



Metro Inner Development Assessment Panel Agenda

Meeting Date and Time: Wednesday, 7 May 2025; 9:30am
Meeting Number: MIDAP/71
Meeting Venue: 140 William Street, Perth

A live stream will be available at the time of the meeting, via the following link:
[MIDAP/71 - 7 May 2025 - City of Nedlands](#)

PART A – INTRODUCTION

1. Opening of Meeting, Welcome and Acknowledgement
2. Apologies
3. Members on Leave of Absence
4. Noting of Minutes

PART B – CITY OF NEDLANDS

1. Declarations of Due Consideration
2. Disclosure of Interests
3. Form 1 DAP Applications
 - 3.1 Lot 414 (No.37) Clark Street, Nedlands - Five (5) Group Dwellings – DAP/24/02785
 - 3.2 Lot 381 (No.6) Alexander Road, Dalkeith - 5 Multiple Dwellings – DAP/2820
4. Form 2 DAP Applications
5. Section 31 SAT Reconsiderations

PART C – OTHER BUSINESS

1. State Administrative Tribunal Applications and Supreme Court Appeals
2. Meeting Closure

Please note, presentations for each item will be invited prior to the items noted on the agenda and the presentation details will be contained within the related information documentation



ATTENDANCE

Specialist DAP Members

Karen Hyde (Presiding Member)

Francesca Lefante (Deputy Presiding Member)

Peter Lee

DAP Secretariat

Kristen Parker

Claire Ortlepp

Part B – City of Nedlands

Cr Fergus Bennett (Local Government DAP Member, City of Nedlands)

Cr Blane Brackenridge (Local Government DAP Member, City of Nedlands)



PART A – INTRODUCTION

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PART B – CITY OF NEDLANDS

1. Declarations of Due Consideration

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3. Form 1 DAP Applications

3.1 Lot 414 (No.37) Clark Street, Nedlands - Five (5) Group Dwellings –
DAP/24/02785

3.2 Lot 381 (No.6) Alexander Road, Dalkeith - 5 Multiple Dwellings –
DAP/2820

4. Form 2 DAP Applications

Nil.

5. Section 31 SAT Reconsiderations

Nil.



PART B – CITY OF NEDLANDS

Item 3.1 – Lot 414 (No. 37) Clark Street, Nedlands

The Responsible Authority Report (RAR) originally submitted by the City of Nedlands and published within this agenda did not contain a Responsible Authority Recommendation but included an officer recommendation for approval, subject to conditions. The City of Nedlands Council subsequently considered the matter at their Ordinary Council Meeting on Tuesday 29 April 2025 where they resolved to recommend refusal.

Accordingly, this agenda was updated on 2 May 2025 to include the Council authorised recommendation of refusal. The alternate officer recommendation for approval can now be found at the end of the RAR. The planning assessment within the body of the RAR remains consistent with the originally published RAR.

Item 3.2 – Lot 381 (No. 6) Alexander Road, Dalkeith

The Responsible Authority Report (RAR) originally submitted by the City of Nedlands and published within this agenda did not contain a Responsible Authority Recommendation but included an officer recommendation for refusal. The City of Nedlands Council subsequently considered the matter at their Ordinary Council Meeting on Tuesday 29 April 2025 where they resolved to endorse the officer recommendation.

Accordingly, this agenda was updated on 2 May 2025 to include the Council authorised recommendation of refusal. The officer recommendation for refusal has been removed from the RAR as it has become the Responsible Authority Recommendation. The planning assessment within the body of the RAR remains consistent with the originally published RAR.

**Part B – Item No. 3.1 - Lot 414 (NO. 37) CLARK STREET,
NEDLANDS – FIVE GROUPED DWELLING DEVELOPMENT**

Form 1 – Responsible Authority Report
(Regulation 12)

DAP Name:	Metro Inner Development Assessment Panel
Local Government Area:	City of Nedlands
Applicant:	Lateral Planning
Owner:	Pacific View Realty Group Pty Ltd
Value of Development:	\$2.6 million
Responsible Authority:	City of Nedlands
Authorising Officer:	Bruce Thompson, Director Planning & Development
LG Reference:	DA24-99122
DAP File No:	DAP/24/02785
Application Received Date:	23/10/2024
Report Due Date:	24 April 2025
Application Statutory Process Timeframe:	90 Days (with an additional 97 days agreed)
Attachment(s):	<ol style="list-style-type: none"> 1. Location Plan 2. Amended Development Plans received 31 March 2025 3. Schedule of Submissions 4. Design Review Panel Minutes 5. Applicants Planning Report received 23 October 2024 6. Landscaping Plan received 30 January 2025 7. Sustainable Design Report received 30 January 2025 8. Arboricultural Report received 10 April 2025

Responsible Authority Recommendation

It is recommended that the Metro Inner Development Assessment Panel resolves to:

1. **Refuse** DAP Application reference DAP/24/02785 and accompanying plans date stamped 31 March 2025 (Attachment 2) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015* and the provisions of the City of Nedlands Local Planning Scheme No. 3 for the following reasons:

Reasons

- a) The proposed development does not comply with Clause 67(2)(m)(x) of Schedule 2 of the *Planning and Development (Local Planning Schemes) Regulations 2015* as the development is not sympathetic to the loss of privacy to the adjoining neighbour as a result of the orientation of the development to the north and results in a loss of amenity.
- b) The proposed development does not comply with Clause 67(2)(p) of Schedule 2 of the *Planning and Development (Local Planning Schemes) Regulations 2015* and does not meet the design principles of Clause 1.2 Trees and Landscaping of State Planning Policy 7.3 – Residential Design Codes (Volume 1) as the development does not provide high quality landscaping.
- c) The built form and scale of the proposed development does not comply with Clause 67(2)(m) of Schedule 2 of the *Planning and Development (Local Planning Schemes) Regulations 2015*, does not meet the design principles of Clause 3.3 Street setbacks, Clause 5.1 Open space and Clause 3.4 Lot boundary setbacks of State Planning Policy 7.3 – Residential Design Codes (Volume 1) and is not consistent with the existing or desired streetscape character as outlined in LPP Precincts 5.14.

Reasons for Responsible Authority Recommendation

The development proposes building bulk which is outside of the expectations of the R60 density code and will unduly impact the amenity of the adjoining properties and surrounding streetscapes. Furthermore, the orientation of the dwellings facing north will result in a loss of amenity and privacy for the adjoining neighbour. For these reasons, the application is recommended for refusal.

Details: outline of development application

Region Scheme	Metropolitan Region Scheme
Region Scheme - Zone/Reserve	Urban
Local Planning Scheme	Local Planning Scheme No. 3
Local Planning Scheme - Zone	Residential (R60)
Structure Plan/Precinct Plan	N/A
Use Class and permissibility:	'P' Permitted
Lot Size:	940.9m ² total area Lot 1: 209m ² Lot 2: 185m ² Lot 3: 188m ² Lot 4: 180m ² Lot 5: 181m ²
Existing Land Use:	Residential – Single House
State Heritage Register	No
Local Heritage	<input checked="" type="checkbox"/> N/A

	<input type="checkbox"/> Heritage List <input type="checkbox"/> Heritage Area
Design Review	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other
Bushfire Prone Area	No
Swan River Trust Area	No
Region Scheme	Metropolitan Region Scheme

Proposal:

Existing Land Use	Residential (Single House)
Proposed No. Storeys	Three
Proposed No. Dwellings	Five
Proposed Land Use	Residential (Grouped Dwellings)

Assessment of the proposal is based on:

- Amended Development Plans received 31 March 2025 (**Attachment 2**)
- Landscaping Plan received 30 January 2025 (**Attachment 6**)
- Sustainable Design Report received 30 January 2025 (**Attachment 7**)
- Arboricultural Impact Assessment received 10 April 2025 (**Attachment 8**)

Background:Site Context

The development is located at 37 Clark Street, Nedlands. The parent lot has a total lot area of 940.9m² and is located on the street block bound by Bruce Street to the west and Clark Street to the south (**Attachment 1**). The lot is somewhat irregular in shape with the northern and southern boundaries being angled. The site has a 46m frontage to Bruce Street and a 23m frontage to Clark Street. The lot features a slight slope of 1.3m from the southern side of the site (Clark Street) to the northern boundary. The site is occupied by the existing single storey dwelling.

The site is zoned Residential with a density code of R60. All dwellings adjacent and adjoining the subject site are also coded R60. The site received conditional subdivision approval for 5 green title lots on 24 September 2024 (WAPC ref 589-24).

Streetscape Character

The locality is experiencing a transition to a higher density and scale of development. The Residential zoned properties within the immediate locality were up-coded upon the gazettal of Local Planning Scheme No. 3 (LPS3), transitioning from R12.5 to R60. As a result, the streetscape of both Bruce Street and Clark Street feature a mix of single residential houses reflective of the previous R12.5 coding under Town Planning Scheme No. 2 and newly constructed two-storey grouped dwelling developments. Of the R60 coded properties within the street blocks of Cooper Street (north), Edward Street (south) and Bruce Street (west), there are four recently completed three two-storey grouped dwellings (35 Cooper Street, 32 Clark Street,

24 Clark Street), one three-storey four grouped dwelling development (7 Edward Street) one two-storey five grouped dwelling development (14 Clark Street) and one three-storey five grouped dwelling development (18 Cooper Street).

In response to the emerging development context, the City has prepared Local Planning Policy 5.14 Precincts (LPP 5.14) to guide future development. The policy contains built form controls to ensure that new development contributes to the desired future character of the wider Broadway Precinct as a medium to high density area.

Land Use

The Residential Design Codes (Volume 1) (R-Codes) defines grouped dwellings as the following:

“a dwelling that is one of a group of two or more dwellings on the same lot such that no dwelling is placed wholly or partly vertically above or below another, except where special conditions of landscape or topography dictate otherwise and includes a dwelling in a strata titles scheme with common property”

Similarly, the R-Codes defines a single house(s) as the following:

“a dwelling standing wholly on its own green title or survey-strata lot, together with any easement over adjoining land for support of a wall or for access or services and excludes dwellings on titles with areas held in common property”

Although the subject site at Lot 414 (No. 37) Clark Street has received conditional subdivision approval for the creation of 5 green title lots, this subdivision has not yet been acted upon, nor have the lots or titles been created. There is no guarantee that this conditional subdivision approval will ever be acted upon, therefore the development should be assessed against the current land tenure and not on the future and uncertain land tenure.

As such, at the time of receiving the application, advertising, assessing and determining the application, the subject lot is one singular lot. Given the above, the proposed development has been lodged, advertised and assessed by the City as ‘Five Grouped Dwellings’.

Legislation and Policy:

Legislation

- *Planning and Development Act 2005*
- *Planning and Development (Local Planning Schemes) Regulations 2015 (LPS Regulations)*
- *Planning and Development (Development Assessment Panels) Regulations 2011*
- *Metropolitan Region Scheme*
- *City of Nedlands Local Planning Scheme No.3 (LPS 3)*

State Government Policies

- *State Planning Policy 7.0 – Design of the Built Environment (SPP 7.0)*

Planning Codes

- Residential Design Codes Volume 1 (R-Codes Vol. 1)

Local Policies

- Local Planning Policy 1.1 Residential Development (LPP 1.1)
- Local Planning Policy 1.3 Sustainable Design (LPP 1.3)
- Local Planning Policy 5.14 Precincts (LPP 5.14)
- Local Planning Policy 7.2 Design Review Panel (LPP 7.2)
- Local Planning Policy 7.3 Consultation of Planning Proposals (LPP 7.3)

Strategies

- City of Nedlands Local Planning Strategy

Consultation:

Public Consultation

In accordance with the City's Local Planning Policy 7.3 Consultation of Planning Proposals, the development was advertised for a period of 14 days, from 15 November 2024 to 29 November 2024 to 43 owners and occupiers.

At the close of the advertising period, the City received 5 submissions, 4 objecting to the proposal and one supporting the proposal. A full schedule of submissions can be found at **Attachment 3**. A summary of the objections is provided in **Table 1** below.

Table 1: Summary of Submissions	
Issue Raised	Officer comments
Bulk and scale	Bulk and scale of the development is considered to be consistent with the expected density as outlined in the R60 density code. The lots are considered to achieve the open space, street setbacks and lot boundary setbacks as discussed in detail below.
Street trees	The development is not proposing the removal of any street trees. There are four street trees adjacent to the subject site, two on Clark Street and two on Bruce Street. Adequate setbacks have been provided from the crossovers to the street trees and confirmed by the City's Parks department. Further, a condition of approval has been recommended ensuring that the street trees are protected during the construction and life of the trees.
Sightlines	The development has been provided with a 3m-by-3m truncation at the corner of Clark Street and Bruce Street in accordance with standard model subdivision conditions. Further, there are no structures located within the sight lines of each individual driveway. The proposal

	achieves the deemed-to-comply provisions in relation to sight lines.
Loss of vegetation	The development is proposing the retention of 2 trees located within the subject site. Given the number of existing trees on site worthy of retention and the density coding of R60 which permits higher density of development, the retention of 2 trees is considered appropriate.
Lack of parking on site	Officers support the proposed car parking provision. Refer to discussion on <i>Parking</i> within this RAR.
Visual privacy concerns (east and west)	All major openings of habitable rooms from the proposed development satisfy the deemed-to-comply provisions of the R-Codes Volume 1 Clause 3.10 in relation to external properties. This is achieved either through providing adequate setbacks to lot boundaries, providing obscure and fixed windows or high-light windows or the cone of vision does not capture major openings or active habitable spaces on adjoining properties.
Lack of Landscaping	<p>The landscaping that has been provided largely achieves the deemed-to-comply provisions. Where it does not, it is considered to achieve the design principles as discussed below.</p> <p>The soft landscaping within the front setback area achieves the deemed-to-comply provisions and with the retention of existing trees and street trees will provide for an attractive setting for the streetscape.</p> <p>A Landscaping plan has been provided (Attachment 6). This features species selection, ground coverings and finishes.</p>
Building height	The height of the dwellings satisfies the deemed-to-comply provisions of the R-Codes Volume 1 (R60, 11m concealed roof height). The dwellings propose a maximum height of 10.1m to 10.7m. The differing in height is a result of the natural slope of the land. As such, the dwellings will take a 'stepped approach' in height responding to the natural ground levels.
Traffic	The proposal is for 5 grouped dwellings. This level of development is consistent with the density coding of R60. The level of traffic generated from this development is considered appropriate to the density code.
Noise	<p>The following condition of approval has been recommended in relation to air conditioning units:</p> <p>"Prior to occupation, all functional utilities (excluding solar collectors and EV chargers) shall be not visible from the primary or secondary street, integrated into the design of the development and located/screened so that they are not visually obtrusive and minimise</p>

	noise sources to the development site and adjoining properties, to the satisfaction of the City” Furthermore, all noise levels are required to comply with the <i>Environmental Protection (Noise) Regulations 1997</i> at all times,
Overshadowing	The overshadowing proposed satisfies the deemed-to-comply provisions of the R-Codes Volume 1. All shadow cast from the development will fall onto the subject site of the road reserve of Clark Street. There is no shadow cast on 21 June at 12pm onto any adjacent external properties.
Light spill	The following condition of approval has been recommended in relation to external lighting: “External lighting shall comply with the requirements of Australian Standard 4282 – Control of Obtrusive Effects of Outdoor Lighting to the satisfaction of the City of Nedlands.”

Amended plans for the proposal were submitted to the City on 31 March 2025 which included the following changes:

- Changes to Bruce Street elevation massing on second floor and increases to setbacks.
- Increase to setback to all floors of House A to Clark Street to achieve the deemed-to-comply of 1.5m (LPP 5.14)
- Reduce garage widths to functional minimum
- Reduce crossover widths to House A, B and E.
- Internal changes to configuration
- Provide Landscape Plan
- Provide Sustainability Report
- Provide Arborist Report

The amendments are minor in nature and did not trigger the need for formal re-advertising of the proposal. However, they were distributed to submitters for their information.

All submissions on this proposal have been given due regard in this assessment in accordance with Clause 67(y) of the *Planning and Development (Local Planning Schemes Regulations) 2015*.

Referrals/consultation with Government/Service Agencies

The application was not referred to Government or service agencies.

Design Review Panel Advice

The development was reviewed by the City’s Design Review Panel on two occasions, with a final review by the Chair of the DRP, based on 31 March 2025 plans, on 11 April 2025. Full DRP Minutes are provided at **Attachment 4**. A summary of the Panel and Chair’s evaluation of the proposal at each stage of the review process is provided in **Table 2** below.

Table 2: DRP Design Quality Evaluation			
	Supported		
	Further Information Required		
	Not supported		
SPP 7.0 Principles	DRP 1 4 November 2024	DRP 2 10 February 2025	Chair Review 11 April 2025
1. Context and Character			
2. Landscape Quality			
3. Built Form and Scale			
4. Functionality and Built Quality			
5. Sustainability			
6. Amenity			
7. Legibility			
8. Safety			
9. Community			
10. Aesthetics			

The final comments from the DRP Chair stated overall support for the proposal noting the following:

- *Increased setbacks, especially to Bruce Street on level 2*
- *Added modulation and depth to the houses*
- *Provided an arborist report in support of removing the large tree on Clark Street*
- *Retention of large tree on Clark Street*
- *Bruce Street massing is now close to the intentions of the LPP*
- *Dropping bedrooms on level 2 to reduce the third-floor dominance on the Bruce Street streetscape*
- *Improving bin storage and cloths drying amenity*
- *Provision of significant western sun shading*
- *Reduction of crossover widths*
- *Improved materiality of the houses*

I remain unclear on the status of three of the four street trees. The arborist supports their retention, but the architectural and landscape drawings are silent on these three trees. However, a condition could be added to any approval requiring the protection and retention of all street trees in line with the arborist's report.

Landscaping

The City’s Parks department are satisfied with the distance provided between the street trees and proposed crossovers. All street trees are to be retained.

A condition of approval has been recommended to ensure their protection during construction. Further, a condition of approval has been recommended requiring an updated Landscape Plan be provided prior to the issue of a building permit to amend discrepancies between the plans.

Planning Assessment:

The proposal has been assessed against all relevant legislative requirements of LPS 3, and State and Local Planning Policies as outlined above. The matters below have been identified as key considerations for the determination of this application.

State Planning Policy 5.4 Road & Rail Noise

State Planning Policy 5.4 – Road and Rail Noise seeks to minimise the adverse impacts of transport noise on noise-sensitive development. The subject site is located a minimum of 150m south of Stirling Highway and is located within State Planning Policy 5.4 trigger distance of 200m. This triggers an assessment against Table 2 – Noise Exposure Forecast of the Policy. The sites are expected to have a noise exposure of 54dB and therefore do not fall within an exposure category. This indicates that the proposal does not require the implementation of any further noise mitigation measures.

Local Planning Policy 1.3 Sustainable Design – Residential

Local Planning Policy 1.3 Sustainable Design – Residential (LPP 1.3) applies additional sustainability requirements for new grouped dwellings to improve environmentally design. The Policy provides the following objectives relating to this proposal:

- Reduce the urban heat island effect by enforcing maximum solar absorptance ratings in relation to roof colourings.
- Each new dwelling to be provided with a minimum 3kw photovoltaic solar panel system.
- All water fittings to be within 1 star of the maximum Water Efficiency Labelling Standard (WELS)
- Landscaping plans to include irrigation efficiency as outlined in the Policy.
- An Environmentally Sustainable Design (ESD) Report is to be prepared demonstrating how the development achieves Design Principle 5: Sustainability from State Planning Policy 7.0.

Conditions of approval have been recommended to enforce the above-mentioned objectives. Additionally, a recommended condition of approval will be imposed to require the recommendations as set out on the Environmentally Sustainable Design (ESD) Report prepared by EMERGEN be incorporated prior to occupation.

Draft Local Planning Policy 3.4 Tree Retention

The City is currently forming Local Planning Policy 3.4 Tree Retention which provides a planning mechanism for the protection of regulated trees on Residential lots coded R25 to R80 within the City. The draft Policy underwent public consultation between 31 January 2025 to 21 February 2025. As the Policy has been advertised it is considered to be a 'seriously entertained' document. As such, the Policy should be given some weight in this assessment.

The subject site has 11 existing trees. An Arborist Report has been provided at **Attachment 8**. Of the 11 trees on site, five are considered 'regulated trees' as per the Policy definition (#6, #10, #11, #12, #13). The proposal is seeking to remove three of the 'regulated trees'; #6, #12 and #13. Of these three trees, the Arborist

Report only recommended retention of tree #6 as trees #12 and #13 were deemed not worthy of retention.

The applicant worked with the City to provide amended plans in response to public submissions and the City's concerns. Based on the original plans, the City was not supportive of the primary and secondary street setbacks of House A. This lot is constrained due to have access to two primary streets, required location of driveway and tree retention. The Arborist Report states that tree #6 is not worthy of retention due to poor structural condition. This existing poor condition combined with the proposed footprint of House A, and lot constraints would result in significant SPZ and TPZ encroachment.

The City supports the retention of #10 and #11. These trees are located in the front setback area along Bruce Street and will aid in preserving the local context and character of the area as a 'leafy green suburb'.

Given the proposal is retaining two large mature trees, the site features four existing street trees and the amended plans have shown improvements to primary and secondary street setbacks to align more with the existing and future streetscape character, the City is supportive of removal of tree #6.

Conditions of approval have been recommended ensuring that the trees to be retained on site and street trees are to be protected during construction and the life of the development.

Residential Design Codes Volume 1 (R-Codes Volume 1)

The R-Codes Volume 1 provide controls applicable to grouped dwelling residential development in the form of deemed-to-comply development standards and design principles.

The R-Codes Volume 1 (Part C) are the development standards that apply to grouped dwellings in residential areas coded R30 and above in the form of deemed-to-comply development standards and design principles. Where an element of the proposal does not meet the deemed-to-comply provisions of the R-Codes it requires consideration against the Design Principles.

The below elements of the proposal do not meet the deemed-to-comply provisions, however, are considered to adequately satisfy the Design Principles.

- Private Open Space
- Trees and Landscaping
- Size and layout of dwellings
- Solar access and natural ventilation
- Parking
- Open space
- Street setbacks
- Lot boundary setbacks
- Streetscape
- Access
- Visual privacy (internal)

The design principles relevant to the above are discussed below.

Assessment Against Design Principles:

The below elements of the proposal do not meet the deemed-to-comply provisions, however, are considered to adequately satisfy the Design Principles.

Private Open Space (Clause 1.1)

House A proposes private open space located in the primary street setback area and private open spaces of Houses B to E are not directly accessible from the primary living space of the dwelling. The design principles for private open space consider the functionality of private open space to allow for entertaining and leisure as well as access to northern sun and soft landscaping. The proposed private open space is considered to meet the design principles for the following reasons:

- Each dwelling has access to a private open space on both the ground and upper floors. The ground floor private open space is of sufficient size to be functional and provide resident amenity. Further, all dwellings feature an additional terrace on the first floor which is located from either a primary living space (House B-E) or an additional living space (House A).
- Although the private open spaces of Houses B-E are not directly accessible from the primary living area of the dwelling, they are able to be used in conjunction with the internal space and provide resident amenity in addition to the ground floor primary garden area.
- All private open spaces on the site are orientated for occupant amenity. All dwellings feature a private open space that has access to northern natural sunlight and ventilation.
- All private open spaces allow for sufficient uncovered area to permit winter sun and facilitate the addition of soft landscaping and tree planting areas.
- There is no communal open space proposed.

Trees and Landscaping (Clause 1.2)

The tree planting area in the front setback of House B is not of sufficient size to accommodate a small tree (9m²). The design principles for trees and landscaping consider the retention of existing trees, the provision of new trees and high-quality landscaping and sufficient deep soil area to sustain healthy tree and plant growth. The proposed trees and landscaping are considered to meet the design principles for the following reasons:

- The site as a whole is proposing the retention of three existing trees, the planting of 11 small trees, and retention of the four existing verge trees. The Arborist Report as outlined in **Attachment 8** recommended the retention of this tree as it is in excellent health and has good structure (category A tree quality). This tree is 20-30m in height and is visible from the street and surrounding properties. The retention of this tree is significant for local fauna and to aid the development to achieve the desired future character of the area as outlined in LPP Precincts 5.14.
- As viewed from the streetscape of Bruce Street and Clark Street, the soft landscaping and tree planting provided within the development aids in enhancing the built form and streetscape and pedestrian amenity.
- Should the DAP approve the application, an amended Landscape Plan by Kelsie Davies Architect is recommended to respond to the DRP comments and updates to plans.

- The location of soft landscaping on site has been positioned within the private open space on the ground floor and planters added to first floor balconies. This aids in enhancing the visual appeal and amenity for future residents by providing an appealing outlook.

Solar access and natural ventilation (Clause 2.2)

House A does not have an opening from the primary living space facing between north-west and east or adjoining uncovered area. The design principles for solar access and natural ventilation consider optimising layout for winter solar gain whilst balancing site constraints. The proposal is considered to meet the design principles for the following reasons:

- The site constraints for House A restrict the dwelling from obtaining access to adequate northern light. The dwelling is restricted as it is located on the corner and requires vehicle access from the secondary street, whilst considering adequate separation to verge trees and safety. As such, the primary living space cannot be orientated north or west. Natural ventilation and access to sunlight is achieved through the double storey void located along the southern elevation and large bi-fold sliding doors along the western elevation.

Parking (Clause 2.3)

The development proposes no visitor parking bays in lieu of one. The design principles for parking consider the availability of on-street parking and the proximity of the site to public transport. The proposed parking is considered to meet the design principles for the following reasons:

- The WAPC has conditionally approved a subdivision plan on 24 September 2024 for the creation of five lots as proposed, without the requirement of a visitor bay.
- The subject site is located 150m south of Stirling Highway, which carries two high frequency bus routes providing residents and visitors with alternative forms of transport to and from the site.
- Each dwelling is provided with two resident car bays, which exceeds the minimum deemed-to-comply criteria of one bay. The second resident bay is capable of accommodating visitor vehicles on the site if required. As the site is within 250m walkable catchment of a high frequency bus route, the provision of 6 parking bays (inclusive of one visitor bays) is the minimum deemed-to-comply outcome. The development provides a total of 10 car parking bays, a technical 'oversupply' of minimum parking requirements.
- Further, Houses B-E have their own independent driveway that is capable of accommodating at least one additional car for visitor has its own independent driveway
- Time restricted on-street parking is available on Bruce Street, Clark Street and surrounding streets and may be utilised by visitors. Three-hour parking is available between the hours of 8am and 5pm on the eastern side of Bruce Street and on-hour parking is available between the hours of 8am and 5pm on the southern side of Clark Street.
- In terms of transport options, the development provides two car parking bays per dwelling, has sufficient availability of on-street parking and proximity to relatively high frequency public transport. The combination of these factors results in sufficient car parking for the proposal.

Open space (Clause 5.1)

House B and House D propose 37.8%-37.9% open space in lieu of 40%. The design principles for open space consider the impact of building bulk, provision of adequate sun and ventilation and ability to use external spaces for outdoor pursuits and recreation. The proposed open space is considered to meet the design principles for the following reasons:

- The dwellings feature ample opportunities for residents to pursue outdoor pursuits either on the ground or upper floors. All dwellings feature primary garden areas which achieve the size as required by C1.1.1/C1.1.2. Furthermore, all dwellings feature an additional private open space located on the first and second floor. The combination of private open spaces on the ground and upper floors allows for the residents to undertake a range of outdoor activities.
- The amount of open space does not have an adverse effect on the streetscape. The design of the proposal considers the surrounding context, including the architectural styles, setbacks, and building heights of other development in the locality. Overall, the three-storey built form, and scale complements the existing and future streetscape character.
- All habitable rooms within the dwellings are provided with operable major openings across all floors. This ensures appropriate access to natural light and ventilation for occupants.
- Adequate space is provided on site for external fixtures and essential facilities.

Street Setback (Clause 3.3) and Precincts 5.14

The primary street setbacks as outlined in LPP 5.14 Precincts – Broadway Precinct states the following:

- Ground and 1st storey – 4m
- All other storeys – 6m

The minimum primary street setback may be reduced (as per footnote 2) by up to 2m provided the area of any building (including a garage or unenclosed balcony) encroaching into the setback area is compensated for by at least an equal area of open space that is located between the setback area and a line drawn parallel to it at twice the setback distance.

The development proposes the following primary street setbacks:

- House A - Second floor - 3.5m (balcony) in lieu of 6m – 11.8m² encroachment – no compensation provided
- House B - First floor - 2.6m in lieu of 4m – 4.1m² encroachment – not sufficient compensation provided
- House B - Second floor - 3.8m (balcony) in lieu of 6m – 10.6m² encroachment – no compensation provided
- House C - Second floor - 4.0m (balcony) in lieu of 6m – 8.7m² encroachment – not sufficient compensation provided
- House D - First floor - 3.0m in lieu of 4m – 2.6m² encroachment – no compensation provided

- House D - Second floor - 3.8m (balcony) in lieu of 6m– 11.7m² encroachment - no compensation provided
- House E - Second floor - 3.7m (balcony) in lieu of 6m – 11.4m² encroachment – no compensation provided

The design principles for street setbacks consider ensuring consistency with existing and future streetscape and character, provide sufficient space for landscaping, provide building articulation and positively contributes to the streetscape. The proposed street setbacks are considered to meet the design principles for the following reasons:

- The proposed intrusions into the primary street setbacks on the first and second floor are from balconies and terraces. Although these are considered when assessing encroachments into the setback area, this type of intrusion is unlikely to have a negative impact on the streetscape. The built form of the balconies and terraces are entirely open in nature and consist of a 0.7m wall with open balustrading atop. The remainder of the solid building (external face of wall) is setback further on both the first and second floors. This is considered to be appropriate and does not detract from the character of the streetscape.
- The lot is irregular in shape along its southern and northern boundaries. As a result, the proposed dwellings will be built on stepped angle when viewed from Bruce Street. This angled design results in a 'worst case' measurement being taken as opposed to an average or the reality of a 'stepped approach'. Due to this, the reduced street setback is unlikely to have any negative impacts on the streetscape.
- The Bruce Street elevation incorporates articulation through varied setbacks, break in solid mass using balconies, varied materiality and colour palette and minor projections. The incorporation of these elements breaks up any perception of building bulk as viewed from the streetscape.
- The reduced primary street setbacks do not restrict the dwellings from providing landscaping in the front setback areas. The primary street setbacks of the ground floor achieve the deemed-to-comply provisions, with only upper floor reductions.

Lot boundary setbacks (Clause 3.4)

The development proposes the following lot boundary setbacks:

- House A – second floor wall along the eastern elevation is setback 1.6m in lieu of 3.0m.
- House E – second floor wall along the northern elevation is setback 1.5m in lieu of 3.0m.

The design principles for lot boundary setbacks consider the locations streetscape character, transition between sites with varying land uses or intensity of development and to provide adequate solar access and natural ventilation and impact on privacy. The proposed northern and eastern lot boundary setbacks are considered to meet the design principles for the following reasons:

- The eastern wall of House A is adjacent to the front setback area of the adjoining property at 35 Clark Street. The existing dwelling at 35 Clark Street is setback approximately 8.5m from the front boundary and is unlikely to be

impacted by the reduced setback of this wall. The reduced setback is unlikely to have any negative impacts on the amenity of the adjoining residents given sufficient setbacks to habitable rooms or outdoor living areas.

- The northern wall of House E is adjacent to the existing driveway and garage at the adjoining property at 36 Cooper Street. The reduced setback is unlikely to have a negative impact on the amenity of the adjoining residents given sufficient setbacks to habitable rooms or outdoor living areas.
- All side elevations are well articulated and provide varying setbacks. The facades have been designed to include multiple openings and diversity of materiality which will aid in breaking up the overall building bulk of the development.
- Ventilation is ensured through the adequate separation of between 1.5m and 1.6m of the walls.
- The lot boundary setbacks do not impact any other property in terms of overlooking or overshadowing.
- The proposed lot boundary setbacks are overall consistent with the site's density code and is typical for a grouped dwelling proposal. Noting that the locality is expected to undergo a gradual transition to a higher density and scale, the setbacks proposed do not impede adjacent properties from redeveloping to a similar scale of development.

Streetscape (Clause 3.6)

The garage widths of Houses B to E are between 61% to 70% in lieu of a maximum 60%. The design principles for garage widths consider if the dwelling facades contribute positively to the streetscape, provides opportunities for passive surveillance entries are legible and safe, visual connectivity is maintained between the dwelling and the street. The proposed garage width is considered to meet the design principles for the following reasons:

- The Bruce Street elevation features multiple articulations, varied materiality and provisions of balconies. This provides for an attractive setting complimentary to the streetscape.
- The garage doors will feature timber cladding to match the surrounding walls. This will provide a seamless look allow the garage doors to 'blend in' with the development, as opposed to standing out.
- Visual connectivity is maintained between each dwelling and the streetscape via the first and second floor major openings from primary living areas and bedrooms facing the street. The width of the garage doors does not restrict visual connectivity.
- Entries to the dwellings are legible via the clear definition of the porch and pedestrian path leading to the front door.
- Garages of Houses B to E have been reduced in width to a functional minimum to reduce impacts on the streetscape.

Visual privacy (Clause 3.10)

The development proposes the following visual privacy variations. All overlooking is internal to the subject site and does not impact on any other lot.

- House B – First - Dining/kitchen along the eastern elevation overlooks the outdoor living area of House C and not setback 4.5m.

- House D – First - Dining/kitchen along the eastern elevation overlooks the outdoor living area of House C and not setback 4.5m

The design principles for visual privacy consider overlooking of major openings and active habitable spaces, the reduction of overlooking through various design elements and screening devices. The proposed visual privacy intrusions are considered to meet the design principles for the following reasons:

- All sources of overlooking are internal to the subject site only. All sources of overlooking are indirect only within the cone of vision. The overlooking from House B and House D are offset via the placement of external screening or building articulation. The overlooking from House D is likely to fall onto the drying and services courtyard of House C. This is unlikely to have any negative impacts on the future amenity of the residents.

Minor Variations

The key elements of the development proposal which require consideration have been outlined in this report. The application also involves technical variation driveway angle alignment (3.7 Access). This is considered a technical variation due to the angle of the parent lot with no adverse impact on the amenity of adjoining properties or surrounding area, or on the amenity of future residents.

Local Planning Policy 5.14 Precincts

Local Planning Policy 5.14 Precincts (LPP 5.14) applies to land coded R40 to R-AC1 within the Broadway Precinct. LPP 5.14 has been developed ensure new development enhances streetscapes, respectfully responds to the context and character of the area, promotes sustainable building design and effectively manages transition between high and medium densities.

LPP 5.14 replaces or augments several deemed-to-comply provisions of the R-Codes Vol. 1, and these have been addressed above where applicable. The policy also includes the objective to 'define the desired future character of the Precincts in context of their zoning and density code' and provides a desired future character statement for the Broadway sub-precinct.

“Residential sub-precinct form: New development in the residential zones will fit comfortably within the existing open, leafy streetscapes through street setbacks which achieve a balance between the prevailing street setbacks and the expectation of the density code. New developments will maintain and, where possible, enhance the existing urban street canopy by prioritising tree canopy and deep soil areas. Vehicle access and parking will be designed to minimise visual impact and dominance over the street and to maximise space for deep soil and trees within the street setback and verge areas. The existing detached streetscape character will be retained by minimising the impact of boundary walls on the street.”

The proposed development in its current form satisfies the desired future character statement and objectives of the Policy due to the following reasons:

- The development is considered to maintain an appropriate setback both Bruce Street and Clark Street. The built form outcome provides an

appropriate transition between the existing streetscape character with the emerging context as a medium density area as recommend in this Policy.

- The surrounding locality is coded R60 which is consistent with the subject site. Although there is currently a varying level of intensity of development within the area given the existing older dwellings with the newer grouped dwelling development emerging, the proposal is considered to provide the appropriate built form transitions.
- The development contributes to the dwelling stock available in the locality via the addition of five new three storey dwellings, featuring 3 bedrooms and 3 bathrooms.
- The architectural style of the development is of high-quality and provides a high level of amenity for its residents. The City's Design Review Panel considers the building façade materials and design to be attractive and add to the streetscape appeal.

Conclusion:

The application has been assessed against the planning framework relevant to the site. In instances where the proposal does not satisfy a deemed-to-comply development standard, the proposal is considered to achieve the relevant design principles and policy objectives. In light of the above, it is recommended that conditional approval be granted to the development application subject to conditions.

Officer Recommendation

It is recommended that the Metro Inner Development Assessment Panel resolves to:

1. **Approve** DAP Application reference DAP/24/02785 and accompanying plans date stamped 31 March 2025 (Attachment 2) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of the City of Nedlands Local Planning Scheme No.3, subject to the following conditions:

General Conditions

1. Pursuant to clause 26 of the Metropolitan Region Scheme, this approval is deemed to be an approval under clause 24(1) of the Metropolitan Region Scheme.
2. This decision constitutes planning approval only and is valid for a period of 4 years from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect.
3. All works indicated on the approved plans shall be wholly located within the lot boundaries of the subject site.
4. Prior to the issue of a Demolition and Building Permit, a Demolition Plans and Construction Management Plan shall be submitted and approved to the satisfaction of the City. The approved Demolition Plan and Construction

Management Plan shall be observed at all times throughout the construction and demolition processes to the satisfaction of the City.

Engineering

5. Prior to occupation, new or modified vehicle crossovers shall be constructed to the City's specification and thereafter maintained to the satisfaction of the City of Nedlands.
6. Prior to occupation, any redundant crossovers shall be removed and the verge and kerbing reinstated to the City's specifications, at the expense of the applicant and to the satisfaction of the City of Nedlands.
7. All stormwater discharge from the development shall be contained and disposed of on-site unless otherwise approved by the City of Nedlands.

Design

8. Prior to the issue of a building permit, the materials, finishes and colours (as shown and annotated on the approved plans) shall be shown on the building permit plans (unless otherwise approved by the City), enacted prior to practical completion of the development and thereafter remain in place for the life of the development to the satisfaction of the City.
9. Prior to occupation, all screening as shown on the approved plans be screened and erected and in accordance with the Residential Design Codes by:
 - i. fixed and obscured glass to a minimum height of 1.6 metres above finished floor level; or
 - ii. fixed screening devices to a minimum height of 1.6 meters above finished floor level that are at least 75% obscure and made of a durable material:
or
 - iii. an alternative method of screening approved by the City of Nedlands.

The screening shall be thereafter maintained to the satisfaction of the City of Nedlands.

10. Prior to occupation, walls on or adjacent to lot boundaries are to be finished externally to the same standard as the rest of the development in:
 - a. Face brick;
 - b. Painted render;
 - c. Painted brickwork; or
 - d. Other clean finish as specified on the approved plans.

And are to be thereafter maintained to the satisfaction of the City of Nedlands

11. Prior to occupation, all air-conditioning plant, satellite dishes, antennae and any other plant and equipment to the roof of the building shall be located or screened to the satisfaction of the City of Nedlands.
12. All external lighting is to be designed and located to prevent any increase in light spill onto the adjoining properties and comply with the requirements of Australian Standard 4282 – Control of Obtrusive Effects of Outdoor Lighting.

13. Clothes drying areas shall be located and/or screened to not be visible from the street or adjoining properties to the satisfaction of the City of Nedlands.

Landscaping

14. Prior to the issue of a building permit, a revised landscaping plan shall be submitted to and approved by the City of Nedlands. Prior to occupation, landscaping is to be installed and maintained in accordance with that plan, or any modifications approved thereto, for the lifetime of the development thereafter, to the satisfaction of the City of Nedlands.
15. Prior to the issue of a building permit, the landscaping plan is to demonstrate water efficient design by a suitably accredited professional. The landscaping plan is to be installed prior to occupation and maintained for the life of the development.
16. The development shall comply with the tree protection measures outlined in the approved Arboricultural Impact Assessment, prepared by Paperbark Technologies date stamped 10 April 2025 to the satisfaction of the City of Nedlands. The approved measures are to be monitored by the arborist and implemented for the duration of the demolition and construction processes to the satisfaction of the City of Nedlands.
17. The retained trees shall be protected and maintained through the duration of the demolition and construction processes to the satisfaction of the City of Nedlands. Should the tree(s) die or be damaged, this may be subject to retrospective development approval.
18. The street trees within the verge in front of the lots are to be protected and maintained through the duration of the demolition and construction processes to the satisfaction of the City of Nedlands. Should the trees die or be damaged, they are to be replaced with a specified species at the owner's expense and to the satisfaction of the City of Nedlands.

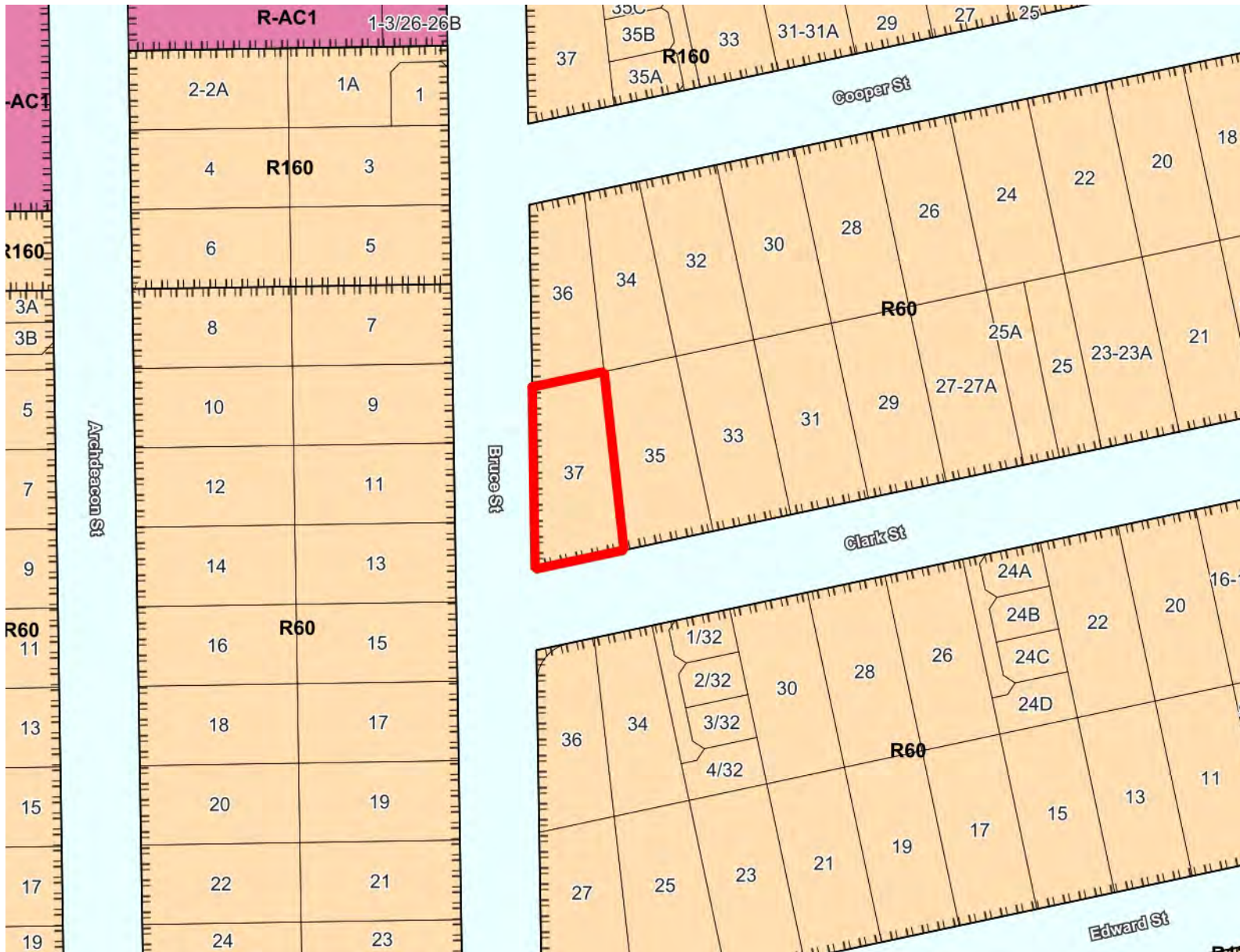
Sustainability

19. Prior to occupation, the recommendations contained within the Sustainable Design Assessment Report by EMERGEN dated received 30 January 2025, or any approved modifications, are to be carried out and maintained for the lifetime of the development to the satisfaction of the City of Nedlands.
20. Prior to the issue of a building permit, specifications to be provided demonstrating all water fittings such as taps, toilets and showers (excluding kitchen sinks and laundries) are within 1 star of the maximum Water Efficiency Labelling Standard (WELS) to the satisfaction of the City. The approved fittings are to be installed prior to occupation.
21. Prior to issue of a building permit, specifications shall be provided demonstrating that the roof colour and material has a maximum solar absorptance rating of 0.4 for metal flat roofs not visible from the street or adjacent properties, 0.5 for all other metal roofs, and 0.58 for roof tiles. The specified roof colour is to be installed to the satisfaction of the City.

22. Prior to occupation, a minimum 3kw (per dwelling) photovoltaic solar panel system is to be installed to the satisfaction of the City.

Advice notes

- a. This is a Planning Approval only and does not remove the responsibility of the applicant/owner to comply with all relevant building, health and engineering requirements of the City, any obligations under the Strata Titles Act, or the requirements of any other external agency.
- b. A building permit is required for the works.
- c. The Construction Management Plan and Demolition Management Plans are to be prepared in the manner and form provided by the City of Nedlands.
- d. Visually permeable means the vertical surface has:
 - Continuous vertical gaps of 50mm or greater width occupying not less than one third of the total surface area;
 - Continuous vertical or horizontal gaps less than 50mm in width, occupying at least one half of the total surface area in aggregate; or
 - A surface offering equal or lesser obstruction to view as viewed directly from the street.
- e. It is recommended that the applicant liaise with the adjoining property owner(s) regarding the possible retention or replacement of the existing dividing fence along the common lot boundary. Please refer to the City's FAQ Sheet regarding *Dividing Fences Act 1961* for the rights and responsibilities of landowners regarding dividing fences.
- f. A list of preferred tree species suitable to the area can be found in the Sustainable Landscaping Information document here: <https://www.nedlands.wa.gov.au/documents/660/sustainable-landscaping-information>
- g. Separate approval is required from the City of Nedlands for any works located within the verge, including landscaping and crossovers. A Vehicle Crossover Permit application is required to be submitted and approved by the City of Nedlands prior to verge works commencing.
- h. The applicant/owner is advised that noise levels are to comply with the Environmental Protection (Noise) Regulations 1997 at all times.



- Labels**
- House Numbers
 - Road Names
 - Swan River Label
 - Indian Ocean Label
 - Local Roads
 - LPS3 RCodes
 - Property Boundaries
 - Current Cadastre
 - Lodged Cadastre
 - Lodged Built Stratas
 - Lodged Survey Stratas
 - LPS3 Zones**
 - Mixed Use
 - Residential



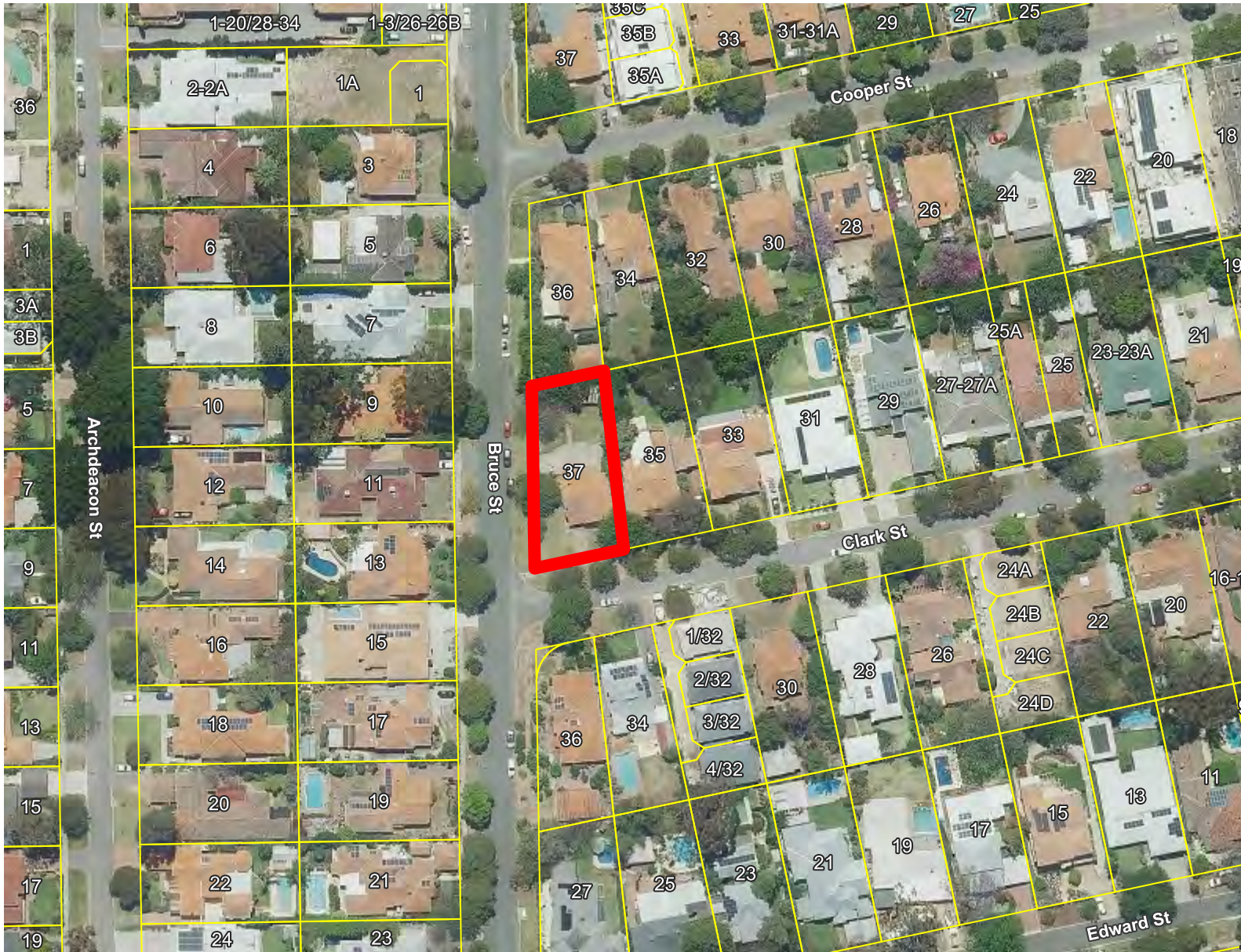
The City of Nedlands accepts no responsibility for the accuracy of this image or the results of any actions taken when using this image

Zoning Map

15/04/2025

1:1500





- Labels
- House Numbers
 - Road Names
 - Property Boundaries



The City of Nedlands accepts no responsibility for the accuracy of this image or the results of any actions taken when using this image

Aerial Image

15/04/2025

1:1500



CLIENT:	PLAN: P 2948	LOT AREA: 941m ²	SURVEY DATE: 22/02/24
BUILDER: WELINK GROUP PTY LTD	C/T Vol: 845 Fol: 41		MAP REFERENCE:
BUILDER JOB #	HEIGHT DATUM: AHD	COASTAL ZONE: 950m(river)	
AUTHORITY: CITY OF NEDLANDS	HEIGHT CORRECTION TO AHD: NO	SEWER INFORMATION: YES	

FEATURE SYMBOL LEGEND

POWER

CABLE DOME	POWER POLE	CONSUMER POLE	EXPOSED CABLES	STAYWIRE ANCHOR
CABLE BOX	POWER MARKER	LIGHT POLE		

WATER

WATER METER / TAP	FIRE HYDRANT	STOP VALVE	RETIC VALVE	WATER MARKER
TAP	FLUSHING POINT	BORE		

SEWERAGE

SEWER M/H (SQUARE LID)	SEWER M/H (ROUND LID)	INSPECTION SHAFT	INSPECTION OPENING	HOUSE CONNECTION

TELSTRA

TELSTRA PIT	TELSTRA MANHOLE	TELSTRA MARKER	TELSTRA PILLAR

DRAINAGE

DRAIN M/H (SQUARE LID)	DRAIN M/H (ROUND LID)	SIDE ENTRY PIT	DRAINAGE GRATE	COMBINED ENTRY PIT	DRAINAGE MARKER

GAS

GAS METER	GAS VALVE	GAS MARKER

SURVEY MARKS

PEG FOUND	PEG GONE	BENCH MARK	NAIL & PLATE	PEN MARK	NAIL

MISCELLANEOUS INFO.

STREET SIGN	SPOT HEIGHT	UNKNOWN SERVICE MARKER	BOLLARD	OVERHEAD POWER LINE	SEWER LINE	FENCE LINE	WINDOW / OPENING

SERVICE DETAILS

WATER: L
OVERHEAD POWER: NS
U/G POWER: L
TELSTRA: L
GAS: L
SEWER: L

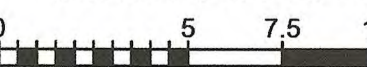
SERVICE NOTES

L: LOCATED NS: NO SERVICE
A: AVAILABLE BUT NOT LOCATED
TBC: LOCATION TO BE CONFIRMED

SEWER JUNCTION DETAILS

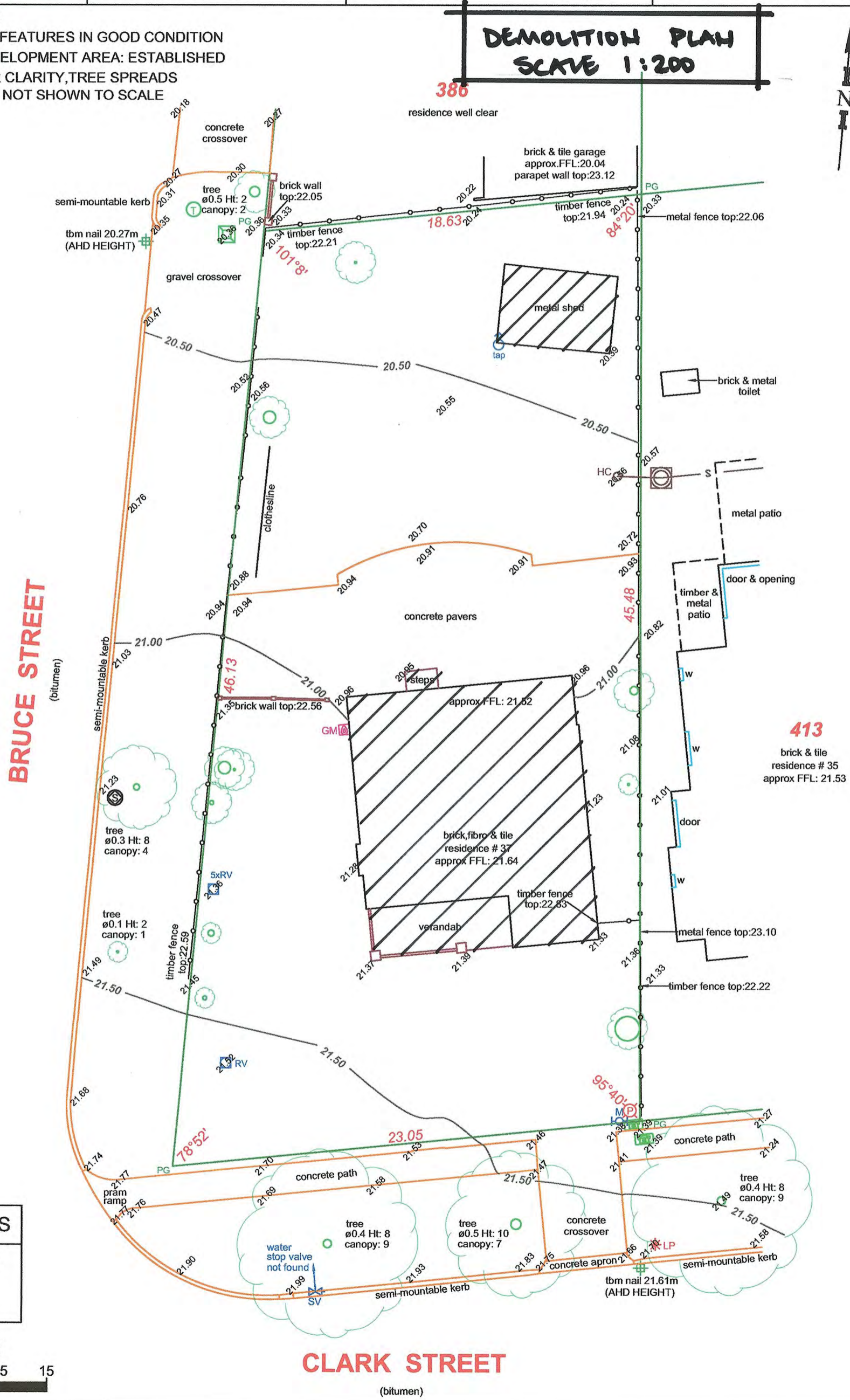
HOUSE CONNECTION (HC) IL:	19.32
UP DISTANCE:	0
DEPTH TO CONNECTION:	1.24

SCALE 1:200 AT A3 SIZE



NOTES
(1) ALL FEATURES IN GOOD CONDITION
(2) DEVELOPMENT AREA: ESTABLISHED
(3) FOR CLARITY, TREE SPREADS ARE NOT SHOWN TO SCALE

DEMOLITION PLAN SCALE 1:200



CLARK STREET

(bitumen)

IMPORTANT FEATURE SURVEY NOTES

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 2. The sewer junction on this plan has been plotted using information provided by the Water Corporation. A site inspection is required by the builder / developer in order to verify the position and depth of the sewer connection.
 3. The lot dimensions shown on this feature survey plan have been taken from L.T.O survey plans. The final repegged dimensions may vary due to adjustments made during field survey.
 4. All service information shown of this plan should be verified with the relevant authorities. 5. Pro West Surveying does not accept liability for any loss or damage caused by the use of this feature survey plan for any purpose.

Licensed and Engineering Surveying Consultants

T (08) 9242 8247 E: admin@prowestsurveying.com.au
F (08) 9242 8296 Web: www.prowestsurveying.com.au
Po Box 1463 Osborne Park DC 6916 DWG# 6980001

CLIENT:	PLAN: P 2948	LOT AREA: 941m ²	SURVEY DATE: 22/02/24
BUILDER: WELINK GROUP PTY LTD	C/T Vol: 845 Fol: 41	MAP REFERENCE:	
BUILDER JOB #	HEIGHT DATUM: AHD	COASTAL ZONE: 950m(river)	
AUTHORITY: CITY OF NEDLANDS	HEIGHT CORRECTION TO AHD: NO	SEWER INFORMATION: YES	

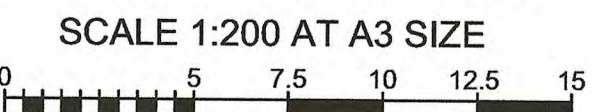
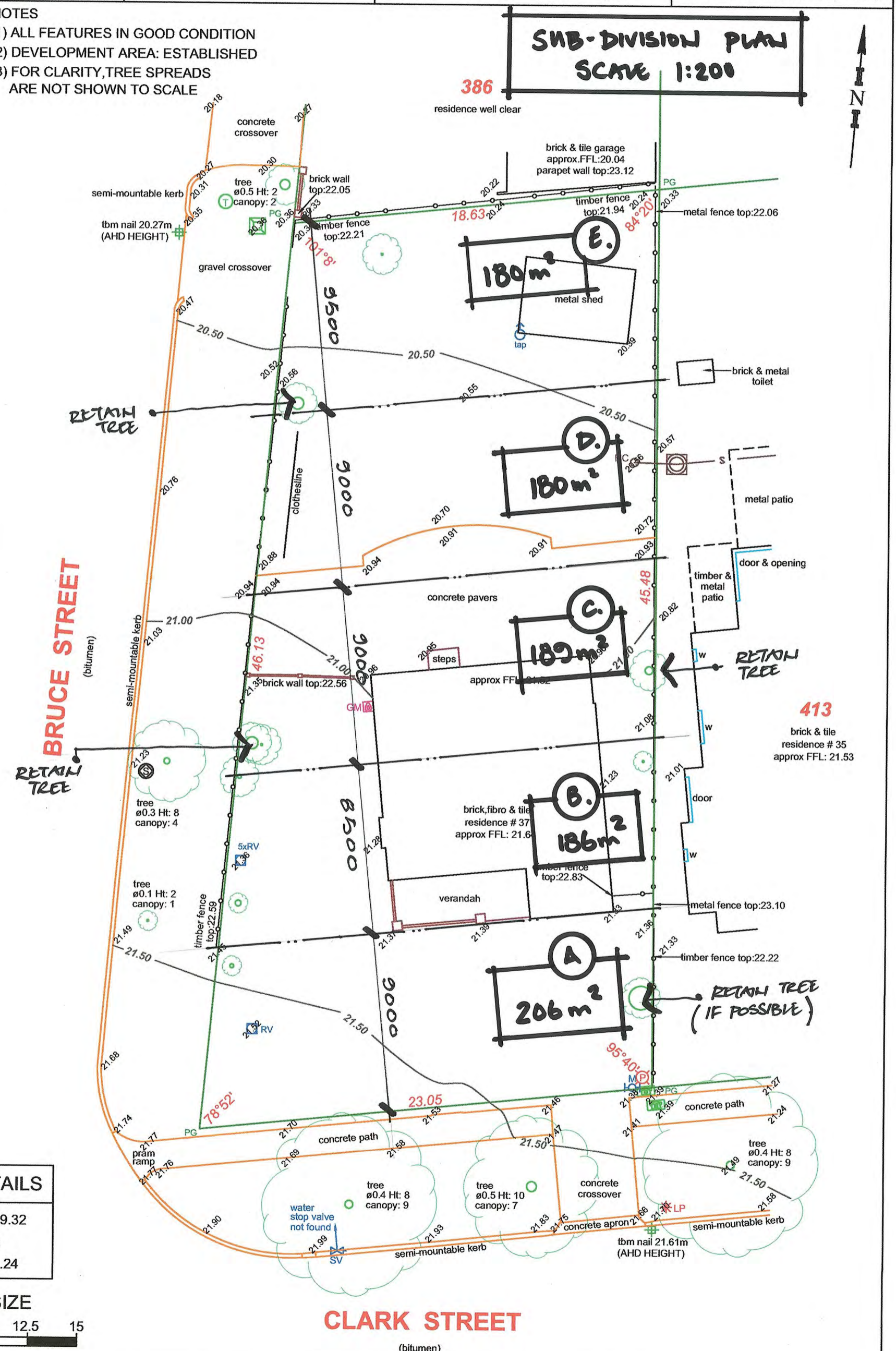
FEATURE SYMBOL LEGEND

NOTES

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- (3) FOR CLARITY, TREE SPREADS ARE NOT SHOWN TO SCALE

POWER	
CABLE DOME	POWER POLE
CABLE BOX	CONSUMER POLE
POWER MARKER	EXPOSED CABLES
LIGHT POLE	STAYWIRE ANCHOR
WATER	
WATER METER / TAP	FIRE HYDRANT
TAP	STOP VALVE
FLUSHING POINT	RETIC VALVE
BORE	WATER MARKER
SEWERAGE	
SEWER M/H (SQUARE LID)	SEWER M/H (ROUND LID)
INSPECTION SHAFT	INSPECTION OPENING
HOUSE CONNECTION	
TELSTRA	
TELSTRA PIT	TELSTRA MANHOLE
TELSTRA MARKER	TELSTRA PILLAR
DRAINAGE	
DRAIN M/H (SQUARE LID)	DRAIN M/H (ROUND LID)
SIDE ENTRY PIT	DRAINAGE GRATE
COMBINED ENTRY PIT	DRAINAGE MARKER
GAS	
GAS METER	GAS VALVE
GAS MARKER	
SURVEY MARKS	
PEG FOUND	PEG GONE
DRILL HOLE	BENCH MARK
PEN MARK	NAIL & PLATE
	NAIL
MISCELLANEOUS INFO.	
STREET SIGN	SPOT HEIGHT
UNKNOWN SERVICE MARKER	BOLLARD
OVERHEAD POWER LINE	SEWER LINE
FENCE LINE	WINDOW / OPENING
SERVICE DETAILS	
WATER: L	OVERHEAD POWER: NS
U/G POWER: L	TELSTRA: L
GAS: L	SEWER: L
SERVICE NOTES	
L: LOCATED	NS: NO SERVICE
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SEWER JUNCTION DETAILS	
HOUSE CONNECTION (HC) IL:	19.32
UP DISTANCE:	0
DEPTH TO CONNECTION:	1.24

SUB-DIVISION PLAN
SCALE 1:200



IMPORTANT FEATURE SURVEY NOTES

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BUILDER: WELINK GROUP PTY LTD	C/T Vol: 845 Fol: 41	MAP REFERENCE:	
BUILDER JOB #	HEIGHT DATUM: AHD	COASTAL ZONE: 950m(river)	
AUTHORITY: CITY OF NEDLANDS	HEIGHT CORRECTION TO AHD: NO	SEWER INFORMATION: YES	

FEATURE SYMBOL LEGEND

POWER	
CABLE DOME	POWER POLE
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WATER METER / TAP	FIRE HYDRANT
TAP	STOP VALVE
FLUSHING POINT	RETIC VALVE
BORE	WATER MARKER
SEWERAGE	
SEWER M/H (SQUARE LID)	SEWER M/H (ROUND LID)
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HOUSE CONNECTION	
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TELSTRA PIT	TELSTRA MANHOLE
TELSTRA MARKER	TELSTRA PILLAR
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SIDE ENTRY PIT	DRAINAGE GRATE
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GAS	
GAS METER	GAS VALVE
GAS MARKER	
SURVEY MARKS	
PEG FOUND	PEG GONE
DRILL HOLE	BENCH MARK
PEN MARK	NAIL & PLATE
	NAIL
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SEWER JUNCTION DETAILS	
HOUSE CONNECTION (HC) IL:	19.32
UP DISTANCE:	0
DEPTH TO CONNECTION:	1.24

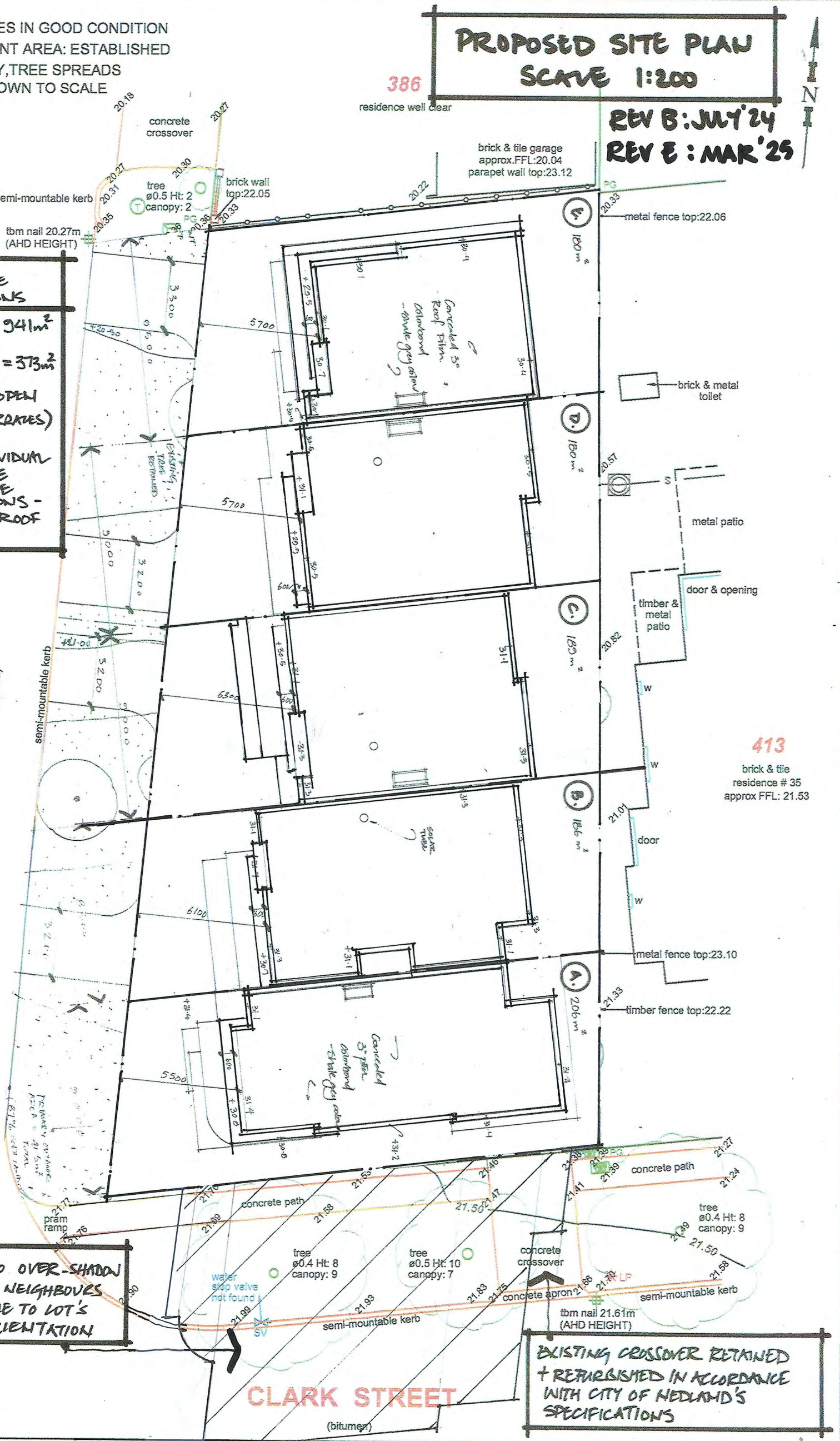
NOTES
(1) ALL FEATURES IN GOOD CONDITION
(2) DEVELOPMENT AREA: ESTABLISHED
(3) FOR CLARITY, TREE SPREADS ARE NOT SHOWN TO SCALE

OPEN SPACE CALCULATIONS
OVERALL = 941m²
TOTAL OPEN SPACE PROP. = 373m²
≈ 39.6% OPEN (NOT INC. TERRACES)
* FOR INDIVIDUAL LOTS A - E OPEN SPACE CALCULATIONS - REFER TO ROOF PLAN

BRUCE STREET
(bitumen)

PROPOSED SITE PLAN
SCALE 1:200

REV B: JULY 24
REV E: MAR '25

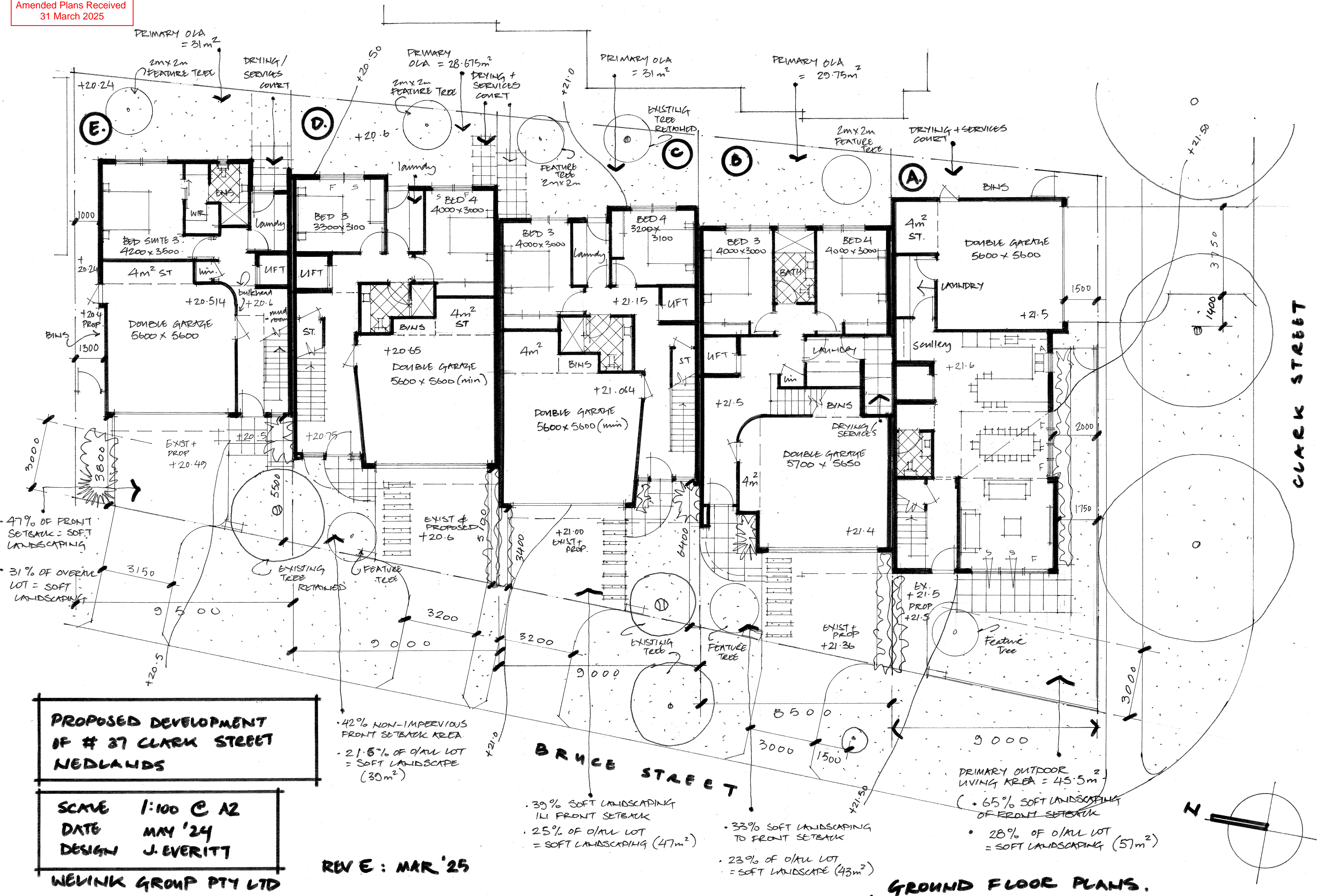


NO OVER-SHADOW TO NEIGHBOURS DUE TO LOT'S ORIENTATION

EXISTING CROSSOVER RETAINED + REFURBISHED IN ACCORDANCE WITH CITY OF NEDLAND'S SPECIFICATIONS

IMPORTANT FEATURE SURVEY NOTES

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- 47% OF FRONT SETBACK = SOFT LANDSCAPING
- 31% OF OVERALL LOT = SOFT LANDSCAPING

- 42% NON-IMPERVIOUS FRONT SETBACK AREA
- 21.8% OF O/ALL LOT = SOFT LANDSCAPE (39m²)

- 39% SOFT LANDSCAPING IN FRONT SETBACK
- 25% OF O/ALL LOT = SOFT LANDSCAPING (47m²)

- 33% SOFT LANDSCAPING TO FRONT SETBACK
- 23% OF O/ALL LOT = SOFT LANDSCAPE (43m²)

- 65% SOFT LANDSCAPING OF FRONT SETBACK
- 28% OF O/ALL LOT = SOFT LANDSCAPING (57m²)

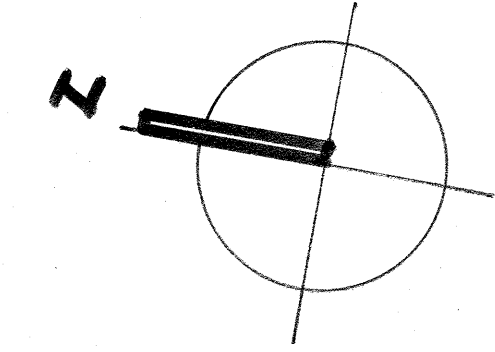
PROPOSED DEVELOPMENT OF # 37 CLARK STREET NEDLANDS

SCALE 1:100 @ A2
 DATE MAY '24
 DESIGN J. EVERITT

REV E: MAR '25

WEVINK GROUP PTY LTD

GROUND FLOOR PLANS.



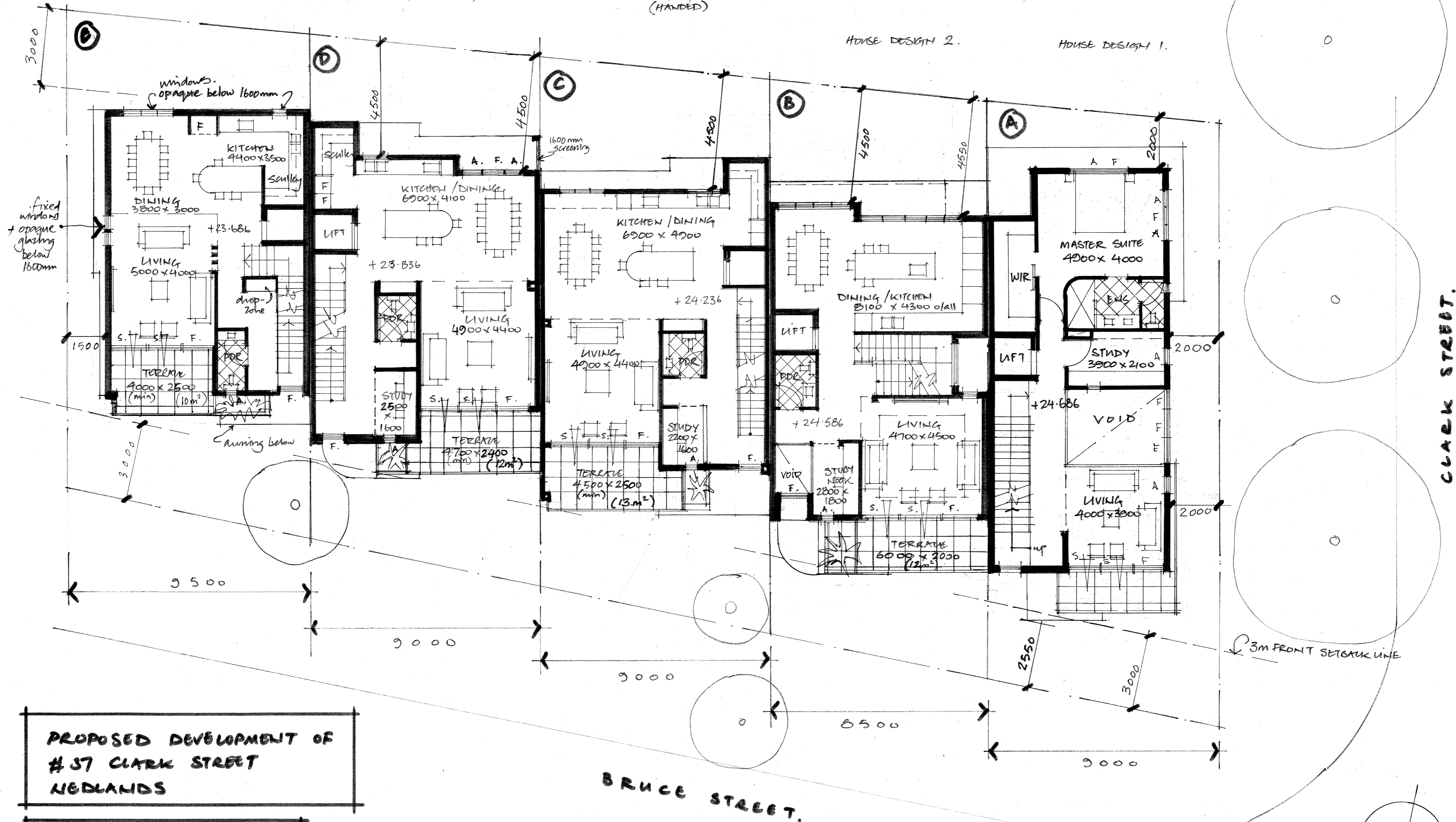
HOUSE DESIGN 4.

HOUSE DESIGN 3

HOUSE DESIGN 3.
(HANDED)

HOUSE DESIGN 2.

HOUSE DESIGN 1.



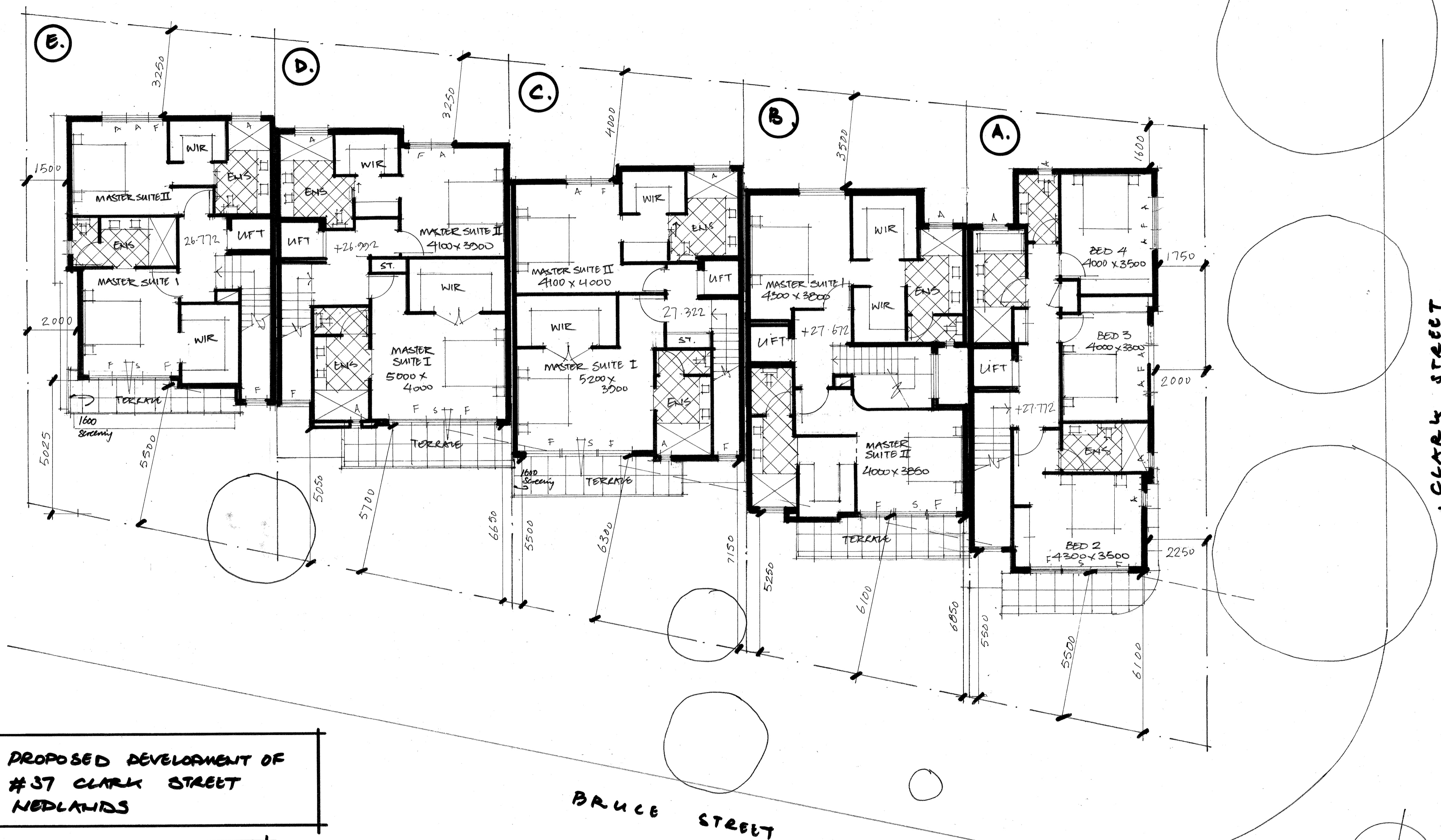
PROPOSED DEVELOPMENT OF
#57 CLARK STREET
NEDLANDS

SCALE 1:100 @ A2
DATE MAY '24
DESIGN J. EVERITT

NBLINK GROUP PTY LTD

REV A : JULY '24
REV B : JULY '24
REV C : SEPT '24
REV D : DEC '24
REV E : MAR '25

FIRST FLOOR PLANS



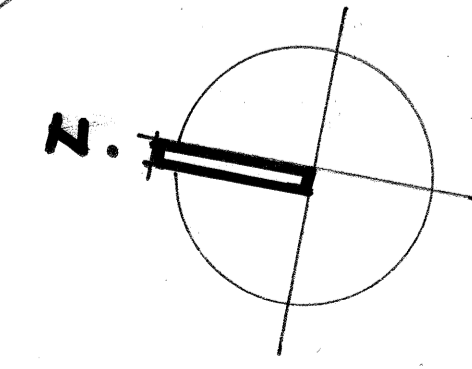
PROPOSED DEVELOPMENT OF
 #37 CLARK STREET
 NEDLANDS

SCALE 1:100 @ A2
 DATE MAY '24
 DESIGN J. EVERITT

WELINK GROUP PTY LTD

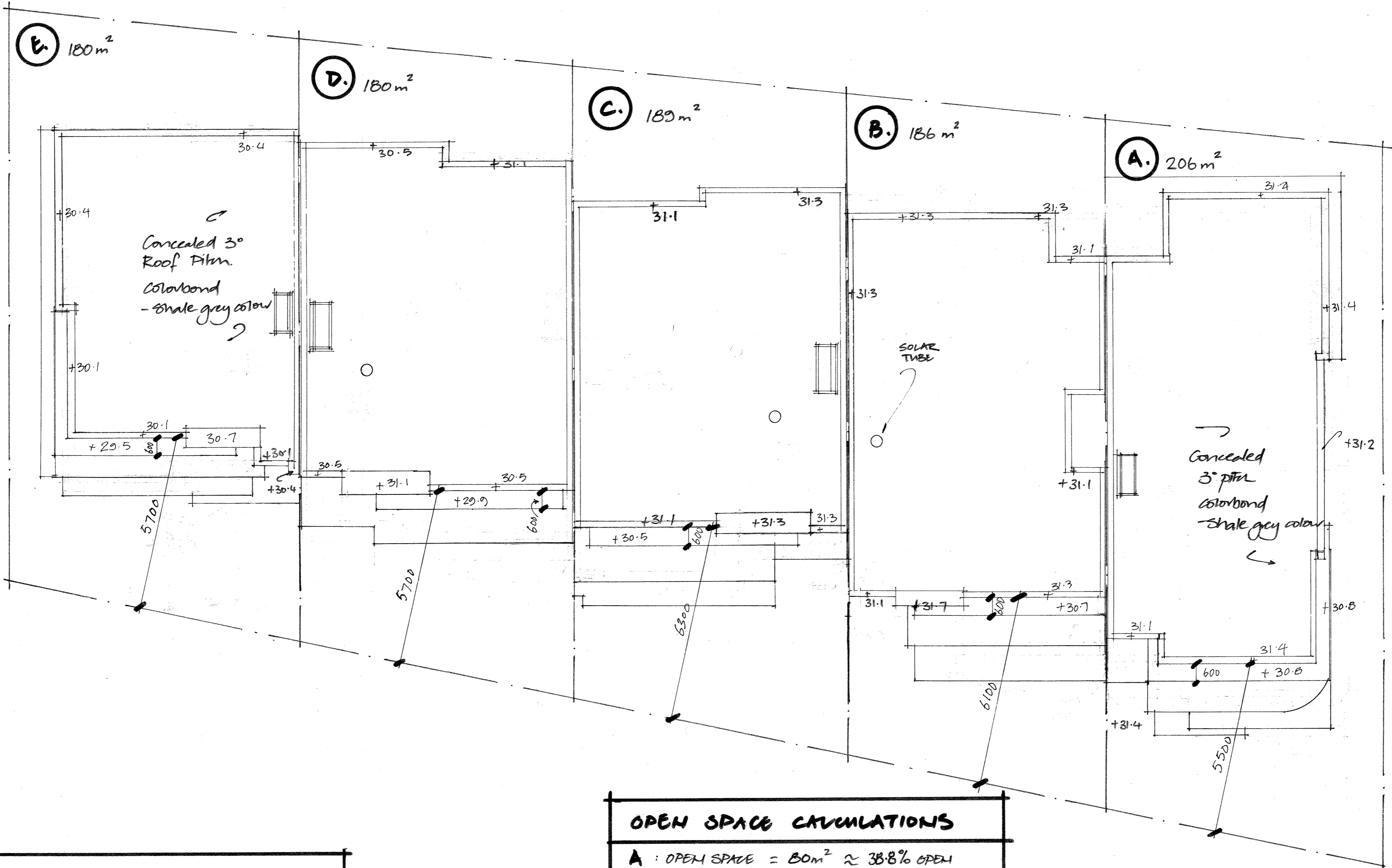
REV E: MAR '25

SECOND FLOOR PLANS



BRUCE STREET

CLARK STREET



AREA SCHEDULE.

A. GROUND FL.

- Garage/store 37m²
- Living 89m²
- Attic 6m²

FIRST FL.

- Living 116m²
- Terrace 7m²

SECOND FL.

- Living 122m²

TOTAL LIVING 327m²
GRAND TOTAL 377m²

D. GROUND FL.

- Garage/store 40m²
- Living 73m²

FIRST FL.

- Living 91m²
- Terrace 12m²

2ND FLOOR.

- Living 119m²

TOTAL LIVING 283m²
GRAND TOTAL 335m²

C + D.

GROUND FL.

- Garage/store 41m²
- Living 71m²

FIRST FL.

- Living 94m²
- Terrace 12m²

SECOND FL.

- Living 113m²

TOTAL LIVING 278m²
GRAND TOTAL 331m²

E. GROUND FL.

- Garage/store 42m²
- Living 57m²

FIRST FL.

- Living 80m²
- Terrace 12m²

SECOND FL.

- Living 81m²

TOTAL LIVING 218m²
GRAND TOTAL 268m²

OPEN SPACE CALCULATIONS

- A : OPEN SPACE = 80m² ≈ 38.8% OPEN
 - B : OPEN SPACE = 73m² ≈ 39.2% OPEN
 - C : OPEN SPACE = 77m² ≈ 40.7% OPEN
 - D : OPEN SPACE = 68m² ≈ 37.7% OPEN
 - E : OPEN SPACE = 81m² ≈ 45% OPEN
- (* AREAS DO NOT INCLUDE UPPER FLOOR TERRACES)

OVERALL DEVELOPMENT = 941m²
OVERALL OPEN SPACE = 379m²
≈ 40.2% OPEN OVERALL (REG REQUIRES 40%)

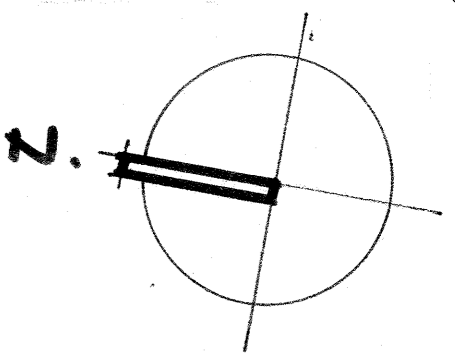
- REV E : MAR '25

**PROPOSED DEVELOPMENT OF
#37 CLARK STREET
NEDLANDS**

SCALE 1:100 @ A2
DATE MAY '24
DESIGN J. EVERITT

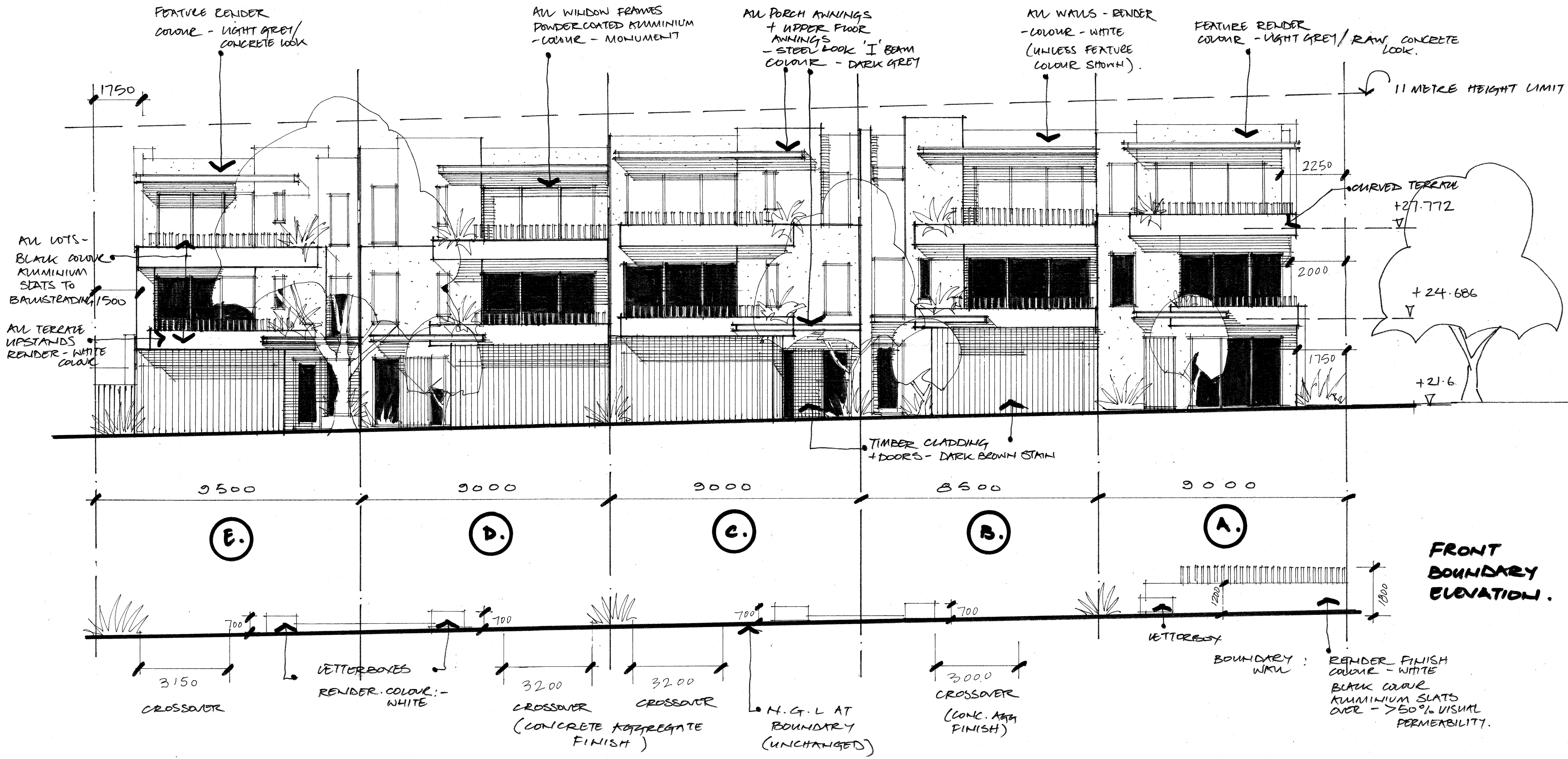
WBLINK GROUP PTY LTD

- REV A : JULY '24
- REV B : JULY '24
- REV C : DEC '24
- REV D : DEC '24



. ROOF PLAN.

**BRUCE STREET ELEVATION.
(WEST FACING)**



REV. E : MAR '25

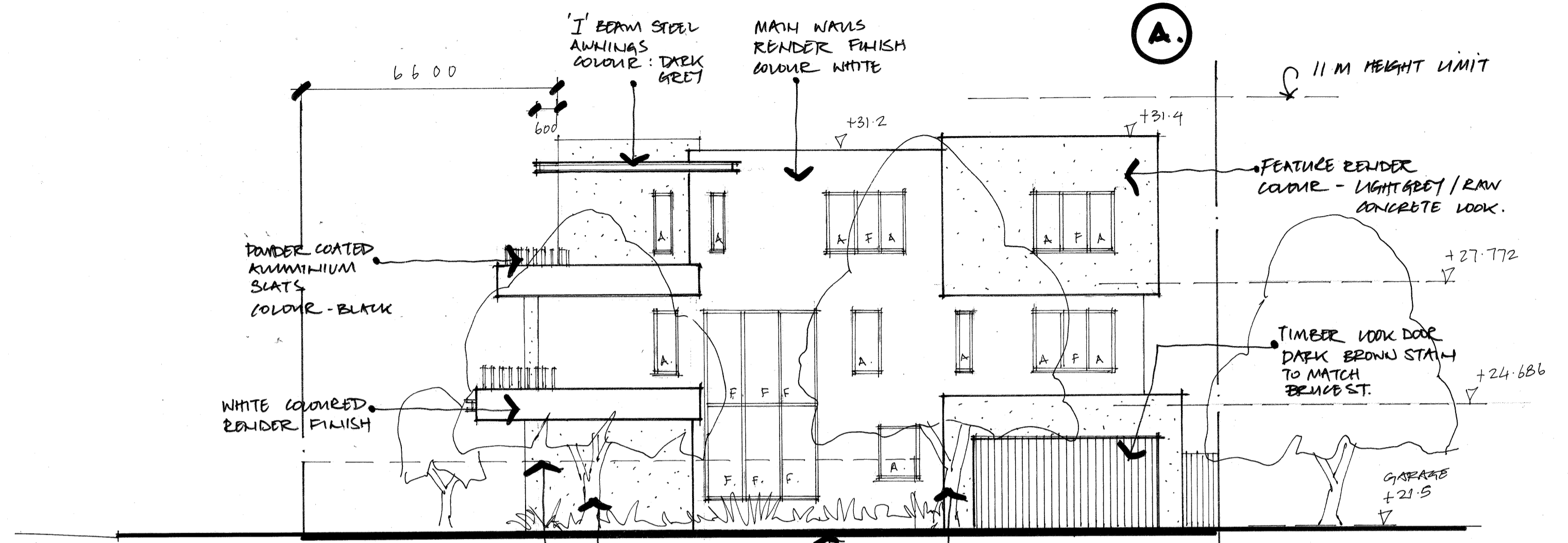
PROPOSED DEVELOPMENT OF
37, CLARK STREET
NEDLANDS

WELINK GROUP

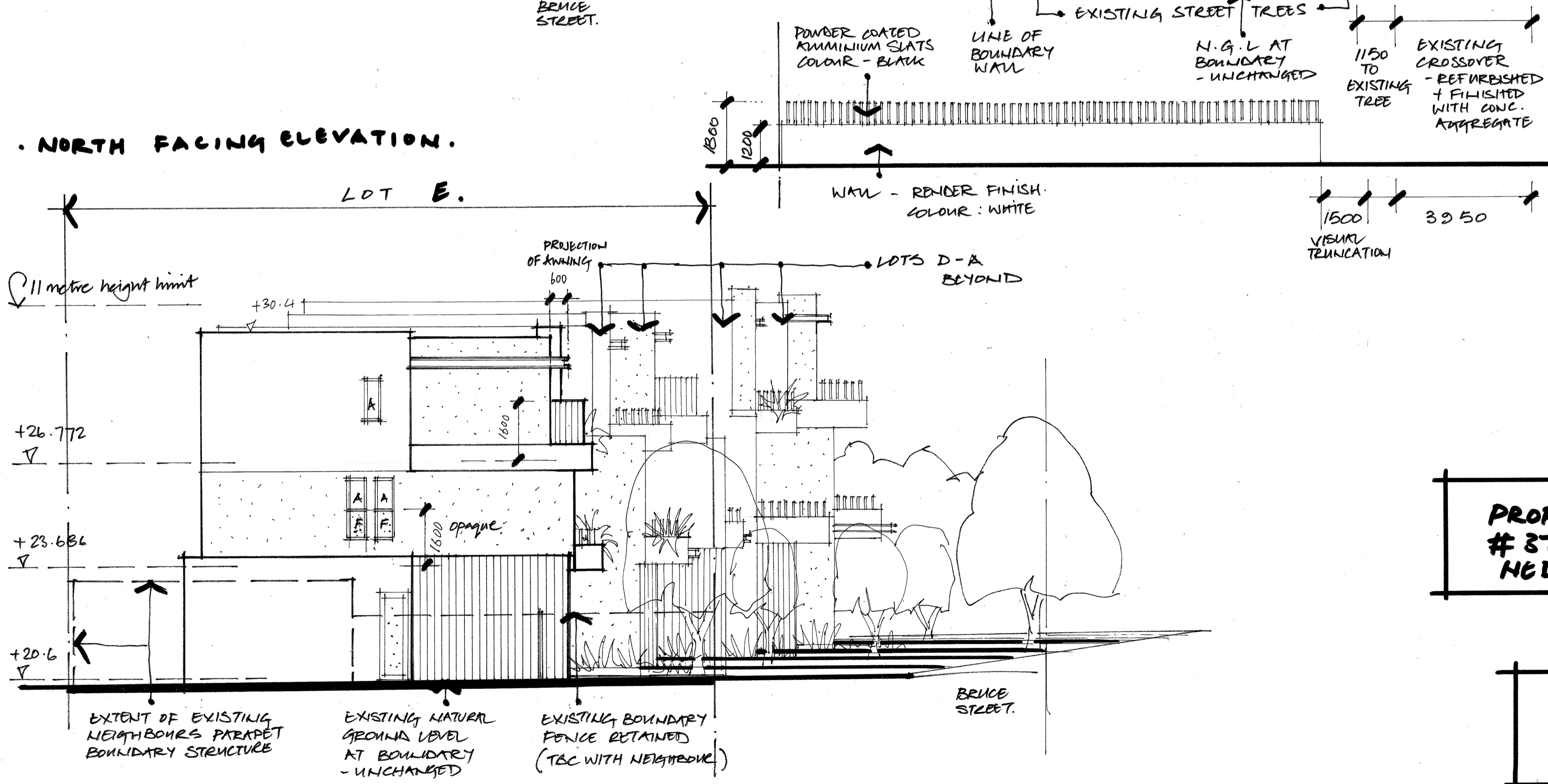
SCALE 1:100 @ A2
DATE MAR '24
DESIGN J. EVERITT

CLARK STREET (SOUTH FACING) ELEVATION.

WINDOW TYPE SCHEDULE	
A.	- AWNING WINDOW
F.	- FIXED WINDOW
S.	- SLIDING WINDOW



. NORTH FACING ELEVATION.



BOUNDARY FENCE ELEVATION

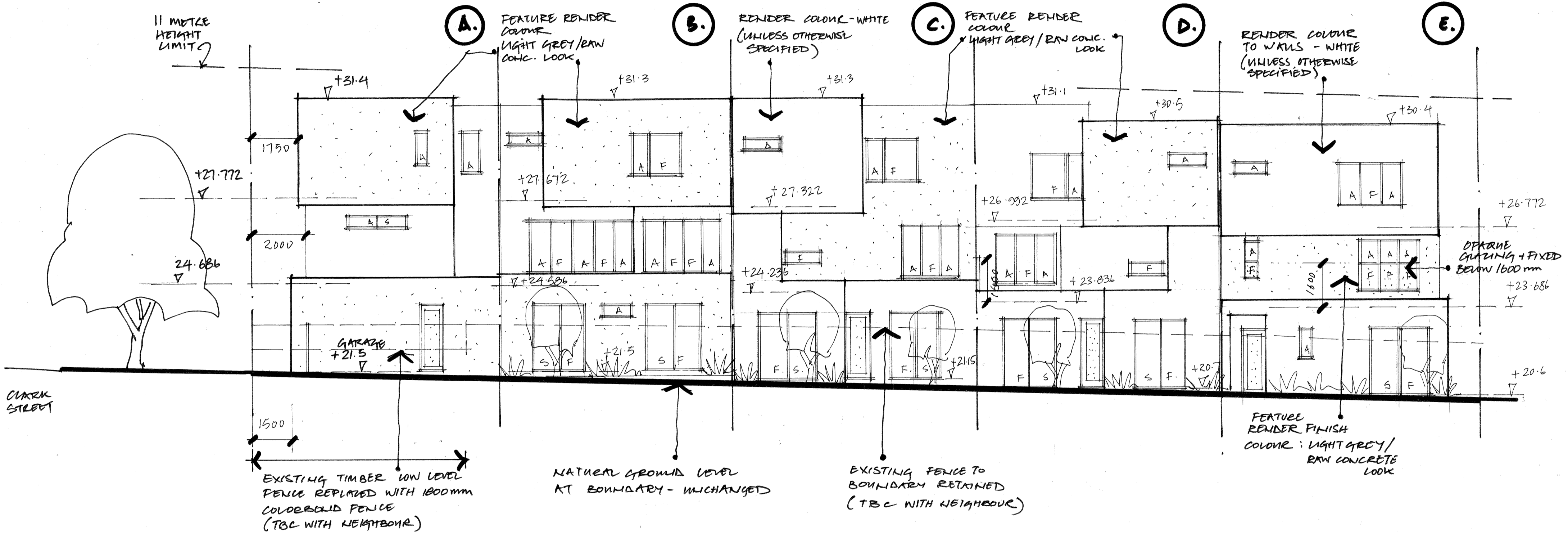
REV E: MAR '25

PROPOSED DEVELOPMENT OF
37, CLARK STREET
NEDLANDS

WELINK GROUP PTY LTD

SCALE 1:100 @ A2
DATE MAR '24
DESIGN J. EVERITT

.REAR (EAST FACING) ELEVATION.



REV E : MAR '25

PROPOSED DEVELOPMENT OF
 # 37, CLARK STREET
 NEDLANDS

NELINK GROUP
 PTY . LTD

SCALE 1:100 @ A2
 DATE MAR '24
 DESIGN J. EVERITT

Schedule of Submissions
37 Clark Street, Nedlands



City of Nedlands
nedlands.wa.gov.au

No.	Submission	Applicant Response
Objections		
1	<p>The existing 6 car bays will be removed. The bays are important to not just us and our friends and family but to the many students and workers who use them as well. Where will their visitors park? I bet no one thinks of this when approving these developments in a suburb already under strain from students and workers. This may not be one of your items listed but none the less is vitally important.</p> <p>Also the two street trees will obviously be removed, a further lessening of our 'tree lined suburb'.</p> <p>This is already a very busy road and the corner of Bruce st and Clarke st a particularly dangerous corner where visibility is not good.</p> <p>This development is out of scale with the surrounding homes and certainly not in keeping with the aesthetics of the area. Here we see the removal of another older home.</p> <p>The people who benefit from this is the developer and the city council. The neighbours get no benefit from this type of development. We just get the disadvantages and problems that arise after completion when it's too late. Long term residents are totally ignored when it comes to these large developments.</p>	<p><u>On Street Parking Bays</u> Subdivision Approval for five (5) lots has been granted at the Site. Proposed Lots B – E do not have access to a secondary street, necessitating access via Bruce Street. The proposed crossovers have been designed at functional minimum widths (3.2m – 3.3m) to minimise impacts on existing on-street parking.</p> <p><u>Street Trees</u> The proposed driveways have been redesigned to ensure all existing street trees within the verge are retained.</p> <p><u>Sightlines</u> A 3m x 3m truncation is required at the junction of Clark Street and Bruce Street, which will significantly improve visibility and sightlines at this intersection.</p> <p><u>Bulk and Scale</u> The proposed number of dwellings comply with the permitted density under the applicable zoning. Design elements, including setbacks and articulation, have been incorporated to reduce the perceived bulk and ensure compatibility with the evolving character of the area</p>



2	<p>The orientation of the dwellings positions the habitable rooms so that they face towards the neighbours which diminishes the visual privacy for all, especially that of the neighbouring property which will have 5 separate dwellings and their combined occupancy overlooking them through multiple large unobstructed windows.</p> <p>The dimensions and abundance of windows ensures overlooking to the neighbour. This is a lack of visual privacy, which must be addressed. The size of these windows facing the side boundary shows no regard for the residents of the existing house. At the very least solutions for screening on all these windows should be realistic and effective. This means that an owner or visitor to the dwelling when sitting or standing by a window should be unable to view the activities of their neighbours below but the amount of light able to enter the dwelling will not be affected and the residents will still be able to look out horizontally.</p> <p>Minimal space is allocated to natural landscaping and insufficient room for even medium sized trees to grow in the future. Not only does this conflict with one of Nedlands' most notable and appealing characteristics, its lush green canopy cover or 'visual aesthetic' but it also has many other negative attributes including but not limited to; increased temperatures, decreased air quality, soil erosion and the loss of habitat for the local birds, insects and other wildlife that are essential to a healthy ecosystem.</p> <p>Typically families have 2 cars. The common driveways to the five houses at the intersection of Clark and Bruce Street will have up to 10 cars going in and out of the grouped dwelling making this a dangerous section of the road. The</p>	<p><u>Visual Privacy (East)</u> Table 3.10a of the R-Codes Volume 1 Part C requires major openings to be setback a minimum of 4.5m. Major openings to Lots A – D exceed this requirement. Openings within 4.5m of the eastern boundary for Lot E will feature opaque glazing to a minimum height of 1.6m to mitigate overlooking.</p> <p><u>Landscaping</u> The development includes a detailed landscaping plan, which is enclosed to this correspondence. The plan demonstrates a high-quality landscaped outcome and aims to enhance the visual amenity of the development and streetscape to Bruce Street and Clark Street.</p> <p><u>On Street Parking Bays</u> Refer to previous response above.</p> <p><u>Vehicle Safety</u> Bruce Street is a low-traffic, single-lane residential road in a suburban neighbourhood. The proposed additional crossovers are consistent with the established streetscape character of Bruce Street and will not result in undue impacts on vehicle safety. The design ensures compliance with sightline and safety requirements for safe entry and egress.</p>
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	placement of the driveways will remove the already existing parking area, reducing community parking for visitors and deliveries entirely along this section of the street. Bruce street is already excessively busy with traffic.	
3	See attached Submission 1 - Redacted	<p><u>Building Height</u> In accordance with Table 3.2a of the R-Codes Volume 1 Part C, the maximum permitted building height is 3 storeys (11m). The development complies with the maximum building height permitted at the Site.</p> <p><u>Impact on traffic along Bruce Street</u> The development complies with the permitted density for the site. The minor increase in residential traffic is acceptable in this location.</p> <p><u>Lack of visitor parking</u> Visitor parking bays are not required for single dwellings; however, the R-Codes Volume 1, Part C requires one (1) visitor bay for developments with four or more grouped dwellings. A five-lot subdivision has been conditionally approved for the site, but as it has not yet been implemented, the proposal technically does not comply with this requirement. Notwithstanding, each dwelling includes its own driveway, which can accommodate visitor parking, with any overflow managed via existing on-street public parking bays. This approach aligns with the City's advice to the Design Review Panel.</p>



		<p><u>Visual Privacy (west)</u> The objection relates to perceived overlooking of dwellings on the western side of Bruce Street. The RCodes Volume 1 Part C only require visual privacy measures where overlooking affects adjoining residential properties, not where view</p>
4	See attached Submission 2 - Redacted	<p><u>Bulk and Scale & Density</u> The proposed number of dwellings comply with the permitted density under the applicable zoning. Design elements, including setbacks and articulation, have been incorporated to reduce the perceived bulk and ensure compatibility with the evolving character of the area.</p> <p><u>Impact on Traffic / Parking</u> Refer to responses above.</p> <p><u>Noise (Services)</u> All mechanical plant and equipment, including air-conditioning units will be positioned and screened to comply with the Environmental Protection (Noise) Regulations 1997.</p> <p><u>Visual Privacy (East and West)</u> Refer to responses above.</p> <p><u>Overshadowing (East)</u></p>



		The development complies with the deemed-to-comply requirements for solar access to adjoining properties under the R-Codes Volume 1 Part C.
Support		
1	<p>Comments:</p> <p>Lot boundary setbacks (east, north): no impact</p> <p>Visual privacy (north): no impact</p> <p>Primary street setback: no impact</p> <p>Secondary street setback: no impact</p> <p>Open space: no impact</p> <p>Onsite car parking: no impact</p> <p>Garage width: no impact</p> <p>Comment conclusion: I fully support the proposal.</p>	N/A

<i>Architectural Design Review Assessment City of Nedlands Design Review Panel</i>	
Design quality evaluation	Date: 04 November 2024
Application: 37 Clark Street, Nedlands	
Review No.: 1	Time: 3:10pm
Panel: <ul style="list-style-type: none"> • Simon Anderson • Simon Venturi • Tony Blackwell • Dominic Snellgrove City of Nedlands Representatives: <ul style="list-style-type: none"> • Tony Free – Director Planning and Development • Kirsty Klimcke – A/Senior Urban Planner 	
Apply the applicable rating to each Design Principle	<i>Supported</i>
	<i>Further information required</i>
	<i>Not supported</i>
	<i>Yet to be addressed</i>
Summary	<p><u>Site Context</u> The subject site is zoned 'Urban' by the Metropolitan Region Scheme and 'Residential' by the City of Nedlands Local Planning Scheme No.3 and has a density coding of R60. The site is 941m² in area and has a frontage to Clark Street (south) and Bruce Street (west).</p> <p><u>Application History</u> The application proposes a residential development comprising 5 grouped dwellings. The application has been lodged as a DAP. This is the first time this proposal has been presented to the DRP. The site has conditional subdivision approval for five green title lots.</p>
Strengths of the proposal	<ul style="list-style-type: none"> • Ageing in place is a positive of the development, by way of lift provision and a ground floor master suite. • Contemporary and distinctive architecture demonstrates depth, layering, screening and curved elements. • Level 1 streetscape interaction with balconies and extensive glazing promotes passive surveillance. • Functional and well-arranged internal planning. • Minimum driveway width supported. • Majority of units have open streetscape interface with no fences. • Manipulation of lot boundaries to retain trees on site is supported. • No overshadowing of adjacent properties. • Light colour roof is good from an ESD perspective. • The architectural language, articulation and diversity of materiality of the Bruce Street elevation is supported. • Good use of Velux windows and solar tubes.
Principle 1 - Context and character	<i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i>

	<p><u>1a. Comments</u></p> <ul style="list-style-type: none"> Plans, elevations and renders should show the development in the context of the surrounding built environment. Extensive garage frontages along Bruce Street and vehicular crossovers is a poor streetscape outcome. <p><u>1b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Consider reducing the garage widths and driveway / crossover presence on Bruce Street. The applicant is encouraged to show the surrounding buildings on the floorplans, elevations and 3D renders to illustrate how the proposal sits within, impacts on, and contributes to its immediate context.
<p>Principle 2 - Landscape quality</p>	<p><i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i></p> <p><u>2a. Comments</u></p> <ul style="list-style-type: none"> Landscaping is shown in a very indicative way and lacks detail making assessment difficult. <p><u>2b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Appoint a Landscape professional to develop an integrated Landscape Plan for the site. Consider providing an Arborist report to determine if retaining existing trees will be viable based on building location.
<p>Principle 3 - Built form and scale</p>	<p><i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i></p> <p><u>3a. Comments</u></p> <ul style="list-style-type: none"> The reduced primary street and secondary setback on all floors and reduced open space is a sign that the site is arguably overdeveloped. <p><u>3b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Investigate ways to increase the primary street and secondary street setbacks (particularly on the second floor, 6m) and open space. Seek to comply more with planning framework (R-Codes and LPP 5.14). Consider dropping from four to three bedrooms to ease up the development site and allow greater compliance.
<p>Principle 4 - Functionality and build quality</p>	<p><i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life cycle.</i></p> <p><u>4a. Comments</u></p> <ul style="list-style-type: none"> Bins are located in the garages with no ventilation for FOGO bins. Some services are not shown on the drawings. A number of laundries are not functional nor convenient requiring users to move through stores and living areas to access outdoor drying areas. <p><u>4b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Show location of services on plans (AC condensers, clothes line etc) and ensure they do not impact on resident amenity and are not visible from the public realm.

	<ul style="list-style-type: none"> • Consider providing ventilation to garage for bin store. Note 3 bins required. • Improve the functionality of the laundries and the size of some outdoor drying areas (Lot B).
Principle 5 - Sustainability	<i>Good design optimises the sustainability of the built environment, delivering positive environmental, social, and economic outcomes.</i>
	<p><u>5a. Comments</u></p> <ul style="list-style-type: none"> • No information presented to the DRP to undertake an assessment against Principal 5 <p><u>5b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Appoint a ESD professional to develop a comprehensive ESD report. • Consider integrating ESD initiatives such as PV solar panels, EV charging, all electric development (no gas), double glazing, heat pump hot water systems etc.
Principle 6 - Amenity	<i>Good design optimises internal and external amenity for occupants, visitors, and neighbours, providing environments that are comfortable, productive and healthy.</i>
	<p><u>6a. Comments</u></p> <ul style="list-style-type: none"> • Many bedrooms are under 3m in dimension. • A number of studies are 1.6m wide which is very tight. • The corner unit's external space has limited privacy from the streetscape. <p><u>6b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Increase bedroom sizes to be a minimum of 3m in dimension. • Consider unenclosing the studies, possibly positioning on landing to be more open as well as accessible and increasing their width. • Consider increasing the solid fencing to the corner unit to be 1.2m in height in some areas to provide more privacy for the private open space located in front setback area.
Principle 7 - Legibility	<i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i>
	<p><u>7a. Comments</u></p> <ul style="list-style-type: none"> • Nil. <p><u>7b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Nil.
Principle 8 - Safety	<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>
	<p><u>8a. Comments</u></p> <ul style="list-style-type: none"> • Good passive surveillance between dwellings and streetscape. However, this can be improved by increasing the interactivity of the Bruce Street ground level interface by reducing the extent of solid garage doors. • The minimal front setback is problematic in that it typically will likely result in cars parked in front of garages overhanging the boundary / verge. <p><u>8b. Suggested amendments/improvements</u></p>

	<ul style="list-style-type: none"> Concerns with reduced front setback and lack of space on driveway for visitor parking (3m-4m).
Principle 9 - Community	<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
	<p><u>9a. Comments</u></p> <ul style="list-style-type: none"> The interactivity of the Bruce Street ground level interface is minimal. Generous landscaping in the front setback is one opportunity to contribute to the streetscape and surrounding community in a positive way. <p><u>9b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Consider improving streetscape activity on ground floor by reducing garage dominance. Provide further details on, and consider generous planting in the front setback areas.
Principle 10 Aesthetics	<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
	<p><u>10a. Comments</u></p> <ul style="list-style-type: none"> The articulation, diversity of materiality and layering of the Bruce Street elevation is positive. However, the Clark Street elevation does not display any of these characteristics and presents as an unarticulated blank 3 level façade. The rear elevations are also heavily reliant on large areas of painted render with minimal visual interest. <p><u>10b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Consider adding greater visual interest through increased diversity of materiality and articulation along the Clark Street and rear elevations.
SUMMARY	The DRP is surprised the proposal has been lodged given the preliminary sketch format of the drawings that have been provided and a lack of information relating to the landscaping and sustainability components which makes many elements of the proposal hard to assess. This lack of information will likely hinder the design review process and reduce the benefits entailed.
OVERALL RECOMMENDATION	Not Supported

Design Review progress			
	<i>Supported</i>		
	<i>Further information required</i>		
	<i>Not yet supported</i>		
	<i>Yet to be addressed</i>		
	<i>DR1 – 4/11/24</i>	<i>DR2</i>	<i>DR3</i>
Principle 1 - Context and character			
Principle 2 - Landscape quality			
Principle 3 - Built form and scale			
Principle 4 - Functionality and build quality			

Principle 5 - Sustainability			
Principle 6 - Amenity			
Principle 7 - Legibility			
Principle 8 - Safety			
Principle 9 - Community			
Principle 10 - Aesthetics			

<i>Architectural Design Review Assessment City of Nedlands Design Review Panel</i>	
Design quality evaluation	Date: 10 February 2025
Application: 37 Clark Street, Nedlands	
Review No.: 2	Time: 1pm
Panel: <ul style="list-style-type: none"> • Simon Anderson • Simon Venturi • Tony Blackwell • Tony Casella City of Nedlands Representatives: <ul style="list-style-type: none"> • Bruce Thompson– Director Planning and Development • Kirsty Klimcke – A/Senior Urban Planner 	
Apply the applicable rating to each Design Principle	Supported
	Further information required
	Not supported
	Yet to be addressed
Summary	<u>Site Context</u> The subject site is zoned 'Urban' by the Metropolitan Region Scheme and 'Residential' by the City of Nedlands Local Planning Scheme No.3 and has a density coding of R60. The site is 941m ² in area and has a frontage to Clark Street (south) and Bruce Street (west). <u>Application History</u> The application proposes a residential development comprising 5 grouped dwellings. The application has been lodged as a DAP. This is the second time this proposal has been presented to the DRP. The site has conditional subdivision approval for five green title lots.
Strengths of the proposal	<ul style="list-style-type: none"> • Ageing in place is positive by way of lift provision and a ground floor master suite. • A Landscape Architect has been engaged and provided a Landscape Plan. • An ESD consultant has been engaged and an ESD report provided. • ESD initiatives including 3kW PV solar panels for each unit, EV charging, double glazing and a light coloured roof have been committed to. • Greater diversity of materiality on the eastern (rear) elevation has been incorporated. • Level 1 streetscape interaction with balconies and extensive glazing promotes passive surveillance. • Minimum driveway widths on Bruce Street supported. • Good use of Velux windows and solar tubes.
Principle 1 - Context and character	<i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i>
	<u>1a. Comments</u> <ul style="list-style-type: none"> • Massing as viewed from both streetscapes appears bulky, specifically with minimal upper floor setbacks. <u>1b. Suggested amendments/improvements</u>

	<ul style="list-style-type: none"> • Consider increasing the upper level setbacks significantly especially the 2nd floor to Bruce Street to be more in line with the 6m required. • Consider increasing the setback to Clark Street to the minimum 2m.
<p>Principle 2 - Landscape quality</p>	<p><i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i></p> <p><u>2a. Comments</u></p> <ul style="list-style-type: none"> • Lack of detail provided on existing vegetation on site. • Intention to retain trees is supported. However, it appears that the largest and seemingly healthiest tree on site, which contributes significantly to the streetscape and immediate neighbourhood, is to be removed. • Full sun species has been included in the plant mix for shady positions. • The use of Mondo Grass in the driveway and access route to the letterboxes was questioned as a trip hazard and its suitability to withstand vehicular traffic. • The required corner truncation is not shown on the landscape plan. • Access to the corner unit is obscure and non-intuitive. • The corner lot (A) has a somewhat compromised primary garden area being in the front yard and primary setback zone. <p><u>2b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Provide an Arborist report to determine if the trees will be viable based on building location particularly relevant to the large Liquid Amber tree on the South East corner. • Consider redesign of the house on Lot A to facilitate retention of the Liquid Amber and achieve a better primary garden area for this lot. • Provide clearer plans indicating existing vegetation (size, species and health etc) and vegetation which is to retain or be removed. • Re-consider selected species and their locations. • Incorporate corner truncation in the design.
<p>Principle 3 - Built form and scale</p>	<p><i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i></p> <p><u>3a. Comments</u></p> <ul style="list-style-type: none"> • The reduced primary street as well as secondary setback on all floors and general massing does not respond to the planning framework or surrounding context and is a sign that the site is potentially over-developed. <p><u>3b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Investigate ways to increase the primary street and secondary street setbacks (particularly on the second floor, 6m required). The second floor is intended to have a larger setback to not appear bulky on the streetscape. Seek to comply more closely with planning framework (LPP 5.14). • Consider dropping from four to three bedrooms to ease up development of the site and upper level setbacks.
<p>Principle 4 - Functionality and build quality</p>	<p><i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life cycle.</i></p>

	<p><u>4a. Comments</u></p> <ul style="list-style-type: none"> • AC units have not been shown on the drawings. • Bins in garage reduce the space permitted for parking vehicles and can be impractical. The garages are currently inadequate in depth to accommodate cars and bins. • Functionality of internal layout of units should be revisited. • Clothes drying space is minimal. <p><u>4b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Show location of AC units on plans. Ensure they are not visible from public realm. • Consider providing more space for clothes drying areas (min 1.5m clear in courtyards and 1.8m clear in the light well between houses A and B). • Ensure garage depths are adequate to accommodate cars and bins or consider relocating the bins external to the garage. • Providing ventilation (natural or mechanical) to garages with FOGO bins. • Consider opening up the living spaces to increase the landing space abutting the stairs. • Consider greater resolution of house A en-suite and placing bedrooms 2 and 3 BIRs on the external wall.
<p>Principle 5 - Sustainability</p>	<p><i>Good design optimises the sustainability of the built environment, delivering positive environmental, social, and economic outcomes.</i></p>
	<p><u>5a. Comments</u></p> <ul style="list-style-type: none"> • Lack of passive ESD initiatives. • The screening shown on the west elevation has been reduced significantly leaving large extents of west facing windows with no passive shading. • Consider strengthening ESD commitments. <p><u>5b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Consider providing passive shading to large extents of western facing windows. • Consider to committing to more ESD initiatives and increasing the size of the 3kW solar panels for each unit.
<p>Principle 6 - Amenity</p>	<p><i>Good design optimises internal and external amenity for occupants, visitors, and neighbours, providing environments that are comfortable, productive and healthy.</i></p>
	<p><u>6a. Comments</u></p> <ul style="list-style-type: none"> • Most rooms are minimal in size. • Lot A does not have any north facing openings. • Terraces in Lots Of B-E are minimal in size. <p><u>6b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Consider providing a north facing opening to the primary living area of Lot A. • Consider increasing terrace sizes on Lots B-E.
<p>Principle 7 - Legibility</p>	<p><i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i></p>

	<p><u>7a. Comments</u></p> <ul style="list-style-type: none"> The Lot A pedestrian entrance is not particularly legible. <p><u>7b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Consider having Lot A addressing Clark Street or provide other measures to improve legibility of entry.
Principle 8 - Safety	<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>
	<p><u>8a. Comments</u></p> <ul style="list-style-type: none"> None. Supported. <p><u>8b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Nil.
Principle 9 - Community	<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
	<p><u>9a. Comments</u></p> <ul style="list-style-type: none"> Extensive garage frontages along Bruce Street and vehicular crossovers is a poor streetscape outcome. <p><u>9b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Consider improving streetscape interactivity on ground floor level by reducing garage dominance further and crossover/driveway widths to 3m.
Principle 10 Aesthetics	<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
	<p><u>10a. Comments</u></p> <ul style="list-style-type: none"> Lack of material diversity along Clark and Bruce Streets. Large areas of rendered wall and specified materiality increases dominance on the streetscape. The Clark Street elevation currently presents as a rear or secondary (side) elevation rather than an elevation addressing a streetscape. Removal of screening and visual interest along Bruce Street is a loss. <p><u>10b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Consider adding greater visual interest through diversity of materiality along the Clark Street elevation. Consider changes to the 2nd floor elevations materiality and detailing to reduce the 3 level bulk and provide a perception of a lighter/recessed element from the street. Consider incorporating the previous screening on the Bruce Street elevation without compromising the street setback.
SUMMARY	The sketch drawings and their lack of key dimensions make full and accurate assessment difficult. Notwithstanding that, bulk, scale, context and landscape quality all require significant attention.
OVERALL RECOMMENDATION	Not Supported

Design Review progress

	<i>Supported</i>		
	<i>Further information required</i>		
	<i>Not yet supported</i>		
	<i>Yet to be addressed</i>		
	<i>DR1 – 4/11/24</i>	<i>DR2</i>	<i>DR3</i>
Principle 1 - Context and character			
Principle 2 - Landscape quality			
Principle 3 - Built form and scale			
Principle 4 - Functionality and build quality			
Principle 5 - Sustainability			
Principle 6 - Amenity			
Principle 7 - Legibility			
Principle 8 - Safety			
Principle 9 - Community			
Principle 10 - Aesthetics			

From: [Simon Anderson](#)
To: [Kirsty Klimcke](#)
Subject: Re: 37 Clark Street - Five Grouped Dwellings - DRP Chair Review
Date: Friday, 11 April 2025 11:53:07 AM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)

[EXTERNAL EMAIL] DO NOT CLICK links or attachments unless you recognise the sender and know the content is safe. If suspicious REPORT to IT.

Kirsty

Thank you for the opportunity to keep helping with this project.

I have now had a chance to study the amended plans and supporting documents and the response to the comments of the last DRP review.

The amended plans, in my opinion, satisfactorily respond to most of these issues and I can support the amended design proposed with one reservation. In particular the proponent responded in the following manners:

- Increased setbacks, especially to Bruce Street on level 2
- Added modulation and depth to the houses
- Provided an arborist report in support of removing the large tree on Clark Street
- Retention of large tree on Clark Street
- Bruce Street massing is now close to the intentions of the LPP
- Dropping bedrooms on level 2 to reduce the third floor dominance on the Bruce Street streetscape
- Improving bin storage and cloths drying amenity
- Provision of significant western sun shading
- Reduction of crossover widths
- Improved materiality of the houses

I remain unclear on the status of three of the four street trees. The arborist supports their retention but the architectural and landscape drawings are silent on these three trees. However a condition could be added to any approval requiring the protection and retention of all street trees in line with the arborist's report.

Overall the applicant has responded positively to the DRP advice. However the above reservation remains on the status of the street trees. When asked to undertake a Chair review of amended plans I have always closed my comments with a statement of support or otherwise for the proposal in its current form. In this case and for the reasons outlined above, I **support** the current proposal with appropriate conditions resolving the status of the street trees. Following is the updated DRP Design Quality Evaluation.

Simon

Emeritus Professor Simon Anderson LFRAIA
Senior Honorary Research Fellow
M433 School of Design
The University of Western Australia
35 Stirling Hwy, Crawley, Western Australia 6009
Mobile 0405 381 408
Email simon.anderson@uwa.edu.au

From: Kirsty Klimcke
Sent: Thursday, April 10, 2025 3:36 PM
To: Simon Anderson
Subject: RE: 37 Clark Street - Five Grouped Dwellings - DRP Chair Review

Hi Simon,

Apologies.

I have now uploaded the Arborist Report in the link.

I have attached the previous agenda and previous plans (incl landscape plan and sustainability report) to this email.

Kind regards,

Kirsty Klimcke

Acting Senior Urban Planner



Administration Centre
71 Stirling Highway WA 6009
PO Box 9 Nedlands WA 6909
9273 3500
nedlands.wa.gov.au
yourvoice.nedlands.wa.gov.au
  

From: Simon Anderson <simon.anderson@uwa.edu.au>
Sent: Thursday, 10 April 2025 12:50 PM
To: Kirsty Klimcke <kklimcke@nedlands.wa.gov.au>
Subject: Re: 37 Clark Street - Five Grouped Dwellings - DRP Chair Review

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Kirsty

For this one can you please send me:

The arborist report referred to but not on the link
The agenda from last DRP that contains all of the setback and scheme analysis (sorry, I don't save agendas)
The plans from last DRP (I don't save these either)

Sorry to ask but the proponent doesn't provide any comparisons for me to assess the changes.

Simon

Emeritus Professor Simon Anderson LFRAIA
Senior Honorary Research Fellow
M433 School of Design
The University of Western Australia
35 Stirling Hwy, Crawley, Western Australia 6009
Mobile 0405 381 408
Email simon.anderson@uwa.edu.au

From: Kirsty Klimcke <kklimcke@nedlands.wa.gov.au>
Sent: Wednesday, 9 April 2025 10:10 AM
To: Simon Anderson <simon.anderson@uwa.edu.au>
Subject: 37 Clark Street - Five Grouped Dwellings - DRP Chair Review

Good morning Simon,
Revised plans for the above application, received 31 March 2025, have been submitted to the City for consideration, see link [here](#).

Please see below list of changes from the applicant:

- Unit A increase setback on all floors to secondary street (Clark Street)
- Reduction in width of crossovers to Unit B and E.
- Increase in setback of all units on second floor to Bruce Street. Balcony introduced. Adequate compensation still not provided to achieve deemed-to-comply but have drastically improved.
- Unit E second floor increase setback/articulation to northern boundary.
- Alterations to Bruce Street elevations
- Arborist Report provided

The table below is a summary of the DRP scoring from the previous meetings. Can you please provide an update to the table to indicate your review, and any additional comments and/or recommendations.

The City's reports are required to be completed by next Tuesday 15th April given DAP deadlines. Can you please

provide your comments via return email by no later than Monday 14 April 2025 12pm.

DRP Design Quality Evaluation			
	Supported		
	Further Information Required		
	Not supported		
SPP 7.0 Principles	4 November 2024 DRP 1	10 February 2025 DRP 2	Revision 3 (pending Chair Review)
Context and Character			
Landscape Quality			
Built Form and Scale			
Functionality and Built Quality			
Sustainability			
Amenity			
Legibility			
Safety			
Community			
Aesthetics			

Kind regards,

Kirsty Klimcke

Acting Senior Urban Planner



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15 October 2024

Chief Executive Officer
City of Nedlands
71 Stirling Highway
NEDLANDS WA 6009

Application for Development Approval – Five (5) Grouped Dwellings Lot 414 (No. 37) Clark Street, Nedlands

Lateral Planning acts on behalf of Welink Group Pty Ltd (**Client**), the developer of Lot 414 (No. 37) Clark Street, Nedlands (**Site**). We have been instructed by our Client to prepare and lodge this Application for Development Approval (**Application**) for five (5) grouped dwellings at the Site.

To assist in the City's processing of this Application, please find enclosed the following:

- A copy of the signed MRS Form 1, DAP Form 1 and City of Nedlands Development Application Form;
- A copy of the Site Plan, Floor Plans and Elevations and Landscape Plan.

Description of Site

Location

The Site is located approximately 5.5 kilometres south-west of the Perth Central Business area in the municipality of the City of Nedlands and suburb of Nedlands.

The Site is located at the corner of Clark Street and Bruce Street and is 200m south of Stirling Highway.



Cadastral Information

The Site currently comprises one (1) land parcel and is legally described as follows:

- Lot 414 on Plan 2948, Certificate of Title Volume 845 Folio 41.

There are no encumbrances or easements listed on the Certificate of Title.

Site and Surrounds

The Site is 941m² in area with a frontage of 23 metres to Bruce Street to the south and 46.1 metres to the west. Primary access to the Site is taken via an existing crossover to Bruce Street.

The Site is characterised by a gradually sloping topography ranging from approximately 21.7m AHD in the south-western corner to 20.33m AHD at the north-easternmost corner. The proposed dwellings have been designed in a manner to respect and respond to the existing natural topography of the land.

The Site is occupied by an existing dwelling and outbuilding that will be demolished as part of the redevelopment of the Site.

With respect to the wider context, the surrounding locality is predominately characterised by a residential neighbourhood comprised of single and grouped dwellings, ranging in size from 1 to 3 storeys in height.



Subdivision Approval

In September 2024, the Western Australian Planning Commission (**WAPC**) issued subdivision approval for creation of five (5) survey-strata lots at the Site (WAPC Ref: 589-24). The proposed lot areas are detailed in the following table:

Proposed Lot	Area (m ²)	Frontage
Proposed Lot 1	204.5m ² (excluding truncation area)	9.44m to Bruce Street (Primary) 23.05m to Clark Street (Secondary)
Proposed Lot 2	185m ²	8.66m to Bruce Street
Proposed Lot 3	188m ²	9.17m to Bruce Street
Proposed Lot 4	180m ²	9.17m to Bruce Street
Proposed Lot 5	181m ²	9.68m to Bruce Street

Condition 5 of the Subdivision Approval requires the provision of a 3m x 3m truncation at the corner of Clark Street and Bruce Street. The proposed dwellings have been designed to cater for the required 3m x 3m truncation which will be ceded at the upon subdivision.

A copy of the Subdivision Approval is enclosed within the application documentation.

Proposed Development

This Application seeks approval for the development of five (5) three-storey Grouped Dwellings on the Site.

Lot A is designed to take primary access at the rear via a crossover to Clark Street. The ground floor contains the garage, primary living area, kitchen, dining and outdoor living areas. The upper floors contain the bedrooms, secondary living areas and a terrace.

Lots B to E are designed to take primary access via crossovers to Bruce Street. The ground floors contain the garage, secondary living area and a bedroom. The first levels contain the primary living area, kitchen dining and terrace area, with the upper floor containing the master suite and additional bedrooms.

Town Planning Considerations

State Planning Framework

Metropolitan Region Scheme

The Site is zoned 'Urban' under the Metropolitan Region Scheme (MRS) and is not affected by any MRS reserves.

State Planning Policies and Planning Codes

The following State Planning Policies are relevant to the consideration of the Application:

- State Planning Policy 5.4 – Road and Rail Noise (**SPP5.4**);
- State Planning Policy 7.0 – Design of the Built Environment (**SPP7.0**); and
- Residential Design Codes Volume 1 (**R-Codes**).

State Planning Policy 5.4 – Road and Rail Noise

The proposed development is subject to SPP5.4 due to the Site's proximity to Stirling Highway, which is identified as an 'Other Significant Traffic Route'. In this regard, the table below details the distance of the Site to the edge of the carriageway.

Transport Corridor Classification	Road Name	Number of Lanes	Distance (Lot Boundary to nearest edge of carriageway)
Other Significant Freight / Traffic Route	Stirling Highway	4	160m

Utilising the Initial Screening Assessment and Noise Exposure Forecast table contained within SPP5.4, due to the Site's distance from Stirling Highway, the Site is not subject to excess noise.

Transport Corridor Classification	Number of lanes (both directions), including bus/priority lanes and entrance/exit ramps	Forecast noise exposure category based on lot distance (m) from edge of nearest main road carriageway (not entrance/exit ramps)																				Forecast Excess Noise Level, dB	Exposure Category	Policy requirements for noise-sensitive land-use and/or development		
		adjacent	10	20	30	40	50	60	70	80	90	100	110	120	130	140	150	175	200	225	250				275	300
Strategic freight/major traffic route • 500 or more Class 7-12 Austroads vehicles per day, or • 50,000+ vehicles per day	2 to 4 lanes	72	68	66	65	63	62	61	61	60	59	59	58	57	57	56	55	54	53	52	51	50	0 to 1	A	No further measures	
	5 to 6 lanes	74	70	68	66	65	64	63	62	61	61	60	59	59	58	58	57	56	55	54	53	52	1 to 3	A+	No noise-sensitive land-use and/or development is acceptable, subject to:	
	7 to 8 lanes	76	72	69	68	66	65	64	64	63	62	62	61	60	60	59	58	57	56	55	54	53	4 to 7	B	Mitigation measures in accordance with an approved noise management plan;	
	9 to 10 lanes	77	73	70	69	67	66	65	65	64	63	63	62	61	61	60	59	58	57	56	55	54	8 to 11	C	or quiet house package as specified	
Other significant freight/traffic routes • Any actual or planned future State Administered Road • Local Government Roads Carrying 100 or more Class 7 – 12 Austroads vehicles/day • 25,000+ vehicles per days vehicles/day	Urban Region Scheme areas 60-80 km/hr	1 to 2 lanes	67	64	62	61	60	59	58	57	56	56	55	54	54	53	53	52	51	50	49	48	47	12 to 15	D	Noise-sensitive land-use and/or development is not recommended. There is no default quiet house option due to excessive forecast noise; professional design input is required in order to achieve compliance with relevant criteria. If noise-sensitive land-use and/or development is unavoidable, an approved noise management plan is required to demonstrate compliance with the noise target (see Table 1).
	Urban Region Scheme areas 100+ km/hr	3 to 6 lanes	69	66	64	63	62	61	60	59	58	58	57	56	56	55	55	54	53	52	51	50	49	16+	E	
	Rural areas 60-80 km/hr	1 to 2 lanes	62	59	57	56	55	54	53	52	51	51	50	49	49	48	48	46	45	44	43	42	41			
	Rural areas 100+ km/hr	3 to 4 lanes	66	63	61	60	59	58	56	56	55	54	53	53	52	52	51	50	49	48	47	46	45			

Accordingly, an Acoustic Assessment has not been prepared and implementation measures to mitigate the effects of traffic noise is not required.

State Planning Policy 7.0 – Design of the Built Environment

SPP7.0 contains 10 Design Principles to guide the built form of development. The City's Design Review Panel will review the development having regard to the Design Principles of SPP7.0.

Residential Design Codes – Volume 1

The development has been assessed against the Deemed-to-Comply provisions in Part C of Volume 1 of the R-Codes. The development satisfies the majority of the Deemed-to-Comply provisions, as applicable to Grouped Dwellings on land coded R60 and as modified the City’s adopted Local Planning Policies.

The table provide at Attachment 1 described the Deemed-to-Comply variations and provides a Design Principles Assessment for each.

Refer to **Attachment 1 – R-Codes Assessment**.

Local Planning Framework

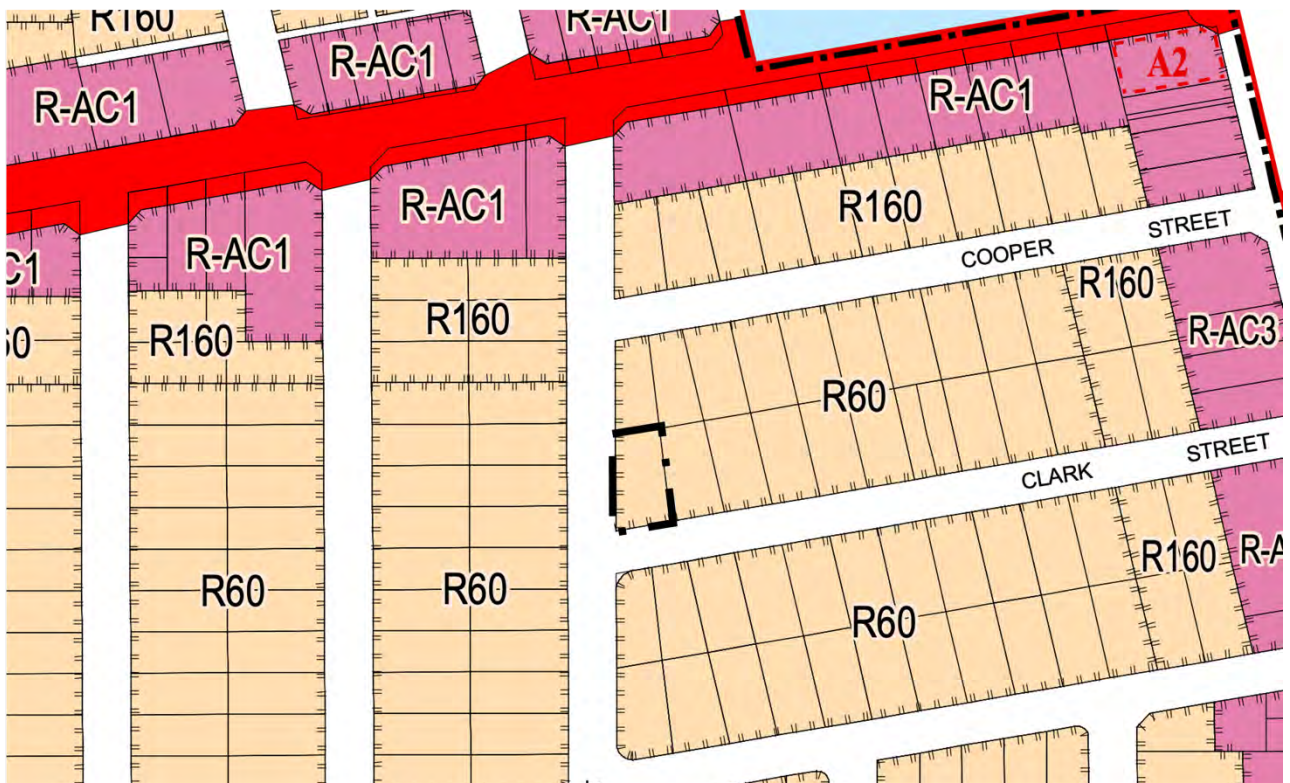
City of Nedlands Local Planning Scheme No. 3

Zoning

Pursuant to the City of Nedlands Local Planning Scheme No. 3 (**LPS3**), the Site is zoned ‘Residential’ with an applicable density code of R60.

The R60 density code applies to the surrounding properties to the immediate north, south, east and west.

Residential uses, including single houses, grouped dwellings and multiple dwellings, are permitted (‘P’) uses in the Residential Zone.



Development Provisions

Clause 25 of LPS3 states that the Residential Design Codes (**R-Codes**) form part of the Scheme, as modified by Clause 26 of LPS3. None of the modifications set out in Clause 26 apply to land with a density code of R60. Further discussion of the R-Codes is provided below.

Local Planning Policies

The City has adopted various Local Planning Policies pursuant to LPS3, including the following Local Planning Policies relevant to the consideration of this Application:

- Local Planning Policy 1.1 – Residential Development (**LPP1.1**);
- Local Planning Policy 3.2 – Waste Management (**LPP3.2**); and
- Local Planning Policy 5.14 – Precincts (**LPP5.14**).

LPP1.1, LPP3.2 and LPP5.14 contain provisions that augment or vary certain provisions of the R-Codes as applicable to land coded R60. An assessment against LPP3.2 is also provided below.

Waste Management

LPP3.2 requires a Waste Management Plan be prepared for any development proposing five (5) or more grouped dwellings. This section of the town planning statement addresses the proposed waste management of the development demonstrating compliance with LPP3.2.

LPP3.2 includes estimated waste generation rates for residential development. Based on the waste generation rates included in LPP3.2, it is estimated that each dwelling will generate the following amount of waste:

- General Waste: 120L / week.
- Recycling: 240L / fortnight.
- Green Waste: 240L / fortnight.

As shown on the Development Application plans, each dwelling has a designated area for the storage of bins. It is anticipated that waste collection for Dwellings B – E will occur from Bruce Street, and waste collection for Dwelling A will occur from Clark Street.

Each resident will be responsible for presenting the bins on bin collection day and returning bins to the bin store area after collection.

As noted at Clause 3.1.4 of LPP3.2, the City may introduce a Food organic Garden organic waste stream in the future, and allocated for a 240L FOGO bin will need to be catered for. Each dwelling can accommodate an additional 240L bin in the designated bin storage areas.

It is anticipated a condition on planning approval requiring a waste management plan being lodged with the City prior to building permit being issued.

Conclusion

This Town Planning Statement has been prepared in support of an Application to construct five (5) three-storey grouped dwellings at Lot 414 (No. 37) Clark Street, Nedlands. The development has been architecturally designed by Justin Everitt Design, drawing on the context of the surrounding locality and responding to the site opportunities and constraints to create dwellings that are attractive and result in a high-quality streetscape.

The proposed development satisfies the relevant considerations listed in the City's Local Planning Scheme No. 3, is consistent with the principles of orderly and proper planning and will not have a detrimental impact on the amenity of the locality.

Having regard to the above and the information enclosed within the Application, we are of the view that the proposed development is a positive addition to the Nedlands precinct and is capable of being supported by the City and DAP.

Should you require any further information or clarification in relation to this matter, please contact Adrian Dhue on 0400 411 114.

Yours faithfully,



Adrian Dhue

Lateral Planning

GENERAL NOTES:
 1. THIS IS A CONCEPT PLAN ONLY.
 2. ALL STRUCTURES SUBJECT TO ENGINEERING AND COUNCIL APPROVAL.
 3. ALL MEASUREMENTS TO BE CHECKED PRIOR TO CONSTRUCTION.

City of Nedlands
 Amended Plans Received
 30 January 2025

SELECTED TREE SPECIES



SELECTED GROUNDCOVERS



SELECTED STRAPPY



SELECTED SHADE PLANTING



SELECTED SHRUB PLANTING



SELECTED FEATURE SPECIES



REV	DATE	DWN	APP	DESCRIPTION
A	AL	KD	LANSAE ONET AGES

PLANTING SCHEDULE

Symbol	Species	Common Name	Quantities	Size
Trees:				
CITlem	Citrus limon	Eureka Lemon	As Shown	100L
CORfic	Corymbia ficifolia (grafted)	WA Red Flowering Gum	As Shown	100L
EUCvic	Eucalyptus victrix	Little Ghost Gum	As Shown	100L
HYMfla	Hymenoporum flavum	Native Frangipani	As Shown	100L
LAGind	Lagerstroemia indica 'Acoma' (White)	Dwarf White Crepe Myrtle	As Shown	100L
Groundcovers and Cascading:				
ADEscu	Adenanthos cuneatus	Coral Carpet	2/m2	140mm
BANniv	Banksia Nivea	Honeygot Dryandra	2/m2	140mm
DICsil	Dichondra 'Silver Falls'	Silver Falls	2/m2	140mm
EREBlu	Echeveria imbricata	Blue Rose	2/m2	140mm
EREBlu	Eremophila 'Blue Horizon'	Blue Horizon	2/m2	140mm
FICpum	Ficus pumila	Creeping Fig	2/m2	140mm
GREgin	Grevillea 'Gin Gin Gem'	Gin Gin Gem	2/m2	140mm
HIBsca	Hibbertia scandens	Snake Vine	2/m2	140mm
MYOpar	Myoporum parvifolium 'Yareena'	Yareena	2/m2	140mm
ROSoft	Rosmarinus officinalis 'Prostratus'	Creeping Rosemary	2/m2	140mm
SCAhum	Scaevola humilis 'Purple Fusion'	Fan Flower	2/m2	140mm
Strappy:				
CONcan	Conostylis candidans	Grey Cottonheads	3/m2	140mm
DIAeme	Dianella tasmanica 'Emerald Arch'	Emerald Arch	3/m2	140mm
DIAbla	Dianella tasmanica 'Blaze'	Blaze	3/m2	140mm
FIONod	Fionia nodosa	Knobby Club Rush	3/m2	140mm
LOMTan	Lomandra 'Tanika'	Tanika	3/m2	200mm
LOMNya	Lomandra 'Nyala'	Nyala	3/m2	200mm
Shrubs:				
LELBro	Leucophyta brownii	Silver Cushion Bush	3/m2	140mm
MURpan	Murraya paniculata	Orange Jessamine		200mm
OLEaxi	Olearia axillaris	Coastal Daisy	3/m2	200mm
PIMfer	Pimelea ferruginea	Rice Flower	3/m2	200mm
PITmis	Pittosporum tobira 'Miss Muffet'	Miss Muffet	3/m2	200mm
RHAori	Rhapidolepis 'Oriental Pearl'	Dwarf Indian Hawthorn	2/m2	200mm
SYZora	Syzgium 'Orange Twist'	Lilly Pilly	2/m2	200mm
WESaus	Westringia 'Aussie Box'	Aussie Box	3/m2	200mm
WESgre	Westringia 'Grey Box'	Grey Box	3/m2	200mm
Shade Planting:				
AJJrep	Ajuga reptans	Bungie weed	3/m2	140mm
CLImin	Clivia miniata	Bush Lily	3/m2	200mm
CORdu	Correa 'Dusky Bells'	Dusky Bells	3/m2	140mm
DARcit	Darwinia citriodora	Darwinia	2/m2	140mm
DICrep	Dicentra repens	Kidney plant	3/m2	140mm
LIRjus	Liriope 'Just Right'	Just Right	3/m2	140mm
OPHjap	Ophiopogon japonicus	Mondo Grass	3/m2	200mm
PHIXan	Phlox xanthocarpa	Xanadu	3/m2	200mm
SANtri	Sansevieria trifasciata laurentii	Mother-in-law's Tongue	3/m2	200mm
VIOhed	Viola hederacea	Native Violet	3/m2	200mm
Feature Plants:				
ANIfila	Anigozanthos flavidus	Kangaroo Paw	2/m2	12L
GARaug	Gardenia augusta	Gardenia Romance	2/m2	30L
ZAMfur	Zamia furfuracea	Cardboard Palm	2/m2	12L

NOTES

GENERAL

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RRGATON

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REV	DATE	DWN	APP	DESCRIPTION
A	20.01.25	ALC	KD	LANDSCAPE CONCEPT PLAN
B	29.01.25	ALC	KD	UPDATED LANDSCAPE CONCEPT PLAN



LEGEND TREES

- EXISTING TREES TO BE REMOVED
- EXISTING TREES TO BE RETAINED
- SELECTED SMALL TREES EG. RED FLOWERING GUM CANOPY: 5m
- SELECTED SMALL TREES EG. CREPE MYRTLE CANOPY: 4m
- SELECTED SMALL TREES EG. NATIVE FRANGIPANI CANOPY: 4m
- SELECTED FRUITING TREES EG. CITRUS CANOPY: 3m

FEATURE PLANTS

- SELECTED FEATURE PLANTING EG. GARDENIA, BIRD OF PARADISE, KANGAROO PAW

PLANTING

- PLANTING TYPE 01 GROUNDCOVERS
- PLANTING TYPE 02 SHRUBS
- PLANTING TYPE 03 STRAPPY/MIXED
- PLANTING TYPE 04 SHADE PLANTING
- SELECTED LAWN SPECIES EG. SOFT LEAF BUFFALO

MISC FINISHES

- PAVING TO BE CONFIRMED
- PERMEABLE PAVED STEPPERS
- DEEP SOIL AREAS (DSA)

LANDSCAPE CALCULATIONS

Lot/Unit	Lot Area	Landscaped Area	Deep Soil Area	Front Setback landscape %	No. of Trees
1	181m ²	43.2m ² (23%)	40.6m ² (22.4%)	18m ² (41%)	3
2	180m ²	30m ² (16.6%)	33m ² (18.3%)	17.6m ² (47.5%)	3
3	188m ²	39m ² (20.7%)	40m ² (21.2%)	16.8m ² (43%)	3
4	185m ²	31m ² (16.7%)	31m ² (16.7%)	10.8m ² (30%)	3
5	209m ²	41.6m ² (20%)	47m ² (22.4%)	25m ² (69%)	2
Total:	943m²	184.6m² (19.5%)	191.6m² (20%)	88m² (46%)	14

NOTE: VERGE SPECIES SELECTED FROM CITY OF NEDLANDS 'NATURE STRIP IMPROVEMENT GUIDELINES' DOCUMENT



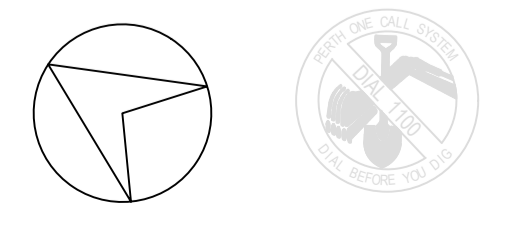
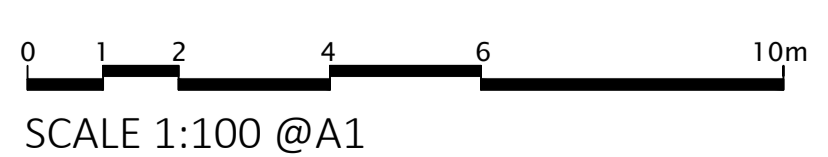
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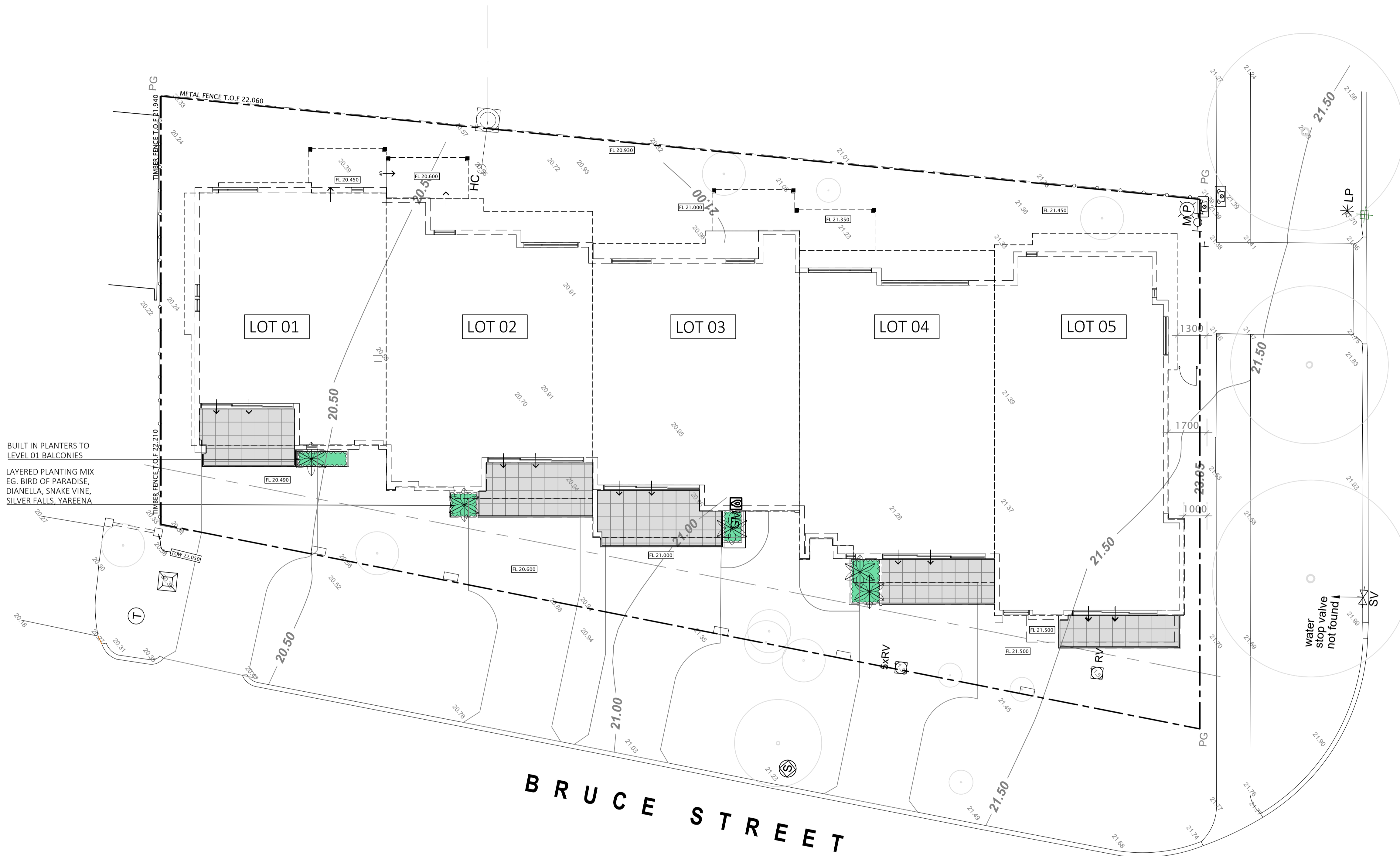
REV B



CLARK STREET TOWNHOUSES LANDSCAPE CONCEPT PLAN_ GROUND FLOOR

WELINK GROUP PTY LTD
 37 CLARK STREET, NEDLANDS

REV	DATE	DWN	APP	DESCRIPTION
A	20.01.25	ALC	KD	LANDSCAPE CONCEPT PLAN
B	29.01.25	ALC	KD	LANDSCAPE CONCEPT PLAN - UPDATED



LEGEND TREES

- EXISTING TREES TO BE REMOVED (Red dashed circle with 'X')
- EXISTING TREES TO BE RETAINED (Black solid circle)
- SELECTED SMALL TREES EG. RED FLOWERING GUM CANOPY: 5m (Green tree icon)
- SELECTED SMALL TREES EG. CREPE MYRTLE CANOPY: 4m (Green tree icon)
- SELECTED SMALL TREES EG. NATIVE FRANGIPANI CANOPY: 4m (Green tree icon)
- SELECTED FRUITING TREES EG. CITRUS CANOPY: 3m (Green tree icon)

FEATURE PLANTS

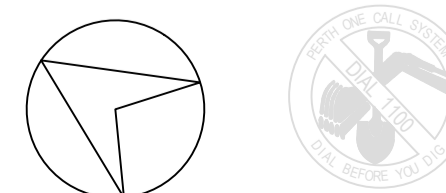
- SELECTED FEATURE PLANTING EG. GARDENIA, BIRD OF PARADISE, KANGAROO PAW (Flower icons)

PLANTING

- PLANTING TYPE 01 GROUNDCOVERS (Light green pattern)
- PLANTING TYPE 02 SHRUBS (Dark green pattern)
- PLANTING TYPE 03 STRAPPY/MIXED (Medium green pattern)
- PLANTING TYPE 04 SHADE PLANTING (Yellow-green pattern)
- SELECTED LAWN SPECIES EG. SOFT LEAF BUFFALO (Light green pattern)

MISC FINISHES

- PAVING TO BE CONFIRMED (Grey grid pattern)
- PERMEABLE PAVED STEPPERS (Grey stepped pattern)





EMERGEN

City of Nedlands
Amended Plans Received
30 January 2025

SUSTAINABLE DESIGN ASSESSMENT REPORT

37 CLARK STREET, NEDLANDS WA

PREPARED BY

ALJENRY VALINO & NYONIKA OBERAI

Project: 131379

Published Date: 28/01/2025



DOCUMENT REVISION

DATE	DOCUMENT	COMPLETED BY	REVIEWED BY	APPROVED BY	REV NO.
28/01/2025	Architectural Plans	Aljenry Valino	Nyonika Oberai	Glenn Underwood	1

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DRAWING NO.	DRAWING TITLE	REVISION NO.
0325 20250117 Arch. Set	Architectural Drawings	Design Approval

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DISCLAIMER

The contents of this report are based on the documentation and plans provided by the Client to EMERGEN/CADDS Group. The outcomes presented are representative of schematic systems, schedules, and project design. These results should not be considered as actual energy usage.





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1 OVERVIEW

EMERGEN has developed a sustainable design report on the proposed grouped dwellings at 37 Clark Street, Nedlands WA in consultation with Lateral Planning.

The purpose of this report is to support the development application by identifying the principles incorporated in the design that meet sustainable objectives and targets for the site.

1.1 STATE PLANNING POLICY AND LOCAL PLANNING POLICIES

EMERGEN (a division of CADDIS GROUP), in collaboration with the design team (Lateral Planning) has developed a sustainable design strategy aligning with **State Planning Policy 7.0 and LPP of City of Nedlands**, which focuses on the Design of the Built Environment - specifically, Principle 5: Sustainability.

This report serves the vital purpose of bolstering the development application by articulating the sustainability principles and commitments for the project site. We acknowledge the significance of State Planning Policy 7.0 and LPP of City of Nedlands in promoting sustainability within the built environment. Good design, as outlined in the policy, is not only about aesthetics but also optimises the sustainability of our built surroundings, yielding positive outcomes on environmental, social, and economic fronts.

Our approach to sustainable landscape and urban design adheres closely to the established water-sensitive urban design principles, ensuring minimal adverse impacts on existing natural features and ecological processes while promoting green infrastructure at all scales of the project. Furthermore, our strategy for sustainable built environments embraces passive environmental design measures tailored to local climate and site conditions. This includes careful consideration of optimal orientation, shading, building envelope, and natural ventilation, ultimately reducing reliance on energy-intensive heating and cooling technologies. This, in turn, results in reduced energy consumption, decreased resource usage, and lowered operating costs throughout the project's lifecycle.



1.2 DESIGN AND CONSTRUCT TO AUSTRALIAN BEST PRACTICE

The design team will utilise a structured approach to a sustainable outcome for the design and construction of the development including the following Sustainable Targets.

Table 1: Sustainability Targets

DESCRIPTION	GOAL	SUSTAINABILITY COMMITMENTS
CLEAR AIR	Improve indoor environment quality and health and wellbeing of occupants.	Outdoor air provided to primary areas at a rate at least 50% greater than minimum in AS 1668.2:2012. <i>(TBC based on mechanical consultant).</i>
LIGHT QUALITY		Above 40% of the regularly occupied areas have high level of daylight (above 160 Lux).
EXPOSURE TO TOXINS		The building's paints adhesives, sealants, and carpets are low in TVOC or non-toxic. The building's engineered wood products are low in TVOC or non-toxic. Occupants are not exposed to banned or highly toxic materials in the building.
NATURAL VENTILATION	Increase Fresh Air	Natural Ventilation to all dwellings.
HEAT RESILIENCE	Reduce impacts of long-term performance.	Light Roof Colour (SRI \geq 0.64)
ENERGY USE	Reduce emissions and water use.	All dwellings exceed the minimum NatHERS energy efficiency requirement by 0.5 stars, based on the NCC 2022.
		Provision for solar PV array to supply renewable energy to each dwelling.
WATER USE		High WELS Ratings (these equal to above 30% reduction in potable water). Water Sensitive Urban Design, drip irrigation. Stormwater collected, filtered, and used for sub-soil irrigation
LIGHTING USE		20% reduction in lighting power when compared to NCC DTS.
MOVEMENT AND PLACE	Low carbon options.	Provision for EV Charging Bays. Provision for Bike Storage.



2 PROJECT INFORMATION

The proposed project is located on previously developed land at 37 Clark Street, Nedlands WA. The project is focused on the revitalization of the existing residential lot, with the objective of redeveloping the site consisting of five grouped dwellings.

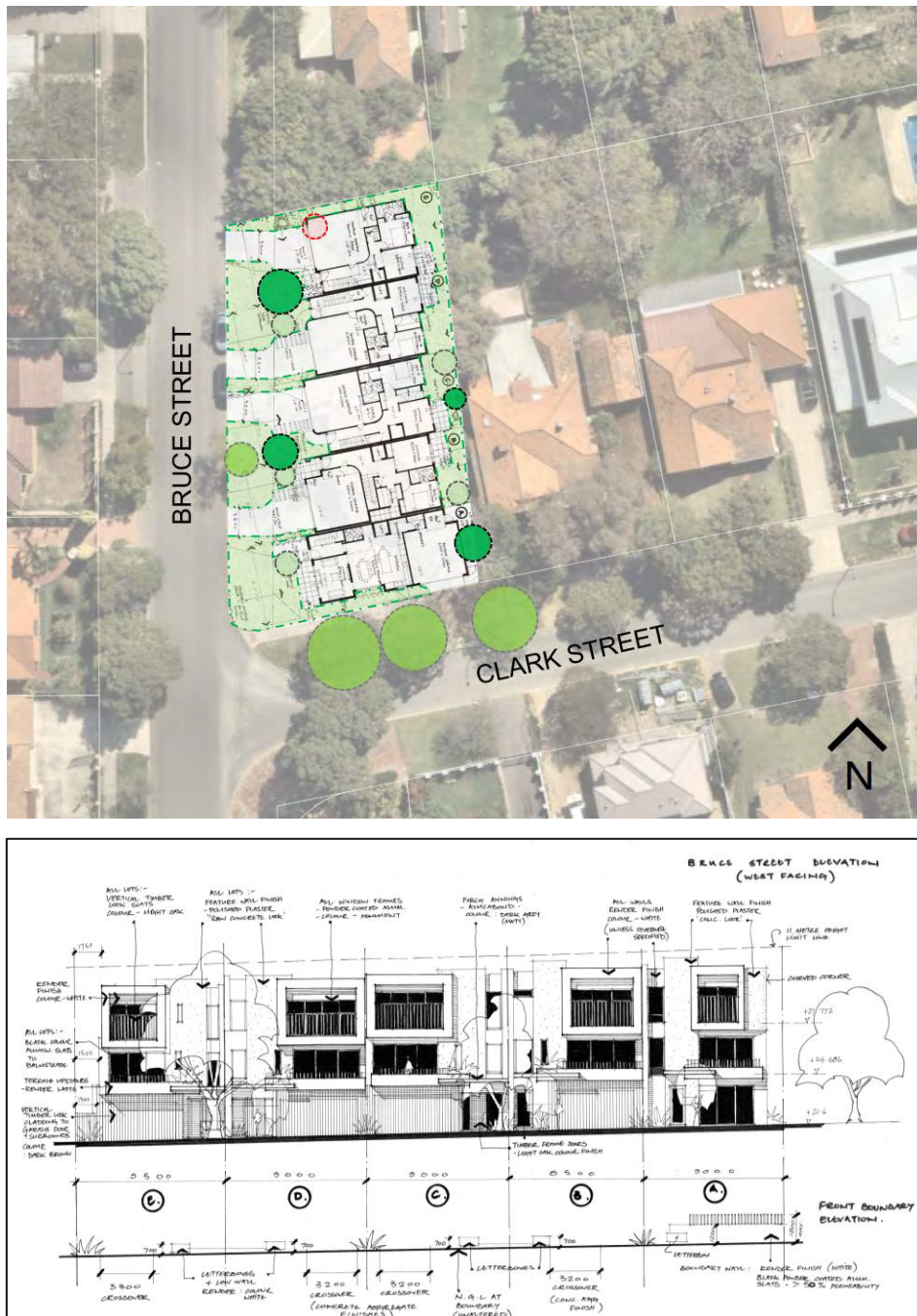


Figure 1: Existing Site Image/ Proposed Development

3 ENERGY USE

3.1 CLASS 1 UNITS (NATHERS) SPECIFICATION

The construction for the proposed residential building is as per plans provided and outlined below.

Table 2: Building Fabric Specification

CONSTRUCTION		DESCRIPTION	REQUIREMENT
ENVELOPE WALLS	External Brick Cavity Walls	230mm Brick Cavity wall (90/50/90) External finishes as per elevations.	Kooltherm K8 25mm R1.25 Insulation (or equivalent)
	Party Walls	230mm Brick Cavity wall (90/50/90).	No Insulation Required.
INTERNAL WALLS	Steel Framed Wall	110mm Brick Venner with plasterboard lining.	No Insulation Required.
	Concrete Wall (Lift Shafts)	150mm Concrete Wall	No Insulation Required.
FLOORS	Slab on Ground	Concrete slab with ground contact.	No Insulation Required.
	Suspended Concrete Slab	Minimum 250mm suspended concrete slab.	Insulated with 30mm K10 Soffit insulation (R1.3) to exposed slab.
	<i>Coverings</i>	<i>As per drawing. If none specified, defaults used.</i>	
ROOFS	Roof Type 1	Suspended concrete roof	R4.1 Insulation Batts to dropped plasterboard ceilings.
	Roof Type 2	Metal deck roof sheeting. Cladding in Shale Grey (SA: 0.44).	60mm Anticon with R4.1 Insulation Batts to dropped plasterboard ceilings.

WINDOW SPECIFICATION		U-VALUE	SHGC (±5%)
EXTERNAL GLAZING 1:	Double Glazed Low-E Clear in Aluminium Frame (<i>Awning, Casement</i>)	4.10	0.47
EXTERNAL GLAZING 2:	Double Glazed Low-E Clear in Aluminium Frame (<i>Sliding, Fixed, Double Hung</i>)	4.10	0.52
SKYLIGHT 1:	Generic Double-Glazed Clear Roof Skylight	4.22	0.72
SKYLIGHT 2:	Solar Tube – Single Clear	2.70	0.26

Note: Glazing values provided are for Whole System (Frame + Glass)



3.2 NATHERS RESULTS

Table 3: Options and Upgrades

DWELLING	GLAZING REDUCTION	CEILING INSULATION R5.0	DOUBLE GLAZED LOW E CLEAR	RATING
A	X	X	X	7.5
C			X	8.0
E			X	8.2

Table 4: NatHERS Outcome

DWELLING	COOLING (MJ/m ²)	HEATING (MJ/m ²)	RATING
A	35.0	17.9	7.5
C	18.8	23.4	8.0
E	16.9	22.6	8.2

3.3 PV GENERATION

PV generation will reduce the operational energy loads and peak demand of the building due to the expected load profile correlating to the available solar resource. PV/Electrical Consultant to undertake sizing of PV system. The aim is to allocate the maximum available area in the roof space for solar panels. A minimum of **3kW** photovoltaic solar panel system per new dwelling will be provided.

4 HEALTH AND WELLBEING

4.1 PROVISIONS FOR INCREASED OUTDOOR AIR

The dwellings will be naturally ventilated. The naturally ventilated space will meet the requirements of AS 1668.4:2012 under all likely weather conditions.

A dwelling is considered cross ventilated where:

- a) Main habitable rooms have openings on at least two walls with a straight-line distance between the centre of the openings of at least 2.1m.
- b) It is a cross-through dwellings, it has a depth of maximum 18m glass line to glass line; or
- c) It is a single-aspect dwellings,
 - i. ventilation openings face within 45° to 90° of the prevailing cooling wind direction; and
 - ii. ventilation openings are equivalent to 7% of the floor area of the room; and
 - iii. room depth is not more than 3 times ceiling height (8m for a 2.7m high ceiling).





Figure 2: Cross Ventilation of all Primary Living Areas

4.2 EXHAUST OR ELIMINATION OF POLLUTANTS

Pollutants generated during cooking processes and from kitchen equipment are controlled in the designated area by directing the kitchen exhaust directly outside of the project, in accordance with a recognized standard.



5 LIGHTING COMFORT

Lighting within the building must meet the following criteria:

- All lighting must be flicker-free.
- Light sources must have a minimum Colour Rendering Index (CRI) average R1 to R8 of 85 or higher and have a CRI R9 of 50 or higher.
- Light sources must meet best practice illuminance levels for each task within each space type with a maintained illuminance that meets the levels recommended in AS/NZS 1680.1:2006 series applicable to the project type and including maintenance.
- The maintained Illuminance values must achieve a uniformity of no less than that specified in Table 3.2 of AS/NZS 1680.1:2006, with a maintenance factor method as defined in AS/NZS 1680.4.; and
- All light sources must have a minimum of 3 MacAdam Ellipses.
- The walls within the field of view of occupants in regularly occupied spaces must have an average surface reflectance value of 0.70 and an average surface illuminance of at least 50% of the horizontal illuminance levels required for task.
- Vertical illuminance in workspaces: ensure that 50% of the horizontal task illuminance reaches the average eye height for 90% of primary spaces using vertical illuminance calculation grid.
- The illuminance values must be calculated in accordance with AS/NZS 1680 series for the relevant task.

5.1 EFFICIENT LIGHTING AND CONTROL

The installed aggregate illumination power has been designed to be **20%** below the maximum illumination power based on maximum allowable lighting power densities defined in Table J7D3a of the NCC 2022. Motion Detectors and daylight sensors are provided to reduce demand.

Table 5: Lighting Characteristics

PARAMETER	PROPOSED BUILDING	REFERENCE BUILDING
LIGHTING TYPE	LED light fittings	LED light fittings
DESIGN ILLUMINANCE (LUX)	Various lux	Various lux
NOMINAL LIGHTING POWER DENSITY (W/M ²)	20% less compared to NCC max requirements.	As per NCC max requirements.
OCCUPANT SENSOR CONTROLS	Motion sensors	N/A
DAYLIGHT CONTROLS	Yes	N/A
OTHER LIGHTING CONTROLS	Timer switches	N/A
ADJUSTMENT FACTOR APPLIED	0.9 – Motion sensor 1 0.7 – Motion sensor 2 0.55 – Motion sensor 3	Room Aspect Ratio



5.2 ARTIFICIAL LIGHTING AND CONTROLS

All lighting will be equipped with light-emitting diodes (LEDs), covering all primary areas. Common area lighting will include controls like occupancy sensors (PIRs) and time switches to minimize energy usage when lighting is not needed.

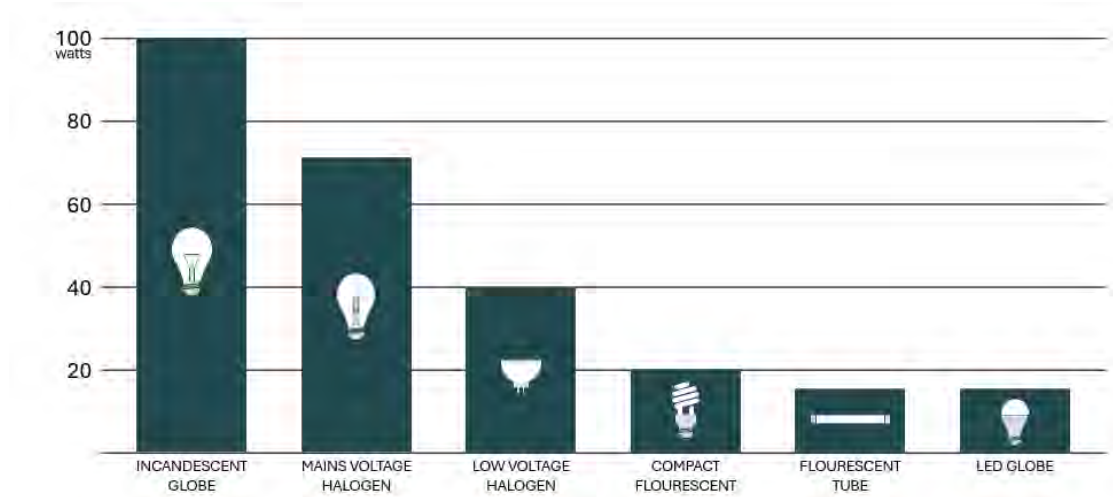


Figure 3: Comparison of LED lighting with other conventional lighting

5.3 DAYLIGHTING

Emergen have completed preliminary daylight modelling. The analysis has been undertaken using IES software at finished floor level based on CIE standard overcast sky. The study boundary includes Primary Spaces only.

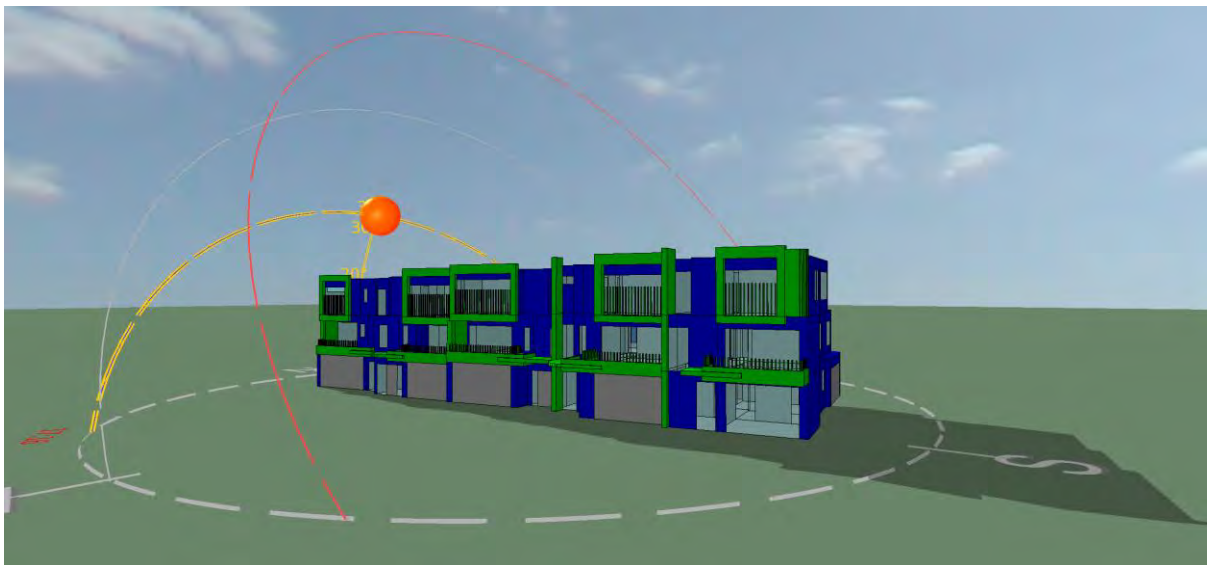


Figure 4: Daylighting Model on IESve Software

Table 6: Calculations Conditions - Daylight

CALCULATION CONDITIONS	
TEST FACTOR	Daylight Factor
SKY FACTOR	CIE Overcast Sky
DATE	September 21 st 12:00pm

Table 7: Daylighting Results

DWELLING	PROPOSE USAGE	NOMINATED AREA (m ²)	COMPLIANT AREA (m ²)	COMPLIANT PERCENTAGE (%)
A	Primary Areas (Living, Dining, & Bedrooms)	136.66	127.02	93.00
B		143.37	126.38	88.20
C		140.91	133.92	95.00
D		137.62	134.04	97.40
E		116.30	114.46	98.40

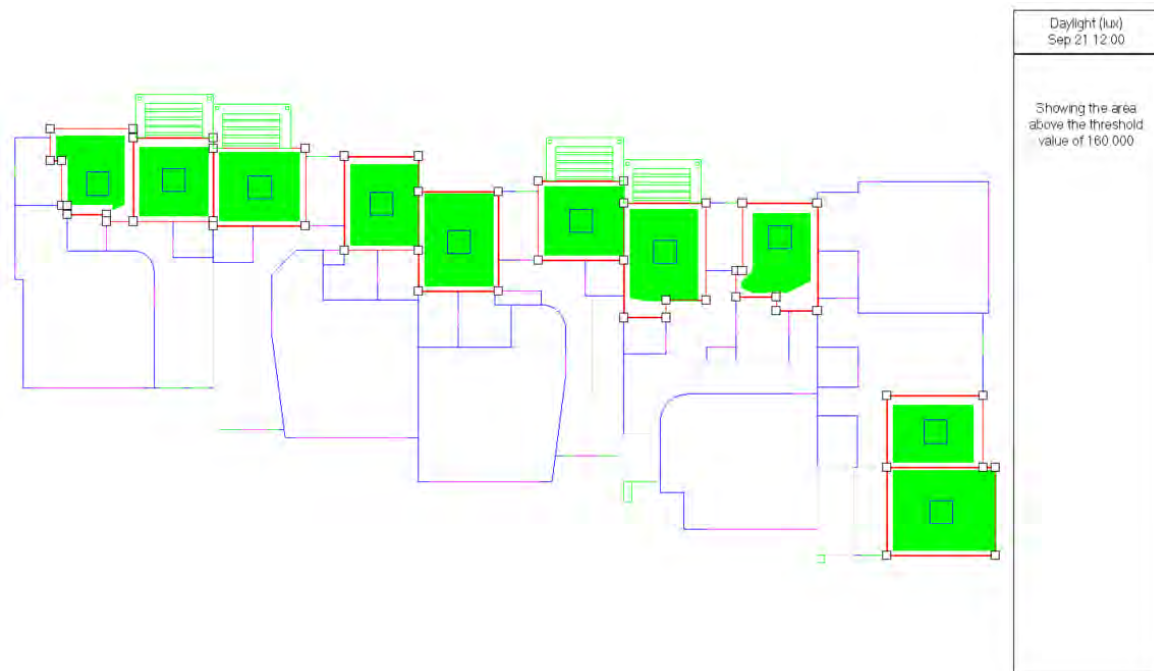


Figure 5: Daylighting on Ground Floor



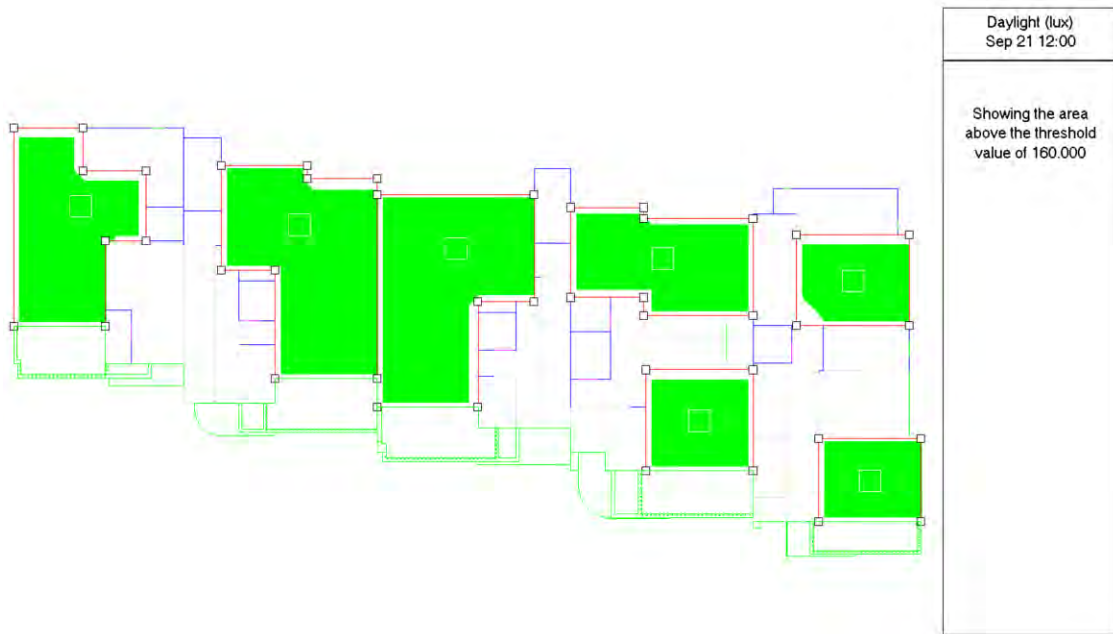


Figure 6: Daylighting on First Floor

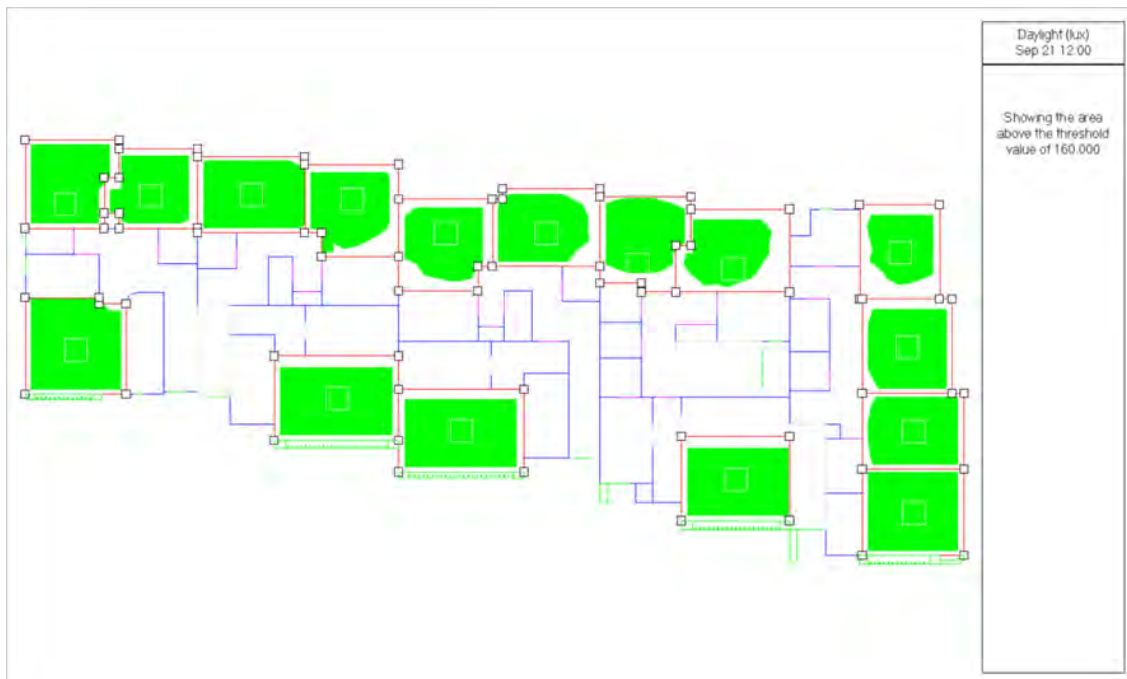


Figure 7: Daylighting on Second Floor

5.4 SOLAR ANALYSIS

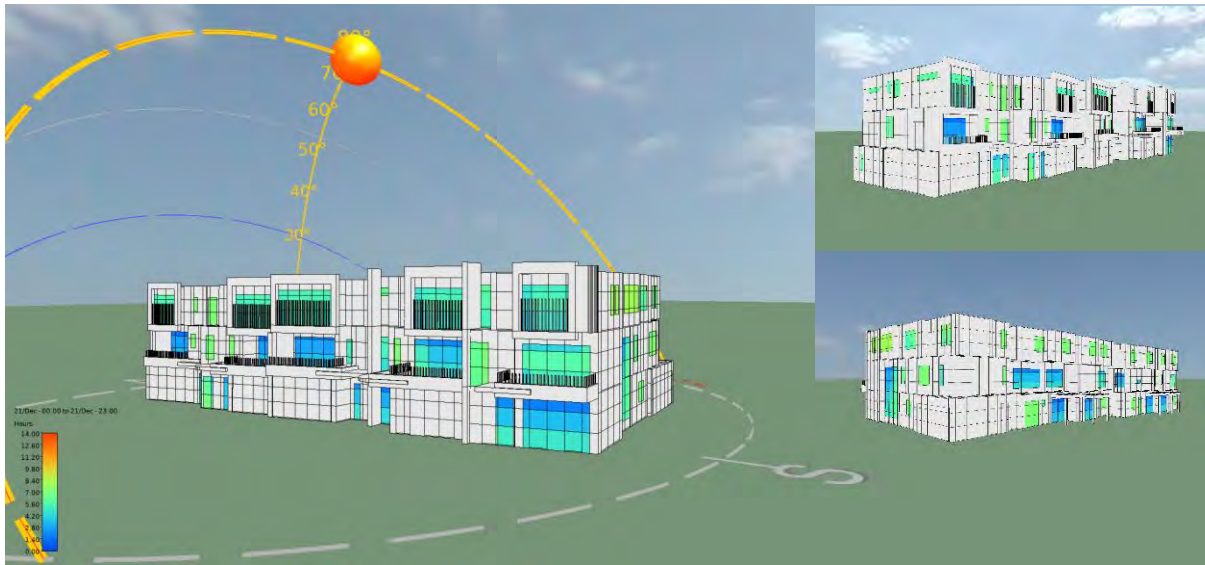


Figure 8: Solar Analysis during Summer

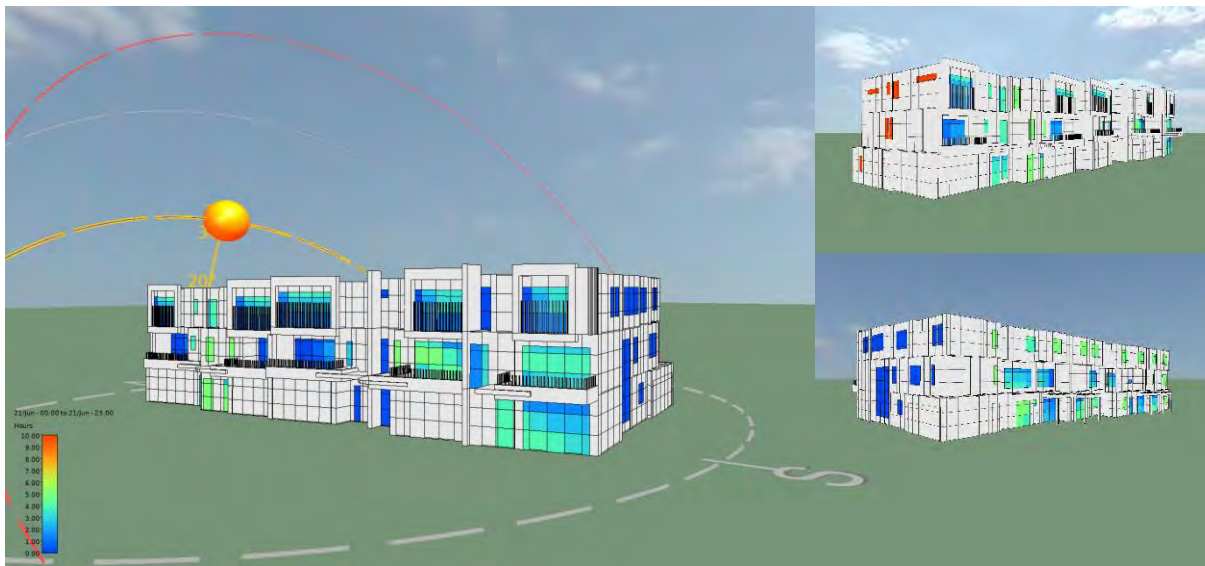


Figure 9: Solar Analysis during Winter

The shading elements have been strategically designed to minimise extended solar exposure during summer months yet maximise solar gains in winter months. During the Summer months each window, receives approximately 1 to 5 hours of daylight over the course of the day, striking a balance between harnessing natural light and mitigating excessive sunlight exposure.



6 WATER EFFICIENCY

6.1 SUSTAINABLE WATER INITIATIVES

Non-potable water use is minimal throughout the proposed design due to the application of water efficient toilets, taps and showers and relatively small areas of irrigated plantings. The design team has identified water capture, grey and black water capture and treatment during the target setting workshop but did not preference these options due to:

- Low non-potable water use requirements of the development
- Logistical constraints, energy use and operational costs associated with grey and black water treatment in office complexes influenced heavily by health regulations (significant periodic inspection and testing required in multiple dwellings applications)
- Space requirements of rainwater harvesting and/or grey and black water treatment tanks
- Relative cost vs benefit of these strategies versus competing strategies which delivered much larger environmental benefits at a lower cost.

All new water fixtures are to ensure that high WELS rating fixtures and fitting are to be specified in line with minimum benchmarks below.

Table 8: WELS Ratings

FIXTURE / EQUIPMENT TYPE	WELS RATING	MINIMUM WATER EFFICIENCY
TAPS	5 Star	3L/Min
TOILETS	5 Star	6L/Min
SHOWERS	3 Star	6L/Min

6.2 WATER EFFICIENT LANDSCAPING

Water-efficient landscaping refers to techniques and strategies aimed at conserving water while maintaining aesthetically pleasing and functional outdoor spaces. This is important in Western Australia due to its semi-arid climate conditions, which often result in water scarcity. Key strategies include:

- **Plant Selection:** Choosing native or drought-tolerant plant species that are well-suited to the local climate can significantly reduce water requirements.
- **Soil Improvement:** Improving soil quality through methods such as mulching and composting can enhance water retention and reduce evaporation, thereby optimizing water usage in landscaping.
- **Irrigation Efficiency:** Employing efficient irrigation systems such as drip irrigation or micro-sprinklers helps deliver water directly to plant roots with minimal waste. Additionally, using smart irrigation controllers that adjust watering schedules based on weather conditions and soil moisture levels can further enhance water efficiency.
- **Water Harvesting:** Capturing rainwater through techniques like rainwater tanks will allow for on-site water storage and reuse, reducing reliance on potable water for landscaping needs.
- Overall, water-efficient landscaping plays a vital role in conserving water resources, promoting sustainability, and mitigating the impacts of water scarcity in urban and rural environments.



7 MOVEMENT AND PLACE

7.1 BICYCLE PARKING FACILITIES & SUSTAINABLE TRANSPORT FACILITIES

The intention of this category is to reduce occupants' dependency on carbon-intensive vehicles. The dwellings are designed to allow for the future installation of EV charging facilities in each garage, and each dwelling is provided with a secured storage area for bikes.

7.2 ENCOURAGING WALKABILITY

The building's design and location encourage walking to and from a number of amenities. This means designing roads within the building boundary to prioritise pedestrians, and either providing within, or being located close to, a number of amenities.

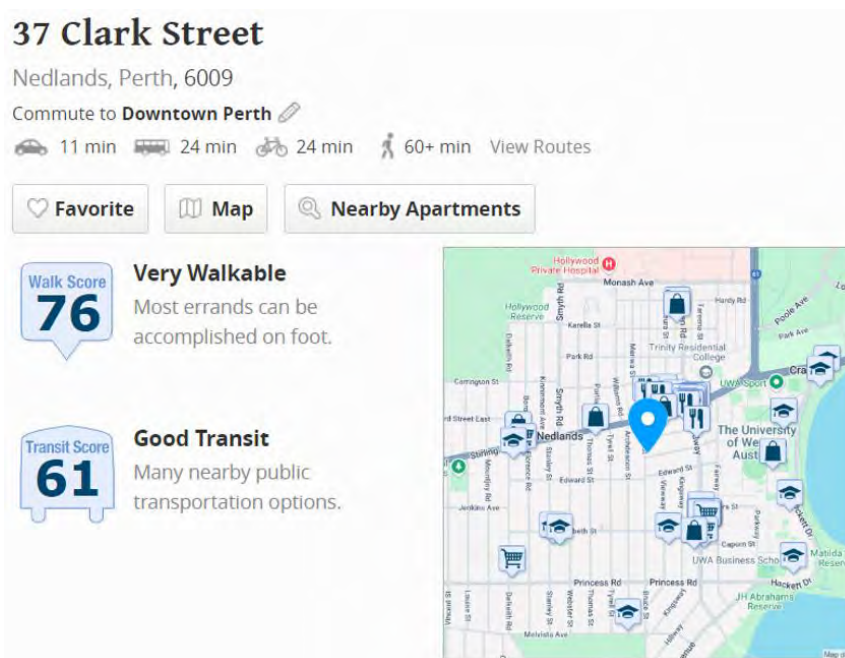


Figure 10: Walkability score of the Site

8 LAND USE AND ECOLOGY

8.1 HEAT ISLAND EFFECT

The project will dedicate more than 75% of the entire site area to include one or a combination of the following:

- Vegetation.
- Roofing materials, including shading structures, having the following:
 - For roof pitched <15°– a three-year SRI of minimum 64; or
 - For roof pitched >15°– a three-year SRI of minimum 34.
- Only where the three-year Solar Reflectance Index (SRI) for products is not available, use the following:
 - For roof pitched <15° – an initial SRI of minimum 82; or





- For roof pitched >15° – an initial SRI of minimum 39.
- Unshaded hard-scaping elements with a three-year SRI of minimum 34 or an initial SRI of minimum 39.
- Hardscaping elements shaded by overhanging vegetation or roof structures, including photovoltaic panels.



Figure 11: Potential design provisions for Heat Island Effect

9 EXPOSURE TO TOXINS

9.1 PAINTS, ADHESIVES, AND SEALANTS

To meet the requirements, at least 95% of internally applied paints, adhesives, sealants (by volume), and carpets (by area) must meet TVOC (Total Volatile Organic Compounds) limits. Compliance with these limits can be achieved through one of the following methods:

1. Product Certification Scheme: The contractor can use products that are certified under a recognized and current Product Certification Scheme at the time of purchase. These schemes assess and verify the TVOC content of the materials, ensuring they meet the specified limits.
2. Laboratory Product Testing: If there are no certified products available, the contractor can conduct laboratory testing on the paints, adhesives, and sealants, to determine their TVOC content. The testing should be carried out using the whole paint, including water and tinters, to obtain accurate results.
3. Absence of Non-compliant Materials: Alternatively, if none of the materials mentioned (paints, adhesives, sealants, and carpets) are present at the time of practical completion (PC), and thus no TVOC emissions are expected, compliance can be achieved.

All paints used for internal application on the job are to have a low TVOC content as outlined below. TVOC content must be based on whole paint (water and tinters included):



Table 9: Paint VOC limits

PRODUCT TYPE / SUBCATEGORY	MAX TVOC CONTENT (G/L OF READY TO USE PRODUCT)
Walls and ceilings – Any gloss level	16
Trim, varnishes and wood stains	75
Primers, sealers and prep coats	65

Maximum TVOC Content Limits for Paints, Varnishes and Protective Coatings

*EU Directive

The TVOC content of the ‘ready-to-use’ paint shall be theoretically calculated as the sum total of the VOCs of each of the raw material component comprising the paint.

Where the TVOC content of individual components is not known, it must be determined experimentally by one of the following testing methods as appropriate:

- ISO Method 17895 (2005), for a material with a presumed VOC content <1%;
- ISO Method 11890-2 (2006), for a material with a presumed VOC <15%;
- ISO Method 11890-1 (2007), for a material with a presumed VOC content >15%;
- ASTM D3960, which is comprised of four individual testing procedures that measures TVOC (D2369) as well as density (D1475) and water content (D4017). Exempt compounds (D4457) must not be subtracted in the calculation of VOC content.

The contractor must obtain written approval from the design team before using any sealant, adhesive, paint, flooring or fit out items. This approval will be contingent on the provision of proof that the product has a VOC content below that noted above.

At the end of construction, the contractor is required to undertake a final audit to ensure that the correct products have been used.

All sealants used in an internal application on the job are to have a low TVOC content as outlined below.

Table 10: Adhesives/Sealants VOC limits

PRODUCT	MAXIMUM TVOC CONTENT (G/LITRE)
General purpose adhesives and sealants	50
Acoustic sealants, architectural sealant, waterproofing membranes and sealant, fire retardant sealants and adhesives	250
Structural glazing adhesive, wood flooring and laminate adhesives and sealants	100
Primers, sealers and prep coats	65
One and two pack performance coatings for floors	140

Maximum TVOC limits for Adhesives & Sealants

*Sealants used to enhance the fire- and water-proofing properties are included.



The testing method applicable to adhesive and sealants is only ASTM D3960 as detailed above for paints. For more information on ASTM D3960 refer to South Coast Air Quality Management District Rule 1168.

The contractor must obtain written approval from the design team before using any sealant, adhesive, paint, flooring or fitout items. This approval will be contingent on the provision of proof that the product has a VOC content below that noted above.

At the end of construction, the contractor is required to undertake a final audit to ensure that the correct products have been used.

All carpets and/or other flooring used on the project are to have low TVOC emission rates as outlined below.

Table 11: Carpet VOC limits

ALL CARPET/FLOORING PRODUCTS MUST COMPLY WITH TVOC EMISSION LEVELS	
Total VOC limit	0.5 mg/m ² per hour
4-pc (4-Phenolcyclohexene) limit	0.05 mg/m ² per hour

Compliance Testing: Refer to Carpet and Rug Institute Green Label (US) OR American Society for Testing and Materials (ASTM) D5116 Guide for Small-Scale Environmental Chamber Determinations of Organic Emissions from Indoor Material/Products OR

For carpets and laminate floor coverings, an option for demonstrating compliance with TVOC levels is as follows: ISO 10580 (also known as ISO/TC 219) – Document N238 – Resilient,

Textile and Laminate Floor Coverings Evaluation of Volatile Organic Compounds Emissions, with a limit of 500µg/m²/hr at 24 hours. OR

For floor coverings (other than carpet), an option for demonstrating compliance with TVOC levels is as follows:

ISO16000 parts 9, 10 and 11 (also known as the EN 13419), with a TVOC limit at three days of 5mg/m²/h and 0.5mg/m²/h at 28 days.

Carpet or other flooring installed as part of the base building works prior to fit out works, can be deemed re-used for the purpose of this credit.

The contractor must obtain written approval from the design team before using any sealant, adhesive, paint, flooring or fit out items. This approval will be contingent on the provision of proof that the product has a VOC content below that noted above.

At the end of construction, the contractor is required to undertake a final audit to ensure that the correct products have been used.

There are two options for demonstrating compliance for carpets, as follows:

Option A - Product Certification:

Carpets certified under a relevant Product Certification Scheme standard recognised by the GBCA under the GBCA assessment Framework for Product Certification Schemes are deemed to satisfy the requirements of this criterion. Relevant GBCA recognized standards are listed on the GBCA web site. The certificate must be current at the time of project registration or submission and list the relevant product name and model.



A UL GREENGUARD Children & Schools® certification current at the time of project registration or submission is another acceptable evidence for demonstrating compliant TVOC levels for carpets.

Option B - Experimental Testing

All carpets comply with the Total VOC (TVOC) limits within Table below. The emission levels detailed in this table must be established by a NATA or another ISO/IEC17025 accreditation laboratory.

Table 12: Flooring VOC limits

ALL CARPET/FLOORING PRODUCTS MUST COMPLY WITH TVOC EMISSION LEVELS – TO ASTM D5116 TEST PROTOCOL	
Carpets using ASTM D5116 Test Protocol:	
Total VOC limit	0.5 mg/m ² per hour
4-pc (4-Phenolcycohexene) limit	0.05 mg/m ² per hour
Carpet using ISO 16000 test protocol (also known as EN 13419)	
TVOC at three days-	0.5 mg/sqm per hour
Flooring using ISO 10580 (also known as ISO/TC 219) – Document	
TVOC at 24 hours - 0	0.5mg/sqm per hour

9.2 FORMALDEHYDE MINIMISATION

All engineered wood products used internally, including exposed and concealed applications, must have low formaldehyde emissions as defined in the table below, or contain no formaldehyde. Engineered wood products are defined as particleboard, plywood, veneer, MDF, Laminated Veneer Lumber (LVL), High-Pressure Laminate (HPL), Compact Laminate and decorative overlaid wood panels and include both finished and unfinished products.

These requirements are not applicable to exterior applications, formwork, internal car park applications, reused engineered wood products or raw timber.

The contractor must obtain approval from the design team before substituting any product.

The limits listed here are defined according to the test method. The levels listed are equivalent results for different test procedures.

Table 13: Formaldehyde emission limits

TEST PROTOCOL	EMISSION LIMIT/ UNIT OF MEASUREMENTS
AS/NZS 2269:2004, testing procedure AS/NZS 2098.11:2005 method 10 for Plywood	< 1.0 mg/L
AS/NZS 1859.1:2004 - Particle Board, with use of testing procedure AS/NZS 4266.16:2004 method 16	< 1.5 mg/L





AS/NZS 1859.2:2004 - MDF, with use of testing procedure AS/NZS 4266.16:2004 method 16	< 1.0 mg/L
JIS A 5908:2003- Particle Board and Plywood, with use of testing procedure JIS A 1460	< 1.0 mg/L
JIS A 5905:2003 - MDF, with use of testing procedure JIS A 1460	< 1.0 mg/L
JIS A1901 (not applicable to Plywood)	< 1.0 mg/L
ASTM D5116	<0.1 (+/- 0.0005) mg/m ² hr (may also be represented as mg/m ² /hr)
ISO 16000 part 9, 10 and 11 (also known as EN 13419)	<0.1 (+/- 0.0005) mg/m ² hr (may also be represented as mg/m ² /hr)
ASTM D6007	0.12mg/m ³ *
ASTM E1333	0.12mg/m ³ **
EN 717-1 (also known as DIN EN 717-1)	0.12 mg/m ³
EN 717-2 (also known as DIN EN 717-2)	3.5 mg/m ² hr (may also be represented as mg/m ² /hr).
<p>*The test report must confirm that the conditions of Table 1 comply for the particular wood product type, the final results must be presented in EN 717-1 equivalent (as presented in the table) using the correlation ratio of 0.98.</p> <p>** The final results must be presented in EN 717-1 equivalent (as presented in the table), using the correlation ratio of 0.98.</p>	



10 CONCLUSION

In conclusion, the presented report provides a comprehensive overview of sustainability commitments that harmonize seamlessly with the core principles of the sustainability philosophy. These initiatives centre around the critical areas of energy conservation, water conservation, and waste reduction, showcasing a concerted effort to create a more environmentally responsible and resource-efficient approach. By prioritising these fundamental aspects, the report underscores a commitment to fostering a greener and more sustainable future.

Table 14: Sustainability Commitments

DESCRIPTION	GOAL	SUSTAINABILITY COMMITMENTS
CLEAR AIR	Improve indoor environment quality and health and wellbeing of occupants.	Outdoor air provided to primary areas at a rate at least 50% greater than minimum in AS 1668.2:2012. <i>(TBC based on mechanical consultant).</i>
LIGHT QUALITY		Above 40% of the regularly occupied areas have high level of daylight (above 160 Lux).
EXPOSURE TO TOXINS		The building's paints adhesives, sealants, and carpets are low in TVOC or non-toxic. The building's engineered wood products are low in TVOC or non-toxic. Occupants are not exposed to banned or highly toxic materials in the building.
NATURAL VENTILATION	Increase Fresh Air	Natural Ventilation to all dwellings.
HEAT RESILIENCE	Reduce impacts of long-term performance.	Light Roof Colour (SRI \geq 0.64)
ENERGY USE	Reduce emissions and water use.	All dwellings exceed the minimum NatHERS energy efficiency requirement by 0.5 stars, based on the NCC 2022.
		Provision for solar PV array to supply renewable energy to each dwelling.
WATER USE		High WELS Ratings (these equal to above 30% reduction in potable water). Water Sensitive Urban Design, drip irrigation. Stormwater collected, filtered, and used for sub-soil irrigation
LIGHTING USE		20% reduction in lighting power when compared to NCC DTS.
MOVEMENT AND PLACE	Low carbon options.	Provision for EV Charging Bays. Provision for Bike Storage.





Arboricultural Report - 37 Clark Street, Nedlands

4th April 2025
Total Tree Count: 16

Brief

This consultant has been commissioned by Welink Group Pty Ltd to inspect and submit a report in respect of 16 selected trees located in and surrounding the proposed new development at 37 Clark Street Nedlands.

The objective of the audit was to undertake an inspection of each tree, carry out an examination of each tree as delineated upon the map attached, to assess each trees health & structure, and itemise whether the tree is suitable to retain within the development. Provide details on remedial tree surgery works and to determine the Tree Protection Zone (TPZ) and Structural Root Zone (SRZ) for each tree.

The survey scope requires:

- The identification of tree species 1 - 16.
- Measurement of the height & canopy spread of the trees.
- Measurement of trunk diameters.
- Tree age.
- Current health and structural condition of the trees.
- Problems identified and itemised.
- Retention Value (Low, Medium, High).
- ULE (Useful Life Expectancy).
- Tree Protection Zone Radius (TPZ).
- Structural Root Zone Radius (SRZ).
- Comments on whether suitable to retain based on the proposed plans.
- Recommended remedial pruning or other work that may be required.

All trees are digitally mapped, and this consultant confirms that the site inspection was carried out on the 4th of April 2025 with a total of 16 trees audited.

This consultant was supplied with the document Plan P 2948.

Summary & Recommendations

The 16 trees inspected were found to be predominantly in good & fair health and good to poor structural condition at this time. A number of the trees within the site have been topped and lopped in previous years rendering the development of epicormic growth. Tree 11 – *Jacaranda mimosifolia* within the site was found to be the best tree within the site and is recommended to retain and protect throughout the demolition and construction phase.

The TPZ and SRZ radius details for each tree provided in the report are based upon AS 4970-2009 *Protection of trees on development sites* and provide a guide to assist with the care and protection of the trees.

The Tree Protection Zone (TPZ) is the calculated area above and below ground at a given distance from the trunk to provide for the protection of the tree's roots and canopy during construction works. The TPZ is calculated based on the tree's trunk diameter at breast height (DBH).

The Structural Root Zone (SRZ) is the area around the base of the tree that's required for structural stability.

The TPZ and SRZ for each tree are provided in the details on the following pages.

Tree Protection Zones



Structural Root Zones



Trees numbered 1-5 are on the verge of the Local Government and are owned, protected and managed by the City of Nedlands. Any works on these trees requires permission from the City of Nedlands.

Trees numbered 6 / 10 / 11 are marked for retention & potential design changes. The TPZ and SRZ for these trees are tabled below.

Tree No	Species	TPZ in meters	SRZ in meters	Comment
6	<i>Liquidambar styraciflua</i>	9	3.3	With the existing design it appears the new structure will be built well within the TPZ and SRZ of this tree. Unless the design is modified it is unlikely this tree will be able to be retained due to proximity and poor structural condition.
10	<i>Harpephyllum caffrum</i>	5.88	2.59	It appears that the new structure will fall outside of both the TPZ and SRZ of this tree however detailed measurements on site will be required to confirm this. Canopy lift & formative pruning will be required for this tree.
11	<i>Jacaranda mimosifolia</i>	6.48	2.53	This tree is considered the best within the site and is recommended to be retained and worked around. It appears that the new structure will fall outside of both the TPZ and SRZ of this tree however detailed measurements on site will be required to confirm this.

In relation to the various protection zones, the calculations are based on AS4970-2009 *Trees on development site*. However, tree root systems are opportunistic, unpredictable, variable, site specific and unique, not all tree root systems conform to these calculated areas.

If the proposed encroachment is less than 10% of the area of the TPZ, and is outside the SRZ, detailed root investigations should not be required.

If the proposed encroachment is greater than 10% of the area of the TPZ and inside the SRZ then detailed non-invasive root investigation is required before any work in the areas are undertaken.

If a detailed root investigation is not undertaken and roots are impacted by the works the trees may become structurally unstable and decline.

Machinery access to and movements within the site have the potential to cause significant root damage and canopy damage by the wounding and breaking of branches, the stripping of bark and the removal of foliage. Pruning requirements will have to be considered before works are undertaken.

Remedial works

1 tree is recommended for some remedial works which consist of the removal of major deadwood and the priority for this removal is low. Another tree is recommended for some canopy lift pruning and formative pruning to allow the tree to be retained. Pruning details and maps are found in this report.

Pruning is to be carried out by a qualified Arborist in a manner consistent with AS 4373-2007 *Pruning of Amenity Trees*.

Additional considerations with the development around the trees

It is recommended that all works are maintained as far away from the TPZ radius of the trees as possible to ensure damage is reduced. If work is to come inside the TPZ radius of any trees it is recommended to confirm the work with a qualified arborist from Paperbark Technologies prior to work being carried out. Selective trees may require further investigation to confirm how to proceed.

Excavation in proximity of trees to be retained is to be carried out by small, tracked machinery fitted with a flat bladed bucket with an Arborist Spotter wherever practicable.

The removal of live canopy and major limbs may significantly damage the trees. Therefore, work methods and equipment must be suitable to the scale of works and operating space to ensure accidental canopy and structural damage are prevented.

It is also recommended that all trees are protected by temporary fencing to delineate tree protection areas consistent with AS4970-2009 and that it is installed prior to works commencing. It is to be an area of tree and root protection with no digging and no storage of equipment or tools. No filling, trenching or other earthworks shall be carried out unless there is written arborist approval. Soil is not to be lowered or raised within this zone and no washing of tools or cement/paint or chemicals is to be deposited in this zone.

Where temporary fencing cannot be achieved due to the restricted amount of practicable space within the site during the development works, it is recommended to provide at least trunk/branch and ground protection to avoid mechanical injuries from small and large machinery.

Storage of tools and equipment and the storage of materials are recommended to be located within an open area, preferably not within 6m of trees to be retained.

The future management, maintenance and condition of the trees have a considerable bearing on their location, with safety to property and people the main priority. Therefore, each tree retained is recommended to be re-inspected on an annual basis to ensure that the trees remain healthy and in a structurally sound condition with a level of risk that is acceptable.



Gray Stead & Zana Sheary - Arboricultural Consultants

Diploma of Arboriculture, ISA Certified Arborist AU-0039A & Quantified Tree Risk Assessors Lic 1082 & 9550

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Limitation of liability

Trees can be managed, but they cannot be controlled. To live or work near a tree involves a degree of risk.

This report only covers identifiable defects present at the time of inspection. Paperbark Technologies accepts no responsibility and cannot be held liable for any structural defect or unforeseen event/situation or adverse weather conditions that may occur after the time of inspection. Paperbark Technologies cannot guarantee that the tree/s contained within this report will be structurally sound under all circumstances, and is not able to detect every condition that may possibly lead to the structural failure of a tree. Paperbark Technologies cannot guarantee that the recommendations made will categorically result in the tree being made safe.

Unless specifically mentioned this report will only be concerned with above ground inspections, as such all observations have been visually assessed from ground level. Trees are living organisms and as such cannot be classified as safe under any circumstances. Trees fail in ways that the arboriculture industry does not fully understand.

The recommendations are made on the basis of what can be reasonably identified at the time of inspection therefore Paperbark Technologies accepts no liability for any recommendations made. All care has been taken to obtain information from reliable sources, however Paperbark Technologies can neither guarantee nor be responsible for the accuracy of information provided by others. In the event that reinspection of the tree/s is recommended it is the client's responsibility to make arrangements with Paperbark Technologies.

Map of Trees at 37 Clark Street, Nedlands



Appendix - Tree Protection Fencing

(AS4970- 2009 *Protection of trees on development sites*)



LEGEND:

- 1 Chain wire mesh panels with shade cloth (if required) attached, held in place with concrete feet.
- 2 Alternative plywood or wooden paling fence panels. This fencing material also prevents building materials or soil entering the TPZ.
- 3 Mulch installation across surface of TPZ (at the discretion of the project arborist). No excavation, construction activity, grade changes, surface treatment or storage of materials of any kind is permitted within the TPZ.
- 4 Bracing is permissible within the TPZ. Installation of supports should avoid damaging roots.

White Mulberry Tree ID #1

Tree Details	
Latin Name:	Marus alba
Latitude:	-31.97932806546
Longitude:	115.81119108472
Tree Height (Estimated) [m]:	2.2
Canopy Spread [m]:	2
DBH [cm]:	11.87
Health:	Good
Structure:	Poor
Problems:	Lopped, Deadwood minor <50, Multi Crown leaders
Notes:	Not worthy of retention due to location
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	2
Structural Root Zone (SRZ) [m]:	2.25
Useful Life Expectancy:	11-20 years
Retention Value:	Low

[Photos](#) [Street View](#) [Map View](#)



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04/04/2025

Jacaranda Tree ID #2

Tree Details

Latin Name: Jacaranda mimosifolia

Latitude: -31.979577068219

Longitude: 115.81115830215

Tree Height
(Estimated) [m]: 8

Canopy Spread [m]: 3

DBH [cm]: 19

Health: Good

Structure: Fair

Problems: Deadwood minor <50,
Epicormic growth,
Leaning, Bark wound

Notes: Retain and protect verge tree

Priority: Not applicable

Tree Work: No Works

Observation
Comments:

Tree Protection Zone
(TPZ) [m]: 2.28

Structural Root Zone
(SRZ) [m]: 1.85

Useful Life Expectancy: 40+ years

Retention Value: High

Photos Street View Map View



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04/04/2025

Jacaranda Tree ID #3

Tree Details

Latin Name:	Jacaranda mimosifolia
Latitude:	-31.979660490278
Longitude:	115.81116575271
Tree Height (Estimated) [m]:	2.3
Canopy Spread [m]:	3
DBH [cm]:	5
Health:	Good
Structure:	Fair
Problems:	Codominant Stem
Notes:	Retain and protect verge tree
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	2
Structural Root Zone (SRZ) [m]:	1.08
Useful Life Expectancy:	40+ years
Retention Value:	High

Photos Street View Map View



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04/04/2025

Queensland Box Tree ID #4

Tree Details

Latin Name:	Lophostemon confertus
Latitude:	-31.97976919165
Longitude:	115.81129986324
Tree Height (Estimated) [m]:	9
Canopy Spread [m]:	10
DBH [cm]:	40
Health:	Good
Structure:	Fair
Problems:	Deadwood minor <50, Multi Crown leaders
Notes:	Retain and protect verge tree
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	4.8
Structural Root Zone (SRZ) [m]:	2.37
Useful Life Expectancy:	20-40 years
Retention Value:	High

Photos Street View Map View



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04/04/2025

Queensland Box Tree ID #5

Tree Details

Latin Name:	Lophostemon confertus
Latitude:	-31.979750232162
Longitude:	115.81138330967
Tree Height (Estimated) [m]:	10
Canopy Spread [m]:	8
DBH [cm]:	42
Health:	Good
Structure:	Fair
Problems:	Lopped, Deadwood minor <50, Multi Crown leaders
Notes:	Retain and protect verge tree
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	5.04
Structural Root Zone (SRZ) [m]:	2.43
Useful Life Expectancy:	20-40 years
Retention Value:	High

Photos Street View Map View



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04/04/2025

Liquidambar Tree ID #6

Tree Details

Latin Name:	Liquidambar styraciflua
Latitude:	-31.979654170441
Longitude:	115.81144291434
Tree Height (Estimated) [m]:	12
Canopy Spread [m]:	12
DBH [cm]:	75
Health:	Good
Structure:	Poor
Problems:	Lopped, Deadwood minor <50, Epicormic growth, Decayed pruning stub
Notes:	Not worthy of retention due to poor structural condition & proximity to building footprint.
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	9
Structural Root Zone (SRZ) [m]:	3.3
Useful Life Expectancy:	20-40 years
Retention Value:	Medium

Photos Street View Map View



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Weeping Bottlebrush Tree ID #7

Tree Details

Latin Name:	Callistemon viminalis
Latitude:	-31.979675657898
Longitude:	115.81121492657
Tree Height (Estimated) [m]:	2.5
Canopy Spread [m]:	3
DBH [cm]:	17.38
Health:	Fair
Structure:	Good
Problems:	Deadwood minor <50, Multi Crown leaders
Notes:	Not worthy of retention due to location
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	2.09
Structural Root Zone (SRZ) [m]:	1.85
Useful Life Expectancy:	11-20 years
Retention Value:	Medium

Photos Street View Map View

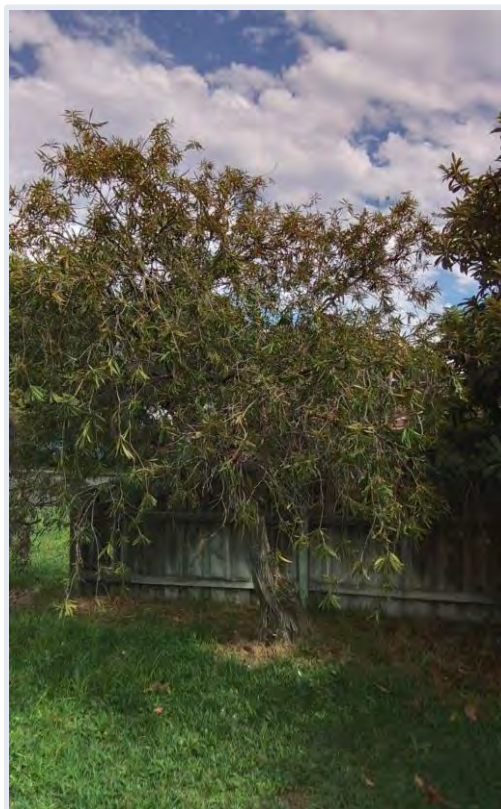


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kings park special Tree ID #8

Tree Details	
Latin Name:	Callistemon Kings Park Special
Latitude:	-31.979645322655
Longitude:	115.81121939689
Tree Height (Estimated) [m]:	6
Canopy Spread [m]:	3.5
DBH [cm]:	14
Health:	Fair
Structure:	Fair
Problems:	Multi Crown leaders
Notes:	Not worthy of retention due to location
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	2
Structural Root Zone (SRZ) [m]:	2.15
Useful Life Expectancy:	11-20 years
Retention Value:	Medium

Photos Street View Map View



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Loquat Tree ID #9

Tree Details

Latin Name: Eriobotrya japonica

Latitude: -31.979602347653

Longitude: 115.81120747601

Tree Height
(Estimated) [m]: 7

Canopy Spread [m]: 4

DBH [cm]: 18

Health: Good

Structure: Fair

Problems: Codominant Stem,
Leaning

Notes: Not worthy of retention due to readily
replaceable.

Priority: Not applicable

Tree Work: No Works

Observation
Comments:

Tree Protection Zone
(TPZ) [m]: 2.16

Structural Root Zone
(SRZ) [m]: 1.61

Useful Life Expectancy: 20-40 years

Retention Value: Medium

Photos Street View Map View



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Kaffir Plum Tree ID #10

Tree Details

Latin Name:	Harpephyllum caffrum
Latitude:	-31.979560636572
Longitude:	115.81121492657
Tree Height (Estimated) [m]:	10
Canopy Spread [m]:	11
DBH [cm]:	49
Health:	Good
Structure:	Good
Problems:	Codominant Stem, Deadwood minor <50
Notes:	Suitable to retain, will require canopy lift pruning and minor formative
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	5.88
Structural Root Zone (SRZ) [m]:	2.59
Useful Life Expectancy:	20-40 years
Retention Value:	Medium

Photos Street View Map View



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04/04/2025

Jacaranda Tree ID #11

Tree Details	
Latin Name:	Jacaranda mimosifolia
Latitude:	-31.979387472271
Longitude:	115.81122237713
Tree Height (Estimated) [m]:	13.5
Canopy Spread [m]:	10
DBH [cm]:	54
Health:	Good
Structure:	Good
Problems:	Deadwood major >50, Epicormic growth, Multi Crown leaders
Notes:	Suitable to retain and alter design to accommodate tree.
Priority:	Low
Tree Work:	Remove major deadwood
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	6.48
Structural Root Zone (SRZ) [m]:	2.53
Useful Life Expectancy:	40+ years
Retention Value:	Very high

[Photos](#) [Street View](#) [Map View](#)



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04/04/2025

Southern Mahogany Tree ID #12

Tree Details

Latin Name:	Eucalyptus botryoides
Latitude:	-31.979341969191
Longitude:	115.81126857075
Tree Height (Estimated) [m]:	13
Canopy Spread [m]:	3
DBH [cm]:	10
Health:	Good
Structure:	Fair
Problems:	Deadwood minor <50, Multi Crown leaders
Notes:	Not worthy of retention due to sunlight suppression
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	2
Structural Root Zone (SRZ) [m]:	1.31
Useful Life Expectancy:	20-40 years
Retention Value:	Medium

Photos Street View Map View



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04/04/2025

Chinese Elm Tree ID #13

Tree Details

Latin Name:	Ulmus parvifolia
Latitude:	-31.979450670941
Longitude:	115.81143099337
Tree Height (Estimated) [m]:	9
Canopy Spread [m]:	7
DBH [cm]:	28.71
Health:	Good
Structure:	Fair
Problems:	Multi Crown leaders
Notes:	Not worthy of retention due to form & potential root encroachment
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	3.45
Structural Root Zone (SRZ) [m]:	2.25
Useful Life Expectancy:	11-20 years
Retention Value:	Medium

Photos Street View Map View

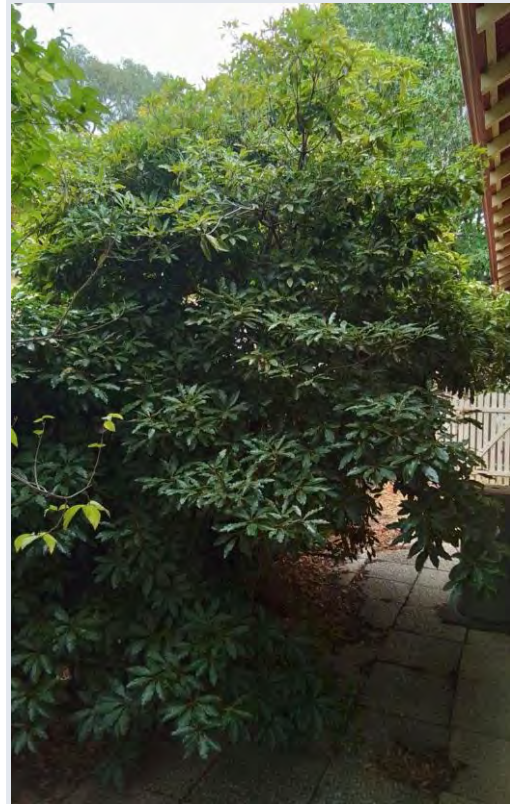


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Sweet Pittosporum Tree ID #14

Tree Details	
Latin Name:	Pittosporum undulatum
Latitude:	-31.979512605625
Longitude:	115.81142652313
Tree Height (Estimated) [m]:	4
Canopy Spread [m]:	4
DBH [cm]:	10
Health:	Good
Structure:	Fair
Problems:	Multi Crown leaders
Notes:	Not worthy of retention as readily replaced.
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	2
Structural Root Zone (SRZ) [m]:	1.4
Useful Life Expectancy:	11-20 years
Retention Value:	Medium

[Photos](#) [Street View](#) [Map View](#)

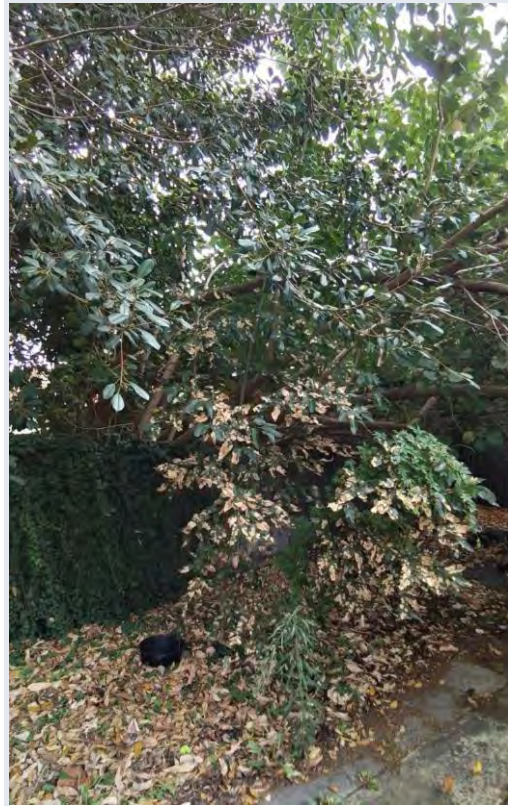


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Southern Mahogany Tree ID #15

Tree Details	
Latin Name:	Eucalyptus botryoides
Latitude:	-31.979339441234
Longitude:	115.81130135332
Tree Height (Estimated) [m]:	7
Canopy Spread [m]:	1
DBH [cm]:	4
Health:	Good
Structure:	Good
Problems:	
Notes:	Not worthy of retention due to form and location within the site.
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	2
Structural Root Zone (SRZ) [m]:	0.94
Useful Life Expectancy:	20-40 years
Retention Value:	Medium

[Photos](#) [Street View](#) [Map View](#)



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04/04/2025

Loquat Tree ID #16

Tree Details	
Latin Name:	Eriobotrya japonica
Latitude:	-31.979566956415
Longitude:	115.81123578818
Tree Height (Estimated) [m]:	0
Canopy Spread [m]:	4
DBH [cm]:	10
Health:	Good
Structure:	Good
Problems:	
Notes:	Not worthy of retention due to location
Priority:	Not applicable
Tree Work:	No Works
Observation Comments:	
Tree Protection Zone (TPZ) [m]:	2
Structural Root Zone (SRZ) [m]:	1.36
Useful Life Expectancy:	20-40 years
Retention Value:	Medium

[Photos](#) [Street View](#) [Map View](#)



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04/04/2025

**Part B – Item No. 3.2 - Lot 381 (No. 6) ALEXANDER ROAD,
DALKEITH – FIVE MULTIPLE DWELLING DEVELOPMENT**

Form 1 – Responsible Authority Report
(Regulation 12)

DAP Name:	Metro Inner Development Assessment Panel
Local Government Area:	City of Nedlands
Applicant:	Lateral Planning
Owner:	Universal United Investments Pty Ltd
Value of Development:	\$6 million
Responsible Authority:	City of Nedlands
Authorising Officer:	Bruce Thompson, Director Planning & Development
LG Reference:	DA24-100314
DAP File No:	DAP/24/02820
Application Received Date:	10 December 2024
Report Due Date:	17 April 2025
Application Statutory Process Timeframe:	90 Days (with an additional 43 days agreed)
Attachment(s):	<ol style="list-style-type: none"> 1. Location Plan 2. Amended Development Plans received 26 March 2025 3. Schedule of Submissions 4. Design Review Panel Minutes 5. R-Code Volume 2 Assessment 6. Applicants Planning Report received 10 December 2024 7. Amended Landscaping Plan dated 27 March 2025 8. Amended Waste Management Plan dated 24 March 2025 9. Amended Traffic Note dated 25 March 2025 10. Sustainability Strategy received 10 December 2024

Responsible Authority Recommendation

It is recommended that the Metro Inner-North Joint Development Assessment Panel resolves to:

Refuse DAP Application reference DAP/24/02820 and accompanying plans dated 26 March 2025 in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of City of Nedlands Local Planning Scheme No. 3, for the following reasons:

Reasons

1. The proposed development does not comply with Clause 67(2)(m) of Schedule 2 of the *Planning and Development (Local Planning Schemes) Regulations 2015* as the development is not considered to be compatible with its setting, including the existing and future streetscape character and relationship of the development with adjoining properties and properties within the locality.
2. The proposed development does not meet the element objective of O2.2.1 and O2.2.4 of the Residential Design Codes Volume 2 in that the overall building height of the development does not respond to the desired future scale and character, including existing buildings on Alexander Road. Further, the building height does not recognise the need for daylight and solar access to the adjoining southern property.
3. The proposed development does not meet the element objective of O2.4.4 of the Residential Design Codes Volume 2 in that the side setbacks do not provide for an appropriate transition to the adjoining properties which contain a lower intensity of development.
4. The proposed development does not meet the element objective of O2.5.1 of the Residential Design Codes Volume 2 in that the overall bulk and scale of the development is not consistent with the existing or planned character of the immediate locality and area.
5. The proposed development does not meet the element objective of O3.2.2 of the Residential Design Codes Volume 2 in that the development will have significant impact on the amenity of the adjoining southern property by restricting access to northern sunlight to habitable spaces.
6. The proposed development does not meet the element objective of O3.9.2 of the Residential Design Codes Volume 2 in that insufficient car parking has been provided for the total number of proposed residents within the dwellings.

Reasons for Responsible Authority Recommendation

The proposed development is not considered to be compatible with its surrounding immediate locality including the existing streetscape and the future streetscape of Alexander Road. As stated, the immediate locality consists of one or two storey dwellings and given recent construction dates, this is unlikely to change in the foreseeable future. The proposed development being four storeys in height will dominate the streetscape and adjoining and adjacent properties. The building height, plot ratio, side setbacks and lack of onsite parking are combined to severely negatively impact amenity of adjoining and adjacent properties and the immediate locality. Particularly, the southern adjoining property at 8A Alexander Road will be the most effected, losing all-natural northern sunlight to their two-storey dwelling. The proposed development is not only not consistent with the immediate locality but also seeks to exceed the building bulk which is expected of an R80 density code and provides no transition to the adjoining properties featuring lower intensity of development. For these reasons the application is recommended for refusal.

Details: outline of development application

Region Scheme	Metropolitan Region Scheme
Region Scheme - Zone/Reserve	Urban
Local Planning Scheme	Local Planning Scheme No. 3
Local Planning Scheme - Zone	Residential (R80)
Structure Plan/Precinct Plan	N/A
Use Class and permissibility:	'P' Permitted
Lot Size:	1012m ²
Existing Land Use:	Residential – Single House
State Heritage Register	No
Local Heritage	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Heritage List <input type="checkbox"/> Heritage Area
Design Review	<input type="checkbox"/> N/A <input checked="" type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other
Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

Proposed Land Use	Residential (Multiple Dwellings)
Proposed Plot Ratio	1191.7m ² / 1.18
Proposed No. Storeys	Four
Proposed No. Dwellings	Five

The application seeks approval for a four-storey residential development comprising five multiple dwellings at 6 Alexander Road, Dalkeith.

Assessment of the proposal is based on:

- Amended Development Plans received 26 March 2025 (**Attachment 2**)
- Applicants Planning Report received 10 December 2024 (**Attachment 5**)
- Amended Landscaping Plan dated 27 March 2025 (**Attachment 6**)
- Amended Waste Management Plan dated 24 March 2025 (**Attachment 7**)
- Amended Traffic Note dated 25 March 2025 (**Attachment 8**)

Background:Site Context

The development is located at 6 Alexander Road, Dalkeith. The lot has an area of 1,012m² and is located on the street block bound by Philip Road to the north and Waratah Avenue to the south (**Attachment 1**). The lot is regular in shape with a 20m frontage to Alexander Road. The lot has a 0.5m-0.6m slope down from the north to the

south. The site currently contains an existing single storey dwelling. The site is zoned Residential with a density code of R80.

Changes to Zoning and Coding of the locality

The locality surrounding the subject site is defined as the stretch of Alexander Road bound by Philip Road to the north and Waratah Avenue to the south. This distinction of the locality was supported under WASAT 41 (Previous SAT Appeal).

The Residential zoned properties within the immediate locality of the site had previously been up-coded from R10 to R20 (on the eastern side of the road) and R10 to R25 (on the western side of the road) under an amendment (No. 148) to the former Town Planning Scheme No. 2 (TPS2) on 15 February 2005 in order to enable higher low density residential development in place of the original very low density residential development (see Figure 1 below).



Figure 1: Zoning and coding details following Amendment No. 148 of TPS 2.

The above-mentioned properties were further up-coded upon the gazettal of Local Planning Scheme No. 3 (LPS3) on 16 April 2019. The eastern side of Alexander Road was up-coded from R20 to R80, and the western side of Alexander Road was up-coded from R25 to R60 to enable medium-rise, medium density residential development.

The abovementioned properties underwent further alterations to their coding under a scheme amendment (No. 8) to Local Planning Scheme No. 3. Scheme amendment No. 8 originally sought to down code the western side of Alexander Road from R60 to R35 and the eastern side of Alexander Road from R80 to R35. This Scheme amendment was created as a response to the community’s vision for the locality as a low-to-medium density residential area in proximity to Waratah Avenue.

Upon gazettal of scheme amendment No. 8 on 4 February 2022, the western side of Alexander Road was down coded from R60 to R40. However, the Minister’s decision was to retain the existing R80 coding along the eastern side of Alexander Road (subject site).

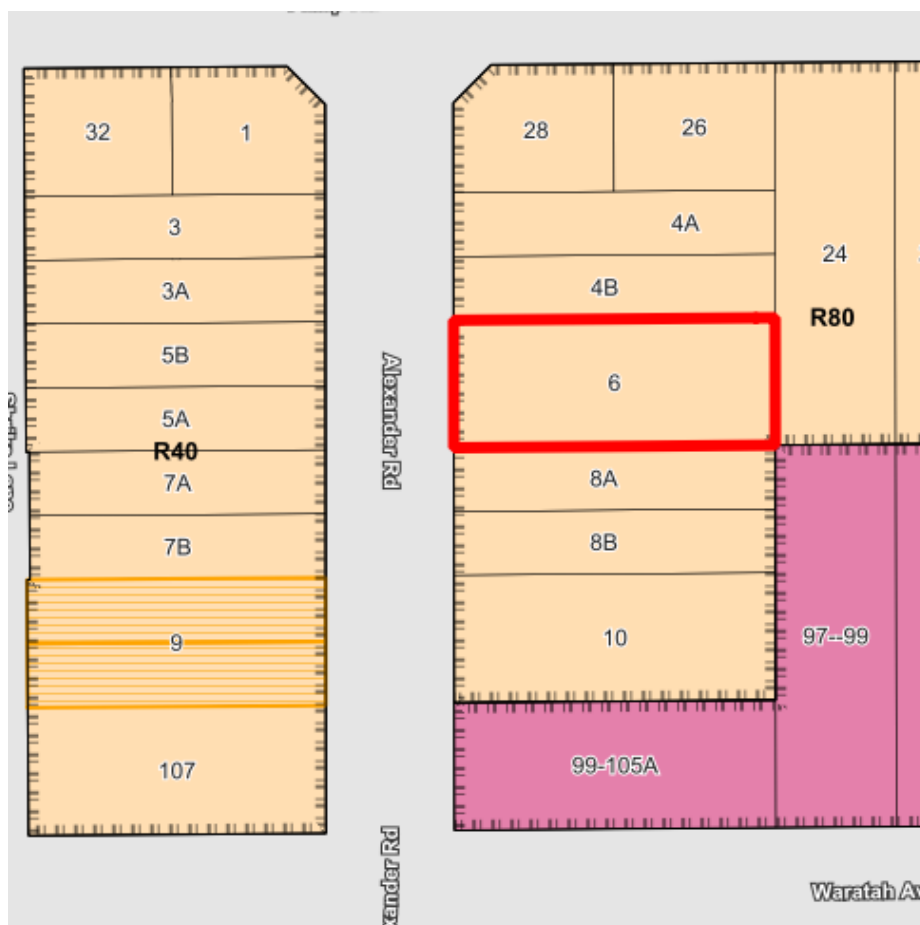


Figure 2 – Current zoning along Alexander Street, Dalkeith

Streetscape Character

Alexander Road is a short street bound to the north by Philip Road and the south by Waratah Avenue. There are 17 properties that have direct street frontage onto Alexander Road. The eastern side of Alexander Road is coded R80, with the exception of the southern property at 99-105A Waratah Avenue which is coded R-AC3. Whilst the western side of the street is coded R40.

Of the 17 properties within the street, eight properties consist of newly constructed double storey single dwellings constructed between 2008 and 2015. Of these eight

properties, five are located on the eastern side of the road (R80) and three are located on the western side of the road (R40).



Figure 3 – New development along Alexander Street

The site at 9 Alexander Road is currently vacant and has recently been subdivided into two side by side lots similar to the remainder of the newly constructed single dwellings along the street. The site has received development approval and building permits to construct two double storey single dwellings.

The site at 5A and 5B Alexander Road has development approval for a two-storey building comprising four multiple dwellings.

The site at 3 Alexander Road has received development approval for two grouped dwellings. This configuration is a front and back design, with the development consisting of two double storey single dwellings.

Of the 16 other properties on the street, only three consist of older dwellings that are likely candidates for development potential (two on the eastern side and one on the western side).

The site is surrounded by two storey single houses on three properties to the north (28 Philip, 4A and 4B Alexander Road), two to the south (8A and 8B Alexander Road) and one to the east (24 Philip Road).

It is assumed that the existing buildings and future developments (approved at 9A and 9B, 5A and 5B and 3 Alexander Road) are unlikely to change soon given the recent nature of construction or intention of construction.

Previous applications and WASAT 41

On the 15th October 2019 a development application (DA19-40779) was lodged with the City for a five storey (above basement garage), five multiple dwelling development on the subject site at 6 Alexander Road. Following public consultation, amended plans were submitted by the applicant which reduced the proposal to a four storey (above basement garage), four multiple dwelling development. The City resolved to refuse the development at the Ordinary Council Meeting on 28 April 2020. The application was refused based on its bulk and scale, overshadowing of the southern property, and inconsistency with Clause 67 (m), (n) of the *Planning and Development (Local Planning Schemes) Regulations 2015* and inconsistency with the aims of the City of Nedlands Local Planning Scheme No. 3 (a).

Following the above determination, the proponent sought review by the State Administrative Tribunal (SAT). Through much deliberation, SAT dismissed the application for review on 17 March 2021 because the proposal did not achieve the Element Objectives of 2.2 Building height, 2.5 Plot ratio and 3.2 Orientation. Further, the Tribunal deemed the proposal was not compatible with its streetscape setting and would have an adverse impact on the amenity of the adjoining southern property in relation to overshadowing and bulk and scale. The proposal was deemed to be *'one storey too high in the western half of the site and the ground floor finished floor level is 0.7 metre above existing ground level'*.

On 1 July 2021 a development application was lodged with the City for a three storey, four grouped dwelling development. This development was approved under delegation on 15 October 2021.

Local Planning Policy 5.8: Alexander and Philip Road Height Limit and Local Planning Policy 5.14: Precincts

In response to the refusal of DA19-40779 and WASAT 41, the City resolved to initiate a Local Planning Policy (LPP 5.8) to establish a 10m height limit on Alexander and Philip Road and initiate a Scheme Amendment (No. 8) (discussed above) to reduce the coding of Alexander Road from R60 and R80 to R35.

At the Ordinary Council Meeting on 25 July 2020, Council adopted LPP 5.8, against officer recommendation. The Policy replaced the Acceptable Outcomes of AO 2.2.1 Volume 2 only to apply a maximum building height of 10m above natural ground level to impacted properties (including subject site).

The height limits in LPP 5.8 were reflective of the intended Scheme Amendment No. 8 (re-coding the immediate locality to R35). However, as discussed, Scheme Amendment No. 8 was not adopted as per the original proposal, with the eastern side of Alexander Road retaining the R80 code (4 storey building height as per AO 2.2.1, Table 2.1). Therefore LPP 5.8 was not reflective of the existing coding and was imposed built form controls that were not consistent with the principles of orderly and proper planning.

Local Planning Policy 5.14 Precincts was adopted at the Ordinary Council Meeting on 27 August 2024. The creation of LPP 5.14 was based on extensive built form modelling and precinct planning that was undertaken in the Waratah Precinct. The Policy states that for multiple dwellings within the R80 code along Alexander Road, a building height of 3 storeys replaces the Acceptable Outcomes of AO 2.2.1 of Volume 2 in relation to building height. Further, the Policy allows an additional storey only where it can demonstrate that the proposed development allows two hours per day of solar access on 21 June for existing solar panels and primary garden areas on neighbouring properties (as per footnote 1).

The adoption of LPP 5.14 at the 27th August 2024 Ordinary Council Meeting also included the revocation of LPP 5.8. The reasoning included in the report to Council outlined that building height issues were now to be addressed by LPP 5.14. Particularly, that *“multiple dwellings in R80 coded areas along Alexander Road [are] limited to 3 storeys unless they can demonstrate suitable protection of adjoining solar panels and outdoor living areas at mid-winter”*. Further, *“a second planning policy covering the same area is unnecessary and would be a cause of confusion”*.

In response to the subject development application and significant community concern, at the time of writing, there is a Notice of Motion by Council to re-instate LPP 5.8 to consider a maximum building height of 10m over the R80 coded lots along Alexander Road (Alexander Road Sub-Precinct) as the Acceptable Outcome. The City has prepared a report that is scheduled to be presented at the 22nd April Ordinary Council Meeting to present Council with options and recommending that LPP 5.14 be amended to include the 10m height along Alexander Road for R80 properties, in lieu of the re-instatement of LPP 5.8.

As the re-instatement of LPP 5.8 or the amendment of LPP 5.14 has not yet been adopted by Council to advertise, it is neither certain nor imminent nor should not be given any significant weight in the assessment of this development application.

Legislation and Policy:

Legislation

- *Planning and Development Act 2005*
- *Planning and Development (Local Planning Schemes) Regulations 2015* (LPS Regulations)
- *Planning and Development (Development Assessment Panels) Regulations 2011*
- *Metropolitan Region Scheme*
- City of Nedlands Local Planning Scheme No.3 (LPS 3)

State Government Policies

- State Planning Policy 7.0 – Design of the Built Environment (SPP 7.0)

Planning Codes

- Residential Design Codes Volume 2 (R-Codes Vol. 2)

Local Policies

- Local Planning Policy 1.1 Residential Development (LPP 1.1)

- Local Planning Policy 1.3 Sustainable Design (LPP 1.3)
- Local Planning Policy 5.14 Precincts (LPP 5.14)
- Local Planning Policy 7.2 Design Review Panel (LPP 7.2)
- Local Planning Policy 7.3 Consultation of Planning Proposals (LPP 7.3)

Strategies

- City of Nedlands Local Planning Strategy

Consultation:

Public Consultation

In accordance with the City's Local Planning Policy 7.3 Consultation of Planning Proposals (LPP 7.3), the original plans received 10 December 2024 were advertised for a period of 28 days, from 15 January 2025 to 12 February 2025 in the following manner:

- Letters posted to all landowners and occupiers within a 200m radius of the site;
- A sign on site was installed at the site's street frontage for the duration of the advertising period;
- An advertisement was published on the City's website with all documents relevant to the application made available for viewing during the advertising period;
- Notice placed in the "Post" local newspaper;
- Posted on the City's social media platforms;
- Notice on the Noticeboard at the City's Administration Office; and
- Community information session held on 29 January 2025

At the close of the first advertising period, the City received 33 submissions, 31 objecting to the proposal, one supporting and one with comments only. Further the City received three late submissions.

A summary of the submissions is provided in **Table 1** below.

Issue Raised	Officer comments
Bulk and scale	Upheld. Bulk and scale are a reason for the recommendation for refusal.
Streetscape context and character	Upheld. Streetscape context and character is a reason for the recommendation for refusal.
Building height	Upheld. Building height is a reason for the recommendation for refusal.
Lack of parking on site	Upheld. Parking is a reason for the recommendation for refusal.
Increased traffic	An amended Traffic Technical Note was provided on 25 March 2025 and forms part of the attachments. The City Transport and Development team believes that the trip generation rate is low given the amount of parking bays provided. Should DAP choose to approve this application, a condition of approval will be recommended to require an amended Traffic Note incorporating the above comments.

Overshadowing and solar access	Upheld. Overshadowing and solar access to the southern property is a reason for the recommendation for refusal.
Noise concerns	Should DAP choose to approve this application, a condition of approval will be recommended by the City requiring an Acoustic Report be provided prior to the issue of a building permit.
Waste management concerns	<p>The City's Waste Management Guidelines for multi-dwelling developments are based on the waste generation rates of residential dwellings. This approach ensures sufficient capacity is allocated for all waste streams. Furthermore, the waste generation rates recommended by the Western Australian Local Government Association (WALGA) are lower than those prescribed by the City of Nedlands. Notably, the Applicant's Waste Management Plan (WMP) (as amended) proposes a waste capacity of 240 litres per dwelling—double the City's recommended 120 litres per dwelling. This equates to a total provision of five 240L general waste bins, as opposed to the three 240L bins required under the City's guidelines. The FOGO and Recycling bin allocation is in line with the City's guidelines.</p> <p>The City's Waste Management Department have recommended that an Amended Waste Management Plan be provided prior to the issue of a building permit to resolve minor discrepancies within the report.</p>
Number of bedrooms and concerns with proposed use of building	<p>The City cannot control the number of bedrooms proposed as part of an application.</p> <p>Should the DAP choose to grant development approval for this proposal. It will be approved as 'Five Multiple Dwellings'. A 'dwelling' is defined in the R-Codes Volume 2 as:</p> <p><i>"a building or portion of a building being used, adapted, or designed or intended to be used for the purpose of human habitation on a permanent basis by a single person, a single family, or no more than six persons who do not comprise a single family."</i></p> <p>As such, this development would not be obtaining approval for student accommodation nor short term accommodation but instead for 'Five Multiple Dwellings'.</p> <p>Should the future use of the building seek to change, further applications and approvals are required.</p>
Privacy concerns	The City deems that the visual privacy as proposed by the development achieves the Element Objectives of

	O3.5.1. Refer to <i>Visual Privacy</i> section of the RAR below.
Revocation of LPP 5.8	LPP 5.8: Alexander and Philip Road Height Limit was revoked by Council at the Ordinary Council Meeting on 27 August 2024. The revocation of LPP 5.8 was completed in accordance with The <i>Planning and Development (Local Planning Schemes) Regulations 2015 Schedule 2 (Deemed Provisions) Part 2 Clause 6 (a)</i> . LPP 5.8 was expressly revoked by a subsequent local planning policy (LPP 5.14: Precincts) prepared in accordance with Clause 4 of the Deemed Provisions. This is detailed by the report and resolution of Council.
Increase to population density	The number of dwellings permitted on any given lot is determined via the coding of the property. Coding of residential land is determined by the local planning framework and informed by state planning objectives.

Amended plans for the proposal were submitted to the City on 26 March 2025 which included the following changes:

- Reduction in floor-to-floor height by 0.2m, resulting in an overall reduction in height of 0.8m.
- Repositioning of fourth floor dwelling (Unit 301) from the front (west) to the back (east) of the building and relocating communal amenities from the back (east) to the front (west).
- Increase to street setbacks on 2nd, 3rd and 4th floor.
- Plot ratio reduced from 1:25 to 1:18 (reduced by 81m²)
- Articulation provided to the southern boundary setbacks.
- Removal of pedestrian canopy on ground floor
- Reconfiguration of lift and stair core location and slight amendments to parking configuration (no changes to number of bays)
- Relocation of bin store
- Reconfiguration of parts of the ground floor
- Reduction in total number of bedrooms from 23 to 21 (Unit 101 and 201)
- Increase to outdoor living areas of Unit 101 and 201
- Addition of operable screening to western balconies of Units 101 and 202
- Updates to the overshadowing diagrams
- Updates to the Waste Management Plan
- Updates to the Traffic Technical Note
- Updates to the Landscape Design Report

The amended plans were advertised for a period of 14 days, from 31 March 2025 to 14 April 2025. The amended plans and additional information were distributed to submitters (33) via email. The email comprised of a list of all amendments, along with a link to the City's Your Voice page to view the amended material. At the close of the second advertising period, the City received responses from 20 submitters advising their previous concerns remain and further comments provided. All submissions can be found at **Attachment 3**.

All submissions on this proposal have been given due regard in this assessment in accordance with Clause 67(y) of the *Planning and Development (Local Planning Schemes Regulations) 2015*.

Referrals/consultation with Government/Service Agencies

The application did not require referral to any Government or service agencies.

Design Review Panel Advice

The development was reviewed by the City's Design Review Panel on two occasions, with a final review by the Chair of the DRP, based on 26 March 2025 plans. Full DRP Minutes are provided at **Attachment 4**. A summary of the Panel and Chair's evaluation of the proposal at each stage of the review process is provided in **Table 2** below.

Table 2: DRP Design Quality Evaluation			
	Supported		
	Further Information Required		
	Not supported		
SPP 7.0 Principles	DRP 1 7 October 2024	DRP 2 4 November 2024	Chair Review 11 April 2025
1. Context and Character			
2. Landscape Quality			
3. Built Form and Scale			
4. Functionality and Built Quality			
5. Sustainability			
6. Amenity			
7. Legibility			
8. Safety			
9. Community			
10. Aesthetics			

The final comments from the DRP Chair were as follows:

- *Greatly improved the front fence treatment*
- *Selected and located appropriate planting species*
- *Articulated the north and south elevations to reduce their bulk and scale*
- *Set back the top floor to the east and the north and reduced floor to floor heights to reduce bulk on the street and overshadowing to the south*
- *Reduced the plot ratio significantly*
- *Created a duplex massing on the street to relate to the narrow grain of neighbouring properties*
- *Resolved visual privacy issues*
- *Greatly improved room and outdoor living functionality and efficiency*
- *Presented modelling to indicate that southern boundary overshadowing meets the requirements of LPP 5.14 in respect of achieving a fourth storey*

Planning Assessment:

The proposal has been assessed against all relevant legislative requirements of LPS3, State and Local Planning Policies and Planning Codes as outlined above. The matters

below have been identified as key considerations for the determination of this application.

Draft Local Planning Policy 3.4 Tree Retention

The City is currently forming Local Planning Policy 3.4 Tree Retention which provides a planning mechanism for the protection of regulated trees on Residential lots coded R25 to R80 within the City. The draft Policy underwent public consultation between 31 January 2025 and 21 February 2025. As the Policy has been advertised it is considered to be a 'seriously entertained' document. As such, the Policy should be given some weight in this assessment.

The proposal is seeking to remove two trees, one unconfirmed species and one *Shinus Terebithifolia* (Broad-leaf Pepper Tree) and retain the *Arbutus Unedo* (Strawberry Tree) in the north-eastern corner of the site. It is unclear from the information provided if these trees would achieve the definition of 'regulated trees' as the site feature survey does not provide height, canopy cover or trunk circumference information.

As such, should DAP choose to approve this development, conditions of approval have been recommended requiring an Arborist Report be provided prior to the issue of a building permit to gain further information on the trees in question.

Local Planning Policy 5.9 Primary Controls for Apartment Developments

Local Planning Policy 5.9 was adopted by the City of Nedlands on 23 August 2022. The policy was prepared to provide guidance to decision makers when considering applications for multiple dwellings assessed against the R-Codes Volume 2.

Policy provision 4.3 states that:

“to respond to the existing and desired future scale, height and character of Nedlands, and to therefore achieve the Element Objectives, a development must meet the default development standards and values provided in Tables 2.1, 2.2 and 2.7 of R-Codes Vol.2 in regards to the Primary Control elements.

Accordingly, the City deems that not meeting, and specifically in the case of height and plot ratio, exceeding these standards and values is not in keeping with the existing and desired future scale, height and character of Nedlands as designated by the relevant LPS3 R-code zoning, and will therefore not achieve the Element Objectives for these critical design elements.”

The proposed development does not meet the values and standards as specified in the R-Codes (as augmented by LPP 5.14) relating to plot ratio. As such, in accordance with policy provision 4.3 and for the reasons discussed above, the development does not achieve the Element Objectives. Additionally, the development does not satisfy the objectives of the policy as the height, bulk and scale and massing of the built form adversely impacts the locality and is not in keeping with the existing or desired future scale of the adjacent lots.

Local Planning Policy 5.14 Precincts

Local Planning Policy 5.14 Precincts (LPP 5.14) applies to residential zoned land coded R40 to R80 within the Waratah Precinct. LPP 5.14 has been developed ensure

new development enhances streetscapes, respectfully responds to the context and character of the area, promotes sustainable building design and effectively manages transition between high and medium densities.

LPP 5.14 replaces or augments several deemed-to-comply provisions of the R-Codes Vol. 1, and these are addressed below where applicable. The policy also includes the objective to 'define the desired future character of the Precincts in context of their zoning and density code' and provides a desired future character statement for the Waratah precinct. The proposed development does not satisfy the desired future character statement due to the following reasons:

- The development does not adhere to the desired future character statement of Alexander Road "*new development in the Alexander Road sub-precinct shall acknowledge the existing pattern of narrow lot development and provide consistency of development along the streetscape.*". The proposed development is entirely out of character when compared to the existing streetscape. The development does not respect the existing consistency of development along Alexander Road.
- The street block along Alexander Road is a mix of R40 and R80. The existing streetscape consists of a maximum two-storey level of development. This is considered to be a low intensity of development. The proposal does not provide the appropriate built form transitions between adjoining dwellings with lower intensity developments. The proposal building height, side setbacks and plot ratio significantly impact the perception of building bulk as observed from neighbouring properties and the public realm.
- The development contributes to the dwelling stock available in the locality via the addition of five new multiple dwellings, featuring a range of 3 to 7 bedrooms apartments.

State Planning Policy 7.3 – Residential Design Codes Volume 2 – Apartments

The proposal has been assessed against all relevant Design Elements of the Residential Design Codes Volume 2 – Apartments (R-Codes) which provides a comprehensive basis for the control of residential development. A full R-Code assessment is provided in **Attachment 5**.

The below elements of the proposal are not considered to achieve the Element Objectives. It is for these reasons refusal is recommended.

Element 2.2 Building Height

The Acceptable Outcome for building height is set out in the City's Local Planning Policy 5.14: Precincts (LPP 5.14). As specified in the policy, the primary height control for multiple dwellings is a maximum of 3 storeys with an additional storey being considered where it is demonstrated that the proposal allows for 2 hours per day solar access on 21 June for existing solar panels and primary garden areas on neighbouring properties. This is consistent with the default height as provided in the R-Codes for the R80 code whilst providing additional requirements with the intent of reducing amenity impacts to neighbouring properties. The indicative heights as set out in Table 2.2 of the R-Codes is 12m for 3 storeys and 15m for 4 storeys. This proposal is four storeys with a maximum height of 13.3m (rear). Based on the City's assessment, the proposal achieves a minimum of 2 hours per day (between 11:30am and 1:30pm) of solar access to the solar panels on 8A Alexander Road. The primary garden area for the dwelling is located in the rear and is not impacted by this development.

In relation to Element Objective O2.2.1, the proposed height is not compatible with the existing or desired future streetscape character of the area. As discussed above in the background section of this report, the existing streetscape comprises 17 residential zoned properties (including the subject site). Of these, eight consist of existing two storey single dwellings (constructed between 2008 and 2015), two lots feature double storey single dwellings that are currently under construction, one recently received development approval for the construction of two double storey grouped dwellings and one has received development approval for the construction of four multiple dwellings, two storeys in height.

Further to the above, of the 16 other properties on the street (excluding the subject site), only three consist of older dwellings that are candidates for development potential. These are located at 107 Waratah Avenue (west), 1 Alexander Road (west) and 10 Alexander Road (east).

Given the recent construction (or proposed construction) of the above-mentioned dwellings, it is unlikely that these will change or intensify in the foreseeable future. Based on the above, all existing dwellings and proposed future dwellings within the immediate locality of Alexander Road are limited to two storeys in height.

It is acknowledged that the eastern side of Alexander Road and the subject site is coded R80 (excluding 105 Waratah Avenue) which generally permits a higher intensity and height of development as outlined in both the R-Codes Volume 2 and the City's LPP 5.14: Precincts. However, given the existing streetscape of the immediate locality, featuring 81% of existing or proposed dwelling types which are limited to two storeys in height, it is reasonable to conclude that a two-storey height forms the existing and desired future scale and character of the street. As such, the three-storey presentation of the proposed development to the street and four storeys to adjoining properties is discordant and incompatible with the two-storey building height of the existing and foreseeable future streetscape.

Based on the above, the proposed four storey height of the development is not considered to respond to the desired future scale and character of the immediate locality including existing buildings that are unlikely to change. This is only the second multiple dwelling development within the immediate locality (the first being 5 Alexander Road, which is two storeys in height), and the bulk and scale proposed would set an undesirable precedent for future developments.

In relation to Element Objective O2.2.4, the proposed development does not appropriately recognise the need for solar access to the adjoining property at 8A Alexander Road. Based on development plans for 8A Alexander Road, the dwelling on that site features large north facing openings on both the ground and upper floors from internal living areas and outdoor living areas orientated north to gain winter sunlight.

Although the proposed development demonstrates that the adjoining property at 8A Alexander Road will receive two hours per day of solar access to both the solar panels and primary garden areas, this outcome represents the minimum acceptable outcome requirement. Satisfaction of the acceptable outcome does not necessarily guarantee the achievement of the element objective. The design of the building does not do enough to recognise or seek to reduce the impact the proposal on solar access for the southern property. Further, the dwelling was constructed in 2013 and is unlikely to change in the foreseeable future.

It is acknowledged that the amended plans as received by the applicant dated 26 March 2025 aim at reducing the impact of the bulk on the streetscape and overshadowing by 'flipping' the massing of the fourth floor and relocating the building bulk to the east (rear). This allows the development to present as more of a three-storey building when viewed from the street. However, the proposal is still not considered to achieve the Element Objectives of O2.2.1 and O2.2.4.

Element 2.4 Side and Rear Setbacks

The Acceptable Outcome for side and rear setbacks is set out in the City's Local Planning Policy 5.14: Precincts (LPP 5.14). As specified in the policy, the side setbacks for multiple dwellings within the R80 code are 2m on the ground and first floor and 3m on all other floors and a 6m average to the rear. The proposal achieves these side and rear setbacks as shown in **Attachment 5**.

In relation to Element Objective O2.4.4, the proposed setbacks from side and rear boundaries of the development do not provide an appropriate transition between adjoining sites with different intensity of development. As stated in the background section of this report, the adjoining northern, southern and eastern properties are single or two storey single dwellings. While the development achieves the nominal acceptable outcomes as outlined in LPP 5.14, the setbacks are considered insufficient in this context. The proposed bulk of the building is exacerbated by the predominantly rectilinear form and lack of meaningful articulation along the northern and southern boundaries which results in overdevelopment, significant bulk and imposing appearance as viewed from both the street and adjoining properties.

The development proposes minimum setbacks, particularly on the third and fourth floors. This does not contribute to a transitional scale which features gradual setbacks between varying built forms. Instead, the proposal does little to reduce the perceived intensity of the development by presenting as an abrupt building 'block' to side and rear properties which feature single or two storey dwellings.

Element 2.5 Plot Ratio

The Acceptable Outcome under Table 2.1 of the R-Codes for an R80 site is a plot ratio of 1.0. The application proposes a plot ratio of 1.18, which equates to an additional 18% or 182.2m² of plot ratio area over the Acceptable Outcome.

As described in the Element Intent of the R-Codes, plot ratio is one method of establishing a volume of development within the 'container' of a building envelope. In the case of the subject development, the plot ratio exceeds the anticipated building envelope 'container' by proposing additional height and insufficient setbacks that are not consistent with the Acceptable Outcomes.

In relation to Element Objective O2.5.1, the proposed bulk and scale of the development is not considered appropriate for the existing or planned character of the area for the following reasons:

- The development does not achieve the Element Objectives for building height as the proposal is not considered to respond to the existing and planned streetscape character which consists of single or two storey dwellings.

- The development does not achieve the Element Objectives for side setbacks as the setbacks proposed are insufficient to provide the expected separation distance expected in a low to medium density locality. The setbacks proposed do not achieve an appropriate transition between the development and the surrounding lower intensity of development.
- The plot ratio proposed is more representative of an R-AC4 density code which considers a plot ratio of 1.2. The intended built form and coding as set out within the City's LPP 5.14 and R80 coding is to provide consistency of development along the streetscape which comprises of single or two storey dwellings.

As stated, the proposed development does not achieve the Element Objectives of 2.2 Building height, 2.4 Side and rear setbacks and 3.2 Orientation. Although the proposed development achieves the acceptable outcome street setback by including articulation and varied building materiality to the street with the aim of mitigating the impact of bulk and scale, the significant exceedance of plot ratio by 182m² or 18% indicates that the overall bulk and scale is not appropriate for an R80 coded area.

Element 3.2 Orientation

There is no Acceptable Outcome limit with respect to the shadow cast at 12pm on 21 June for sites that are coded R80 or higher. As the adjoining site to the south is also coded R80, no maximum shadow limit applies. The proposal results in approximately 78% of the adjoining site area being overshadowed on 21 June.

Whilst it is acknowledged that the height of future buildings contemplated for the area will result in significant shadowing of the adjoining sites in most cases, Element Objective O3.2.2 has not been achieved as the shadow cast by the development has not been reasonably minimised. If the building's height was reduced and southern setbacks increased, there could be a discernible gain in solar access for the residents of 8A Alexander Road. In this context, the excess height and bulk results in shadowing that would not be present were the building designed in a manner that more appropriately responded to the existing and planned future character of the area.

Element 3.9 Car and Bicycle Parking

For the purposes of R-Codes assessment, the subject site is considered to be Location A, within a high frequency public transport area. The application proposes 8 resident car bays and 2 visitor car bays. This achieves the Acceptable Outcomes as outlined in A3.9.2 and Table 3.9.

Notwithstanding the satisfaction of the Acceptable Outcome, the proposed parking is not considered to achieve Element Objective O3.9.2, as the parking provided is not appropriate for the development or surrounding location. The dwelling consists of 3 three-bedroom dwellings, 1 five-bedroom dwelling and 1 seven-bedroom dwelling, totalling 21 bedrooms across the four floors. With the potential of housing 21 persons (one per bedroom) or 42 persons (two per bedroom, each bedroom is provided with a double bed).

Table 3.9 classifies all '2+ bedroom' dwelling under Location A (subject site) as requiring 1 resident bay per dwelling. The definition of a '2+ bedroom' dwelling does not distinguish between a conventionally sized 2- or 3-bedroom dwelling and a 5 - or 7- bedroom dwelling.

Due to that compact nature of apartment development, the R-Codes does not contemplate parking provision for large multiple dwellings. The reality is that the parking required for a '2+ bedroom' dwelling does not apply to a 5- or 7-bedroom dwelling. The R-Codes generalisation of a '2+ bedroom' dwelling does not reflect the functional intensity of larger dwellings and thus underestimates the demand generated by 'oversized' single dwellings.

The proposed 8 resident bays for five dwellings totalling 21 bedrooms equates to less than 0.4 bays per bedroom or 1.6 bays per dwelling. This is not commensurate with the likely parking demand that will be generated by the development, as the actual demand is likely to exceed this provision.

Further, the practical undersupply of on-site car parking will lead to overflow of parking on surrounding residential streets and have adverse amenity impacts on adjoining dwellings in relation to noise, congestion and reduced visual amenity. The western side of Alexander Road does not permit any on-street parking at any times of the day. The eastern side of Alexander Road permits 2-hour parking between 8am and 5pm. The on-street parking parameters are not compatible with resident parking. Given the short-stay nature of the on-street parking, this would only be compatible with visitor parking. Surrounding the subject site, Waratah Village Precinct provides on street parking bays, however these bays are intended to be used by the commercial businesses along Waratah Avenue and not as overflow resident parking.

It is reasonable to consider higher bedroom yields and alternative dwelling types (such as this proposal) as triggering a need for proportional increases in car parking provisions. Additional parking should be provided to a parking ratio of one bay per two bedrooms (totalling 12 resident bays). This provision would be consistent with the intent of a '2+ bedroom' dwelling as outlined in Table 2.9.

The below elements of the proposal are considered to achieve the Element Objectives:

Element 2.3 Street Setbacks

The Acceptable Outcome for primary street setbacks is set out in the City's Local Planning Policy 5.14: Precincts (LPP 5.14). As specified in the policy, the ground and first floor should be setback a minimum of 5m and all other floors setback a minimum of 6m. Footnote 2 goes onto permit a 2m setback reduction if sufficient compensation area is provided behind the street setback line.

The application proposes a 5m setback on the ground and first floor, a 5m (sufficient compensation provided) setback on the second floor and a 7.5m setback on the third floor thereby achieving the Acceptable Outcomes as replaced by LPP 5.14: Precincts.

In relation to Element Objective O2.3.1, the primary street setbacks are considered to present an appropriate response to the existing streetscape which features varying setbacks between 5.5m and 11m. The setbacks provided are consistent with the desired and future character by achieving the setbacks as outlined above in LPP 5.14: Precincts and compliment the surrounding existing single dwellings which are unlikely to change in the near future,

The primary street setbacks are considered to achieve Element Objectives O2.3.2, O2.3.3 and O2.3.4 as detailed within **Attachment 5**.

Element 3.3 Tree Canopy and Deep Soil

The Acceptable Outcome for tree canopy and deep soil require that 10% of the site area is to be provided as deep soil and one large tree to be provided. The development proposes 13% deep soil area and the planting of 2 medium trees and 4 small trees as well as additional on structure planting on all floors.

Although the tree planting provision does not achieve the Acceptable Outcomes, it is considered to achieve the Element Objectives.

Element 3.5 Visual Privacy

The R-Codes Acceptable Outcomes for visual privacy require openings from the development to be set back 3m for bedroom major openings, 4.5m for major openings of other habitable rooms and 6m to unenclosed private outdoor spaces.

Most of the setbacks of the development meet the Acceptable Outcomes with the exception of Unit 201 western facing balcony to the northern and southern boundaries which are setback 5.7m. The orientation of the balcony fronting the street restricts the balconies from direct overlooking the adjacent properties. Further, the line of sight from the balconies falls into the front setback area and driveway/front garden of the adjoining properties. This does not negatively impact the amenity of the adjoining residents and therefore achieved the Element Objective of O3.5.1.

The rest of the openings contribute to the satisfaction of Element Objective O3.5.1 by providing intermittent screening and a sufficient distance to major openings to achieve the expected level of privacy in an apartment development.

Element 4.15 Energy Efficiency

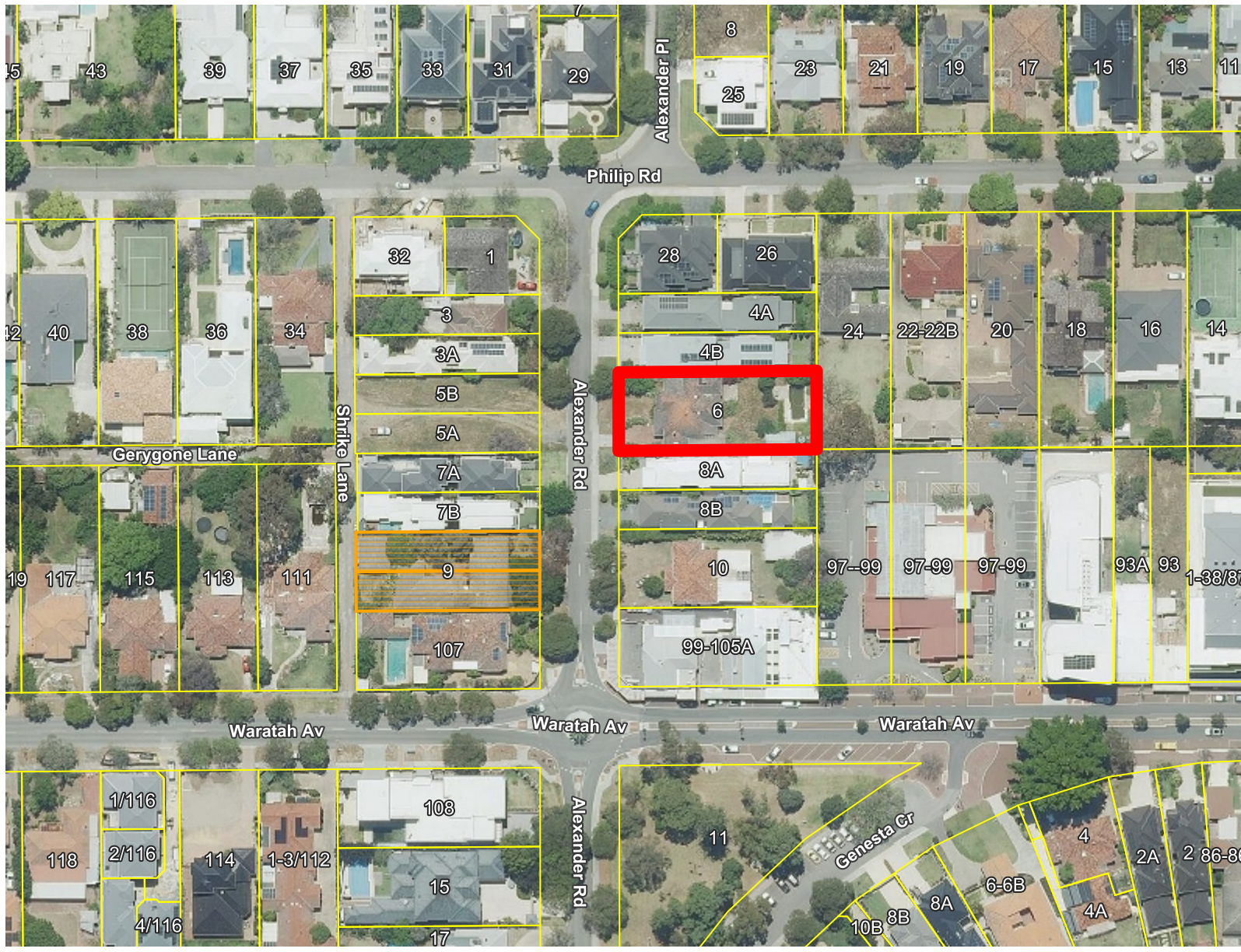
The City's Local Planning Policy 1.3 Sustainable Design (LPP 1.3) augments the Design Guidance of Element 4.15 by providing local objectives for housing design and development, and the City's preferred methods of achieving the Element Objectives. The LPP 1.3 Preferred outcome for multiple dwellings less than \$15 million is for the development to incorporate at least one energy efficiency initiative that exceeds the minimum practice. These can be found in green within Table 1 of LPP 1.3.

Whilst the applicant has not committed to a 5-star green star certification, a Sustainable Design report has been prepared by Full Circle Design Services (**Attachment 10**) which demonstrates that the development achieves the acceptable outcome by the overall development achieving an average of 7.5 stars. In addition, the development proposes a 6kW Solar PV system per dwelling, installation of infrastructure to allow for future installation of EV charging stations, water efficient water fixtures, and products for internal finishes with low volatile organic compounds (VOC) and formaldehyde ratings. A condition of approval is recommended for the above if the application is supported by the DAP.

Conclusion:

The application for a four-storey development consisting of five multiple dwellings is not considered consistent with the City's current planning framework and is not consistent to achieve the Element Objectives of the R-Codes as outlined above. The development is not consistent or compatible with the existing and future streetscape

or character of the immediate locality. Considering the above, it is recommended that the development be refused.



- Labels**
- House Numbers
 - Road Names
 - Property Boundaries
 - Current Cadastre
 - Lodged Cadastre



The City of Nedlands accepts no responsibility for the accuracy of this image or the results of any actions taken when using this image

Aerial Image

15/04/2025

1:1500





- Labels**
- House Numbers
 - Road Names
 - Local Roads
 - LPS3 RCodes
 - Property Boundaries
 - Current Cadastre
 - Lodged Cadastre
 - LPS3 Zones
 - Mixed Use
 - Residential
 - LPS3 Reserves
 - Public Open Space



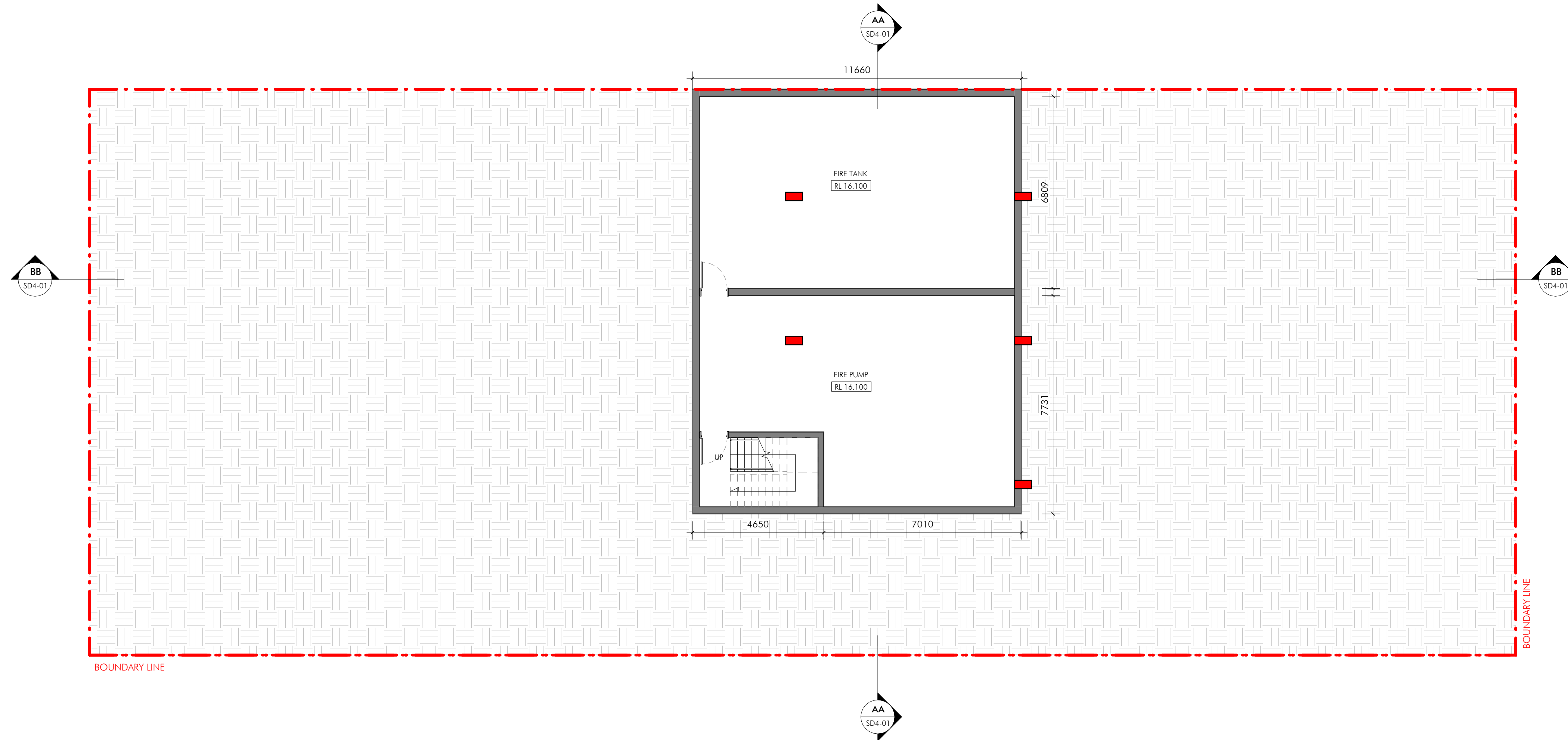
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Zoning Map

15/04/2025

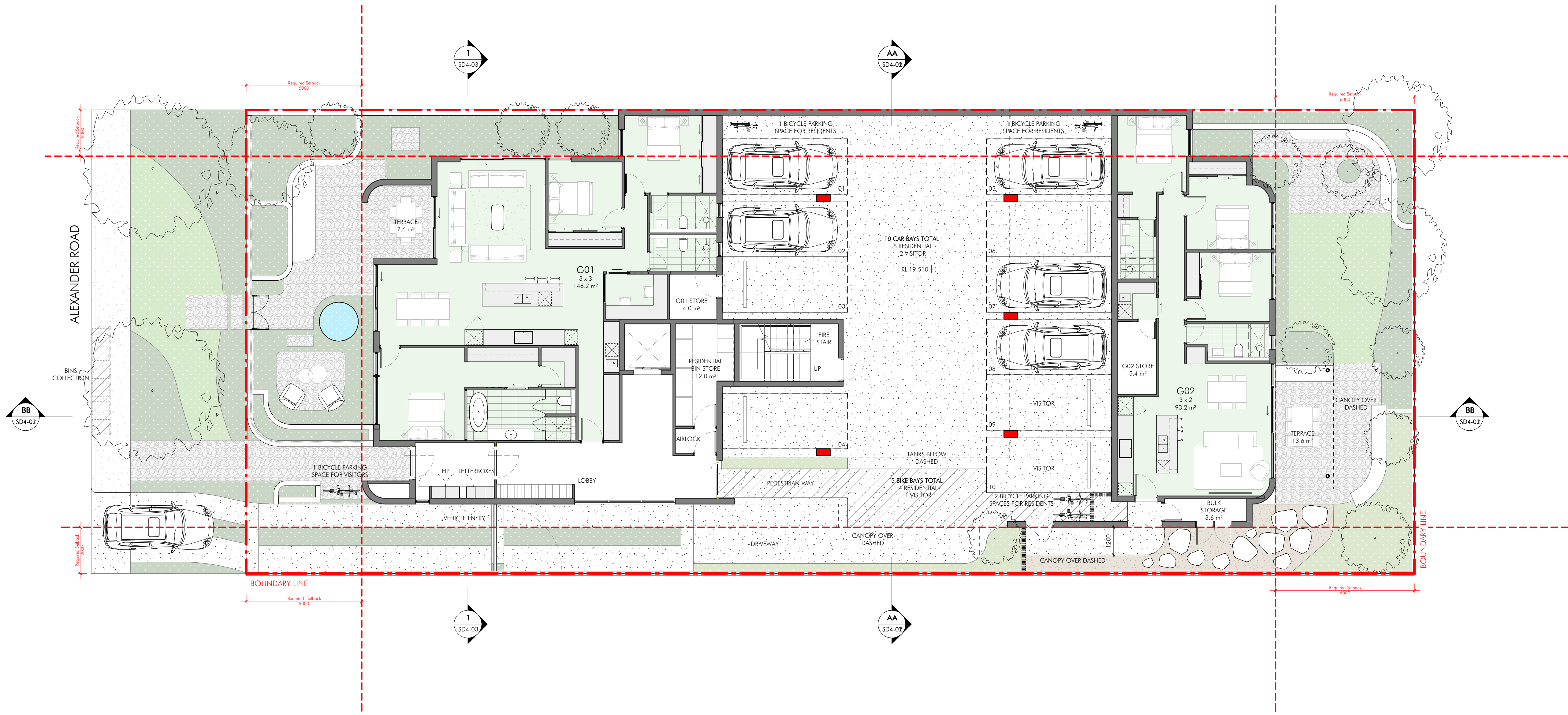
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Apartment Area Schedule			
Unit Number	Level	Layout	Internal Area
G01	Ground Floor	3 x 3	146.2 m ²
G02	Ground Floor	3 x 2	93.2 m ²
101	Level 1	7 x 7.5	390.5 m ²
201	Level 2	5 x 5.5	353.8 m ²
301	Level 3	3 x 3	112.7 m ²
Total Units: 5			1,096.3 m ²

1 Basement
1 : 100



Parking Schedule	
Type/Comments	Count
Car Bay - 2400 x 5400	8
Car Bay - 2400 x 5400 - VISITORS	2
Grand total:	10

Apartment Area Schedule			
Unit Number	Level	Layout	Internal Area
G01	Ground Floor	3 x 3	146.2 m ²
G02	Ground Floor	3 x 2	93.2 m ²
101	Level 1	7 x 7.5	390.5 m ²
201	Level 2	5 x 5.5	35.3 m ²
301	Level 3	3 x 3	112.7 m ²
Total Units: 5			1,096.3 m²

1 Ground Floor
1 : 100

SCALE 1 : 100 @A1
0 1 2 3 4 5 m

DRAWING NAME
GROUND FLOOR PLAN
PROJECT PHASE
SCHEMATIC DESIGN

DRAWING No.
SD2-01

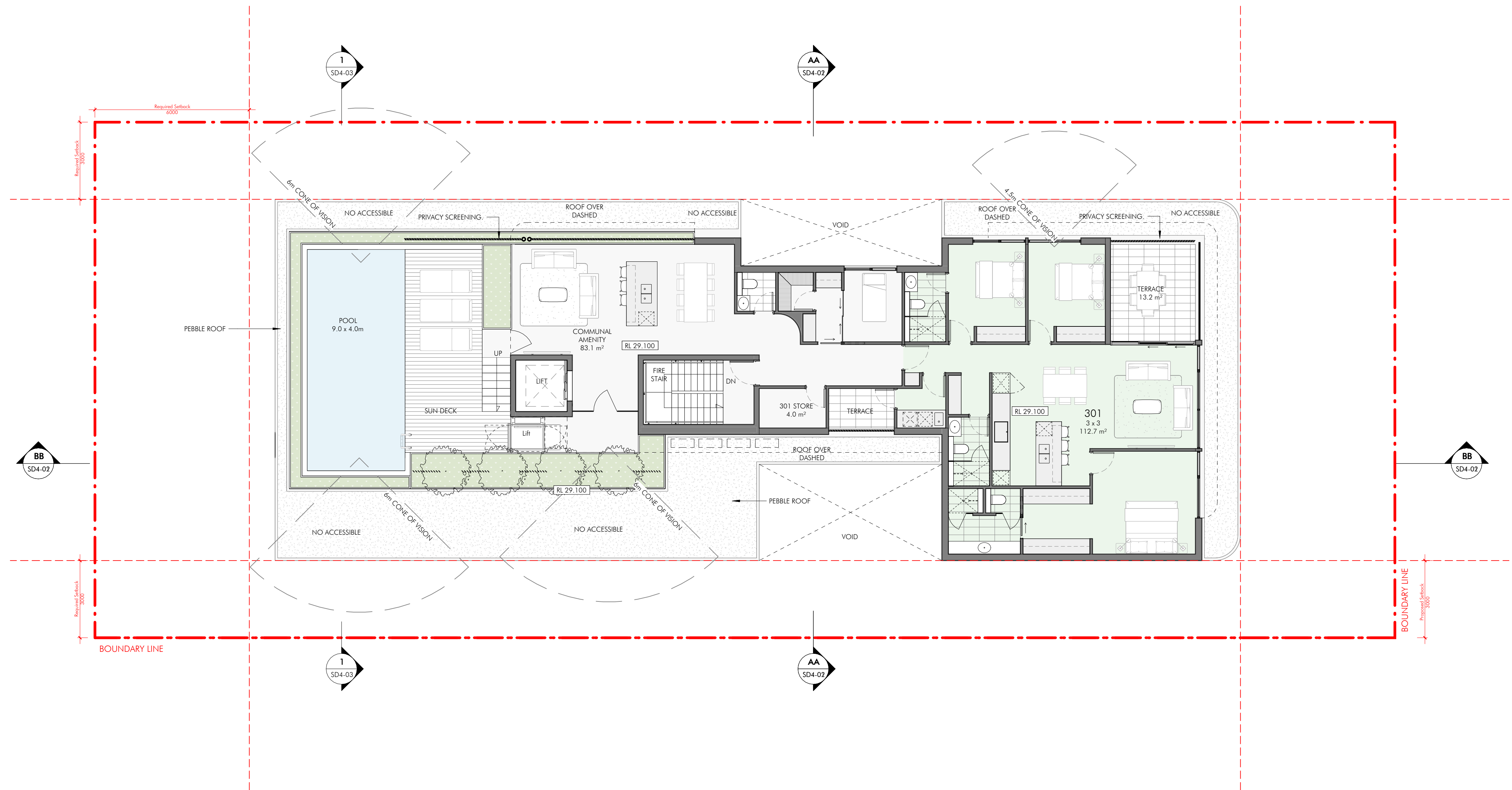
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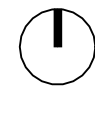
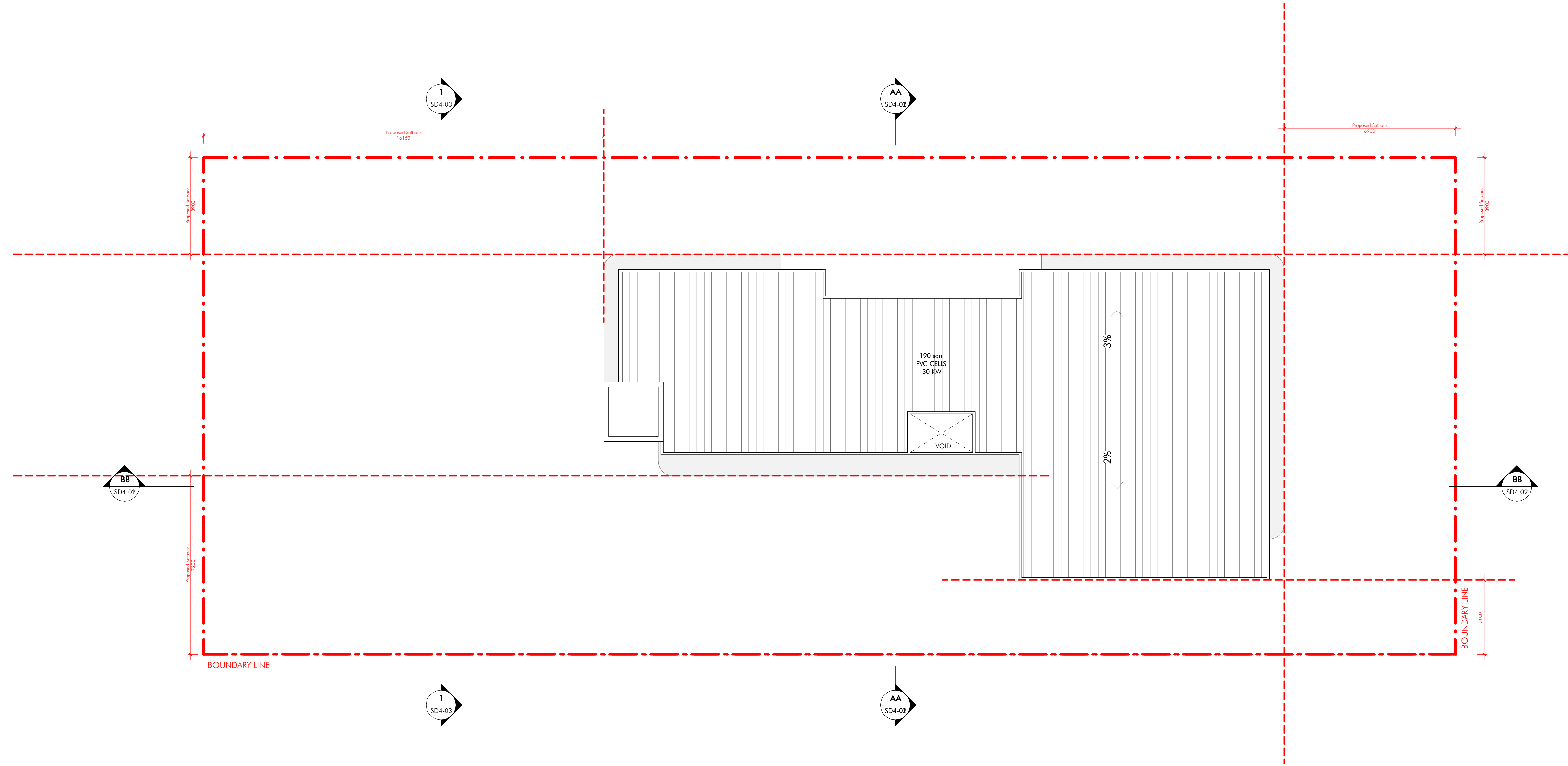
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Total Units: 5			1,096.3 m ²

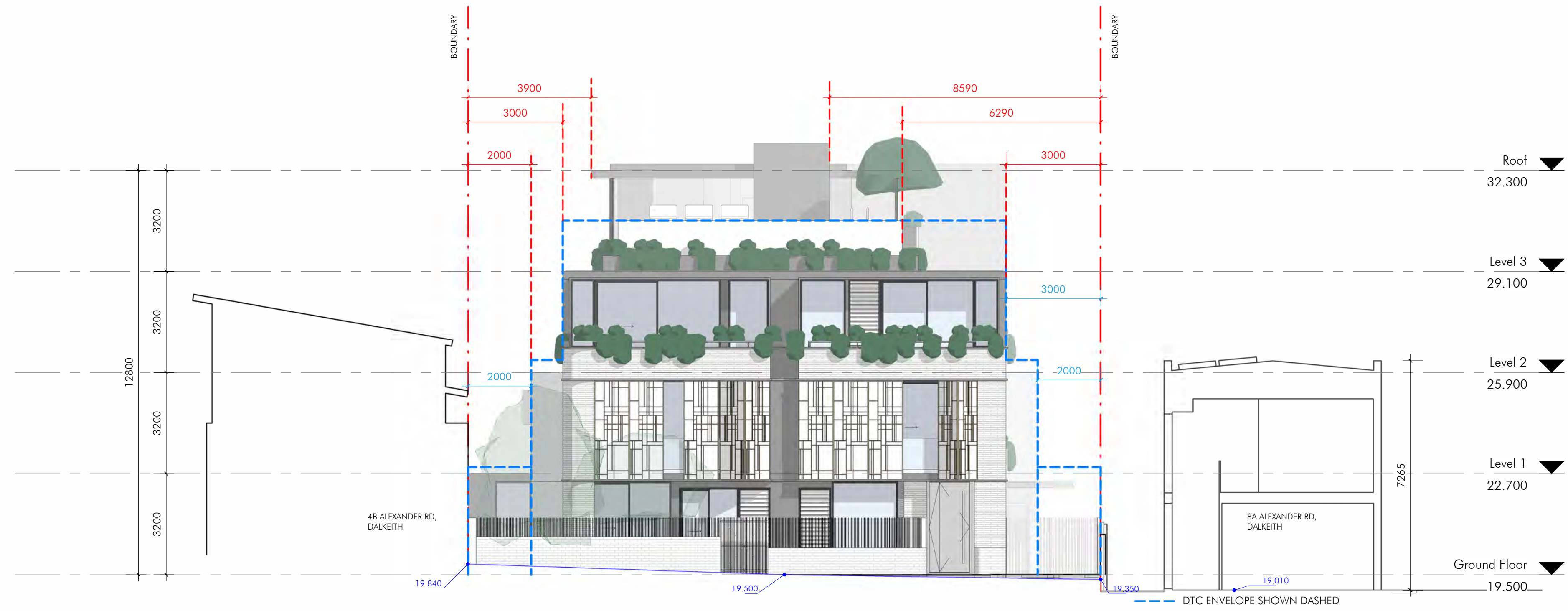


Apartment Area Schedule			
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Total Units: 5			1,096.3 m ²

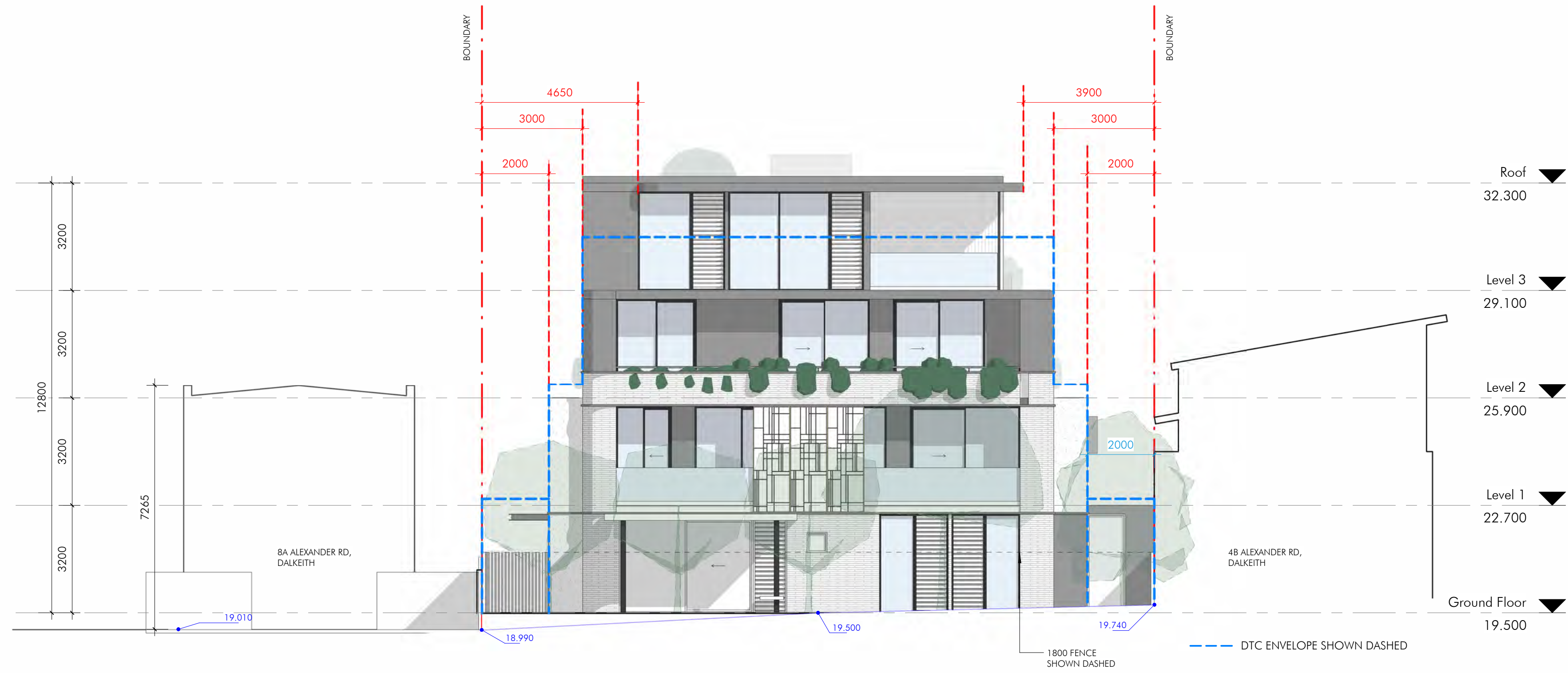


Apartment Area Schedule			
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Total Units: 5			1,096.3 m ²

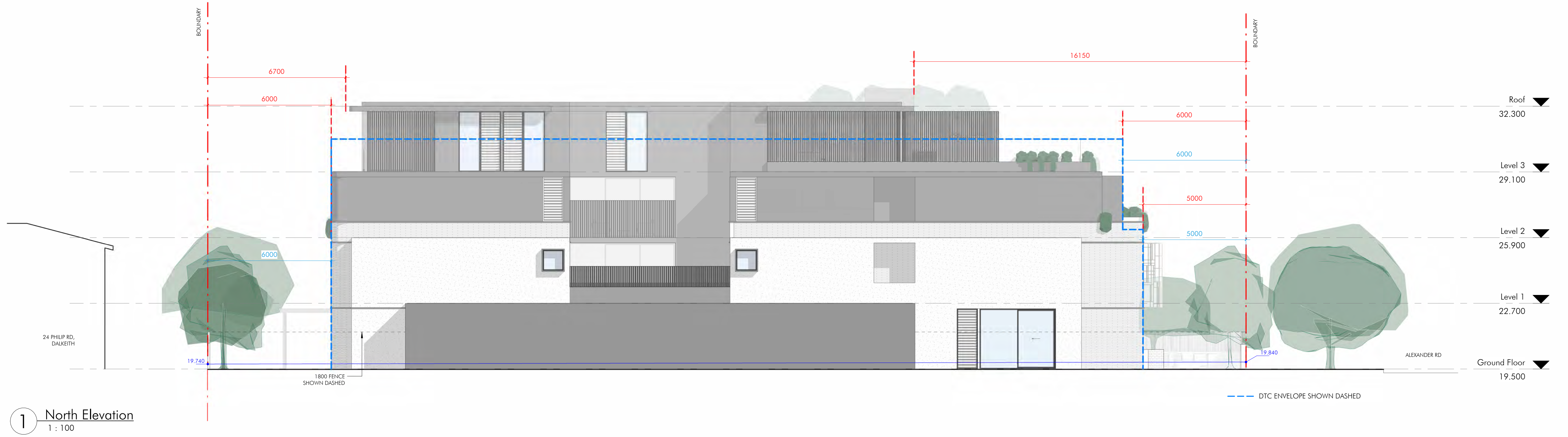




1 West Elevation
1 : 100

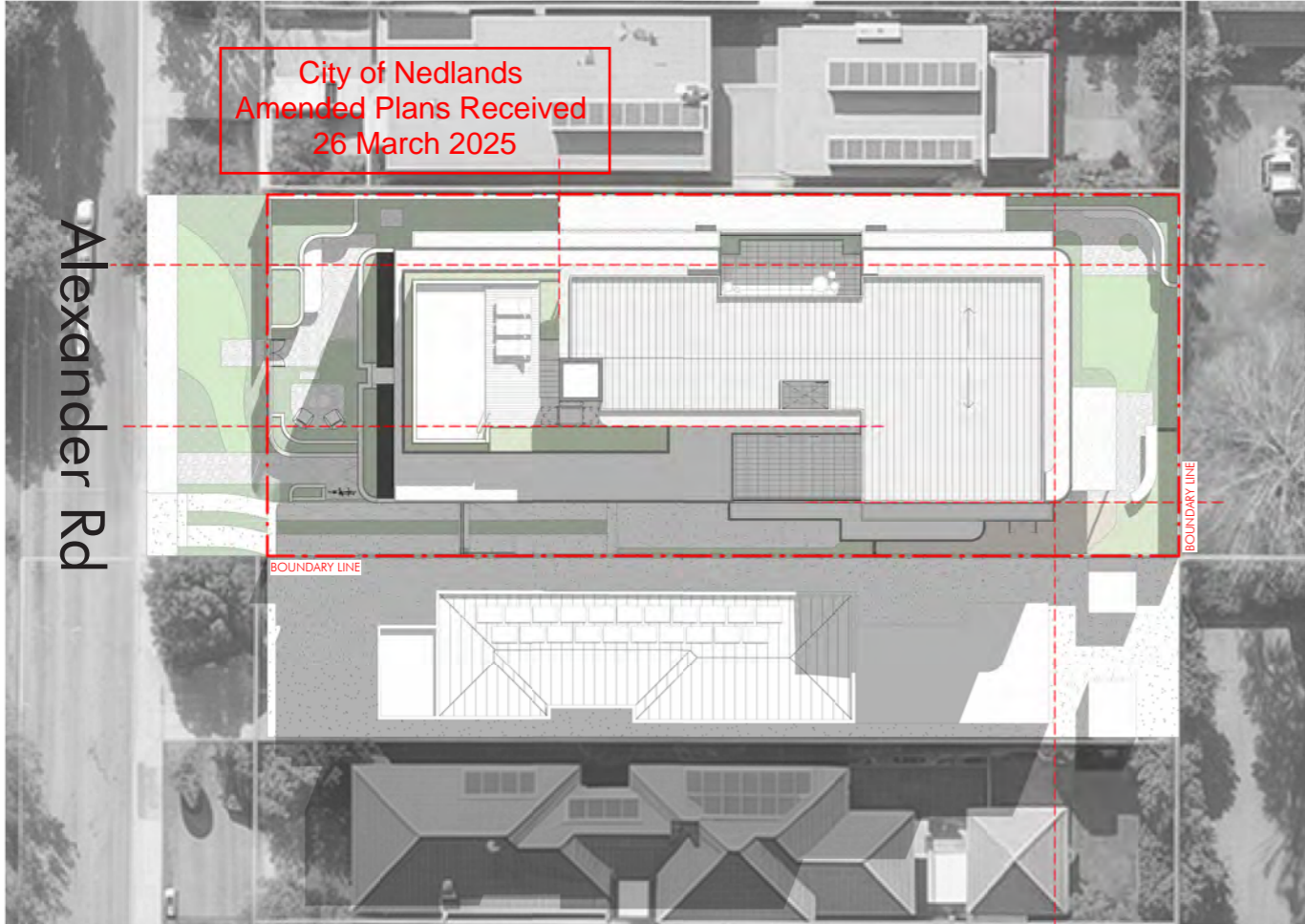


2 East Elevation
1 : 100

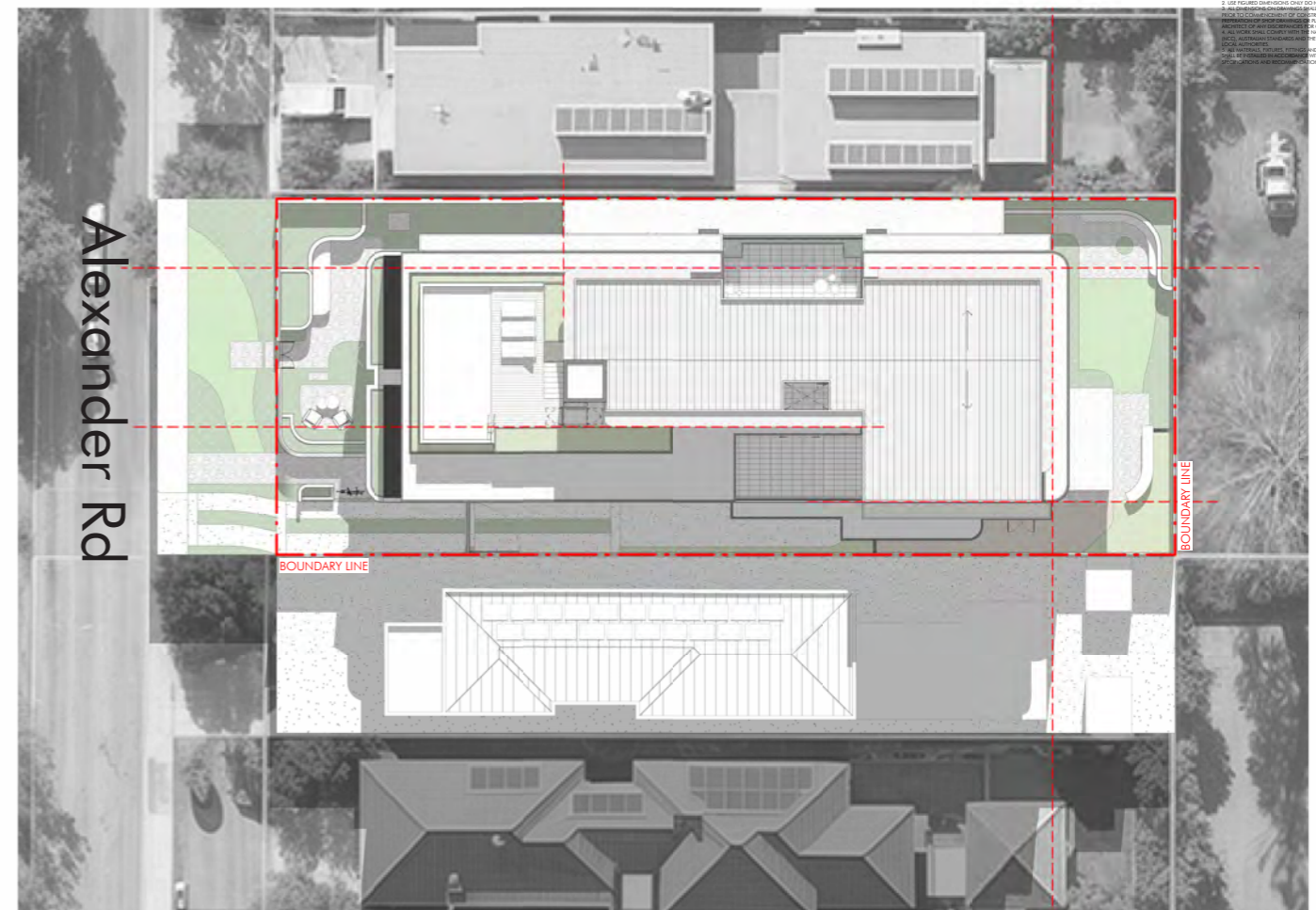


OVERSHADOWING
 1. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL ARCHITECTURAL DRAWINGS.
 2. THIS DRAWING IS FOR INFORMATION ONLY AND DOES NOT CONSTITUTE A CONTRACT.
 3. THE DESIGNER IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED.
 4. THE DESIGNER IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED.
 5. THE DESIGNER IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED.
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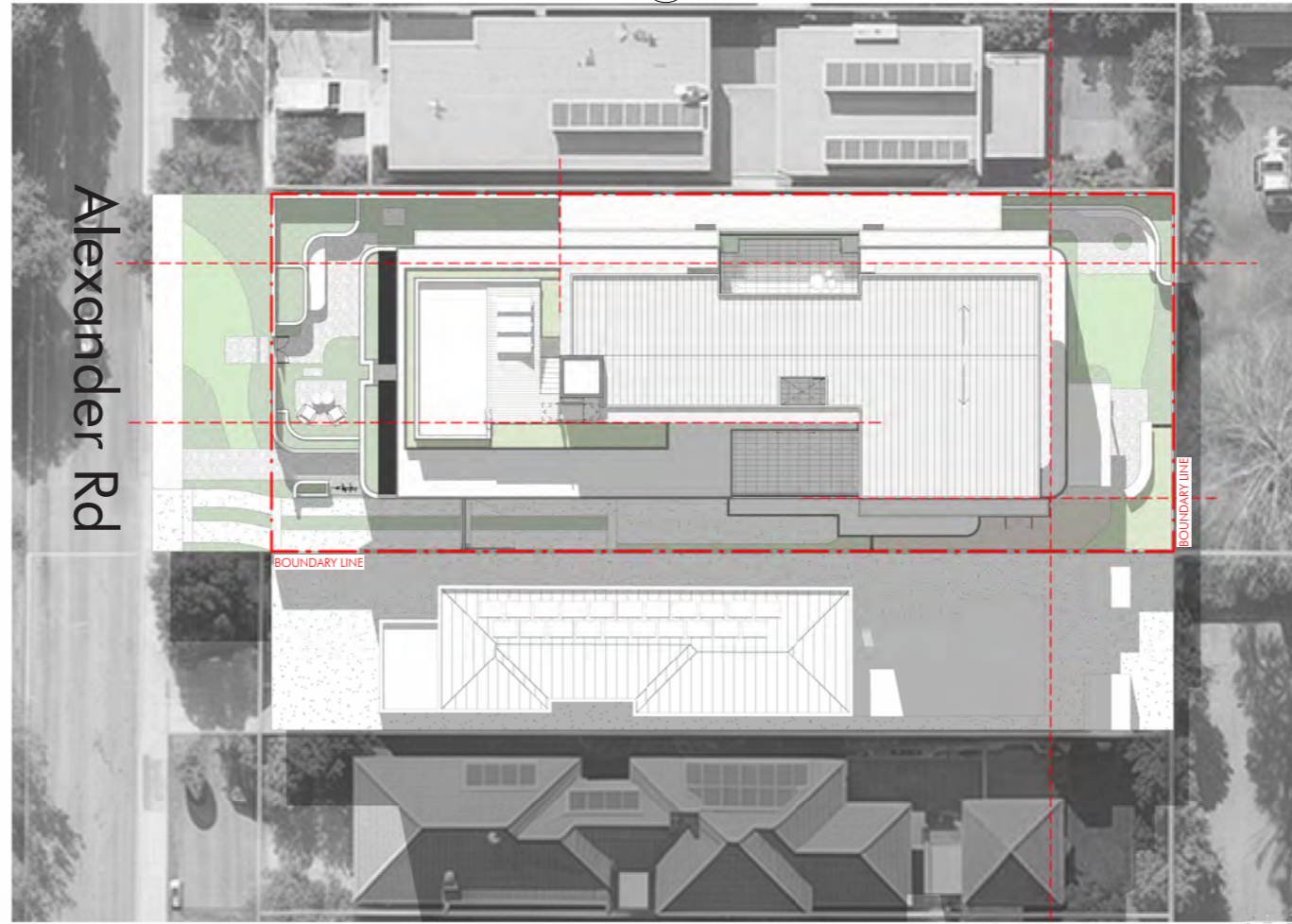
City of Nedlands
 Amended Plans Received
 26 March 2025



1 00. Overshadowing - 11am
 1 : 200



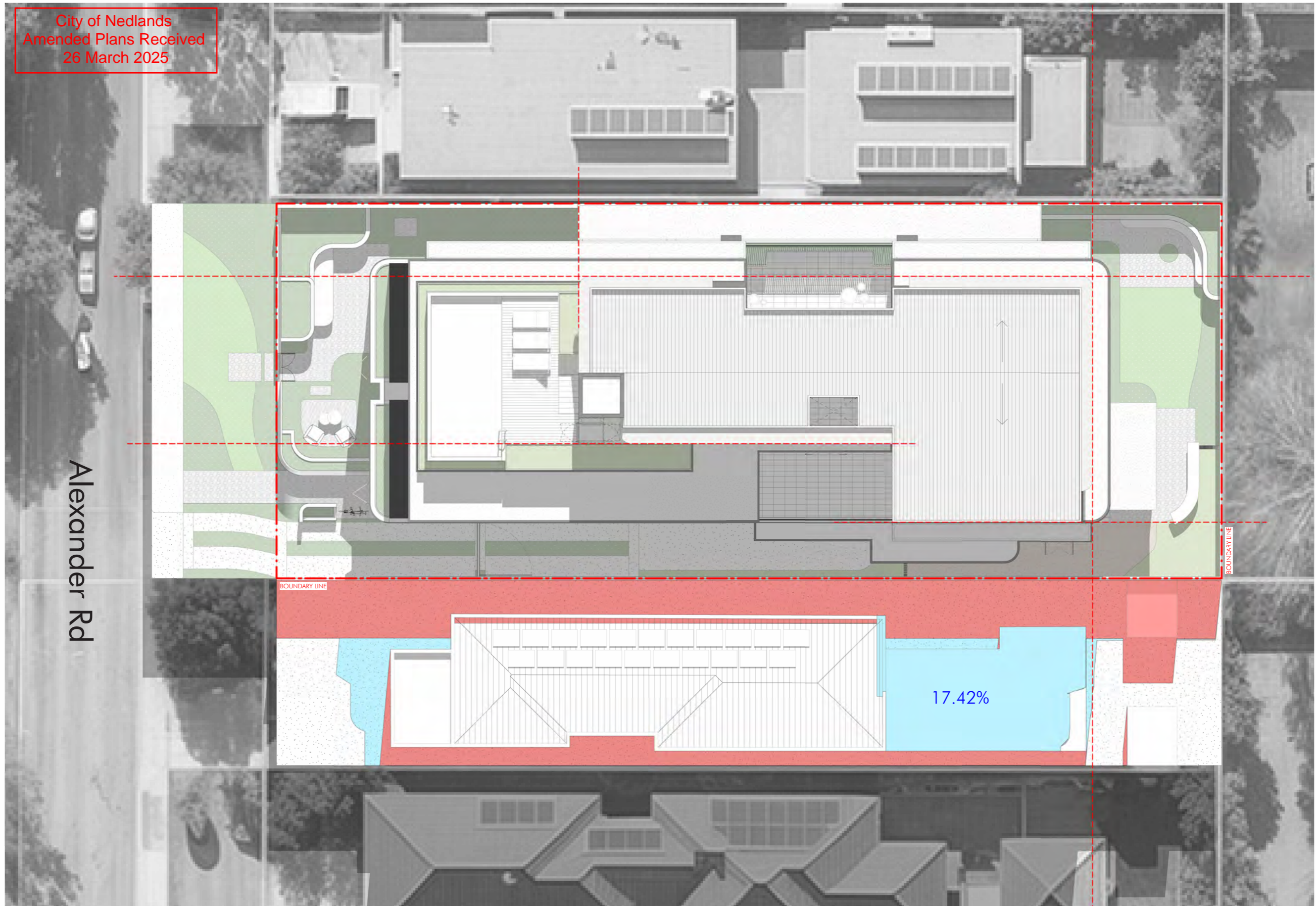
2 00. Overshadowing - 12pm
 1 : 200



3 00. Overshadowing - 1pm
 1 : 200

OVERSHADING NOTES:
 1. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL ARCHITECTURAL DRAWINGS.
 2. THE OVERSHADING AREAS ARE INDICATED BY THE RED AND BLUE SHADING.
 3. THE OVERSHADING AREAS ARE TO BE PROVIDED BY THE BUILDING DESIGNER.
 4. ALL WORK SHALL COMPLY WITH THE APPLICABLE CONSTRUCTION CODES AND REGULATIONS AND THE REQUIREMENTS OF THIS AND ALL APPLICABLE LOCAL AUTHORITIES.
 5. ALL MATERIALS, FINISHES AND BUILDING COMPONENTS SHALL BE PROVIDED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.

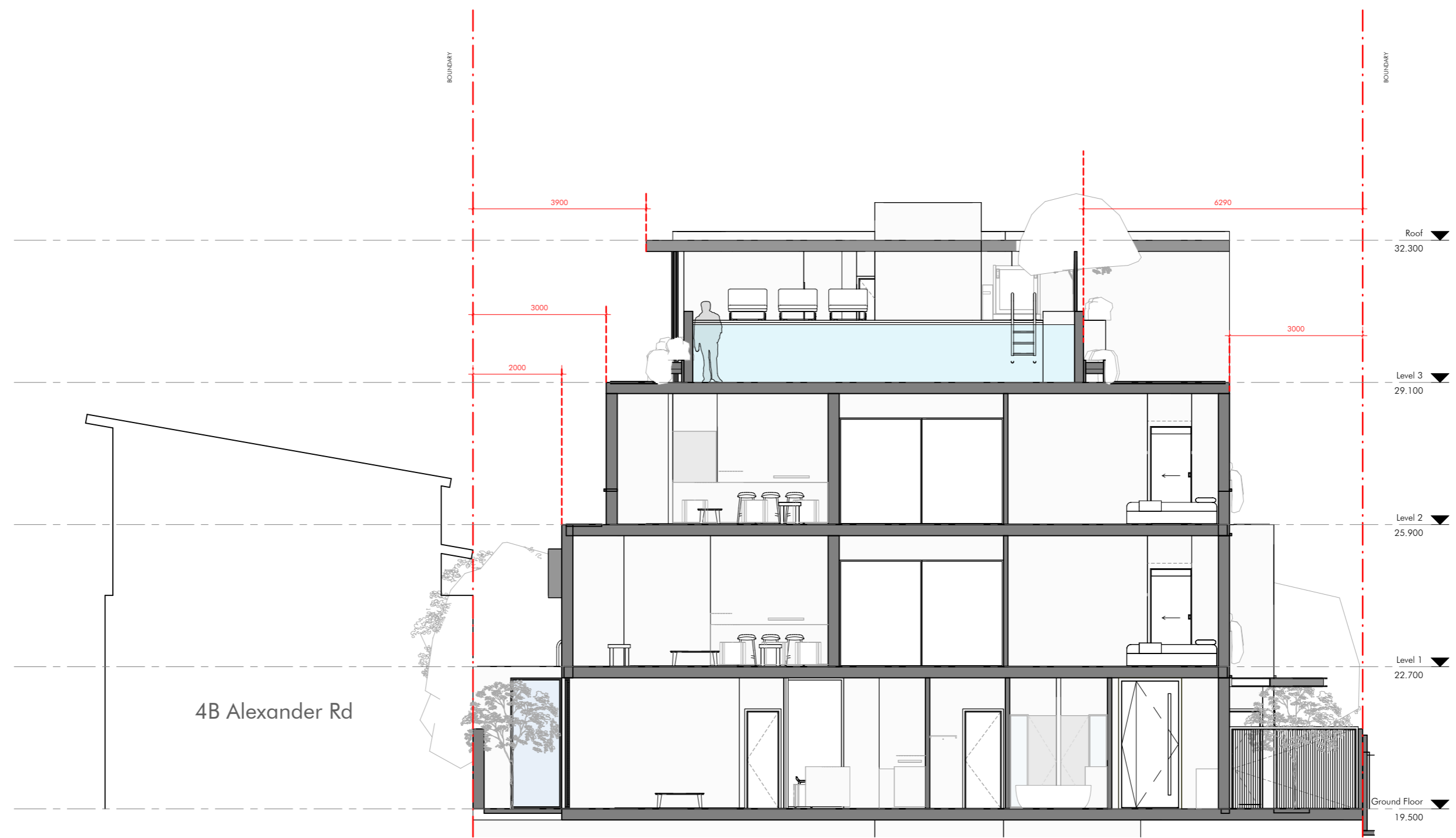
City of Nedlands
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 26 March 2025



COLOR	DESCRIPTION	AREA m ²	%
Red	Shadow projected by the neighbour itself	172.3	34.05%
Blue	Shadow projected by our proposal	88.14	17.42%

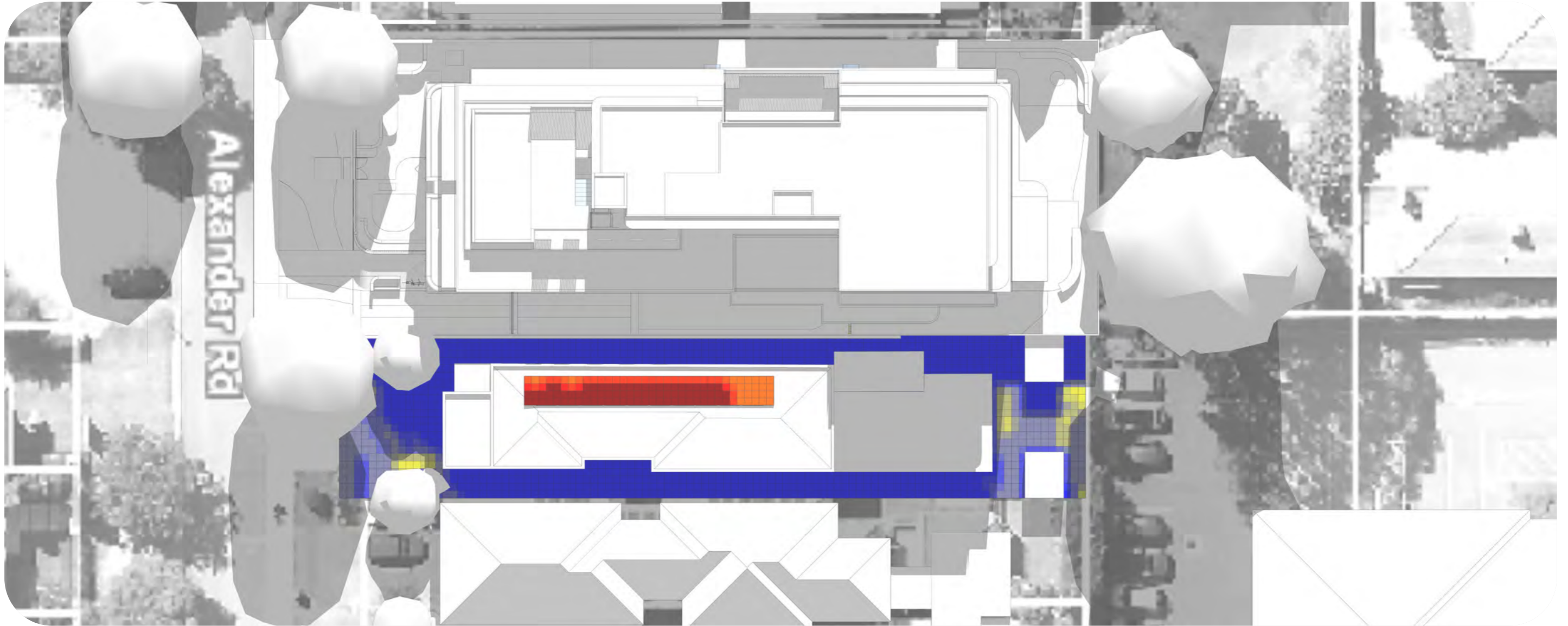
City of Nedlands
Amended Plans Received
26 March 2025

GENERAL NOTES:
1. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL ARCHITECTURAL DRAWINGS AND SPECIFICATIONS AND CONDITIONS.
2. ALL DIMENSIONS ARE UNLESS OTHERWISE SPECIFIED BY THE ARCHITECT.
3. ALL DIMENSIONS ON DRAWINGS ARE TO BE CHECKED BY THE BUILDER.
4. ALL WORK SHALL COMPLY WITH THE APPLICABLE CONSTRUCTION CODES, REGULATIONS, STANDARDS AND THE REQUIREMENTS OF STATE AND LOCAL AUTHORITIES.
5. ALL MATERIALS, FINISHES AND BUILDING COMPONENTS SHALL BE PROVIDED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.

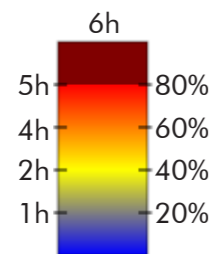


Solar Neighbour

City of Nedlands
Amended Plans Received
26 March 2025



Solar Access 9:00 - 15:00
Winter Solstice - June 21st



City of Nedlands Amended Plans Received 26 March 2025 Renders Comparison



Previous



Revised

Renders Comparison

City of Nedlands
Amended Plans Received
26 March 2025



Previous



Revised

Schedule of Submissions
6 Alexander Road, Dalkeith



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No.	
Advertising 1	
Object	
1	5 apartments with a total of 22 bedrooms with only 10 parking bays is crazy. The traffic and parking, which are already limited in the area, will be awful!! One of the apartments has 8 bedrooms and another five bedrooms. This isn't suitable for the area .. I welcome more density housing in Dalkeith but apartments with 5 and 8 bedrooms is not appropriate
2	I object to the height and size of this proposed development in Alexander Road. Whilst I understand the need for higher density housing in Perth, with our growing population, care should be taken regarding retaining character and living standards when assessing proposals for new development. For instance, finding suitable locations and grouping medium or higher density housing together makes sense. A relevant example of this in Dalkeith is along Waratah Avenue. Waratah Avenue is a main thoroughfare with a mix of residential and commercial structures and taller and denser apartment and unit development is logical there. On the other hand Alexander Road is a Dalkeith side street with single residential dwellings of no more than two stories in height. It is a small street, unlike Waratah Avenue, and not sufficiently wide to take a lot of traffic. I feel the character of Alexander Street and the quality of its residents lifestyle will be negatively affected if this medium height apartment structure is approved. Thankyou for your consideration.
3	<ol style="list-style-type: none">1. Due to it's bulk and scale and, in particular it's overall height, this proposed building will appear as grossly out of place in the proposed location.2. The development as submitted is well in excess of the recently stipulated 10m building height.3. By its bulk and scale this building will adversely impact the market value of most of the properties on Alexander Road between Waratah Ave and Phillip Rd.4. The proposed building submission appears to ignore many of the aspects of the 2021 SAT rulings applicable to this site.5. Overshadowing of buildings on the south side of the proposed development will be severe.6. This proposed building and its potential effect on the the surrounding and nearby properties seems to be quite unapparent to the senior staff of the City with a comment such as "upon learning there was significant interest from the local community."

Schedule of Submissions
6 Alexander Road, Dalkeith



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4	<p>The proposed development should not be approved on grounds of:</p> <ol style="list-style-type: none">1. A 4 story flat development is not sympathetic to the current and future nature of the streetscape. The narrow street contains mainly new 2 story townhouses and further similar developments are proposed. SAT decision WASAT41 which disapproved a similar style of development in 2021 stressed the importance of a development being consistent to the area,2. The proposed development does not confirm to R codes as regards plot ratio, side setbacks and potentially parking requirements,3. The bulk and height of the proposed development unduly affects amenity of adjacent buildings regarding overshadowing, potential noise and garbage requirements. <p>A development with 23 bedrooms and potential for many persons arising from this will lead to considerable increase in street parking issues in what is already a small street that is severely impacted by adjacent retail operations.,</p>
5	<p>We have a number of concerns relating to this development proposal</p> <ol style="list-style-type: none">1. The size and magnitude of the building is inappropriate for the site and the street. Previous development proposals have been declined on this site due to the unfair nature of the proposal for surrounding residents, in particular residences on the north and south boundaries. It was our understanding that the R80 zoning for this site (and this section of Alexander Road) had been revoked after the prior rejected proposals.2. Car parking proposal is grossly inadequate for the development. The streets surrounding this area are heavily congested with traffic and parked vehicles on a daily basis. The council erected parking signs in this area are being ignored by motorists and appear to not be policed by the Nedlands City Council. Street congestion due to vehicles has become a hazard of living in this area.3. Traffic hazard related to the intersection of Alexander Road, Alexander Place and Philip Road. We have lived at 25 Philip Road for nearly 28 years and there has been an increase in near traffic accidents at this intersection due to poor visibility, speeding vehicles on Philip Road, thoroughfare between Watkins Road and Waratah Avenue, as well as the unusual nature of the intersection connecting Alexander Road and Alexander Place across Philip Road. Further increase in parking congestion and traffic related to this development is dangerous to the safety of current residents. <p>We strongly oppose the development proposal as presented.</p>
6	<p>The proposed development is grossly discordant with the immediate vicinity in term of height, bulk and scale. It is surrounded by a large number of relatively new, high quality two-storey houses. The owners of these properties will obviously be grossly impacted. However, the most disturbing feature of this project is that it is clearly designed as student or short-stay accommodation. Level 1 has 8 bedrooms, each with its own ensuite and Level 2 has 6 bedrooms with ensuites. In total there are 23 bedrooms and the parking on the ground floor is grossly inadequate. Used as student accommodation, there could be up to 17 apartments with non-related residents in each. On this basis, the appropriate traffic review would have required 21.375 parking spaces within the building, as compared with 10 parking bays</p>

Schedule of Submissions
6 Alexander Road, Dalkeith



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	<p>proposed on the basis of 5 individual residences. The consequences of overflow parking will be totally unacceptable. Alexander Road is already struggling with parked cars.</p>
7	<p>Whilst we agree in principle with increasing density by subdividing larger blocks any development in the area should be in keeping with the surrounding streetscape and aimed at housing families. We believe all construction in this area should be 2 story or less with some exceptions on Watatah Avenue. The 12 Philip Road construction should never have been approved and should not be repeated. The size and scale of this proposal is completely out of proportion with surrounding properties, the plans show 23 bedrooms on a single block with only 10 parking spaces. A sensible subdivision of townhouses would likely have a maximum of 16 bedrooms. What is the target audience for this type of development? And what services will they require? The precedents set on each side should be adhered to. This is so obviously another cynical profiteering enterprise which will do nothing to enhance the area which has been so carefully relandscaped recently by CON.</p>
8	<p>I object to the proposal for 3 reasons</p> <ol style="list-style-type: none">1. The building height is excessive. Buildings that are 4 stories have no place in narrow streets such as Alexander Road. The proposed building is out of character and in conflict with the other recently constructed dwellings along the road and will be an eyesore.2. The 5 dwelling building proposed has 22 bedrooms and sleeping places for up to 50 people. This is too many and should the number of residents fall into the 30 – 40 range is only 30-40m2 per person well below the norm. This density of occupation is more consistent with a residential care facility or hotel. In fact, two of the levels look like they were designed with this in mind.3. The parking provided for the building is woefully inadequate. While the number of bays may meet to guidelines of 1.25 parking spaces per dwelling, when a dwelling has 6 or 8 bedrooms (as per units 201 and 101 respectively) the value of such guidance is stretched past breaking point. <p>The location of the parking in the centre of the building along a narrow laneway is poorly thought through. When the bays are full (likely most of the time) anyone entering to use the parking space will need to backout to park elsewhere. This will bring consider danger to pedestrians and passing traffic.</p> <p>In addition, due to the restricted space for parking and manoeuvring, the most likely result is many of the residents will chose to park in the</p>

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	<p>nearby streets which are already unable to accommodate existing parking needs during the day. With so many sleeping spots in the proposed building, many likely to be used by adults who drive, the building will be in need in the vicinity of 20 parking places if not more.</p> <p>UPDATE</p> <p>In my submission is raised the point that the amount of parking available under the building was too little and would likely be filled most of the time given the number of people reasonably expected to be living there. This means that patrons who drive in to park when the parking is full would need to back out as there is not turning facility. This has not changed. In fact the new technical not just underlines the danger of backing into the street with a driver with limited sight of the surrounds. The building either needs a car turning table or a carpark full sign at its entrance. Adding a few more spaces is not likely to solve this problem.</p> <p>The waste disposal report is based on the ridiculous assumption that a dwelling with 8 bedrooms will produce the same amount of waste as one with 3 bedrooms. While a 3 bedroom unit may yield the minimum set out in the report, 5 and 8 bedroom units will surely produce more waste, particularly Fogo. Providing 5 refuse and 5 recycling bins (ie one of each per apartment) is sensible and consistent with the normal waste collection once a fortnight. Only one FOGO bin is not credible even though it has a weekly collection. The larger units should have one each with two allocated to the other smaller 3 units. That means space needs to be found for another 3 bins.</p>
9	<p>I write on behalf of my wife [owner] and I [occupier] in order to express our very strong objections to this proposal.</p> <ol style="list-style-type: none"> 1. WASAT 41 [March 2021] rejected a similar proposal. 2. LPP5.8 limits the height of developments within the northern end of Alexander Road to 2 stories. 3. The plot ratio of the proposed development greatly exceeds the allowed ratio of 1.00. 4. The scale and bulk of the proposed building are not consistent with the prevailing [and relatively new] streetscape. 5. Overshadowing of buildings to the south of the proposed development is substantial. 6. The purpose of the proposed building is far from clear given the number of bedrooms with implications for both parking and rubbish management. 7. With 8 relatively newly constructed homes and several more now in the building or planning stages [all no more than 2 storey] the proposed development at 6 Alexander Road will have an enormous negative impact upon the streetscape and amenity of the northern end of Alexander Road.
10	<p>All forms of denser accommodation on the standard suburban residential blocks should be forbidden.</p> <p>This Alexander Road proposal and many others already permitted by the Council do not solve any accommodation shortage, as the developments are expensive and restricted to richer people and they create nothing but congestion in every available sense (environmentally,</p>

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	<p>foot traffic, vehicle parking and traffic, destruction of trees , plants and bird life, garbage and sewerage management, aesthetics), hence reducing the value of the adjoining properties and those in the residential area in which they are being placed. If they are introduced to expensive suburbs, they should be confined to the main roads in the residential area, like Waratah and Adelpa, where they mingle with some commercial properties and are reasonably spread on these roads; not overwhelming the roads. The only reason for the Alexander Road proposal, Philip Road (already near completion) and all others on the quiet residential roads is for developers to make as much money as possible out of a very small site (lot); absolutely no other reason; the consequence is that the property owners who bought into the suburb for the privacy, trees, space and quietness and were prepared to pay TOP DOLLAR for that and TOP DOLLAR for Council rates, now have the values of their properties reduced without any consideration at all from the Council or government. Clearly, the owners must commence a class action to regain the lost value that they are experiencing and will continue to experience if this random, "anything goes" attitude from the Council and government continues.</p>
11	<p>Having recently purchased our new home, we are deeply concerned and distressed to learn that a development, similar to the one rejected in 2020, is once again being proposed. As immediate neighbours to the South of the proposed development, we have several critical concerns: Impact on Sunlight and Privacy</p> <ul style="list-style-type: none">• Our property currently has solar panels that function efficiently under direct sunlight. The proposed development will block winter sunlight, significantly affecting their performance.• The north-facing side of our home features full-length windows, where all living spaces—including the study, bedrooms, kitchen, dining, and lounge areas—rely on natural light. The development will deprive these areas of essential sunlight.• The construction will compromise our privacy, as neighbours will have direct visibility into our rooms. The pool area at the rear will also be shaded, reducing its usability and further affecting privacy. <p>Health and Well-being Concerns</p> <p>A major reason for purchasing this property was the health condition of my wife, Violette, who suffers from dementia (Alzheimer's). Exposure to ample natural light is vital for her well-being, and any reduction in brightness will have negative implications for her quality of life.</p> <p>Neighbourhood Aesthetics and Traffic Concerns</p> <ul style="list-style-type: none">• The street, stretching from Waratah to Phillip Road, is a charming and quiet residential area cherished by its residents. The scale of the proposed development is disproportionate to the character of this neighbourhood.• The introduction of a large, multi-dwelling complex will undoubtedly double or quadruple traffic on the street, making parking a nightmare and causing ongoing disruption. <p>Construction Disruption</p> <ul style="list-style-type: none">• The noise and extended construction timeline of a project of this magnitude would be intolerable, not only for us but for our neighbours as

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	<p>well.</p> <p>Planning and Classification Concerns</p> <p>The current planning application is for a multi-dwelling residence with 23 bedrooms and 14 bathrooms. We are concerned about the future potential for the building to be repurposed into a different classification, such as rental or commercial use, which would further degrade the neighbourhood environment.</p> <p>We respectfully urge the council to use its wisdom and authority to strongly oppose this development, recognizing the detrimental impact it will have on the community and the well-being of its residents.</p> <p>This letter has been written by concerned laypeople, not lawyers or town planners, but by future and passionate residents of Alexander Road, Dalkeith 6009. We sincerely plead with the council to uphold the integrity of our neighbourhood by rejecting this inappropriate development proposal.</p> <p>Sincerely,</p> <p>PS I have commissioned an Architect with expertise in overshadowing and how it affects my future property. I shall submit when I get that report</p>
12	<ol style="list-style-type: none">1. A building of this size out of character with the streetscape.2. Not only will there be a huge increase in the number of residents and their cars coming and going - but their visitors and tradesmen will all have cars. Where will they park? There is seldom a spare space along this stretch of road even now and driving along here can be hazardous as it is 2-way traffic and the road, as in most of Dalkeith, is narrow.3. A development of this size occupies most of the block with minimal landscaping, thus contributing to the increase in temperature of our suburb and indeed out city. Concrete and all the other building materials, absorb heat much more than greenery. Of course this is true for most of the new developments that have occurred. The original houses all had backyards and a front yard! There is enough population density here.
13	<p>As residents of Alexander Rd, I strongly object to the proposed apartment building at 6 Alexander Road.</p> <p>There are key technical aspects that do not comply with the current R80 code in the submission.</p> <p>Technical noncompliance.</p> <ol style="list-style-type: none">1) The plot ratio is stated as 18% greater than is allowed under the code, further the calculation in the proposal does not align to the information provided and suggests the plot ratio is likely to be at least 35% greater than is allowed.2) Setbacks in the proposal do not comply with the current R80 controls even though council has changed these under LPP 5.14 - has this LPP been approved by WAPC? If not, then the standing R80 setback of 3.5M must apply.

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3) The elevation of the adjoining property at 8A has been assumed at the same level as the proposed property at 6 Alexander despite there being a clear elevation change (higher) of at least 0.5m which will have a considerable impact to the overshadowing assessment. Does this have a material impact to the overshadowing analysis and therefore not comply with the ability to build a 4 story apartment block as it doesn't meet the overshadowing criteria. Planning guidelines suggest that deeper setbacks should be considered to mitigate the impacts of sloping blocks.

4) There is insufficient parking allowed for in the proposal for a dwelling with 23 bedrooms which are designed to accommodate up to 46 people in the apartments. This suggests that there could be a requirement for as many as 20 bays should the apartments be sold and repurposed for alternate single use accommodation.

Excessive Bulk and Scale

1) Alexander Road in its current built form is low rise development with 13 existing or under construction properties not exceeding 2 stories in the immediate vicinity of 6 Alexander Road. The proposed apartment block development at 13m does not fit with the prevailing streetscape and will considerably diminish the amenity of the street's residents.

2) The R Codes state - 'Apartment design should respond appropriately to the defined neighbourhood context and character'. 'New development should reflect prevailing streetscape patterns and include significant onsite landscaping to enhance the streetscape and provide amenity for residents and neighbour's'.

3) The previous ruling by Judge DR Parry (WASAT 41, March 2021) for a prior development application of 4 stories at 6 Alexander Rod stated. 'Because of their relatively recent construction, quality and value, the eight new single houses in the immediate locality of the site are unlikely to be demolished in the foreseeable future. The Tribunal observed that these unusual planning (subdivision and development) characteristics of the immediate locality of the site require greater sensitivity from new development - and greater sensitivity than is displayed in the proposed development - in terms of height, bulk and scale fronting the street than would otherwise be the case in an area up-coded to R80. The Tribunal found that the proposed development is discordant and incompatible in its height in terms of its immediate streetscape setting and presents to the street with excessive bulk and scale when viewed in its immediate streetscape context'.

3) The quiet streetscape composing of single dwellings would be significantly compromised with a single apartment building of the scale that is proposed given that the opportunity for any other buildings of this scale to offset its bulk are not readily available. This is not a transitional development in this area.

Revoking LPP 5.8

1) There is significant doubt as to whether the correct process has been followed under Planning and Development (Local Planning Schemes) regulations 2015 when revoking the LPP 5.8. No information was supplied directly to the residents that 5.8 applied to and was hard fought for in conjunction with Nedlands Council.

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	<p>2) The adoption of LPP 5.14 by council was not done with due consideration for the LPP 5.8 when it was bundled with 5 other precincts which again was not adequately publicised as per Clause 87 of the Planning and Development (Local Planning Schemes) Regulations 2015.</p> <p>For the above reasons this development application must be rejected by council.</p>
14	<p>"How many 8 bedroom, 8 bathroom single apartment dwellings has CoN approved for construction in the last decade? Is this ultra high density style of accommodation something that the CoN would encourage more of in future? Will this apartment end up a "white elephant" in which no one wishes to live, which no one wishes to buy, and which might then fall into disrepair?</p> <p>Is the 8x8 apartment on level 1 a "dwelling" as defined in the R-Codes ("...for the purpose of human habitation on a permanent basis by a single person, a single family, or no more than six persons who do not comprise a single family..."). It doesn't pass the pub test.</p> <p>The planning report states that the owner is Universal United Investments Pty Ltd. The report also says ""Dwelling G02 is to be provided as a caretaker dwelling, while all remaining dwellings will be occupied by members of the same family.""</p> <p>Has the specific family who is intending to dwell in the 8 x 8 Level 1 apartment made any binding commitment to reside in the dwelling for the long term?</p> <p>Can the City of Nedlands rule out a future approval of a non-dwelling use in the event that the family vacates the dwelling, and if the dwelling cannot be leased or sold due to market factors?</p> <p>The building's design, including 23 bedrooms and expansive communal living areas across the three upper floors, would be perfectly suited to purposes other than a "dwelling". Has Universal United Investments Pty Ltd made any binding commitment to not repurpose any of the 23 bedrooms in this building for a non-dwelling use? i.e. short stay accommodation?</p> <p>If the building was to change ownership, and if part or all of the 23 bedroom building was to be repurposed as a short stay accommodation or student accommodation, then is there sufficient parking to meet WA R-Code requirements for this type of building use?</p> <p>In the 1st traffic review, Table 1 states there are 4 dwellings with 2+ bedrooms, and 1 dwelling with 1 bedroom. Why was this incorrect traffic review submitted and posted on YOURVOICE?</p> <p>Did the traffic review consider that the building has 23 bedrooms in total, accommodating up to 46 people, when estimating parking requirements and traffic movements?</p>

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The revised traffic review was posted on YOURVOICE website 3 working days before the close of comments on the development application. Is this in accordance with Planning and Development Act 2005, Planning and Development (Local Planning Schemes) Regulations 2015? The revised traffic review Figure 4, does not take into account the large number of cars regularly parked along the eastern side of Alexander Rd due to the shopping precinct just 60m away from the driveway. A large number of traffic movements from the site, with visibility obstructed by parked cars, substantially increases the risk of vehicle collisions.

The revised traffic review estimates 25 trips per day. For a 23 bedroom building, which could accommodate up to 46 people, this doesn't pass the pub test. The report claims to be based on traffic movement volume methods in the NSW Roads and Maritime Services' "Guide to Traffic Generating Developments." This guide suggests: Daily Vehicle Trips: 10.7 trips per dwelling = 53.5 trips per day. Will City of Nedlands please calculate the percentage increase in traffic along Alexander Road as a result of this, and quantify the impact on the amenity of Alexander Rd?

If it is determined that LPP 5.8 has not been revoked in accordance with state planning regulations, and that in fact has always been in effect, then does the proposed building height contravene LPP 5.8?

If it is determined that LPP 5.14 has not been approved by the WA Planning Commission in accordance with state planning regulations, then would the proposed building side setbacks of 2m contravene the R-Code requirements for 3.5m setbacks?

Would CoN planners consider Alexander Rd as a "low rise" streetscape as defined in A2 of the R-Code (Vol.2)?

Section 2.1.1 of the R-Code states: "Apartment design should respond appropriately to the defined neighbourhood context and character". Do CoN Planners think this development responds appropriately to the "low rise" streetscape in Alexander Rd?

The planning report says the plot ratio is 1.18 however if I sum the stated floor areas in the plans (154.4 + 92.3 + 458.2 + 378.9 + 177.7 + 117.5) I get a building area of 1379 m², equating to a plot ratio of 1.36. This exceeds the R-Code AO by 36%!! What is the real plot ratio? The elevation drawings show a fall of approx. 0.5m from north to south across the block. BUT the elevation drawings show the same ground floor slab level for three buildings at 4B, 6, & 8A Alexander Rd. Are the overshadowing drawings correct, or do they underrepresent the actual overshadowing due to the slope of the hill? Have the overshadowing drawings / estimates been verified by a competent person acting for City of Nedlands?

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Table 2-2 of the 1st waste management plan states there are 3 dwellings with 3 bedrooms, and 1 dwelling with 5 bedrooms, and 1 dwelling with 7 bedrooms. Why was this incorrect waste management plan submitted and posted on YOURVOICE?

Did the waste management plan in any way consider that the building has 23 bedrooms in total, and up to 46 people residing in it, when estimating rubbish volumes and bin sizes / numbers?

The waste management plan mentions duties to be performed by the live-in caretaker on 6 occasions. The planning report mentions the caretaker's duties too. Has Universal United Investments Pty Ltd made any binding commitment to always have a live-in caretaker in the building doing these duties? Will the City of Nedlands make this an ongoing condition on the site?

The revised waste management plan was posted on YOURVOICE website 3 working days before the close of comments. Is this in accordance with Planning and Development Act 2005, Planning and Development (Local Planning Schemes) Regulations 2015?

The revised waste management plan does not mention a 6 bedroom apartment, but it does refer to a 5 bedroom apartment. The revised waste management plan appears to be incorrect. Why was this incorrect waste management plan submitted and posted on YOURVOICE a second time?

The revised waste management plan claims that only 200 litres per week of FOGO is produced. Considering the building has 23 rooms, and each room could easily accommodate 2 persons, so it could easily be housing up to 46 persons, does 200 litres of FOGO volume "...pass the pub test..."? The consequence of this gross underestimate of FOGO would be that a lot of FOGO will then be disposed of in the red bins, which is not sustainable. Does City of Nedlands have a vision for sustainability, supported by a consistently applied policy, or not?

Furthermore, the 2 weekly collection of red bins may result in rotting food waste in the red bins, impacting the amenity of the guests living in the apartments.

Level 3 has 2 "Pebble Roof" areas. Are these pebble roofs made of roofing materials, or made of reinforced concrete? If the pebble roofs are reinforced concrete then is it conceivable that a future owner of the building could convert the pebble roofs to balconies in future?

Have CoN planners considered the WASAT 41 ruling? Have CoN planners considered that 5 more two-storey houses will be built in Alexander Rd in 2025, which will further amplify the logic in the WASAT 41 ruling? i.e. within a year or so there will be 13 "...new single houses in the immediate locality of the site ..." with "... relatively recent construction, quality and value..."

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	<p>City of Nedlands has invested time and ratepayers' money in obtaining the WASAT 41 ruling in 2021. Will CoN apply this WASAT 41 precedent in 2025, and refuse this development application?</p>
15	<p>I am writing to OPPOSE/REJECT the 4-storey multiple dwelling development application at #6 Alexander Road, Dalkeith for the following reasons.</p> <p>Nedlands City Council rejected similar plans submitted by BHY back in April 2020. This decision was later upheld by the State Administrative Tribunal (SAT), which found the proposal was “incompatible with the surrounding homes because of its height and would overshadow the neighbouring properties”.</p> <p>SAT ruled that even though the street was zoned for mid-rise development (R80), neighbouring residence would be unfairly impacted because their houses were relatively new and unlikely to be redeveloped anytime soon. SAT found BHY’s proposed development would cause unacceptable overshadowing of #8A Alexander Road.</p> <p>The development application recently submitted by Suida International is very similar in its height, bulk and scale to the that submitted by BHY. The recent development application’s elevation is unsympathetic with existing street architecture. It does not protect, or enhance, existing amenity in a short, narrow street.</p> <p>Nedlands councilors approved a planning policy for the precinct around Alexander and Philip Roads that set a normal 10m height restriction. The council then revoked this decision (very quietly) in late August 2023 without public consultation, and without notifying affected residence in a transparent or open manner.</p> <p>The recently submitted multiple dwelling development application boasts 23 Bedrooms, 14 Bathrooms and an inappropriate/disproportional amount of parking, allowing for only 10 parking bays. It towers over its neighbours at a height of 13.6m. Furthermore, it has been applied for as multiple dwelling (an apartment block in laymen’s terms), which is totally out of character and does not align with the current elevations in Alexander Road. The similarities between the newly submitted Suida International development application and the previously rejected BHY development application are undeniably similar!!</p> <p>A dwelling of this magnitude will not only overshadow #8A Alexander Road, but generate noise pollution, traffic and parking congestion, lighting pollution, waste management and collection issues, and is completely out of character with the existing streetscape.</p>

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	<p>The building could easily be repurposed in the future for student accommodation or Air BB / Short Stay accommodation. Level 1 (2nd storey) has 8 bedrooms, each of which has its own ensuite. The layout is not dissimilar to a student accommodation block with studio apartments and communal living areas. Level 2 (3rd storey) has 6 bedrooms, each of which has its own ensuite. Again, the layout is not dissimilar to a student accommodation block. Furthermore, there is an error in the plans – the developer has stated that Level 2 is a "5 x 5.5" layout, but there are 6 bedrooms, each with an ensuite, plus a communal toilet.</p> <p>If the building was repurposed in the future (highly probable), then potentially 14 rooms could be used for the purpose of studio apartments. Considering the 2 ground floor apartments, plus the level 3 (4th storey) apartment, there could potentially be up to 17 apartments in total, occupied by 17 separate residents.</p> <p>The noise, light, waste, traffic, parking and impact to visual privacy would be enormous. 10 car parking bays are manifestly inadequate for the possible number of residents / apartments that this development application could potentially sustain if repurposed in the future.</p> <p>In summary I strongly oppose the proposed development as it is incompatible with the existing and desired streetscape and character of the street and surrounds. The bulk and scale is excessive and detrimental to neighbouring properties.</p> <p>I urge the council and planners to reject the proposed development, particularly as the SAT rejected a not dissimilar development application only recently.</p> <p>Thank you for the opportunity to comment.</p>
16	<p>I am writing to express my deep concerns regarding the proposed amendments to increase the height regulations for residential buildings within our community. While I understand the need for urban development and the benefits it may bring in terms of housing and economic growth, the repercussions of such a drastic change to our suburban landscape could be far-reaching and detrimental to the existing community.</p> <p>First and foremost, the infrastructure in our residential areas, particularly the roads are already under significant strain. On some days, it takes upwards of 25 minutes to cover the short distance to the Stirling Highway, a journey that should take a fraction of this time. This is a clear indication that our local roads are operating at or near their capacity, with minimal provisions for additional traffic.</p>

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Introducing denser housing units, through the construction of taller buildings, would inevitably lead to an increase in the local population. Our community's infrastructure, again specifically our roads, public utilities, and local schools are currently at capacity, and are not equipped to handle such a rapid increase in demand. The prospect of adding more residents without substantial upgrades to our infrastructure spells a recipe for crime, congestion, longer commute times, safety for the overall community and a strained public service system which would begin to lose its appeal to a vast majority of current community people and those looking to move to the area.

Moreover, increasing the population density within small residential suburbs could lead to various issues, including limited public spaces, reduced privacy, and the loss of the local character that makes our community unique and desirable. Additional strain on power and other utilities could also lead to increased costs for residents, both new and existing, further exacerbating the community's concerns.

While I recognise the importance of development and growth, I strongly believe that such changes need to be approached with caution and careful planning. High-density housing is undoubtedly important for our city's growth, but it should be strategically placed in areas equipped to handle the additional strain. Locations closer to major highways, with accessible public transport options, are far more suited to taller buildings and higher population densities.

I urge the council to reconsider the proposed height increases for residential buildings within our community. Instead, let us focus on sustainable development strategies that prioritise the well-being of existing residents, while still accommodating growth. This may include investing in infrastructure upgrades before any significant population increase or revising the proposal to limit higher-density buildings to areas better suited to the current accommodations, again, sustaining growth.

Thank you for considering my concerns. I hope we can work together as a community to find a solution that ensures the future prosperity of our suburbs without sacrificing the quality of life of its current residents.

Sincerely,

17

This proposed development is completely without merit and should be rejected out of hand. A similar proposed development was unanimously rejected in 2021 by Council and an appeal to SAT, heard by Judge Parry, was also rejected and his findings are equally applicable to this proposal.

Since this SAT decision there have been numerous further planned developments in Alexander Road, all being 2 storey residences of high value and design, rendering this application more discordant and incompatible in height and bulk in terms of the immediate streetscape

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	<p>setting. Further, there must be considerable doubt about the purpose of this 4 storey building with 23 bedrooms and adjoining ensuite bathrooms. The suggestion that it is to house a family is patently absurd. Also, what about the stress all these residents will place on parking and utilities such as water, sewerage, electricity, waste management and the number of bins placed in the street to be collected. This part of Alexander Road is short, narrow and already congested, with street parking at a premium due to customers accessing the shops in Waratah Avenue. All these extra residents will exacerbate an already difficult situation. Then we come to the issues of excessive plot ratio and inadequate side setbacks, overshadowing of adjoining properties, numbers 8A and 8B. These details have been fully explored by my neighbour, Rex Hubbard of 7A Alexander Road in his submission to you. Another issue is the matter of LPP 5.8. It would appear that there is considerable doubt that this issue has been handled in accordance with the requirements and consequently Alexander Road is still subject to a height limit of 10 meters. For all the above reasons, I urge you to reject this application.</p>
18	<p>This proposed development is completely without merit and should be rejected out of hand. A similar proposed development was unanimously rejected by Council in 2021 and an appeal to SAT, heard by Judge Parry, was also rejected and his findings are equally applicable to this proposal. Since this SAT decision there have been numerous further planned developments in Alexander Road, all being 2 storey residences of high value and design, rendering this application more discordant and incompatible in height and bulk, in terms of the immediate streetscape setting. Further, there must be considerable doubt about the purpose of this 4 storey building with 23 bedrooms and adjoining ensuite bathrooms. The suggestion that it is to house a family is patently absurd. Also, what about the stress all these residents will place on parking and utilities such as water, sewerage, electricity, waste management and the number of bins placed on the street to be collected every week. This part of Alexander Road is short, narrow and already congested, with street parking at a premium due to customers accessing the shops in Waratah Avenue. All these extra residents will exacerbate an already unacceptable situation. We then come to the issues of excessive plot ratio and inadequate side setbacks, overshadowing of adjoining properties, Numbers 8A and 8B. These details have been fully explored by my neighbour, in his submission to you. Another issue is the matter of LPP 5.8. It would appear that there is considerable doubt that this issue has been handled in accordance with the requirements and consequently Alexander Road is still subject to a height limit of 10 metres. For all of the above reasons, I urge you to reject this application.</p>

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19	<p>I live a few blocks away from 6 Alexander Road and have lived in Dalkeith for the last 10 years. My objection concerns include</p> <ol style="list-style-type: none">1. the parking, loading and traffic inconvenience. No 12 Philip Road is a multiple dwelling development; It has taken the developers a very long time to finish this project; it has turned Philip Road into a one lane road as one side of the road is used by the workers to park their vehicles. It's very hard to drive along here when two cars in opposite direction are coming at the same time.2. The building at this address is so very high and black in colour. It spoils the landscape in Dalkeith.3. Further, I believe the higher the building, there will be more noise and disturbances to the immediate neighbours.4. the surrounding houses will lose their privacy as the residents on the higher floor of the new development would be able look down in their front yard and back garden. <p>Thank you for your kind attention.</p>
20	<p>This development does not protect nor enhance our AMENITY at Alexander Road Dalkeith and is to the detriment of all residents in the area. Our homes are EAST - WEST which means the overshadowing is huge and this property would mean that there will NOT BE ANY SUNLIGHT in winter. Pls, refer to shadowing modelling done, the new owner of 8A Alexander Road from 10/3/25 when we are settling. This property does not comply with sunlight on solar panels, sunlight through all the windows which ALL face North. It does not comply with traffic laws and There is NO permanent parking in Alexander Road and only a few bays for a few hours as the Street is mainly a thorough fare to the cafes. It does not comply with plot ration which should be 1:1 The bulk is massive and similar to commercial buildings. It does not comply with Residential description at all and 23 bedrooms/14 bathrooms and 10 car bays is not appropriate for a small, narrow residential street. This property needs far more land than is available at #6. Waste management will be an eyesore filling the small pathway to the shops. In conclusion, I object to the proposed development on the basis of the above.</p> <p>The local government needs to refuse the development having regard for Clause 67(y) which was proved and quoted by Judge Parry when he rejected the proposal at SAT on 17/3/2021 . thank you for the opportunity to comment.</p>
21	<p>The proposed building is far too large for the block size and completely inappropriate for the streetscape . The plot ratio is out of all proportion to the R-Code as it appears to exceed it by approximately 36%.</p> <p>This part of Alexander Rd is very narrow and has limited street parking and carries a fair amount of through traffic . As a resident who has lived around the corner from Alexander Rd for 21 years , I can testify that it is always full of cars and caution needs to be applied when dealing with oncoming cars in such a confined area . The addition of multiple cars necessitated by the huge number of people who may be living in the proposed 22 bedrooms in the compound needs to be factored into the application.</p> <p>The overshadowing of the houses on each side of the proposed compound needs to be properly considered and the proximity to the boundary fences , which seems to be much closer than recommended. In particular the overlooking into the adjacent back gardens is of great concern regarding privacy .</p>

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	<p>It seems extraordinary that a few short years after the SAT ruled unfavourably on a similar development application on the same small plot of land ,we are back here fighting the same fight. The SAT ruled that the previous DA would unfairly impact the neighbours through overshadowing and lack of privacy and the same is true of the new DA . The only thing that is different is the name of the developer , otherwise all the issues in contention remain the same .</p>
22	<p>The proposed development is far too bulky and oversized for the site. Alexander Street is a small, very quiet, narrow street with only one and two story buildings in it, many of which are relatively new and in very good condition and which have been purpose designed and built in very recent years with the intention of conforming to planning policies imposed on those new home builders at that time. Alexander Rd has extremely limited street parking available for cars. There is currently very quiet pedestrian traffic comprising residents from surrounding nearby streets cycling and walking to the small cafes, shops and other services available nearby in Waratah Avenue, including the bus service in Waratah Avenue.</p> <p>The proposed development will significantly overshadow and overlook surrounding neighbours in Alexander and Phillip Roads. The proposed landscaping will not be able to protect or prevent this and indeed it is not designed to effectively do so- the largest trees proposed for screening the huge development are 2-3 lemon trees which will provide at best low and very limited greenery and certainly will not contribute to the maintenance of any kind of tree canopy in the street. The rest of the landscaping appears to consist of small, strappy plantings. The development's contribution to the suburb's image as an attractive green, garden suburb is non existent.</p> <p>The plan for accommodation of 22 bedrooms and 21 bathrooms in the building is unreasonable in such a small narrow quiet suburban street and will introduce the very real potential for excess noise and nuisance coming from the building as well as leading to significant congestion of cars of occupiers and their visitors parking in the narrow street. Given the nature of the site, the proposed development is unsuitable. The small recognition of instances of different architecture styles in existing Dalkeith buildings, such as large traditional family homes in wide major access roads like Victoria Avenue does not acknowledge or address the immediate housing context and limited street layout of Alexander Road where it is proposed to site this development.</p> <p>The assertion that the application is for the housing of members of the applicant's family may or may not survive once construction is completed. However, even if this assertion does persist beyond completion of construction and continue for some short time, it does not mean that the intention to house only family members will persist, that those family members can be controlled and will remain living in such a compound nor does it alter the fact that this size and design is equivalent to a strata title erection, a low-cost student accommodation or a boarding house with its attendant increased noise and traffic detractions and with the real potential to severely impact values of immediately surrounding real estate and the quality of the wider area's amenity. It is unsuited to be sited in Alexander Rd Dalkeith.</p>
23	<p>The Proposal is for a 4 storey multiple dwelling. the dwelling proposed much bigger and out of keeping with all other residents in Alexander Road (Road) and surrounding residential streets.</p>

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	<p>The Road is a very short and narrow road in that cars parking on the road create a single lane of traffic. There would be a significant increase in traffic coming into and out of Alexander Road the magnitude of which would exponentially multiply the more of the 21 bedroom (42 sleeping capacity) are occupied.</p> <p>The proposed premises is intended to include 5 premises with a total of 1211.6 square metres of living space on a 1012 square metre block. I consider this to be unacceptable and unreasonable.</p> <p>No explanation is provided as to why so many bedrooms are required for 5 dwellings. An average of over 4 per dwelling. That number of bedrooms suggest to me that the development is for a purpose other than individual dwellings. This number of bedrooms does not reflect the bedrooms provided for in other multiple dwelling developments in Dalkeith or elsewhere.</p> <p>The premises is proposed to have 8 carparks for residents. Yet if there were to be 42 residents then the resident carparking provided would be grossly inadequate and result is a significant amount of street parking which would create significant traffic management issues. I note that the report prepared by Lateral Planning dated 6 December 2024 refers to traffic management but does not provide any detail of their analysis or the assumptions upon which they were based. There is also does not appear to be any proper consideration of the car parking and traffic implications of people visiting one or more of the potential 42 residents at the premises other than by reference to the 2 visitor car bays proposed.</p> <p>Such a large premises would also appear to create over-shadowing issues for the immediate neighbours.</p> <p>My wife and I strongly oppose the proposed development on many grounds including those set out above. This submission is not as complete as it should be do to the limited time available from when we became aware of the proposal and when the submission was required to be filed.</p>
24	<p>We are writing to strongly oppose the proposed Multiple Dwelling Development at 6 Alexander Rd for the following reasons which we will elaborate on below:</p> <ol style="list-style-type: none">1. Concerns about intended use,2. Significant bulk and scale,3. Oversized plot ratio,4. Reduced side setbacks,5. Significant overshadowing,6. Issues with overlooking and visual privacy,7. Insufficient car parking,8. Impacts to local traffic,9. Insufficient waste management,

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10. WASAT 41 Judgement,
11. It goes against the aims of the Nedlands Local Planning Scheme No.3,
12. And it adversely impacts on the amenity of adjoining landowners.

Concerns about intended use:

Looking at the plans, it is difficult to determine the intended use of this building. It has 23 bedrooms, 22 bathrooms, and other living areas which do not align with a typical family home, even if it was to include extended family. The development looks more like a hotel or student accommodation which would be out of place on Alexander Rd, which is completely inappropriate and unnecessary for this area and would go against the R-80 codes in that it must be entirely residential (commercial use is valid for R-AC1 and R-AC3 developments).

We are concerned about the number of people who may live in or use this development and how that may change over time if this is built.

Bulk and scale:

All new developments should respond appropriately to the defined neighbourhood context and character, and should reflect the prevailing streetscape. As Alexander Rd is comprised of recently built 2-storey houses, the construction of 4-storey apartments would be ridiculously out of context and character for the street. The height of the proposed design is 13.6m, significantly higher than the 10m height cap that is imposed for Alexander Road.

Plot ratio:

It appears plot ratio has been miscalculated and misrepresented. If you sum the floor areas stated on plans plot ratio is 1.36. Whereas the allowed plot ratio is 1.00. This exceeds the R-codes by 36%.

Side setbacks:

With a building length of 36m side setbacks should be 3.5m according to the R-codes, however side setbacks are only 2m on the ground and first floor, and 3m on the third floor which is completely inadequate.

Overshadowing:

The amount of overshadowing to the southern neighbours appears to be miscalculated because the slope of the road falls to the south, but this is not shown on the elevation. Please refer to shadow modelling commissioned – which shows that there will not be any sunlight to his property in winter, due to the monstrosity proposed at 6 Alexander. This is completely unacceptable.

Overlooking and privacy:

There is no privacy screens on the level 3 gym which overlooks the backyard of 8a Alexander Rd. Concerns pebble roofs could be converted

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to balconies in the future.

Car parking:

There is not adequate parking provided for the total number of bedrooms, and number of people that could reside there - it could house up to 46 people! And if the building was repurposed as student accommodation or short stay accommodation then a total of 21.3 car bays would be required.

Traffic:

Alexander Rd is a short road around the corner from the shops, and is already extremely busy, with cars constantly parked along the side of the road. Existing residents already find it difficult to safely exit their driveways, and have a visual line of sight to oncoming traffic. Visitors of existing residents find it difficult to get a park in the street, and often have to park in Philip Rd adding to congestion there.

Alexander Rd gets a lot of through traffic, with people going to or from the shops or to school, there are already significant issues with cars traversing the street.

The revised traffic review provided by the proponents grossly underestimates the number of trips per day for the potential number of residents that could reside at the new development. This will result in significantly higher traffic, mixed with heavy street parking will be more dangerous for pedestrians/cyclists/ other residents/ and motorists.

Waste Management:

The development would be entitled to 5 red, 5 green, and 10 yellow bins. On yellow bin days, 15 bins would be out on the verge. Where would they fit especially considering there will be cars parked on the road blocking access for the rubbish truck?

WASAT 41 Judgement:

Judge Parry's previous judgement (WASAT41, 17 March 2021) when he assessed a different 4 storey building proposed for 6 Alexander Rd, still holds true and applies to this new proposed development:

"Because of their relatively recent construction, quality and value, the eight new single houses in the immediate locality of the site are unlikely to be demolished in the foreseeable future. The tribunal observed that these unusual planning (subdivision and development) characteristics of the immediate locality of the site require greater sensitivity from new development – and greater sensitivity than is displayed in the proposed development – in terms of height, bulk and scale fronting the street than would otherwise be the case in an area up-coded to R80. The Tribunal found that the proposed development is discordant and incompatible in its height in terms of its immediate streetscape setting and presents to the street with excessive bulk and scale when viewed in its immediate streetscape context."

Not only that, but since his judgement was made, another two storey house has come under construction, two more two-storey houses are

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about to be built, and another two two-storey houses have been proposed. Thus, thirteen two storey houses will make up the locality, adding increased weight to Judge Parry's previous decision.

The Aims of the Nedlands Local Planning Scheme No.3, and the Proposals Negative Impact on Amenity:
The proposed development does not meet the aims of the Nedlands Local Planning Scheme No.3:

- a) "Protect and enhance local character and amenity
- b) Respect the community vision for the development of the district
- c) To ensure development maintains compatibility with the desired streetscape in terms of bulk, scale, height, street alignment and setbacks of street"

Amenity is defined under Local Planning Schemes and includes the "liveability, comfort, or quality of a place which makes it pleasant and agreeable to be in for individuals and the community. Amenity is important in the public, communal and private domains and includes the enjoyment of sunlight, views, privacy, and quiet. It also includes protection from pollution and odours."

The proposed development at 6 Alexander is inappropriate and does not meet the aims of the Nedlands Local Planning Scheme.

- The bulk and scale of the proposed development is inconsistent with and unsympathetic to its setting and the desired character and amenity of the area.
- The proposed development unreasonably impacts the amenity of the neighbouring properties in respect to overlooking and overshadowing due to its excessive bulk and scale, and lack of boundary setback on its southern side.

We respectfully request that Nedlands Council and Planning reject the current development application for 6 Alexander Rd. The proposed development is non-compliant and is completely inappropriate to the character and context of this street and locality.

We thank you for considering our submission.

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I object to the proposal for the following reasons:

Plot ratio permitted for R80 density is 1.0:1, proposed plot ratio 1.18:1 is non-compliant; & proposed height over 13m. is excessive.

Designated use is for one multi-generational family, comprising one 3 bedroom apartment for live-in caretaker, & four apartments for family, ranging in size from 3 bedrooms to 8 bedrooms (each with ensuite bathrooms) communal areas & powder room, & multiple amenity areas at top level of building including outdoor kitchen & pool area. The proposed use would require regulation & oversight by City of Nedlands to ensure compliance; & its design with more than 20 bedrooms & bathrooms, including an 8 bedroom 8.5 bathroom apartment would potentially accommodate more than 40 people on the single lot.

Multi-storey building likely to require upgrade &/or new infrastructure, as sewage & water services date from 1930s.
Landscaping plan does not mention sewer line along eastern boundary, which will influence choice of species & any structures proposed.
Survey plan is dated 2019, & fence on that boundary has been replaced.

Overlooking neighbouring properties will impact neighbours' privacy & activities at top level outdoor area should have be subject to noise & lighting controls, & also hours of operation, & similarly the fire pit in rear garden, which will have to comply with fire regulations.

Over shadowing neighbouring properties, especially nos. 8 & 8A negatively impacts solar access & residents' amenity.

Waste management will require more than 1 FOGO bin for number of residents & bin collection from Alexander Rd. is likely to pose problems.

Parking for residents & visitors is inadequate, & this short stretch of street is often fully parked by 9am.

Since early 2000s, consequent to increases in zoning from R10 & R12.5, & in this designated Alexander Rd. sub-precinct, new one & two storey houses have been built, are under construction &/or planned, consistent in height with older well maintained family homes in this area, & enhance the streetscape & the overall amenity of the area.

Proposed development is non-compliant with R-Codes & is inappropriate in Alexander Rd. sub-precinct, as it does not fulfil criteria of City of Nedlands LPP5.14, which states 'New development in the Alexander Rd. sub-precinct shall acknowledge the existing pattern of narrow lot development and provide consistency of development along the streetscape.'

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	<p>In March 2021, after SAT assessed a proposal for an apartment building at 6 Alexander Rd., the Tribunal found it was 'discordant and incompatible in its height in terms of its immediate streetscape setting and presents to the street with excessive bulk and scale when viewed in its immediate streetscape context.' This finding is applicable to the current proposal & it should be upheld.</p>
26	<p>Proposal is not a typical residential building but rather a 'community' with a large number of bedrooms (22) and bathrooms (21), clearly designed for more occupants than is expected or desirable in this residential area.</p> <p>The four storey complex is in appropriate for Alexander Street with only one and two storey homes. Neighbouring residences would be unfairly impacted for this development given their houses are relatively new and unlikely to be redeveloped anytime soon, as evidenced by the precedent setting SAT decision. The nominal 10 m limit for the precinct approved by Council should be retained.</p> <p>Deep soil planting areas (page 28/32 Landscape Report) are typically narrow and therefore unsuitable for large vegetation. These narrow areas that do not have large vegetation proposed should be removed from the deep soil planting area calculation.</p> <p>Planning Report - Minimum set back 5.5 m does not meet 6.0 m requirement. 1st storey set back 2 m does not meet 3.0 m requirement.</p> <p>Design Report - page 8 R80 built form ignores that many houses in this zone are relatively new and unlikely to be redeveloped anytime soon.</p> <p>4th storey not justified and overshadowing of adjacent properties is substantial.</p> <p>Traffic Report - Calculation of 1.25 bays per dwelling is unrealistic given the large number of occupants in each dwelling. Therefore the number of bays is undersized and there will be vehicles parking on the street to the detriment of neighbours.</p>
27	<p>I wish to comment on the above application. I believe it is too large for the site and is inappropriate for the area. A two storey building would be in keeping with other developments in the area. I realise that there is larger and higher developments along Waratah ave but this is a urban side street and not a major road. I note there is no provision for visitor parking which will result in a short street in which parking is a premium will only make parking worse. These factors will result in a diminution of the amenity of the street.</p>
28	<p>We own and live near the proposed development. We are one of eight recently built double storied homes in close proximity to the proposed build. The proposed build at 6 Alexander Rd will severely diminish our property value and lifestyle, for the following reasons; The bulk and scale is out of keeping with neighbouring properties, many of which have only been recently built.</p> <p>It will severely overshadow nearby homes.</p> <p>Exceeds the longstanding 10m height limit (only recently changed under questionable circumstances) The large number of bedrooms would indicate high occupancy, causing traffic and parking problems for the street.</p> <p>Present or future owners could seek to change the purpose of the building, to student accommodation etc,</p>

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	Trusting you can prevent this out of character development progressing any further, spoiling our street.
29	See attachment 1
30	See attachment 2
31	See attachment 3
Comments only	
32	<p>The plans submitted for 6 Alexander Rd, Dalkeith seem to have only minor issues to resolve. The Design Review Panel is pretty OK with the design so it comes back to a technical assessment of the planning issues. R Codes Volume 2 provides for an R80 block to have 4 storeys and a 1.0 plot ratio. So height itself is within bounds. The question is whether 1.18 plot ratio is too much. A good guide is to estimate the value of this by calculating 18% of the block value. If the block is worth \$2.5 million (my guess), the value would be \$450,000. Does the property rate this bonus which comes from the reduction of community amenity? I don't know but it seems a bit much to me.</p> <p>The other issue is that the design is uniquely to accommodate one family group, with the smallest unit being allocated to a caretaker manager. This looks like a great idea for them, snuggled around the corner from Waratah Village. But when they die, what becomes of the usage of the property? Are there any concerns that need to be addressed.</p>
Support	
33	Pleasing to see a traffic impact report.
Late Submissions	
34	<p>I am emailing in regards to 6 Alexander Road, Dalkeith, the application to City of Nedlands and DAP for a development, I live and own 3 Alexander Road</p> <p>I have viewed the amended proposal and have concerns regarding the proposed variations. I wish to make a submission, the details of which are as follows;</p> <ul style="list-style-type: none"> • It is a 4 storey apartment block in a street of entirely 1 and 2 storey homes. The WASAT/41 ruling specifically lays out why such a large apartment block is "...discordant and incompatible in its height in terms of its immediate streetscape setting...". Since the

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	<p>WASAT/41 ruling, 5 more houses in the street are either under construction, or are approved and about to start construction. This larger number of new homes of "...<i>relatively recent construction, quality and value...</i>" amplifies the WASAT/41 ruling.</p> <ul style="list-style-type: none">• Its bulk and scale is disparate with the existing built form of the recent builds in the street, contrary to R-Code recommendations to consider the streetscape. The R-Codes state that: "<i>Apartment design should respond appropriately to the defined neighbourhood context and character.</i>"• Its plot ratio is 1.18, which exceeds the R-80 primary controls for plot ratio by 18%.• It grossly overshadows the living areas in the neighbouring property and destroys their amenity.• It has just 8 resident car parking bays and 2 visitor bays in a street with very limited parking available.• There is a 5:1 ratio of red bins to green bins which is at odds with other dwellings in Nedlands where there are more green bins than red bins.• The property has 21 double bedrooms. A single apartment has 7 bedrooms. The developer claims that the apartments are "dwellings", however the design of the building is more consistent with a "use not listed" category (e.g. short stay accommodation, or student accommodation) .
35	<p>We have reviewed the amended proposal and wish to make a submission detailing our grave concerns and why we are against approval ;</p> <p>It is a 4 storey apartment block that is discordant and incompatible in its height in terms of its immediate landscape setting .</p> <p>It is inconsistent with the raft of new houses in the street that are either approved or under construction that are 2 storey and when complying receive support , add value and quality to the streetscape and community</p> <p>We understand the R-Codes state that apartment design should respond appropriately to the defined neighbourhood context and character and we are at a loss to understand how the bulk and scale of this revised proposal might do so.</p> <p>We are advised that its plot ratio exceeds R-80 primary controls for plot ratio by 18% (thats not even close !) and very obviously it grossly overshadows the living areas in neighbouring property's destroying their amenity.</p> <p>In a street with limited parking it has just 8 resident car bays and 2 visitor bays for a proposed 21 double bedroomsstaggering , where are all the extra cars going to go ?</p> <p>Which invites us to consider what the real purpose of this design is ...a 7 bedroom single apartment and 21 bedrooms overall is more</p>

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	<p>consistent with a "use not listed " such as short stay accommodation or hotel accommodation or student accommodation even in the style of a doss house .</p> <p>Inserting perhaps 21 cars and we imagine that could fluctuate between something less and up to 42 if all 21 bedrooms are occupied by couples , into such a small street invites exponential increases in stress and danger let alone a loss of amenity . Our lived experience of 28.5 years - which a traffic expert would never observe - is that the Philip Road /Alexander Road/Alexander intersection is already dangerous , being an offset crossing with no give way signs . People with limited vision , coffee in hand or distracted by phones regularly drive through it and near misses are de rigueur to us as residents who drive through that intersection 5 to 10 times a day .</p> <p>With existing 2 story dwellings built to compliance and others underway , it appears that approval of this proposal creates the environment for what might be circa 30 existing resident cars to increase to possibly circa 72 cars in what is about a 120m street with existing parking restrictions in place. In fact, walking up the street we calculate with current parking restrictions only about 8 cars can legally be parked on Alexander Road between Philip Road and the Warratah Ave roundabout . We are sure Council has its own way of doing maths but even at 4.5m of street space per vehicle a possible 42 new cars - 8 resident car bays introduces the risk of thrusting perhaps 34 additional vehicles into this section of the street . This suggests approval is simply dangerous and likely to diminish social cohesion where responsibility for overcrowding the little street with cars escalating those risks resides with those creating the risk .</p> <p>We find it at a minimum odd that there appears to be a ratio of 5 red to 1 green bin in a City that prides itself on being appropriately green and has more green bins than red bins generally as we advised . The overwhelming red bin ratio supports the points above as to the real intentions of the design . Transient occupants are likely to need more red bins than green.</p> <p>Perhaps such a design sitting next to the university might possibly have some utility but to impose it on a well functioning respectful neighbourhood community that is living in a war zone of external opportunists where we are constantly having to point out failings of proposals is diminishing our amenity and the freedom to enjoy our lives as long term residents/rate payers .</p> <p>We trust that the proposal is rejected and that the local community's reasonable positions is appropriately taken on board .</p>
36	<p>As a Philip Road resident I wish to register my objection to the proposed development at 6 Alexander Road.</p> <p>It is totally out of sync with the street and current dwellings .There are so many negatives to the proposal aesthetics, parking ,</p>

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overshadowing and the list goes on..The fact that 21 bedrooms are planned is so ridiculous ...this could never be regarded as private family residence.

I can only hope that council and common sense prevail to prevent approval of this development.

Advertising 2

Objection

1 Unfortunately, the amended proposal does not address mine and my wife's concerns about the proposed development.

We accept option 2 and maintain our initial objections and make the following additional observations in relation to the amended proposal.

GENERAL OBSERVATIONS

The development, as amended, proposes the construction of a four storey complex comprising 5 separate premises with 21 double bedrooms and 20 bathrooms.

The number of bedrooms and bathrooms raises questions as to the intended nature of the occupancy of the 5 premises. We are unaware of any high density living complexes including 5 and 7 double bedroom/bathrooms in individual apartments. There was no specific explanation for including sleeping capacity for 10 and 14 people respectively in those apartments in the development proposal that we could see.

The proposed size of the development clearly gives rise to justifiable concerns about general amenity of the area, plot ratio, overshadowing etc.

However, the principal focus of our further objection relate to matters addressed in the amended Traffic Technical Note (Note).

Off-Street Parking Supply

The Note refers to the minimum off-street parking facilities to be provided as 1 parking bay for 1 bedroom apartments and 1.25 bays for 2+ bedroom apartments as per RDC2.



The first point to be made is the RDC2 standard referred to is the bare minimum and refers only to 2+ bedroom apartments. It does not address the fact that this proposed development includes 5 and 7 double bedroom apartments.

Interestingly the Australia Standard for parking facilities AS2890.1 suggests that best practice is for there to be 1 parking bay provided for each bedroom in residential apartments.

We note that the RDC2 requirement is for 1.25 bays for 2 bedroom apartments. That being the case then a 7 bedroom apartment should have 3×1.25 bays = 4 bays. A similar uplift should apply to the other apartments in the proposed development.

STREET CONGESTION

The Note does not, in our view, properly or realistically address the traffic generation which could or is likely to be caused by this development.

In the very short consideration of this issue the Note predicts that there would only be 25 vehicle trips per day relating to the development. This assessment appears to be based upon a review of traffic movements at minimum density flats. We do not consider that this is a valid calculation for the development which includes 21 double bedrooms and could accommodate 42 car owning and driving adults.

If it is assumed that only half the maximum occupancy of the development will drive motor vehicles then the assessment of 25 trips per day would effectively translate to only one trip per driver per day which is clearly improbable.

Further the assessment of traffic congestion in the Note does not address the number of visitors who are likely to attend the premises each day and the trips that they make.

The proposed development includes the provision of 2 visitor off-street parking bays which is woefully inadequate where it is proposed there will be 5 separate residential apartments which could accommodate 42 adults.

Visitors who are unable to park in either of the proposed two off-street parking bays will need to park in the street.

Alexander Road is already congested with street parking by those attending the retail establishments on Waratah Avenue.

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	<p>When any car parks on Alexander Road it results the road becoming a single lane but would be totally blocked if cars parked on both sides of the road.</p> <p>It is important to appreciate that the round-about was constructed at the end of Alexander Road coming on to Waratah Avenue to the dangers which have been experienced there in the past when there was much less traffic than will be the case if the development will proceed.</p> <p>VEHICLE SIGHT DISTANCE</p> <p>The Note addresses vehicle sight distance at paragraph 2.2. The analysis in the Note appears to be based upon the assumption that there will be no cars parked on the road. In our experience, during working hours there is almost always cars parked on Alexander Road.</p> <p>That street parking would only increase if the development was to proceed which would create a significant increase in the potential for motor vehicle accidents in the area which has not been addressed in the Note.</p> <p>Please keep us informed of the developments in relation to this developments so as to ensure that all objections to it are properly considered and able to be reviewed.</p>
2	<p>I have viewed the amended proposal and have concerns regarding the proposed variations. Our previous submission applies unchanged to the amended set of plans but would like to add the following additional comments.</p> <ul style="list-style-type: none">• It is a 4 storey apartment block in a street of entirely 1 and 2 storey homes. The WASAT/41 ruling specifically lays out why such a large apartment block is "...discordant and incompatible in its height in terms of its immediate streetscape setting...". Since the WASAT/41 ruling, 5 more houses in the street are either under construction, or are approved and about to start construction. This larger number of new homes of "...relatively recent construction, quality and value..." amplifies the WASAT/41 ruling.• Its bulk and scale is disparate with the existing built form of the recent builds in the street, contrary to R-Code recommendations to consider the streetscape. The R-Codes state that: "Apartment design should respond appropriately to the defined neighbourhood context and character."• Its plot ratio is 1.18, which exceeds the R-80 primary controls for plot ratio by 18%.• It grossly overshadows the living areas in the neighbouring property and destroys their amenity.• It has just 8 resident car parking bays and 2 visitor bays in a street with very limited parking available.• There is a 5:1 ratio of red bins to green bins which is at odds with other dwellings in Nedlands where there are more green bins than red

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	<p>bins.</p> <ul style="list-style-type: none">• The property has 21 double bedrooms. A single apartment has 7 bedrooms. The developer claims that the apartments are “dwellings”, however the design of the building is more consistent with a “use not listed” category (e.g. short stay accommodation, or student accommodation) .
3	<p>With regards to the above, I have viewed the amended proposal and have concerns regarding the proposed variations.</p> <p>My previous submission is still the same and I object to the proposed variations of the 4 storey apartment block:</p> <ol style="list-style-type: none">1. It does not complement the neighbourhood character and landscape.2. Its plot ratio is outside the current R-80, making it bigger than allowed.3. It's will block out sunlight to existing houses in the neighbourhood.4. The 21 double bedrooms suggest a lot of movement in and out of the building. Alexander road is a small quiet road and parking is limited .
4	<p>We wish to object in the strongest possible terms to this proposed development.</p> <p>The sheer scale, bulk and height of the building is completely out of place in Alexander Road, which is a relatively narrow street. The number of bedrooms and bathrooms proposed are not consistent with a single residential building or home. The proposal does not protect or enhance in any way the streetscape, character, or amenity of the neighbourhood. The size and bulk of the proposed building would cause massive overshadowing issues to the Southern neighbour. Increased density zones in existing residential areas that consist mainly of 2 storey dwellings should have a maximum height limit of 10 metres or 2 storeys.</p> <p>There are numerous examples of recent developments that increase density that have been well executed and fit into the existing streetscape, all of the East side of Adelpa Road between Gallop Road and Waratah Avenue is a case in point. All 2 storey homes with 4, 5 or 6 homes where there was once a single residence, and all with appeal to both families moving in to the area or downsizers who wish to have a smaller easy care block.</p> <p>We have no objection to an increase in housing density when it is done sensitively and with respect to the existing residents, housing stock and neighbourhood, but the proposed development at 6 Alexander Road does not seem to consider this in any way.</p> <p>Once again we would like to register our absolute disapproval of the current plans.</p>

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We wish to object in the strongest possible terms to this proposed development. The sheer scale, bulk and height of the building is completely out of place in Alexander Road, which is a relatively narrow street. The number of bedrooms and bathrooms proposed are not consistent with a single residential building or home. The proposal does not protect or enhance in any way the streetscape, character, or amenity of the neighbourhood. The size and bulk of the proposed building would cause massive overshadowing issues to the Southern neighbour. Increased density zones in existing residential areas that consist mainly of 2 storey dwellings should have a maximum height limit of 10 metres or 2 storeys.

There are numerous examples of recent developments that increase density that have been well executed and fit into the existing streetscape, all of the East side of Adelma Road between Gallop Road and Waratah Avenue is a case in point. All 2 storey homes with 4, 5 or 6 homes where there was once a single residence, and all with appeal to both families moving in to the area or downsizers who wish to have a smaller easy care block.

We have no objection to an increase in housing density when it is done sensitively and with respect to the existing residents, housing stock and neighbourhood, but the proposed development at 6 Alexander Road does not seem to consider this in any way.

Once again we would like to register our absolute disapproval of the current plans.

We wish to object in the strongest possible terms to this proposed development.

The sheer scale, bulk and height of the building is completely out of place in Alexander Road, which is a relatively narrow street.

The number of bedrooms and bathrooms proposed are not consistent with a single residential building or home.

The proposal does not protect or enhance in any way the streetscape, character, or amenity of the neighbourhood.

The size and bulk of the proposed building would cause massive overshadowing issues to the Southern neighbour.

Increased density zones in existing residential areas that consist mainly of 2 storey dwellings should have a maximum height limit of 10 metres or 2 storeys.

There are numerous examples of recent developments that increase density that have been well executed and fit into the existing streetscape, all of the East side of Adelma Road between Gallop Road and Waratah Avenue is a case in point. All 2 storey homes with 4, 5 or 6 homes where there was once a single residence, and all with appeal to both families moving in to the area or downsizers who wish to have a smaller easy care block.

We have no objection to an increase in housing density when it is done sensitively and with respect to the existing residents, housing stock

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	<p>and neighbourhood, but the proposed development at 6 Alexander Road does not seem to consider this in any way.</p> <p>Once again we would like to register our absolute disapproval of the current plans.</p>
5	<p>Thank you for your email dated 31/03/2025 regarding the development application for multiple dwellings at 6 Alexander Road, Dalkeith. Following review of the amended plans we continue to have grave concerns about the proposed development and therefore agree with option 3.</p> <p>Option 3 I have viewed the amended proposal and have concerns regarding the proposed variations. My previous submission applies unchanged to the amended set of plans.</p> <p>Please see additional considerations below:</p> <p>We are long term residents of 25 Philip Road since 1997 and do not support a development of this scale on Alexander Road, between Philip Road and Waratah Avenue.</p> <p>Our previous submission to City of Nedlands expressed our concerns:</p> <ul style="list-style-type: none">• the bulk and scale of the proposed 4 storey apartment block is not in keeping with other existing dwellings on Alexander Road• it grossly overshadows neighbour's properties• the large number of bedrooms (21 double bedrooms) and proposed inhabitants (one apartment proposed to have 7 bedrooms) would indicate use other than residential; for example short stay or student accommodation• number of parking bays is grossly inadequate for the potential number of residents• there is worsening traffic congestion in Alexander Road and surrounding streets. We have already witnessed a number of near accidents on our corner of Philip and Alexander Road due to traffic congestion and the thoroughfare through this intersection. Traffic flow and parking congestion associated with such a development will significantly exacerbate this issue. <p>We do not believe the revised plans adequately address our concerns and we reject the proposal.</p> <p>We appeal to the City of Nedlands to reject this development application for 6 Alexander Road, Dalkeith which is totally inappropriate and out of character for this location.</p>

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6	<p>Option 2 I have viewed the amended proposal and have concerns regarding the proposed variations. I wish to make a new submission, the details of which are as follows:</p> <ul style="list-style-type: none">• It is a 4 storey apartment block in a street of entirely 1 and 2 storey homes. The WASAT/41 ruling specifically lays out why such a large apartment block is "...discordant and incompatible in its height in terms of its immediate streetscape setting...". Since the WASAT/41 ruling, 5 more houses in the street are either under construction, or are approved and about to start construction. This larger number of new homes of "...relatively recent construction, quality and value..." amplifies the WASAT/41 ruling.• Its bulk and scale is disparate with the existing built form of the recent builds in the street, contrary to R-Code recommendations to consider the streetscape. The R-Codes state that: "Apartment design should respond appropriately to the defined neighbourhood context and character."• Its plot ratio is 1.18, which exceeds the R-80 primary controls for plot ratio by 18%.• It grossly overshadows the living areas in the neighbouring property and destroys their amenity.• It has just 8 resident car parking bays and 2 visitor bays in a street with very limited parking available.• There is a 5:1 ratio of red bins to green bins which is at odds with other dwellings in Nedlands where there are more green bins than red bins.• The property has 21 double bedrooms. A single apartment has 7 bedrooms. The developer claims that the apartments are "dwellings", however the design of the building is more consistent with a "use not listed" category (e.g. short stay accommodation, or student accommodation) .• The property requires a caretaker to manage waste. Who will this be and how are they funded?
7	<p>We would like to notify you that we have looked at the amended proposals for the still enormous and completely inappropriate commercial size development at 6 Alexander Rd , Dalkeith and wish to nominate Option 3 .</p>
8	<p>Option 3 I have viewed the amended proposal and have concerns regarding the proposed variations. My previous submission applies unchanged to the amended set of plans.</p> <p>I also wish to enhance my previous submission with the following objections:</p>

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	<p>Option 2 I have viewed the amended proposal and have concerns regarding the proposed variations. I wish to make a new submission, the details of which are as follows:</p> <p>My summary objections to the development are:</p> <ul style="list-style-type: none">• It is a 4 storey apartment block in a street of entirely 1 and 2 storey homes. The WASAT/41 ruling specifically lays out why such a large apartment block is "...discordant and incompatible in its height in terms of its immediate streetscape setting...". Since the WASAT/41 ruling, 5 more houses in the street are either under construction, or are approved and about to start construction. This larger number of new homes of "...relatively recent construction, quality and value..." amplifies the WASAT/41 ruling.• Its bulk and scale is disparate with the existing built form of the recent builds in the street, contrary to R-Code recommendations to consider the streetscape. The R-Codes state that: "Apartment design should respond appropriately to the defined neighbourhood context and character."• Its plot ratio is 1.18, which exceeds the R-80 primary controls for plot ratio by 18%.• It grossly overshadows the living areas in the neighbouring property and destroys their amenity.• It has just 8 resident car parking bays and 2 visitor bays in a street with very limited parking available.• There is a 5:1 ratio of red bins to green bins which is at odds with other dwellings in Nedlands where there are more green bins than red bins.• The property has 21 double bedrooms. A single apartment has 7 bedrooms. The developer claims that the apartments are "dwellings", however the design of the building is more consistent with a "use not listed" category (e.g. short stay accommodation, or student accommodation) .
9	<p>We would opt for Option 3.</p> <p>This development has no merit and no place in our street.</p> <p>It is obviously not a residential development, with its ridiculous number of bedrooms and lack of parking facilities.</p> <p>It is excessive in bulk, height and scale, and overshadows number 8A and 8B.</p> <p>It exceeds the plot ratio.</p>

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	<p>A similar development was rejected by Judge Parry at S.A.T. And his findings are damning to this project.</p>
10	<p>My wife and I have carefully considered the amendments set out below. They do not address, adequately, the issues set out in our earlier submission. In particular, the plot ratio continues to exceed the requirement by 18%.</p> <p>We therefore nominate option 3.</p>
11	<p>Thank-you for the updated plans for 6 Alexander Rd.</p> <p>As a near-by neighbour, we remain totally opposed to the amended plans, which only marginally address our initial concerns;</p> <p>The issues of bulk, scale, parking, potential other uses for the building, have not changed.</p> <p>The impact on property values of recently constructed neighbouring properties have not changed.</p> <p>We therefore nominate OPTION 3 as our response.</p>
12	<p>My option is #3. Please see below for further comments.</p> <p>Option 3 I have viewed the amended proposal and have concerns regarding the proposed variations. My previous submission applies unchanged to the amended set of plans.</p> <p>The amendments that the applicant has submitted do NOT sufficiently address the issues in my original objection, therefore my original objection still applies.</p> <p>The applicant has made inconsequential amendments to appear as though she is addressing my objections, but her amendments significantly fall short of producing anything meaningful with respect to the neighbouring properties; most impacted being #8A & #8B Alexander Road.</p> <p>The applicant's amendments are still NOT compliant with the residential R codes and do not address the negative impact to surrounding</p>

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	<p>amenities and neighbouring properties.</p> <p>My original objections still apply.</p>
13	<p>Thank you for the information. I concur that option 3 reflects our position on this development. The parking available and the access to that parking is a woefully inadequate for the number of likely residents.</p> <p>The design provides 12 bedrooms (which look like doubles) in only two of the dwellings and this makes the provision of 2.5 parking spaces laughable. We are still seeking an assurance these floors are not being set up as a care facility or student accommodation</p>
14	<p>I have had a quick look at the revisions and see the minor modifications made. As I am not a close neighbour who is not directly affected by this development I am not able to comment on the effect these very minor changes will make to their lives.</p> <p>I am still very much against this building so I guess Option 3 is my choice.</p> <p>Too much bulk - aesthetically; too many cars moving in to a previously quiet, safe neighbourhood; too much heat absorption and retaining steel, brick, concrete, etc., compared to the former green backyards, contributing to a warming climate; too much power needed to make the building liveable - and so on.</p>
15	<p>These amended plans are NOT compliant for a residential property so option 3 below from me. I personally think the amendments are 1 percenters and an insult to the residents of Alexander Road. None of the serious life changing issues for the Southern neighbour have been addressed.</p> <p>I will discuss the amended plans with the neighbours and I will suggest all respond as per your process individually. Further, the condition of 6 Alexander Road, continues to deteriorate and with winter approaching, there will be rain and water into the pool, no doubt creating another infestation. There has been no effort to tidy this property by the owners and it is an absolute eyesore and hazard. I understand the City have limited powers on this issue but it affects all the residents and visitors to the shops.</p> <p>At the Council in February 2025, it was voted unanimously to reinstate out height cap of 10 m height via LPP5.8 . I have not been able to find</p>

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	any reference to this on Your voice nor have seen any advertising in the Post newspaper. Please advise the timeline for this as time is of the essence. It is now urgent.
16	My objections to this development application remain as per option 3 as previously submitted. The building remains out of contact with regard to the surrounding buildings and with codes as regards, scale, bulk height, amenity and its impact on the living environment of the area.
17	Option 3. Changes are token. Whole scheme does not address reasons for having density increased.
18	<p>We are writing regarding the proposed apartment development at 6 Alexander Road and, specifically, the amended plans submitted by the developer on 26 March.</p> <p>We wish to nominate Option 2 and Option 3 and submit the following concerns for your consideration. While our previous submission still stands, we wish to make the following additional points:</p> <ul style="list-style-type: none"> • Incompatible Height and Streetscape Impact <p>The proposed four-storey development is entirely out of character with a street consisting solely of single and double-storey homes. The WASAT/41 ruling clearly deemed such a height "...discordant and incompatible in its height in terms of its immediate streetscape setting...". Since that ruling, five additional homes have been approved or constructed, reinforcing the area's low-rise character and the ruling's relevance.</p> <ul style="list-style-type: none"> • Bulk, Scale, and Plot Ratio <p>The development's bulk is excessive and inconsistent with surrounding homes. Its 1.18 plot ratio exceeds R80 limits by 18%. While the amended design reduces plot ratio slightly, this was achieved by converting bedrooms into large roof terraces—doing little to reduce the building's visual mass.</p> <ul style="list-style-type: none"> • Overshadowing and Overlooking <p>The proposal causes severe overshadowing, especially to the southern neighbour's living areas, and creates unacceptable overlooking into the homes and private open spaces of neighbouring properties to the north, east, and south.</p> <ul style="list-style-type: none"> • Inadequate Car Parking <p>The proposed eight resident bays and two visitor bays are insufficient for a development potentially housing over 40 residents. Should the building be used for short-stay or student accommodation, the car parking shortfall would be even more significant, with an estimated 21 bays required. Street parking on Alexander Road is already limited and heavily used.</p> <ul style="list-style-type: none"> • Underestimated Traffic Impacts <p>The traffic review accompanying the proposal underestimates the vehicle movements generated by a development of this size. Alexander</p>

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	<p>Road is a short, busy street near local shops and schools, already affected by congestion and limited visibility for residents exiting driveways. Increased traffic will exacerbate safety risks for pedestrians, cyclists, and motorists.</p> <ul style="list-style-type: none">• Inadequate Waste Management <p>The waste management plan is unbalanced and unworkable. A proposed 5:1 red to green bin ratio is contrary to typical household waste generation in Nedlands, where green bins are often more heavily used. Weekly green waste collection and fortnightly red/yellow bin collection will not support a development of this scale, likely resulting in overflow, odour, and misuse of public or neighbouring bins.</p> <ul style="list-style-type: none">• Unclear Intended Use <p>The building contains 21 bedrooms, 21 bathrooms, 8 kitchens, and numerous living areas—an unusual layout for standard family dwellings. The configuration resembles student housing or short-stay accommodation, which is not appropriate for this residential street, nor permissible under R80 zoning. The design suggests a commercial or mixed-use purpose, inconsistent with the area’s residential character.</p> <p>In conclusion, we strongly oppose the proposed 21 bedroom development at 6 Alexander Road. The amended plans fail to address fundamental concerns and would have a profoundly negative impact on the amenity, streetscape, and liveability of the area.</p> <p>Thank you for considering our submission.</p>
19	<p>I object to the proposal for the following reasons:</p> <ul style="list-style-type: none">• The amended plans result in overall height reduction of only 0.08m., & the four storey building’s height, more than 12.5m., is excessive. This is incompatible with the character of the built environment in Alexander Rd. between Waratah Ave., & Philip Rd. that comprises single storey & two-storey homes, including five new dwellings built, under construction &/or approved since March 2001, when SAT (WASAT/41) ruled proposed apartment building at 6 Alexander Rd. 'discordant and incompatible in its height in terms of its immediate streetscape setting and presents to the street with excessive bulk and scale when viewed in its immediate streetscape context.'• Plot ratio permitted for R80 density is 1.0:1; the proposed plot ratio of 1.18 exceeds this by 18%. The amended plans only slightly reduced plot ratio of 2024 plans by ‘converting’ two bedrooms into covered terraces opening to north & south sides of Levels 1 & 2. The plot ratio, bulk & scale of this proposed building are excessive.• Overshadowing by the proposed apartment building negatively affects the southern neighbours’ amenity especially living/dining/kitchen areas & impacts solar access.

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- The proposed apartment building overlooks neighbours' homes, outdoor living areas & gardens impacting their privacy & overall amenity. Screening per amended plans is inadequate to ensure the existing privacy & amenity is maintained in future.
- Parking for residents (8 bays) & visitors (2 bays) is inadequate for a 21 bedroom apartment building to accommodate 40 or more adults & children, & the short stretch of Alexander Rd. between Philip Rd. & Waratah Ave. is often fully parked. Traffic congestion is common & there would be considerably more increased traffic in these three streets associated with the proposed 21 bedroom apartments than for two houses on the lot, similar to those under construction at 9 Alexander Rd.
- Waste management will require more than 1 FOGO bin for 40+ residents & green waste associated with garden/landscape maintenance; & the weekly bin collection from Alexander Rd. is likely to pose problems.
- Neighbours have been informed designated use of this multi-residential building is for one multi-generational family, with the apartments comprising one 3 bedroom apartment for live-in caretaker, & four apartments for family, ranging in size from 3 bedrooms to 7 bedrooms (each with ensuite bathrooms) communal areas & powder room, & multiple amenity areas at top level of building including outdoor kitchen & pool area. This use would require regulation & oversight by City of Nedlands to ensure compliance in relation to WA's short stay regulations, as with its 21 bedrooms & bathrooms (including the 7 bedroom 7.5 bathroom apartment) the building potentially accommodates more than 40 people. Activities at the top level outdoor area should have be subject to safety regulations, noise & lighting controls, & also hours of operation. The fire pit in the rear garden, will have to comply with fire regulations.
- The proposed multi-storey apartment building will likely require upgrade &/or new infrastructure, as sewage & water services date from 1930s, & upgrades to the electricity supply will need to be addressed. Note: Landscaping plan does not mention the sewer line along eastern boundary (as shown on Survey Plan, 2019), which will influence choice of species & any structures proposed.

Conclusion: The proposed development at 6 Alexander Rd. does not meet all the Acceptable Outcomes or Element Objectives set out in Vol. 2 of the R-Codes.

Since early 2000s, consequent to increases in zoning from R10 & R12.5, in this area of Alexander Rd., Alexander Place, Philip Rd., numerous new one & two storey houses have been built, are under construction &/or approved, consistent in height with older well maintained family homes in this area, & they enhance the streetscape & overall amenity of the area. The proposed very large four storey multi-residential building

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	<p>is inappropriate in Alexander Rd. between Philip Rd. & Waratah Ave., as it does not enhance achieve this; & it does not 'acknowledge the existing pattern of narrow lot development and provide consistency of development along the streetscape', which was an important criterion in City of Nedlands LPP5.14.</p> <p>In March 2021, after SAT assessed a proposal for an apartment building at 6 Alexander Rd., the Tribunal found it was 'discordant and incompatible in its height in terms of its immediate streetscape setting and presents to the street with excessive bulk and scale when viewed in its immediate streetscape context.' Consequently, that proposal did not proceed. The Design Report for 6 Alexander Rd., DAP/24/0280, refers to a 'previously approved scheme' for an apartment building at 6 Alexander Rd., & images purportedly show the 2024-25 scheme 'exceeds the outcome of the deemed to comply scheme'. The Design Report did not mention the Tribunal's finding, which is equally applicable to the current (2024-25) proposal & the finding should be upheld.</p>
20	<p>Like many in the neighbourhood, we were not made aware of this policy change at the time.</p> <p>As the immediate southern neighbours of No. 6 Alexander Road, we are the most directly and severely impacted by the proposed development.</p> <p>We would like to raise the following concerns regarding the proposed amendments:</p> <p>1. Height</p> <p>A reduction of 0.8 metres from the original proposed height is a minor adjustment in the context of an overall four-storey structure. This does little to address the scale of the development. The comparative imagery provided for the original and revised designs is misleading if the only modification is a reduction of 0.8m.</p> <p>2. Height and Plot Ratio</p> <p>The combination of excessive height and an inflated plot ratio results in a building that is completely out of character with the established streetscape, which predominantly consists of single- and double-storey homes.</p> <p>According to the Residential Design Codes (R-Codes), Volume 2, maintaining compatibility with the surrounding streetscape is a key consideration. Yet, the developers are continuing to "push the envelope":</p>



- The initial plot ratio sought was 1:1.25
- Upon request to revise the plans, they submitted a revised ratio of 1:1.18

Both figures remain well above the standard 1:1 and are not in keeping with the local character.

3. Sunlight Impact

While relocating the fourth-floor dwelling from the front to the rear may address solar access to my rooftop panels, it entirely ignores the impact on my living spaces—particularly the large north-facing windows, which are essential for passive solar gain during winter. My studies show on the winter solstice most of my home is in shadow This significantly compromises the amenity of our home.

4. Parking

Reducing the number of bedrooms from 23 to 21 and providing only 8 resident and 2 visitor parking bays is highly inadequate. It is difficult to understand how this meets planning expectations for a development of this scale.

Overflow parking will inevitably spill onto Alexander Road—a narrow residential street already limited in capacity

5. Privacy and Noise Concerns

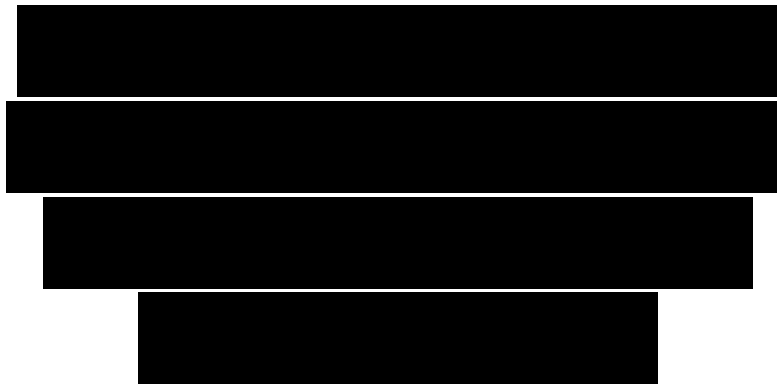
There are terraces located on both the north and south sides of the building, particularly on the 1st and 2nd floors, which directly overlook my living areas. This raises significant concerns regarding both privacy and noise. The close proximity of these terraces compromises the sense of seclusion within my home, especially in key living spaces. Additionally, there is a high likelihood that these areas will be used as entertainment spaces, which could result in considerable noise levels, particularly during evenings and weekends. These factors combined pose a serious impact on the comfort and quality of life within my residence.



	<p>5.</p> <p>With so many different people living in such close proximity, the possibility of conflict is high, which could disrupt the peace and quiet of our neighborhood.</p> <p>7. Ownership and Compliance</p> <p>We would also like to raise the question of whether the shareholders of United Universal Pty Ltd meet the requirements of Australian residential ownership regulations. We believe this is a matter that warrants clarification.</p> <p>In Conclusion</p> <p>The proposed development is disproportionate, disruptive to the existing character of the area, and significantly affects the amenity of neighbouring properties—ours in particular. We respectfully urge the council and planning authorities to take these concerns seriously and ensure that development remains consistent with community expectations.</p>
21	<p>I nominate Option 3:</p> <p><u>Option 3</u></p> <p>I have viewed the amended proposal and have concerns regarding the proposed variations. My previous submission applies unchanged to the amended set of plans.</p>

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Comments to City of Nedlands by:



Questions:

1. How many 8 bedroom, 8 bathroom single apartment dwellings has CoN approved for construction in the last decade? Is this ultra high density style of accommodation something that the CoN would encourage more of in future? Will this apartment end up a “white elephant” in which no one wishes to live, which no one wishes to buy, and which might then fall into disrepair?
2. Is the 8x8 apartment on level 1 a “dwelling” as defined in the R-Codes (“...**for the purpose of human habitation on a permanent basis by a single person, a single family, or no more than six persons who do not comprise a single family...**”). It doesn’t pass the pub test.
3. The planning report states that the owner is **Universal United Investments Pty Ltd**. The report also says "Dwelling G02 is to be provided as a caretaker dwelling, while all remaining dwellings will be occupied by members of the same family."
 - Has the specific family who is intending to dwell in the 8 x 8 Level 1 apartment made any binding commitment to reside in the dwelling for the long term?
 - Can the City of Nedlands rule out a future approval of a non-dwelling use in the event that the family vacates the dwelling, and if the dwelling cannot be leased or sold due to market factors?
4. The building’s design, including 23 bedrooms and expansive communal living areas across the three upper floors, would be perfectly suited to purposes other than a “dwelling”. Has **Universal United Investments Pty Ltd** made any binding commitment to not repurpose any of the 23 bedrooms in this building for a non-dwelling use? i.e. short stay accommodation?
5. If the building was to change ownership, and if part or all of the 23 bedroom building was to be repurposed as a short stay accommodation or student accommodation, then is there sufficient parking to meet WA R-Code requirements for this type of building use?
6. In the 1st traffic review, Table 1 states there are 4 dwellings with 2+ bedrooms, and 1 dwelling with 1 bedroom. Why was this incorrect traffic review submitted and posted on YOURVOICE?
7. Did the traffic review consider that the building has 23 bedrooms in total, accommodating up to 46 people, when estimating parking requirements and traffic movements?
8. The revised traffic review was posted on YOURVOICE website 3 working days before the close of comments on the development application. Is this in accordance with Planning and Development Act 2005, Planning and Development (Local Planning Schemes) Regulations 2015?

Dual key apartment - an apartment with a common internal corridor and lockable doors to sections within the apartment so that it is able to be separated into two independent units. For the purposes of assessment they are considered as two units.

Dwelling - a building or portion of a building being used, adapted, or designed or intended to be used for the purpose of human habitation on a permanent basis by a single person, a single family, or no more than six persons who do not comprise a single family.

Enclosed - an area bound on three or more sides by a permanent wall and covered in an impervious material.

Façade - the external face of a building, generally the principal face, facing a public street or space.

Freehold scheme - has the meaning given by section 8(2) of the *Strata Titles Act 1985*.

Frontage - the width of a lot at the primary street setback line, provided that in the case of battleaxe or other irregularly shaped lots, it shall be as determined by the decision-maker.

Functional utilities - Functional utilities associated with the dwelling including, but not limited to air-conditioning, plant, clothes drying, hot water systems, antennas and satellite dishes, solar collectors, invertors and batteries, electric vehicle charging and rainwater tanks.

Green façade / green wall - a wall with fixtures to facilitate climbing plants. It can also be a cladding structure with growing medium to facilitate plant growth.

Green roof - a roof surface that supports the growth of vegetation comprising of a waterproofing membrane, drainage layer, organic growing medium (soil) and vegetation. Green roofs can be classified as either extensive or intensive, depending on the depth of substrate used and the level of maintenance required. Intensive green roofs are generally greater than 300mm deep and are designed as accessible landscape spaces with pathways and other features. Extensive green roofs are generally less than 300mm deep and are generally not trafficable.

Green title lot - A lot owned in fee simple issued with a certificate of title under the *Transfer of Land Act 1893*, other than a strata lot or a survey-strata lot.

Greywater - wastewater from clothes washing machines, showers, bathtubs, hand washing, lavatories and sinks that are not used for disposal of chemical or chemical-biological ingredients. Excludes water from the toilet (which is regarded as 'blackwater'). Refer to *Department of Health Guidelines for the Non-potable Uses of Recycled Water in Western Australia*.

Grouped dwelling - a dwelling that is one of a group of two or more dwellings on the same lot such that no dwelling is placed wholly or partly vertically above or below another, except where special conditions of landscape or topography dictate otherwise, and includes a dwelling in a strata titles scheme with common property.

Questions:

9. The revised traffic review Figure 4, does not take into account the large number of cars regularly parked along the eastern side of Alexander Rd due to the shopping precinct just 60m away from the driveway. A large number of traffic movements from the site, with visibility obstructed by parked cars, substantially increases the risk of vehicle collisions.
10. The revised traffic review estimates **25 trips per day**. For a 23 bedroom building, which could accommodate up to 46 people, this doesn't pass the pub test. The report claims to be based on traffic movement volume methods in the NSW Roads and Maritime Services' "Guide to Traffic Generating Developments." This guide suggests: Daily Vehicle Trips: 10.7 trips per dwelling = **53.5 trips per day**. Will City of Nedlands please calculate the percentage increase in traffic along Alexander Road as a result of this, and quantify the impact on the amenity of Alexander Rd?
11. If it is determined that LPP 5.8 has not been revoked in accordance with state planning regulations, and that in fact has always been in effect, then does the proposed building height contravene LPP 5.8?
12. If it is determined that LPP 5.14 has not been approved by the WA Planning Commission in accordance with state planning regulations, then would the proposed building side setbacks of 2m contravene the R-Code requirements for 3.5m setbacks?
13. Would CoN planners consider Alexander Rd as a "low rise" streetscape as defined in A2 of the R-Code (Vol.2)?
14. Section 2.1.1 of the R-Code states: "***Apartment design should respond appropriately to the defined neighbourhood context and character***". Do CoN Planners think this development responds appropriately to the "low rise" streetscape in Alexander Rd?
15. The planning report says the plot ratio is 1.18 however if I sum the stated floor areas in the plans (154.4 + 92.3 + 458.2 + 378.9 + 177.7 + 117.5) I get a building area of 1379 m², equating to a plot ratio of 1.36. This exceeds the R-Code AO by 36%!! What is the real plot ratio?

Questions:

16. The elevation drawings show a fall of approx. 0.5m from north to south across the block. BUT the elevation drawings show the same ground floor slab level for three buildings at 4B, 6, & 8A Alexander Rd. Are the overshadowing drawings correct, or do they underrepresent the actual overshadowing due to the slope of the hill? Have the overshadowing drawings / estimates been verified by a competent person acting for City of Nedlands?
17. Table 2-2 of the 1st waste management plan states there are 3 dwellings with 3 bedrooms, and 1 dwelling with 5 bedrooms, and 1 dwelling with 7 bedrooms. Why was this incorrect waste management plan submitted and posted on YOURVOICE?
18. Did the waste management plan in any way consider that the building has 23 bedrooms in total, and up to 46 people residing in it, when estimating rubbish volumes and bin sizes / numbers?
19. The waste management plan mentions duties to be performed by the live-in caretaker on 6 occasions. The planning report mentions the caretaker's duties too. Has **Universal United Investments Pty Ltd** made any binding commitment to always have a live-in caretaker in the building doing these duties? Will the City of Nedlands make this an ongoing condition on the site?
20. The revised waste management plan was posted on YOURVOICE website 3 working days before the close of comments. Is this in accordance with Planning and Development Act 2005, Planning and Development (Local Planning Schemes) Regulations 2015?
21. The revised waste management plan does ***not*** mention a 6 bedroom apartment, but it does refer to a 5 bedroom apartment. The revised waste management plan appears to be incorrect. Why was this incorrect waste management plan submitted and posted on YOURVOICE a second time?
22. The revised waste management plan claims that only 200 litres per week of FOGO is produced. Considering the building has 23 rooms, and each room could easily accommodate 2 persons, so it could easily be housing up to 46 persons, does 200 litres of FOGO volume "...pass the pub test..."? The consequence of this gross underestimate of FOGO would be that a lot of FOGO will then be disposed of in the red bins, which is not sustainable. Does City of Nedlands have a vision for sustainability, supported by a consistently applied policy, or not?
23. Furthermore, the 2 weekly collection of red bins may result in rotting food waste in the red bins, impacting the amenity of the guests living in the apartments.
24. Level 3 has 2 "Pebble Roof" areas. Are these pebble roofs made of roofing materials, or made of reinforced concrete? If the pebble roofs are reinforced concrete then is it conceivable that a future owner of the building could convert the pebble roofs to balconies in future?
25. Have CoN planners considered the WASAT 41 ruling? Have CoN planners considered that 5 more two-storey houses will be built in Alexander Rd in 2025, which will further amplify the logic in the WASAT 41 ruling? i.e. within a year or so there will be 13 "...*new single houses in the immediate locality of the site ...*" with "... *relatively recent construction, quality and value...*"
26. City of Nedlands has invested time and ratepayers' money in obtaining the WASAT 41 ruling in 2021. Will CoN apply this WASAT 41 precedent in 2025, and refuse this development application?

Supporting Information and Notes:

WASAT 41:



Completing construction 2025

Dev.App. at Dec.2024

Dev.App. at Dec.2024

1st floor approx. footprint

Commencing construction 2025

Commencing construction 2025

All lengths and vectors illustrated are indicative and not accurately dimensioned.

Since the WASAT 41 ruling, the Alexander Rd / Phillip Rd streetscape changes will further reinforce the WASAT's reasoning:

- 1 Alexander Rd: A two storey house is in construction.
- 3 Alexander Rd: Two 2-story houses are proposed in a 2024 development application.
- 9 Alexander Rd: Two 2-storey houses are about to commence construction (block has been demolished, but construction hasn't started).

By the time the 6 Alexander Rd block of flats is built, it will be adversely affecting the streetscape with thirteen (13) "... new single houses in the immediate locality of the site ..." with "... relatively recent construction, quality and value..."

The block of flats would be even more "...discordant and incompatible in its height in terms of its immediate streetscape setting..." compared to the WASAT 41 judgement in 2021.

Excessive Bulk and Scale:



The length, bulk & scale of the proposed 1st, 2nd and 3rd floors is much longer than other relatively new houses in the street, casting a much larger shadow.

All lengths and vectors illustrated are indicative and not accurately dimensioned.

Bulk and Scale:

The suburban context surrounding 6 Alexander Rd is "Low Rise".

The R-Codes state that:

- "Apartment design should respond appropriately to the defined neighbourhood context and character."
- "New development should reflect the prevailing streetscape patterns and include significant onsite landscaping to enhance the streetscape and provide amenity for residents and neighbours."

Self-evidently, plonking a 4 story block of flats in a street surrounded by 2 story buildings would fail to meet this fundamental requirement in the R-Codes.

To quote Judge DR Parry (WASAT41, 17-Mar-2021) when he assessed a different 4 story building proposed for 6 Alexander Rd:

"Because of their relatively recent construction, quality and value, the eight new single houses in the immediate locality of the site are unlikely to be demolished in the foreseeable future. The Tribunal observed that these unusual planning (subdivision and development) characteristics of the immediate locality of the site require greater sensitivity from new development - and greater sensitivity than is displayed in the proposed development - in terms of height, bulk and scale fronting the street than would otherwise be the case in an area up-coded to R80. The Tribunal found that the proposed development is discordant and incompatible in its height in terms of its immediate streetscape setting and presents to the street with excessive bulk and scale when viewed in its immediate streetscape context."

2.1.1 Setting the local planning framework

Good design responds and contributes to the context and character of the area as defined by social, economic and environmental characteristics as well as natural and built features.

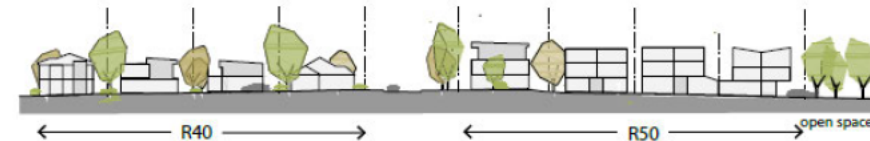
The Primary Controls in this policy are informed by neighbourhood character types that describe the context and character of different neighbourhoods, centres and precincts. The six neighbourhood character types are illustrated at Appendix A2. Local governments should identify the context and character of neighbourhoods and precincts via strategic planning processes and in consultation with the community, industry and other key stakeholders. Apartment design should respond appropriately to the defined neighbourhood context and character.

SUBURBAN CONTEXTS

Low-rise

Context: Neighbourhoods that provide a mix of detached housing, group housing and apartments. They are predominately 1-2 storeys but may include 3-storey development on neighbourhood connector streets, adjacent to open space and/or on key sites. Apartment development should be located within walking distance of public transport, local shopping and local open space and may create a transition zone between a lower density residential area and higher density land uses.

Character: Streetscapes have a landscaped character and prevailing built form patterns of 1-2 storey detached dwellings with defined front and side setbacks. New development should reflect the prevailing streetscape patterns and include significant on-site landscaping to enhance the streetscape and provide amenity for residents and neighbours.]



Note: Refer to R-Codes Volume 1 for R40 and R50 development.

Medium-rise

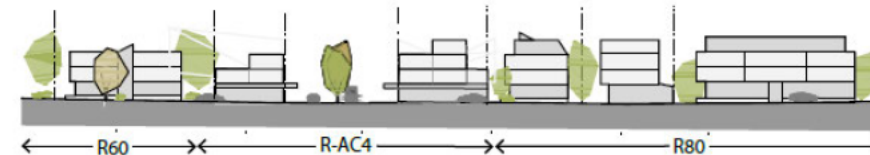
Context: Neighbourhoods with a landscaped residential setting that include a diversity of detached housing, group housing and apartment developments up to 3-4 storeys. The neighbourhood has good walkability to public transport, local services and quality open space, and may be located adjacent to higher density land uses or an urban corridor.

Character: Streetscapes have a landscaped character and built form patterns are defined by overall scale of the streetscape rather than individual building height or style. New development should reflect the prevailing patterns of side setbacks and respond to the existing or planned scale and materiality of the area. Development should include on-site landscaping to enhance streetscape and provide amenity for residents and neighbours.

Neighbourhood centre

Context: A local or neighbourhood centre within a low - medium rise residential area. Neighbourhood centres should be located within a short walk of transport and other amenities and include built form and uses that activate the street. Land uses within a neighbourhood centre may include residential, community facilities, local shopping and commercial activities.

Character: Neighbourhood centres have a compact and cohesive urban form that complements the surrounding residential character in scale and style. New development should contribute to the public realm by enhancing and activating the streetscape and contributing to creating a distinct and appealing character for the centre.



Note: Refer to R-Codes Volume 1 for R60 development.

Plot Ratio:

Block size = 1012 m²

Floor area from summing the floor areas stated on their plans = 1379 m² (i.e. 154.4+92.3+458.2+378.9+177.7+117.5). This summary data is on pages 2 - 7 inclusive here:

chrome-extension://efidnbmmrnlpcjpcjcfndmbaj/http://ehs-production-australia.s3.ap-southeast-2.amazonaws.com/52346f2a842f5d6a999b67c7776b11d10e696/original/1736826434/abaSc0f99f90c7c9bbe437fc3f9_6_Alexander_Road_Development_Plans_-_For_Advertising.pdf?X-Amz-Algorithm=AWS4-HMAC-SHA256&X-Amz-Credential=AKIAKKNQAKIWF0U1F5%2F20250127%2Fap-southeast-2%2Fsa3%2Faws4_request&X-Amz-Date=20250127T02%3B302&X-Amz-Expires=300&X-Amz-SignedHeaders=host&X-Amz-Signature=9612902223767d713a80e642f9c34919ee80e8c27a0f9d736e64e76c4a3e8f

Inferred plot ratio: = 1.36
i.e. exceeds R-Code by 36%

Plot ratio area - the gross total area of all floors of buildings on a development site, including the area of any internal and external walls but not including:

- the areas of any lift shafts
- stairs or stair landings common to two or more dwellings
- machinery, air conditioning and equipment rooms
- space that is wholly below natural ground level
- areas used exclusively for the parking of wheeled vehicles at or below natural ground level;
- storerooms
- lobbies, bin storage areas, passageways to bin storage areas or amenities areas common to more than one dwelling; and
- balconies, eaves, verandahs, courtyards and roof terraces.

Table 2.1 Primary controls table

	Applies to R-Code areas, default settings apply unless alternative provisions defined in local planning instruments		
	Medium-rise	Higher density residential	
Streetscape contexts and character refer A2			
Site R-Coding	R80	R100	R160
Building height (storeys) refer 2.2	4	4	5
Boundary wall height (storeys) ^{1,2} refer 2.4	2 ³		2 ³
Minimum primary and secondary street setbacks refer 2.3	2m		2m
Minimum side setbacks ⁵ refer 2.4	3m		3m
Minimum rear setback refer 2.4	3m		6m
Average side setback where building length exceeds 16m refer 2.4	3.5m	3.5m	4.0m
Plot ratio ⁷ refer 2.5	1.0	1.3	2.0

Inadequate Parking:

If this block of flats is assumed to be 5 dwellings, they need 6.25 bays for residents + 2 for visitors = 8.25. They have designed 10, so on the face of it, it complies with R-Code.

HOWEVER, this block of flats could be very easily repurposed as Student Accommodation or an analogous purpose.

In which case there could be up to 17 apartments in total, 14 of which may be single bedroom+ensuite apartments.

If the building was to repurposed as Student accommodation then the following parking spaces would be required by the R-Code:

- 14 spaces for the 14-off 1-bed studio apartments
- 3.75 spaces for the 3 multi-room apartments.
- 3 visitor bays for the first 12 dwellings
- 5/8 of a bay for dwellings #13-#17
- **TOTAL = 21.375 parking spaces required if Level 1 and Level 2 were to be repurposed in future as student accommodation.**

The revised traffic review estimates 25 trips per day. For a 23 bedroom building, which could accommodate up to 46 people, this doesn't pass the pub test. The report claims to be based on traffic movement volume methods in the NSW Roads and Maritime Services' "Guide to Traffic Generating Developments." This guide suggests: Daily Vehicle Trips: 10.7 trips per apartment = 53.5 trips per day.

Table 3.9 Parking ratio

Parking types		Location A	Location B
Car parking ¹	1 bedroom dwellings	0.75 bay per dwelling	1 bay per dwelling
	2+ bedroom dwellings	1 bay per dwelling	1.25 bays per dwelling
	Visitor	1 bay per four dwellings up to 12 dwellings 1 bay per eight dwellings for the 13th dwelling and above	
Bicycle parking ¹	Resident	0.5 space per dwelling	
	Visitor	1 space per 10 dwellings	
Motorcycle/ Scooter parking ²	Developments exceeding 20 dwellings provide 1 motorcycle/scooter space for every 10 car bays		

¹ Calculations of parking ratios shall be rounded up to the next whole number.
² For each five motorcycle/scooter parking bays provided in accordance with Table 3.9, car parking bays may be reduced by one bay.

Definitions:
Location A: includes all land located within:
- 800m of a train station on a high-frequency rail route, measured in a straight line from the pedestrian entry to the train station platform to any part of a lot;
- 250m of a high-frequency transit route, or multiple transit routes that if combined have timed stops every 15 minutes during weekday peak periods (7-9am and 5-7pm), measured in a straight line from along any part of the transit route to any part of the lot; and/or
- within the defined boundaries of an activity centre.
Location B: not within Location A.

Overshadowing:

The "Design Report" has rather incomprehensible overshadowing drawings. I can't comment on them as I can't understand them. See them on page 21 here:

https://ehq-production-australia.s3.ap-southeast-2.amazonaws.com/5a682107313571352b294c162395a1343d7ccb27/original/1736828055/e6b7700ab36a3222b231758c98cd44cd_6_Alexander_Road_-_Design_Report_-_For_Advertising.pdf?X-Amz-Algorithm=AWS4-HMAC-SHA256&X-Amz-Credential=AKIA4KKNOAKIFWFOUYFI%2F20250118%2Fap-southeast-2%2F3%2Faws4_request&X-Amz-Date=20250118T125320Z&X-Amz-Expires=300&X-Amz-SignedHeaders=host&X-Amz-Signature=d19f589adf6edcb01278ed6bab20129e600d5d0351db4d875127d385eb6f77a1

3.2

ELEMENT OBJECTIVES

Development is to achieve the following Element Objectives:

- 3.2.1 **Building** layouts respond to the **streetscape**, topography and **site** attributes while optimising **solar** and **daylight access** within the **development**.
- 3.2.2 **Building** form and orientation minimises overshadowing of the **habitable rooms**, **open space** and **solar collectors** of neighbouring properties during **mid-winter**.

ACCEPTABLE OUTCOMES

Acceptable Outcomes are likely to assist in satisfying the objectives but are not a comprehensive 'deemed-to-comply' list. In order to achieve the Element Objectives, proposals may require additional and/or alternative design solutions in response to the site conditions, streetscape and design approach.

- A 3.2.1 Buildings** on **street** or public realm **frontages** are oriented to face the public realm and incorporate direct access from the street.
- A 3.2.2 Buildings** that do not have **frontages to streets** or public realm are oriented to maximise northern **solar access** to living areas.
- A 3.2.3 Development** in **climate zones** 4, 5 and 6 shall be designed such that the shadow cast at midday on 21st June onto any adjoining property does not exceed:
 - adjoining properties coded R25 and lower – 25% of the **site area**¹
 - adjoining properties coded R30 – R40 – 35% of the site area¹
 - adjoining properties coded R50 – R60 – 50% of the site area¹
 - adjoining properties coded R80 or higher – Nil requirements.]
- A 3.2.4** Where adjoining **sites** are coded R40 or less, **buildings** are oriented to maintain 4 hours per day **solar access** on 21 June for existing **solar collectors** on neighbouring sites.

¹Where a development site shares its southern boundary with a lot, and that lot is bound to the north by other lot(s), the limit of shading at A 3.2.3 shall be reduced proportionally to the percentage of the affected properties northern boundary that abuts the development site. (Refer to Figure A7.2 in Appendix 7)

The R-Code has no protection for houses in R-80 zone.

Overshadowing: Drawings Do Not Appear to Consider Different Block Heights

The elevation drawings show an fall of approx. 0.5m from north to south across the block. See page 8 here:

chrome-extension://ehidomnmilopajpg/definima/https://shg-production-sustralia.s3.ap-southeast-2.amazonaws.com/623487864250de99e8e7c7776a121a09e8f/original/1736526434/attached/99c7c9b0ee4376c939_6_Alexander_Road_-_Development_Plans_-_For_Advertising.pdf?X-Amz-Algorithm=AWS4-HMAC-SHA256&X-Amz-Credential=AKIA8KNGAKIFWOLY7%2F20250127%2Fap-southeast-2%2Fis%2Faws4_request&X-Amz-Date=20250127T023830Z&X-Amz-Expires=300&X-Amz-SignedHeaders=host&X-Amz-Signature=9a239a22237674713aa0e642f8c34819ae806e1c27abf9d736d64e76c4a7e8f

BUT the elevation drawings show the same ground floor slab level of 19.5m for all the following buildings:

- 8A Alexander Rd (downhill)
- 6 Alexander Rd (proposed)
- 4B Alexander Rd (uphill)

However Alexander Rd is on a slight incline, going downhill from the proposed block of flats towards 8A Alexander Rd.

Critically, 8A Alexander Rd very likely has a lower ground floor level than illustrated in the elevation drawing.

If there is a difference in ground floor slab elevations then this would render the overshadowing drawings incorrect.

CONSIDERATIONS FOR LOCAL GOVERNMENTS PLANNING GUIDANCE

- PG 2.4.1 Test side and rear **setbacks** with height controls to understand the potential for overshadowing of the **site**, adjoining properties and **open space**.
- PG 2.4.2 In existing areas test side and rear **setbacks** with **the requirements for visual privacy**, tree retention and other known characteristics of the locality.
- PG 2.4.3 Consider nil side **setbacks** where the desired **streetscape** character is for continuous **development**, such as in dense urban areas, main **streets** or for **podiums** within centres.
- PG 2.4.4 Table 2.1 makes provision for some section of build-to-boundary **development** in **streetscapes** that largely comprise detached development. Consider whether the extent of this development is appropriate, or whether additional build-to-boundary may be permitted without detrimental effect on neighbours or the streetscape to improve efficiency and flexibility of ground floor layouts. Consider the appropriate settings for particular streetscapes or neighbourhoods.
- PG 2.4.5 In **streetscapes** that largely contain build-to-boundary (attached) **development** consider whether constraints for nil-**setback** boundary walls are appropriate.
- PG 2.4.6 On **sloping sites**, consider increasing side and rear **setbacks** where new **development** is uphill to minimise overshadowing and assist with **visual privacy**.

Overlooking / Privacy:

No privacy screening on the Level 3 Gymnasium, so people in the Gym can look into the backyard of 8A Alexander Rd.

Level 3 has 2 "Pebble Roof" areas.

- Are these pebble roofs made of roofing materials, or made of reinforced concrete?
- If the pebble roofs are reinforced concrete then is it conceivable that they could be converted to balconies in future years?
- If the building was to be repurposed for student accommodation in future then is it conceivable that these modifications might be easily made by a future owner of the flats?
- If the roofs were to be converted to balconies in future, would this result in unenclosed private outdoor spaces only 3m from the boundary, when a 6m setback is required by the R-Code?

Table 3.5 Required privacy setback to adjoining sites

Cone of vision from unscreened (refer Figure series A7.3):	First 4 storeys		5th storey and above
	Adjoining sites coded R50 or lower	Adjoining sites coded higher than R50	
Major opening to bedroom, study and open access walkways	4.5m	3m	Refer Table 2.7
Major openings to habitable rooms other than bedrooms and studies	6m	4.5m	
Unenclosed private outdoor spaces	7.5m	6m	

12 February 2024

Chief Executive Officer
City of Nedlands
PO Box 9
NEDLANDS WA 6909

Attention: Planning Department – planning@nedlands.wa.gov.au

**OBJECTION SUBMISSION – PROPOSED MULTIPLE DWELLING DEVELOPMENT
LOT 381 (#6) ALEXANDER ROAD, DALKEITH**

This submission is made in respect of the proposed Multiple Dwelling Development at 6 Alexander Road, Dalkeith.

We thank the City of Nedlands for giving us the opportunity to make a submission on the proposed development. This submission is an objection to the proposed development.

We have sought expert Town Planning and Architect advice in reviewing the proposed development and preparing this submission.

This submission is made on the following basis:

1. Aims of the Scheme
2. Objectives of LPP5.14 Waratah Precinct
3. Concerns of Land Use / Occupancy Type
4. Car Parking
5. Traffic
6. Plot Ratio
7. Amenity Considerations

These are set out in detail below.

Aims of the Scheme

The Aims of the City's LPS3 include the following relevant Aims:

- Protect and enhance local character and amenity; and
- Respect the community vision for the development of the district

These aims are the first two aims set out in the Scheme, and they are important. Accordingly, the City should consider the protection of amenity and the community vision for the district when assessing the application.

Our view is that the proposed development does not protect amenity; in fact it will impinge upon the amenity of adjoining residential dwellings.

Waratah Precinct LPP5.14

The City's LPP5.14 applies to the proposed development, and the site is within the Waratah Precinct.

The Desired Future Character Statement of the Waratah Precinct of LPP5.14 sets out that "New development in the Alexander Road sub-precinct shall acknowledge the existing pattern of narrow lot development and provide consistency of development along the streetscape."

Accordingly, new development (including the one proposed at 6 Alexander Road), is to acknowledge the existing narrow lot pattern, particularly those to the south of the development site at #8A and #8B, where there are significant amenity impacts through overshadowing and bulk/scale. In our view, the proposed development does nothing to acknowledge the narrow lot pattern that exists along Alexander Road.

Land Use / Occupancy Type

The proposed development (at least Units 101 and 201) appears to be for the purpose of student or shared accommodation, with nearly all bedrooms having its own bathroom, nearly all bedrooms in Units 101 and 201 having been shown with a TV on the wall, along with large shared communal spaces both internally and separately on Level 3.

The proposed development comprises (5) dwellings, 23 bedrooms (with two bedrooms containing two single beds, which results in 25 sleeping locations), 22 bathrooms(!!) and large communal/shared spaces, and 10 car parking spaces.

For the proponent to suggest that this is a normal residential dwelling type, or for the purposes of a wealthy family/ies, is considered absurd.

The issue is highlighted more particularly with Unit 101 which is 458m² in area and comprises 8 bedrooms (however, with one room showing two beds, i.e. 9 sleeping locations) with 8.5 bathrooms and has communal areas such as 8-ball table. This is not a normal residential apartment dwelling, clearly being designed for shared living such as student accommodation or similar.

Further, Unit 201 is described in the Apartment Area Schedule and Planning Report as comprising 5 bedrooms, however it is clear from the plans that there is 6 bedrooms on this level, with one room actually having two beds (possibly being able to sleep 7 people).

Level 3 comprises a large, separate communal space for the shared occupants and includes gym, massage, sauna, entertaining and pool. These types of amenities are not what you would typically see for a small 5 dwelling apartment development.

The Planning Report explains only briefly the intent for multi-generational living to accommodate the members of the same facility, however there is no discussion about how this can/will be enforced. If a typical family vacates, there is nothing stopping the owner from renting out each room. A more likely scenario is that each bedroom will be rented out (i.e. student accommodation), producing a high income for the landowner.

We are aware of another proposal (SDAU Application) recently advertised in Nedlands for the purpose of Student Accommodation (#37-43 Stirling Highway), and this is being assessed as a Use Not Listed (University Accommodation Facility) which is defined (generally) as:

"University Accommodation Facility – means a facility –

- a. *Whose predominant purpose is to accommodate:*
 - (i) *students while studying at a tertiary education institution; and*
 - (ii) *staff of a tertiary institution or visiting staff to a tertiary education institution.*
- b. *That is designed and managed to facilitate the predominant purpose;*
- c. *That typically provides students with a principal place of residence for 3 months or more; and*
- d. *That may have shared facilities, such as a communal living room, kitchen and amenities."*

This definition more closely aligns what is proposed. It should be noted that the proposal for 6 Alexander Road is applied for as simply "Residential", which is not defined in LPS3.

The City should consider assessing the proposal as a Use Not Listed, or at least, voice concerns about the intended occupancy of the development. It has not been made clear that this proposal is for normal "Residential" use. It appears to have been driven to provide bedrooms with their own bathrooms and large communal areas which would be defined as Student Accommodation in other local planning schemes.

Alternatively, there is also the definition of Residential Building in the R-Codes which is defined as:

RESIDENTIAL BUILDING - a building or portion of a building, together with rooms and outbuildings separate from such building but incidental thereto; such building being used or intended, adapted or designed to be used for the purpose of human habitation:

- temporarily by two or more persons; or
- permanently by seven or more persons, who do not comprise a single family, but does not include a hospital or sanatorium, a prison, a hotel, a motel or a residential school.

Unit 101 being for 8 / 9 people could be treated as a Residential Building as per the definition under the R-Codes, and same for Unit 201 which could accommodate 7 people. The design of the dwellings with separate bathrooms and a TV in each bedroom indicates that this will be a development that will be rented out room by room. This is not what the Residential zone is for, or for what the normal Residential land use term envisages. It is clear on this basis, that these dwellings should be considered as Residential Building land uses, given the intent for seven or more people to occupy them (at least Units 101 and 201).

A "Residential Building" use would also be classified as a Use Not Listed in the City's LPS3.

In considering a Use Not Listed (either for Student Accommodation or Residential Building), the local government is to determine whether such a use/activity is consistent with the objectives of the Residential zone and the impact on amenity. We are aware of many in the locality who have voiced their amenity concerns with respect to over-development and overshadowing, and we are aware of an Overshadowing Study prepared by an Architect representing adjacent owners that demonstrates the impact of overshadowing.

Normally, with Student accommodation or Residential Building uses, there would need to be an operational management plan prepared to ensure its operations are suitable and do not negatively impact on the amenity of surrounding residential dwellings or other occupants. This has not been provided.

For a Use Not Listed, the City would also be required to advertise the application more widely, in accordance with Clause 64 of the Deemed Provisions, for a "Complex Application" which is defined as "an application for approval of development that is a use of land if the use is not specifically referred to in the zoning table for this Scheme in respect of the zone in which the development is located."

For these reasons, we have concerns with the land use / occupancy type proposed for this development, due to the unusual design of the dwellings, with many bedrooms and bathrooms, seemingly designed for shared/student accommodation with shared facilities. Subsequently, there may also be an issue in the way in which the application is to be considered and advertised, as a Use Not Listed proposal is required to be advertised as a Complex Application.

Car Parking

The previous proposal at 6 Alexander Road, Dalkeith involved 4 x 3 bedroom dwellings, with 8 car parking bays and 4 visitor bays, i.e. 12 parking bays aligning with 12 bedrooms. This proposal was refused in 2020.

The new development comprises (5) dwellings, 23 bedrooms (with two bedrooms containing two single beds, which results in 25 sleeping locations), 22 bathrooms(!!) and large communal/shared spaces, and 10 car parking spaces (i.e. less than the previously proposed development of 4 dwellings (4 x 3 bedroom dwellings).

The applicant's traffic report describes that providing 1.25 car parking bays per 2+ bedroom dwelling is adequate because it simply meets the RDC2 requirements, and that proposed 5x dwellings would necessitate 7 car parking bays + 2 visitor bays. But with a proposal for two very large dwellings, one with 8 bedrooms, another with 5 bedrooms, there is the very real scenario that these dwellings will generate a significantly higher traffic/parking requirement. As set out above, there are 25 sleeping locations across the site, together with a concern that these dwellings appear to be designed not for a family, but for student or similar accommodation, which may result in a greater number of cars on site than compared with normal dwellings. This may mean that there could be a higher traffic/parking generation for the development, which has not been considered by the traffic consultant.

It is appreciated that the car parking provided is consistent with the relevant development standard in the planning framework. However, it is our view that the number of parking bays proposed (10), considering the highly unusual dwelling design, is grossly inadequate for the development, and will likely result in parking issues inside the development, but also on surrounding streets and street verges. This is not an appropriate outcome and the City should be concerned about this aspect of the proposal. The relevant R-Code does not relate to dwellings comprising the number of bedrooms proposed and as such common sense must prevail in applying rigour to the significant shortfall that clearly results.

Alexander Road has 'no parking' signs along the eastern side of the roadway (same side as the subject land), so if there is a parking problem, illegal parking may occur.

Using clause 67(s) of the Deemed Provisions, the City is to consider the adequacy (or lack thereof) of "... parking of vehicles". The City is able to recommend refusal on the basis of Clause 67(s), stating that there is an inadequate number of parking provided for the unusual dwelling design proposed.

Further, with regard to clause 67(zb), the City is able to consider any other planning consideration the local government considers appropriate. In this respect, the City, if it believes it to be appropriate, could recommend refusal on the basis of Clause 67(zb), stating that the inadequate parking provided could result in parking issues in the street and locality in general.

Traffic

In respect of traffic report provided, the vehicle trip generation suggested as being 3 vehicle movements in the AM peak and PM peak is considered low, with potentially 25 people living in the development. This does not compute and should be revisited.

Plot Ratio

A Plot Ratio of 1:1 applies at the R80 residential density code, being 1012m² of plot ratio area. The R-Code requirement is varied, as the development proposes 1201.3m², being 1.18:1, and an 18% increase of the primary controls for R80. This is not insignificant. The applicant's Planning Report provided indicates that the additional 189m² does not have a "meaningful impact" on overall bulk. We disagree, as this 189m² area is more than the area of the upper-level dwelling (Unit 301) which comprises 117.5m². If the plot ratio area was brought back to 1:1 and, for instance, if Unit 301 was removed, this would have a drastic effect on the overall bulk as viewed from the street and adjacent properties, as well as overshadowing impacts. Accordingly, the additional 189m² of plot ratio area (i.e. more than a complete proposed upper-level dwelling) does impact on bulk and impacts on amenity (overshadowing).

The previously refused proposal at 6 Alexander Road, Dalkeith involved a plot ratio variation 1.15, exceeding the default provision of 1:1 and exceeding the plot ratio area by approx 152m².

The new proposal has a plot ratio of 1:1.18, or 189m², representing a larger variation than the previously refused scheme.

The Element Objective is that the overall bulk and scale of the development is appropriate for the existing or planned character of the area.

The proposed plot ratio exceeds the anticipated volume of development for the R80 coding. In order to achieve the default plot ratio control, a further reduction in building height and/or additional boundary setbacks would be required. Considered on its own, the plot ratio suggests the development as proposed is too large for its context. Considered together with the other concerns raised in this submission, the development ought not to be supported.

Using clause 67(m) of the Deemed Provisions, the City is to consider "the compatibility of the development with its setting including the relationship of the development to development on adjoining land in the locality, including, but not limited to, the likely effect of the height, bulk scale, orientation and appearance of the development". The local government is able to recommend refusal on the basis of Clause 67(x), due to the effect of bulk, scale and resulting overshadowing.

Amenity Considerations

Amenity considerations are informed by a combination of elements that assess the impact of a development on future residents and on surrounding properties, including streetscape.

The proposed development will have a detrimental impact on the present streetscape/amenity of the residents on Alexander Road, including the likely future streetscape, which despite the rezoning remains intact and developed for primarily single houses.

External amenity considerations are informed by the development's orientation (overshadowing), landscaping contribution (tree canopy and deep soil areas), visual privacy (overlooking), interface to the public realm (public domain interface), pedestrian and vehicle access and car and bicycle parking (and considering whether there is adequate parking).

Our view is that there are unacceptable amenity impacts arising through overshadowing (refer shadow diagrams prepared by Architect engaged by residents) and anticipated impacts associated with inadequate car parking facilities for the oversized development. Further, there may be other amenity impacts arising from the occupancy type, with the possibility of many people living together.

Conclusion

In conclusion, we object to the proposed development on the basis of the above.

The local government is able to refuse the development having regard for Clause 67(y) which enables the local government to consider "any submissions received on the application". The local government is to have regard for the submissions received, take them seriously and is able to take a strong position against the development.

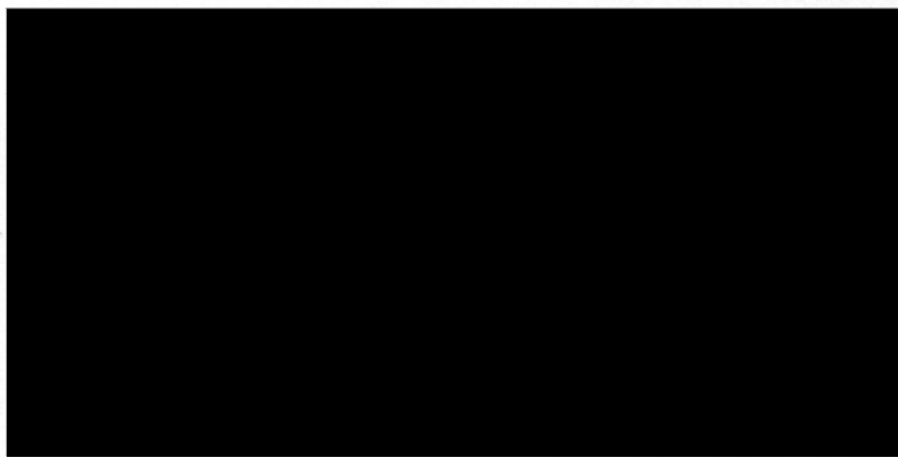
Overall, we have concerns with the proposed occupancy of the development, seemingly being designed for shared or student accommodation, with two of the dwellings providing in excess of 7 sleeping locations, which would ordinarily be classified as "Residential Building" uses. Such uses are to be considered as Use Not Listed, advertised for a wider/longer period under the Regulations, and greater discretion required to consider the use against the objective of the Residential zone.

Further, the plot ratio variation, resulting additional bulk and overshadowing impacts are not supported and are considered to impact on the amenity of adjoining residents.

Finally, the lack of car parking facilities, particularly for the larger dwellings, should not be supported.

We seek for the City to recommend refusal of the application to the DAP based on these issues.

Yours sincerely





Our ref: CHW:SRJ:1185492

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12 February 2025

Chief Executive Officer
City of Nedlands
71 Stirling Highway
NEDLANDS WA 6009

By Email: planning@nedlands.wa.gov.au

**cc: mayor.argyle@nedlands.wa.gov.au; cr.smyth@nedlands.wa.gov.au;
cr.amiry@nedlands.wa.gov.au; cr.hodsdon@nedlands.wa.gov.au;
cr.brackenridge@nedlands.wa.gov.au; cr.coghlan@nedlands.wa.gov.au;
cr.bennett@nedlands.wa.gov.au; cr.youngman@nedlands.wa.gov.au**

Dear Mrs Shannon

City of Nedlands – Development Application – 6 Alexander Road, Dalkeith

Overview

- 1 Lavan makes this submission on behalf of the owners listed in Appendix 1 to this letter (see **enclosed**) (together, the **Owners**).
- 2 The Owners object to the proposed Development Application for a Multiple Dwelling Residential Development (**Proposed Development**) at 6 Alexander Road, Dalkeith (**Subject Site**).
- 3 The Subject Site:
 - 3.1 is located within the City of Nedlands (**City**) local government district;
 - 3.2 is zoned “urban” pursuant to the *Metropolitan Region Scheme (MRS)*;
 - 3.3 is zoned “residential” pursuant to the City’s *Local Planning Scheme No. 3 (Scheme)*;

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Our Values:

Excellence
Courage
Collaboration
Fulfilment
Integrity

Lavan respectfully acknowledges the traditional owners of the land on which we live and work.

- 3.4 is zoned “R80” pursuant to the Scheme and the *Residential Design Codes (R-Codes)*, with certain elements of the R-Codes purported to be varied by local planning policy; and
- 3.5 is located on the Eastern side of a short section of Alexander Road, between Philip Road to the North and Waratah Avenue to the South, with a predominant housing type on that portion of Alexander Road being single houses developed to R10 and R20 densities.

LPP 5.14 and LPP 5.8

- 4 In or around August 2020, *Local Planning Policy 5.8 – Alexander and Philip Road Height Limit (LPP 5.8)* was adopted by the City to vary the R-Codes and to provide height limits for R40 and R80 lots to protect and enhance local character.
- 5 In or around June 2024, the City advertised its Draft Local Planning Policy – Precincts (available at [Draft Local Planning Policy - Precincts | Your Voice Nedlands](#)) (**Draft Notice**), which would later become its *Local Planning Policy 5.14 – Precincts (LPP 5.14)*. Relevantly, LPP 5.14 purports to amend elements of Volume 2 of the R-Codes, including building height, for precincts including the Waratah Precinct (which includes Alexander Road).
- 6 On 20 September 2024, the City published a notice entitled *Local Planning Policy 5.14: Precincts – Adopted August 2024* (available at [Public Notice - Local Planning Policy 5.14: Precincts - Adopted August 2024 » City of Nedlands](#)) (**Revocation Notice**). This Revocation Notice stated that LPP 5.14 had been adopted by the City and that the following local planning policies had been revoked:
- 6.1 *Local Planning Policy 4.3 – Waratah Village Laneway Requirements (LPP 4.3)*;
- 6.2 LPP 5.8;
- 6.3 *Local Planning Policy 5.10 – Broadway Precinct (LPP 5.10)*;
- 6.4 *Local Planning Policy 5.11 – Hampden Hollywood Precinct (LPP 5.11)*; and
- 6.5 *Local Planning Policy 5.12 – Nedlands Stirling Highway Activity Corridor – Residential (LPP 5.12)*.
- 7 Clause 4(1) of Schedule 2 of the *Planning and Development (Local Planning Schemes) Regulations 2015 (WA)* (**Deemed Provisions**) reads as follows:
- (1) *If the local government resolves to prepare a local planning policy the local government must, unless the Commission otherwise agrees, advertise the proposed policy as follows –*
- (a) *publish in accordance with clause 87 the proposed policy and a notice giving details of –*
- (i) *the subject and nature of the proposed policy; and*

- (ii) *the objectives of the proposed policy; and*
 - (iii) *how the proposed policy is made available to the public in accordance with clause 87; and*
 - (iv) *the manner and form in which submissions may be made; and*
 - (v) *the period for making submissions and the last day of that period;*
- (b) *if, in the opinion of the local government, the policy is inconsistent with any State planning policy, give notice of the proposed policy to the Commission;*
 - (c) *give notice of the proposed policy in any other way and carry out any other consultation the local government considers appropriate.*

8 It is our submission, with respect, that LPP 5.14 is invalid (entirely or in part – insofar as it purports to revoke LPP 5.8) as it does not comply with clause 4(1)(a)(i) of the Deemed Provisions.

9 We submit that the Draft Notice, being the notice that the City was required to publish in accordance with clause 4(1)(a) of the Deemed Provisions, is insufficient for the purposes of clause 4(1)(a)(i) as it does not give the required details of the subject and nature of LPP 5.14 (including the purported revocation of LPP 5.8).

10 This is because, in failing to mention the revocation of LPP 5.8 in its Draft Notice, the City did not appropriately outline the impact of LPP 5.14 on one of its existing local planning policies. Importantly, the remit of LPP 5.8, in seeking to balance planning discretion relating to building height in sensitive residential locations, was a significant factor for the community and would have been well understood by elected members. The City, in failing to appropriately advertise the revocation of LPP 5.8, has therefore not afforded procedural fairness to the community.

11 We submit that this failure is emphasised by the explicit reference in the Draft Notice to other local planning policies which were inevitably revoked by LPP 5.14, being LPP 5.10, LPP 5.11, LPP 5.12 and reference to the draft version of LPP 4.3.

12 In light of the above, it is our submission that, in the context of the lack of clarity or certainty as to the legal validity of LPP 5.14 (or indeed the revocation of LPP 5.8), the City is unable to satisfactorily consider any matters within the ambit of LPP 5.14, including the Proposed Development. Put simply, the Owners have significant concerns on the apparent gap in the planning framework as it currently applies to the Subject Site with the apparent revocation of LPP 5.8 and the potential invalidity of LPP 5.14.

13 In our view, careful consideration should be given by the City to the next steps to ensure certainty in the planning framework moving forward, including deferring the assessment of this application and clarifying the planning framework as it pertains to this locality.

Objection to Proposed Development

14 Although we are of the opinion that the City cannot consider the Proposed Development until questions surrounding the legal validity of LPP 5.14 and the revocation of LPP 5.8 are resolved, if the City does not agree with this conclusion and moves forward with the assessment of the Proposed Development, the Owners object to the Proposed Development for the following reasons.

15 Volume 2 of the R-Codes apply to this proposal (irrespective of any variation). Importantly, Volume 2 of the R-Codes are a performance-based policy, meaning that:

...Applications for development approval need to demonstrate that the design achieves the objectives of each design element. While addressing the Acceptable Outcomes is likely to achieve the Objectives, they are not a deemed-to-comply pathway and the proposal will be assessed in the context of the entire design solution to ensure the Objectives are achieved.

The *BHY* decision

16 In *BHY Alexander Unit Trust and City of Nedlands* [2021] WASAT 41 (***BHY***), the State Administrative Tribunal (**Tribunal**) considered an earlier but very similar development application which sought the construction of a four storey (above basement garage) building comprising four multiple dwellings at the Subject Site.

17 Somewhat coincidentally, the Tribunal had to contend with a planning framework in flex (including questions about the relevance of a local planning framework), but ultimately came to a conclusion based on the objectives of the relevant R-Codes provisions.

18 The decision of *BHY* should be carefully considered by the City, noting that the R-Codes considerations remain the same, and that the Proposed Application is significantly larger than the development application refused by the City and upheld by the Tribunal in *BHY*. In this respect, we emphasise that the Proposed Development seeks approval of an additional 11 bedrooms and 10 bathrooms, but 2 less car parking spaces, than the development application considered in *BHY*.

19 In *BHY*, the Tribunal noted that the development proposed achieved the element objectives of many of the design elements of Volume 2 of the R-Codes. Ultimately however, the Tribunal upheld the City's decision to refuse the development application considered in that case, finding that the proposed development did not achieve the following objectives from Volume 2 of the R-Codes:

19.1 Element Objectives O 2.2.1 (the height of development and desired future scale and character of the street) and O 2.2.4 (the height of development and the need for daylight and solar access) (see paragraphs 46 to 67 of *BHY*);

19.2 Element Objectives for design element 2.5 (plot ratio) (see paragraphs 68 to 76 of *BHY*); and

- 19.3 Element Objectives 2.2 (building height) and 3.2 (orientation), in particular O 3.2.2 (building form and orientation minimising overshadowing) (see paragraphs 72 to 85 of *BHY*).
- 20 In addition to these findings, the Tribunal also decided that the proposal was not consistent with elements of relevant considerations set out in clause 67 of the Deemed Provisions, namely, the likely impact on amenity (of the neighbouring property) and streetscape (both existing and likely future) (see paragraphs 86 to 107 of *BHY*).
- 21 In particular, the Tribunal recognised that the proposed application was “discordant and incompatible in its height in terms of its immediate streetscape setting and presents to the street with excessive bulk and scale when viewed in its immediate streetscape context” (see paragraph 90 of *BHY*). We would remind the City that the Proposed Development, at four-storeys, is the same height as the development considered in *BHY*, and fails to heed the Tribunal’s recommendations for height and setback as described at paragraph 116 of *BHY*.
- 22 In this respect, it is our submission that the Proposed Development fails to address or overcome any of the concerns highlighted in the *BHY* decision.
- 23 If anything, we submit that the streetscape has now become even more formalised than it was at the time of *BHY*, with the additional recent construction of single houses in the street (see discussion in paragraphs 52 and 90 to 91 of *BHY*).
- 24 The Tribunal noted in this respect that “the four storey presentation of the proposed development is not only discordant and incompatible with the current predominant building height in the immediate locality of the site (one to two storeys) and the two storey building height of the eight new single houses, including the adjoining and adjacent buildings to the site, but also with the likely future predominant building height in the immediate locality of the site (two to three storeys)” (see paragraph 56 of *BHY*).
- 25 In particular, the Proposed Development has not addressed the significant amenity impacts caused by the overshadowing and the visual impact of the bulk and scale of the development application considered in *BHY* (see paragraphs 101 to 107 of *BHY*).
- 26 In the *BHY* decision, the Tribunal actually confirmed that, to appropriately respond to the desired future scale and character of Alexander Road, the proposed development would need to present with an apparent height of three storeys (see discussion in paragraph 59 of *BHY*), particularly as it addressed its southern neighbour (8A Alexander Road, Dalkeith).
- 27 In addition, it is unclear from the plans submitted with the Proposed Development how it has addressed the previous concerns regarding overshadowing to the property at 8A Alexander Road, Dalkeith. The *BHY* decision considered in detail how Element Objective O 2.2.4 could be met, but the Proposed Development has failed to adequately demonstrate how it addresses the Tribunal’s concerns (see discussion at paragraphs 61 to 67 of *BHY*).

- 28 If anything, the Owners are concerned by the implications contained within the Design Report document prepared by Space Collective Architects and submitted with the Proposed Development (**Design Report**). The Owners remind the proponents that, as demonstrated by the *BHY* decision, it is incorrect to refer to the proposed development considered in *BHY* as “previously approved” (see pages 24 and 26 to 28 of the Design Report).
- 29 It is our submission, with respect, that the City cannot reasonably approve the Proposed Development when the *BHY* decision has made it clear that the correct and preferable decision in respect of these kinds of developments on the Subject Site is to refuse their approval. This submission applies irrespective of the weight given to LPP 5.14 or LPP 5.8 and is based on the fundamentals of the available discretion under the R-Codes.

Land use

- 30 In addition to the significant amenity and streetscape concerns which have already been considered by the Tribunal in *BHY*, we highlight to the City the substantial questions which exist surrounding the land use attached to the Proposed Development.
- 31 Although the Proposed Development is listed as a “Multiple Dwelling Residential development”, the sheer size of the individual units, coupled with the internal design, suggests that these units are not intended for residential use but are more consistent with designs that would be put forward for student or shared accommodation.
- 32 In particular, the communal open space and internal amenities further suggest that the Proposed Development will not be occupied by members of the same family and are therefore not designed with multi-generational living in mind. Consequently, development standards relating to parking requirements that usually apply to dwellings of a considerably smaller size result in anomalous outcomes, including a significant shortfall in onsite carparking.
- 33 We are instructed that the rationale provided by the proponent for the layout is to accommodate a large family in a multigenerational setting and the multiple dwelling format was simply used to allow for future ability to sell parts of the building when no longer required.
- 34 Importantly, if the Subject Site was proposed to be developed as a single dwelling (which is in essence what is being asked for by the proponent – multigenerational house), the building would be required to be significantly smaller in size and lower in height (and could not be decided by a Development Assessment Panel).

Summary

- 35 For the reasons outlined above, the Owners object to the Proposed Development.
- 36 In summary, it is our submission that:

- 36.1 in the context of the lack of clarity or certainty as to the legal validity of LPP 5.14, the City is unable to satisfactorily consider the Proposed Development; and
- 36.2 even if LPP 5.14 is found to be legally valid, the City should refuse the Proposed Development based on its failure to appropriately address the concerns highlighted in the *BHY* decision in relation to the key objectives in Volume 2 of the R-Codes and the lack of clarity surrounding the proposed land use.
- 37 Please do not hesitate to contact me or Sarah Jones should you have any queries.

Yours sincerely



Craig Wallace
Partner

Encl

Please notify us if this communication has been sent to you by mistake. If it has been, any privilege between solicitor and client is not waived or lost and you are not entitled to use it in any way.

<i>Architectural Design Review Assessment City of Nedlands Design Review Panel</i>	
Design quality evaluation	Date: 07/10/2024
Application: 6 Alexander Road	
Review No.: 1	Time: 4:30pm – 5.30pm
Panel: <ul style="list-style-type: none"> • Simon Anderson – Chair • Simon Venturi – Deputy Chair • Dominic Snellgrove • Tony Blackwell City of Nedlands Representatives: <ul style="list-style-type: none"> • Chantel Weerasekera – A/Coordinator Planning Approvals • Katerina Kovaceski - Urban Planning Administration Officer 	
Apply the applicable rating to each Design Principle	Supported
	Further information required
	Not supported
	Yet to be addressed
Summary	<u>Site Context</u> The subject site is zoned ‘Urban’ by the Metropolitan Region Scheme and ‘Residential’ by the City of Nedlands Local Planning Scheme No.3 and has a density coding of R80. The site is 1012m2 in area. <u>Application History</u> The application proposes a four storey residential building with five multiple dwellings and rooftop communal facilities. The application is in pre-lodgment stage. This is the first time the application has been presented to the design Review Panel.
Strengths of the proposal	<ul style="list-style-type: none"> • Instructive presentation including contextual analysis. • Interesting inter-generational living typology. • Landscape professional appointed and generous deep soil provision. • 100% cross ventilation and high levels of north facing solar access. • High levels of streetscape engagement and passive surveillance. • ESD professional appointed and an ESD narrative is being developed. • Natural light to lift lobby. • Parking concealed from view from public realm in centre of site. • Generous communal facilities for residents.
Principle 1 - Context and character	<i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i>
	<u>1a. Comments</u> <ul style="list-style-type: none"> • Passive surveillance is generally good. • Streetscape interaction could be improved. <u>1b. Suggested amendments/improvements</u> <ul style="list-style-type: none"> • Further justification is required for the height in the context of the new local planning policy. • Provide direct street pedestrian access for the ground floor front unit.
Principle 2 - Landscape quality	<i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i>

	<p><u>2a. Comments</u></p> <ul style="list-style-type: none"> • Landscape professional appointed early and initial landscape design looks positive. • Generos deep soil provided. • Concern with species selection - rainforest species lack adaptability. • Third floor landscaping appears difficult to maintain. • A hybrid front fence / wall is shown on the perspectives, but no further detail provided. <p><u>2b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Species selection needs attention. • Provide further information in relation to maintenance access provisions. • Provide details of front fence / wall.
<p>Principle 3 - Built form and scale</p>	<p><i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i></p> <p><u>3a. Comments</u></p> <ul style="list-style-type: none"> • Height being sought with a measurably detrimental impact on the adjoining property by way of overshadowing, especially over their PV solar panels. • Lack of mitigation of building bulk especially on the south west corner and 4 level high south wall with little setback or articulation. <p><u>3b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Introduce recess to south side of building footprint near lift core to break up building bulk. • Consider a 3 storey proposal as this would not have fewer challenges associated with overshadowing and approvals process. • Alternatively consider setting the 4th floor back to demonstrate no adverse effect on over-shadowing or streetscape in comparison to a 3 storey proposal. • Consider a setback to the 4th floor facing the streetscape to reduce the prominence and visibility of this level from the streetscape. • Further justification is required for plot ratio variation.
<p>Principle 4 - Functionality and build quality</p>	<p><i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life cycle.</i></p> <p><u>4a. Comments</u></p> <ul style="list-style-type: none"> • No sections or roof plan provided. • No indication of tanks and pumps – four storeys triggers sprinkler requirements. <p><u>4b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Provide sections and roof plans and indicate location of services. Ensure services are appropriately screened. • Consider the adaptability of the units if they were to be sold separately in the future.
<p>Principle 5 -</p>	<p><i>Good design optimises the sustainability of the built environment, delivering</i></p>

Sustainability	<i>positive environmental, social, and economic outcomes.</i>
	<p><u>5a. Comments</u></p> <ul style="list-style-type: none"> • Early appointment of an ESD professional is good however further information is required in relation to ESD. <p><u>5b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Continue developing ESD narrative and consider integrating further ESD initiatives. • Provide a comprehensive ESD Report for the project.
Principle 6 - Amenity	<i>Good design optimises internal and external amenity for occupants, visitors, and neighbours, providing environments that are comfortable, productive and healthy.</i>
	<p><u>6a. Comments</u></p> <ul style="list-style-type: none"> • Some apartments are awkwardly planned – might be a result of the width of the building. • Some rooms are currently shown without windows. • Lack of information on windows / visual privacy – Some visual privacy issue arise due to 2m northern setback. • Some balconies are small in relation to apartment sizes. <p><u>6b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Ensure all habitable rooms have openings. • Increase size of some balconies. • Provide further clarity on windows and visual privacy. • Consider introducing a recess on the south side of the building similar to the north side which will assist in providing all rooms with windows, visual privacy and overshadowing as well as reducing the visual bulk and scale of the south façade.
Principle 7 - Legibility	<i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i>
	<p><u>7a. Comments</u></p> <ul style="list-style-type: none"> • Entrance to rear ground level unit from carpark is abrupt. <p><u>7b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Improve the entrance experience to ground floor rear unit.
Principle 8 - Safety	<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>
	<p><u>8a. Comments</u></p> <ul style="list-style-type: none"> • Blind corner exists around stairwell. <p><u>8b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Resolve blind corner.
Principle 9 - Community	<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
	<p><u>9a. Comments</u></p> <ul style="list-style-type: none"> • Ground floor streetscape interaction could be improved. <p><u>9b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Provide direct street access for the ground floor front unit

	<ul style="list-style-type: none"> • Provide further detail on street fencing.
Principle 10 Aesthetics	<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
	<u>10a. Comments</u> <ul style="list-style-type: none"> • No elevations provided – insufficient information to adequately address aesthetics. • Southern elevation appears bulky. <u>10b. Suggested amendments/improvements</u> <ul style="list-style-type: none"> • Further information needed on materials and finishes. • Consider introducing a curve to the sharp SW corner which doesn't currently work with the Art Deco references and adding a recesses to break up the bulk and scale of the southern facades.
SUMMARY	Applicants are commended for coming in early in the design process. The proposed topology is interesting and the apartments will receive a high level of amenity. The deep soil is generous and appreciated. Further justification is required for the height and plot ratio sought, particularly in relation to the southern adjoining property which is unlikely to be developed. Further design development and detail is needed in relation to the overall bulk and scale as well as aesthetics.
OVERALL RECOMMENDATION	Not supported at this stage. Requires Further Attention

Design Review progress			
	<i>Supported</i>		
	<i>Further information required</i>		
	<i>Not yet supported</i>		
	<i>Yet to be addressed</i>		
	<i>DR1</i>	<i>DR2</i>	<i>DR3</i>
Principle 1 - Context and character			
Principle 2 - Landscape quality			
Principle 3 - Built form and scale			
Principle 4 - Functionality and build quality			
Principle 5 - Sustainability			
Principle 6 - Amenity			
Principle 7 - Legibility			
Principle 8 - Safety			
Principle 9 - Community			
Principle 10 - Aesthetics			

<i>Architectural Design Review Assessment City of Nedlands Design Review Panel</i>	
Design quality evaluation	Date: 04/11/2024
Application: 6 Alexander Road	
Review No.: 2	Time: 4:10pm – 5.20pm
Panel: <ul style="list-style-type: none"> • Simon Anderson – Chair • Simon Venturi – Deputy Chair • Dominic Snellgrove • Tony Blackwell City of Nedlands Representatives: <ul style="list-style-type: none"> • Dillon Reid – Senior Urban Planner • Katerina Kovaceski - Urban Planning Administration Officer 	
Apply the applicable rating to each Design Principle	■ <i>Supported</i>
	■ <i>Further information required</i>
	■ <i>Not supported</i>
	■ <i>Yet to be addressed</i>
Summary	<p><u>Site Context</u> The subject site is zoned ‘Urban’ by the Metropolitan Region Scheme and ‘Residential’ by the City of Nedlands Local Planning Scheme No.3 and has a density coding of R80. The site is 1012m² in area.</p> <p><u>Application History</u> The application proposes a four storey residential building with five multiple dwellings and rooftop communal facilities. The application is in pre-lodgment stage. This is the second time the application has been presented to the design Review Panel.</p>
Strengths of the proposal	<ul style="list-style-type: none"> • Comprehensive presentation. • Interesting inter-generational living typology. • Inclusion of services and utilities location, including fire tanks and pumps. • The increased setback of the 4th floor from the southern boundary contributes to reduced bulk and decreased overshadowing. • A curve has been added on the south west corner which softens the front elevation and a recess has been added on the south façade both of which provide a more cohesive aesthetic. • The entrance experience to the rear ground floor apartment has improved. • The increased size and extent of the front facing balconies is positive. • Landscape professional appointed with generous deep soil provision, generous planting in front setback and generally thoughtful landscape design. • 100% cross ventilation and high levels of north facing solar access. • Streetscape engagement and passive surveillance have improved through direct streetscape access for the ground level unit, the ground floor alfresco design and increased balcony widths. • Internal floor plans are generous and functional. • External materiality is improved and makes some reference to local character.

	<ul style="list-style-type: none"> • ESD professional appointed, a comprehensive ESD report including a Life Cycle Assessment and initiatives which exceed minimum requirements (no gas / all electric, EV charging, 30kW solar panels). • Natural light to lift lobby. • Parking concealed from view from public realm in centre of site. • Generous communal facilities for residents.
Principle 1 - Context and character	<p><i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i></p> <p><u>1a. Comments</u></p> <ul style="list-style-type: none"> • Nil <p><u>1b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Further justification related to the height and its relationship with the emerging character of the area may be required. Refer to comments in the Built Form & Scale Principle.
Principle 2 - Landscape quality	<p><i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i></p> <p><u>2a. Comments</u></p> <ul style="list-style-type: none"> • Concern with some species selection regarding Shot Hole Borer and the water sensitive nature of some tree / plant species. • Third floor landscaping which previously appeared difficult to access for maintenance purposes, now resolved. • A hybrid front fence / wall is shown on the perspectives, but no further detail provided. <p><u>2b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Species selection needs attention. • Provide details of front fence / wall.
Principle 3 - Built form and scale	<p><i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i></p> <p><u>3a. Comments</u></p> <ul style="list-style-type: none"> • Some ambiguity of the overshadowing over the southern neighbour remains. Overshadowing is potentially detrimental to the outdoor living space and solar panels of the adjoining lot. • The west end of the southern elevation is a blank façade and could be improved. • The minimal front setback of the upper levels may be imposing on the street. • Plot ratio exceeds the acceptable outcome and results in a bulky building. <p><u>3b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> • Further overshadowing information (ie plan view shadow diagrams) should be provided to determine clarity of solar access quality and quantity for the southern neighbour. • Consider the addition of highlight windows or further articulation to the western end of the southern elevation.

	<ul style="list-style-type: none"> Consider an increase in the setback or further articulation of the western façade at the upper levels to reduce the dominance of the building and visual prominence of the upper level on the street. Consider a reduction in the size of the building or further articulation of the built form to reduce massing. This may include larger or additional recesses on the southern elevation.
Principle 4 - Functionality and build quality	<p><i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life cycle.</i></p> <p><u>4a. Comments</u></p> <ul style="list-style-type: none"> A/C condensers are located on the southern roof / balcony and may not be easily accessible for all apartments given access through the upper level apartment is required. <p><u>4b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Consider relocation or redesign to enable independent access to A/C condensers.
Principle 5 - Sustainability	<p><i>Good design optimises the sustainability of the built environment, delivering positive environmental, social, and economic outcomes.</i></p> <p><u>5a. Comments</u></p> <ul style="list-style-type: none"> Error on roof plan specifying 50KW solar panel system. <p><u>5b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Fix error in ESD reporting / plan notation.
Principle 6 - Amenity	<p><i>Good design optimises internal and external amenity for occupants, visitors, and neighbours, providing environments that are comfortable, productive and healthy.</i></p> <p><u>6a. Comments</u></p> <ul style="list-style-type: none"> The floor plan on level 3 could be improved for functionality. The courtyard balcony space provided on level 1 may be low quality as it will be screened for the entire length. The ratio of outdoor and balcony space to internal apartment space is low. The south terrace on level 2 is screened to one side and may be a poor quality space. There may still be some visual privacy issues to resolve. The adaptability of the building for future multi-owner use is possible and could be improved. <p><u>6b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Reconfigure the space on level 3 so that the Gym and Theatre are one side of the entrance to the floor and the apartment door is on the other side to allow more flexible use by non-family members in the future. Increase the size and setback of some balconies. Consider minor modifications to the design which increase the adaptability of the building in the long term.
Principle 7 - Legibility	<p><i>Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.</i></p>

	<p><u>7a. Comments</u></p> <ul style="list-style-type: none"> Unclear which parking bays are for visitors which will also be located behind the secure line which often get used by residents rather than visitors. <p><u>7b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Denote which car bays are for visitors.
Principle 8 - Safety	<i>Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.</i>
	<p><u>8a. Comments</u></p> <ul style="list-style-type: none"> Nil <p><u>8b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Nil
Principle 9 - Community	<i>Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.</i>
	<p><u>9a. Comments</u></p> <ul style="list-style-type: none"> Nil <p><u>9b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Nil
Principle 10 Aesthetics	<i>Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.</i>
	<p><u>10a. Comments</u></p> <ul style="list-style-type: none"> Nil. <p><u>10b. Suggested amendments/improvements</u></p> <ul style="list-style-type: none"> Nil.
SUMMARY	The proposed topology is interesting and the apartment sizes are generous, however meaningful outdoor space on the upper levels may be lacking. Overall, the design has improved markedly from the initial concept. Further justification and information is required for the height and plot ratio sought, particularly in relation to the overshadowing of the southern adjoining property which is unlikely to be developed.
OVERALL RECOMMENDATION	Not supported at this stage. Requires Further Attention

Design Review progress			
	<i>Supported</i>		
	<i>Further information required</i>		
	<i>Not yet supported</i>		
	<i>Yet to be addressed</i>		
	<i>DR1</i>	<i>DR2</i>	<i>DR3</i>
Principle 1 - Context and character			
Principle 2 - Landscape quality			
Principle 3 - Built form and scale			
Principle 4 - Functionality and build quality			

Principle 5 - Sustainability	Yellow	Green	
Principle 6 - Amenity	Red	Yellow	
Principle 7 - Legibility	Yellow	Green	
Principle 8 - Safety	Yellow	Green	
Principle 9 - Community	Yellow	Green	
Principle 10 - Aesthetics	Yellow	Green	

From: [Simon Anderson](#)
To: [Kirsty Klimcke](#)
Subject: Re: 6 Alexander Road - Five Multiple Dwellings - Request for Chair Review
Date: Friday, 11 April 2025 12:23:48 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)

[EXTERNAL EMAIL] DO NOT CLICK links or attachments unless you recognise the sender and know the content is safe. If suspicious REPORT to IT.

Kirsty

Thank you for the opportunity to keep helping with this project.

I have now had a chance to study the amended plans and supporting documents and the response to the comments of the last DRP review.

The amended plans and documents, in my opinion, satisfactorily respond to the last DRP comments and I can support the amended design proposed. **Please note that this support assumes that LPP 5.14 is applicable.**

In particular and not exclusively, and in approximate order of the comments of the last DRP report, the proponents have:

- Greatly improved the front fence treatment
- Selected and located appropriate planting species
- Articulated the north and south elevations to reduce their bulk and scale
- Set back the top floor to the east and the north and reduced floor to floor heights to reduce bulk on the street and overshadowing to the south
- Reduced the plot ratio significantly
- Created a duplex massing on the street to relate to the narrow grain of neighbouring properties
- Resolved visual privacy issues
- Greatly improved room and outdoor living functionality and efficiency
- Presented modelling to indicate that southern boundary overshadowing meets the requirements of LPP 5.14 in respect of achieving a fourth storey

Overall the applicant has responded extensively and positively to the DRP advice. When asked to undertake a Chair review of amended plans I have always closed my comments with a statement of support or otherwise for the proposal in its current form. In this case and for the reasons outlined above and recognising my assumption of the applicability of LPP 5.14, I **support** the current proposal. Following is the updated DRP Design Quality Evaluation.

Simon

Emeritus Professor Simon Anderson LFRAIA
Senior Honorary Research Fellow
M433 School of Design
The University of Western Australia
35 Stirling Hwy, Crawley, Western Australia 6009
Mobile 0405 381 408
Email simon.anderson@uwa.edu.au

From: Kirsty Klimcke
Sent: Tuesday, April 8, 2025 9:57 AM
To: Simon Anderson
Subject: 6 Alexander Road - Five Multiple Dwellings - Request for Chair Review

Good morning Simon,

Revised plans for the above application, received 26 March 2025, have been submitted to the City for consideration, see link [here](#).

Additionally, the applicant has prepared a presentation for you independently responding to previous minutes, this is located within the drop box link.

Please see below list of changes from the applicant:

- Reduction in floor-to-floor height by 0.2m, resulting in an overall reduction in height of 0.8m.
- Repositioning of fourth floor dwelling (Unit 301) from the front (west) to the back (east) of the building and relocating communal amenities from the back (east) to the front (west).

- Increase to street setbacks on 2nd, 3rd and 4th floor.
- Plot ratio reduced from 1:25 to 1:18 (reduced by 81m2)
- Articulation provided to the southern boundary setbacks.
- Removal of pedestrian canopy on ground floor
- Reconfiguration of lift and stair core location and slight amendments to parking configuration (no changes to number of bays)
- Relocation of bin store
- Reconfiguration of parts of the ground floor
- Reduction in total number of bedrooms from 23 to 21 (Unit 101 and 201)
- Increase to outdoor living areas of Unit 101 and 201
- Addition of operable screening to western balconies of Units 101 and 202
- Updates to the overshadowing diagrams
- Updates to the Waste Management Plan
- Updates to the Traffic Technical Note
- Updates to the Landscape Design Report

The table below is a summary of the DRP scoring from the previous meetings. Can you please provide an update to the table to indicate your review, and any additional comments and/or recommendations.

The City's reports are required to be completed by next Tuesday 15th April given DAP deadlines. Can you please provide your comments via return email by no later than Monday 14th April 2025 12pm.

DRP Design Quality Evaluation			
	Supported		
	Further Information Required		
	Not supported		
SPP 7.0 Principles	7 October 2024 DRP 1	4 November 2024 DRP 2	Revision 3 (pending Chair Review)
Context and Character			
Landscape Quality			
Built Form and Scale			
Functionality and Built Quality			
Sustainability			
Amenity			
Legibility			
Safety			
Community			
Aesthetics			

Kind regards,

Kirsty Klimcke

Acting Senior Urban Planner



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SPP 7.3 R-CODES VOL ELEMENT 2.2	BUILDING HEIGHT	
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
<p>O2.2.1 – The height of development responds to the desired future scale and character of the street and local area, including existing buildings that are unlikely to change.</p>	<p>The height of the development is consistent with the future intended character of the locality, primarily comprising the R80 street block which contains development to a similar scale.</p>	<p>Objective not achieved</p> <p>The locality surrounding the subject site is defined as the stretch of Alexander Road bound by Philip Road to the north and Waratah Avenue to the south. This distinction of the locality was supported under WASAT 41.</p> <p>Alexander Road is a short street bound to the north by Philip Road and the south by Waratah Avenue. There are 17 properties that have direct street frontage onto Alexander Road. The eastern side of Alexander Road is coded R80, with the exception of the southern property at 99-105A Waratah Avenue which is coded R-AC3. Whilst the western side of the street is coded R40.</p> <p>Of the 17 properties, 8 properties consist of newly constructed double storey dwellings constructed between 2008 and 2015. Of these 8 properties, 5 are located on the eastern side of the road (R80) and 3 are located on the western side of the road (R40).</p>



The site at 9 Alexander Road is currently vacant and has recently been subdivided into two side by side lots similar to the remainder of the newly constructed single dwellings along the street. The site has received development approval and building permits to construct two double storey single dwellings.

The site at 5A and 5B Alexander Road has development approval for four multiple dwellings that are two storey in height.

The site at 3 Alexander Road has obtained a recent approval for two grouped dwellings. This configuration is a front and back design, with the development consisting of two double storey single dwellings.

Of the 16 other properties on the street, only three consist of older dwellings that are likely candidates for development potential (two on eastern side and 1 on the western side).

The site is surrounded by two storey single houses on three properties to the north (28 Philip, 4A and 4B

		<p>Alexander Road) and two to the south (8A and 8B Alexander Road).</p> <p>The proposed four storey development is not considered appropriate within the locality. The proposed development is out of character with the existing streetscape, which is single and two storeys in nature. It is assumed that the existing buildings and future developments (approved at 9A and 9B and 5A and 5B Alexander Road and 3 Alexander Road) are unlikely to change in the near future given the recent nature of construction or intention of construction. This is the first multiple dwelling development within the immediate locality, and the bulk and scale proposed would set an undesirable precedent for future developments.</p>
<p>O2.2.2 – The height of buildings within a development responds to changes in topography.</p>	<p>The topography of the development site remains relatively flat.</p>	<p>Objective achieved</p> <p>There is a slope of approximately 0.5m-0.6m from the north to the south of the site.</p> <p>The proposed finished floor level of 19.5 is an average of the natural ground level of the site.</p> <p>The finished floor level of the proposed building is approximately 0.5m higher than the finished floor level of the adjacent southern property at 8A Alexander Road and 0.8m lower than the finished floor level of the adjacent northern property at 4B Alexander Road.</p> <p>This demonstrates that the finished floor level of the proposal is responding to the natural ground level as view from the street with a stepped down approach from north to south.</p>
<p>O2.2.3 – Development incorporates articulated roof design and/or roof top communal open space where appropriate.</p>	<p>The development incorporates a high-quality roof top terrace with internal and external communal amenities. Roof design features articulation through varied setbacks and materiality to reduce visibility.</p>	<p>Objective achieved</p> <p>The development proposes a flat roof design as not to be dominant from the streetscape. Further, communal</p>

		open space has been located on the western portion of the four floor adjacent to the street.
<p>O2.2.4 – The height of development recognises the need for daylight and solar access to adjoining and nearby residential development, communal open space and in some cases, public spaces.</p>	<p>Throughout the Design Review Process, the design of the development has adapted to respond to comments from the Panel members to improve daylight and solar access to adjoining sites. The development responds to the context and provides considerable daylight and solar access to neighbouring properties. Space Collective has provided a comprehensive sunlight study which demonstrates the quantity and quality of sunlight provided for the southern adjoining property.</p>	<p>Objective not achieved</p> <p>The height of the proposed development does not appropriately recognise the need for solar access to the adjoining property at 8A Alexander Road. Although 2 hours of solar access have been provided for the existing solar panels and primary garden area, this is a bare minimum. As detailed below in <i>Orientation</i>, the development results does not consider the need for daylight and solar access to the adjoining southern property with all northern light to internal living spaces being completely overshadowed based on the plans the City has on file.</p>

ACCEPTABLE OUTCOMES

Acceptable Outcome pathway may not be applicable where a performance solution is provided

A2.2.1 – Development complies with the building height limit (storeys) set out in Table 2.1, except where modified by the local planning framework, in which case development complies with the building height limit set out in the applicable local planning instrument.

(Excerpt from table 2.1)

Streetscape contexts and character <i>refer A2</i>	Medium-rise	Higher density residential		Neighbourhood centre	Mid-rise urban centres	High density urban centres		Planned areas
Site R-Coding	R80	R100	R160	R-AC4	R-AC3	R-AC2	R-AC1	R-AC0
Building height (storeys) <i>refer 2.2</i>	4	4	5	3	6	7	9	

See below.

LOCAL PLANNING FRAMEWORK	REQUIREMENT
<p><i>Does the local planning framework amend or replace the above stated controls? If yes, state the applicable requirement:</i></p>	<p>Precincts LPP 5.14 Alexander Road sub precinct – R80 Multiple dwellings: 3 storeys¹ ¹ Additional storey can be considered where it is demonstrated that the proposal allows for 2 hours per day solar access on 21 June for existing solar panels and primary garden areas on neighbouring properties.</p> <p>Proposed: 4 storeys (12.8m) 2 hours of sunlight is achieved to solar panels of adjoining southern property between 11:30am and 1:30pm (City's assessment)</p>



ELEMENT 2.3		STREET SETBACKS	
ELEMENT OBJECTIVES		APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>		<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O2.3.1 – The setback of the development from the street reinforces and/or complements the existing or proposed landscape character of the street.	The proposed development is set back from the Alexander Road to compliment adjoining development, supporting the City’s desire for ground level residential uses within an active mixed-use locality. The 5.5m ground level setback exceeds the requirements of the R-Codes, and responds to the Local Policy framework	Objective achieved The street setbacks to all floors achieve the LPP 5.14 street setbacks which replace the Acceptable Outcomes. The setbacks presents an appropriate response to the existing streetscape which features varying setbacks between 5.5m and 11m. The proposed setbacks are considered to complement the surrounding existing single houses which are unlikely to change in the near future. The front setback area is landscaped to reinforce the traditional development pattern of the street. The fourth floor achieves a greater setback than as required in LPP 5.14 and consists of low wall to the pool so as to be less visually dominant on the streetscape.	
O2.3.2 – The street setback provides a clear transition between the public and private realm.	The street setback are at the ground floor has been designed to enhance public interaction through a visually permeable front fence, complimented with dedicated pedestrian entry to the ground level dwelling.	Objective achieved The street setback generally provides for an appropriate transition between the public and private realm via clear and legible entries and appropriate landscaping treatments.	
O2.3.3 – The street setback assists in achieving visual privacy to apartments from the street.	A larger ground level set back is proposed (as compared to the R-Codes) in a view to maintain the open streetscape appearance. Visual privacy is achieved through low scale fencing, landscaping and the positioning of active habitable spaces.	Objective achieved The location of the units’ internal living spaces, particularly bedrooms and living areas, are setback sufficiently from the street to provide expected levels of	

		<p>visual privacy. The outdoor living areas of most units are either screened from the street or setback adequately.</p> <p>A mixture of solid and visually permeable fencing along the primary street assists in providing privacy to the outdoor living area and internal habitable spaces of the ground floor front facing unit.</p>
<p>O2.3.4 – The setback of the development enables passive surveillance and outlook to the street.</p>	<p>Balconies are provided at each of the upper levels which directly overlook the street. The positioning of the ground level outdoor active area further enhances passive surveillance.</p>	<p>Objective achieved</p> <p>The street elevation proposes balconies which are setback approximately 6.0m from the primary street, providing for an uninterrupted visual outlook and adequate opportunities for passive surveillance</p>
<p>ACCEPTABLE OUTCOMES</p> <p><i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i></p>		
<p>A3.2.1 – Development complies with the street setback set out in Table 2.1, except where modified by the local planning framework, in which case development complies with the street setback set out in the applicable local planning instrument</p> <p><i>(Excerpt from table 2.1)</i></p>		<p>See below.</p>

Streetscape contexts and character <i>refer A2</i>	Medium-rise	Higher density residential		Neighbourhood centre	Mid-rise urban centres	High density urban centres		Planned areas
Site R-Coding	R80	R100	R160	R-AC4	R-AC3	R-AC2	R-AC1	R-AC0
Building height (storeys) <i>refer 2.2</i>	4	4	5	3	6	7	9	Refer to local planning scheme.
Boundary wall height (storeys)^{1,2} <i>refer 2.4</i>	2 ³	2 ³		2	3	4		
Minimum primary and secondary street setbacks <i>refer 2.3</i>	2m	2m		2m or Nil ⁵	2m or Nil ⁵	2m or Nil ⁵		

4) Minimum secondary street setback 1.5m

(5) Nil setback applicable if commercial use at ground floor

LOCAL PLANNING FRAMEWORK	REQUIREMENT
<p><i>Does the local planning framework amend or replace the above stated controls? If yes, state the applicable requirement:</i></p>	<p>Precincts LPP 5.14</p> <p>Alexander Road sub precinct – R80</p> <p>Ground and 1st: 5m</p> <p>All other storeys: 6m</p> <p>² Minimum primary street setback may be reduced by up to 2m, provided the area of any building (including garage or unenclosed balcony) encroaching into the setback areas is compensated for by at least equal area of open space (of which 50 per cent is provided as deep soil) that is located between the setback area and a line drawn parallel to it at twice the setback distance.</p> <p>Proposed:</p> <p>Ground: 5.0m</p>

	<p>First: 5.0m</p> <p>Second: 5.0m to planter – sufficient compensation provided behind setback line</p> <p>Third: 7.5m to pool wall</p>
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ELEMENT 2.4		SIDE AND REAR SETBACKS	
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT	
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>		
O2.4.1 – Building boundary setbacks provide for adequate separation between neighbouring properties.	The development is compliant with side boundary setbacks.	<p>Objective achieved</p> <p>The development proposes side setbacks ranging between 2.0m and 3.0m to the north and south on all levels. Additionally, the building features articulation along the northern and southern boundaries to provide separation between the development and adjoining properties.</p> <p>The development proposes rear setbacks ranging from 3.5m on the ground floor and 6m on all other floors.</p> <p>Building up to the boundary is proposed only along the northern boundary on the ground floor and boundary walls satisfy the Acceptable Outcomes as the boundary walls do not exceed two thirds of the boundary length.</p>	
O2.4.2 – Building boundary setbacks are consistent with the existing streetscape pattern or the desired streetscape character.	<p>The proposed development is consistent with the existing and future desired streetscape character by providing responsive, high-quality residential development in a landscaped setting.</p> <p>The development maintains a high degree of activation of the streetscape through the positioning of balconies and habitable spaces.</p>	<p>Objective partially achieved</p> <p>The setbacks themselves are consistent with the expected density code of R80. The setbacks range from 2m to 3m and are increased on the fourth floor to a maximum of 7.9m. The proposal itself is not considered consistent with the streetscape. It should be expected that the fourth floor provides greater setbacks along the northern and eastern boundaries.</p>	
O2.4.3 – The setback of development from side and rear boundaries enables retention of existing trees and provision of deep soil areas that reinforce the landscape character of the area,	Side and rear setbacks are compliant with the policy framework. The development achieves 16% deep soil area, with a comprehensive landscaping strategy to ensure a high-quality design response.	<p>Objective achieved</p> <p>The Strawberry Tree along the north-eastern boundary is to be retained. This retained tree will be incorporated into the design of the rear garden. Sufficient deep soil area</p>	

support tree canopy and assist with stormwater management.		has been provided for the existing and proposed new trees.
<p>O2.4.4 –The setback of development from side and rear boundaries provides a transition between sites with different land uses or intensity of development.</p>	<p>Site planning for the development has been informed by comprehensive assessment of existing development at adjoining properties. Priority has been given to maintaining a suitable scale, ensuring sunlight penetration and daylight to habitable areas of existing development.</p>	<p>Objective not achieved</p> <p>The adjoining northern, southern and eastern properties are single or two storey single dwellings. The proposed bulk of the building is exacerbated by the rectilinear form and lack of meaningful articulation along the northern and southern boundaries which results in a visual impression of overdevelopment, significant bulk and imposing appearance as viewed from both the street and adjoining properties. The development proposes bare minimum setbacks, particularly on the third and fourth floors. This does not present as a transitional scale which permits gradual setbacks between varying built form typologies. Instead, the proposal does little to reduce the perceived intensity of the development by presenting as an abrupt building ‘block’ to side and rear properties which feature single or two storey dwellings</p>
<p>ACCEPTABLE OUTCOMES</p>		
<p><i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i></p>		
<p>A2.4.1 - Development complies with the side and rear setbacks set out in Table 2.1, except where:</p> <ul style="list-style-type: none"> a) modified by the local planning framework, in which case development complies with the side and rear setbacks set out in the applicable local planning instrument <p>AND /OR</p> <ul style="list-style-type: none"> b) a greater setback is required to address 3.5 <i>Visual privacy</i>. <p><i>(Excerpt from table 2.1)</i></p>	<p>See below</p>	

Minimum side setbacks^a <i>refer 2.4</i>	3m	3m	Nil			General local dev plan and/or precinct controls as applicable	
Minimum rear setback <i>refer 2.4</i>	3m	6m	6m	Nil	Nil		
<p>(1) Wall may be built up to a lot boundary, where it abuts an existing or simultaneously constructed wall of equal or greater proportions</p> <p>(2) Where the subject site and an affected adjoining site are subject to different density codes, the length and height of any boundary wall on the boundary between them is determined by reference to the lower density code</p> <p>(3) Boundary wall only permitted on one boundary, and shall not exceed 2/3 length.</p> <p>Boundary setbacks will also be determined by provisions for building separation and visual privacy within this SPP and building separation provisions of the NCC.</p>							
A2.4.2 – Development is setback from the boundary in order to achieve the Objectives outlined in 2.7 <i>Building separation</i> , 3.3 <i>Tree canopy and deep soil areas</i> , 3.5 <i>Visual privacy</i> and 4.1 <i>Solar and daylight access</i> .							See Objectives 2.7 Building separation, 3.3 Tree canopy and deep soil areas, 3.5 Visual privacy
LOCAL PLANNING FRAMEWORK			REQUIREMENT				
<i>Does the local planning framework amend or replace the above stated controls? If yes, state the applicable requirement:</i>			<p>Precincts LPP 5.14</p> <p>Alexander Road sub precinct – R80</p> <p>Side setbacks</p> <p>Ground and 1st: 2m</p> <p>All other storeys: 3m</p> <p>Boundary walls</p> <p>1 storey</p> <p>2/3 the length of the lot boundary behind the front setback line</p> <p>3m behind primary setback line</p> <p>1 side</p> <p>Proposed:</p> <p><u>North</u></p> <p>G: Nil (24.8m) (24.8m / (50.5m - 5.0m) 45.5m = 54%, 1 storey, 11.1m behind front setback. Remainder setback 2.0m</p>				

1st floor: 2.0m minimum
 2nd floor: 3.0m minimum
 3rd floor: 4.5m minimum

South

G: 2.0m minimum
 1st floor: 2.0m minimum
 2nd floor: 3.0m minimum
 3rd story: 3.0m minimum

Rear av 6m, min 3m

Proposed:

G: min 3.5m to patio, av 6.19m

Setback	Length of setback portion	Proportionate weighting ((portion length / total length) x setback
12m	2.0m	1.2
7.0m	1.0m	0.35
3.5m	5.5m	0.9625
6.0m	8.2m	2.46
7.1m	1.0m	0.355
8.7m	2.0m	0.87
	TOTAL	6.1975m

1st floor: 6.0m
 2nd floor: 6.0m
 3rd floor: 7.5m

ELEMENT 2.5		PLOT RATIO	
ELEMENT OBJECTIVES		APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>		<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
<p>O2.5.1 – The overall bulk and scale of development is appropriate for the existing or planned character of the area.</p>		<p>A plot ratio of 1.18:1 is proposed, being 1,201.3m² of plot ratio floor area. Ultimately, the plot ratio proposed is not an accurate representation of the overall development scale, as a traditional multiple dwelling would incorporate additional exclusions (e.g., storerooms, corridors and lobbies etc.) which are not proposed. The 18% variation represents 189m² of PRFA.</p> <p>Overall building mass is managed through articulation and a careful consideration of siting on the Site. The upper level has been recessed and presented in a recessive colour pallet to reduce overall visual prominence.</p> <p>The proposed scale of the development is consistent with the future scale of development in the locality, promoted through the Precincts Policy – the Precincts Policy contemplates development to 4 storeys where 2-hours of sunlight is provided to adjoining sites. This has been achieved.</p>	<p>Objective not achieved</p> <p>The application proposes a plot ratio of 1.18 which exceeds the Acceptable Outcome by 182.2m².</p> <p>The overall bulk and scale of the development is not appropriate for the existing or planned character of the area. As discussed above, it is likely that the recently constructed two storey single houses occupying half of the residential properties in the immediate locality of the site, including, significantly, those that adjoin and are adjacent to the site on the three properties to the north and on the two properties to the south, are likely to remain for the foreseeable future. Furthermore, the planned construction of two storey single houses/multiple dwellings at 3, 5A and 5B and 9A and 9B Alexander Road, reinforce the statement of the ‘planned character of the area’.</p>
ACCEPTABLE OUTCOMES			
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>			

A2.5.1 – Development complies with the plot ratio requirements set out in Table 2.1, except where modified by the local planning framework, in which case development complies with the plot ratio set out in the applicable local planning instrument.

(Excerpt from table 2.1)

Streetscape contexts and character <i>refer A2</i>	Low-rise		Medium-rise		Higher density residential		Neighbourhood centre	Mid-rise urban centres	High density urban centres		Planned areas
	R40	R50	R60	R80	R100	R160	R-AC4	R-AC3	R-AC2	R-AC1	R-AC0
Plot ratio ⁷ <i>refer 2.5</i>	0.6	0.7	0.8	1.0	1.3	2.0	1.2	2.0	2.5	3.0	

Refer to Definitions for calculation of plot ratio

AO: 1.0

Original plans

Proposed: **1.25** – 1273.5m² / 1012m²

G: 272.5m²
 1st: 475.7m²
 2nd: 400m²
 3rd: 125.3m²

Amended plans received 26 March 2025

Proposed: **1.18** – 1191.7m² / 1012m²

G: 262m²
 1st: 423.2m²
 2nd: 383.5m²
 3rd: 123m²

ELEMENT 2.6		BUILDING DEPTH	
ELEMENT OBJECTIVES		APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>		<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O2.6.1 – Building depth supports apartment layouts that optimise daylight and solar access and natural ventilation.	The building and apartment layout is designed to optimise daylight access and natural ventilation to all apartments. All dwellings achieve 100% natural ventilation and solar access.	Objective achieved All apartments have a minimum of two aspects and have north-eastern facing openings to optimise opportunities for daylight and solar access and natural ventilation. Refer to 4.1 Solar and daylight access and 4.2 Natural ventilation.	
O2.6.2 – Articulation of building form to allow adequate access to daylight and natural ventilation where greater building depths are proposed.	All apartments are dual aspect and provided with natural gross ventilation.	Objective achieved The development does not propose any single aspect apartments which are located on each side of a central circulation corridor.	
O2.6.3 – Room depths and / or ceiling heights optimise daylight and solar access and natural ventilation.	Generous floor to ceiling heights are proposed throughout the development, increasing sunlight penetration into habitable spaces. Where deep rooms are proposed, dual aspect openings are provided to maximise cross ventilation and daylight penetration.	Objective achieved Room depths and ceiling heights provide adequate solar access and air flow to indoor habitable spaces.	
ACCEPTABLE OUTCOMES			
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>			
A2.6.1 – Developments that comprise single aspect apartments on each side of a central circulation corridor shall have a maximum building depth of 20m. All other proposals will be assessed on their merits with particular consideration to <i>4.1 Solar and daylight access</i> and <i>4.2 Natural ventilation</i> .		Development does not propose single aspect apartments. Each apartment addresses at least 2 lot boundaries with openings. Refer to 4.1 Solar and daylight access and 4.2 Natural ventilation.	

ELEMENT 2.7 BUILDING SEPARATION		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O2.7.1 – New development supports the desired future streetscape character with spaces between buildings.	Development siting is respectful of the neighbouring properties, while reflecting the desired future scale noting the recently adopted Precincts LPP.	Objective achieved The proposal achieves an appropriate setback from adjoining buildings as viewed from the street. The setbacks proposed reflect both the existing and desired future streetscape.
O2.7.2 – Building separation is in proportion to building height.	Separation between the proposed and existing development is reflective of the R80 density allocation and principally guided by providing good amenity outcomes directed by visual privacy setbacks.	Objective achieved Setbacks generally increase as the height increases in accordance with the R80 zone.
O2.7.3 – Buildings are separated sufficiently to provide for residential amenity including visual and acoustic privacy, natural ventilation, sunlight and daylight access and outlook.	Development siting provides for residential amenity, natural ventilation, sunlight and daylight access and mitigates overlooking.	Objective achieved The building achieves acceptable levels of internal amenity as discussed in the other sections throughout this assessment.
O2.7.4 – Suitable areas are provided for communal and private open space, deep soil areas and landscaping between buildings	Private open space is provided for each dwelling, combined with generous communal open space.	Objective achieved The development provides suitable areas for communal open space on the fourth floor and provides sufficient private open space per dwelling.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A2.7.1 – Development complies with the separation requirements set out in Table 2.7.	16.8m between ground floor units. All other dwellings are on own independent floor	

Table 2.7 Building separation

	Separation between:	Building height		
		≤ 4 storeys (up to 15m)	5-8 storeys (up to 28m)	≥ 9 storeys (over 28m)
Within site boundary	Habitable rooms/balconies	12m	18m	24m
	Habitable and non-habitable rooms	7.5m	12m	18m
	Non-habitable rooms	4.5m	6m	9m
To adjoining property boundaries	Habitable rooms/balconies and boundary	Refer 2.4 <i>Side and rear setbacks</i> (Table 2.1) and 3.5 <i>Visual privacy</i> (Table 3.5)	9m	12m

Distances apply from major openings of rooms, or the inside of balustrading of balconies.
Average dimensions may be applied subject to major openings meeting other requirements for privacy, daylight and the like.

Adjoining properties refer to 2.4 and 3.5

ELEMENT 3.2		ORIENTATION	
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT	
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>		
O3.2.1 – Building layouts respond to the streetscape, topography and site attributes while optimising solar and daylight access within the development.	The building has been designed to respond to the context and site orientation, achieving solar and daylight access compliance.	Objective achieved All dwellings have multiple aspects and optimise opportunities for daylight, solar access and natural ventilation.	
O3.2.2 – Building form and orientation minimises overshadowing of the habitable rooms, open space and solar collectors of neighbouring properties during mid-winter.	<p>Throughout the Design Review Panel process, the proposed development has undergone thorough analysis and has been adapted to minimise overshadowing on neighbouring properties.</p> <p>Lot boundary setbacks have also been increased to improve solar access into adjoining lots, and ensure the development sits comfortably within the setting.</p>	<p>Objective not achieved</p> <p>Given the R80 coding, there are no applicable Acceptable Outcomes for design element 3.2 Orientation.</p> <p>However, the proposal must still satisfy the Element Objectives as the R-Codes Volume 2 is a performance-based policy.</p> <p>The plans demonstrate that the proposed development does not aim to ‘minimise overshadowing’. Instead the development proposes to overshadow 78% of the southern property at 8A Alexander Road. The building will entirely overshadow all north facing openings of the dwelling at 8A Alexander Road. Based on the plans the City has on file, the dwelling features multiple large north facing openings, in both the ground and upper floors.</p>	
ACCEPTABLE OUTCOMES			

Acceptable Outcome pathway may not be applicable where a performance solution is provided

<p>A3.2.1 – Buildings on street or public realm frontages are oriented to face the public realm and incorporate direct access from the street.</p>	<p>The development addresses Alexander Road and offers direct pedestrian and vehicle access from the street.</p>
<p>A3.2.2 – Buildings that do not have frontages to streets or public realm are oriented to maximise northern solar access to living areas.</p>	<p>N/A</p>
<p>A3.2.3 – Development in climate zones 4, 5 and 6 shall be designed such that the shadow cast at midday on 21st June onto any adjoining property does not exceed:</p> <ul style="list-style-type: none"> – adjoining properties coded R25 and lower – 25% of the site area¹ – adjoining properties coded R30 – R40 - 35% of the site area¹ adjoining properties coded R50 – R60 – 50% of the site area¹ – adjoining properties coded R80 or higher – Nil requirements. <p>(1) Where a development site shares its southern boundary with a lot, and that lot is bound to the north by other lot(s), the limit of shading at A3.2.3 shall be reduced proportionally to the percentage of the affected properties northern boundary that abuts the development site. (Refer to Figure A7.2 in Appendix 7)</p>	<p>Adjoining sites are coded R-80; nil requirement applicable.</p>
<p>A3.2.4– Where adjoining sites are coded R40 or less, buildings are oriented to maintain 4 hours per day solar access on 21 June for existing solar collectors on neighbouring sites.</p>	<p>N/A. Adjoining sites are coded R80.</p>

ELEMENT 3.3 TREE CANOPY AND DEEP SOIL AREAS		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O3.3.1 – Site planning maximises retention of existing healthy and appropriate and protects the viability of adjoining trees.	The site is largely cleared of remnant vegetation. A mature tree is proposed to be retained in the north-east of the site.	Objective partially achieved The existing site features three trees. One tree, a Strawberry Tree, is proposed to be retained at rear of site. Whilst the retention of one tree is appreciated, no Arborists Report has been provided to support the removal of the remaining two trees, nor to demonstrate that the development will protect the viability of the retained tree.
O3.3.2 – Adequate measures are taken to improve tree canopy (long term) or to offset reduction of tree canopy from pre-development condition.	Six trees are proposed at ground level; 2 trees are proposed for the upper-level communal area. Each ground level tree is provided with adequate deep soil area.	Objective partially achieved The proposed landscaping plan includes the provision of two new medium trees and four new small trees, in addition to the Strawberry Tree being retained. Whilst this involves a net increase of four trees from the pre-development condition, it is not considered an ideal design response given that there is the opportunity to provide larger trees within the front and rear.
O3.3.3 – Development includes deep soil areas, or other infrastructure to support planting on structures, with sufficient area and volume to sustain healthy plant and tree growth.	As outlined in the Landscaping Plan, 135m² of deep soil is proposed, being 13% of the site area. Deep soil areas are principally provided to the front and rear of the site, managing the interaction of the development with adjoining properties and the streetscape.	Objective achieved The development incorporates 135m ² / 13% of deep soil areas, which exceeds the area recommended by A3.3.4. A majority of this area is consolidated to the front and rear of the site where the width and depth of the soil is sufficient to support healthy plant and tree growth.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		

<p>A3.3.1 – Retention of existing trees on the site that meet the following criteria:</p> <ul style="list-style-type: none"> – healthy specimens with ongoing viability AND – species is not included on a State or local area weed register AND – height of at least 4m AND/OR – trunk diameter of at least 160mm, measured 1m from the ground AND/OR – average canopy diameter of at least 4m. 	<p>The north-eastern tree is proposed to be retained (Strawberry Tree). Health and viability of the retention of the tree will be monitored throughout development. Arborist report is required to determine the specifications of the tree.</p>											
<p>A3.3.2 – The removal of existing trees that meet any of the criteria at A3.3.1 is supported by an arboriculture report.</p>	<p>Not supplied.</p>											
<p>A3.3.3 – The development is sited and planned to have no detrimental impacts on, and to minimise canopy loss of adjoining trees.</p>	<p>The development does not impact on any street trees or trees on adjoining sites.</p>											
<p>A3.3.4 – Deep soil areas are provided in accordance with Table 3.3a. Deep soil areas are to be co-located with existing trees for retention and/or adjoining trees, or alternatively provided in a location that is conducive to tree growth and suitable for communal open space.</p> <p>Table 3.3a Minimum deep soil area and tree provision requirements</p> <table border="1" data-bbox="152 786 763 1297"> <thead> <tr> <th>Site Area</th> <th>Minimum deep soil area</th> <th>Minimum requirement for trees¹</th> </tr> </thead> <tbody> <tr> <td>Less than 700m²</td> <td rowspan="2">10%</td> <td>1 medium tree and small trees to suit area</td> </tr> <tr> <td>700 – 1,000m²</td> <td>2 medium trees OR 1 large tree and small trees to suit area</td> </tr> <tr> <td>> 1,000m²</td> <td>7% if existing tree(s) retained on site (% site area)</td> <td>1 large tree and 1 medium tree for each additional 400m² in excess of 1000m² OR 1 large tree for each additional 900m² in excess of 1000m² and small trees to suit area</td> </tr> </tbody> </table> <p>¹ Minimum requirement for trees includes retained or new trees Refer Table 3.3b for tree sizes</p>	Site Area	Minimum deep soil area	Minimum requirement for trees ¹	Less than 700m ²	10%	1 medium tree and small trees to suit area	700 – 1,000m ²	2 medium trees OR 1 large tree and small trees to suit area	> 1,000m ²	7% if existing tree(s) retained on site (% site area)	1 large tree and 1 medium tree for each additional 400m ² in excess of 1000m ² OR 1 large tree for each additional 900m ² in excess of 1000m ² and small trees to suit area	<p>Site Area: 1012m² DSA required: 101.2m² 10% of site area DSA provided: 135m² (13%).</p>
Site Area	Minimum deep soil area	Minimum requirement for trees ¹										
Less than 700m ²	10%	1 medium tree and small trees to suit area										
700 – 1,000m ²		2 medium trees OR 1 large tree and small trees to suit area										
> 1,000m ²	7% if existing tree(s) retained on site (% site area)	1 large tree and 1 medium tree for each additional 400m ² in excess of 1000m ² OR 1 large tree for each additional 900m ² in excess of 1000m ² and small trees to suit area										

A3.3.5 – Landscaping includes existing and new trees with shade producing canopies in accordance with Tables 3.3a and 3.3b.

Table 3.3b Tree sizes

Tree size	Indicative canopy diameter at maturity	Nominal height at maturity	Required DSA per tree	Recommended minimum DSA width	Minimum DSA width where additional rootable soil zone (RSZ) width provided ¹ (min 1m depth)	Indicative pot size at planting
Small	4-6m	4-8m	9m ²	2m	1m (DSA) + 1m (RSZ)	100L
Medium	6-9m	8-12m	36m ²	3m	2m (DSA) + 1m (RSZ)	200L
Large	>9m	>12m	64m ²	6m	4.5m (DSA) + 1.5m (RSZ)	500L

¹ Rootable areas are for the purposes of determining minimum width only and do not have the effect of reducing the required DSA.

Landscaping includes 4 new small trees and 2 new medium trees which achieve the acceptable outcome.

A3.3.6 – The extent of permeable paving or decking within a deep soil area does not exceed 20 per cent of its area and does not inhibit the planting and growth of trees.

Compliant

A3.3.7 – Where the required deep soil areas cannot be provided due to site restrictions, planting on structure with an area equivalent to two times the shortfall in deep soil area provision is provided.

In addition to achieving 13% deep soil, the application proposes approximately 16.8m² on structure planting

ELEMENT 3.4	COMMUNAL OPEN SPACE	
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O3.4.1 – Provision of quality communal open space that enhances resident amenity and provides opportunities for landscaping, tree retention and deep soil areas.	The proposed development incorporates extensive areas of communal open space on the roof terrace. The area of communal open space is also extensively landscaped to encourage communal use for amenity.	Objective achieved The communal open space will enhance the resident amenity by providing features such as a pool, entertaining space and a massage room.
O3.4.2 – Communal open space is safe, universally accessible and provides a high level of amenity for residents.	The communal open space areas have been designed by See Design and Space Collective. All areas of communal open space are universally accessible and serve to provide high quality amenity for residents, providing internal and external amenity spaces.	Objective achieved The communal open space is safe with access via the stairs only by residents of the building. Amenity is ensured through the facilities provided.
O3.4.3 – Communal open space is designed and oriented to minimise impacts on the habitable rooms and private open space within the site and of neighbouring properties.	Communal open space has been set back from lot boundaries to minimise the potential for actual overlooking, and manage perceived overlooking.	Objective achieved subject to condition Communal open space is located separately to the proposed dwellings. It is unlikely that noise will impact the habitable rooms of the dwellings. Should DAP choose to approve the application, a condition of approval is recommended to provide an Acoustic Report to identify noise sources.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A3.4.1 – Developments include communal open space in accordance with Table 3.4		Less than 10 dwellings Required: 0m ² Proposed: 222.9m ²

Table 3.4 Provision of communal open space			
Development size	Overall communal open space requirement	Minimum accessible / hard landscape area (included in overall area requirement)	Minimum open space dimension
Up to 10 dwellings	Informal seating associated with deep soil or other landscaped areas	NA	NA
More than 10 dwellings	Total: 6m ² per dwelling up to maximum 300m ²	At least 2m ² per dwelling up to 100m ²	4m

A3.4.2 – Communal open space located on the ground floor or on floors serviced by lifts must be accessible from the primary street entry of the development.	Communal open space is accessible from the primary street via lobby and lift
A3.4.3 – There is 50 per cent direct sunlight to at least one communal open space area for a minimum of two hours between 9am and 3pm on 21 June.	Direct northern sunlight to pool area open space and fourth floor internal communal space
A3.4.4 – Communal open space is co-located with deep soil areas and/or planting on structure areas and/ or co-indoor communal spaces.	Landscaping to communal areas provided.
A3.4.5 – Communal open space is separated or screened from adverse amenity impacts such as bins, vents, condenser units, noise sources and vehicle circulation areas.	Bins and vehicular circulation areas provided on ground floor.
A3.4.6 – Communal open space is well-lit, minimises places for concealment and is open to passive surveillance from adjoining dwellings and/or the public realm.	Well lit and no places for concealment
A3.4.7 – Communal open space is designed and oriented to minimise the impacts of noise, odour, light-spill and overlooking on the habitable rooms and private open spaces within the site and of neighbouring properties.	Communal open space is orientated towards the street

ELEMENT 3.5		VISUAL PRIVACY																		
ELEMENT OBJECTIVES		APPLICANT COMMENT		ASSESSOR COMMENT																
<i>Development is to achieve the following Element Objectives</i>		<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>																		
<p>O3.5.1 – The orientation and design of buildings, windows and balconies minimises direct overlooking of habitable rooms and private outdoor living areas within the site and of neighbouring properties, while maintaining daylight and solar access, ventilation and the external outlook of habitable rooms.</p>		<p>No direct overlooking is caused by the development. Site planning has been carefully managed to ensure the cone of vision does not extent into neighbouring properties. Equally, consideration has been given to the design of adjoining development to ensure amenity and privacy is maintained for future residents of the development.</p>		<p>Objective achieved</p> <p>The openings to bedrooms, living rooms and terraces are either provided with screening or set back adequate distances as referred to in Table 3.5.</p> <p>201 front balconies are setback 5.7m from the northern and southern boundaries. This cone of vision falls onto driveways or front setback areas. This will not impact habitable rooms or private living spaces of the adjoining dwellings.</p>																
ACCEPTABLE OUTCOMES																				
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>																				
<p>A3.5.1 – Visual privacy setbacks to side and rear boundaries are provided in accordance with Table 3.5.</p> <p>Table 3.5 Required privacy setback to adjoining sites</p>		<p>All adjoining properties coded R80</p> <p>101</p> <p>Front balcony – 6.0m to north and south</p> <p>Bedrooms – 2.0m to north and south (reliant on high level windows)</p> <p>Terraces – north and south entirely screened</p> <p>Rear balcony/bedrooms – 6.0m east, north and south</p> <p>201</p> <p>Front balcony - north and south 5.7m in lieu of 6.0m</p> <p>Bedroom – north 3.0m</p> <p>Terraces – north and south entirely screened</p> <p>Rear balcony/bedrooms – 6.0m east, north and south</p> <p>301</p> <p>Communal space – north screened, south setback 6m+</p>																		
<table border="1"> <thead> <tr> <th rowspan="2">Cone of vision from unscreened:</th> <th colspan="2">First 4 storeys</th> <th rowspan="2">5th storey and above</th> </tr> <tr> <th>Adjoining sites coded R50 or lower</th> <th>Adjoining sites coded higher than R50</th> </tr> </thead> <tbody> <tr> <td>Major opening to bedroom, study and open access walkways</td> <td>4.5m</td> <td>3m</td> <td rowspan="3">Refer Table 2.7</td> </tr> <tr> <td>Major openings to habitable rooms other than bedrooms and studies</td> <td>6m</td> <td>4.5m</td> </tr> <tr> <td>Unenclosed private outdoor spaces</td> <td>7.5m</td> <td>6m</td> </tr> </tbody> </table>		Cone of vision from unscreened:	First 4 storeys		5th storey and above	Adjoining sites coded R50 or lower	Adjoining sites coded higher than R50	Major opening to bedroom, study and open access walkways	4.5m	3m	Refer Table 2.7	Major openings to habitable rooms other than bedrooms and studies	6m	4.5m	Unenclosed private outdoor spaces	7.5m	6m			
Cone of vision from unscreened:	First 4 storeys		5th storey and above																	
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Major openings to habitable rooms other than bedrooms and studies	6m	4.5m																		
Unenclosed private outdoor spaces	7.5m	6m																		

	Bedrooms – north setback 4.5m Terrace – screened to north, setback 6m to east Living/bed east – setback 6m+
A3.5.2 – Balconies are unscreened for at least 25 per cent of their perimeter (including edges abutting a building).	Achieved
A3.5.3 - Living rooms have an external outlook from at least one major opening that is not obscured by a screen.	Living spaces have outlook independently or not impact by screening
A3.5.4 – Windows and balconies are sited, oriented, offset or articulated to restrict direct overlooking, without excessive reliance on high sill levels or permanent screening of windows and balconies.	The development seeks to provide major openings where applicable on site whilst using screening when required to provide sufficient privacy to neighbours.

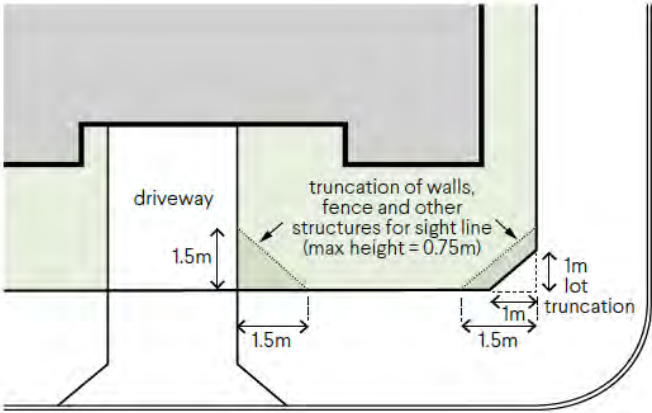
ELEMENT 3.6 PUBLIC DOMAIN INTERFACE		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O3.6.1 – The transition between the private and public domain enhances the privacy and safety of residents.	The transition between the public and private domain is delineated by way of a low-scale, permeable fence. The street setback area is occupied by a substantial outdoor living area which enhances use, privacy and interaction.	Objective achieved There is a clear separation between the public and private domain. The proposal is considered to provide a sufficient level of privacy and safety for residents. Four units address the public domain and provide for adequate passive surveillance.
O3.6.2 – Street facing development and landscape design retains and enhances the amenity and safety of the adjoining public domain, including the provision of shade.	Substantial contextual analysis was undertaken as part of design development to ensure the development and landscape concept is reflective of the locality.	Objective achieved Street facing development provides an active frontage via direct pedestrian access to the ground floor front facing unit and the main residential entry to the side. The front setback area is well landscaped to enhance the amenity and visual appeal of the development. The two existing street trees will be complementary to the development and streetscape.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A3.6.1 – The majority of ground floor dwellings fronting onto a street or public open space have direct access by way of a private terrace, balcony or courtyard.		One ground floor unit proposed with direct access to the street via terrace.
A3.6.2 – Car-parking is not located within the primary street setback; and where car parking is located at ground level behind the street setback it is designed to integrate with landscaping and the building façade (where part of the building).		Car parking is not provided in the street setback and is located in garage which is screened from view of street.
A3.6.3 – Upper level balconies and/or windows overlook the street and public domain areas.		The balconies for 101 and 201 overlook the street.

<p>A3.6.4 – Balustrading includes a mix of visually opaque and visually permeable materials to provide residents with privacy while maintaining casual surveillance of adjoining public domain areas.</p>	<p>Balustrading uses a mix of solid and visually permeable materials.</p>
<p>A3.6.5 – Changes in level between private terraces, front gardens and the ground floor level of the building and the street level average less than 1m and do not exceed 1.2m.</p>	<p>Maximum level difference between the street and the GF FFL is approximately 0.16m</p>
<p>A3.6.6 – Front fencing includes visually permeable materials above 1.2m and the average height of solid walls or fences to the street does not exceed 1.2m.</p>	<p>Solid to 1.2m, visually permeable above.</p>
<p>A3.6.7 – Fencing, landscaping and other elements on the frontage are designed to eliminate opportunities for concealment.</p>	<p>Entrance off the street is direct and offers no opportunities for concealment.</p>
<p>A3.6.8 – Bins are not located within the primary street setback or in locations visible from the primary street.</p>	<p>Bins are not located within the primary street setback and are screened from the street.</p>
<p>A3.6.9 – Services and utilities that are located in the primary street setback are integrated into the design of the development and do not detract from the amenity and visual appearance of the street frontage.¹ (1) Firefighting and access to services such as power and water meters require careful consideration in the design of the front façade. Consult early with relevant authorities to resolve functional requirements in an integrated design solution.</p>	<p>Services will be standard-type connections given the small number of dwellings. These will be located as required to meet utility provider requirements. The standard water and power connections necessary will use equipment that is commonly seen in the locality.</p>

ELEMENT 3.7 PEDESTRIAN ACCESS AND ENTRIES		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O3.7.1 – Entries and pathways are universally accessible, easy to identify and safe for residents and visitors.	All pathways and entrances are universally accessible. The main entrance is weather protected for residents and visitors.	Objective achieved The entry pathway is directly accessible from the street, easy to identify, and is safe for residents and visitors. The vehicle entry point is distinctly separate from the pedestrian entry so there is no opportunity for pedestrian and vehicles to conflict. The pedestrian entry is at-grade and step-free and the entry experience will be safe and easily navigable
O3.7.2 – Entries to the development connect to and address the public domain with an attractive street presence.	A dedicated pedestrian entrance is provided to the development (which is separate from the vehicle access), which connects directly to the public realm.	Objective achieved Entries to building connect directly to the public realm with an attractive street presence and an active frontage. The pedestrian entry provides a straight path to the building without opportunities for concealment and is provides for weather protection.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A3.7.1 – Pedestrian entries are connected via a legible, well-defined, continuous path of travel to building access areas such as lift lobbies, stairs, accessways and individual dwelling entries.		The pedestrian entry pathway leads directly to lobby door and lift.
A3.7.2 – Pedestrian entries are protected from the weather.		Pedestrian entry is covered and protected from the weather.
A3.7.3 – Pedestrian entries are well-lit for safety and amenity, visible from the public domain without opportunity for concealment, and designed to enable casual surveillance of the entry from within the site.		Pedestrian entry provides a straight path to the building without opportunities for concealment.

<p>A3.7.4 – Where pedestrian access is via a shared zone with vehicles, the pedestrian path is clearly delineated and/or measures are incorporated to prioritise the pedestrian and constrain vehicle speed.</p>	<p>N/A – Pedestrian Access is not taken via a shared zone.</p>
<p>A3.7.5 – Services and utilities that are located at the pedestrian entry are integrated into the design and do not detract from the amenity of the entry.</p>	<p>No services or utilities that are located at the pedestrian entry.</p>
<p>A3.7.6 – Bins are not located at the primary pedestrian entry.</p>	<p>Bin store is internal to the site and outside the pedestrian entry.</p>

ELEMENT 3.8		VEHICLE ACCESS	
ELEMENT OBJECTIVES		APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>		<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O3.8.1 – Vehicle access points are designed and located to provide safe access and egress for vehicles and to avoid conflict with pedestrians, cyclists and other vehicles.	The vehicle access point has been designed to minimise conflict through the separation of vehicle and pedestrian entries. Landscaping elements have also been provided between the pedestrian entry and vehicle access point to reinforce separation.	Objective achieved The development proposes one 3m wide vehicle access points via Alexander Road. This is distinctly separated from the pedestrian access to avoid potential conflict. All vehicle access points are legible and safe. Although sightlines do not achieve A3.8.7, the pedestrian path is located directly adjacent to the road. Therefore, vehicles have an additional 5.0m upon exiting the site until the pedestrian path.	
O3.8.2 – Vehicle access points are designed and located to reduce visual impact on the streetscape.	The vehicle access point is positioned adjacent the southern boundary, allowing a submissive design outcome through the complete expression of development across the Site. The recessive design allows the façade of the development to be the dominant design outcome.	Objective achieved The vehicle access point has been limited to one for the development and has been kept to a functional minimum (3.0m). The proposed vehicle access point will not negatively impact the streetscape and is comparable to the existing crossover/vehicle access point at the site.	
ACCEPTABLE OUTCOMES			
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>			
A3.8.1 – Vehicle access is limited to one opening per 20m street frontage that is visible from the street.		One vehicle access point via Alexander Road.	
A3.8.2 – Vehicle entries are identifiable from the street, while being integrated with the overall façade design and/ or located behind the primary building line.		The vehicle entry is clear from Alexander Road.	
A3.8.3 – Vehicle entries have adequate separation from street intersections.		The vehicle entry is not nearby street intersections.	

<p>A3.8.4 – Vehicle circulation areas avoid headlights shining into habitable rooms within the development and adjoining properties.</p>	<p>Vehicle circulation areas are within parking garage. No habitable rooms will be impacted by headlights.</p>
<p>A3.8.5 – Driveway width is kept to a functional minimum, relative to the traffic volumes and entry/egress requirements.</p>	<p>Driveway is 3.0m in width.</p>
<p>A3.8.6 – Driveways designed for two way access to allow for vehicles to enter the street in forward gear where:</p> <ul style="list-style-type: none"> - the driveway serves more than 10 dwellings - the distance from an on-site car parking to the street is 15m or more OR - the public street to which it connects is designated as a primary distributor, district distributor or integrated arterial road. 	<p>Driveway serves less than 10 dwellings. 29.5m distance from the street to under-croft parking. The driveway does not provide for two-way access but does allow for forward gear entry and exit. Swept path analysis provided</p>
<p>A3.8.7 – Walls, fences and other structures truncated or reduced to no higher than 0.75m within 1.5m of where walls, fences, other structures adjoin vehicle access points where a driveway meets a public street and where two streets intersect (refer Figure 3.8a).</p>  <p>Figure 3.8a Truncation at street corner to provide sightlines (refer A3.8.7).</p>	<p>Northern VCA 0.8m solid fencing proposed Should DAP choose to approve the application, a condition of approval has been recommended requiring the relocation of this wall outside of the sight lines truncation.</p>

ELEMENT 3.9 CAR AND BICYCLE PARKING		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O3.9.1 – Parking and facilities are provided for cyclists and other modes of transport.	Two dedicated bicycle spaces are provided at ground level of the development for residents. Visitor bike parking is provided separately, toward the front of the development. However, given the size of each dwelling and generous storage areas, bikes are expected to be stored within dwellings.	Objective achieved A total of 5 bicycle parking spaces are provided (one for each dwelling and visitor).
O3.9.2 – Car parking provision is appropriate to the location, with reduced provision possible in areas that are highly walkable and/or have good public transport or cycle networks and/or are close to employment centres.	Parking provision is compliant with overall dwelling yield and is appropriate having regard to the locality, availability of public transport and multigenerational nature of the development.	Objective not achieved The development has been provided with a total of 10 car parking bays comprising of 8 residents bays and 2 visitor bays. This achieves A3.9.2. The ground floor comprises of two 3 bedroom units, the first floor comprises of an 5 bedroom unit, the second floor comprises of a 7 bedroom unit and the third floor comprises another 3 bedroom unit. Across all floors, the development features 21 bedrooms. The development has the potential to house 21 persons (1 per bedroom) or 42 persons (2 per bedroom). The car parking provision provided is not appropriate to the location. It is unreasonable to apply the same requirements of a 2+ bedroom dwelling to a 5 or 7 bedroom dwelling. The lack of onsite parking for the proposed residents will negatively impact the locality via way of residents using street parking on Alexander Road.

<p>O3.9.3 – Car parking is designed to be safe and accessible.</p>	<p>Swept paths are provided within the Traffic report which demonstrates vehicles can safely manoeuvre within the site to leave in forward gear. .</p>	<p>Objective achieved Car parking circulation area meets AS2890.1 standards and provides direct access to the building without having to return to the street.</p>																									
<p>O3.9.4 – The design and location of car parking minimises negative visual and environmental impacts on amenity and the streetscape.</p>	<p>Car parking is sleeved behind the ground level dwelling, minimising any negative visual impact.</p>	<p>Objective achieved The location of car parking is hidden from view of the street and located in the centre of the ground floor of the development.</p>																									
<p>ACCEPTABLE OUTCOMES</p> <p><i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i></p>																											
<p>A3.9.1 – Secure, undercover bicycle parking is provided in accordance with Table 3.9 and accessed via a continuous path of travel from the vehicle or cycle entry point.</p> <p>Table 3.9 Parking ratio</p> <table border="1" data-bbox="232 727 965 1046"> <thead> <tr> <th>Parking types</th> <th></th> <th>Location A</th> <th>Location B</th> </tr> </thead> <tbody> <tr> <td rowspan="3">Car parking¹</td> <td>1 bedroom dwellings</td> <td>0.75 bay per dwelling</td> <td>1 bay per dwelling</td> </tr> <tr> <td>2+ bedroom dwellings</td> <td>1 bay per dwelling</td> <td>1.25 bays per dwelling</td> </tr> <tr> <td>Visitor</td> <td>1 bay per four dwellings up to 12 dwellings 1 bay per eight dwellings for the 13th dwelling and above</td> <td></td> </tr> <tr> <td rowspan="2">Bicycle parking¹</td> <td>Resident</td> <td>0.5 space per dwelling</td> <td></td> </tr> <tr> <td>Visitor</td> <td>1 space per 10 dwellings</td> <td></td> </tr> <tr> <td>Motorcycle/ Scooter parking²</td> <td colspan="3">Developments exceeding 20 dwellings provide 1 motorcycle/scooter space for every 10 car bays</td> </tr> </tbody> </table> <p>¹ Calculations of parking ratios shall be rounded up to the next whole number. ² For each five motorcycle/scooter parking bays provided in accordance with Table 3.9, car parking bays may be reduced by one bay.</p> <p>Definitions: Location A: within 800m walkable catchment of a train station and/or 250m of a transit stop (bus or light rail) of a high-frequency route and/or within the defined boundaries of an activity centre. Location B: not within Location A.</p>		Parking types		Location A	Location B	Car parking ¹	1 bedroom dwellings	0.75 bay per dwelling	1 bay per dwelling	2+ bedroom dwellings	1 bay per dwelling	1.25 bays per dwelling	Visitor	1 bay per four dwellings up to 12 dwellings 1 bay per eight dwellings for the 13th dwelling and above		Bicycle parking ¹	Resident	0.5 space per dwelling		Visitor	1 space per 10 dwellings		Motorcycle/ Scooter parking ²	Developments exceeding 20 dwellings provide 1 motorcycle/scooter space for every 10 car bays			<p>Location A - Resident: 2.5 bays required / 4 bays provided Visitor: 1 bays required / 1 bay provided</p>
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<p>A3.9.2 – Parking is provided for cars and motorcycles in accordance with Table 3.9.</p>		<p>Location A - Resident: 7 bays required / 8 bays provided Visitor: 2 bays required / 2 bays provided</p>																									
<p>A3.9.3 – Maximum parking provision does not exceed double the minimum number of bays specified in Table 3.9</p>		<p>Parking does not exceed double the minimum number of bays.</p>																									
<p>A3.9.4 – Car parking and vehicle circulation areas are designed in accordance with AS2890.1 (as amended) or the requirements of applicable local planning instruments.</p>		<p>The vehicle parking bays satisfy the AS2890.1</p>																									

A3.9.5 – Car parking areas are not located within the street setback and are not visually prominent from the street.	No car parking is proposed in the street setback area or is visible from the street.
A3.9.6 – Car parking is designed, landscaped or screened to mitigate visual impacts when viewed from dwellings and private outdoor spaces.	Car parking spaces are not visible from dwellings or private outdoor spaces.
A3.9.7 – Visitor parking is clearly visible from the driveway, is signed ‘Visitor Parking’ and is accessible from the primary entry or entries.	Visitor parking provided within the central parking area. Visitor parking will be marked and signed. Condition
A3.9.8 – Parking shade structures, where used, integrate with and complement the overall building design and site aesthetics and have a low reflectance to avoid glare into apartments.	N/A.
A3.9.9 – Uncovered at-grade parking is planted with trees at a minimum rate of one tree per four bays.	N/A – not uncovered
A3.9.10 – Basement parking does not protrude more than 1m above ground, and where it protrudes above ground is designed or screened to prevent negative visual impact on the streetscape.	N/A

ELEMENT 4.1 SOLAR AND DAYLIGHT ACCESS		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.1.1 – In climate zones 4, 5 and 6: the development is sited and designed to optimise the number of dwellings receiving winter sunlight to private open space and via windows to habitable rooms.	All dwellings within the development receive a minimum of 2 hours of daylight into habitable rooms on 21 June. Outdoor living areas are also positioned to maximise amenity through daylight access, located on the eastern northern and western facades.	Objective achieved The proposal is designed to optimise winter sunlight access for all dwellings. All dwellings have a living room and private open space which obtains at least 2 hours direct sunlight between 9am and 3pm on 21 June.
O4.1.2 – Windows are designed and positioned to optimise daylight access for habitable rooms.	Windows are located to maximise daylight to habitable rooms, with each habitable room including at least one opening / window.	Objective achieved Windows to habitable rooms are designed and positioned in the development to optimise daylight access. All habitable rooms in each dwelling have windows that provide daylight access, and no rooms rely on light wells as a form of daylight access.
O4.1.3 – The development incorporates shading and glare control to minimise heat gain and glare: <ul style="list-style-type: none"> - from mid-spring to autumn in climate zones 4, 5 and 6 AND - year-round in climate zones 1 and 3. 	Various active and passive design measures have been included to minimise heat gain and glare to improve the overall energy demand and emissions of the building – Refer to ESD Strategy.	Objective achieved Shading devices have been proposed over a majority of openings, particularly those north facing, so as to minimise heat gain and glare.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A4.1.1 – In climate zones 4, 5 and 6 <u>only</u> : <ul style="list-style-type: none"> a) Dwellings with a northern aspect are maximised, with a minimum of 70 per cent of dwellings having living rooms and private open space that obtain at least 2 hours direct sunlight between 9am and 3pm on 21 June AND 	All dwellings have a living room and private open space which obtains at least 2 hours direct sunlight between 9am and 3pm on 21 June.	

<p>b) A maximum of 15 per cent of dwellings in a building receiving no direct sunlight between 9am and 3pm on 21 June.</p>	
<p>A4.1.2 – Every habitable room has at least one window in an external wall, visible from all parts of the room, with a glazed area not less than 10 per cent of the floor area and comprising a minimum of 50 per cent of clear glazing.</p>	<p>Achieved</p>
<p>A4.1.3 – Lightwells and/or skylights do not form the primary source of daylight to any habitable room.</p>	<p>No dwellings rely on lightwells for solar access.</p>
<p>A4.1.4 – The building is oriented and incorporates external shading devices in order to:</p> <ul style="list-style-type: none"> - minimise direct sunlight to habitable rooms: <ul style="list-style-type: none"> ▪ between late September and early March in climate zones 4, 5 and 6 only AND ▪ in all seasons in climate zones 1 and 3 permit winter sun to habitable rooms in accordance with A 4.1.1 (a). 	<p>Minimal north facing major openings. Shaded by upper floors.</p>

ELEMENT 4.2 NATURAL VENTILATION		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.2.1 – Development maximises the number of apartments with natural ventilation.	100% of dwellings in the development are naturally cross ventilated.	Objective achieved The development maximises natural ventilation. The proposal exceeds acceptable outcome A2.2.2 in that 100% of apartments are at least dual aspect.
O4.2.2 – Individual dwellings are designed to optimise natural ventilation of habitable rooms.	As above, all dwellings in the development are designed to be naturally ventilated. High and low openings have also been provided to improve the transition of air through the development.	Objective achieved All habitable rooms have openings which are of an adequate size to facilitate natural ventilation. No habitable rooms rely on light wells as a source for air.
O4.2.3 – Single aspect apartments are designed to maximise and benefit from natural ventilation.	No single aspect apartments are proposed.	n/a – no single aspect apartments proposed.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A4.2.1 – Habitable rooms have openings on at least two walls with a straight-line distance between the centre of the openings of at least 2.1m.		Not all habitable rooms have openings on at least two walls, however where this does occur the distance achieve this outcome.
A4.2.2 – <ul style="list-style-type: none"> (a) A minimum 60 per cent of dwellings are, or are capable of, being naturally cross ventilated in the first nine storeys of the building (b) Single aspect apartments included within the 60 per cent minimum at (a) above must have: <ul style="list-style-type: none"> a. ventilation openings oriented between 45° – 90° of the prevailing cooling wind direction AND b. room depth no greater than 3 x ceiling height (c) For dwellings located at the 10th storey or above, balconies incorporate high and low level ventilation openings. 		No single aspect units proposed. All dwellings are capable of natural cross-ventilation.

A4.2.3 – The depth of cross-over and cross-through apartments with openings at either end and no openings on side walls does not exceed 20m.	No crossover or cross through apartments.
A4.2.4 – No habitable room relies on lightwells as the primary source of fresh-air.	No dwellings rely on lightwells for ventilation.

ELEMENT 4.3 SIZE AND LAYOUT OF DWELLINGS		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.3.1 – The internal size and layout of dwellings is functional with the ability to flexibly accommodate furniture settings and personal goods, appropriate to the expected household size.	All of the proposed dwellings have functional internal floor layouts and exceed the minimum floor areas and dimensions.	Objective achieved All units are of an appropriate size and layout that allows functionality and flexibility appropriate to the expected number of occupants. The dwellings have been designed with open plan living spaces which are conveniently co-located with private open spaces.
O4.3.2 – Ceiling heights and room dimensions provide for well-proportioned spaces that facilitate good natural ventilation and daylight access.	All ceiling heights and room dimensions satisfy the minimum requirements in Table 4.3a and 4.3b.	Objective achieved The proposed ceiling heights and room dimensions achieve acceptable outcomes A 4.3.1, A 4.3.2, and A 4.3.3. All units are well-proportioned to allow for access to sunlight and ventilation in every habitable room.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A4.3.1 – Dwellings have a minimum internal floor area in accordance with Table 4.3a.	G01 3 x 3 = 157m ² (100m ² req) G02 3 x 2 = 105m ² (95m ² req) 101 7 x 7 (+1 toilet) = 448m ² (123m ² req) 201 5 x 5 (+1 toilet) = 409m ² (113m ² req) 301 3 x 3 = 123m ² (100m ² req)	

Table 4.3a Minimum floor areas for dwelling types

Dwelling type	Minimum internal floor area
Studio	37m ²
1 bed	47m ²
2 bed x 1 bath ¹	67m ²
3 bed x 1 bath ¹	90m ²

¹An additional 3m² shall be provided for designs that include a second or separate toilet, and 5m² for designs that include a second bathroom.

A4.3.2 – Habitable rooms have minimum floor areas and dimensions in accordance with Table 4.3b.

Table 4.3b Minimum floor areas and dimensions for habitable rooms

Habitable room type	Minimum internal floor area	Minimum internal dimension
Master bedroom	10m ²	3m
Other bedrooms	9m ²	3m
Living room – studio and 1 bed apartments	N/A	3.6m
Living room – other dwelling types	N/A	4m

¹ Excluding robes

All bedrooms achieve relevant minimum floor area and internal dimension. All living rooms achieve the minimum internal dimension

A4.3.3 – Measured from the finished floor level to finished ceiling level, minimum ceiling heights are:

- Habitable rooms – 2.7m
- Non-habitable rooms – 2.4m
- All other ceilings meet or exceed the requirements of the NCC.

Min 3.2m floor to floor height

A4.3.4 – The length of a single aspect open plan living area is equal to or less than 3 x the ceiling height. An additional 1.8m length may be provided for a kitchen, where the kitchen is the furthest point from the window in an open plan living area provided that the maximum length does not exceed 9m.

No single aspect open plan living areas proposed.

ELEMENT 4.4 PRIVATE OPEN SPACE AND BALCONIES		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.4.1 – Dwellings have good access to appropriately sized private open space that enhances residential amenity.	All dwellings have access to a private open space which satisfies Table 4.4.	Objective achieved All apartments are provided with balconies or a terrace space which exceed the minimum area and dimensions recommended in acceptable outcome A4.4.1. The private open spaces are appropriately sized to enhance residential amenity.
O4.4.2 – Private open space is sited, oriented and designed to enhance liveability for residents.	Private open space areas are orientated to maximise solar access.	Objective achieved Private open space for each dwelling is accessible from the primary living space and habitable rooms of each apartment. The private open space for each dwelling exceeds the minimum area and dimensions recommended in acceptable outcome A4.4.1.
O4.4.3 – Private open space and balconies are integrated into the overall architectural form and detail of the building.	Private open spaces are designed as part of the building and contribute to the overall form and aesthetic of the development.	Objective achieved The private open space for the units comprises balconies or a ground floor terrace areas which are well-sized and appropriately located. All balconies feature screening and detailing which is adequately integrated into the architectural form and detail of the building.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A4.4.1 – Each dwelling has private open space accessed directly from a habitable room with dimensions in accordance with Table 4.4.		U1 – min 2.4m dimension and 22m2 U2 – min 2.4m dimension and 25m2

Table 4.4 Private open space requirements			U3 – min 2.4m dimension and 33m ² U4 – min 2.4m dimension and 42m ² U5 – min 2.4m dimension and 14m ²
Dwelling type	Minimum Area ¹	Minimum Dimension ¹	
Studio apartment + 1 bedroom	8m ²	2.0m	
2 bedroom	10m ²	2.4m	
3 bedroom	12m ²	2.4m	
Ground floor / apartment with a terrace	15m ²	3m	
¹ Services and fixtures located within private open space, including but not limited to air-conditioner units and clothes drying, are not visible from the street and/or are integrated into the building design.			
A4.4.2 – Where private open space requires screening to achieve visual privacy requirements, the entire open space is not screened and any screening is designed such that it does not obscure the outlook from adjacent living rooms.			101/201 feature private open space that is entirely screened. However, the adjacent living rooms have their own openings. The screening does not restrict this outlook.
A4.4.3 – Design detailing, materiality and landscaping of the private open space is integrated with or complements the overall building design.			Landscaping is integrated into balconies and courtyards to provide privacy and soften the external appearance.
A4.4.4 – Services and fixtures located within private open space, including but not limited to air-conditioner units and clothes drying, are not visible from the street and/or are integrated into the building design.			No services are located within private open spaces or visible from the street. All services are integrated into the design of the building.

ELEMENT 4.5 CIRCULATION AND COMMON SPACES		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.5.1 – Circulation spaces have adequate size and capacity to provide safe and convenient access for all residents and visitors.	Extent of corridors throughout the development is limited, given development typology. Corridors / lobby areas have a minimum dimension of 1.2m (entry to unit 301), however, generally 1.65 – 3.0m in width.	Objective achieved Communal corridors are of an adequate size and capacity to provide safe and convenient access for all residents and visitors.
O4.5.2 – Circulation and common spaces are attractive, have good amenity and support opportunities for social interaction between residents.	The lobby areas incorporate ample manoeuvring area, and provide attractive entrances to dwellings.	Objective achieved Circulation and common spaces have some access to natural sunlight and will be artificially lit for safe and legible access. The circulation space can provide opportunities for social interaction between residents where possible.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A4.5.1 – Circulation corridors are a minimum 1.5m in width.		Circulation corridors have a minimum width of 1.5m (amended plans received 31 March 2025)
A4.5.2 – Circulation and common spaces are designed for universal access.		All common spaces can be accessed via lifts.
A4.5.3 – Circulation and common spaces are capable of passive surveillance, include good sightlines and avoid opportunities for concealment.		Circulation and common spaces have limited opportunities for concealment.
A4.5.4 – Circulation and common spaces can be illuminated at night without creating light spill into the habitable rooms of adjacent dwellings.		Internal hallways can be lit at night without impacting units.
A4.5.5 – Bedroom windows and major openings to living rooms do not open directly onto circulation or common spaces and are designed to ensure visual privacy and manage noise intrusion.		No bedroom windows or major openings open onto circulation or common spaces

ELEMENT 4.6		STORAGE	
ELEMENT OBJECTIVES		APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>		<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.6.1 – Well-designed, functional and conveniently located storage is provided for each dwelling.	Each dwelling is provided with access to an internal storage area. All storage areas are compliant with Table 4.6.	Objective achieved Each dwelling has exclusive use of a separate, weatherproof, bulky goods storage area, which achieves the dimensions in accordance with Table 4.6. The storerooms are located adjacent to the individual units. They are adequately integrated into the design of the building and not readily visible from the public domain. Additionally, all dwellings feature sufficient internal storage areas via walk in robes or linen closets.	
ACCEPTABLE OUTCOMES			
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>			
A4.6.1 – Each dwelling has exclusive use of a separate, ventilated, weatherproof, bulky goods storage area. This can be located either internally or externally to the dwelling with dimensions in accordance with Table 4.6.		Each unit has a separate, secure storage space. G02/101/201 5m2 Min 1.5m dimension Minimum 2.1m in height G01/301 4m2 Min 1.5m dimension Minimum 2.1m in height	

Table 4.6 Storage requirements

Dwelling type	Storage area ¹	Minimum dimension ¹	Minimum height ¹
Studio dwelling	3m ²	1.5m	2.1m
1 bedroom dwelling	3m ²		
2 bedroom dwellings	4m ²		
3 bedroom dwellings	5m ²		
¹ Dimensions exclusive of services and plant.			

A4.6.2 – Bulky good stores that are not directly accessible from the dwelling/private open space are located in areas that are convenient, safe, well-lit, secure and subject to passive surveillance.

Storerooms are conveniently located, safe, well-lit, secure and subject to passive surveillance by residents.

A4.6.3 – Storage provided separately from dwellings or within or adjacent to private open space¹, is integrated into the design of the building or open space and is not readily visible from the public domain.

All storerooms are provided separately from dwellings. The storerooms are located adjacent to the unit entrance and are not readily visible from the public domain

(1) Storage on/adjacent to private open space is additional to required open space area and dimensions.

ELEMENT 4.7 MANAGING THE IMPACT OF NOISE		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.7.1 – The siting and layout of development minimises the impact of external noise sources and provides appropriate acoustic privacy to dwellings and on-site open space.	The development has been designed to mitigate impacts of external noise sources having designed communal areas on the roof and commercial uses on the ground floor for vertical separation. Service areas and plant equipment have also been located away from habitable spaces.	Objective achieved subject to condition Given the size of the development and the layout of the apartments, it is considered likely that it can be operated in a manner that meets acoustic regulations. A condition is recommended to require an Acoustic Report which will identify which sources of noise may need to be attenuated.
O4.7.2 – Acoustic treatments are used to reduce sound transfer within and between dwellings and to reduce noise transmission from external noise sources.	Acoustic treatments will be implemented, as appropriate, to reduce sound transfer from external sources.	Objective achieved subject to condition The development has been designed so that noise generating areas are generally separated from residential areas. An acoustic report will be required via condition that demonstrates that the development can achieve compliance with the National Construction Code BCA and the Environmental Protection (Noise) Regulations 1997. Should the development be approved, the recommendations of the provided acoustic report should be incorporated into the development at building permit stage. It is noted that specific mechanical equipment has not been detailed.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A4.7.1 – Dwellings exceed the minimum requirements of the NCC, such as a rating under the AAAC Guideline for Apartment and Townhouse Acoustic Rating (or equivalent).	AO can be achieved via Condition	

	No acoustic report or compliance with NCC demonstrated. This has been placed as a condition of approval should the DAP choose to approve.
A4.7.2 – Potential noise sources such as garage doors, driveways, service areas, plant rooms, building services, mechanical equipment, active communal open space and refuse bins are not located adjacent to the external wall of habitable rooms or within 3m of a window to a bedroom.	Driveways located adjacent to walls of habitable rooms of units 1 and 2 on ground floor.
A4.7.3 – Major openings to habitable rooms are oriented away or shielded from external noise sources.	Major openings do not face onto external noise sources such as communal open space or utilities.

ELEMENT 4.8 DWELLING MIX		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.8.1 – A range of dwelling types, sizes and configurations is provided that caters for diverse household types and changing community demographics.	The development provides a multigenerational development, to be occupied by a single family. The dwelling typologies are reflective of the nature of the development, an objective for ageing in place.	Objective achieved The development proposes three 3 bedroom 3 bathroom units, one 5 bedroom 5 bathroom unit and one 7 bedroom 7 bathroom unit. Each dwelling is intended to provide a similar level of amenity as a stand-alone single house. The units are designed to allow for various types and ‘ageing in place’.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A4.8.1 – a) Dwelling mix is provided in accordance with the objectives, proportions or targets specified in a local housing strategy or relevant local planning instrument OR b) Where there is no local housing strategy, developments of greater than 10 dwellings include at least 20 per cent of apartments of differing bedroom numbers.	There is no adopted housing strategy in place. The development is less than 10 dwellings	

<p>A4.8.2 – Different dwelling types are well distributed throughout the development, including a mix of dwelling types on each floor.</p>	<p>n/a one dwelling per floor except ground floor</p>
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<p>ELEMENT 4.9</p>	<p>UNIVERSAL DESIGN</p>
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<p>ELEMENT OBJECTIVES</p>	<p>APPLICANT COMMENT</p>	<p>ASSESSOR COMMENT</p>
<p><i>Development is to achieve the following Element Objectives</i></p>	<p><i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i></p>	

<p>O4.9.1 – Development includes dwellings with universal design features providing dwelling options for people living with disabilities or limited mobility and/or to facilitate ageing in place.</p>	<p>The development is designed with dwellings that exceed the minimum LHA design requirements to facilitate ageing in place.</p>	<p>Objective achieved subject to Condition The proposal has the ability to achieve at least 20% of apartments meeting Silver Level requirements. This has not been clearly detailed on the plans and thereby a condition of approval is recommended to ensure this is adequately detailed prior to building permit.</p>
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<p>ACCEPTABLE OUTCOMES <i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i></p>
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<p>A4.9.1 –</p> <ul style="list-style-type: none"> a) 20 per cent of all dwellings, across a range of dwelling sizes, meet Silver Level requirements as defined in the Liveable Housing Design Guidelines (Liveable Housing Australia) OR b) 5 per cent of dwellings are designed to Platinum Level as defined in the Liveable Housing Design Guidelines (Liveable Housing Australia). 	<p>Limited information has been provided to demonstrate universal access. Notwithstanding it is acknowledged that the design is capable of providing at least 20% of apartments meeting Silver Level requirements. This has been placed as a condition of approval should DAP choose to approve the development.</p>
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ELEMENT 4.10 FAÇADE DESIGN		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.10.1 – Building façades incorporate proportions, materials and design elements that respect and reference the character of the local area.	Throughout the design review, the building has been adapted to respect and respond to the character of the locality. Building façade treatments take inspiration from development within the broader Nedlands area, which has been comprehensively documented in Design Review presentations. The design of the upper level is recessive to minimise perceived bulk.	Objective achieved The building facades include articulation, varying materiality. The design has been supported by the City’s DRP.
O4.10.2 – Building façades express internal functions and provide visual interest when viewed from the public realm.	Building façade incorporates design elements that provide rhythm and visual interest. Balconies to all levels provide an active frontage to the public realm.	Objective achieved The street elevation building façade provides visual interest to the street. The amended plans as received on 26 March 2025 aim at providing a break in the front facing façade to present more as two independent dwellings to be more consistent with the streetscape.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A4.10.1 – Façade design includes: <ul style="list-style-type: none"> – scaling, articulation, materiality and detailing at lower levels that reflect the scale, character and function of the public realm – rhythm and visual interest achieved by a combination of building articulation, the composition of different elements and changes in texture, material and colour. 	The façade design includes appropriate articulation and materiality. The façade design has been supported by the City’s Design Review Panel. The design is contemporary, references existing building materials	
A4.10.2 – In buildings with height greater than four storeys, façades include a defined base, middle and top for the building.	N/A not greater than 4 storeys	

A4.10.3 – The façade includes design elements that relate to key datum lines of adjacent buildings through upper-level setbacks, parapets, cornices, awnings or colonnade heights.	Supported by DRP
A4.10.4 – Building services fixtures are integrated in the design of the façade and are not visually intrusive from the public realm.	Building services are integrated into the overall design of the building.
A4.10.5 – Development with a primary setback of 1m or less to the street includes awnings that: <ul style="list-style-type: none"> - define and provide weather protection to entries - are integrated into the façade design - are consistent with the streetscape character. 	Development is setback more than 1m from primary street
A4.10.6 – Where provided, signage is integrated into the façade design and is consistent with the desired streetscape character.	N/A Signage not provided

ELEMENT 4.11 ROOF DESIGN		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.11.1 – Roof forms are well integrated into the building design and respond positively to the street.	Roof forms is seamlessly integrated into the design of the development and complements the overall architectural finish and scaling of the development.	Objective achieved The development proposes a flat roof is largely concealed from view behind the façade. The proposed roof form is considered to positively contribute to the building and streetscape.
O4.11.2 – Where possible, roof spaces are utilised to add open space, amenity, solar energy generation or other benefits to the development.	Roof space provides communal open space uses providing extensive high-quality amenity for future residents and opportunities for landscaping.	Objective achieved The roof space features the communal open space for use by residents.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		

A4.11.1 – The roof form or top of building complements the façade design and desired streetscape character.	The flat roof design is consistent with the overall design of the development.
A4.11.2 – Building services located on the roof are not visually obtrusive when viewed from the street.	All services are to be conditioned to be screened from view.
A4.11.3 – Useable roof space is safe for users and minimises overlooking and noise impacts on private open space and habitable rooms within the development and on adjoining sites.	Should DAP choose to approve the development, a condition of approval has been recommended ensuring that an Acoustic Report is provided. The visual privacy is considered to achieve the Element Objectives, see above <i>Visual Privacy</i> .

ELEMENT 4.12 LANDSCAPE DESIGN		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.12.1 – Landscape design enhances streetscape and pedestrian amenity; improves the visual appeal and comfort of open space areas; and provides an attractive outlook for habitable rooms.	Refer to the Landscaping Report attached to the Development Application Report. The development incorporates extensive landscaping, providing 13% of the site as deep soil. The landscape strategy presents a cohesive design throughout the development which enhances amenity and integration of the development within the locality.	Objective achieved There is sufficient landscaping provided within the front setback area to enhance the streetscape of Alexander Road and pedestrian amenity.
O4.12.2 – Plant selection is appropriate to the orientation, exposure and site conditions and is suitable for the adjoining uses.	The planting selection has been based on the specific climactic conditions. Waterwise species are proposed to ensure low water usage, whilst creating an inviting landscape for future residents. Varied tree species have also been selected to create tree canopies increasing shade across the site.	Objective achieved The amended landscaping plan provided includes appropriate plant selection to the site's orientation and site conditions.

<p>O4.12.3 – Landscape design includes water efficient irrigation systems and where appropriate incorporates water harvesting or water re-use technologies.</p>	<p>The irrigation strategy of the proposed landscape is likely to be:</p> <ul style="list-style-type: none"> - Watering only at times of the day to minimise water evaporation loss as per Water Corporation guidelines; - Intensive care and watering during establishment period to ensure healthy plant growth. - Programmed irrigation systems and allowance for adjustment / shut down during prolonged periods of rain. 	<p>Objective achieved via condition</p> <p>The amended landscaping plan will be required to demonstrate waterwise principles.</p>
<p>O4.12.4 – Landscape design is integrated with the design intent of the architecture including its built form, materiality, key functional areas and sustainability strategies.</p>	<p>Landscaping has been focussed throughout the development to enhance high activity areas and areas of communal open space.</p>	<p>Objective achieved</p> <p>Landscape design is integrated with the design of the built form and is complimentary to the design.</p>
<p>ACCEPTABLE OUTCOMES</p> <p><i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i></p>		
<p>A4.12.1 – Submission of a landscape plan prepared by a competent landscape designer. This is to include a species list and irrigation plan demonstrating achievement of Waterwise design principles.</p>	<p>AO achieved subject to condition placement</p> <p>A Landscape Plan has been submitted by See Design Studio (amended 23 March 2025). Species selection has been included. No information on irrigation plans have been provided. A condition of approval has been placed requiring an updated landscape plan should DAP choose to approve the development.</p>	
<p>A4.12.2 – Landscaped areas are located and designed to support mature, shade-providing trees to open space and the public realm, and to improve the outlook and amenity to habitable rooms and open space areas.</p>	<p>Landscaping plan incorporates a relatively high number of trees reflective of the relatively large rear and front deep soil areas complemented by on structure planting. Apartments and terraces are oriented to look towards the front and rear landscaping.</p>	

<p>A4.12.3 – Planting on building structures meets the requirements of Table 4.12.</p> <p>Table 4.12 Planting on structure: minimum soil standards for plant types and sizes</p> <table border="1"> <thead> <tr> <th>Plant type</th> <th>Definition</th> <th>Soil volume</th> <th>Soil depth</th> <th>Soil area</th> </tr> </thead> <tbody> <tr> <td>Large tree</td> <td>Over 12m high, crown spread at maturity</td> <td>76.8m³</td> <td>1,200mm</td> <td>64m² with minimum dimension 7m</td> </tr> <tr> <td>Medium tree</td> <td>8-12m high, crown spread at maturity</td> <td>36m³</td> <td>1,000mm</td> <td>36m² with minimum dimension 5m</td> </tr> <tr> <td>Small tree</td> <td>4-8m high, crown spread at maturity</td> <td>7.2m³</td> <td>800mm</td> <td>3m × 3m</td> </tr> <tr> <td>Small ornamentals</td> <td>3-4m high, crown spread at maturity</td> <td>3.2m³</td> <td>800mm</td> <td>2m × 2m</td> </tr> <tr> <td>Shrubs</td> <td>--</td> <td>--</td> <td>500-600mm</td> <td>--</td> </tr> <tr> <td>Ground cover</td> <td>--</td> <td>--</td> <td>300-450mm</td> <td>--</td> </tr> <tr> <td>Turf</td> <td>--</td> <td>--</td> <td>200mm</td> <td>--</td> </tr> </tbody> </table>	Plant type	Definition	Soil volume	Soil depth	Soil area	Large tree	Over 12m high, crown spread at maturity	76.8m ³	1,200mm	64m ² with minimum dimension 7m	Medium tree	8-12m high, crown spread at maturity	36m ³	1,000mm	36m ² with minimum dimension 5m	Small tree	4-8m high, crown spread at maturity	7.2m ³	800mm	3m × 3m	Small ornamentals	3-4m high, crown spread at maturity	3.2m ³	800mm	2m × 2m	Shrubs	--	--	500-600mm	--	Ground cover	--	--	300-450mm	--	Turf	--	--	200mm	--	<p>The application proposes minimal planting on structure (16m²) and landscaping is mostly proposed within deep soil zones.</p>
Plant type	Definition	Soil volume	Soil depth	Soil area																																					
Large tree	Over 12m high, crown spread at maturity	76.8m ³	1,200mm	64m ² with minimum dimension 7m																																					
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Turf	--	--	200mm	--																																					
<p>A4.12.4 – Building services fixtures are integrated in the design of the landscaping and are not visually intrusive.</p>	<p>Outcome achieved.</p>																																								

ELEMENT 4.15 ENERGY EFFICIENCY		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<p><i>Development is to achieve the following Element Objectives</i></p>	<p><i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i></p>	
<p>O4.15.1 – Reduce energy consumption and greenhouse gas emissions from the development.</p>	<p>Refer to Sustainability Report attached to the Development Application Report.</p> <p>The development has high sustainable design targets, seeking to achieving an 5 Star Green Star rating (to be certified by an accredited assessor).</p>	<p>Objective achieved</p> <p>The Sustainability Report provided includes multiple initiatives that exceed the basic requirements as outlined in LPP 1.3.</p> <p>The overall development achieves an average of 7.5 stars. In addition, the development proposes a 6kW Solar PV system per dwelling, installation of infrastructure to allow</p>

		for future installation of EV charging stations, water efficient water fixtures, and products for internal finishes with low volatile organic compounds (VOC) and formaldehyde ratings. A condition of approval is recommended for the above.
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ACCEPTABLE OUTCOMES
Acceptable Outcome pathway may not be applicable where a performance solution is provided

<p>A4.15.1 –</p> <p>a) Incorporate at least one significant energy efficiency initiative within the development that exceeds minimum practice (refer Design Guidance) OR</p> <p>b) All dwellings exceed the minimum NATHERS requirement for apartments by 0.5 stars.¹</p> <p>(1) Compliance with the NCC requires that development shall achieve an average star-rating across all dwellings that meets or exceeds a nominated benchmark, and that each unit meets or exceeds a slightly lower benchmark. Compliance with this Acceptable Outcome requires that each unit exceeds that lower benchmark by at least half a star.</p>	<p>As above, the development incorporate numerous energy efficiency initiatives within the development that exceeds minimum practice.</p>
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LOCAL PLANNING FRAMEWORK	REQUIREMENT
<i>Does the local planning framework amend or replace the above stated controls? If yes, state the applicable requirement:</i>	LPP 1.3 Sustainable Design

ELEMENT 4.16	WATER MANAGEMENT AND CONSERVATION	
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	

O4.16.1 – Minimise potable water consumption throughout the development.	The development minimises the requirement for water consumption by incorporating high WELS rated fixtures and fittings.	Objective achieved subject to condition A condition of approval is recommended to ensure that the recommendations contained within the EMERGEN report are to be carried out and maintained for the lifetime of the development to the satisfaction of the City.
O4.16.2 – Stormwater runoff from small rainfall events is managed on-site, wherever practical.	The design includes maintains all stormwater runoff from small rainfall events on-site.	Objective achieved Stormwater runoff will be maintained on site.
O4.16.3 – Reduce the risk of flooding so that the likely impacts of major rainfall events will be minimal.		Objective achieved subject to condition Standard condition will be imposed to contain water in case of 1 in 100 year storm event.
ACCEPTABLE OUTCOMES		
<i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i>		
A4.16.1 – Dwellings are individually metered for water usage.		Not indicated – can be conditioned
A4.16.2 – Stormwater runoff generated from small rainfall events is managed on-site.		Adequate stormwater containment to be demonstrated at building permit stage but no issues have been raised with current design
A4.16.3 – Provision of an overland flow path for safe conveyance of runoff from major rainfall events to the local stormwater drainage system.		Adequate stormwater containment to be demonstrated at building permit stage but no issues have been raised with current design

ELEMENT 4.17	WASTE MANAGEMENT	
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	

<p>O4.17.1 – Waste storage facilities minimise negative impacts on the streetscape, building entries and the amenity of residents.</p>	<p>Waste storage is located internally and is not visible from the street.</p>	<p>Objective achieved Waste storage is located internally to the development and screened from view from the streetscape and adjoining properties.</p>
<p>O4.17.2 – Waste to landfill is minimised by providing safe and convenient bins and information for the separation and recycling of waste.</p>	<p>Refer to Waste Management Plan attached to the Development Application Report. The WMP concludes the proposed development provides sufficiently sized Bin Storage Areas for storage of refuse and recyclable.</p>	<p>Objective achieved An amended WMP is required to ensure bin store room achieves sufficient size as per the City’s LPP – Waste Management.</p>
<p>ACCEPTABLE OUTCOMES</p> <p><i>Acceptable Outcome pathway may not be applicable where a performance solution is provided</i></p>		
<p>A4.17.1 – Waste storage facilities are provided in accordance with the Better Practice considerations of the <i>WALGA Multiple Dwelling Waste Management Plan Guidelines</i> (or local government requirements where applicable).</p>	<p>The design of the waste storage area is generally in accordance with the City’s Local Planning Policy – Waste Management. Should DAP choose to approve the development, an conditional of approval will be recommended ensuring that an amended WMP be provided to respond to the City’s comments regarding bin store size and verge placement.</p>	
<p>A4.17.2 – A Level 1 Waste Management Plan (Design Phase) is provided in accordance with the <i>WALGA Multiple Dwelling Waste Management Plan Guidelines - Appendix 4A</i> (or equivalent local government requirements).</p>	<p>Waste Management Plan has been provided. As per above</p>	
<p>A4.17.3 – Sufficient area is provided to accommodate the required number of bins for the separate storage of green waste, recycling and general waste in accordance with the <i>WALGA Multiple Dwelling Waste Management Plan Guidelines - Level 1 Waste Management Plan (Design Phase)</i> (or local government requirements where applicable).</p>	<p>As per above</p>	
<p>A4.17.4 – Communal waste storage is sited and designed to be screened from view from the street, open space and private dwellings.</p>	<p>Communal waste storage is screened from view from the primary street and located away from the dwellings in the undercroft.</p>	
<p>LOCAL PLANNING FRAMEWORK</p>	<p>REQUIREMENT</p>	

Does the local planning framework amend or replace the above stated controls? If yes, state the applicable requirement:	Local Planning Policy – Waste Management See objectives assessment above.
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ELEMENT 4.18 UTILITIES		
ELEMENT OBJECTIVES	APPLICANT COMMENT	ASSESSOR COMMENT
<i>Development is to achieve the following Element Objectives</i>	<i>Outline the rationale demonstrating that the proposal has met the Element Objectives, through either a performance based solution or using the Acceptable Outcomes. The Design Guidance provided in the policy may be of assistance.</i>	
O4.18.1 –The site is serviced with power, water, gas (where available), wastewater, fire services and telecommunications/broadband services that are fit for purpose and meet current performance and access requirements of service providers.	The site is adequately services with all utilities given its location.	Objective achieved All required services are available.
O4.18.2 – All utilities are located such that they are accessible for maintenance and do not restrict safe movement of vehicles or pedestrians.	Utilities located externally are separated from pedestrian areas to reduce conflict with vehicles and pedestrians.	Objective achieved Location of utilities do not impede safe movement of vehicles or pedestrians.
O4.18.3 – Utilities, such as distribution boxes, power and water meters are integrated into design of buildings and landscape so that they are not visually obtrusive from the street or open space within the development.	Services are incorporated into the building design so that they are not visually intrusive.	Objective achieved subject to Condition Not sufficiently noted on plans. It is recommended that this be conditioned to be demonstrated prior to building permit.
O4.18.4 – Utilities within individual dwellings are of a functional size and layout and located to minimise noise or air quality impacts on habitable rooms and balconies.	All apartments are provided with utilities (i.e. laundry) which are appropriately located to provide amenity to residents.	Objective achieved Utilities are functional to the size of units. The air conditioning units have been positioned on the roof and screened to minimise adverse air quality and noise impacts on dwellings.
ACCEPTABLE OUTCOMES		

Acceptable Outcome pathway may not be applicable where a performance solution is provided

A4.18.1 – Utilities that must be located within the front setback, adjacent to the building entry or on visible parts of the roof are integrated into the design of the building, landscape and/or fencing such that they are accessible for servicing requirements but not visually obtrusive.	No utilities indicated within the front setback or adjacent to the building entry. PV system on roof is flush with the roof and not visually obtrusive.
A4.18.2 – Developments are fibre-to-premises ready, including provision for installation of fibre throughout the site and to every dwelling.	Outcome achieved.
A4.18.3 – Hot water units, air-conditioning condenser units and clotheslines are located such that they can be safely maintained, are not visually obtrusive from the street and do not impact on functionality of outdoor living areas or internal storage.	AC condensers are located on roof of floor 3. These are required to be screened. Should DAP choose to approve the development, a condition of approval will be recommended ensuring these be screened from the street and adjoining properties.
A4.18.4 – Laundries are designed and located to be convenient to use, secure, weather-protected and well-vented; and are of an overall size and dimension that is appropriate to the size of the dwelling.	Laundries are located within individual units and are of an overall size and dimension that is appropriate to the size of the dwelling.

City of Nedlands
Received
10 December 2024

Application for Development Approval

Five Multiple Dwellings

Lot 381 (No. 6) Alexander Road
Dalkeith

LATERAL
PLANNING

City of Nedlands
Received
10 December 2024

Document Control

Reference	0369
Location	Perth
Client	Universal United Investments Pty Ltd
Document Title	Application for Development Approval
Document File Name	0369 20241204R dh.docx
Document Date	6 December 2024
Document Version	V1.1
Author	Daniel Hollingworth

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1.0 Introduction

Lateral Planning acts for Universal United Investments Pty Ltd (**Client**), the owner of Lot 381 (No. 6) Alexander Road, Dalkeith (**Site**).

This Town Planning Report (**Report**) has been prepared in support of the enclosed Application for Development Approval (**Application**), which seeks approval from the Metro Inner District Development Assessment Panel (**DAP**) for the development of a 4-storey multiple dwelling development (**Proposed Development**). The Proposed Development incorporates:

- A total of 5 multiple dwellings:
 - 2 dwellings at ground level; and
 - 1 dwelling at each of the three upper storeys.
- 117m² of communal amenities;
- 10 ground level car parking bays (8 resident and 2 visitor); and
- 13% deep soil areas.

The development team which has assisted in preparing the Application is listed below.

DISCIPLINE	COMPANY
Client	Universal United Investments Pty Ltd
Architect	Space Collective
Town Planner	Lateral Planning
Landscape Architect	See Design Studio
Sustainability	Full Circle Design Services
Traffic Engineer	Shawmac
Waste Management	Talis



2.0 Site Description

2.1 Location

The Site is located within the City of Nedlands, generally within the Nedlands / Dalkeith peninsular area. The Site is approximately 1km south-west of the Nedlands Golf Club, 750m north of the David Cruickshank Reserve, and 700m south-east of the John Leckie Pavilion. The general locality comprises large quarter-acre blocks set among wide, tree-lined streets. Allotments comprise large single houses, subdivided townhouses and multiple dwelling development, presenting a diverse residential product reflective of an area in transition.



Figure 1: Contextual Location

The Site is bound by existing residential properties to the north, east and south, and Alexander Road to the west. Nedlands Community Centre is located immediately south-east of the Site, forming a part of the mixed-use development positioned along Waratah Avenue. The street block contains several different built form examples, including single storey dwellings, multiple storey mixed use, 4 storey apartment development, grocery store and retail tenancies, double-storey townhouses and the Nedlands Community Centre.

Pedestrian movement is facilitated by a network of footpaths that extends throughout the locality. Public transport to and from the Site is provided via nearby bus stops 17640 and 17644, located approximately 100m south-east of the Site, providing connectivity to the Claremont bus port and railway station.

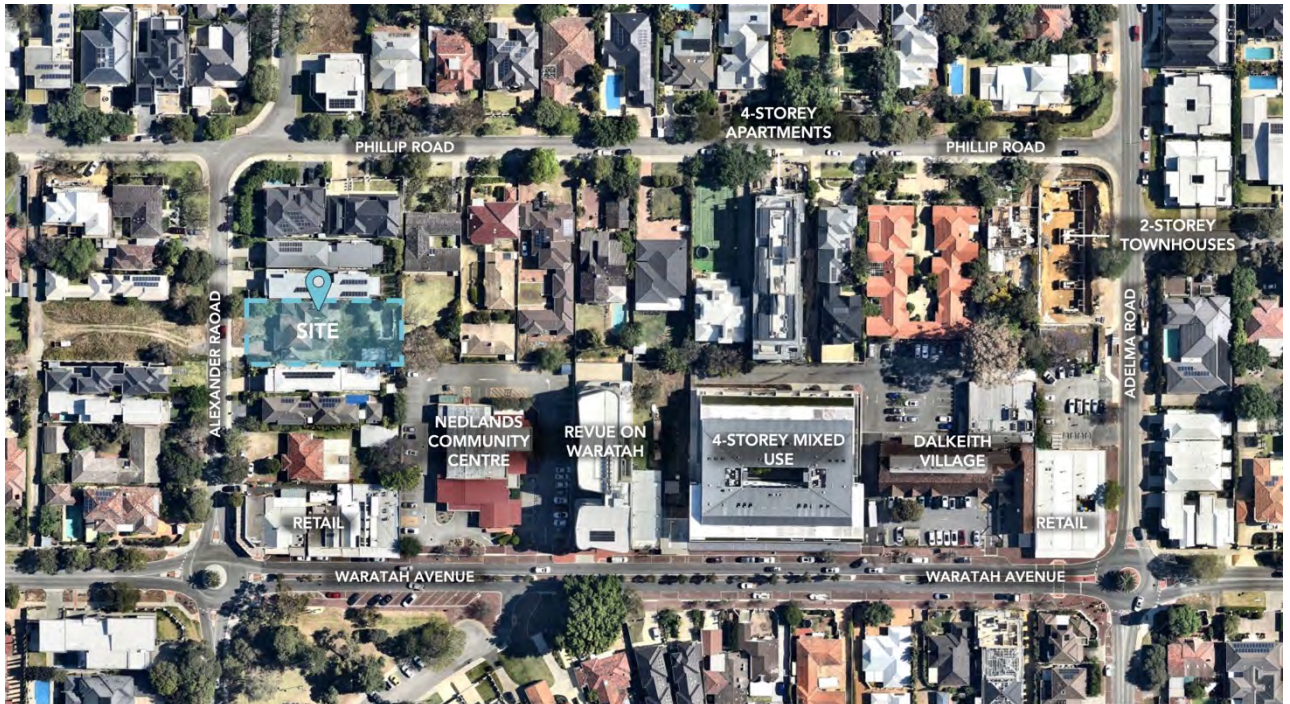


Figure 2: Site Location

2.2 Cadastral Information

The Site comprises a single land parcel, being:

- Lot 381 on Plan 3395, Certificate of Title Volume 1056 and Folio 279.

The Site has a total area of 1,012m², with a frontage of 20.04m to Alexander Road. Northern and southern lot boundaries have a dimension of 50.51m.



Figure 3: Site Plan

3.0 Description of Proposed Development

The Proposed Development comprises a multi-generational, 4-storey Residential development at the Site. The Proposed Development comprises 5 dwellings, as below.

UNIT	LEVEL	AREA (M ²)	BEDROOMS	BATHROOMS	CAR BAYS
G01	Ground floor	154.6	3	3	2
G02	Ground floor	92.2	3	2	1
101	Level 1	464.3	8	8.5	2
201	Level 2	383.0	5	5.5	2
301	Level 3	117.5	3	3	1

The general locality is an area in transition, noting the increase in density of throughout the locality to R40 and R80. Existing development throughout Dalkeith shows little in terms of consistent design outcomes, presenting typical suburban streets with minimal cohesion or shared aesthetic (shown below).



Figure 4: Existing Residential Streetscape Examples

Through an exploration of the wider Nedlands and Dalkeith locality, Space Collective has identified consistent representation of ‘art deco’ themes, manifesting as soft edges, curved focal points, vertical features and strong horizontal lines. Each of these subtle design elements have been incorporated into the Proposed Development to ensure the development sits comfortably within the setting and is representative of the wider locality.



Figure 5: Western Elevation of Proposed Development

Below is an overview of the key components of the Proposed Development.

3.1 Residential Dwelling Overview

The Proposed Development provides 5 generous dwellings, each of which promotes a high level of internal user amenity. Units G01, G02 and 301 are more 'traditional' dwellings, in that they are of a more common typology.

While the development has been purposefully designed for multi-generational living, each dwelling has been designed to operate independently in a view to maximise future adaptability. This is particularly important for dwellings 101 and 201 which occupy a full floorplate and can operate as independent penthouses.

Dwelling G02 is to be provided as a caretaker dwelling, while all remaining dwellings will be occupied by members of the same family. The building caretaker will be responsible for maintaining common landscaped spaces, implementation of the Waste Management Plan and day-to-day maintenance.

Each dwelling is provided with private open space which is accessible directly from primary and secondary living areas. The below table summarises the accessible and landscaped private open space allocated to each dwelling.

	G01	G02	101	201	301
Area (m ²)	114	145	46	76	25

The outdoor living areas do not generate any privacy issues for adjoining properties, however, substantially improve streetscape engagement and surveillance.

3.2 Common Amenities

Communal open space and internal amenities are provided at the rooftop level, occupying approximately two-thirds of the upper level. In total, 117.7m² of communal amenities are provided, being a rate of 23.54m² per dwelling. The communal amenities area includes:

- Yoga room / gymnasium;
- Massage room;
- Sauna and changing facilities;
- Kitchen, dining and living space;
- Outdoor sun deck; and
- Pool.

Figure 6 below is an artist impression of the rear pool area, showing the secluded ambiance of the common space delivered through thoughtful architectural and landscape design. It is worth noting at this stage that common amenities are not required for this development.

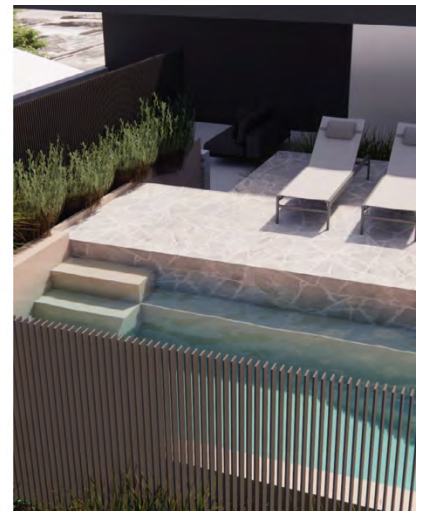


Figure 6: Communal Open Space

4.0 Design Review

As part of early design development, the project team has presented to the City's Design Review Panel on two occasions. Feedback on the proposal has generally be supportive, with guidance given on managing scale through additional built form articulation.

Copied below are the 'strengths' of the proposal, as provided by the Design Review Panel.

- Comprehensive presentation.
- Interesting inter-generational living typology.
- Inclusion of services and utilities location, including fire tanks and pumps.
- The increased setback of the 4th floor from the southern boundary contributes to reduced bulk and decreased overshadowing.
- A curve has been added on the south west corner which softens the front elevation and a recess has been added on the south façade both of which provide a more cohesive aesthetic.
- The entrance experience to the rear ground floor apartment has improved.
- The increased size and extent of the front facing balconies is positive.
- Landscape professional appointed with generous deep soil provision, generous planting in front setback and generally thoughtful landscape design.
- 100% cross ventilation and high levels of north facing solar access.
- Streetscape engagement and passive surveillance have improved through direct streetscape access for the ground level unit, the ground floor alfresco design and increased balcony widths.
- Internal floor plans are generous and functional.
- External materiality is improved and makes some reference to local character.
- ESD professional appointed, a comprehensive ESD report including a Life Cycle Assessment and initiatives which exceed minimum requirements (no gas / all electric, EV charging, 30kW solar panels).
- Natural light to lift lobby.
- Parking concealed from view from public realm in centre of site.
- Generous communal facilities for residents.

The Panel has provided the following assessment of the proposal:

SPP 7.0 PRINCIPLES	DRP1	DRP2
Principle 1 – Context and Character		
Principle 2 – Landscape Quality		
Principle 3 – Built Form and Scale		
Principle 4 – Functionality and Build Quality		
Principle 5 – Sustainability		
Principle 6 – Amenity		
Principle 7 – Legibility		
Principle 8 – Safety		
Principle 9 – Community		
Principle 10 – Aesthetics		

5.0 Town Planning Considerations

5.1 Metropolitan Region Scheme

5.1.1 Zones

The Site is included within the 'Urban' zone pursuant to the Metropolitan Region Scheme (MRS). Land to all side of the Site is similarly included within the Urban zone.

5.1.2 Reserves

No MRS reservations are within or abut the Site.

5.2 State Planning Policy 7.0 – Design of the Built Environment

State Planning Policy 7.0 – Design of the Built Environment (SPP7.0) sets out the objectives, measures, principles and processes which apply to the design and assessment of built form proposal throughout the State.

To assist with design development, the project team met with officers of the City and its Design Review Panel on several occasions prior to lodgement. Our engagement with the City has explored basic built form massing, interaction with surrounding development, streetscape engagement, internal design and amenity, communal areas and street scape design.

We have included below a high-level assessment of the proposal against the 10 design principles of 'good design', demonstrating the high level of architectural and urban design quality.

CONTEXT AND CHARACTER



The proposed development has been designed with keen sensitivity to the local context, particularly emphasising a scale that respects nearby properties. The design integrates into the established fabric, contributing positively to the distinctive character of the locality. The selection of materials and construction details further enhances local distinctiveness. This approach aligns with the future character of the area, delivering appropriate built form density, compatible with promoted population growth and sustainable infrastructure considerations. The focus on site-specific responses ensures a respectful adaptation to the existing landscape, reinforcing the commitment to a development that becomes an integral part of its evolving context.

LANDSCAPE QUALITY



Landscape design response has focused on responding to its verdant surrounds with thoughtful and abundant plantings. The unique character of Dalkeith has informed the visual language of the garden space, with a focus on layered texture and foliage along with welcoming curved forms to create a place appropriate modern take on the Art Deco architecture of Dalkeith. Aligned with good design principles, all spaces are designed to be attractive, comfortable, and vibrant, contributing vitality to the development. The landscaping strategy carefully considers water and soil management, solar access, and habitat creation. The integration of hard and soft landscape elements aims for well-designed, engaging spaces that enhance local identity. The design ensures optimal amenity, functionality, and weather protection.

BUILT FORM AND SCALE



The proposed development is 4-storeys in height and has been carefully designed to integrate harmoniously with the surrounding built environment. The height and massing are appropriate in this location and achieve a seamless transition between existing development and the future urban character of the Alexander Road sub-precinct. The design of the southern elevation has been extensively considered to mitigate negative offsite impacts and maintain a high level of amenity to the adjoining areas of private open space and primary living areas. Horizontal articulation, materiality changes and stepped setbacks reduce visual bulk, enhance the building's relationship with the streetscape, and reinforce a human scale at ground level. These design measures ensure the development respects the local context while contributing positively to the future vision for sustainable residential density.

FUNCTIONALITY AND BUILD QUALITY



Functional spaces are tailored to their intended purpose, fostering positive relationships between different areas and facilitating ease of use. The design prioritises flexibility and adaptability, accommodating all potential future requirements without major modifications. Emphasis on high build quality involves the use of durable materials and finishes, ensuring resilience to wear and tear while minimising maintenance needs. The proposal considers the full life cycle and addresses potential climate change impacts, reflecting a commitment to long-term sustainability. The integration of services is seamlessly executed, ensuring they enhance rather than detract from the appearance, functionality, and serviceability of the final development.

SUSTAINABILITY



As set out within the Sustainability Report, the development is targeting a 7.5 Star NatHERS rating. Passive environmental design measures respond to local climate and site conditions, emphasising optimal orientation, shading, thermal performance, and natural ventilation. The project minimises energy use, resource consumption, and operating costs over its life-cycle.

AMENITY



Each apartment is thoughtfully designed with generous internal living spaces that provide comfort, functionality and adaptability for modern living. Complementing the internal proportions, are outdoor areas which have been designed to encourage outdoor pursuits. The overall layout of the development, including the internal and outdoor spaces have been strategically designed to maximise sunlight penetration, privacy as well as the integration of shading elements to ensure usability and comfort throughout the year.

LEGIBILITY



The architectural layout achieves intuitive wayfinding, with a clear delineation of the public and private realms. Visual cues, interconnected routes and particular design elements foster a sense of orientation, seamlessly integrating with the built form. Pedestrian and vehicle access has been separated to ensure safety of residents and guests, and to provide a simplified entry sequence for visitors. Access to both ground level dwellings has been provided by way of separate, secure entries. Importantly, entry of G02 is taken via the delineated pedestrian way, offering a private and meaningful entry into the courtyard area.

SAFETY



The development maximises opportunities for passive surveillance in public and communal areas, ensuring well-lit and secure access points that are easily maintained and appropriate to the development's purpose. The design establishes a positive and clearly defined relationship between public and private spaces, addressing the need for optimal safety and security within the development and its connection to the adjacent public realm. Vehicular transport routes are carefully designed to integrate safety requirements in a way that mitigates negative impacts on pedestrian amenity.

COMMUNITY



At ground level, the development establishes a welcoming and landscaped interface that enhances the pedestrian experience along Alexander Road, creating a sense of openness and connection to the surrounding streetscape. At upper levels, private and communal open spaces provide opportunities for interaction and relaxation while ensuring passive surveillance and engagement with the street below.

AESTHETICS



Drawing from the existing art deco heritage and design language of Dalkeith, the proposed development seeks to contribute to the design excellence of the community. The dynamic façade provides articulated an articulated and engaging presentation of the built form that is sympathetic to the existing and intended future context.

5.3 City of Nedlands Local Planning Scheme No. 3

5.3.1 Zoning

The Site is included within the 'Residential' zone of the City of Nedlands Local Planning Scheme No. 3 (LPS3). Land to the north, east, south and west is included within the Residential zone; land to the south-east is included within the Mixed-Use zone.

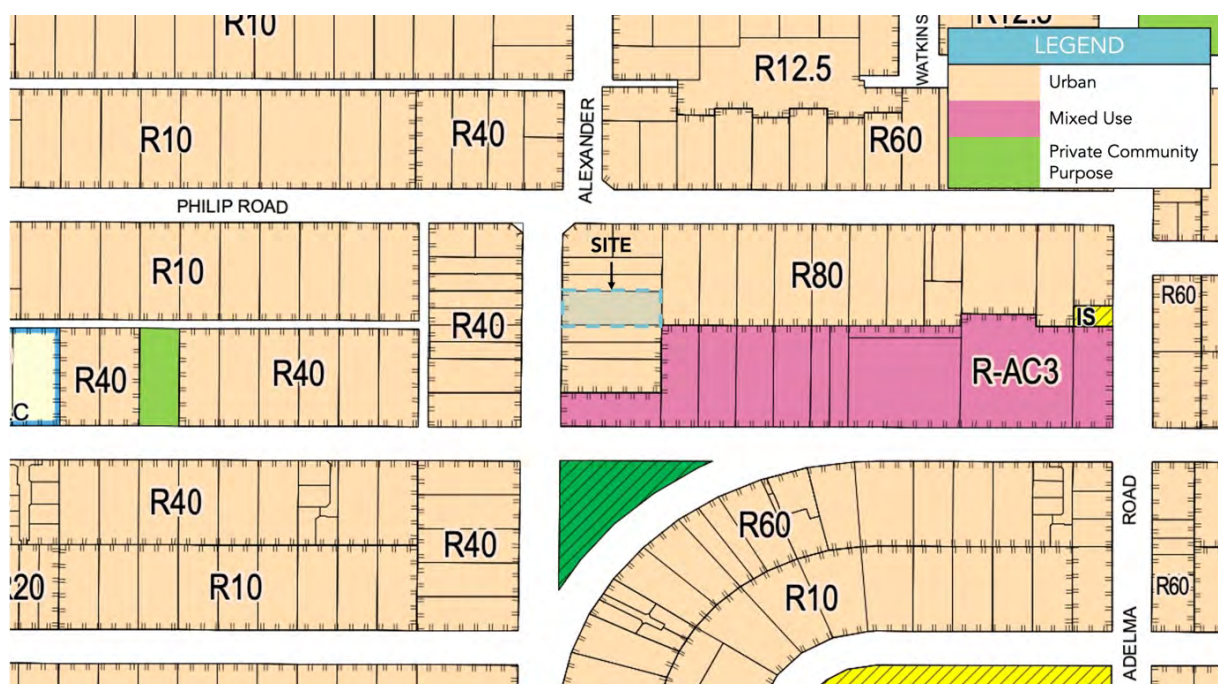


Figure 7: Local Planning Scheme No. 3 Zoning

The Site is allocated a residential density code of R80. Land to the north, east and south is similarly allocated the R80 density; land west is allocated R40; land to the south-east is R-AC0.

Table 2 of LPS3 sets out the Objectives of the Residential zone, as follows:

- *To provide for a range of housing and a choice of residential densities to meet the needs of the community.*
- *To facilitate and encourage high quality design, built form and streetscapes throughout residential areas.*
- *To provide for a range of non-residential uses, which are compatible with and complementary to residential development.*
- *To ensure development maintains compatibility with the desired streetscape in terms of bulk, scale, height, street alignment and setbacks.*

The proposed development supports the zone objectives by offering a unique multigenerational living model that promotes aging in place and family cohesion while ensuring flexibility for future occupancy. The high-quality design incorporates generous landscaping, setback articulation, and carefully selected materials to manage bulk and scale, enhancing the streetscape and integrating with the surrounding area. Communal facilities provide shared amenity, reinforcing the development high standard and community focus of the development. Although solely residential, the design aligns with the intent of the Residential zone by maintaining compatibility with the character of the locality and providing adaptable housing options.

5.3.2 Land Use Permissibility

The Application seeks approval for the 'Residential' land use. Within the Residential zone, the Residential land use is Permitted (P).

The Residential land use is not defined within LPS3, however, is included within the zoning table.

5.4 Residential Design Codes Volume 2

Volume 2 of the Residential Design Codes (**R-Codes**) is applicable to Application. A complete assessment is included as an attachment to this Report. Where an element of the R-Codes is varied by the City's planning framework, those elements are considered separately at section 5.5 of this report.

The need for Element Objectives assessment for aspects of the Proposed Development are extremely limited; the Proposed Development is generally compliant with Acceptable Outcomes. However, comment has been provided for each Element Objective and Acceptable Outcome in the attached R-Codes assessment.

We have considered plot ratio below, noting plot ratio is not amended through the City's local planning framework.

5.4.1 Plot Ratio

Permitted plot ratio for the R80 density is 1.0:1, being 1,012m² of plot ratio floor area. A plot ratio of 1.18:1 is proposed, being 1,201.3m² of plot ratio floor area or a 18% variation to the primary controls.

Given the atypical nature of this multi-generational development, plot ratio is not an accurate representation of the overall or actual bulk of the Proposed Development. This is particularly evidence when considering that a traditional multiple dwelling development would incorporate additional areas which would be excluded from calculation (e.g., storerooms, corridors and lobbies etc.). Simply put, the internal layout of the Proposed Development is more efficient than a comparable development incorporating, for example, 4 dwellings per floor which would require additional storerooms, corridors and lobbies.

The 18% variation represents 189m² of plot ratio floor area, which, in our view, does not have a meaningful impact on overall bulk. Overall building mass is managed through articulation and a careful consideration of siting on the Site. The upper level has been recessed and presented in a recessive colour pallet to reduce overall visual prominence.

The proposed scale of the development is consistent with the future scale of development in the locality, promoted through the Precincts Policy which specifically contemplates development to 4 storeys where 2-hours of sunlight is provided to adjoining sites. This has been achieved.

In our view, as the Proposed Development successfully manages potential off-site impacts within the Site (e.g., overlooking, overshadowing), does not seek to vary setbacks and satisfies all qualitative criteria (e.g., natural ventilation, solar access, room sizes etc), the development presents as an acceptable development response for the Site

5.5 City of Nedlands Local Planning Policies

5.5.1 City of Nedlands Local Planning Policy 5.14 – Precincts

The City of Nedlands Local Planning Policy 5.14 – Precincts (**LPP 5.14**), adopted in August 2024, provides built form requirements to response to the unique context of various precincts and promote design quality of new development in the City of Nedlands.

LPP 5.14 replaces or augments the following Acceptable Outcomes of R-Codes Volume 2:

- Building Height;
- Minimum Primary Street Setback;
- Minimum Secondary Street Setback;
- Rear Setback; and
- Boundary Walls.

Under the provisions of LPP 5.14, the Site is located in the 'Waratah' precinct and the 'Alexander Road' sub-precinct. The following table provides an assessment of the development against the 'Primary Controls' applicable to the Alexander Road sub-precinct, noting the footnotes have also reproduced below:

	Required	Proposed
Building Height³	3 storeys ¹	4 storeys
Min. Primary Street Setback^{2, 4, 5}	Ground & 1st Storey: 5m All other storeys: 6m	Ground & 1 st Storey: 5.5m All other storeys: 5.5m
Min. Secondary Street Setback⁵	2m	N/A
Min. Side Setback	Ground & 1st Storey: 2m All other storeys: 3m	Ground: nil (north); 2m (south) 1 st storey: 2m (north); 2m (south) 2 nd storey: 3m (north); 3m (south) 3 rd storey: 3.7m (north); 5m (south)
Rear setback^{6, 10}	Avg: 6m Min: 3m	6m
Boundary walls^{7, 8, 10}	1 storey 2/3 the length of the lot boundary behind the front setback line 3m behind the primary setback line 1 side boundary only OR 2 side boundaries ⁹	1 storey <2/3 of the lot boundary behind the front setback line >10m behind primary setback line. Up to 1 side boundary only.
<p>1. Additional storey can be considered where it is demonstrated that the proposal allows for 2 hours per day solar access on 21 June for existing solar panels and primary garden areas on neighbouring properties.</p> <p>2. Minimum primary street setback may be reduced by up to 2m, provided the area of any building (including a garage or unenclosed balcony) encroaching into the setback areas is compensated for by at least an equal area of open space (of which 50 per cent is provided as deep soil) that is located between the setback area and a line drawn parallel to it at twice the setback distance.</p> <p>3. On a development site where basement parking is proposed, additional building height of up to 1.5m may be considered by the City, where there is minimal adverse impact on the streetscape and amenity of adjoining properties.</p> <p>4. Provisions of the R-Codes relating to the primary street setback of carports apply. Primary street setbacks referred to in Table B and Table 3.3a of the R-Codes Volume 1 are replaced by the setbacks within this Policy.</p> <p>5. Reduced setbacks may be considered where a significant existing tree is retained on site. An Arboriculturist report is to be provided to demonstrate the building location will not harm the long term viability of the tree.</p> <p>6. For the purpose of assessing average setbacks, lot boundary walls and patios are to be included. Refer to Appendix 1 – Rear Averaging Methodology.</p> <p>7. Where the subject site and an affected adjoining site are subject to different density codes, the length and height of any boundary wall on the boundary between them is determined by reference to the lower density code.</p> <p>8. Walls may be built up to a lot boundary where it abuts an existing or simultaneously constructed wall of equal or greater proportions. For R40, R60 and R80 lots the wall is to be located a minimum 3m behind the primary street setback line.</p> <p>9. Boundary walls may be built up to two side boundaries where a minimum 20 per cent deep soil area is provided OR 15 per cent deep soil area where a significant existing tree is retained on site.</p> <p>10. Boundary setbacks will also be determined by provisions for building separation, deep soil and visual privacy with R-Codes Vol. 1 and Vol. 2, and building separation provisions of the National Construction Codes. (see 5.3 of this Policy for further building separation guidance).</p>		

As demonstrated in the table above, the proposed development is largely compliant with the primary controls, apart from a 0.5m setback primary street setback variation at the 2nd and 3rd storeys. We have considered the merits of proposed variations below.

Building Height

The nominal building height for the Alexander Road sub-precinct is 3 storeys; Note (1) permits consideration of an additional storey where more than 2 hours of solar access is provided to solar panels and primary garden areas on adjoining properties on 21 June.

As the Proposed Development has an overall height of 4 storeys, regard is to be given to the solar access of the southern neighbour. It is worth noting that the R-Codes do not include provisions relating to the extent of shadow cast over an adjoining property for the R80 density. In this regard, the extent to which the planning framework requires consideration of solar access is limited to Note (1) in LPP 5.14 – being 2 hours of solar access on 21 June.

Figure 8 below shows the massing of the Proposed Development which has been designed to provide the following solar access outcomes of the southern adjoining dwelling:

- Existing solar panels: 6 hours of direct sunlight on 21 June.
- Northern outdoor living area: 4 – 6 hours of direct sunlight on 21 June.
- Eastern outdoor living area: minimum of 2 - 6 hours of direct sunlight on 21 June.

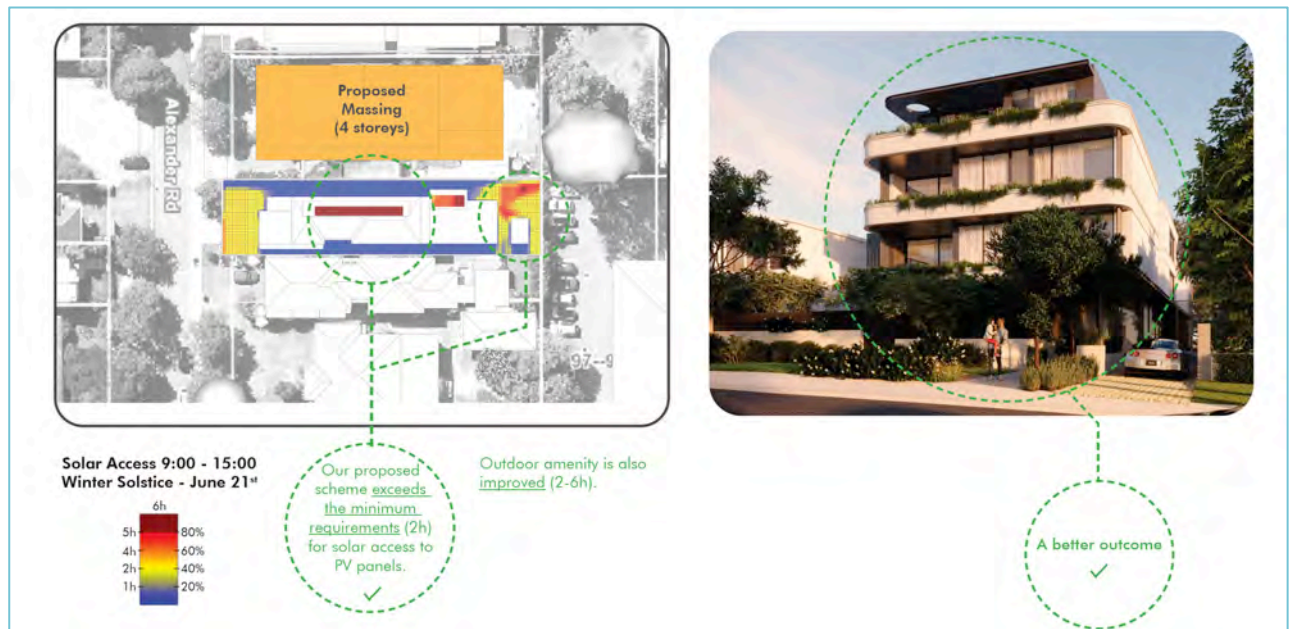


Figure 8: Solar Access Diagram

Having regard to the analysis undertaken by Space Collective, overall massing of the Proposed Development provides more than 2 hours of daylight access. As the fourth storey may be considered, Space Collective has undertaken further detailed analysis of the actual impact of the additional storey. Additional modelling has been provided in the architectural report and reproduced below, demonstrating the beneficial amenity impact to the southern property delivered through site planning.

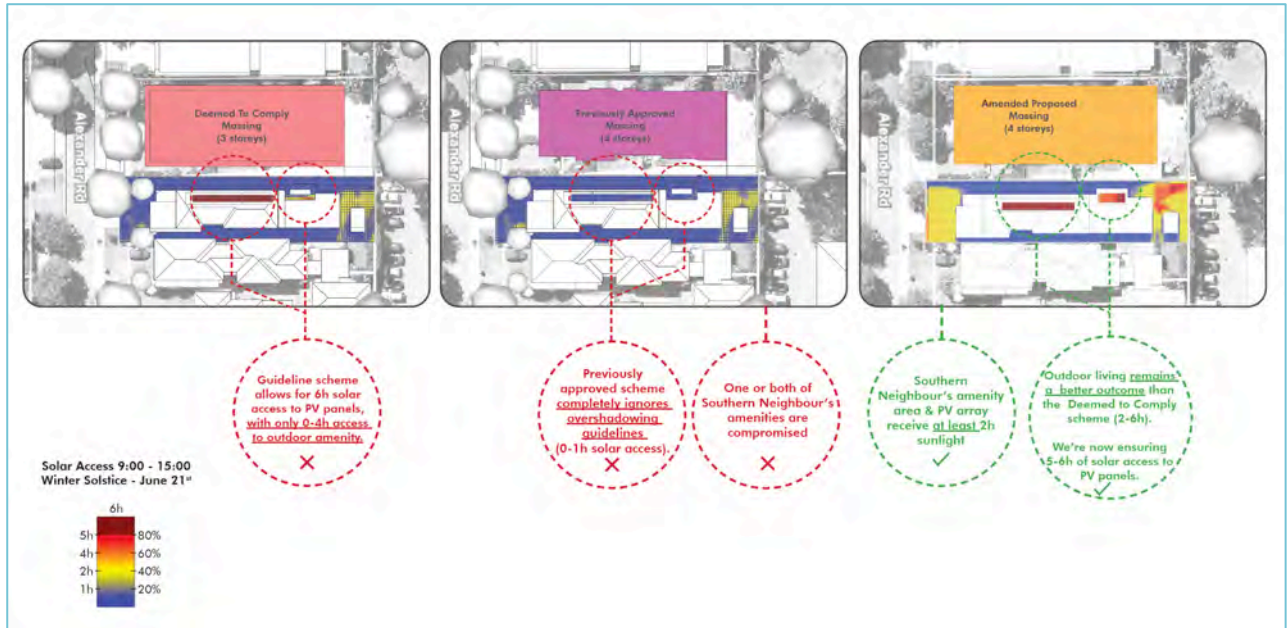


Figure 9: Solar Access Development Scenario Comparison

1. **Scenario 1 (left):** Models a three-storey development with boundary setbacks that comply with the Primary Controls, resulting in very limited sunlight access to the outdoor living area of the southern neighbour.
2. **Scenario 2 (centre):** Models the previously approved four-storey development at the Site. The previous approval provided very limited solar access to the neighbour's solar panels and northern outdoor living area.
3. **Scenario 3 (right):** The current Proposed Development as provided for in the attached Development Plans. As shown, the massing provides 5 – 6 hours of solar access to the solar panels and up to 6 hours of solar access to the northern outdoor living area.

Modelling demonstrates that the site planning and massing of the Proposed Development provides solar access into the southern neighbouring property which exceeds a deemed-to-comply outcome. This is particularly important when recognising that development at the R80 density is generally not required to consider overshadowing or daylight access.

Our consideration of daylight access has not been limited to solar panels; we have also considered the positioning on living areas and outdoor living areas of the southern dwelling. A floorplan for the southern dwelling is included within the architectural package and identifies:

- No bedrooms are provided adjoining the common boundary. All bedrooms within the southern dwelling are oriented south;
- The rear dining / living area is oriented north, however, is set back from the common boundary approximately 4m. The rear dining and living area adjoin a covered outdoor living area which restricts daylight access;

- The Proposed Development incorporates an additional set back from the common boundary to maximise daylight access.

While daylight access to the southern owner can be considered as the first hoop for the consideration of a 4 storey development, it is also necessary to explore bulk, scale and overall suitability of the development response to the locality.

The proposed development demonstrates a carefully considered approach to bulk and scale, ensuring compatibility with the local context while delivering a high-quality design outcome. The southern elevation has been carefully articulated, featuring increased setbacks and strategic cutouts to minimise overshadowing and preserve the amenity of the adjoining southern property. These design measures effectively balance the need for additional density with the preservation of neighbouring amenity.

Compared to a compliant three-storey development or the previously approved design, the current proposal significantly reduces overshadowing impacts. This is achieved through thoughtful massing and an enhanced design approach that integrates an additional storey while maintaining a cohesive and balanced architectural form. The upper level is purposefully recessive, using a subdued colour palette and increased setbacks to visually reduce its prominence and soften its impact on the streetscape.

Further enhancing the design, the materiality of the development has been carefully selected to provide a high-quality streetscape appearance. A mix of durable and visually appealing materials contributes to a contemporary aesthetic while integrating seamlessly with the surrounding neighbourhood art-deco character. The public-facing elevation is animated by balconies that span the full width of the façade, introducing depth and reducing visual bulk while offering active and engaging interfaces with the street.

Overall, the proposed design reflects a highly responsive and refined approach to the opportunities and constraints of the Site. The Proposed Development achieves a sensitive integration of increased density while respecting the established neighbourhood character, ensuring that the bulk and scale of the development are suitable for the locality.

In this regard, we are of the view the height of the Proposed Development maintains amenity of the southern neighbour and presents an overall bulk and scale which is consistent with the streetscape. On this basis, we are of the view the 4-storey building height is capable of support.

Primary Street Setbacks

The Proposed Development seeks a variation to the upper-level (second and third storeys) primary street setback, proposing 5.5m in lieu of 6.0m.

The proposed setbacks for the development represent a well-balanced design response, ensuring a high-quality streetscape while addressing the scale and visual presence of the building

along Alexander Road. The ground and first storeys are set back 5.5m, exceeding the minimum requirement of 5.0m. This additional setback enhances the pedestrian experience by creating a more generous interface with the street, which, combined with increased ground-level landscaping, contributes to a lush, green streetscape. The inclusion of additional tree canopy not only softens the appearance of the development but also mitigates the perception of bulk, adding to the visual and environmental quality of the public realm.

The second and third storeys also adopt a 5.5m setback to their balcony balustrades, with the habitable spaces set back a minimum of 6.9m, exceeding the required upper-level setback. Furthermore, the roofline of the upper level is recessed to a 6.9m setback, strategically reducing its visibility and the overall bulk of the development when viewed from the street. The design incorporates a darker material palette for the upper level, purposefully selected to create a recessive effect and reduce its visual prominence. This, paired with thoughtful articulation, establishes a balanced and layered façade that is sympathetic to the existing streetscape.

The minor reduction in the balcony setbacks for the upper levels does not result in adverse impacts, such as overshadowing or overlooking, due to the generous setbacks of the habitable spaces and the intentional design measures to minimise these effects. By ensuring a consistent 5.5m balcony setback for all levels, the development maintains a cohesive architectural language, avoiding abrupt transitions in form and scale.

Overall, the proposed setbacks and landscaping enhancements deliver a design that respects the site's constraints, improves the streetscape experience, and aligns with the broader objectives of creating a high-quality and visually appealing residential environment.

As such, the variation to the primary street setback for the upper levels is an appropriate response and capable of support.



6.0 Technical Consultant Inputs

6.1 Landscape Architect

See Design Studio has been engaged to develop the landscaping strategy for the Proposed Development which has received consistent support from the Design Review Panel. The landscape design has taken cues from the landscaped Dalkeith locality and art deco architectural language of existing development.

The landscaping plan provides 13% deep soil throughout the ground level. Deep soil areas are provided within the front and rear setback area, allowing implementation and maturation of vegetation. Through the generous spaces allocated to deep soil, the landscaping strategy allows for significant tree canopy which will complement the surrounding streetscape.

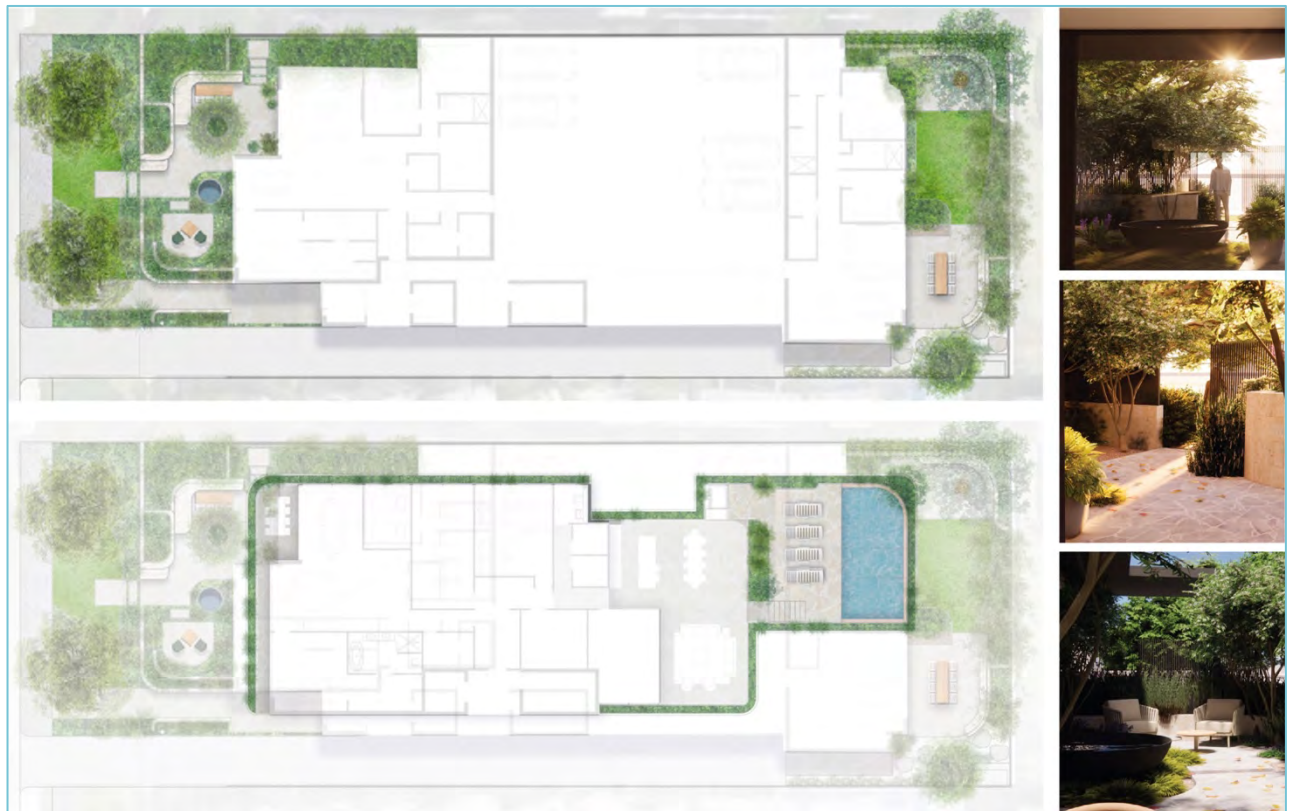


Figure 10: Landscape Concept

6.2 Environmentally Sustainable Design

Full Circle Design Services has prepared a comprehensive Sustainability Strategy in support of the Application. The Sustainability Strategy is attached to this Report. The sustainability strategy has been developed having regard to four key themes: *healthy living, minimised carbon footprint, sustainable transport, and future resilience*.

Key sustainable features include on-site photovoltaics and batteries, a high-performance building envelope, electric vehicle infrastructure, native planting, and a fossil-fuel-free operation. The project exceeds the City of Nedlands LPP 1.3 *Sustainable Design* standards by mitigating the

urban heat island effect, incorporating water-efficient appliances and waterwise planting, facilitating EV use, and establishing a renewable energy embedded network.

These measures collectively provide significant environmental benefits, future-proof the development, and enhance the quality of life for residents. The table below provides a summary of the proposed sustainability features of the development.

Design Feature	Benchmark Performance	Verification Method	Healthy Living	Building Footprint	Climate Resilience	Sustainable Transport
EV Facilitation	Dedicated EV DB, Capacity to service 5 bays.	Design documents		X		X
Brown Field Development	100% Site previously developed, improved ecology score	Design documents	X	X	X	X
Efficient Envelope	Improvement over BCA 2022	Building License Report	X	X	X	
Acoustic Performance	Design report and performance verification at completion.	Report	X			
Native Planting	Demonstrate 30% reduction in water use against Green Star Standards	Report at Building License		X	X	
Fossil Fuel Free Site	No fossil fuels for domestic hot water, cooking or heating	Design documents		X	X	
Envelope Pressure Test	Test 10% of building apartment envelope	Report at practical completion.	X	X	X	
Renewable Energy	Potential for Solar PV Panels and onsite storage	Performance Specification		X	X	
Urban Heat Island	Achieve 75% of site with a 3-year SRI of 64 or greater	Design documents	X	X	X	
Waste Plan	Waste store to allow for three streams	Design documents		X		
Embodied Energy	Target 20% reduction in life cycle footprint, as demonstrated with comparison to similar buildings	Memo at Building Licence		X	X	
Water Efficiency	WELS rated fittings – Within 1 Star of best available	Design documents		X	X	
Low Toxicity Materials	Utilise low VOC and formaldehyde internal finishes, adhesives and sealants.	Material review at Building License	X			
Low Carbon Materials	Utilise R32 Refrigerant and improved materials to improve upfront carbon by 20%	LCA Report at Building License		X	X	
Natural Ventilation	Comply with AS 1668.4 for natural ventilation to all apartments	Report at Building License	X	X	X	
Operational Footprint	20% reduction in operational energy through envelope and services. Facilitate NABERS certification in future.	Report at Building License		X	X	

Figure 11: Summary of Sustainability Strategy

6.3 Traffic Engineer

A Traffic Engineering Technical Note has been prepared by Shawmac, which focuses primarily on the capacity of the road network, vehicle trip generation and functionality of the parking layout. A Technical Note is the appropriate level of advice noting the development will generate a low traffic volume. In this context, the Technical Note concludes as follows:

- The existing road network will have sufficient capacity to accommodate the traffic generated by the development and no modifications are required.
- The proposed crossover achieves the required sight distance to vehicles in all directions.

- The required truncations at the driveway are mostly achieved but are slightly reduced by the boundary fence to the south. The minor encroachment is not considered a major issue for various reasons, including:
 - o The pedestrian path on the development side of Alexander Road is adjacent to the kerb and not along the property boundary. Vehicles exiting the site will sit mostly within the verge and will have sufficient visibility to pedestrians before crossing the footpath.
 - o Based on the building entry point and obstructions within the verge area (trees, signs, light poles), it is highly unlikely that pedestrian will walk in the area of the visual truncation.
- The provision of 8 residential parking bays and 2 visitor parking bays satisfies the minimum parking requirements of the Residential Design Codes Volume 2.
- The development proposes 4 resident and 1 visitor parking space for bicycles on the ground floor. The bicycle parking provision meets the minimum requirements.
- A review of the parking concludes that the layout complies with Australian Standards.
- The crash history does not indicate any issues with the road network. The traffic generated by the site will be low and is unlikely to increase the risk of crashes to unacceptable levels.
- The existing external path network is considered to be adequate.
- The existing public transport services are considered to be adequate.

While the potential impact of the development is low, the Technical Note has summarised the comprehensive assessment which has been undertaken to demonstrate the suitability of the Proposed Development

6.4 Waste Management Plan

A Waste Management Report has been prepared for the development, addressing the generation, storage and collection of waste from the Proposed Development. The Waste Management Report deals with general refuse, recyclables and FOGO, demonstrating that the design:

- adequately caters for the anticipated volume of waste to be generated;
- provides an adequately sized Bin Storage Area, including appropriate bins; and
- allows for efficient collection of bins by appropriate waste collection vehicles.

Anticipated weekly waste generation rates at included at section 2.3 of the Waste Management Plan, summarised below.

APARTMENT	REFUSE GENERATED (L)	RECYCLABLES GENERATED (L)	FOGO GENERATED (L)
Three Bedroom (x3)	360	360	120
Five Bedroom (x1)	120	120	40
Seven Bedroom (x1)	120	120	40
TOTAL	600	600	200

The anticipated weekly waste generation can be accommodated within the bin store provided at ground level. The Waste Management Plan concludes as follows:

As demonstrated within this WMP, the Proposal provides a sufficiently sized Bin Storage Area for storage of refuse, recyclables and FOGO, based on the estimated waste generation volumes and suitable configuration of bins. This indicates that an adequately designed Bin Storage Area has been provided, and collection of refuse, recyclables and FOGO can be completed from the Proposal.

The above is achieved using:

- *Five 240L refuse bins, collected once each fortnight;*
- *Five 240L recycling bins, collected once each fortnight; and*
- *One 240L FOGO bin, collected once each week.*

The City will collect refuse, recyclables and FOGO from the Proposal utilising its kerbside collection service. The City's waste collection vehicle will service the bins from the Bin Presentation Area on Alexander Road.

A strata manager/caretaker will oversee the relevant aspects of waste management at the Proposal.

ALEXANDER ROAD

DALKEITH

Landscape Architecture

Development Application Report

27.03.2025



See Design Studio
Landscape Architects

Acknowledgment of Country



See Design Studio operates in Walyalup (Fremantle) on Whadjuk country of the Noongar nation.

We pay our respects to their Elders past and present and recognise their ongoing care for and connection to the land that we are working on.



Contents








The Site	4
Concept Diagrams	6
Selections	16
Deep Soil + Trees	25

Site Context

- Legend**
- The Site
 - Bus route
 - Shared use path
 - Eateries and small businesses
 - Playground
 - Indian Ocean
 - Sports Facilities
 - Public parks and reserves



Site Analysis

- Legend**
-  The Site
 -  Bus route
 -  Bus stop
 -  Food and beverage
 -  Public open space
 -  Eateries and small businesses
 -  Existing street tree





CONCEPT DIAGRAMS

Landscape Character



Green Suburb

Dalkeith is a leafy suburb with many mature trees and established gardens. The planting design for Alexander Road will respond to its verdant surrounds with thoughtful and abundant plantings. Species will be a mix of native water wise species and iconic sculptural plants to create a unique and distinctive planting palette



Unique Character of Dalkeith

The Art Deco architecture of Dalkeith will inform the visual language of the garden space at Alexander Road. A focus on layered texture, materials and foliage along with welcoming curved forms will create a place appropriate modern take on this heritage style.

Ground floor design imagery



layering



sculptural planting



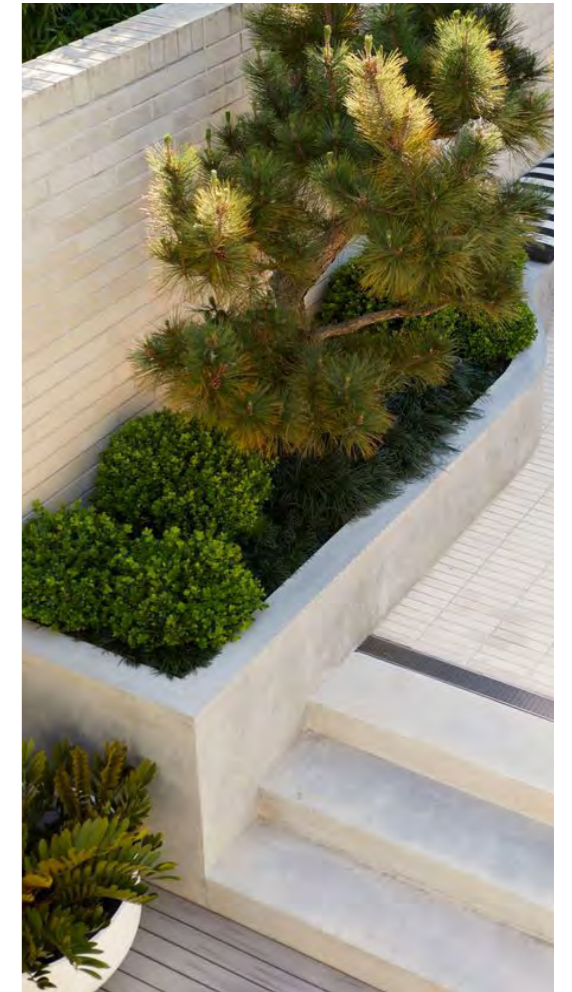
curved forms

Podium design imagery

contemporary fencing



trailing plants




built in planters

Ground Floor Landscape Plan



Legend

- (01) Main Pedestrian Access
- (02) Driveway + Ground Cover Planting
- (03) Verve Garden
- (04) Private Courtyard
- (05) Private Courtyard - Garden Access
- (06) Privacy Wall / Fence
- (07) Relaxation Zone
- (08) Outdoor Dining
- (09) Outdoor Shower
- (10) Water Feature
- (11) Eureka Lemon Tree

1:200 @ A3 

1st Floor Landscape Plan



Legend

- 01 Pots to divide Juliette balcony
- 02 Courtyard/Terrace
- 03 Living Area Balcony
- 04 Perimeter Planting
- 05 Screening (to match front fence)

1:200 @ A3



2nd Floor Landscape Plan



Legend

- 01 Living Area Balcony
- 02 Terrace
- 03 Perimeter Planting

1:200 @ A3



3rd Floor Landscape Plan



Legend

- 01 Pool Zone
- 02 Sun Deck
- 03 Terrace
- 04 Perimeter Planting
- 05 Private Screening
- 06 Meyer Lemon Tree

1:200 @ A3



Landscape Visualisations



View 1 - Street View



View 2 - Main Pedestrian Access



Species Selection Indicative species utilised in visualisations. Refer to selections for proposed materials and planting species

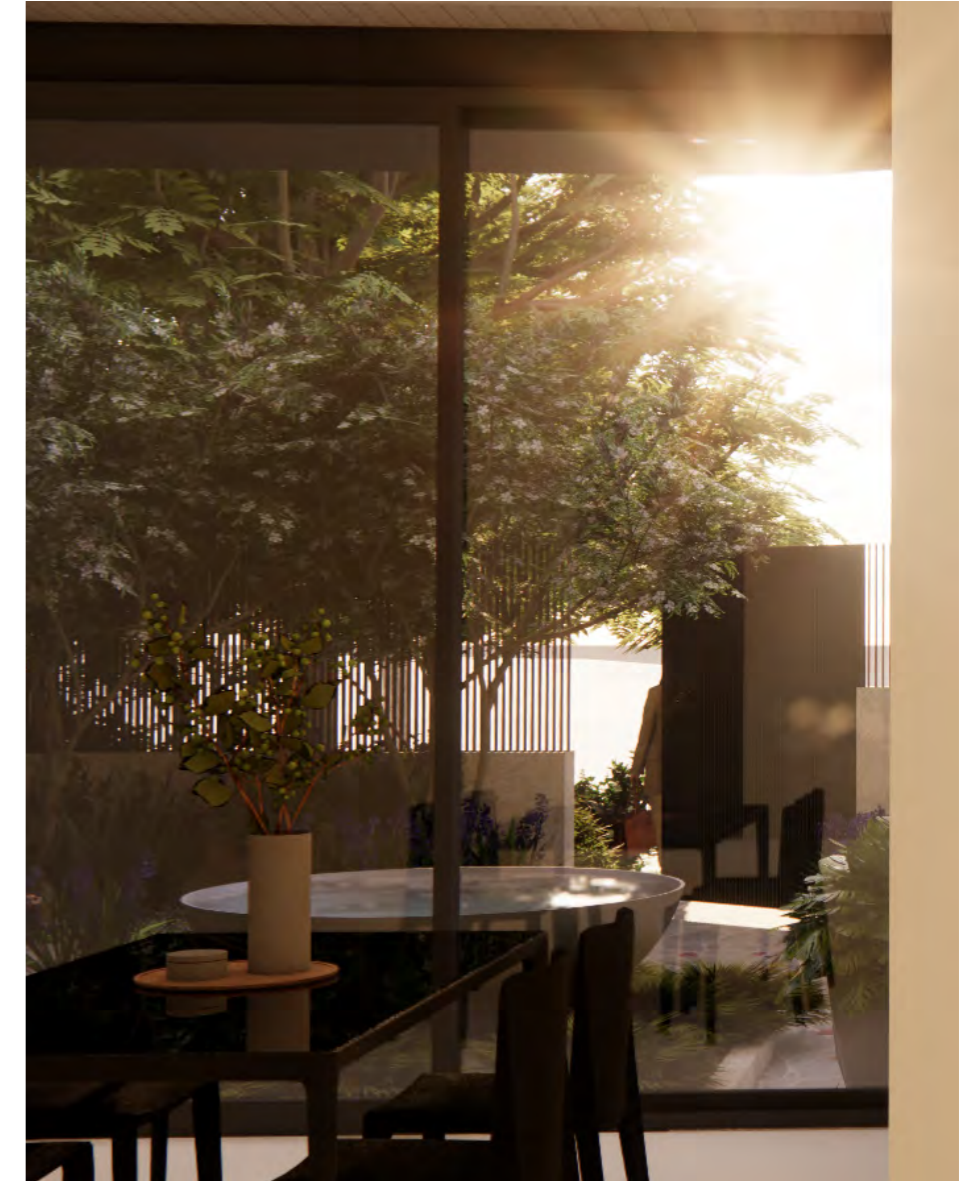
Landscape Visualisations



View 3 - View toward private garden gate



View 4 - View toward relaxation zone



View 4 - View from kitchen / dining, looking outside



Species Selection Indicative species utilised in visualisations. Refer to selections for proposed materials and planting species



SELECTIONS

Material Palette

Surface Treatments



Material: Accoya timber deck
Colour: Natural
Finish: Natural



Material: Scala - Eco outdoor crazy pave
Colour: Natural
Finish: Natural
Unit size: Varies



Material: Summerstone fines stabilised gravel
Colour: Natural



Material: Exposed aggregate concrete
Colour: Corsica



Stone Cobble
Supplier: Eco Outdoor
Colour: Garonne

Material Palette

Wall Treatments



Material: Natural Limestone
Colour: Natural
Unit size: TBC



Material: Concrete
Colour: Grey
Finish: Smooth



Material: Steel blade fencing
Colour: TBD



Material: Rendered wall
Colour: TBD

Ground Floor Planting Palette

Low plants



Viola hederacea



Ajuga 'Chocolate Chip'



Hibbertia glossularifolia



Thyme 'Creeping'



Dymondia 'Silver Carpet'



Dichondra repens



Banksia petiolaris



Eremophila 'Kalbarri Carpet'



Grevillea 'Mello Yellow'



Myoporum insulare prostrate



Senecio 'Chalky Sticks'

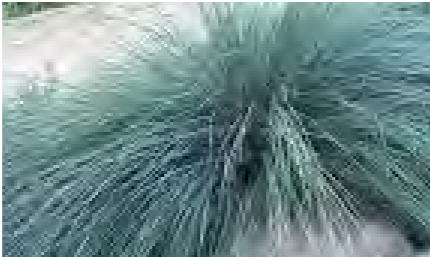


Rosemary prostrate

Strappy Plants



Lomandra 'Evergreen Baby'



Lomandra 'Seascape'



Lomandra 'Tanika'



Liriope 'Emerald Casade'



Poa 'Ellendale'



Orthrosanthus multiflorus



Dietes robinsoniana



Phormium tenax

Northern aspect succulents and shrubs



Aloe 'Fairy Pink'



Euphorbia tirucalli



Senecio vitalis



Agave 'Blue Glow'



Aloe plicatilis



Crassula 'Bluebird'



Kalanchoe silver spoons



Acacia cognata



Raphiolepis 'Oriental Pearl'



Westringia 'Aussie Box'



Leucophyta brownii



Alyxia buxifolia

Southern aspect climbers and shrubs



Ficus pumila



Cissus 'Ellen Danica'



Plectranthus argenteus



Philodendron 'Xanadu'



Asparagus 'meyersii'



Arthropodium cirratum



Microsorium 'Kangaroo Fern'



Dianella 'Tasred'



Pittosporum 'Miss Muffet'



Acanthus mollis



Philodendron 'Lickety Split'



Alpinia caerulea

Podium Planting Palette



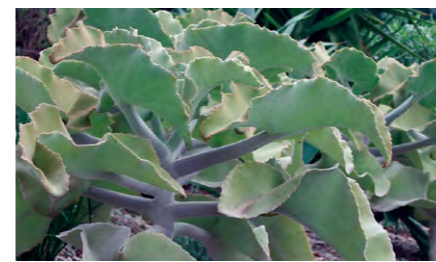
Correa nummulariifolia



Cotyledon macrantha



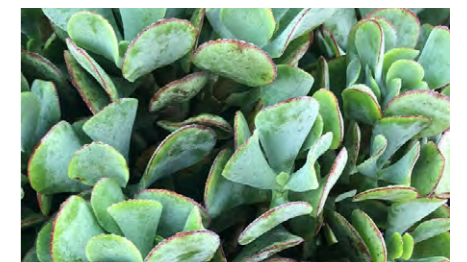
Carissa 'Desert Star'



Kalanchoe baharensis



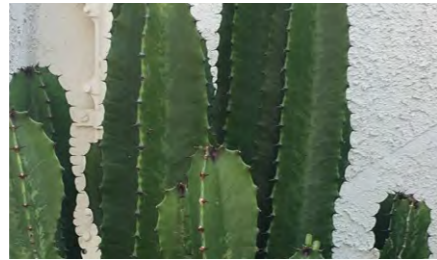
Euphorbia tirucalli



Crassula 'Bluebird'



Acacia 'Winter Flame'



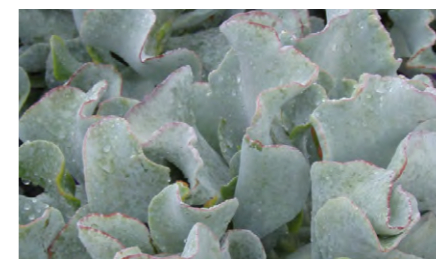
Euphorbia ingens



Leucophyta 'Silver Nugget'



Crassula 'Hobbit'



Cotyledon silver waves

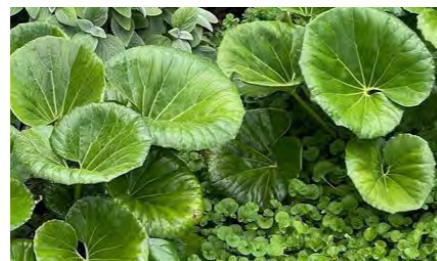


Aloe 'Outback Orange'

Potted plants



Asplenium sp.



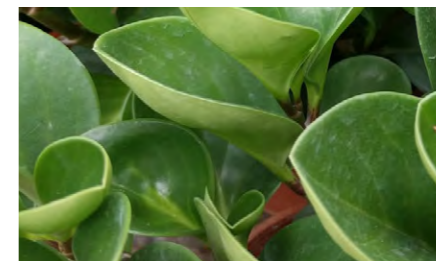
Ligularia reniformis



Ficus elastica 'Burgundy'



Ficus 'Sabre'



Peperomia obtusifolia



Citrus 'Lemon'



Citrus 'Lime'



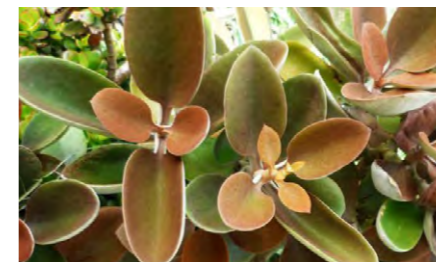
Laurus 'Miles Choice'



Agave attenuata



Rhipsalis baccifera



Kalanchoe 'Copper Spoons'



Aloe barbaeae

Verge Planting Palette



Chrysocephalum apiculatum



Grevillea 'Gingin Gem'



Hemiandra pungens



Viola hederacea



Myoporum 'Yareena'



Eremophila 'Amber Carpet'



Poa poiformis



Leucophyta brownii



Westringia 'Mundi'



Ficinia nodosa



Correa nummulariifolia

Cascading and climbing plants



Trachelospermum jasminoides



Ficus pumila



Parthenocissus quinquefolia



Acacia saligna prostrate



Carpobrotus virescens



Dichondra Silver Falls



Rosemary prostrate



Austrostipa stipiodes



Poa 'Eskdale'



Lomandra 'Shara'



Liriope 'Emerald Cascade'



'Lomandra Tanika'

Tree Species



Lagerstroemia 'Natchez'
6h x 4w



Agonis 'Lemon Lime'
4h x 4w



Waterhousia 'Sweeper'
8h x 4w



Citrus 'Meyer Lemon'
3h x 3w



Phoenix roebelenii
3h x 2w



Tristaniopsis 'Luscious'
8h x 6w



Jacaranda
8h x 8w




Arbutus unedo (existing)
4h x 4w

Planting Narrative Plan



Legend

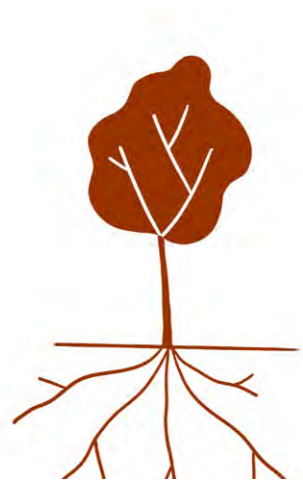
- Native and Endemic species
- Native and mediterranean species
- Shade tolerant species

1:200 @ A3 



DEEP SOIL + TREES

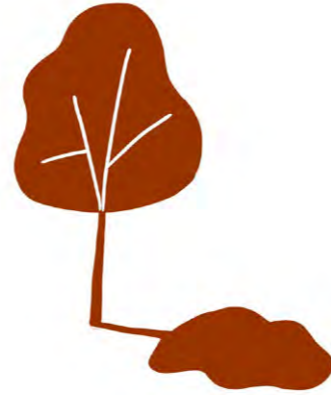
Deep Soil Zone Benefits



Healthy roots = healthy tree



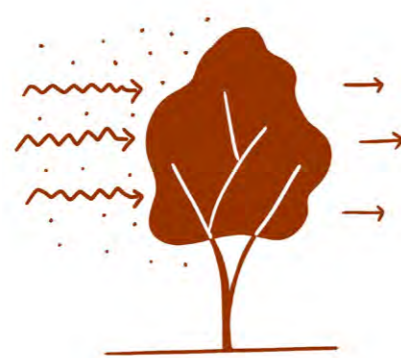
Enhance water filtration



Comfortable Micro-Climate + Reduction of Urban Heat Island Effect



Create Habitat



Improve Air Quality



Community Health through increased canopy coverage



Trees and gardens make a significant contribution to the ecology, character and amenity of neighborhoods. They provide habitat for fauna, shade, storm water management and micro-climate benefits, as well as improve apartment outlook and privacy.

The provision of deep soil areas to support and sustain the development of tree canopy can also make a major contribution to the retention of existing trees. A deep soil area is an area of soil that is free of built structure and has sufficient area and depth to support tree growth and infiltrate rainwater. Site planning should seek to co-locate deep soil areas with existing trees on and adjacent to the site, and in locations best suited to the development of a viable tree canopy and landscaping.

Deep Soil Calculation

Summary

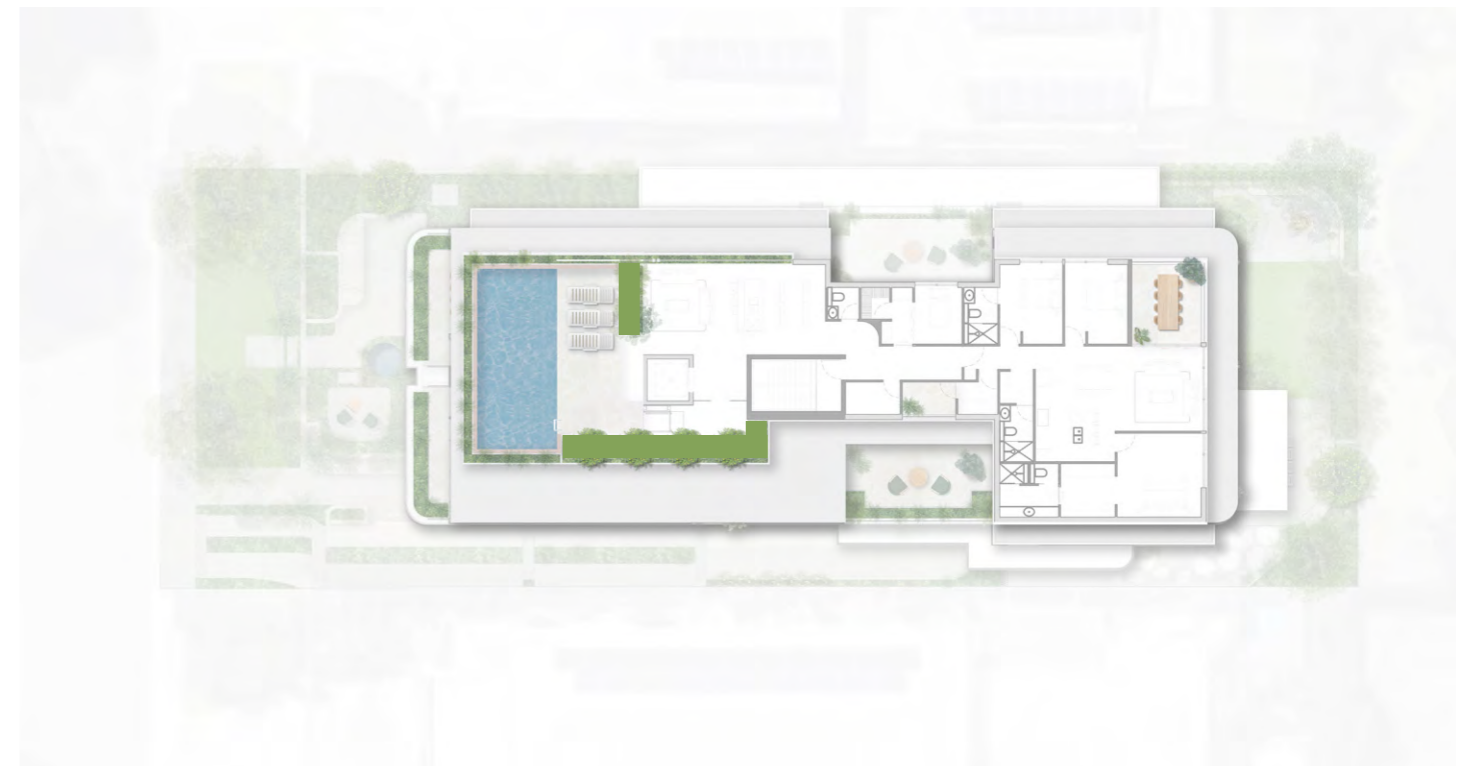
Deep Soil Area (DSA) Total Summary	
Total Site Area	1,012 sqm
Deep Soil Planting Required	101 sqm (10% of site area)
Achieved Deep Soil Area (DSA)	
Ground Floor DSA	135.4 sqm (13.4% of site area)
Ground Floor DSA Shortfall	0 sqm
Podium planting requirement (min 1m wide x 1m deep)	0 sqm
Additional Planting	
Planting on Structure (min 1m wide x 1m deep)	16.8 sqm (1.7% of site area)
TOTAL DSA AND PLANTING ON STRUCTURE	
Total Achieved DSA	152.2 sqm
Total DSA percentage	15%

- Deep Soil Planting
- Planting on Structure

Ground Floor



3rd Floor



Tree Requirements

Tree Requirements	Area
Site Area	1,012 sqm
Site wide tree requirements (>1,000m2)	2 medium & small trees to suit area
TOTAL TREE NUMBERS	Large trees = 0 total
	Medium trees = 2 total
	Small trees = 4 total

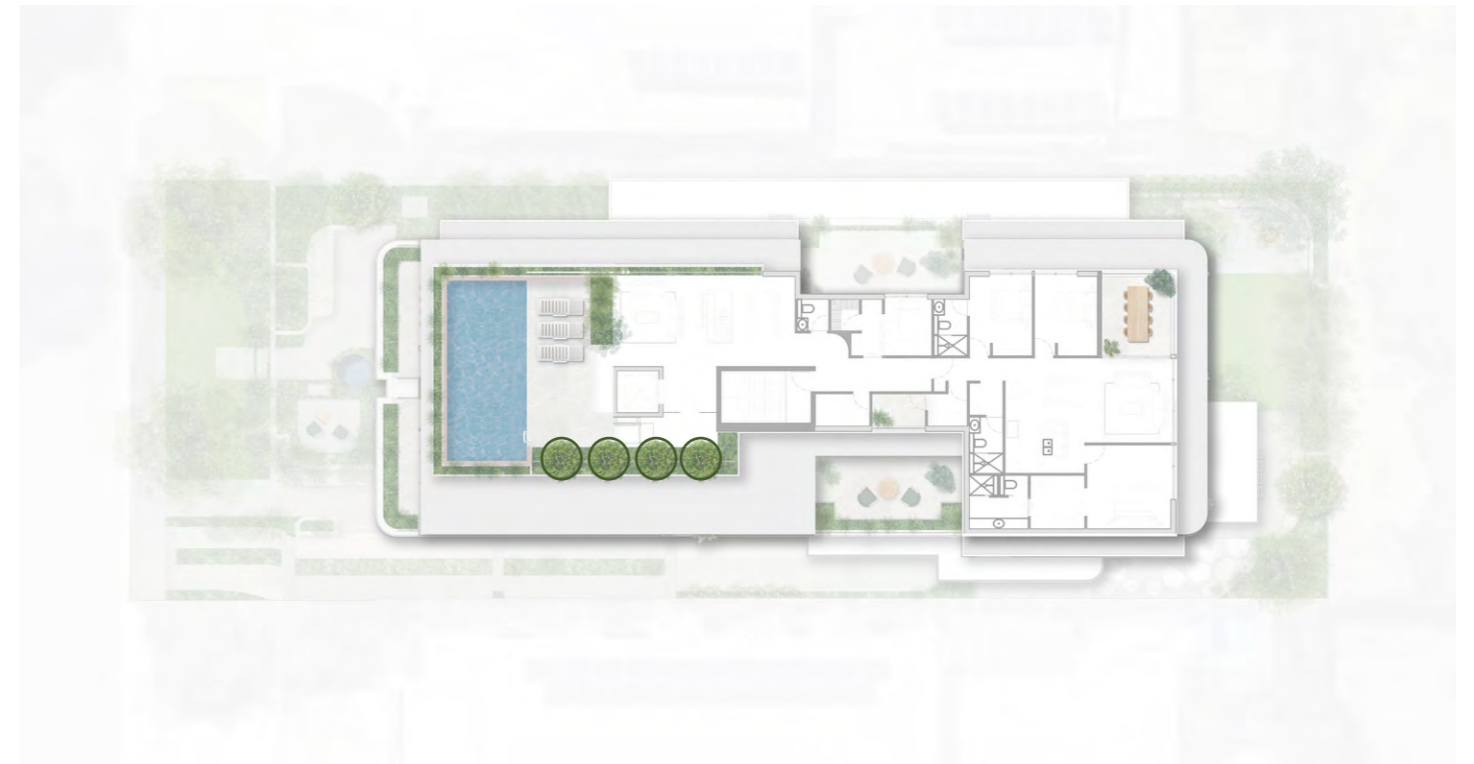
Legend

- Retained tree
- Small tree (9m2 DSA per tree)
- Medium tree (36m2 DSA per tree)
- Small tree/large shrub (not counted for DSA)

Ground Floor



3rd Floor



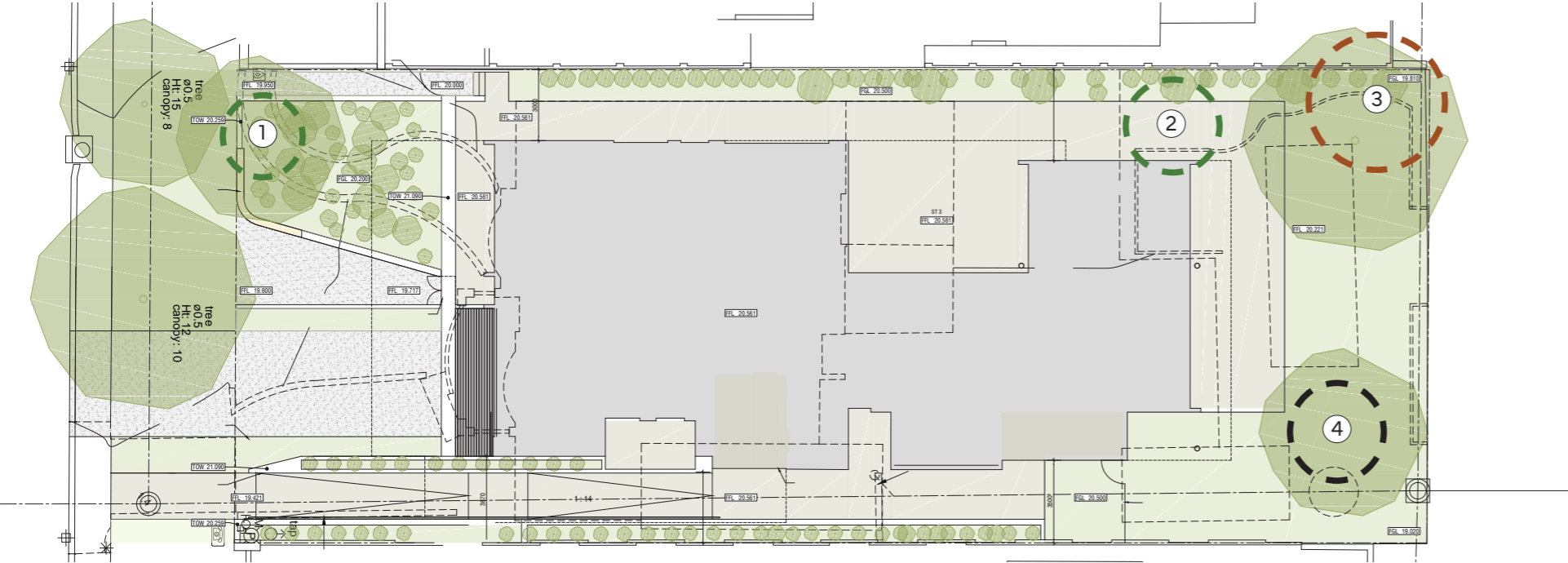
Tree Demolition Plan

Legend

-  Existing Tree to be removed
-  Tree to be retained
-  Tree - does not exist

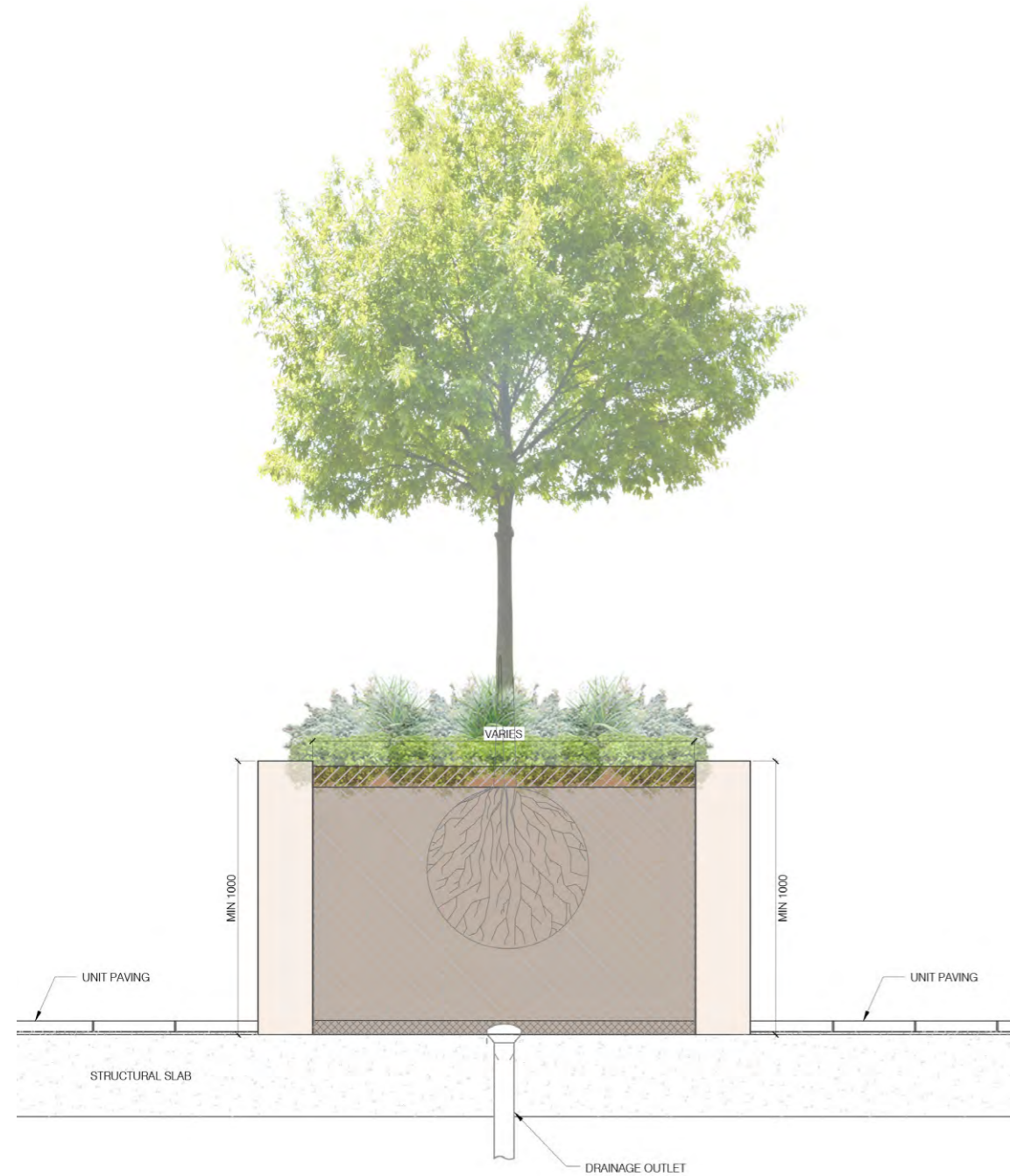
Existing Tree Schedule

1. *Shinus terebithifolia* - markedly smaller than shown on survey
2. Unconfirmed Tree Species
3. *Arbutus unedo*
4. Tree - does not exist

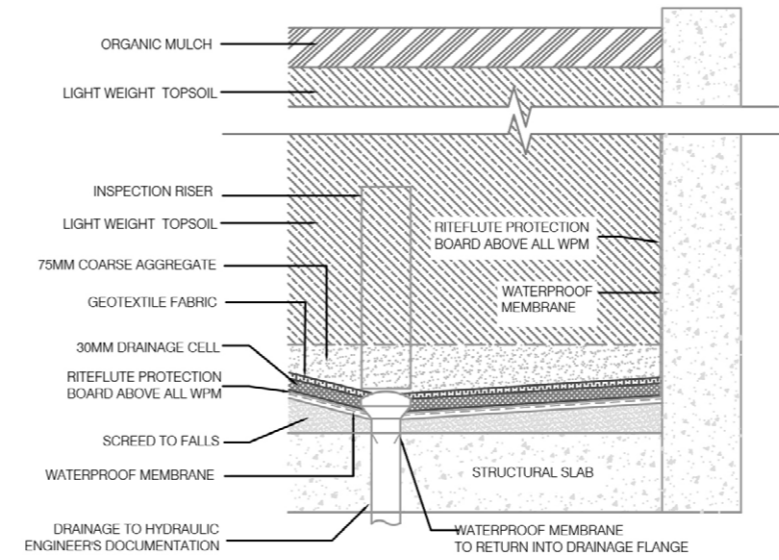


Planting on Podium Detailing

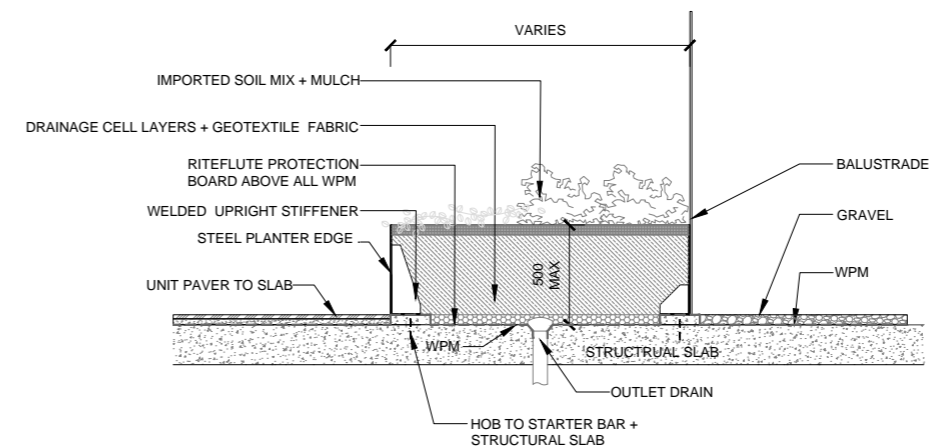
Typical 1m Deep Planter on Podium Detail



Typical Planter Lining Detail



Typical Shallow Planter on Podium Detail



See Design Studio
Landscape Architects

Please feel free to contact us with any inquires.

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Waste Management Plan

6 Alexander Road, Dalkeith

Prepared for Universal United Investments Pty Ltd

24 March 2025

Project Number: WMP24099

DOCUMENT CONTROL					
Version	Description	Date	Author	Reviewer	Approver
1.0	First Approved Release	4/12/2024	SC/AB	DP	AB
2.0	Second Approved Release	31/01/2025	MA	DP	DP
3.0	Third Approved Release	24/03/2025	MA	AB	AB
Approval for Release					
Name	Position	File Reference			
Ann Brouwer	Project Manager – Waste Management Consultant	WMP24099-02_Waste Management Plan_3.0			
Signature					
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Executive Summary

Universal United Investments Pty Ltd is seeking development approval for the proposed residential development located at 6 Alexander Road, Dalkeith (the Proposal).

To satisfy the conditions of the development application the City of Nedlands (the City) requires the submission of a Waste Management Plan (WMP) that will identify how waste is to be stored and collected from the Proposal. Talis Consultants has been engaged to prepare this WMP to satisfy the City's requirements.

A summary of the bin size, numbers, collection frequency and collection method is provided in the below table.

Proposed Waste Collection Summary

Waste Type	Generation (L/week)	Bin Size (L)	Number of Bins	Collection Frequency	Collection
Refuse	600	240	Five	Once each fortnight	City of Nedlands
Recycling	600	240	Five	Once each fortnight	City of Nedlands
FOGO	200	240	One	Once each week	City of Nedlands

The City will collect refuse, recyclables and FOGO from the Proposal utilising its kerbside collection service. The City's waste collection vehicle will service the bins from the Bin Presentation Area on Alexander Road.

A strata manager/caretaker will oversee the relevant aspects of waste management at the Proposal.

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2.1	Proposed Tenancies	2
2.2	Waste Generation Rates	2
2.3	Waste Generation Volumes	3
3	Waste Storage	4
3.1	Internal Bins	4
3.2	Bin Sizes.....	4
3.3	Bin Storage Area Size	4
3.4	Bin Storage Area Design.....	6
4	Waste Collection	7
4.1	Bulk Waste	8
5	Waste Management	9
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Table 2-2: Estimated Waste Generation.....	3
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Diagram 1: Bin Storage Area

Diagram 2: Bin Presentation Area

Figures

Figure 1: Locality Plan

1 Introduction

Universal United Investments Pty Ltd is seeking development approval for the proposed residential development located at 6 Alexander Road, Dalkeith (the Proposal).

To satisfy the conditions of the development application the City of Nedlands (the City) requires the submission of a Waste Management Plan (WMP) that will identify how waste is to be stored and collected from the Proposal. Talis Consultants has been engaged to prepare this WMP to satisfy the City's requirements.

The Proposal is bordered by residential properties to the north, east and south and Alexander Road to the west, as shown in Figure 1.

1.1 Objectives and Scope

The objective of this WMP is to outline the equipment and procedures that will be adopted to manage waste (refuse, recyclables and FOGO) at the Proposal. Specifically, the WMP demonstrates that the Proposal is designed to:

- Adequately cater for the anticipated volume of waste to be generated;
- Provide an adequately sized Bin Storage Area, including appropriate bins; and
- Allow for efficient collection of bins by appropriate waste collection vehicles.

To achieve the objective, the scope of the WMP comprises:

- Section 2: Waste Generation;
- Section 3: Waste Storage;
- Section 4: Waste Collection;
- Section 5: Waste Management; and
- Section 6: Conclusion.

2 Waste Generation

The following section shows the waste generation rates used and the estimated waste volumes to be generated at the Proposal.

2.1 Proposed Tenancies

The anticipated volume of refuse, recyclables and FOGO is based on the number of apartments at the Proposal. The Proposal consists of the following:

- Three Bedroom Apartments – 3;
- Five Bedroom Apartments – 1; and
- Eight Bedroom Apartments – 1.

2.2 Waste Generation Rates

In order to achieve an accurate projection of waste volumes for the Proposal, consideration was given to the City's *Draft Local Planning Policy and Guidelines* (28 July 2020).

Table 2-1 shows the waste generation rates which have been applied to the Proposal.

Table 2-1: Waste Generation Rates

Tenancy Use Type	City's Guideline Reference	Refuse Generation Rate	Recycling Generation Rate	FOGO Generation Rate
Three Bedroom Apartments	Three Plus Bedrooms	120L/week	120L/week	40L/week
Five Bedroom Apartments	Three Plus Bedrooms	120L/week	120L/week	40L/week
Eight Bedroom Apartments	Three Plus Bedrooms	120L/week	120L/week	40L/week

2.3 Waste Generation Volumes

Waste generation is estimated by volume in litres (L) as this is generally the influencing factor when considering bin size, numbers and storage space required.

Waste generation volumes in litres per week (L/week) adopted for this waste assessment is shown Table 2-2. It is estimated that the apartments at the Proposal will generate 600L of refuse, 600L of recyclables and 200L of FOGO each week.

Table 2-2: Estimated Waste Generation

Residential Apartments	Number of Apartments	Waste Generation Rate (L/week)	Waste Generation (L/week)
Refuse			
Three Bedroom Apartments	3	120	360
Five Bedroom Apartments	1	120	120
Eight Bedroom Apartments	1	120	120
Total			600
Recyclables			
Three Bedroom Apartments	3	120	360
Five Bedroom Apartments	1	120	120
Eight Bedroom Apartments	1	120	120
Total			600
FOGO			
Three Bedroom Apartments	3	40	120
Five Bedroom Apartments	1	40	40
Eight Bedroom Apartments	1	40	40
Total			200

3 Waste Storage

Waste materials generated within the Proposal will be collected in the bins located in the Bin Storage Area, as shown in Diagram 1, and discussed in the following sub-sections.

3.1 Internal Bins

To promote positive recycling behaviour and maximise diversion from landfill, there will be sufficient room within each apartment for a minimum of three kitchen type bins for the source separation of refuse, recyclables and FOGO.

Purchase and maintenance of the internal bins will be the responsibility of the residents. Residents will be encouraged to utilise kitchen caddies with compostable liners to collect FOGO waste for disposal.

Waste from these internal bins will be transferred by the residents to the Bin Storage Area and deposited into the appropriate bins.

All bins will be colour coded and labelled in accordance with Australian Standards (AS 4123.7) to assist residents to dispose of their separate waste materials in the correct bins.

3.2 Bin Sizes

Table 3-1 gives the typical dimensions of standard bin sizes that may be utilised at the Proposal. It should be noted that these bin dimensions are approximate and can vary slightly between suppliers.

Table 3-1: Typical Bin Dimensions

Dimensions (m)	Bin Sizes			
	120L	240L	660L	1,100L
Width	0.5	0.6	1.3	1.4
Depth	0.6	0.8	0.8	1.3
Height	1.0	1.1	1.2	1.5

Reference: City of Nedlands LPP – 4.0 Bin Size and Colour – Table 3

3.3 Bin Storage Area Size

To ensure sufficient area is available for storage of the bins, the amount of bins required for the Bin Storage Area was modelled utilising the estimated waste generation in Table 2-2, bin sizes in Table 3-1 and based on collection of refuse and recyclables once each fortnight and FOGO once each week.

Based on the results shown in Table 3-2, the Bin Storage Area has been sized to accommodate:

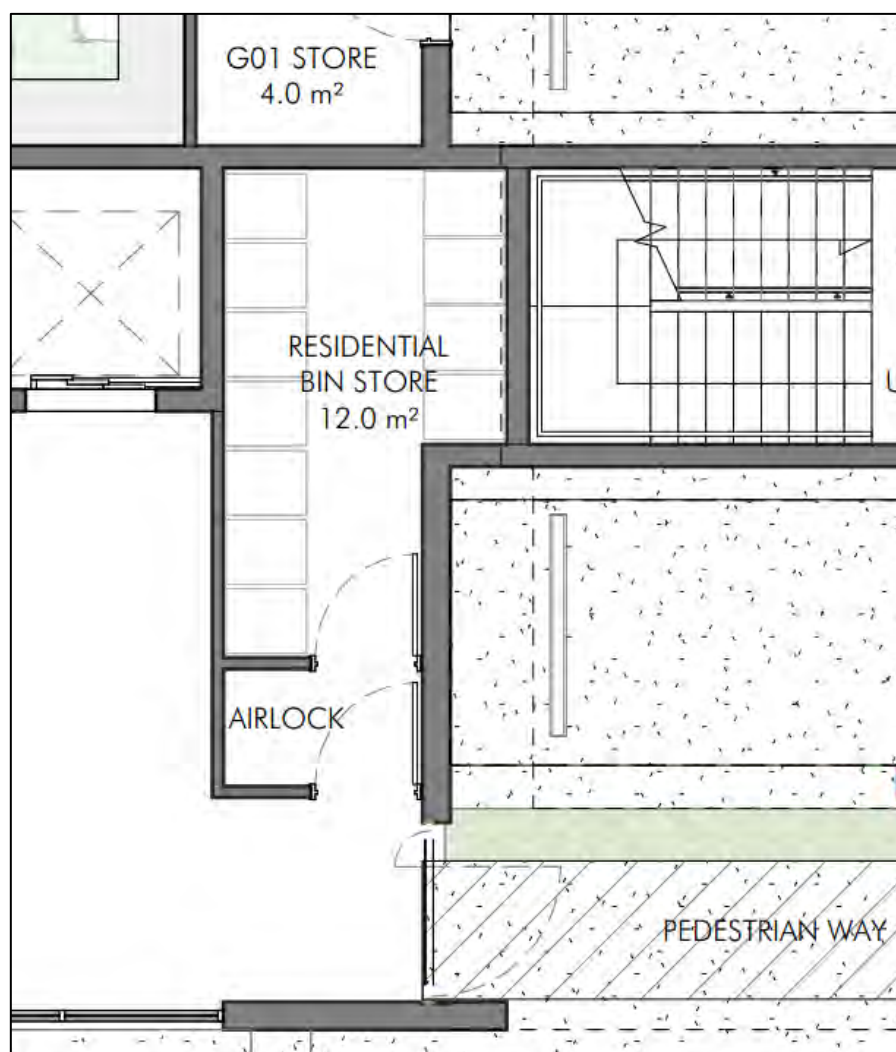
- Five 240L refuse bins;
- Five 240L recycling bins; and
- One 240L FOGO bin.

Table 3-2: Bin Requirements for Bin Storage Area

Waste Stream	Waste Generation (L/week)	Number of Bins Required			
		120L	240L	660L	1,100L
Refuse	600	10	5	2	2
Recycling	600	-	5	2	2
FOGO	200	-	1	-	-

The configuration of these bins within the Bin Storage Area is shown in Diagram 1. It is worth noting that the number of bins and corresponding placement of bins shown in Diagram 1 represents the maximum requirements assuming fortnightly collections of refuse and recyclables and one collection each week of FOGO.

Diagram 1: Bin Storage Area



3.4 Bin Storage Area Design

The design of the Bin Storage Area will take into consideration:

- Smooth impervious floor sloped to a drain connected to the sewer system;
- Taps for washing of bins and Bin Storage Area;
- Adequate aisle width for easy manoeuvring of bins;
- No double stacking of bins;
- Doors to the Bin Storage Area self-closing and vermin proof;
- Doors to the Bin Storage Area wide enough to fit bins through;
- Ventilated to a suitable standard;
- Appropriate signage;
- Undercover where possible and be designed to not permit stormwater to enter the drain;
- Located behind the building setback line;
- Bins not to be visible from the property boundary or areas trafficable by the public; and
- Bins are reasonably secured from theft and vandalism.

Bin numbers and storage space within the Bin Storage Area will be monitored by the strata manager/caretaker during the operation of the Proposal to ensure that the number of bins and collection frequency is sufficient.

4 Waste Collection

The City will service the Proposal and provide five 240L bins for refuse, five 240L bins for recyclables and one 240L bin for FOGO.

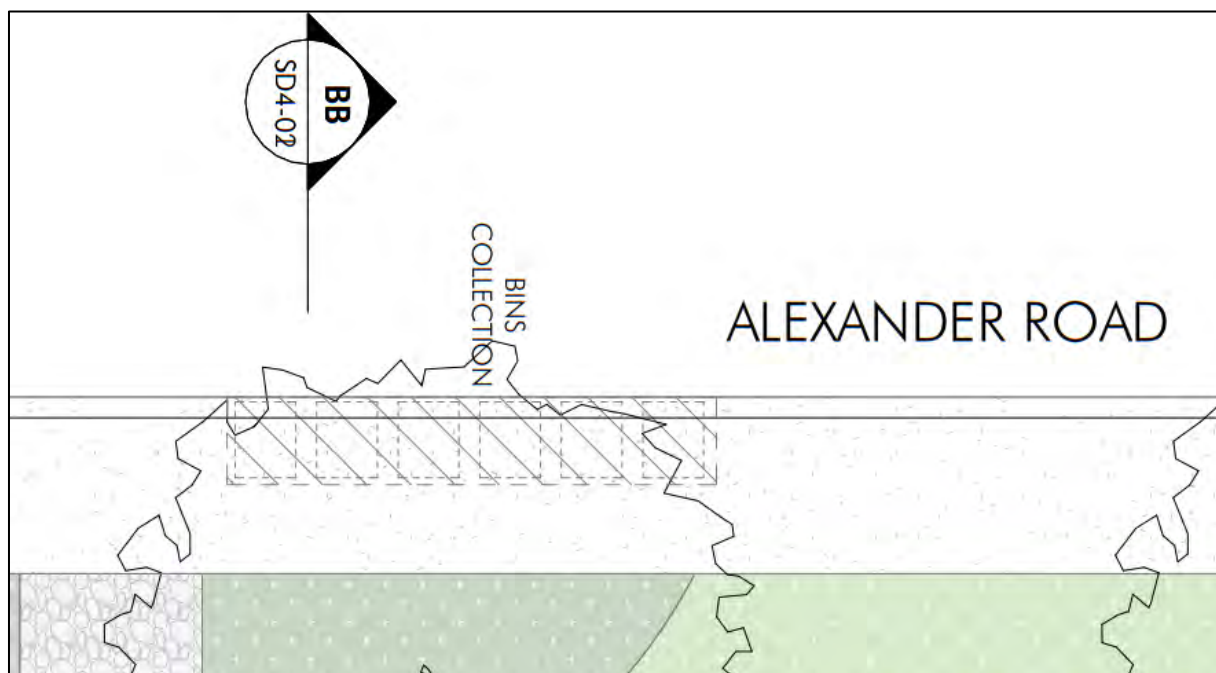
The City will collect refuse and recyclables once each fortnight and FOGO once each week from the Proposal utilising the City's side arm waste collection vehicle.

The City will service bins from the Bin Presentation Area on the verge on Alexander Road at the front of the Proposal, as shown in Diagram 2. Note, Diagram 2 shows the maximum number of bins that would be required on the verge on collection day.

Bins will be presented for collection 1m from the verge with the wheels and handles facing away from the street. The bins will remain clear of obstructions such as power poles, signs and street trees, and will be placed so as not to obstruct pedestrians, footpaths or bike lanes. Bins will be lined up neatly and in a single row along the verge, with sufficient space between each bin to facilitate collection by the City's side arm waste collection vehicle.

The strata manager/caretaker will ferry the bins to and from the Bin Presentation Area on collection days. The travel path between the Bin Storage Area and the Bin Presentation Area will be of flat surface and kept free of obstacles. The strata manager/caretaker will return the bins to the Bin Storage Area as soon as possible on the same day following collection.

Diagram 2: Bin Presentation Area



4.1 Bulk Waste

The City provides two bulk waste collections each financial year, details of the services provided by the City can be found on the City's website.

Each apartment has a minimum allocated storage room and an additional bulk waste storage area of 3.4m² has been provided to be utilised for the temporary storage of bulk waste.

Sufficient verge space, clear of planting has been allocated for temporary placement of bulk collection materials on the verge, or for placement of a 10m³ bulk bin by the City's contractor, depending on which service option the City provides to the Proposal.

5 Waste Management

A strata manager/caretaker will be engaged to complete the following tasks:

- Monitoring and maintenance of bins and the Bin Storage Area;
- Cleaning of bins and Bin Storage Area, when required;
- Ferrying of bins to and from the Bin Storage Area and Bin Presentation Area on collection days;
- Ensure residents at the Proposal are made aware of this WMP and their responsibilities thereunder;
- Monitor resident behaviour and identify requirements for further education and/or signage;
- Monitor bulk waste accumulation and assist with its removal, as required;
- Regularly engage with residents to develop opportunities to reduce waste volumes and increase resource recovery; and
- Regularly engage with the City to ensure efficient and effective waste service is maintained.

6 Conclusion

As demonstrated within this WMP, the Proposal provides a sufficiently sized Bin Storage Area for storage of refuse, recyclables and FOGO, based on the estimated waste generation volumes and suitable configuration of bins. This indicates that an adequately designed Bin Storage Area has been provided, and collection of refuse, recyclables and FOGO can be completed from the Proposal.

The above is achieved using:

- Five 240L refuse bins, collected once each fortnight;
- Five 240L recycling bins, collected once each fortnight; and
- One 240L FOGO bin, collected once each week.

The City will collect refuse, recyclables and FOGO from the Proposal utilising its kerbside collection service. The City's waste collection vehicle will service the bins from the Bin Presentation Area on Alexander Road.

A strata manager/caretaker will oversee the relevant aspects of waste management at the Proposal.

Figures

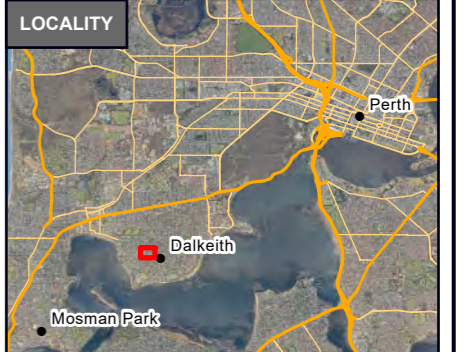
Figure 1: Locality Plan



LEGEND

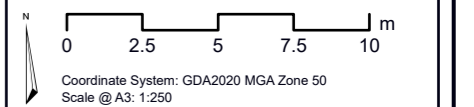
- Site Boundary
- Cadastrre**
- Freehold
- Road
- Easement

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LOCALITY

6 Alexander Road
Dalkeith WA 6009
Universal United Investments Pty Ltd



Prepared: B Roelofse	Date: 17/09/2024
Reviewed: A Brouwer	Revision: A
Project: WMP24099	



Figure 01

Data source: Roads, Cadastre • Landgate, 2024, Imagery: Nearmap, 2024.



Assets | Engineering | Environment | Noise | Spatial | Waste

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Technical Note

Subject: Proposed Residential Development
6 Alexander Road, Dalkeith

Date: 25th March 2025

Author: P. Nguyen

Reviewed:

L. De Leon

Client: Space Collective Architects

1. Introduction

Shawmac have been engaged by Space Collective Architects to undertake a traffic review of a proposed residential development in Dalkeith.

The site address is 6 Alexander Road in Dalkeith. The local authority is the City of Nedlands. An aerial view of the existing site is shown in Figure 1.



Figure 1: Aerial View (October 2024)



1.1. Proposed Development

The site has an existing residential dwelling which will be demolished. The proposed development is a multi-storey building with 5 apartments.

Vehicle access is proposed from Alexander Road via a 3.0m wide driveway and crossover.

The site layout is shown in Figure 2.

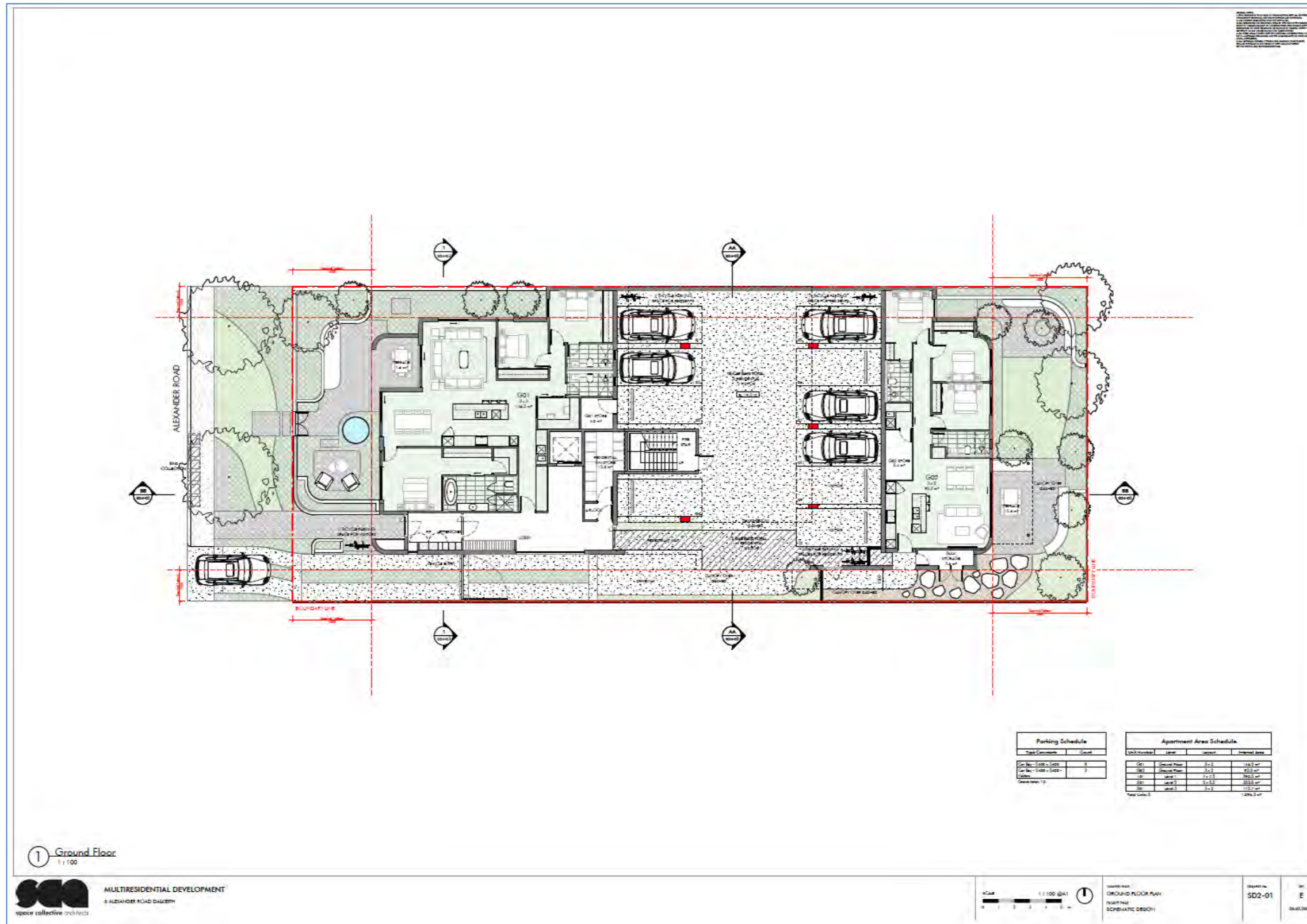


Figure 2: Site Layout

2. Vehicle Access

2.1. Vehicle Access

According to the City of Nedlands *Specification for the Construction of Crossovers*, the minimum width of any crossover is 3.0m and the maximum width of any crossover and/or adjoining crossover is to be 6.0m.

As the site is zoned R80, the Residential Design Codes Volume 2 (RDC2) is applicable. RDC2 does not specify minimum driveway widths but Acceptable Outcome A 3.8.5 recommends that the driveway width is kept to a functional minimum relative to the traffic volumes and entry / egress requirements.

On this basis, the 3.0m wide driveway and crossover is adequate.

2.2. Vehicle Sight Distance

Sight distance requirements from vehicle exit points are defined in Figure 3.2 of Australian Standard AS2890.1-2004 *Parking facilities Part 1: Off street car parking* (AS2890.1) which are based on the Austroads Stopping Sight Distance (SSD) shown in Figure 3.

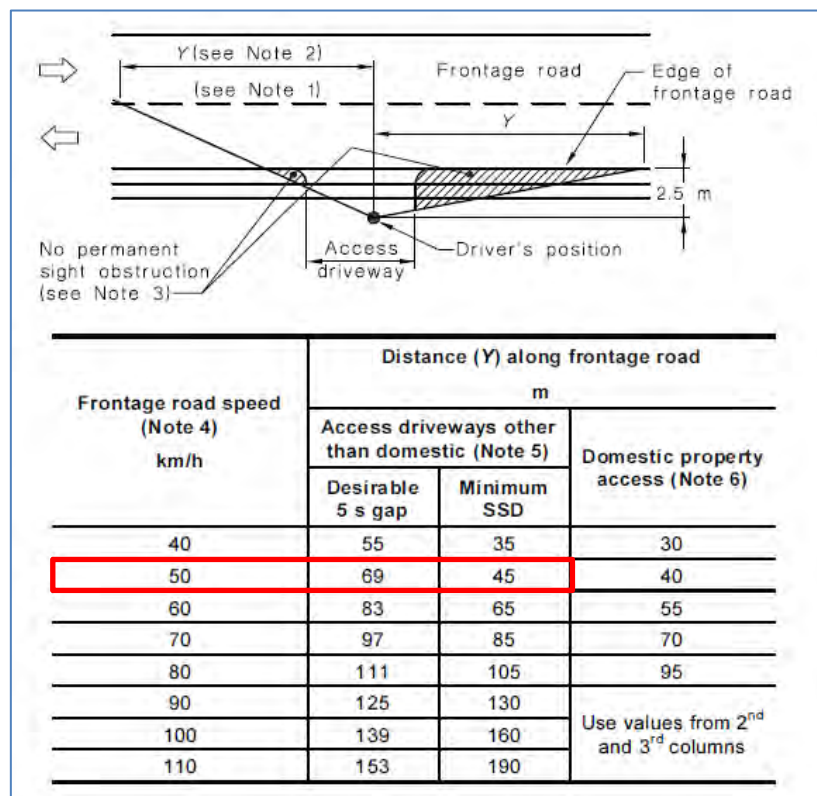


Figure 3 : AS2890.1 Sight Distance Requirements

Based on the 50km/h speed limit along Alexander Road, the minimum required SSD is 45m.

As shown in Figure 4, the minimum required sight distance is achieved in both directions. Vertically, the road is relatively flat with no major crests or sags that reduce the sight distance below the minimum requirement.

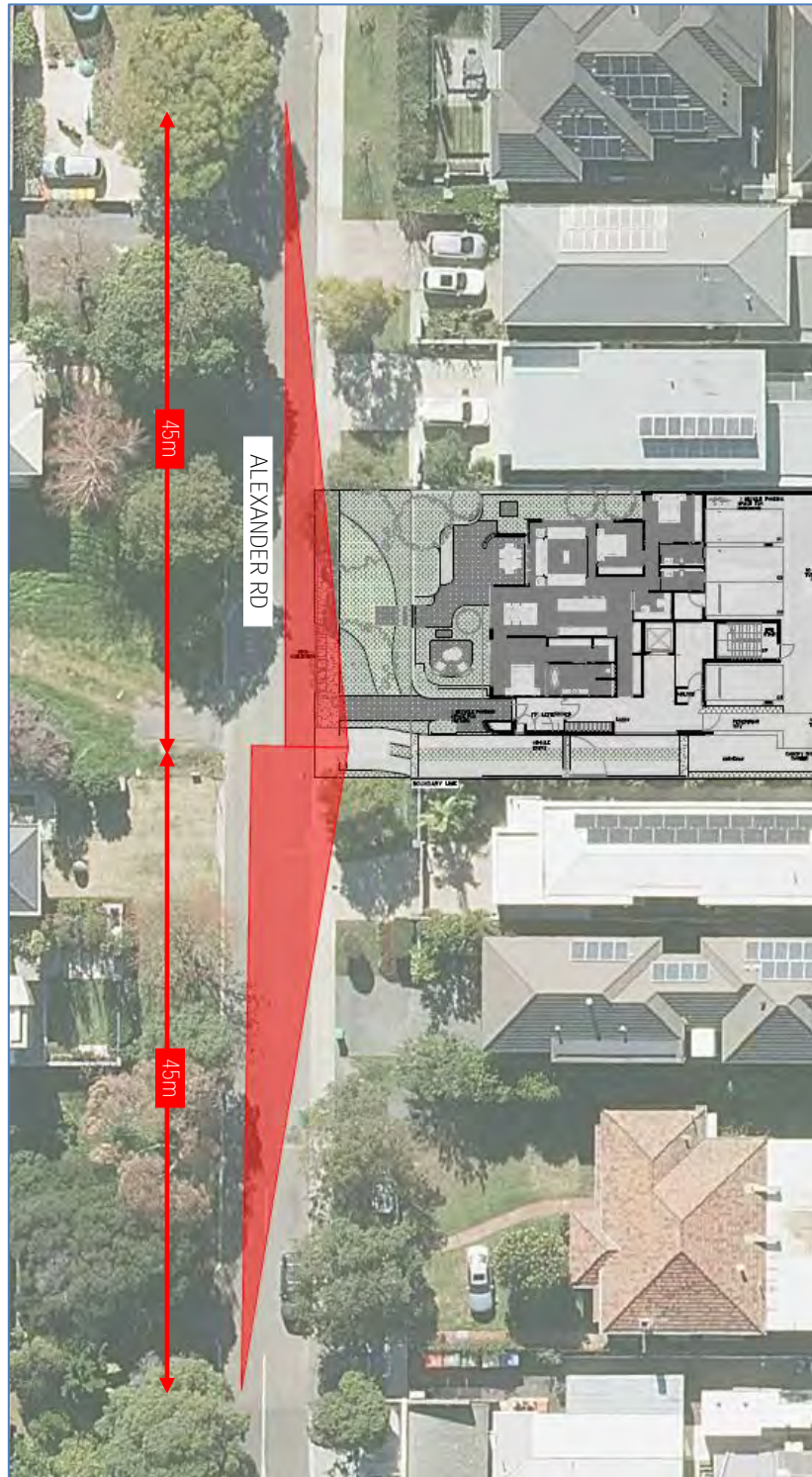


Figure 4: Sight Distance Check



2.3. Pedestrian Sight Distance

To allow for visibility between vehicles and pedestrians on the driveway, RDC2 requires visual truncations to be kept clear of obstruction as shown in Figure 5.

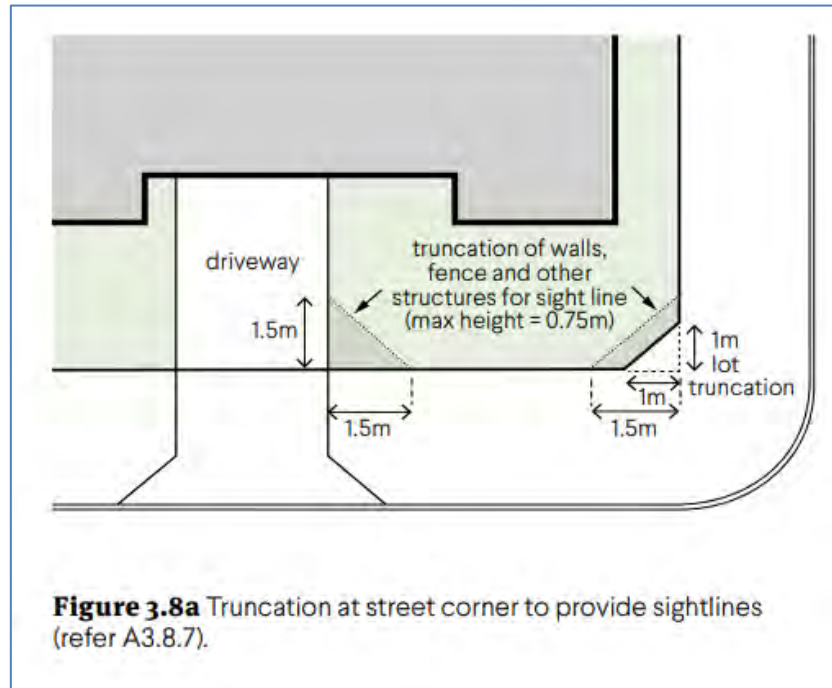


Figure 5: RDC2 Pedestrian Sight Distance Requirements

As shown in Figure 6, the required truncations are mostly achieved but are slightly reduced by the boundary fence to the south. The encroachment is not considered a major issue for various reasons, including:

- The pedestrian path on the development side of Alexander Road is adjacent to the kerb and not along the property boundary. Vehicles exiting the site will sit mostly within the verge and will have sufficient visibility to pedestrians before crossing the footpath.
- Based on the building entry point and obstructions within the verge area (trees, signs, light poles), it is highly unlikely that pedestrian will walk in the area of the visual truncation.

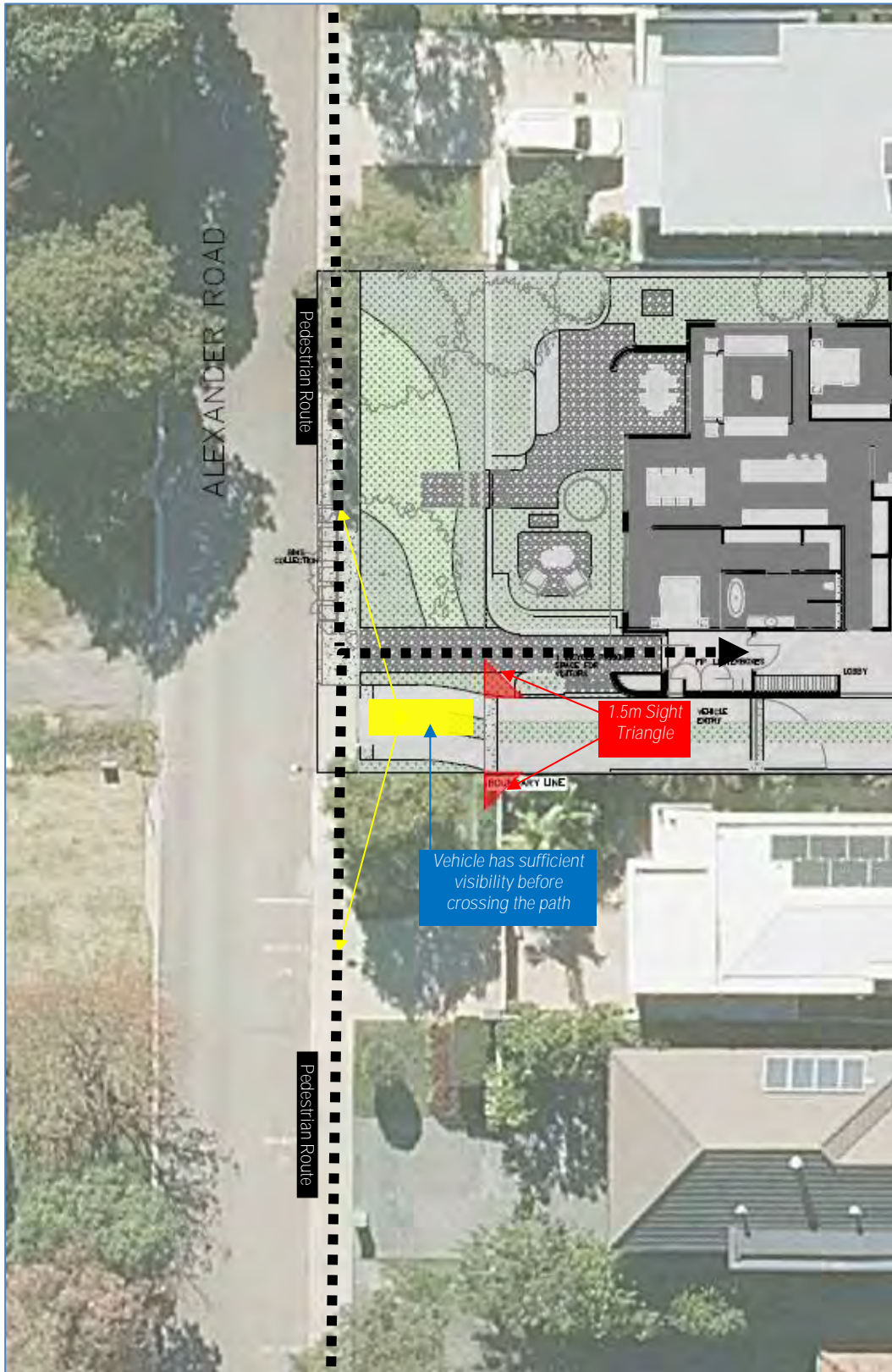


Figure 6: RDC2 Pedestrian Sight Distance Check

3. Parking and Layout Assessment

3.1. Parking Supply

The car parking requirements according to RDC2 are calculated in Table 1. As the site is not located within a defined activity centre or within the prescribed distances to high frequency public transport, the Location B requirements have been applied. As noted in RDC2, parking requirements are rounded up to the next whole number.

Table 1: RDC2 Car Parking Requirements – Location B

Land Use	Unit	Requirement	Quantum	Required Bays
Apartment	1 bedroom dwellings	1 bay per dwelling	0	0
	2+ bedroom dwellings	1.25 bay per dwelling	5	7
	Visitor	1 bay per 4 dwellings (up to 12 dwellings)	5	2
Total Required				7 resident bays 2 visitor bays

As above, the development requires a minimum of 7 resident bays and 2 visitor bays. The proposed parking provision is 8 resident bays and 2 visitor bays which satisfies the minimum requirements.

3.2. Motorcycle Bays

As the development proposes less than 20 dwellings, no motorcycle / scooter parking is required.

3.3. Parking Layout

The layout and dimensions of the car parking areas have been assessed for compliance with Australian Standards AS2890.1-2004 *Parking facilities Part 1: Off street car parking* (AS2890.1) as detailed in Table 2.

Table 2: AS2890.1 Car Parking Compliance

Dimension	Requirement	Provided
<i>90 degree parking – Class 1A – Residential, domestic parking</i>		
Car Bay Width	2.4m	2.4m
Car Bay Length	5.4m	5.4m
Parking Aisle Width	5.8m	6.0m
Blind Aisle Extension	1.0m	1.0m

As shown, the parking layout is compliant with AS2890.1. A vehicle swept path analysis has been undertaken to check manoeuvring for the bays at the end of the parking aisle. The analysis has been completed in AutoTURN vehicle tracking software using the Australian Standard B85 vehicle as required for parking manoeuvres. The results of the analysis are attached as Appendix A and these demonstrate that the end bays will accommodate the B85 vehicle. The splay of the crossover will also need to be widened slightly to accommodate turning.

3.4. Provision for Service Vehicles

Waste from the development will be stored in the bin store area on the ground level. Waste will be collected via kerbside collection along the verge and so there is no requirement to accommodate waste vehicles on the site.

4. Traffic Generation Characteristics

The volume of traffic generated by the proposed building has been estimated using trip generation rates from the NSW Roads and Maritime Services (RMS) *Guide to Traffic Generating Developments* as detailed in Table 3.

The peak hour trip rates are based on the peak hour of the adjacent road network typically occurring between 7 to 9am and between 4 to 6pm.

Table 3: Proposed Development Vehicle Trip Generation

Land Use	Units	Quantity	Generation Rate			Number of Trips		
			Daily	AM Peak	PM Peak	Daily	AM Peak	PM Peak
Medium Density Residential Flat Buildings	Dwellings	5	5	0.5	0.5	25	3	3

As shown, the proposed development is predicted to generate approximately 25 vehicle trips per day including 3 trips during the morning peak hour and 3 during the afternoon peak hour.

According to the WAPC TIA guidelines, an increase of between 10 to 100 peak hour vehicles is considered to have a low to moderate impact and is generally deemed acceptable without requiring detailed capacity analysis.

The estimated 3 vehicles per hour is below this range and so the development traffic is considered to have a very low impact and can be accommodated within the existing capacity of the road network.

5. Pedestrian and Cyclist Access

5.1. Accessibility

Most of the roads in the surrounding area have at least one footpath. The external path network is well established and considered to be adequate for pedestrians and cyclists to safely travel between the site and surrounding areas.

5.2. Bicycle Parking

According to RDC2, the bicycle parking requirements are 0.5 resident spaces per dwelling and 1 visitor space per 10 dwellings. For 5 apartments, the minimum requirement is 3 resident bicycle spaces and 1 visitor bicycle space.

The development proposes 4 resident bicycle spaces and 1 visitor bicycle space which satisfies the minimum requirements.

6. Public Transport Access

The only public transport service operating within reasonable distance of the site is Transperth Bus Route 24 which operates between East Perth and Claremont Station. The closest stop is on Waratah Avenue, approximately 100m walking distance from the site.

The demand for public transport is expected to be low and the existing available services are adequate.



7. Site Specific Issues and Safety Issues

7.1. Crash History

The crash history of the adjacent road network was obtained from the MRWA Reporting Centre. The search included Alexander Road between Philip Road and Waratah Avenue over the five-year period from January 2020 to December 2024. Only two crashes have been recorded on the above section of road and both occurred at the intersection between Alexander Road and Waratah Avenue (1 right angle crash and 1 hit object).

This intersection was upgraded from a stop-controlled intersection to a roundabout in early 2020 which will have improved the safety of the intersection significantly. The detailed crash history indicates that the right angle crash occurred before the intersection was upgraded.

The proposed development will generate a low volume of traffic and there is no indication that the development would increase the risk of crashes to unacceptable levels.

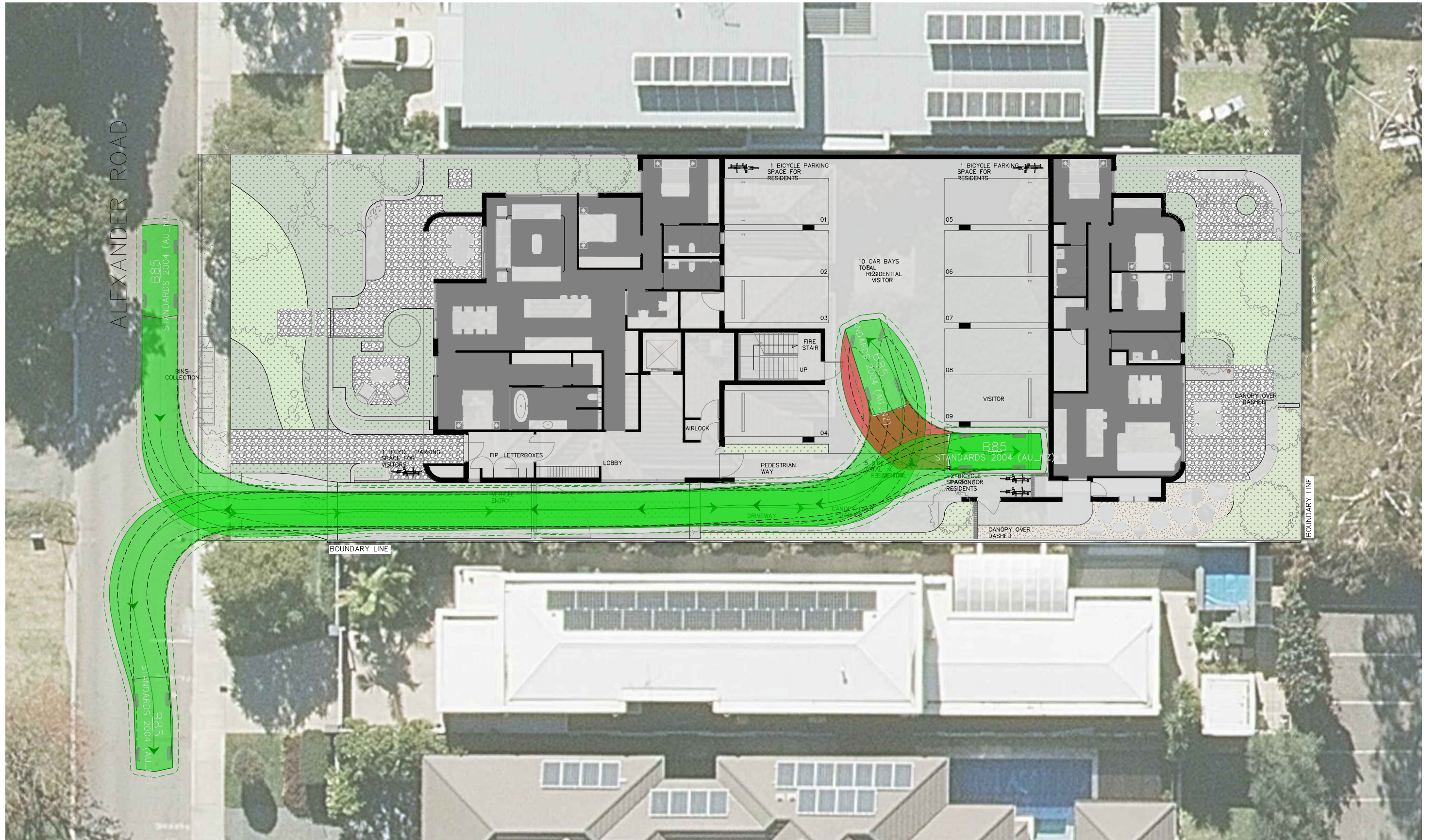
8. Conclusion

This Transport Impact Statement for the proposed residential development at 6 Alexander Road in Dalkeith concluded the following:

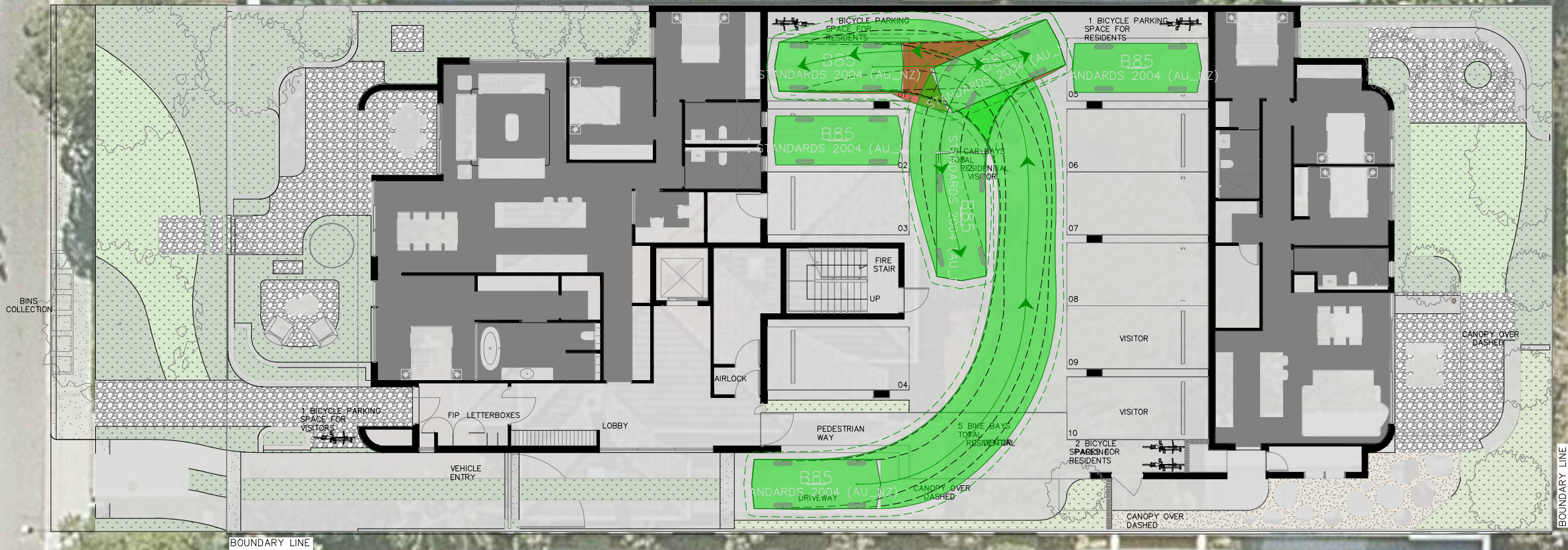
- The existing road network will have sufficient capacity to accommodate the traffic generated by the development and no modifications are required.
- The proposed crossover achieves the required sight distance to vehicles in all directions.
- The required truncations at the driveway are mostly achieved but are slightly reduced by the boundary fence to the south. The minor encroachment is not considered a major issue for various reasons, including:
 - The pedestrian path on the development side of Alexander Road is adjacent to the kerb and not along the property boundary. Vehicles exiting the site will sit mostly within the verge and will have sufficient visibility to pedestrians before crossing the footpath.
 - Based on the building entry point and obstructions within the verge area (trees, signs, light poles), it is highly unlikely that pedestrian will walk in the area of the visual truncation.
- The provision of 8 residential parking bays and 2 visitor parking bays satisfies the minimum parking requirements of the Residential Design Codes Volume 2.
- The development proposes 4 resident and 1 visitor parking space for bicycles on the ground floor. The bicycle parking provision meets the minimum requirements.
- A review of the parking concludes that the layout complies with Australian Standards.
- A vehicle swept path analysis indicates that the parking layout will accommodate the B85 vehicle in a satisfactory manner. The splay of the crossover will need to be widened slightly to accommodate turning.
- The crash history does not indicate any issues with the road network. The traffic generated by the site will be low and is unlikely to increase the risk of crashes to unacceptable levels.
- The existing external path network is considered to be adequate.
- The existing public transport services are considered to be adequate.



9. Appendix A – Vehicle Swept Paths



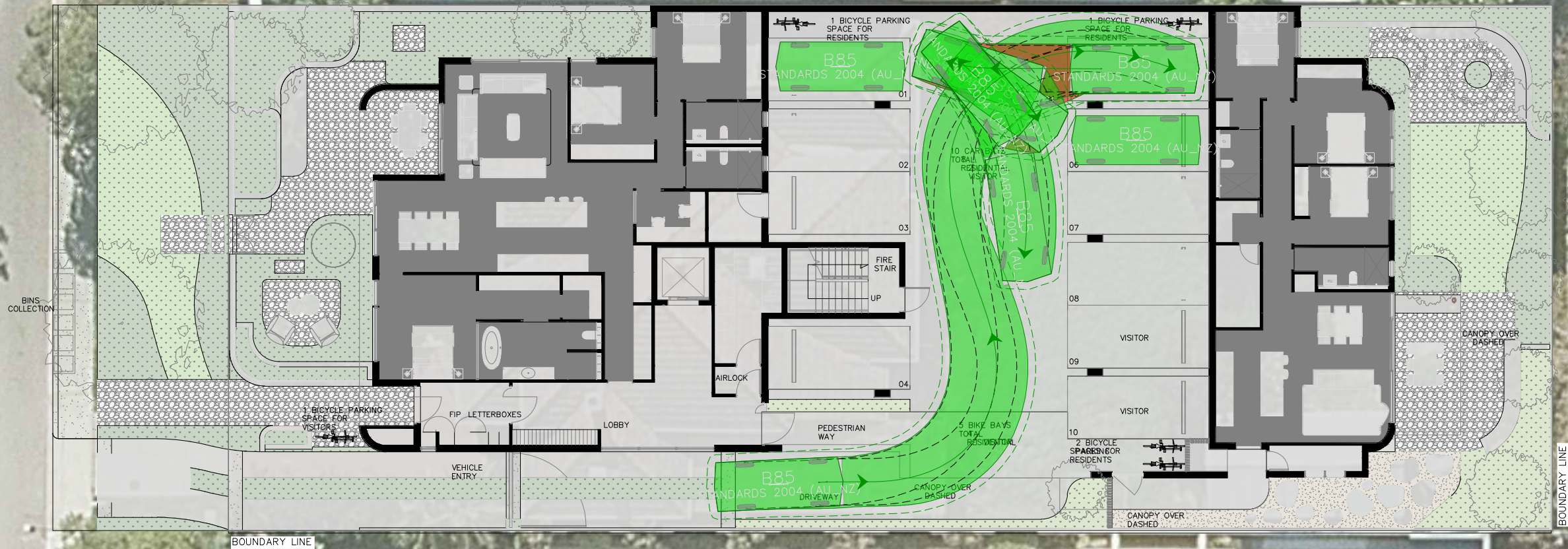
ALEXANDER ROAD



BOUNDARY LINE

BOUNDARY LINE

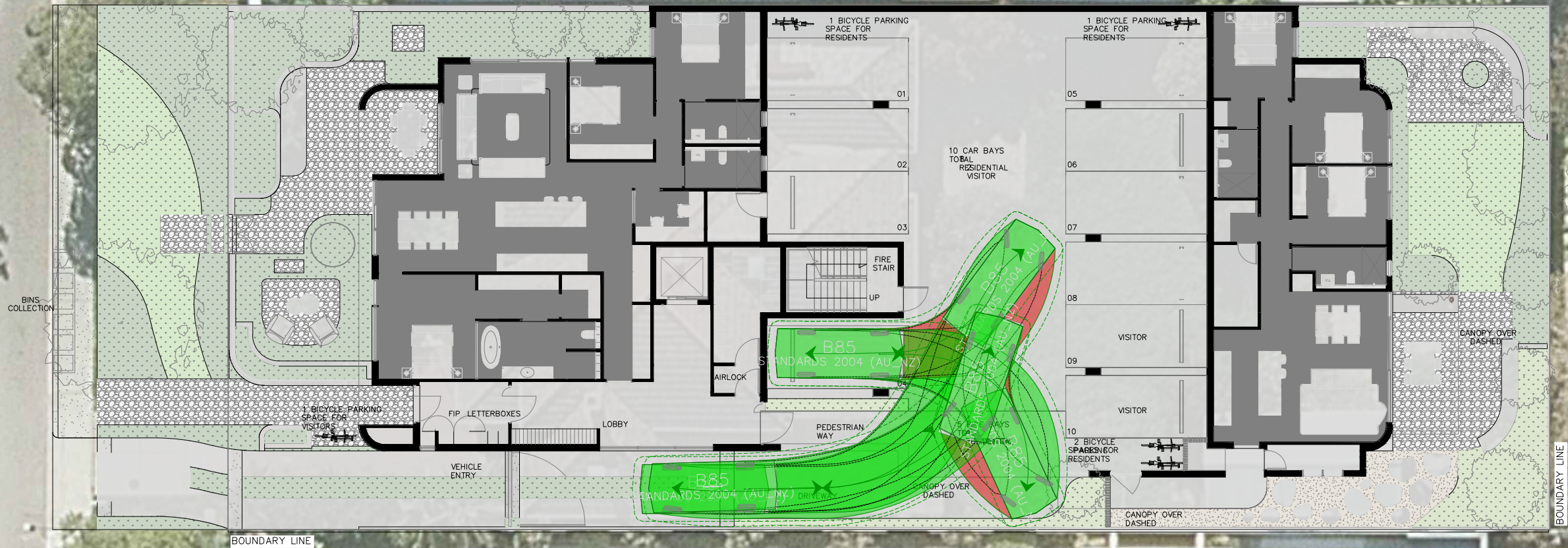
ALEXANDER ROAD



BOUNDARY LINE

BOUNDARY LINE

ALEXANDER ROAD



BOUNDARY LINE

BOUNDARY LINE

December 2024

6 Alexander Road

Sustainability Strategy

Graham Agar
FULL CIRCLE DESIGN SERVICES

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ACN: 163 742 890	
ABN: 84 163 742 890	

Title:	6 Alexander Road - Sustainability Strategy
Author:	Graham Agar
Client:	SCA
Contact:	Felipe Soto
Description:	<p>This report provides the ESD strategy for the proposed multi-residence development at 6 Alexander Road in Dalkeith</p> <p>The project includes a single building with five dwellings over four floors.</p> <p>The design is targeting best practice sustainable design features across a range of assessment categories including renewable generation, thermal performance, embodied energy and sustainable transport.</p>

Revision	Date	Checked by	Transmitted by
A	15 th September 2024		GEA
B	16 th September 2024		GEA
C	23 rd October 2024		GEA
D	15 th November 2024		GEA
E	3 rd December 2024		GEA

Distribution	Revision								
Receiver	A	B	C	D	E				
SCA	X	X	X	X	X				

Executive Summary

The proposed development at 6 Alexander Road in Dalkeith includes five, very high quality residences, across four levels, including a ground floor secure car park.

Each apartment has access to their own external space and will include a high performance envelope to maximise views, natural light and thermal comfort.

Considering the nature of the project and its location, the project team have developed four key sustainable design themes which support the project's value proposition and add to the experience for owners and occupiers:

- Healthy living
- Minimised carbon footprint
- Sustainable transport
- Future resilience

Supporting these themes are a range of sustainable design features and inclusions which will provide verifiable performance improvement over current business as usual design and construction practices. These features include:

- Photovoltaic array and batteries on site
- Development on a previously developed site
- Life cycle assessment
- Electric vehicle infrastructure
- Envelope performance improvement
- Native planting
- Fossil fuel free site
- Low GWP refrigerant

The project will exceed the requirements published within the City of Nedlands LPP 1.3 Sustainable Design – Residential by:

- Mitigating urban heat island
- Providing water efficient appliances and tapware
- Using waterwise planting and irrigation
- Facilitating future EV use
- Avoiding fossil fuels on site
- Providing renewable energy and an embedded network.

This report provides additional information on the proposed themes and the verification pathway for the project.

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1. Introduction

FCDS have been commissioned to assist the 6 Alexander Road design team in developing and implementing a holistic sustainable design strategy which meets best practice design outcomes and provides verifiable performance metrics for external review and comment.

This report presents the proposed ESD themes the project has elected to pursue and outlines key design features to deliver measurable performance against these themes.

1.1 Project Description

The development includes 5 new high end dwellings, located over four floors in the Perth suburb of Dalkeith.

The design team will be targeting high performance materials to optimise occupant thermal comfort, provide cross flow natural ventilation, exceptional levels of natural light and low operating carbon emissions.

1.2 Existing Site

The site is part of a well developed residential community, on a currently bare lot. The new development can improve biodiversity outcomes for the area with additional native planting and pedestrian connections across the lot.

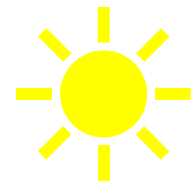
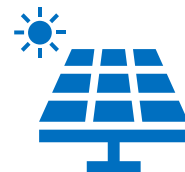


Above: Lot Location (Google Maps)

1.3 Strategic Project Aims

In progressing this development, the project team are keen to deliver best practice sustainable design outcomes. As part of this commitment, the project team has selected the following key sustainable design themes for the project. Design features, aligning with national best practice, have also been proposed as a means to demonstrate the project has met its intended outcomes:

- Healthy living
 - The design should promote occupant health and wellbeing, both through internal design decisions and access to external spaces.
- Minimised footprint and operational costs
 - The design must ensure that operational utility use – such as energy and water – are minimised, with long life cycle products and low carbon technology where possible.
 - The development should minimise impacts to nature and local ecology.
- Climate resilience and future focus
 - The development has a design life of in excess of 50 years. The final product must provide high performance in current climatic conditions, with flexibility and resilience to suit weather patterns in 2080 and beyond.
- Sustainable transport
 - The location of the project can assist occupants in reducing their dependence on cars. Integration and facilitation of sustainable transport options is key for project success.



1.4 City of Nedlands Brief

The project is less than 10 dwellings and less than \$15M project budget, the project therefore needs to comply with the four requirements in green below and any two others. The design team intend to comply with five of these bonus features and addresses an additional feature not considered required for smaller developments:

Feature	Performance Targets	Comment
Performance Target	NABERS Rating	The development can undertake a NABERS rating. Performance targets are noted above.
Envelope Pressure Testing	Undertake pressure testing on 10% of apartments	Not applicable – less than 10 dwellings.
Embedded meter network	Provide a network for internal billing.	Not required, less than 10 dwellings. Minimal common area to track.
Natural Ventilation	Compliant with AS 1668.4	Compliant, elevations and window areas to meet target requirements.
Exhaust of Pollutants	Provide exhaust systems for kitchens laundries and toilets.	Compliant, to be documented in later stages.
Toxicity and Indoor Environment	Utilise low VOC & formaldehyde products	Compliant, low VOC intent for paints, floor coverings, adhesives and sealants. Compliant, E0 joinery and composite wood products.
Amenities	Provide common spaces	Not applicable, less than 30 bedrooms.
Urban Heat Island	75% of site area does not contribute to urban heat island effects.	Compliant, most of the development is planting or roof. Compliant finishes to be selected for sun deck.
Upfront Carbon Reduction	20% Performance improvement	Not applicable, less than 10 dwellings. Design team to target reduced footprint without project specific LCA.
Energy and Water Use	Provide a pool blanket	Pool blanket design to be incorporated within planning.
Energy Source	Avoid fossil fuels on site,	Compliant, design intent is for fossil fuel free site.
Renewable Energy	Operate common areas as net energy neutral	Not required, less than 30 bedrooms. Substantial solar array provided in any event.
Water Use	Provide water efficient appliances	Compliant, refer target performance within this report.
Irrigation Efficiency	Waterwise landscaping	Compliant, refer landscape report. Irrigation to be water efficient, drip style. Mostly native, drought resilient planting.
Active Living Support	End of trip facilities.	Not applicable, no commercial use on site.
Bike Storage	Provide 1 bike store per dwelling	Bike parking areas to be allocated with development, likely the ground floor car park.
EV Parking	Provide BCA 2022 compliance EV provisions	Compliant, refer section 5.1 following.
Car Share	Provide a car share vehicle and bay	Not applicable, less than 10 dwellings
NatHERS	Exceed BCA by 0.5 Stars	Compliant, target rating of 7.5 Star NatHERS average
Water Management	Provide water reuse system	No water reuse system is proposed for this development.

2. Healthy Living

The project is intended to support occupants keen to be a part of a healthy and active community and is well located for walkable access to coffee and restaurants, groceries, newsagents and health services.

The design includes connection to the existing pedestrian infrastructure, with pedestrian routes maintained from the street the building entry and through the car park.



Above: Current street view (Google Maps)

Other design features to improve occupant health outcomes through the design features detailed in the following sections.

2.1 Low Toxicity Materials

Internal finishes and joinery will prioritize low toxicity through minimisation of VOCs and formaldehyde:

- Walls and ceiling paints will target Ultra Low VOC compliance (<5g/l)
- Adhesives, sealants, trim paints, primers and sealers will comply with Green Star best practice benchmarks.
- All internal joinery will be certified as low (E0 or E1) formaldehyde

At building license stage the design team will provide a performance specification for all relevant products and, where selected, will ensure materials meet the performance requirements above.

2.2 High Performance Envelope

The building design will prioritize the protection of glazing through the use of deep balconies to protect from low angle summer afternoon sun.

Initial modelling of apartments has been undertaken, targeting an 8 Star outcome:

Construction Element	Insulation / Performance
External Walls	R2.7 Insulation 90mm
Floors	R1.9 Insulation – 40mm Board + Reflective Layer to car park
Ceiling	R3.0 Bulk, where exposed above
Concrete Roof	Light finish or planting, 80mm R3.8 board below
Metal Roof	R1.5, Reflective insulation under roof sheets
Sliding Doors	High performance double glazing and frames.
Awning Windows	

Overall, the project intends to target a 7.5 Star NatHERS average rating for the apartments. This is half a star better than BCA minimum requirements and improves thermal comfort and energy efficiency. At building license, the Section J compliance report will demonstrate the performance improvement.

2.3 Natural Ventilation

The design will ensure that all apartments meet or exceed the ventilation requirements of AS 1668.4 for natural ventilation.

The building orientation will assist with access to prevailing breezes, with most units having access to dual aspect natural ventilation.

At building license stage the mechanical designer will certify appropriate ventilation provisions for all habitable spaces.

In addition to the above, the project also intends to facilitate active transport through the integration with local footpaths and pedestrian infrastructure.

2.4 Acoustic Performance

The design is targeting best practice sustainability outcomes for the site through management of noise levels and exceeding BCA requirements for noise separation in walls and floors.

Compliance will be demonstrated through acoustic reporting at Building License, with verification of some performance elements on site at practical completion.

3. Minimised Footprint

The Greenhouse Effect and global warming has been identified as the most significant environmental challenge of our times. Increasingly corporations and governments are embracing the need to act through setting performance goals and penalising carbon inefficiency with increasing utility costs.

The Alexander Road project team are aware of the need for projects to act responsibly with their ecological footprint both initially and in operation through life cycle assessment and building design. Best practise design can successfully reduce energy and water consumption, divert waste from landfill and minimise refrigerant carbon footprint. Site selection can minimise impact on existing fauna and offer the potential for developments to act as a restorative force.

Many of the features in other categories contribute to improved building carbon efficiency, including natural ventilation, an efficient envelope and water efficiency measures. Sustainable transport options also reduce the carbon footprint of building occupants and visitors.

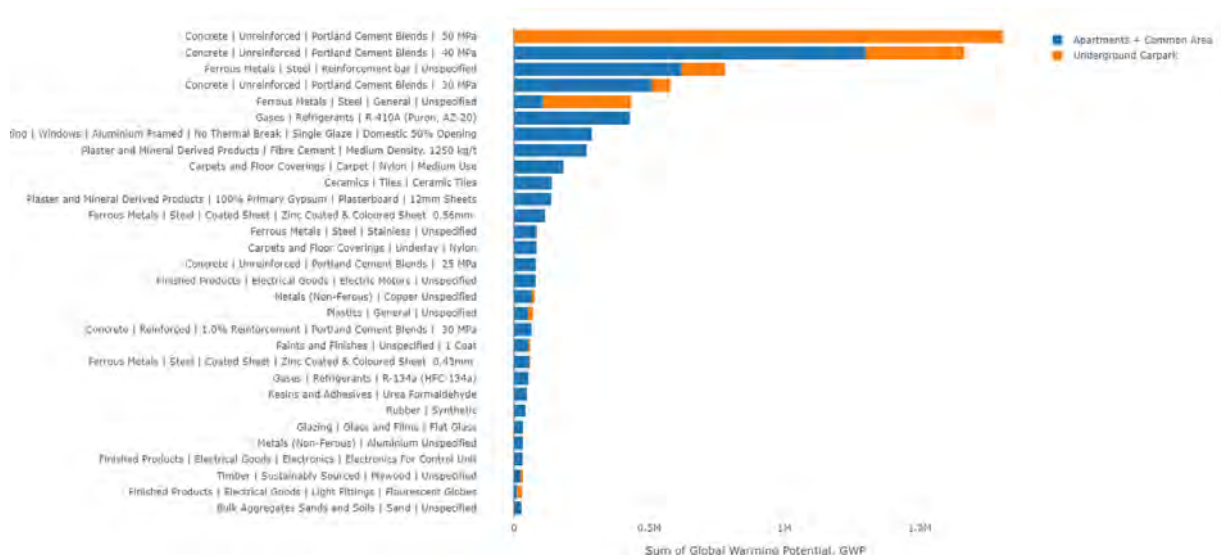
The following additional design features are intended to delivery embodied and operational carbon footprint improvements:

3.1 Considered Development

The building will be within a well established community, on a brown field site, limiting impacts to local ecology and providing the potential to improve biodiversity with native planting.

3.2 Embodied Energy

The proposed design will be targeting a reduction in upfront carbon through material selections, and design rationalisation. The image below shows the typical breakdown and embodied of different construction materials for apartment buildings over a project lifetime.



Assessment of similar style developments shows that concrete and steel make up the majority of the building's upfront carbon emissions. The design team will be reviewing the potential for the use of a post-tensioned slab, replacement of Portland cement and selection of sustainably sourced steel to assist in minimising this footprint. In addition, window frames will consider the use

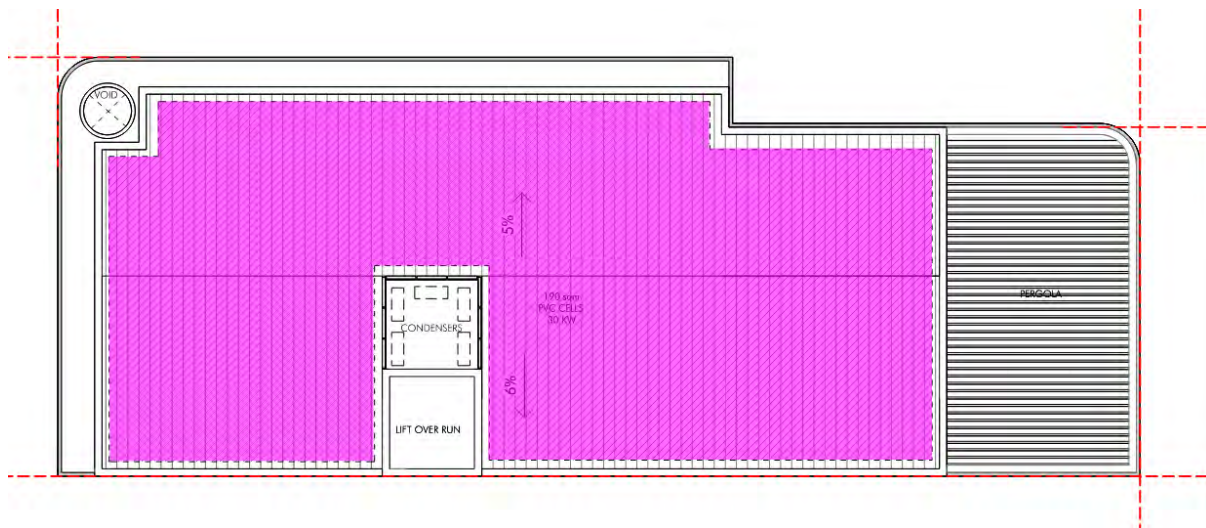
of LocAl Capral frames – achieving a reduction of around 50% in embodied carbon emissions in window frames.

In addition to aluminium, concrete and steel, refrigerants can also play a major role in building embodied and upfront carbon. The mechanical design will seek to utilise R32 refrigerant in lieu of the more common blends, with systems aiming to be kept small and local to limit capacity and refrigerant in pipework – a reduction of around 60% in refrigerant carbon footprint.

The project team will review finalised design against similar building types and provide comment at completion.

3.3 Renewable Energy

The design intent is to include potential for roof mounted solar panels for each building, targeting around 6kW per dwelling. The system is expected to produce more than 50MWh of electricity annually and should offset more than 75% of the annual electrical demand of the building.



Note the image shows sufficient area for ~40kW peak output array.

In addition, the design will include battery systems to improve utilisation of solar on site and provide a measure of grid resilience in the event of instability.

3.4 Overall Performance

Overall, the project team are seeking to demonstrate at least a 30% performance improvement in operational energy and 10% water reduction against typical performance. Design features above will contribute to this target:

- | | |
|-------------------------------|--|
| • Improved building envelope: | Half star margin on Section J Compliance |
| • Efficient Services | 10% Margin on Section J Compliance |
| • Low water use | WELS rated fittings and appliances |
| • Controls ventilation | Automated lighting and car park |

4. Future Resilience

FCDS has undertaken a base review of the project and the potential likely impacts of climate change on the site, based on the following likely impacts of climate change in South West WA:

Variable	Current	Predicted	Expected Change	Possible Range
Annual Average Temperature (°C)	25.8 °C	29.3 °C	+3.5°C	28.4 – 30°C
Number of days over 35°C	28	63	+35	50-72
Annual Average Rainfall (mm)	851.7mm	698mm	-18%	536 – 809mm
Summer	40.5mm	38.5mm	-5%	27.9 – 55.1mm
Autumn	144.1mm	135.4mm	-6%	98 – 162.8mm
Winter	398.2mm	282.7mm	-29%	223 – 338.5mm
Spring	147.5mm	94.4mm	-36%	60.5 – 126.8mm
Annual Average Potential Evaporation	1800mm	1836mm	+2%	1818-1872mm
Annual Relative Humidity	55%	54.4%	-0.6%	53.7 – 55%

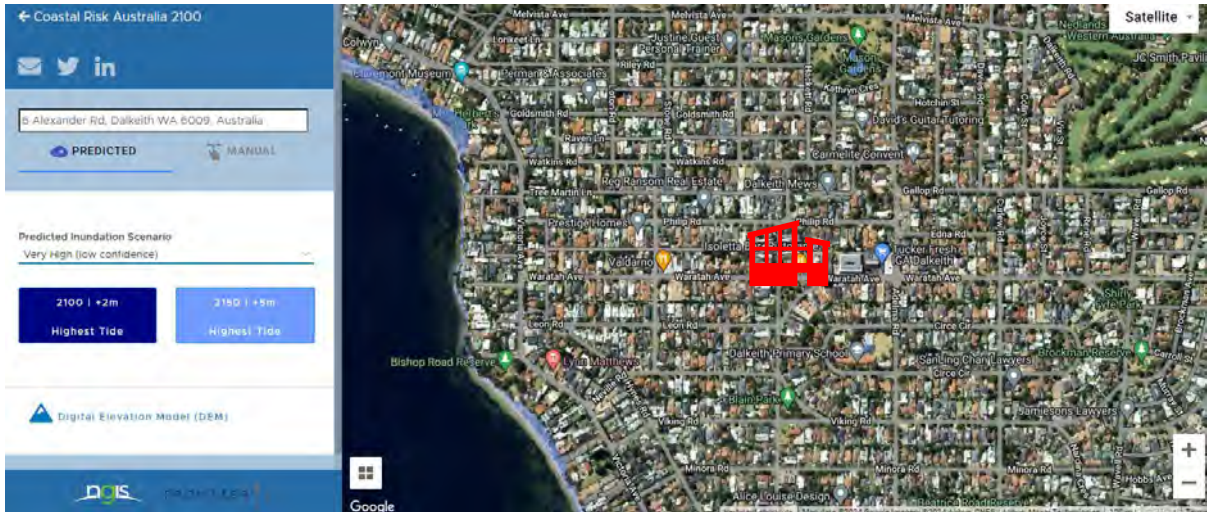
4.1 Risk Review

The development site is not considered to be at significant risk of climate impacts, as confirmed through the GBCA climate change risk assessment checklist:

Checklist	Criteria Response	Future Climate Exposure Reviewed?	Risk to the project identified?	Risk treatment identified?	Design / Operational Measure
Has the project area been previously impacted by extreme climate events?	No	Yes	No	No	NA
Is the project located in a cyclone zone?	No	Yes	No	No	NA
Is the project located in or adjacent to a bushfire prone area?	No	Yes	No	No	NA
Is the project located in or adjacent to a flood prone area?	No	Yes	No	No	Local high topological point. Access at various points and levels.
Is the project located at or adjacent to the coastline or tidally influenced waterway?	No	Yes	No	No	NA
Will the project accommodate occupants vulnerable to the impacts of climate extremes?	No	Yes	No	No	Improved envelope. Efficient Air Conditioning Onsite generation.

4.1.1 Flood Risk – Very High Inundation Scenario

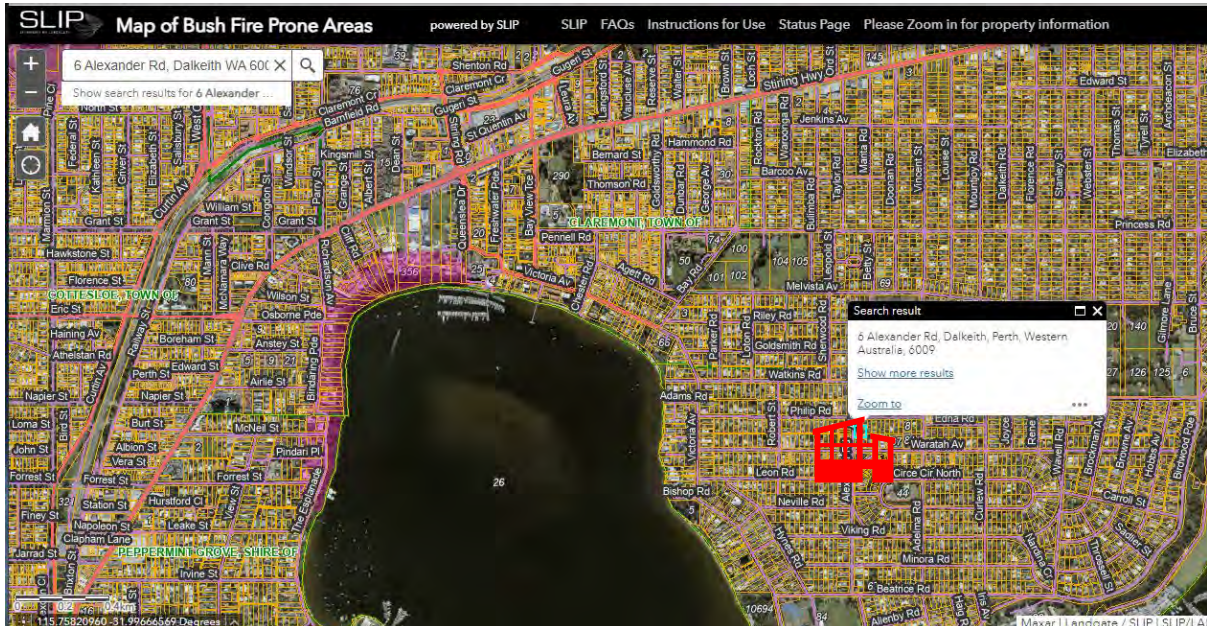
Coastal risk maps indicate that the site is well above inundation even under a 5m high sea level rise.



Above: Coastal Risk Australia

4.1.2 Bushfire Prone Areas

Bushfire zone maps show that the site well away from areas considered bushfire prone.



Above: Bushfire Prone Areas - DFES

4.2 Design Response

Notwithstanding the low risk rating, the design team have included the following features to mitigate risks and provide an improved outcome for occupants and the local community:

Climate Change Impact	Risk	Proposed Response
Increased temperatures lead to increased bushfire risk and intensity.	Moderate due to current classification of site.	Multiple orientations for natural ventilation. Provide filters on air conditioning units to mitigate bushfire smoke infiltrating into apartments.
Rising sea levels and increased flood risk.	Very low	Ensure structure can adapt to changing water levels.
Reduced rainfall	Increasing requirement for irrigation, increased cost of scheme water.	Utilise smart irrigation, including moisture detection and prioritise drought tolerant planting. Utilise native planting where possible.
Increased temperatures lead to increased reliance on air conditioning.	Building is unable to provide comfortable environment for extended periods.	Provide high efficiency air conditioning systems with automatic controls. Upgrade building envelope in excess of BCA minimum requirements. Reduce urban heat island effect.
Increased temperatures lead to increased power demand.	Operating cost increases as electrical prices increase. Power security becomes questionable.	Good control systems and energy efficient design. Potential for solar photovoltaics for renewable generation.

Further specific design features are discussed in the following sections.

4.2.1 Water Efficient Fittings

Where provided, fittings and appliances will target high water efficiency, generally within 1 Star of the best available WELS rating for each fitting type:

Fixture Type	Minimum WELS rating	Maximum Flow Allowable
Taps	5 stars	4.5-6 L/min
Urinals	5 stars	1.0 L / Flush + Smart demand flush device
Toilets	4 stars	<3.5 L average flush <4.7L full flush, <3.2 half flush
Showers	3 stars	6.0 – 7.5 L/min
Clothes Washing Machines	4 stars	Size Dependent
Dishwashers	5 stars	Size Dependent

Low flow fittings save water, operational costs, carbon emissions and energy use when they utilise hot water.

4.2.2 Native or Drought Tolerant Planting

The design team will be including water efficient planting and irrigations to target a 30% reduction in water use against Green Star benchmarks and to minimise operational costs and carbon emissions.

Building license documentation will include these elements.

4.2.3 Urban Heat Island Mitigation

The project team intend to meet Green Star best practice guidelines for urban heat island by maintaining 75% of the site as either vegetation, shaded areas or elements with a 3-year SRI of 64 or greater.

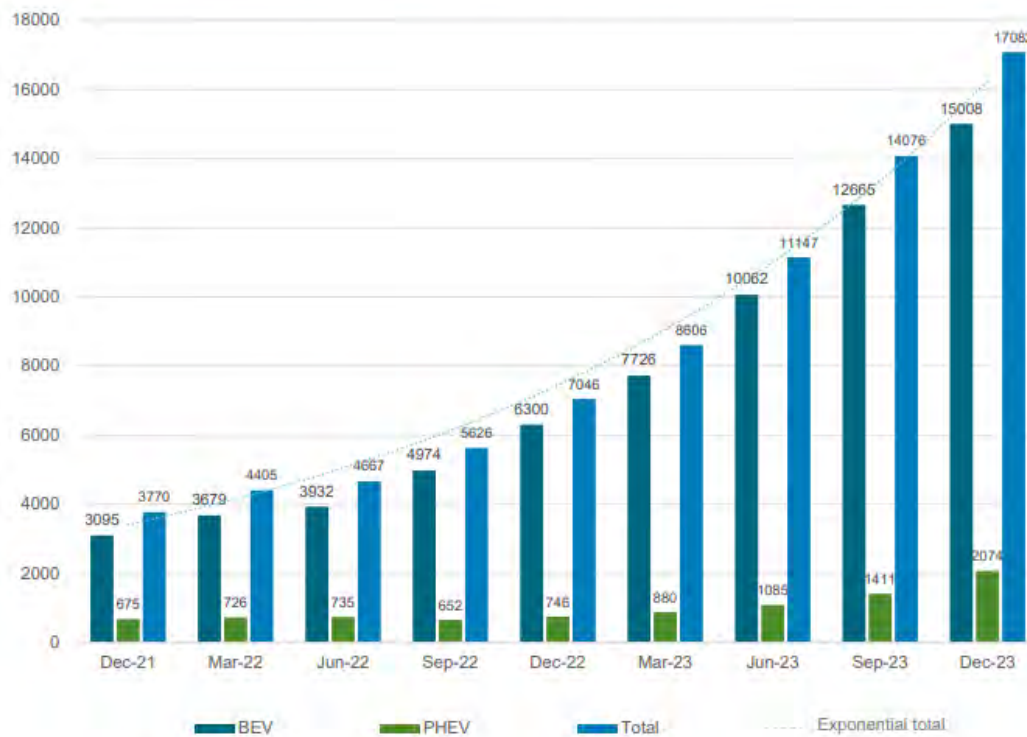
Building license plans and finish selections will demonstrate this performance outcome.

5. Sustainable Transport

Increasingly, Perth drivers are transitioning away from tradition Internal Combustion Engine vehicles – with the current rate of 6.6% of new vehicles (2023) set to be ~10% by 2025 and more than 50% in 2030 (<https://www.watoday.com.au/politics/western-australia/future-planning-is-this-the-year-petrol-will-disappear-from-wa-20231218-p5esbe.html>).

The graph below shows the uptake in electric vehicles since the end of 2021 (Western Australian Electric Vehicle Analysis Summary 2023 Q4 – Department of Transport):

Figure 1: Cumulative electric vehicle data December 2021 – December 2023



Recognising this shift, the Alexander Road design will be seeking to facilitate alternate transport options, as described in the following sections.

5.1 Electric Vehicle Facilitation

The design will include a dedicated EV distribution board for the carpark, sized to cater for ~5 EV chargers. The design will include key infrastructure provisions, including:

- Load management systems to deliver at least 12kWh per bay between 11PM and 7AM daily
- Space to allow for electricity metering, as part of the embedded network provision
- Cable containment consideration to provide chargers to all bays
- Potential for use of 7kW / 32A type two chargers
- Chargers for 10% of bays at opening

Electrical design documentation will show these provisions at building license stage.

6. Feature Summary

Many of the features proposed above offer performance improvements across a range of themes. The following table outlines the proposed features and the demonstration pathway – aligning with national best practice benchmarks – selected for the project team.

Design Feature	Benchmark Performance	Verification Method	Healthy Living	Building Footprint	Climate Resilience	Sustainable Transport
EV Facilitation	Dedicated EV DB, Capacity to service 5 bays.	Design documents		X		X
Brown Field Development	100% Site previously developed, improved ecology score	Design documents	X	X	X	X
Efficient Envelope	Improvement over BCA 2022	Building License Report	X	X	X	
Acoustic Performance	Design report and performance verification at completion.	Report	X			
Native Planting	Demonstrate 30% reduction in water use against Green Star Standards	Report at Building License		X	X	
Fossil Fuel Free Site	No fossil fuels for domestic hot water, cooking or heating	Design documents		X	X	
Envelope Pressure Test	Test 10% of building apartment envelope	Report at practical completion.	X	X	X	
Renewable Energy	Potential for Solar PV Panels and onsite storage	Performance Specification		X	X	
Urban Heat Island	Achieve 75% of site with a 3-year SRI of 64 or greater	Design documents	X	X	X	
Waste Plan	Waste store to allow for three streams	Design documents		X		
Embodied Energy	Target 20% reduction in life cycle footprint, as demonstrated with comparison to similar buildings	Memo at Building Licence		X	X	
Water Efficiency	WELS rated fittings – Within 1 Star of best available	Design documents		X	X	
Low Toxicity Materials	Utilise low VOC and formaldehyde internal finishes, adhesives and sealants.	Material review at Building License	X			
Low Carbon Materials	Utilise R32 Refrigerant and improved materials to improve upfront carbon by 20%	LCA Report at Building License		X	X	
Natural Ventilation	Comply with AS 1668.4 for natural ventilation to all apartments	Report at Building License	X	X	X	
Operational Footprint	20% reduction in operational energy through envelope and services. Facilitate NABERS certification in future.	Report at Building License		X	X	



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PART C – OTHER BUSINESS

- 1. State Administrative Tribunal Applications and Supreme Court Appeals**
- 2. Meeting Closure**