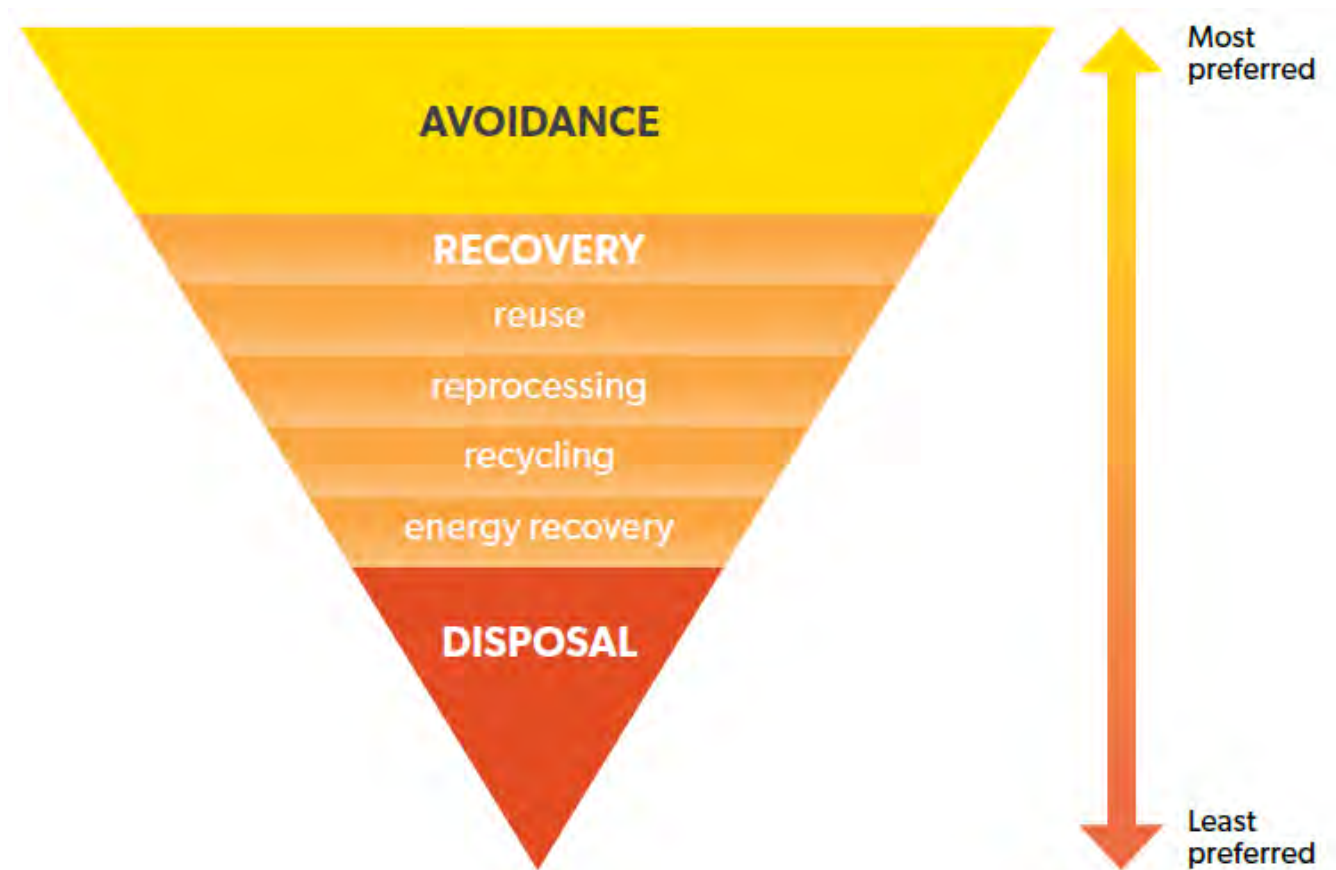


Figure 2: Waste hierarchy

Source: Waste Authority WA *Waste Avoidance and Resource Recovery Strategy for 2030*.

Resource recovery options recover value from materials, thereby offsetting the environmental impacts of extracting and processing raw materials. Energy recovery is the least preferred recovery option. Disposal is the least preferred option. Disposal generally recovers the least value from materials and delivers the least environmental benefit.

4.2 Circular economy

A circular economy (Figure 3) makes use of established sustainability concepts, including life cycle thinking and resource efficiency. A circular economy should consider the flow of both materials and energy. It moves away from the linear 'take, make, use and dispose' model, to one which keeps materials and energy circulating in the economy for as long as possible.

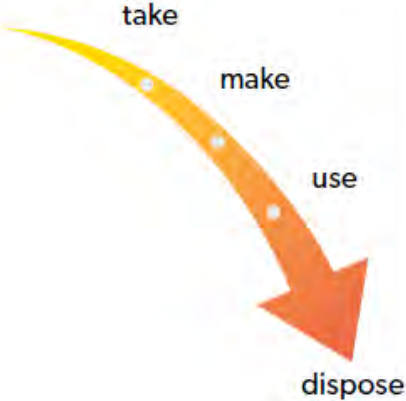

Current approach	Circular economy
	
Linear flow of materials – 'take, make, use and dispose' model.	Circular flow of materials – materials sorted and retained in the economy for as long as possible.
Limited use of renewable materials and energy.	Preference for renewable materials and energy.
Significant volumes of materials disposed of and lost to the economy. Loss of embodied materials, energy and water.	Materials recovered as high up the waste hierarchy as possible. Embodied materials, energy and water retained in the economy. Organic materials re-enter and regenerate the environment safely (for example, as compost).
Materials managed locally and globally.	Preference to manage materials locally to reduce the costs and impacts of transport, and to provide local employment and investment opportunities.
Economic value of materials, employment and investment not fully accounted for.	Economic value of materials, employment and investment accounted for.
Limited focus on life cycle thinking.	Products designed and manufactured to minimise environmental impact through whole of life.

Figure 3: Transitioning to a circular economy



5 Proposed development

The proposal for the subject site is for a grouped dwelling residential development, comprising:

- A total of 19 residential dwellings;
- 10 resident car parking bays;
- 6 visitor car parking bays;
- 12 bicycle parking spaces for the shared use of residents and visitors;
- Communal resident facilities including playground, undercover recreation area, barbeque and garden; and,
- A centralised bin store for the development.

Vehicle access to the site is proposed via one crossover on Grand Promenade and one crossover on Chester Avenue. Waste collection will be undertaken onsite within the car park.

People walking and cycling will access the development from the external path network abutting the site.

The proposed development plans are included for reference in Appendix A.

6 Waste generation

6.1 Waste generation rates

The waste generation rates for general waste and recyclables are sourced from the WALGA Guidelines. Residential waste generation rates for different multiple dwelling sizes are detailed in Table 1.

Table 1: Residential waste generation rates

Waste Stream	Dwelling Size	Waste Generation Rate/ Storage Requirement	Waste Generation Rate/ Storage Requirement
		<12 dwellings	>12 dwellings
General Waste (on FOGO system)	1 bedroom	30L/week	40L/week
	2 bedroom	50L/week	60L/week
	3+ bedroom	70L/week	80L/week
Recycling	1 bedroom	40L/week	20L/week
	2 bedroom	60L/week	40L/week
	3+ bedroom	120L/week	90L/week
FOGO	1 bedroom	40L/week	20L/week
	2 bedroom	80L/week	40L/week
	3+ bedroom	140L/week	60L/week

6.2 Waste generation calculations

The waste generation calculations are detailed in Appendix D. The estimated waste generation for the development is:

- General Waste: around 1,100L per week.
- Recyclables: around 720L per week.
- FOGO: around 720L per week.



7 Waste systems

7.1 Internal waste storage

Designers should aim to incorporate sufficient space within the kitchen, laundry room or other convenient location within each dwelling for the temporary storage of accumulated waste and recycling. Space should be sufficient to allow for the separate storage of recyclables and general waste. Possible ways to encourage recycling and minimise contamination include:

- Provision of adequate internal storage space within dwellings (kitchen or laundry) to accommodate the temporary storage of at least 2 days' worth of general waste, organics (where applicable) and recycling. This should comprise:
 - A minimum 30L general waste bin
 - A minimum 30L commingled recyclables bin
- Provision of reusable, robust containers to residents to assist them in transporting recyclable materials from their dwelling to recycling bins without resorting to plastic garbage bags e.g., reusable, washable tote-bags.
- Posting signage in public areas of the building (including the bin store) to educate residents/tenants about the location and use of the waste management system, including what materials are suitable for recycling and composting.
- Providing information in Strata and/or sales documents, to educate residents/tenants about the location and use of the waste management system, including what materials are suitable for recycling and general waste.

7.2 External bin storage areas

Bin storage areas for this development must be adequate to contain all waste and recycled material generated on the premises for the proposed waste collection frequency.

7.2.1 Bin size, quantity and colour

It is proposed to provide the following bins in a centralised bin store:

- 4 x 660L General waste (red lid bin).
- 3 x 660L Commingled recycling (yellow lid bin).
- 2 x 660L FOGO (green lid bin).

The number of bins required for the development is detailed in Appendix D.

7.2.2 Bin storage area size

As detailed in Table 2, each 660L bin has a footprint area of 1.16m². A 50mm gap is allowed between the bins to allow easy pull movement, and suitable clearance allowed between opposite bins and in walkways to maintain convenient walking access. The proposed bin storage area size is sufficient to accommodate the required bins. An indicative bin layout plan is shown in Appendix B.

Table 2: Larger Mobile Garbage Bin (MGB) dimensions

Bin capacity	660L	770L	1100L	1300L	1700L
Height (mm)	1250	1425	1470	1408	1470
Depth (mm)	850	1100	1245	1250	1250
With (mm)	1370	1370	1370	1770	1770
Approximate Footprint (m ²)	1.16	1.5	1.7	1.21	1.27

Source: WALGA

7.2.3 Bin storage area design

Urbii has checked the proposed bin storage location and confirmed that required clearances are provided. A bin storage plan is included in Appendix B.

. The bin store will provide the following features:

- **Ventilation and odour:** For enclosed storage and service areas, the air flowing from any storage areas should not exit close to residences. Ventilation openings should be protected against flies and vermin and located as near the ceiling and floor as possible, but away from the windows of dwellings. If a forced ventilation or air conditioning system is used (for enclosed storage areas), It should be in accordance with the ventilation requirements of the Building Code of Australia and *Australian Standard AS 1668.2 The use of Ventilation and Air Conditioning in Buildings*. It should not be connected to the same ventilation system supplying air to the units.
- **Lighting:** Artificial light controlled by switches will be located near the bin store entrance.
- **Noise:** Bins will be collected from the development car park.
- **Aesthetics:** The bin store is consistent with the overall aesthetics of the development.
- **Vermin:** Self-closing doors will be provided to eliminate access to vermin. Doors will be able to be locked open when required.
- **Washing bins and waste storage area:** The internal bin store will have bin-washing facilities including an adequate supply of water with hose cock and have floor drainage installed. The building caretaker will be responsible for washing bins (or contracting a provider to wash bins) and for maintenance of the bin store.



7.3 Access to bins

Residents will access the bin store through two access doors from the car park. The waste collection contractor (City of Stirling) will access the bin store to wheel bins to the adjoining site car park for collection. Once bins are emptied, they will be returned to the bin store by the contractor.

Access to the bin store will be provided to the contractor. If the bin store is locked then this will include a key or pin pad number to unlock the bin store door.

8 Waste collection

8.1 Waste vehicle types

Waste collection will be serviced by the City of Stirling. A rear loader waste truck will be used to collect waste from the site car park on scheduled waste collection days. Correspondence from the City of Stirling waste department indicates that the following waste truck specification is likely to be used, as detailed in Figure 4.

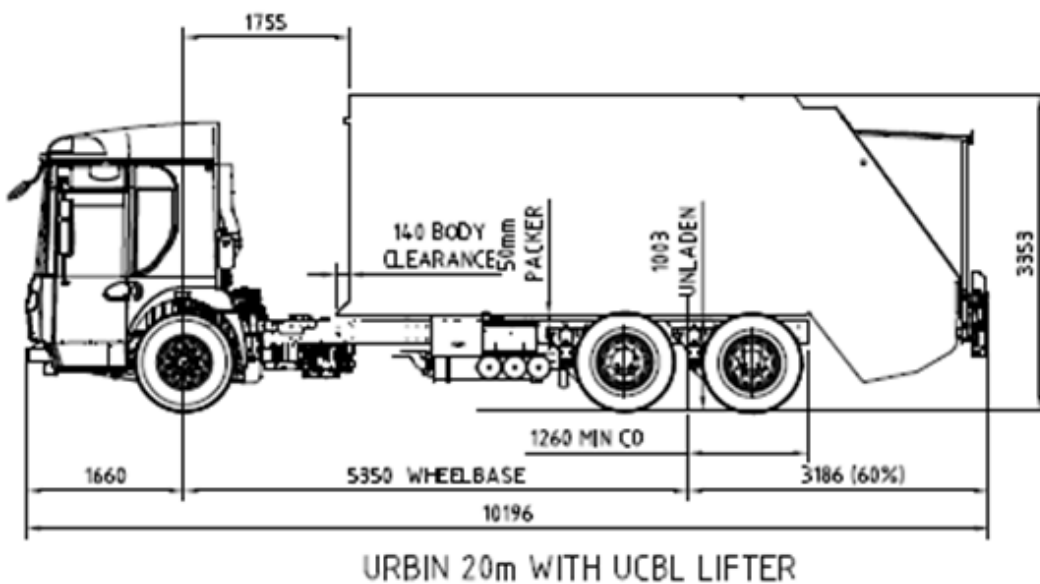


Figure 4: City of Stirling waste truck

8.2 Waste collection frequency

By the time the development is completed, it is anticipated that the City may have transitioned to a 3-bin FOGO service. The following waste collection regime is therefore proposed:

- General waste: Standard 660L bin service collected fortnightly.
- Recycling: Standard 660L bin service collected fortnightly.
- FOGO: Standard 660L service collected weekly.

8.3 Waste collection method and presentation points

Waste collection will be facilitated internally within the car park accessed from Chester Avenue. The City's waste department has advised they are able to provide an onsite collection service. Waste vehicles will reverse into the car park from Chester Avenue. Chester Avenue is a minor access road carrying local traffic only. Therefore, no issues are anticipated with reversing waste trucks into the site.



Australian Standard AS2890.2 *Off-street commercial vehicle facilities*, permits a maximum of one reversing manoeuvre (entry or exit) from a minor road, subject to permission from the relevant authority:

3.2.3.2 Minor road access

Where providing regular service from a minor road, manoeuvring on-street, if permitted by the relevant authority, shall be strictly limited to one reverse movement either onto or off the street, and be subject to determination of both safety and obstruction to other on-street traffic.

NOTE The AV vehicle class is the largest vehicle to be considered for reverse manoeuvres.

The swept path of the maximum size design vehicle using the facility may be allowed to occupy the entire width (less specified clearances) of a two-way access driveway when the vehicle is entering or leaving the minor road.

The City of Stirling will likely require an agreement to be signed by the building management company or representatives to give permission for staff to enter into the property to undertake waste collection. This typically includes an indemnity agreement for council staff.

Access to the bin store will be provided to the contractor. If the bin store is locked then this will include a key or pin pad number to unlock the bin store door.

8.4 Vehicle access and manoeuvring

Swept path analysis was undertaken using the City's waste vehicle template, detailed in Figure 4.

The swept path analysis is presented in Appendix C, and confirms that there is satisfactory road and intersection geometry for waste truck access and manoeuvring.

8.5 Consultation with the City of Stirling waste department

Following email correspondence with the City of Stirling Waste & Fleet department on 9 July 2025, the City confirmed that collections for the development may be provided via onsite drive-on rear-loader service, with the truck reversing in from Chester Avenue to the onsite bin store.

Onsite servicing is subject to a Property Manager/Strata waiver limiting liability and may attract a fee if approved by Council (indicatively \$40 per visit).

The City operates a three-bin system (general waste weekly, recycling fortnightly, garden organics fortnightly). FOGO is not expected to be introduced until FY26/27 at the earliest (apartments likely in FY27/28, both subject to Council approval).

The City also advised it is developing updated waste guidelines for new developments (timing towards the end of the year) and confirmed that reversing in off Chester Avenue is acceptable for the onsite collection arrangement.

9 Additional waste requirements

9.1 Bulk waste

Bulk waste can be temporarily stored in the bin store, or individual store rooms until it is removed by either private service or the City of Stirling.

The City replaced household bulk hard waste verge collections with a range of on demand collections, allowing residents to dispose of these items year-round, at a time when it is needed.

Residents can book online for on demand bulk waste collection.

9.2 E-Waste

Storage space for E-waste will be accommodated in storage areas or individual dwellings. E-waste will be disposed of in a suitable manner, such as bulk drop-off to the tip or using public battery recycling boxes.

9.3 Garden organics

The site caretaker will manage garden organic waste associated with landscape maintenance. Garden waste can be placed in bins if there is space or can be removed by trailer to be disposed of offsite in a suitable location.



10 Waste management

The building caretakers will be responsible for:

- Cleaning the bin storage areas and facilities; and,
- Regularly cleaning their bins.

Residents will dispose of their waste in the bin store. Residents should comply with the City's waste sorting requirements and only place permitted waste in each respective bin type. Waste that does not belong in any bin should be disposed of through another appropriate method.

A copy of the Waste Management Plan will be maintained within the premises and strata plan for reference and records.

11 Signage and education

Ongoing education, to support the waste management service, is one of the most important factors in encouraging residents to continue to utilise services and systems as originally intended. The following recommendations have been adapted from the WALGA Guidelines.

“Educational signage should:

- Clearly identify what items are and are not accepted in the general waste and recycling systems. If signage within the bin store is not possible due to space or other restrictions, bin stickers may be appropriate.*
- Outline appropriate waste management behaviour i.e. placing refuse/recyclables inside as opposed to adjacent to bins, placing mixed recyclables into the bin loose (not in a plastic bag), closing bin lids etc.*
- Where the strata body/building management holds tenants’ induction schemes, these should include the use of waste and recycling facilities. The strata body/building management, in conjunction with the Local Government, should issue a leaflet on the correct use of the waste and recycling facilities and the materials recycled. Tenants’ handbooks should include a section on the correct use of general waste and recycling facilities.*
- Ensuring education is ‘ongoing’ is beneficial because it tackles the transient nature of residents and differences between different Local Government services. All waste and recycling bins or receptacles should be clearly and correctly labelled and signage should be erected in bin storage areas to instructing residents as to the correct separation of recyclables from general waste.*
- Any hazards or potential dangers associated with the waste facilities, including those from the use of any waste handling equipment, should also be clearly identified. It is recommended that building managers post information in communal areas which clearly identified the relevant points of contact regarding recycling and/or other services within the development. As part of the ongoing education program, welcome packs should be produced and provided to all new residents. These packs should contain information outlining the developments waste management system, required actions and appropriate waste management behaviours.”*



12 Conclusion

As demonstrated within this Waste Management Plan, the proposed residential development provides sufficient bin storage and adequate bins to service the site for general waste and recyclables.

Furthermore, the servicing of the bins by the City of Stirling can be adequately achieved without having an adverse impact on the site and the local street network.

Appendices

Appendix A: Proposed development plans





OVERALL SITE PLAN
SCALE: 1:500

A.	PREPARED BY	MDC ARCHITECTS
B.	DESIGNED BY	MDC ARCHITECTS
C.	APPROVED BY	MDC ARCHITECTS
D.	DATE	11/08/2025
E.	PROJECT NO.	322 GRAND PROMENADE
F.	CLIENT	DALE ALCOCK PROJECTS
G.	PROJECT ADDRESS	322 GRAND PROMENADE, DIANELLA, VIC 3086
H.	PROJECT TYPE	CONCEPT

Use Mapped Dimensions of all items. Do not scale or measure off the plan. All dimensions are in millimetres unless otherwise stated. All dimensions are to the centre of the item unless otherwise stated. All dimensions are to the centre of the item unless otherwise stated. The client warrants the accuracy of the information and documents provided to MDC ARCHITECTS and accepts that MDC ARCHITECTS is not responsible for any errors or omissions.



MDC
ARCHITECTS

272 GARDNER STREET, BRISBANE, QUEENSLAND 4000
PH: 07 3250 1000
WWW.MDCARCHITECTS.COM.AU



DALE
ALCOCK
PROJECTS

322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING
OVERALL SITE PLAN
SCALE: 1:500
DATE: 11/08/2025
PROJECT NO: 322 GRAND PROMENADE
CLIENT: DALE ALCOCK PROJECTS
PROJECT TYPE: CONCEPT



OVERALL FIRST FLOOR PLAN

SCALE: 1:200

A.	DATE:	11/08/2024
B.	BY:	MDC ARCHITECTS
C.	FOR:	DALE ALCOCK PROJECTS
D.	PROJECT:	322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING
E.	DATE OF PREVIOUS EDITION:	11/08/2024
F.	REVISION:	CONCEPT

Use Metric dimensions in all drawings. Do not scale or measure off drawings. All dimensions are to be taken from the finished floor level. The design is subject to the approval of the relevant authorities and is not to be used for any other purpose without the approval of MDC ARCHITECTS and Dale Alcock Projects.



MDC ARCHITECTS
272 GARDNER STREET, BRIDGEVIEW VIC 3048
PH: 03 9330 1000
WWW.MDCARCHITECTS.COM.AU




322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING
OVERALL FIRST FLOOR PLAN
DATE: 11/08/2024
BY: MDC ARCHITECTS
FOR: DALE ALCOCK PROJECTS
PROJECT: 322 GRAND PROMENADE, DIANELLA - GROUPED HOUSING
DATE OF PREVIOUS EDITION: 11/08/2024
REVISION: CONCEPT




OVERALL ROOF PLAN

A.	11/20/2024	REVISION	REVISION 1 - AS PER APPROVALS
B.	11/20/2024	REVISION	REVISION 2 - AS PER APPROVALS
C.	11/20/2024	REVISION	REVISION 3 - AS PER APPROVALS
D.			
E.			
F.			
G.			
H.			
I.			
J.			
K.			
L.			
M.			
N.			
O.			
P.			
Q.			
R.			
S.			
T.			
U.			
V.			
W.			
X.			
Y.			
Z.			

Use all dimensions as shown. Do not scale or measure off drawings. All dimensions are in millimeters. All dimensions are to the center of the element unless otherwise stated. The client warrants the accuracy of all dimensions and contents hereof and shall be responsible for any errors or omissions. All dimensions are to be used for construction purposes only. The client warrants the accuracy of all dimensions and contents hereof and shall be responsible for any errors or omissions.



MDC ARCHITECTS
272 GARDNER STREET, BRISBANE QLD 4000
AUSTRALIA

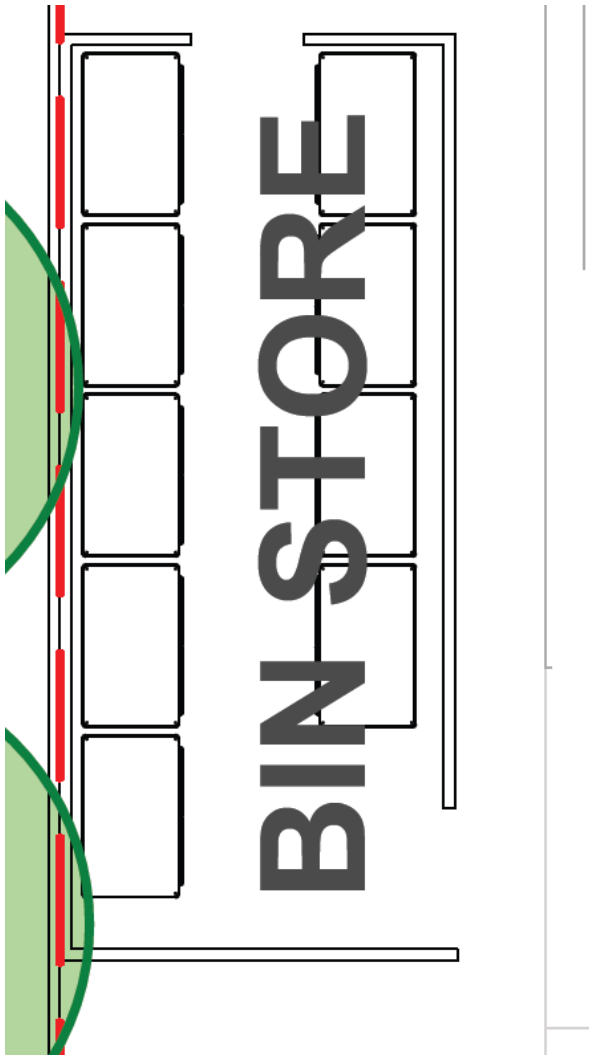


DALE ALCOCK PROJECTS

322 GRAND PROMENADE, DIABELLA - GROUPED HOUSING
OVERALL ROOF PLAN

DATE: 11/20/2024
DRAWING NO: 11/20/2024
CONCEPT

Appendix B: Bin storage and presentation plans



Appendix C: Swept path analysis

Swept path diagrams are included in this section of the report. Different coloured lines are employed to represent the various envelopes of the vehicle swept path, as described below:

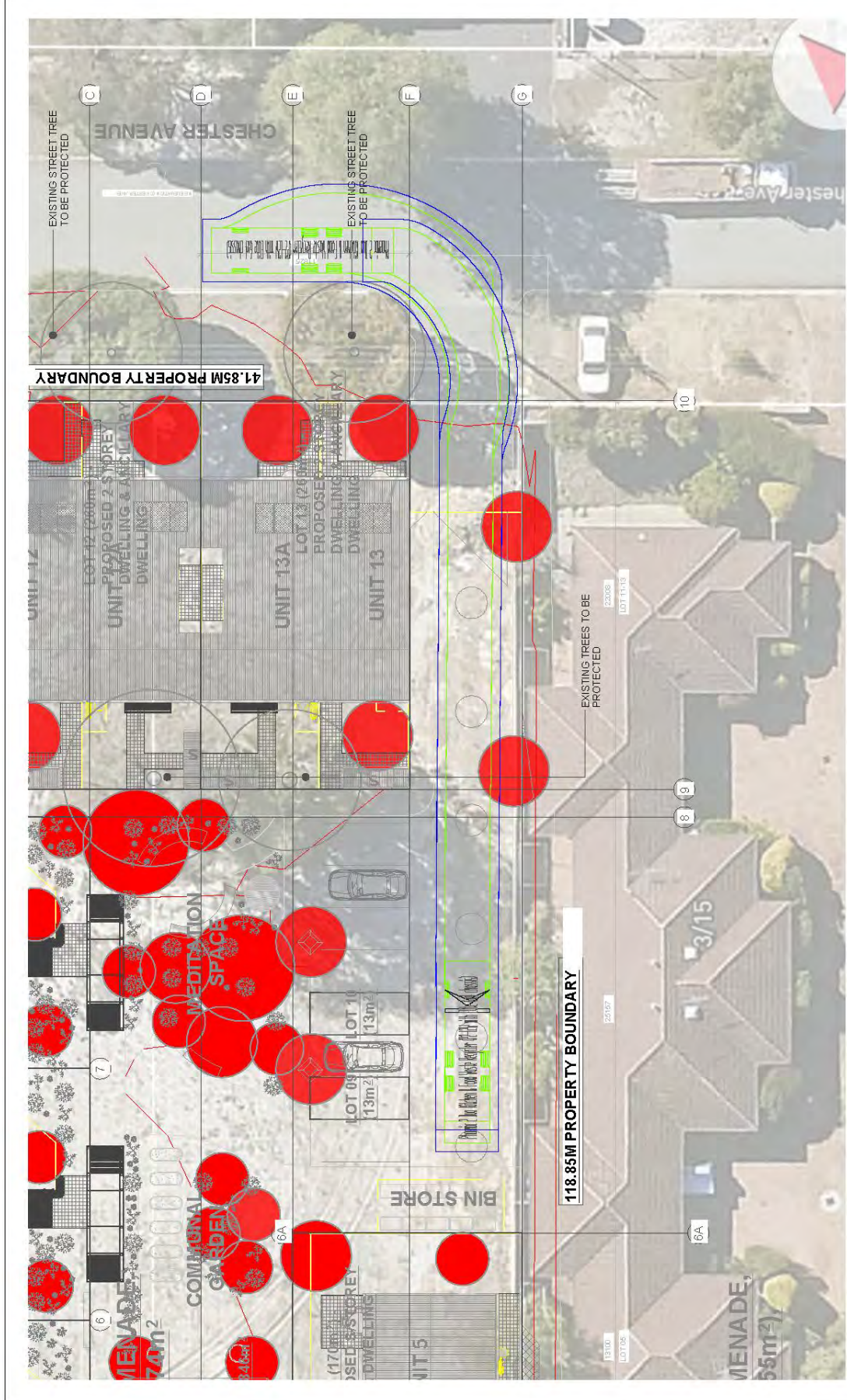
Cyan represents the wheel path of the vehicle

Green represents the vehicle body envelope

Blue represents a 500mm safety buffer line, offset from the vehicle swept path

The swept path diagrams are also provided separately in high-quality, A3 PDF format.



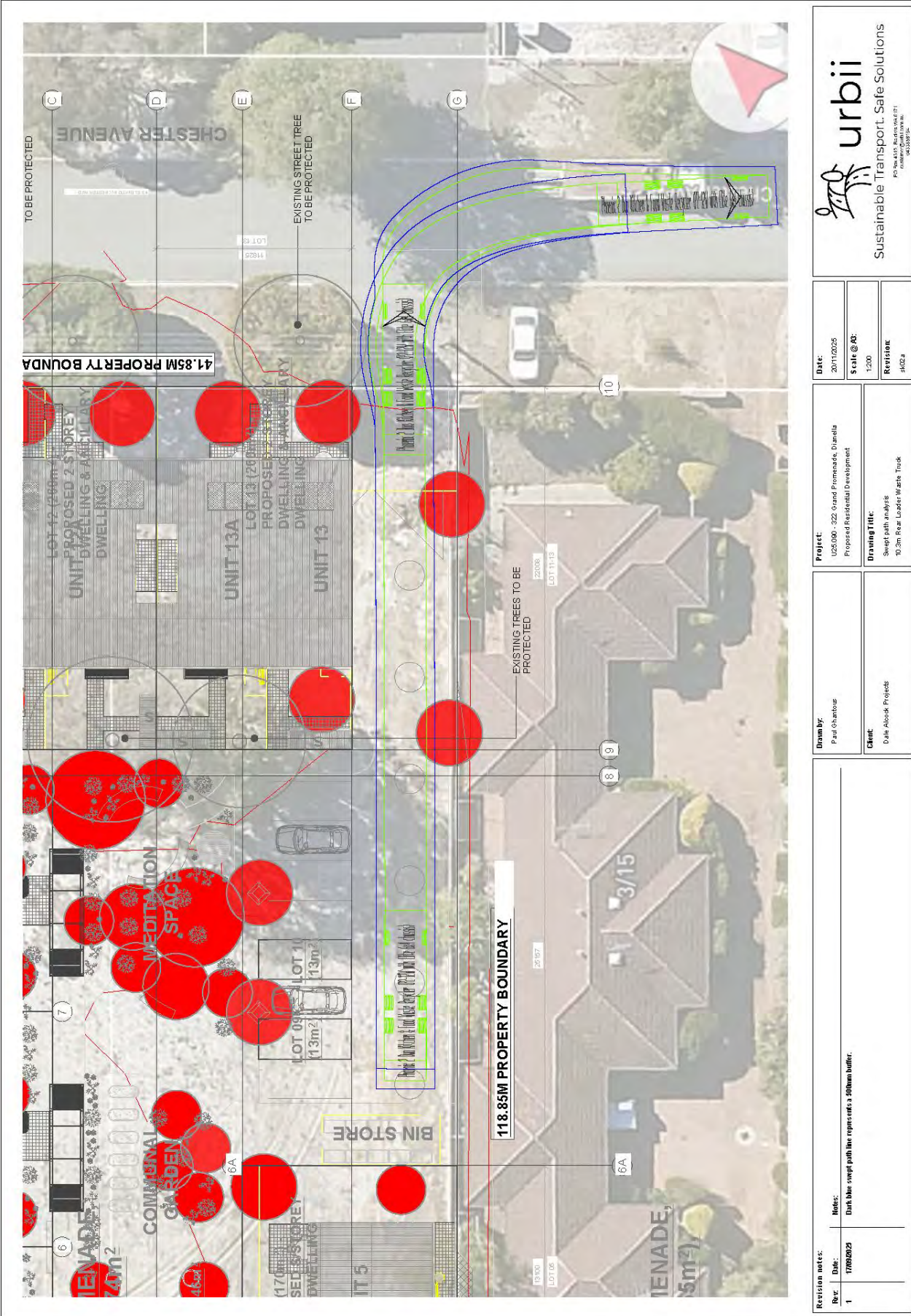


Revision notes: Rev. 1 Date: 17/05/2023 Notes: Dark blue swept path line represents a 500mm buffer.		Drawn by: Paul Chantous		Project: U25.090 - 322 Grand Promenade, Dianella Proposed Residential Development		Date: 20/11/2025	
		Client: Ode Developments		Drawing Title: Swept path analysis 10.2m Rear Loader Waste Truck		Scale @ A0: 1:200	
						Revision: #01a	



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Melbourne, VIC 3048
www.urbii.com.au



Revision notes: Rev: 1 Date: 17/09/2023 1 Dark blue swept path line represents a 500mm buffer.		Drawn by: Paul Chantous		Project: U25000 - 322 Grand Promenades, Danella Proposed Residential Development		Date: 20/11/2025	
		Client: Dale Alcock Projects		Drawing Title: Swept path analysis 10.2m Rear Loader Waste Truck		Scale @ A0: 1:200	
				Drawing Title: Swept path analysis 10.2m Rear Loader Waste Truck		Revision: #02.a	

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PO Box 4157, Business West EFTI
 1000 Lakeside Drive, Mt. Wellington, NZ
 021 780 0000

Appendix D: Waste calculations

Table 3: Weekly waste generation and collection – Residential 3 bin system

2 Bin System
 3 Bin System

Clear Form
Recalculate

Residential Less Than 12 Dwellings

Number of 1 Bedroom Apartments	0
Number of 2 Bedroom Apartments	0
Number of 3 or more Bedroom Apartments	0

Residential Greater Than 12 Dwellings

Number of 1 Bedroom Apartments	2
Number of 2 Bedroom Apartments	17
Number of 3 or more Bedroom Apartments	0

Results - Weekly Waste Generation

Residential Dwellings			
General Waste	Food & Garden Organics	Comingled Recyclables	Total
1100L	720L	720L	2540L
Commercial Food & Beverage			
General Waste	Food & Garden Organics	Comingled Recyclables	Total
0L	0L	0L	0L
Retail			
General Waste	Food & Garden Organics	Comingled Recyclables	Total
0L	0L	0L	0L
Other Commercial			
General Waste	Food & Garden Organics	Comingled Recyclables	Total
0L	0L	0L	0L
Totals			
General Waste	Food & Garden Organics	Comingled Recyclables	Total
1100L	720L	720L	2540L



General Waste

Weekly generation (L)	Bin size (L)	Number of bins	Weekly collection frequency	Weekly capacity (L)
1100	660	4	0.5	1320

Recycling

Weekly generation (L)	Bin size (L)	Number of bins	Weekly collection frequency	Weekly capacity (L)
720	660	3	0.5	990

FOGO

Weekly generation (L)	Bin size (L)	Number of bins	Weekly collection frequency	Weekly capacity (L)
720	660	2	1	1320

City of Stirling
3 Dec 2025
RECEIVED

Attachment 8a - Water Corporation Referral Response



Your Ref: DA25/1830
Our Ref: 208696730 - DAP431666
Enquiries: Kerry Moore
Direct Tel: 9420 2209
Email: building.services@watercorporation.com.au

23 March 2026

City Of Stirling
PO BOX 130
OSBORNE PARK WA 6017

Attention of: Frank Callisto

Re: Lots 28 & 29, 322A Grand Promenade Dianella

Reticulated water is available to the subject lot. A 20mm water service exists on LOT 28 whereas LOT 29 does not have a water service.

Wastewater

Reticulated sewerage is available to the subject lot. Any portion of the proposed building which is within the zone of influence to sewer main may require suitable footings in accordance with our technical guidelines. Please refer to our website: www.watercorporation.com.au/Developing-and-building/Working-near-assets.

Approval for works

Any works carried out in proximity to our Assets must receive prior approval by applying for an Asset Protection Risk Assessment (APRA). To assess whether the proposed development will require an APRA, details of the Prescribed Proximities are available on our website: www.watercorporation.com.au/Developing-and-building/Working-near-assets/Approval-for-works.

Building Approval Application

The applicant is required to submit a Multi Residential Application by using our online portal BuilderNet: login-builder.net.watercorporation.com.au.

Attachments required for approval will include:

- Final construction site & architectural floor plans
- Engineer certified piling detail plans (if required)

The information provided above is subject to review and may change. If the proposal has not proceeded within the next 6 months, please contact us to confirm that this information is still valid. Please provide the above comments to the landowner, developer and/or their representative.

Should you have any queries or require further clarification on any of the above issues, please do not hesitate to contact our Enquiries Officer.

Kind regards

Kerry Moore

Kerry Moore
Advisor - Building Services
Development Services
Assets Planning & Delivery Group
E building.services@watercorporation.com.au
T 13 13 95



watercorporation.com.au

Attachment 8b - Public Transport Authority of Western Australia

From: Salkeld, Christopher <Christopher.Salkeld@pta.wa.gov.au>
Sent: Thursday, 23 April 2026 1:18 PM
To: Meron Nega
Cc: Holden, Brad; Robinson, Richard; Bus Stops
Subject: RE: 322A Grand Promenade Stirling

OFFICIAL

Hello Meron,

Our team has reviewed the development application and does not identify any significant issues that would prevent approval. The only matters we note for consideration are outlined below.

1. Line of Sight – Grand Promenade Access

Due to the number of bus services and their respective service frequencies at this location, sight distance for vehicles exiting the site from the Grand Promenade frontage may be affected if a bus is stationary within the bay (refer to Image 1). This is dependent on where line-of-sight measurements have been taken—either at the kerb edge or 3 metres back, in accordance with MRWA standards.

If the developer's designer has considered this scenario, deemed the risk acceptable, and confirms that PTA is indemnified against any future complaints from residents regarding sight-line limitations, then PTA is satisfied to accept this arrangement.

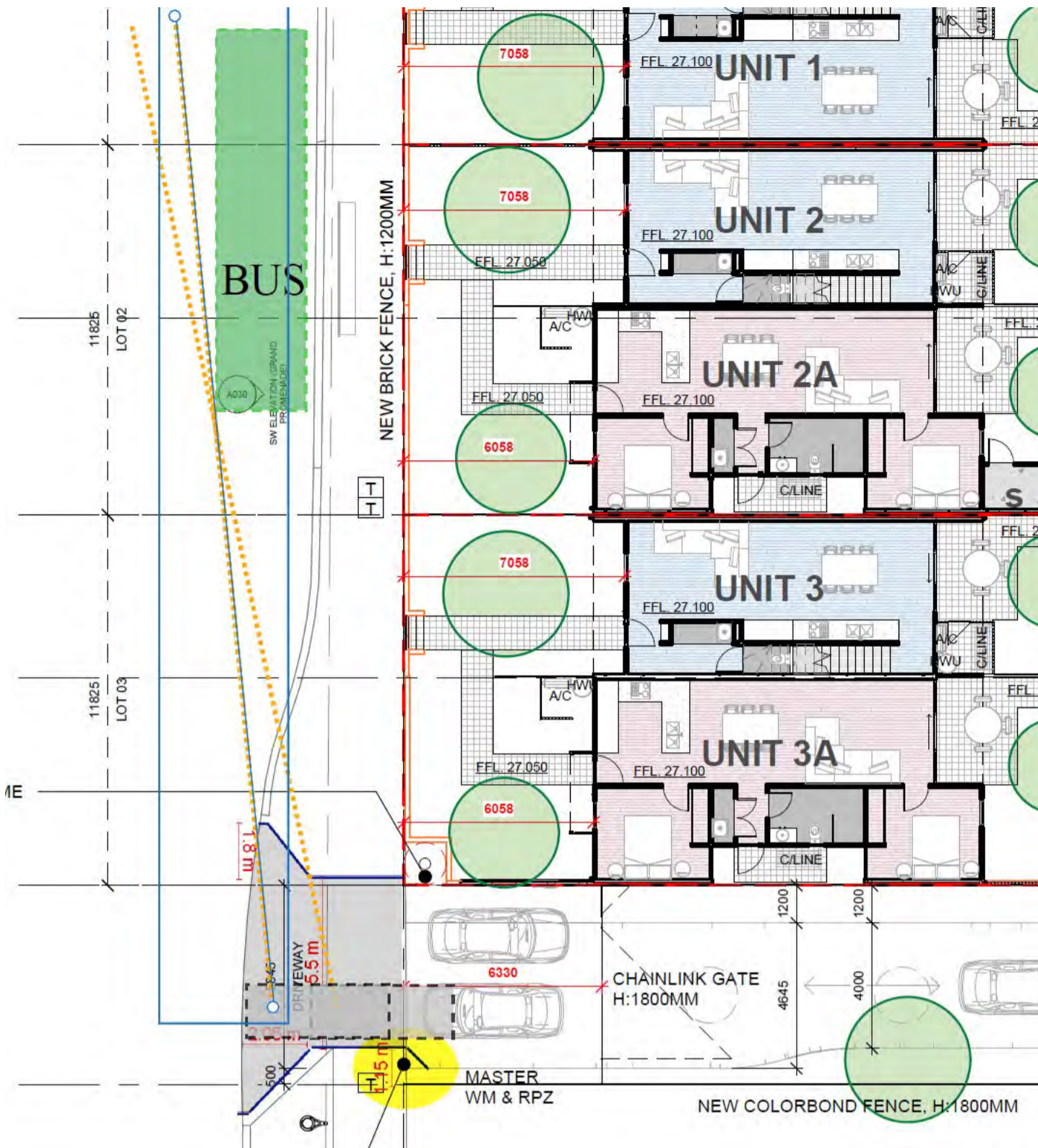
2. Bus Shelter Orientation, Privacy, and Future Replacement

The existing shelter is currently oriented facing the development and is directly in line with the entry gate servicing Units 2 and 2A. Should any privacy concerns be raised by residents in the future, it is noted that the shelter's orientation and its interface with the development have been identified, assessed, and accepted through the development approval process by both the developer's designer and Council. As such, PTA would not be responsible for addressing privacy impacts associated with the approved design, which would be reasonably apparent to residents at the time of purchase.

Additionally, the shelter at this location is of considerable age and is likely to require replacement in the near future. At such time if the council elects to replace the shelter, it would need to remain in its current orientation. Reversing the shelter would place it directly against the development wall and the access gate to Units 2 and 2A. If a reversal were requested by Council, the shelter would need to be relocated further south—most likely directly in front of Unit 2A. This relocation would necessitate a complete reconstruction of the bus hardstand. PTA would not bear responsibility for funding such works.

Please contact me if you require any clarification or further detail.

City of Stirling
23 April 2026
RECEIVED



Chris Salkeld
Senior Project Officer Bus Stop Infrastructure | Transperth Regional And School Bus Services
Division
Public Transport Authority of Western Australia
Tel: (08) 9326 3196 Mob: 0477 525 459
Email: christopher.salkeld@pta.wa.gov.au | Web: www.pta.wa.gov.au