

Regional Development Assessment Panel Agenda

Meeting Date and Time: Tuesday, 14 July 2026; 9:30am
Meeting Number: RDAP/78
Meeting Venue: 140 William Street, Perth

A live stream will be available at the time of the meeting, via the following link:
[RDAP/78 - 14 July 2026 - City of Bunbury - Shire of Leonora](#)

PART A – INTRODUCTION

1. Opening of Meeting, Welcome and Acknowledgement
2. Apologies
3. Noting of Minutes

PART B – CITY OF BUNBURY

1. Declarations of Due Consideration
2. Disclosure of Interests
3. Form 1 DAP Applications
 - 3.1 Lot 260 (No.390) Willinge Drive, Glen Iris – Warehouse & hardstand – DAP/25/02928
4. Form 2 DAP Applications
5. Section 31 SAT Reconsiderations

PART C – SHIRE OF LEONORA

1. Declarations of Due Consideration
2. Disclosure of Interests
3. Form 1 DAP Applications
 - 3.1 Lot 12 & Lot 13 (No. 70 & 64) Kurrajong Street, Leonora – Workforce Accommodation – DAP/26/03040
4. Form 2 DAP Applications
5. Section 31 SAT Reconsiderations

PART D – OTHER BUSINESS

1. State Administrative Tribunal Applications and Supreme Court Appeals
2. Meeting Closure

Please note, presentations for each item will be invited prior to the items noted on the agenda and the presentation details will be contained within the related information documentation

DAP Members
Eugene Koltasz (Presiding Member)
Dale Page (Deputy Presiding Member)
Andrew Howe
Cr Todd Brown (Part B – City of Bunbury)
Cr Cheryl Kozisek (Part B – City of Bunbury)

DAP Secretariat
Shanara Wijethunga
Ashlee Kelly

PART A – INTRODUCTION

- 1. Opening of Meeting, Welcome and Acknowledgement**
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PART B – CITY OF BUNBURY

1. Declarations of Due Consideration

2. Disclosure of Interests

3. Form 1 DAP Applications

- 3.1 Lot 260 (No.390) Willinge Drive, Glen Iris – Warehouse & hardstand –
DAP/25/02928

4. Form 2 DAP Applications

Nil

5. Section 31 SAT Reconsiderations

Nil

PART B – Item 3.1 – LOT 260 (#390) WILLINGE DRIVE GLEN IRIS – WAREHOUSE & HARDSTAND

Form 1 – Responsible Authority Report (Regulation 12)

DAP Name:	Regional DAP
Local Government Area:	City of Bunbury
Applicant:	Taylor Burrell Barnett
Owner:	Giacci Holdings Pty Ltd
Value of Development:	\$10 million <input type="checkbox"/> Mandatory (Regulation 5) <input checked="" type="checkbox"/> Opt In (Regulation 6)
Responsible Authority:	City of Bunbury
Authorising Officer:	Alan Ferris, Chief Executive Officer
LG Reference:	DA/5/2025/119/1
DAP File No:	DAP/25/02928
Application Received Date:	24 June 2025
Report Due Date:	2 July 2026
Application Statutory Process Timeframe:	90 Days
Attachment(s):	<ol style="list-style-type: none"> 1. Location Plan and Local Planning Scheme Plan 2. Site Photos 3. Planning Report and Further Information Response 4. Final Development Plans and Landscaping Plans 5. Transport Impact Statement 6. Stormwater Management Plan 7. Schedule of Submissions 8. Wimbridge Precinct Structure Plan

Responsible Authority Recommendation

That the Regional DAP resolves to:

1. **Accept** that the DAP Application reference DAP/25/02928 is appropriate for consideration as a “Warehouse/Storage” land use and compatible with the objectives of the zoning table in accordance with Clause 16 of the City of Bunbury Local Planning Scheme No.8.
2. **Approve** DAP Application reference DAP/25/02928 and accompanying plans in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of the City of Bunbury Local Planning Scheme No. 8 and Greater Bunbury Region Scheme, subject to the following conditions:

Conditions

1. This decision constitutes development approval only and is valid for a period of four years from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect.
2. All development shall be in accordance with the submitted plans (by TBB Planning, titled 'Qube Picton PLP – Bulk Storage Shed & Temp Hardstand Proposal', project number T24072, Revision D), with the following exceptions:
 - a. any additional works required by Conditions 7, 9, 10, 12 and 14 relating to landscaping, the ceding/construction of the road reserve, water management and acoustic compliance.

Amended plans consistent with this condition must be submitted to the City for approval prior to commencement of works

3. All works required to satisfy a condition of this approval are required to be installed / constructed and maintained in accordance with the approved plans and conditions of approval for the life of the development.
4. At all times, the development the subject of this development approval must comply with the definition of 'Warehouse / Storage' as contained in Part 6 of the City of Bunbury Local Planning Scheme No. 8.
5. The retaining walls, including footings and associated structures, shall be constructed wholly within the lot boundaries, and finished to a quality finish and professional standard, to the satisfaction of the City of Bunbury.
6. A Construction Management Plan shall be submitted to and approved by the City prior to issuing a building permit. The management plan shall include details regarding mitigation measures to address impacts associated with construction works and shall be prepared to the specification and satisfaction of the City. The construction works shall be undertaken in accordance with the approved Construction Management Plan.
7. Before the development is occupied, the landscaping plan is to be amended to provide an additional shade tree for the parking bay area with a clean trunk to 2m. The landscaping plan must be planted, established, reticulated, and thereafter maintained to the satisfaction of the City of Bunbury.
8. Before the development is occupied, the recommendations of the final approved Transport Impact Statement must be constructed and implemented to the satisfaction of the City of Bunbury.

Access and parking arrangements must be designed in accordance with Australian Standards, Austroads Guidelines and City of Bunbury requirements. Detailed design plans must be submitted to the City of Bunbury for approval prior to construction commencing.

9. Prior to the issuing of a building permit, engineering drawings and specifications for the construction of the Local Road reserve marked on the approved plan(s) are to be submitted for the approval of the City of Bunbury. The road is to be suitable for industrial heavy vehicle access and the engineering drawings are to be in accordance with Australian Standards,

Austrroads Guidelines, Main Roads and City of Bunbury requirements. Before the development is occupied, the 'Local Road' reserve is to be constructed in accordance with the approved plans to the satisfaction of the City of Bunbury

10. Regarding Condition 9, before the development is occupied the Local Road reserve marked on the approved plans(s) must be ceded free of cost to the Crown.
11. Before the development is occupied, any alterations, relocation, or damage of existing infrastructure within the road reserve must be completed and reinstated to the specification and satisfaction of the City of Bunbury.
12. Prior to the issuing of a building permit, an amended Stormwater and Drainage Management Plan, prepared in accordance with the Department for Water's Stormwater Management Manual must be submitted for the approval of the City of Bunbury, in consultation with Main Roads Western Australia. The amended stormwater management plan is to address the following items:
 - a. For the channel to the North property boundary –
 - i. Extension of the channel on the northern boundary for drainage onto the Picton Deviation to maintain the existing flow path
 - ii. A geometry of the channel demonstrating that any earthworks or excavation, as well as access paths are kept within the property boundary, to avoid constructing private infrastructure within the future road reserve.
 - iii. Detailed design drawings demonstrating typical channel section/s, earthworks extents, and construction specification. As the proposed channel will be quite flat, it will require some silt trapping capacity.
 - b. Removal of all stormwater detention infrastructure from the ceded road reserve and demonstrating the displaced stormwater is appropriately contained and managed within private property boundaries.
 - c. Verify site condition will cater for stormwater management including:
 - i. Perform soil testing on existing site conditions to verify the assumed 2m/day infiltration rate exists.
 - ii. Verifying the nominated ground water levels shown on the Stormwater Management Plan are the seasonal maximum
 - d. Details of the internal subsoil drainage layout demonstrating the appropriate control of ground water levels, noting potential mounding of groundwater may occur at building locations.
13. Before the development is occupied, the recommendations of the approved Stormwater and Drainage Management Plan must be constructed and implemented to the satisfaction of the City of Bunbury.
14. Prior to the issuing of a building permit, a report must be prepared by a qualified acoustic engineer, who is a member of the Australian Acoustical Society, detailing the possible noise emissions from the subject development and comparing them to relevant noise criteria.

Should the predicted / measured results not comply with the criteria, the report must detail appropriate actions and mitigation measures to be undertaken to ensure that noise emissions do not contravene the provisions of the *Environmental Protection Act 1986* and the *Environmental Protection (Noise) Regulations 1997*.

15. Before the development is occupied, the outcomes and recommendations of the acoustic report provided under Condition 14 must be implemented on site to the satisfaction of the City of Bunbury.
16. Prior to the issuing of a building permit, a dust management plan prepared by a suitably qualified practitioner must be submitted for the approval of the City of Bunbury. The dust management plan must address how dust emissions and sand drift will be controlled on-site from all roads, internal storage areas and external storage areas. Before the development is occupied, the recommendations of the approved dust management plan must be implemented to the satisfaction of the City of Bunbury.
17. A notification, pursuant to Section 70A of the *Transfer of Land Act 1893* is to be placed on the certificates of title of the lot. Notice of this notification is to be included on the diagram or plan of survey (deposited plan). The notification is to state as follows:

“The lot is located within 500m of an abattoir and has the potential to be affected by odour and noise.”

Before the development is occupied, the City is to be provided a copy of the revised certificate of title.

Advice Notes

1. This is a development approval only. The applicant/owner is advised that it is their responsibility to ensure that the proposed development complies with all other applicable legislation, local laws and/or license requirements that may relate to the development. Works such as de-watering, native vegetation clearing or working near existing infrastructure may require separate approvals from relevant private or government agencies. Licencing requirements may also apply under the Environmental Protection Act 1986.
2. The development is subject to the Building Act 2011, Building Regulations 2012 and Building Code of Australia. A separate building permit must be granted before the development commences, where offences occur statutory penalties apply. The applicant/owner is advised to liaise further with the City of Bunbury on (08) 9792 7000.
3. Regarding Condition 12, stormwater management measures on-site may be able to be reduced through a contribution to the downstream drainage outfall solution outlined in the Precinct Local Water Management Strategy.
4. Regarding Condition 12, measures should be implemented within the detention basins to control and limit mosquito breeding. For further information regarding this, please contact the City of Bunbury's Health Services.
5. Please be aware that the subject land is located in a high groundwater table area. Treatments such as subsoil drainage may be required to prevent any increases to groundwater table levels on adjoining properties.
6. Prior to works commencing, the applicant is required to obtain a works and thoroughfare permit for any works within Council land other than a crossover.

A permit application form can be obtained on the City's website www.bunbury.wa.gov.au

7. Construction works are to comply with the relevant City of Bunbury standard drawings and specifications, Australian Standards and/or IPWEA Subdivision Guidelines.
8. The development the subject of this development approval is required to comply with the requirements of the Department of Mines, Industry Regulation and Safety (Resources Safety Section) in relation to the storage of dangerous goods. Further information may be found at www.dmp.wa.gov.au

Details: outline of development application

Region Scheme	Greater Bunbury Region Scheme
Region Scheme - Zone/Reserve	Urban
Local Planning Scheme	City of Bunbury Local Planning Scheme No. 8
Local Planning Scheme - Zone/Reserve	Service Commercial
Structure Plan/Precinct Plan	Wimbridge Precinct Structure Plan
Structure Plan/Precinct Plan - Land Use Designation	'Service Commercial' and 'Local Road'
Use Class and permissibility:	'P' use
Lot Size:	9.0285 ha
Existing Land Use:	Vacant section of lot Rest of lot, Warehouse / Storage
State Heritage Register	No
Local Heritage	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Heritage List <input type="checkbox"/> Heritage Area
Design Review	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other
Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

Development approval is sought for a proposed 'Warehouse / Storage' facility and a hardstand area. The proposed development will comprise of a 7940m² warehouse building, 5290m² external storage bunkers, 18740m² hardstand area and associated vehicle manoeuvring areas for use by QUBE. The new development will utilise an existing access off Willinge Drive through an internal road network which is partially within a designated 'Local Road' reserve. The warehouse building, storage bunkers and hardstand area will be utilised as a bagging/storage area for mineral sands. It is

expected that on average the development will be serviced by 9 trucks per day, with this extending to 12 trucks per day during peak periods.

Proposed Land Use	'Warehouse / Storage'
Proposed Net Lettable Area	7940m ²
Proposed No. Storeys	1
Proposed No. Dwellings	N/A

A location plan (aerial), and site photos are provided at Attachment 1 and 2. Accompanying planning report, development plans and technical reports are provided in Attachments 3 to 6.

Background:

Lot 260 (#390) Willinge Drive is 9.028ha and currently accommodates four bulk storage warehouses operated by QUBE on the southern portion of the site. The subject northern portion of the site is currently vacant. The landowner also owns neighbouring properties at Lot 101 (#16) The Avenue, Lot 108 (#18) Yookson Road and Lot 2 (#20) Yookson Road, with QUBE operations occurring across all these sites.

The subject lot is zoned 'Service Commercial' to the North, 'Light Industry' to the South and contains reserves for 'Public Purposes (Drainage)' and 'Local Roads'. In addition, portions of the subject lot fall within Special Control Area Abattoir and Abattoir Notification Area. The proposed development is located within the 'Service Commercial' zone and partially includes work in a 'Local Roads' section. Refer to Attachment 1.

Existing development and previous development approvals for the site are predominantly within the south of the lot which is zoned 'Light Industry'. The subject portion of the site zoned 'Service Commercial' is largely vacant however contains some fill, which was authorised in accordance with a 2019 development approval and some regrowth vegetation.

City officers provided preliminary advice to the applicant in 2024 advising relevant planning framework information, including the requirements of the Wimbridge Precinct Structure Plan and associated Local Water Management Strategy (LWMS).

Legislation and Policy:

- *Planning and Development Act 2005;*
- *Planning and Development Act (Development Assessment Panels) Regulations 2011;*
- *Planning and Development (Local Planning Schemes) Regulations 2015 (the Regulations);*
- *Greater Bunbury Region Scheme (GBRS); and*
- *City of Bunbury Local Planning Scheme No. 8 (LPS8 or the Scheme).*

State Government Policies

- State Planning policy 3.6 – Infrastructure Contributions
- State Planning Policy 7.0 – Design of the Built Environment.
- Development Control Policy 5.1 – Regional Roads (Vehicular Access)

Structure Plans/Activity Centre Plans

- Wimbridge Precinct Structure Plan

Planning Strategy / Local Policies

- City of Bunbury Local Planning Strategy;
- Local Planning Policy 3 – Zone Development Standards;
- Local Planning Policy 3.1 – Access & Parking for Pedestrians Bicycles and Pedestrians;
- Local Planning Policy – Development in Flood Affected Areas; and
- Local Planning Policy No.48 – Landscaping.

Consultation:

Public Consultation

Public consultation was undertaken to surrounding landowners as the proposal:

- includes a building height variation; and
- includes varying the Wimbridge Precinct Structure Plan by proposing to contain all stormwater associated with a 1 in 10 and 1 in 100 stormwater event onsite, rather than contributing to the downstream drainage stormwater solution. This stormwater management is not strictly consistent with the relevant LWMS.

One submission was received objecting to the proposal with the following issues raised:

Issue Raised	Officer comments
Proposed stormwater management plan is inconsistent with the LWMS in not contributing to a downfall stormwater solution to the area.	Since public consultation, the applicant has revised the stormwater management plan based on advice from Department of Water and Environmental Regulation (DWER), Main Roads WA (MRWA) and the City. It is acknowledged that the stormwater management plan is not strictly consistent with the LWMS due to the proposal seeking to contain all stormwater associated with a 1 in 10 and 1 in 100 stormwater event onsite, in lieu of contributing to the downstream drainage stormwater solution prescribed in the LWMS. This is further discussed below in the stormwater section.
Proposed discharge outlet should not discharge onto neighbouring properties. All stormwater should be maintained within lot boundaries	It is noted that there is an existing discharge outlet present within the designated 'Local Road' reserve. This outlet is an existing asset that partially discharges water from the landowners existing developed area and partially from roads within the area. The subject development proposal does not propose to use this discharge outlet or propose to contribute additional stormwater discharge onto surrounding lots. All stormwater associated with this development is proposed to be maintained within lot boundaries.

<p>Concern with siteworks outside lot boundaries</p>	<p>The plans have been revised to ensure all siteworks are within the development property boundaries and are maintained by retaining walls on the lot boundaries. A standard condition is recommended to ensure that these retaining walls are located entirely within the property boundaries.</p>
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Referrals/consultation with Government/Service Agencies

The City of Bunbury referred the application to the following government agencies and service providers:

- Main Roads Western Australia (MRWA);
- Department of Water and Environmental Regulation (DWER); and
- Department of Planning, Lands and Heritage (DPLH) – Land Use Planning.
- Department of Planning, Lands and Heritage (DPLH) – Aboriginal Heritage.

The proposal was referred to DPLH – Land Use Planning due to approval being required under the Greater Bunbury Region Scheme (GBRS) as the subject lot abuts a ‘Primary Regional Road’ reserve (Willinge Drive). DPLH advised that they did not object to the application subject to consideration of the proposal against the Wimbridge Structure Plan, the recommendations of DWER on drainage matters, the recommendations of MRWA, and consideration to the ultimate road layout envisaged by the Wimbridge Structure Plan. DPLH provided clarity that the application and Responsible Authority Report may be addressed under delegation provided the City accepts the recommendation and advice of MRWA.

Upon referral to MRWA, it was initially advised that MRWA objected to the proposal based on works occurring within the adjoining proposed ‘Primary Regional Road’ reserve (future Picton Deviation road) and due to potential insufficiencies of the stormwater infrastructure. The applicant has since revised the plans to ensure that all development associated with the proposal is contained within the subject lot boundaries and have ensured that the stormwater related to the development does not discharge into the identified ‘Primary Regional Road’ to the north. MRWA have subsequently removed their objection subject to a condition requiring the stormwater management plan be amended to address certain technical details.

DWER raised concerns regarding the stormwater catchment assessment provided by the applicant and raised broader concerns regarding approving the application in lieu of a contribution to the downstream drainage stormwater solution as outlined under the LWMS associated with the endorsed Wimbridge Structure Plan. Comments regarding the sufficiency of the stormwater management measures have been addressed below. DWER have been provided with the amended stormwater management information and have confirmed they support the proposal subject to some technical modifications/clarifications to the stormwater management plan. This can be addressed through the above noted condition.

The DPLH Aboriginal Cultural Heritage Branch advised that the subject site does not intersect with any known Aboriginal heritage Places or Registered Sites.

Design Review Panel Advice

Not Applicable

Planning Assessment:

The proposal has been assessed against the relevant legislative requirements of the Scheme, State and Local Planning Policies, outlined in the Legislation and Policy section of this report. The following matters have been identified as key considerations for the determination of this application.

Wimbridge Structure Plan 2015

The City has given due regard to the Wimbridge Structure Plan as required under the *Planning and Development (Local Planning Schemes) Regulations 2015*.

Structure Plan Map

The proposal is included within an area identified on the map as 'Mixed Business' and 'Indicative Access Road' (Attachment 8). This 'Mixed Business' area was originally intended to be developed into smaller 'Mixed Business' lots with internal access provided by the 'Indicative Access Road'. Development within this area has not progressed in this manner with instead the area dominated by larger 'Warehouse / Storage' uses.

The use of 'Warehouse / Storage' is considered consistent with the intent of the 'Mixed Business' designation identified in the structure plan, which is now zoned 'Service Commercial' under LPS8. In terms of the development location, whilst it encroaches over the 'Indicative Access Road', this is considered supportable as the proposal seeks to develop the lot consistently with the surrounding area and will provide access from a designated 'Local Road'.

Structure Plan Provisions

An assessment against provisions 1-20 of the structure plan provisions can be found below. An assessment has not been provided against provisions 21-26 as these relate to a different lot.

Note	Requirement	Assessment
1.	No residential or other sensitive uses are to be located on land nominated within the abattoir and Water Treatment Special Control Area.	Not applicable to this application. This development does not propose residential or sensitive land uses.
2.	Land use proposals within Special Control Area 3 - Glen Iris Service Corridor Buffer Area, as identified in the GBRS, shall be assessed against the following criteria outlined in Division 4 of the Greater Bunbury Region Scheme - the purpose of the area: - planning requirements: and - consultation requirements.	Not applicable to this application. The proposed development not located within Special Control Area 3.
3.	The Staged implementation of the intersection of the proposed north	Access for the development is achieved from existing access from

	<p>south subdivision road with the future Picton Deviation is to be negotiated by the developer with Main Roads WA. The subdivision road is allowed to be a continuous road until the Deviation occurs at which time arrangement will apply unless agreed otherwise with Main Roads.</p> <p>The southern section of the subdivision road shall be connected to the Picton Deviation via a left in only (from the Highway) arrangement. The northern section of the subdivision road shall be connected to the Picton Deviation via a full tee junction arrangement.</p>	<p>Willinge Drive. Development does not propose access from the future Picton Deviation and does not encroach on this future road reserve</p>
4.	<p>All development shall incorporate storm water management systems in accordance with the Drainage Management Strategy and associated Local Water Management Strategy prepared by Thompson McRobert Edgeloe Group. No temporary drainage structures will be approved. If any drainage management requirements cross property boundaries, the developer shall negotiate a resolution to the satisfaction of the City of Bunbury prior to any subdivision or development approvals.</p>	<p>Does not comply</p> <p>This has been further expanded on below, however the application does not contribute to the intended downstream drainage stormwater solution to the West of the development outlines in the LWMS. The application seeks to contain all stormwater on-site associated with the development.</p>
5.	<p>The land within the 150m Water Treatment Buffer is excluded from residential and other sensitive as defined by EPA Statement No.3, land uses unless otherwise negotiated with Aqwest following a risk management assessment of the final chlorine storage area, and to the satisfaction of the City.</p>	<p>Not applicable to this application.</p> <p>The proposed development is located outside of 150m Water Treatment Buffer and not a sensitive land use.</p>
6.	<p>Suitable landscape buffers shall be provided to residential land uses to the satisfaction of the City of Bunbury, and all costs shall be the responsibility of the developer.</p>	<p>Not applicable to this application.</p> <p>The proposed development does not abut any land identified within the Structure Plan as being residential.</p>
7.	<p>Prior to the approval of any subdivision or development application, the developer/s shall prepare a Drainage Staging and</p>	<p>Does not comply</p> <p>As noted in point 4 above, the developer seeks to address this by</p>

	Contributions Plan to the satisfaction of the City of Bunbury to address the timing, staging, land acquisitions and subdividers' contributions towards the implementation of drainage infrastructure as identified by the 'Drainage Management Strategy.'	providing stormwater infrastructure that contains all stormwater on-site associated with the development.
8.	South Western Highway proposed realignment to be set aside as a separate lot pending acquisition by Main Roads WA.	Not applicable to this application. South Western Highway realignment does not impact upon the lot subject to this application.
9.	Detailed Area Plan/s shall be required as a condition of subdivision for all lots fronting the existing and future primary regional road. Development adjacent to the Bypass must be orientated towards the Bypass in terms of building facades and landscaping as specified in such Detailed Area Plan/s. There shall be no direct access permitted onto the Bypass.	Not applicable to this application.
10.	A detailed Area Plan is required as a condition of subdivision for any land designated as R40 or higher.	Not applicable to this application.
11.	Area subject to further investigation. Subject to separate structure planning.	Not applicable to this application. The proposed development is located outside of this identified area.
12.	The future Wimbridge Road shall be severed and a cul-de-sac is to be constructed by Main Roads WA when the South Western Deviation is constructed.	Not applicable to this application. Proposed development is located outside of this identified area.
13.	Drainage Investigation Area to be in accordance with the Drainage Management Strategy and associated Local Water Management Strategy.	Not applicable to this application. Proposed development is located outside of this identified area.
14.	The area marked 'Temp Drainage Basin' shall be subject to a legal agreement naming the City of Bunbury as having an interest in the land as an asset with all associated costs borne entirely by developer/s. The legal agreement shall be in place prior to any site works or development commencing within the Structure Plan Area, and shall remain in place until the	No legal agreement or development contribution plan exists over the Wimbridge Structure Plan area. The proposed development is located outside of this identified area.

	downstream drainage infrastructure, as identified in the Drainage Management Strategy', is implemented to the satisfaction of the City of Bunbury. The developer is to demonstrate, to the satisfaction of the City of Bunbury, that the 'Temp Drainage Basin' will satisfy all the requirements as identified in the 'Drainage Management Strategy.'	
15.	No development or subdivision approvals will be issued for the area marked for 'Temporary Drainage Basin' until such time as the full implementation of the Western Drainage Outlet, as identified in the Drainage Management Strategy, has been achieved.	The proposed development is located outside of this identified area. Proposal does not contribute to the Western Drainage Outlet – this has been further commented on in the stormwater section below.
16.	'Industry Noxious', 'Industry Port', 'Industry - General' uses are not permitted under the 'Industry' classification on the Structure Plan.	Not applicable to this application. The proposed development is proposing a 'Storage/Warehouse' land use.
17.	The proposed Winsor Street Extension to the Port Access Road is to be set aside as a separate lot pending acquisition by Main Roads WA.	Not applicable to this application.
18.	Site contamination investigations are to be undertaken for Lots 58, 100, 201, 202 and 203, as applicable, prior to subdivision application stage.	Not applicable to this application.
19.	A Fire Management Plan is to be prepared and endorsed by Department of Fire and Emergency Services (DFES) prior to subdivision application stage.	Not applicable to this application.
20.	The Water Corporation is intending to acquire land to co-locate a sewer pump station adjacent to the existing Agwest site which will require modification to the structure plan.	Not applicable to this application. The proposed development is located outside of this identified area.

Provisions 4 and 7 of Structure Plan - Stormwater Management and Precinct Drainage

As noted above, the applicant has requested that the City vary the requirement of provision 7 of the structure plan for contributions to broader precinct stormwater infrastructure. The LWMS identifies the subject site as being within Pre-Development

Catchment PD3, which generally slopes towards the south-west and contributes to downstream drainage issues as a result.

The LWMS suggests that all properties being developed within Catchment PD3 should contribute to a downstream stormwater drainage solution, being a culvert outlet which goes under South-Western Highway and deposits water to the Preston River. As part of this, whilst stormwater associated with 1 in 10 stormwater events are required to be maintained on-site, stormwater associated with 1 in 100 are to be captured at a precinct level. Under the structure plan these events are initially to be contained at the temporary drainage basin located at the south-western corner of Catchment PD3 until the downstream drainage stormwater solution outlined above is constructed. The City currently has an application for such a drainage solution.

In lieu of contributing to the downstream drainage stormwater solution outlined in the LWMS, the applicant seeks to contain all stormwater associated with the 1 in 10 and 1 in 100 storm event on-site. The stormwater management plan seeks to achieve this with drainage basins being included within the lot boundaries of the proposed development (Attachment 6). It is noted that the stormwater management plan provided includes some shortcomings which require amendment in order to support the proposal:

- The proposed stormwater basin to the south extends into the designated 'Local Road' reserve (under the Structure Plan and Local Planning Scheme).
- The existing stormwater-controlled outlet is proposed to be retained which currently flows onto adjoining lots.
- MRWA have noted technical concerns with the stormwater infrastructure in terms of ensuring water is appropriately channelled at the Northern property boundary.
- DWER have noted technical concerns with additional clarifications required for the stormwater management measures.

Regarding the stormwater basin within the designated 'Local Road' reserve, it is acknowledged that this stormwater infrastructure has been previously endorsed as part of previous development applications. The subject proposal will increase the usage of this drainage basin from the directing of water from the development into the basin. The City considers that this infrastructure being retained within the 'Local Road' reserve would fetter the development of that 'Local Road' reserve. This limiting of the development of this road reserve would also prejudice the Wimbridge Structure Plan which requires this road connection.

In light of the above, the recommended conditions have included a condition to revise the stormwater management plan and amended development plans to ensure all stormwater (including the capacity of existing open drains within the 'Local Road' reserve) is contained within the development boundaries and outside of the designated 'Local Road' reserve.

It is noted that the existing stormwater discharge outlet, this stormwater asset is an existing approved asset from a historic development application. This development does not further contribute to the flow experienced from this drainage outlet. It is considered unreasonable to require the drainage outlet to be removed when the development is not exacerbating the issue or increasing the drainage flow.

In relation to MRWA and DWER's comments, their concerns are technical in nature and can be addressed through a further detailed Stormwater Management Plan.

Local Planning Scheme No.8, State and Local Planning Policies

Land Reserved as 'Local Road'

As noted above, the development relies on an existing access from Willinge Drive (a 'Primary Regional Road') with an incoming access to the West and outgoing access to the East (Attachment 5). This access encroaches into a designated 'Local Road' reserve under the City's Local Planning Scheme No. 8 (Attachments 1 and 8). In addition, the landowner currently utilises a number of the existing public roads (Rossiter St, Burnell St) as effectively a private internal road network which discourages public usage. There are a number of planning issues with the retention of this road in private ownership.

Firstly, there is a general presumption against direct private access onto a 'Primary Regional Road' under the WAPC's Development Control Policy 5.1 – Regional Roads (Vehicular Access)(DCP5.1). Whilst this access is existing, the proposal anticipates an increase to traffic volumes with a maximum of 12 trucks per day and 4 light vehicles. The proposal will therefore intensify to some extent the vehicular access. DCP 5.1 discourages the intensification of private access onto a 'Primary Regional Road' and recommends utilising alternative accesses where available.

Secondly, the relevant designated 'Local Road' is intended within the Wimbridge Precinct Structure Plan to provide a central road link between development West of this property to Willinge Drive, providing through movement from South Western Highway to Willinge Drive. The proposal incorporates the retention of the 'Local Road' reserve privately whilst also proposing to develop over the designated 'Local Road' reserve (for instance, with the proposed crossover layout) and to use the surrounding 'Local Road' reserve network for private usage. This has the potential to fetter development to the South and West of the property and has the potential to prejudice the overall structure plan which provides for this access to Willinge Drive. It is noted that the owner's existing private usage over the public road network already discourages public through traffic from the South to Willinge Drive.

State Planning Policy 3.6 – Infrastructure Contributions (SPP3.6) addresses development contributions. Clauses 6.4, 6.8 and Schedule 1 of SPP3.6 outline standard infrastructure contributions that may be levied through the subdivision and development process, including land contributions for roads and associated infrastructure works. As per these provisions, the ceding and construction to an appropriate road standard of the 'Local Road' reserve is considered to have a sufficient nexus and to be reasonable as:

- It will help facilitate the broader Wimbridge Structure Plan;
- It will help improve the permeability and traffic flow to the existing road network within the area which supports industrial development to the South and West. The construction and ceding of this road reserve would allow for through traffic from Southern lots to Willinge Drive;
- The traffic generated by the proposal intensifies the use of the relevant access network;
- The development relies on a mix of privately owned 'Local Road' reserves and private use of public roads which hampers through traffic inconsistently with Local Planning Scheme No.8 and the Wimbridge Structure Plan.

- Permitting the development to retain and intensify a private access directly onto a 'Primary Regional Road' is inconsistent with DCP5.1.

Development Standards

The proposal has been assessed against the relevant Scheme and Policy environment. The application includes variations to building height, building design and appearance, car parking, landscaping (shade trees) and maximum sign height.

City of Bunbury Local Planning Scheme No. 8			
Provision	Requirement	Proposal	Assessment
Schedule 5 – Additional Zone Development Requirements Table. <i>Front Setback</i>	Primary Street: Minimum 4m	<i>Warehouse /Storage Bunkers</i> Primary Road (designated local road): 15m <i>Hardstand Storage Area</i> Primary Road (designated local road): 5m	Complies.
<i>Secondary Street Setback</i>	Secondary Street: Minimum 3m	<i>Warehouse /Storage Bunkers</i> Designated Primary Regional Road: 21m Willinge Drive: 193m <i>Hardstand Storage Area</i> Designated Primary Regional Road: 5m Willinge Drive: 10m	Complies.
<i>Side and Rear Setbacks</i>	Side: Minimum 0m	Side (West): 35m	Complies.
<i>Landscaping</i>	Street setback areas to be landscaped for the full frontage to an average width of 3m, but with a minimum width of not less than 1.5m Of the required landscaping, a maximum of up to	Landscaping has been provided within street setback towards all streets in accordance with requirement.	Complies

	<p>50% may be hardscaped.</p> <p>Other setback areas not used for access, parking or waste are to be landscaped.</p>		
<i>Vehicle Parking and Access</i>	<p><u>Warehouse/storage</u> 1 bay per 100 square metres of n/a of premises not open to the public and used for storage</p> <p>Required – 79 bays</p>	Bays provided – 7 bays	<p>Does not comply.</p> <p>The proposed development is part of the larger site and does not result in an increased number of staff or visitors to the site. This is further addressed through the applicants Traffic Impact Statement (see comment below). The City considered this variation to be appropriate.</p>
City of Bunbury Local Planning Policy 3 – Zone Development Requirements <i>Building Height</i>	Maximum building height 15m	<p>Maximum height of 17.75m above Natural Ground Level</p> <p>Finished Floor Level to Ridge height is 15.5m</p>	<p>Does not comply.</p> <p>The relevant variation has occurred largely due to the amount of fill proposed for the site to allow for the development. Similar variations have been supported for the warehouses to the south in terms of Floor to Ridge height and the development (following siteworks) will be at the same level as this development. The experience of building height and bulk will therefore be matched by</p>

			surrounding built form. The City considers the variation supportable considering the context of the site.
<i>Building Design and Appearance</i>	<p>Building façades and walls that front a street (primary and/or secondary) are to be detailed and articulated.</p> <p>Building facades facing a primary street are to be constructed of brick, painted concrete, masonry or glass, for a minimum of 2/3 wall height.</p> <p>All external surfaces of concrete tilt panel must be painted.</p> <p>Building are designed to address the street.</p> <p>Building orientation and frontages to a street must be designed for public safety and include major openings for passive surveillance.</p>	<p>Proposed building to be clad predominantly with concrete tilt panel at the bottom third of the building and colorbond for the top two thirds.</p> <p>37% of the façade towards the local road is concrete tilt panel.</p> <p>Tilt panel can be conditioned to be painted</p> <p>No windows proposed although a roller door faces Willinge Drive.</p>	<p>Does not comply.</p> <p>The City considers that the proposed built form with the mixture of painted concrete panel is appropriate to the surrounding area/the site context which consists predominantly of industrial sheds.</p> <p>The lack of windows/openings is also considered consistent with this predominant surrounding industrial context.</p> <p>As such this variation is considered acceptable.</p>
<i>Landscaping</i>	Street front setback areas are to be landscaped for the full frontage of the lot/development site, excluding any pedestrian and vehicular access	Landscaping is proposed towards each boundary with a minimum depth of 4.3m but predominant depth of 5m.	<p>Does not comply</p> <p>The City considers that the shade tree variation can be addressed through a revised</p>

	<p>point areas to a minimum width of 1.5m.</p> <p>Other setback areas not used for access, parking or waste management are to be landscaped.</p> <p>Shade trees planted at a rate of no less than 1 tree per 8 car parking bays – 1 required</p>	No shade tree demonstrated for provided parking bays.	landscaping plan to provide the tree.
Special Control Area Abattoir	No residential or other sensitive uses (as defined by EPA Guidance Statement No. 3 – Separation Distances Between Industrial and Sensitive Land Uses, June 2005) are to be located on land nominated within this Special Control Area.	-	Complies Lot is not a sensitive land use
Special Control Area Abattoir Notification Area	All proposed lots within 500m from Lot 100 on Deposited Plan: 61127 Wimbridge Road, Picton, (the abattoir) shall be subject to a Notification on the Title advising occupiers that the lot is located within 500m of an abattoir and has the potential to be affected by odour and noise.	-	Does not comply Condition recommended to include notification on title
Local Planning Policy – Signage and Advertising			
Provision	Requirement	Proposal	Assessment
Wall sign maximum height	Maximum of 12m permitted height above natural ground level	Maximum of 12.7m height above natural ground level	Does not comply The City considers that the variation

			<p>can be supported. The relevant signage will be at a proportionate height to the building and will be consistent with the height of signs on the other buildings on-site.</p>
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Traffic Impacts

A Transport Impact Statement (TIS) prepared by PTG Consulting has been provided to support the application (Attachment 5). This is to provides traffic volume with reference to required parking requirements and address traffic movement associated with the development.

As noted above, due to the floor area of the proposed building, the parking bay requirement is 79 parking bays with 7 parking bays provided on-site. The parking for this development should be considered in the context of Qube’s overall operations which occur over multiple lots with reciprocal parking arrangements in place to cater for parking over the multiple lots. In addition, the TIS notes that there is an expected vehicle movement of 4 light vehicles daily to cater for the development with 9-12 trucks also anticipated per day. The existing parking reciprocal parking arrangement and low volume of expected traffic, particularly light vehicle traffic, supports the proposed reduction of parking bay provision on-site.

Regarding vehicle movement, the TIS details that service vehicles will enter at the West side of the designated ‘Local Road’ reserve and exit onto the East side of the designated ‘Local Road’ reserve, and from there onto Willinge Drive. The proposed traffic circulation system is considered somewhat reasonable in that the proposed route caters for the manoeuvring of the vehicles that the building is designed for. However, it is noted that the proposed route relies on the private ownership of designated ‘Local Road’ reserves providing access to the development. As noted above, the vehicle movements identified is an intensification of this use and justifies the ceding/construction of these designated ‘Local Road’ reserves under LPS8.

Vegetation Clearing

The proposal includes the clearing of all existing vegetation on the lot. Aerial imagery indicates that this vegetation is regrowth from the last 10 years and it is predominantly minor shrubs. This vegetation has not been identified to have particular environmental value and the proponent will include landscaped areas in the proposal which will further offset this removal.

Matters to be Considered

The proposal has been assessed against the relevant matters to be considered under clause 67(2) of the Schedule 2 of the *Planning and Development (Local Planning Schemes) Regulations 2015*, particularly with regards to compatibility of the development with its setting, alignment with the Wimbridge Structure Plan and drainage.

The proposed development caters for traffic movement although it relies on the use of a designated 'Local Road' reserve and direct access to a 'Primary Regional Road'. In addition, the development caters for the required stormwater volume noted to be addressed with the relevant LWMS, although it relies on stormwater retention within a designated 'Local Road' reserve which has the potential to fetter future development.

In noting these issues, subject to recommended conditions addressing remaining stormwater and access issues, the proposed development is considered capable of being consistent with the objectives of the zone and the Wimbridge Structure Plan. As discussed above, whilst the development is not consistent with the overall intent of the Wimbridge Structure Plan of smaller subdivided lots, the owner seeks to develop the lot consistently with the surrounding area which is considered supportable. The proposal is also considered to be compatible with the future character of the area based on the current planning framework.

Conclusion:

The application for development approval has been assessed against the relevant planning framework with the 'Warehouse/Storage' land use considered consistent with the objectives of the 'Service Commercial' zone. The development is not necessarily consistent with the overall intent of the Wimbridge Structure Plan to create smaller lots but is considered acceptable when considering the surrounding context of how this structure plan has developed.

The proposed development is recommended for approval subject to conditions, including the submission of an amended stormwater management plan, the relocation of the stormwater infrastructure from the 'Local Road' reserve, and the ceding and construction of this road reserve. A copy of the recommended conditions have been provided to the applicant for review and they have noted that they contest the following:

- Ceding and constructing of the 'Local Road' reserve
- Removal of stormwater infrastructure from the 'Local Road' reserve

The City notes these objections raised. However, for the reasons outlined above, including improved traffic flow as envisaged in the Wimbridge Structure Plan and LPS8. The City maintains its recommendation that conditions be imposed requiring the road reserve to be ceded and constructed. Should DAP determine against ceding and construction (or decide solely to support ceding of the road reserve) the City recommends an additional condition be included to require all stormwater infrastructure to be removed from the future 'Local Road' reserve as this may constrain the future development of the road network.



Lot 260 Lot Boundary

Development Area

Designated Local Road

Willinge Drive

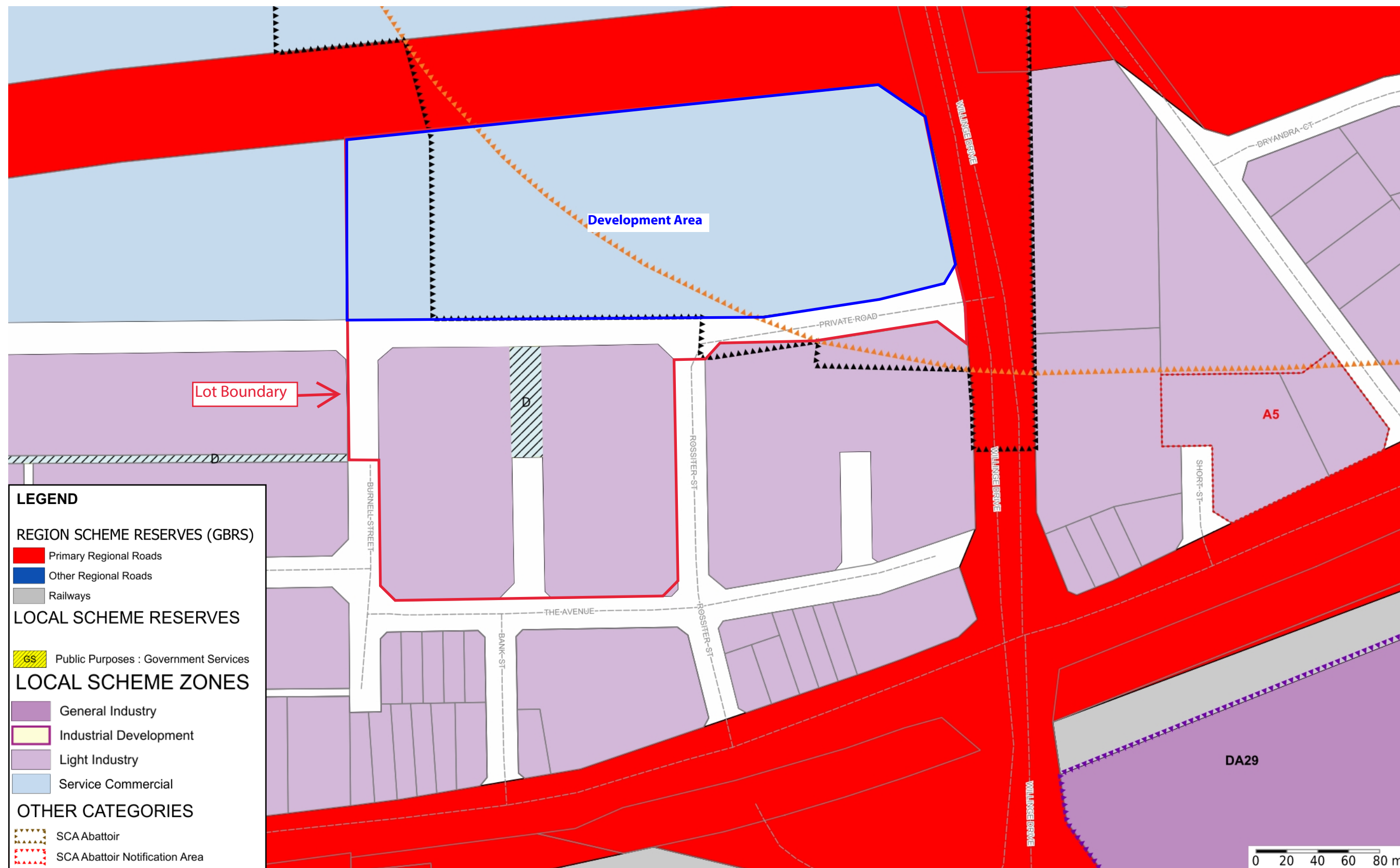
Burnell St

Rosster St

The Avenue

South Western Hwy

0 25 50 m



LEGEND

REGION SCHEME RESERVES (GBRS)

- Primary Regional Roads
- Other Regional Roads
- Railways

LOCAL SCHEME RESERVES

- GS Public Purposes : Government Services

LOCAL SCHEME ZONES

- General Industry
- Industrial Development
- Light Industry
- Service Commercial

OTHER CATEGORIES

- SCA Abattoir
- SCA Abattoir Notification Area



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5/06/2026

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Development Application

Bulk Storage Facility and Hardstand
Lot 260 (390) Willinge Drive, Glen Iris

Prepared for Q Design & Construction Pty Ltd
Prepared by Taylor Burrell Barnett
May 2025



Document Information

Development Application

Bulk Storage Facility and Hardstand
Lot 260 (No.390) Willinge Drive, Gen
Iris

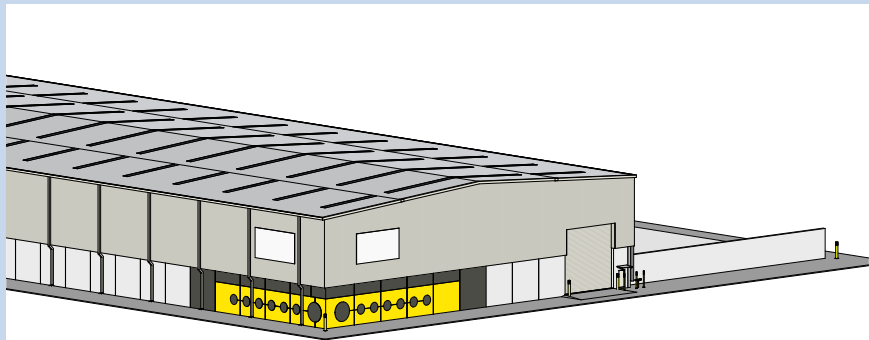
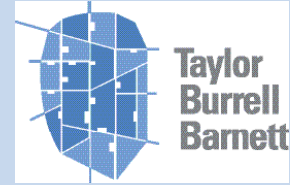
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Doc ID: A RPT Lot 260 (No.390) Willinge Drive, Glen Iris

Revision	Status	Author	Approved by	Date Issue
0.1	Draft	L Martins	J Ross	15 May 25
1.0	Final	L Martins	J Ross	19 May 25

Disclaimer

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1.0 Introduction

1.1 Project Description

Taylor Burrell Barnett, acting on behalf of our client, Q Design & Construct, has prepared this report in support of an application for development approval for the use and development of land at Lot 260 (No.390) Willinge Drive, Glen Iris (subject site), for the purpose of developing a bulk storage facility and associated hardstand.

The development application has been prepared in accordance with the provisions of the City of Bunbury's *Local Planning Scheme No.8* (LPS 8) and the Wimbridge Precinct Structure Plan.

The subject application has been made to the Development Assessment Panel (DAP) in accordance with the *Planning and Development (Development Assessment Panel) Regulations 2010*.

The subject site is in the ownership of Giacci Holdings Pty Ltd who are signatories to the application.

The development proposes the construction of a bulk storage facility (warehouse) inclusive of external storage and hardstand areas, forming an extension to the existing operations and facilities on the subject site and adjoining Lot 101, which is owned and operated by the same owner.

The warehouse will front an existing internal private road and will be accessed by both light and heavy vehicles via dedicated crossovers to the broader local road network.

Construction is scheduled to commence in late 2025, following receipt of the necessary planning and building permit approvals, with completion and occupation anticipated in 2026.

Table 1 provides a summary of the proposed development.

Table 1: Development Summary

Proposal	Details
LPS8 Zoning	Service Commercial
LPS8 Land Use Definition	'Warehouse / Storage' - means premises including indoor or outdoor facilities used for <ol style="list-style-type: none"> a. the storage of goods, equipment, plant or materials; or b. the display or sale by wholesale of goods.
Zoning Table - Land Use Permissibility	'Warehouse / Storage - 'P'
Construction Timeframe	6 - 12 months
Development Details	Summary
Building Height	Ridge height of 15.5m (eave height of 13.5m)
Land Details	Lot 260 (No.390) Willinge Drive, Glen Iris
Vehicle Access	Two new crossovers (entry and exit only) via Rossiter Road Existing access provided via Rossiter Road via Willinge Drive
Number of Parking Spaces	7 parking bays including 1 ACROD parking bay

1.2 Pre-Lodgement Consultation

Pre-lodgement consultation has been undertaken with the City of Bunbury with respect to the proposal. **Table 2** below provides an overview of the discussions in relation to the development application.

Table 2: Pre-Lodgement Meeting Outcomes

Date	Who	Notes / Outcomes
16 September 2024	City of Bunbury & Taylor Burrell Barnett	<p>Road Reservations</p> <ul style="list-style-type: none"> The City advised it has no current plan to acquire the land traversing Lot 260 which is identified as 'Local Road' within LPS8. Development over zoned land is generally supported, but the City would not support any works that compromise the future provision of the east-west road reserve. <p>Internal Road Requirement</p> <ul style="list-style-type: none"> The City acknowledged that the loop road shown in the Wimbridge Structure Plan served to support small-lot subdivision of Lot 260, but that this was not included as part of the Local Road reserve under LPS8. As a result it was recognised that this land was capable of development where small lot subdivision was not proposed. <p>Drainage Management</p> <ul style="list-style-type: none"> Coordinated drainage across the broader area is subject to the endorsed Local Water Management Strategy (LWMS) and any development or subdivision of the subject site will need to be justified in this context.

1.3 Previous Approvals

1.3.1 Development Approval (5.2018.178.1)

On 7 September 2018, development approval was granted for the construction of a warehouse/storage facility on the subject site, including associated hardstand for truck parking (LG Ref: 2018.178.1)(Figure 1).

1.3.2 Development Approval (5.2021.165.1)

On 11 October 2021, the City of Bunbury granted development approval for a workshop and ancillary office on Lot 101. Lot 101 (No. 16) The Avenue, Picton, is located directly east of the subject site, with Rossiter Street forming its western and northern boundaries shared with Lot 260, as illustrated in Figure 1.

The workshop services trucks transporting materials to and from the existing storage sheds on the surrounding site. Lot 101 is owned and operated by the proponent, with both Lot 101 and Lot 260 operating as a single integrated facility.

1.3.3 Development Approval (5.2024.87.1)

On 9 August 2024, development approval was granted for an extension to the previously approved warehouse building. The area that had originally been used for truck parking under the 2018 approval was amended to accommodate this warehouse extension.

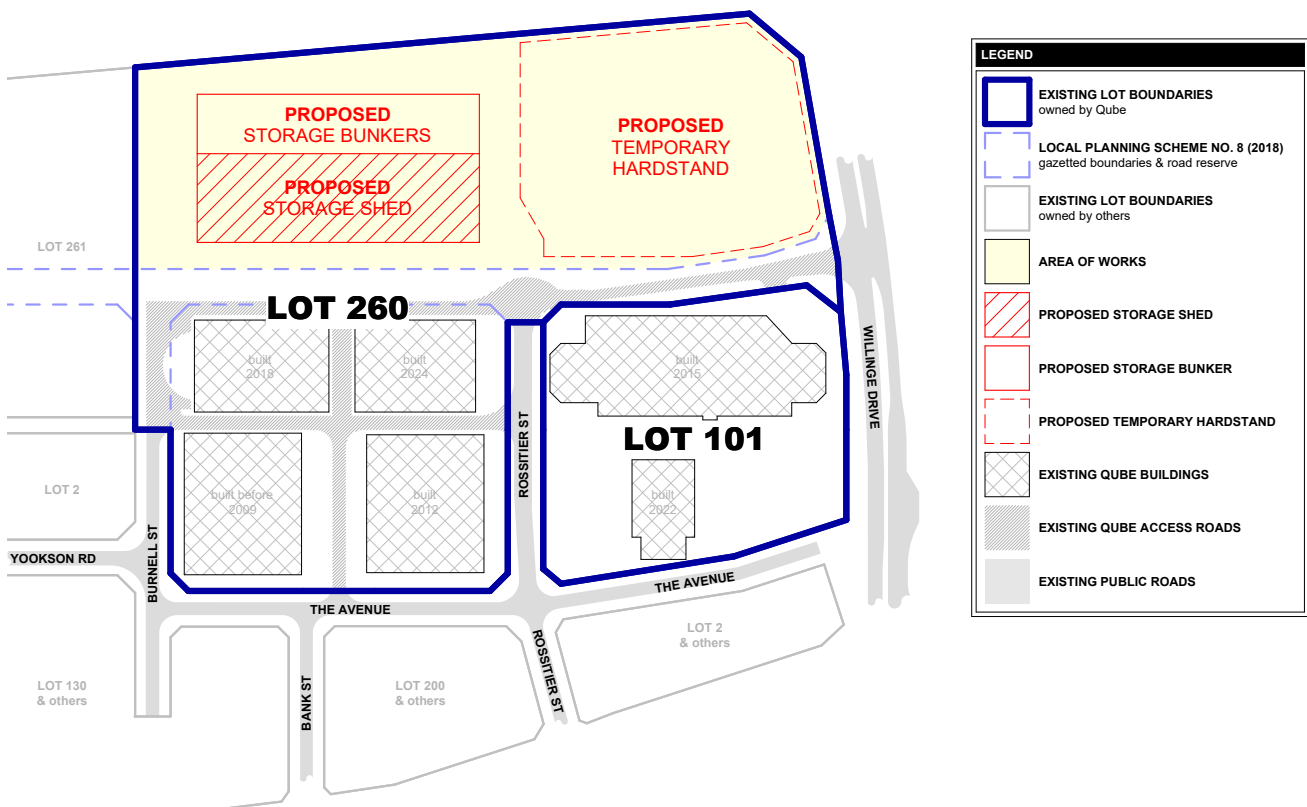


Figure 1: Existing Development and Approvals Plan - Lot 260 and 101

2.0 Site Analysis

2.1 Location

The subject site is within the suburb of Glen Iris, situated within the municipality of the City of Bunbury. It is located approximately 8km south-east of the Bunbury town-centre and 5km south-east of the Bunbury Port, refer **Figure 2**.

The subject site is well connected to major freight routes, including the South Western Highway (south), Forrest Highway (north), and the Bunbury Railway (west), providing strong access to surrounding centres.

The site is located within the City's light industrial area, proximate to the Picton Industrial Park. This broader area is identified as a future employment and economic hub, with approximately 436 hectares zoned for a mix of industrial and service commercial uses.

2.2 Surrounding Land Uses

The land surrounding the subject site is primarily developed for warehousing and manufacturing uses (refer to **Figure 3**), including auto-body shops, manufacturing businesses, and specialised equipment and repair services. Several nearby properties also feature open-air storage of industrial materials.

The proposed warehouse and hardstand development is consistent with the existing built form and considered compatible with the area's future industrial character.

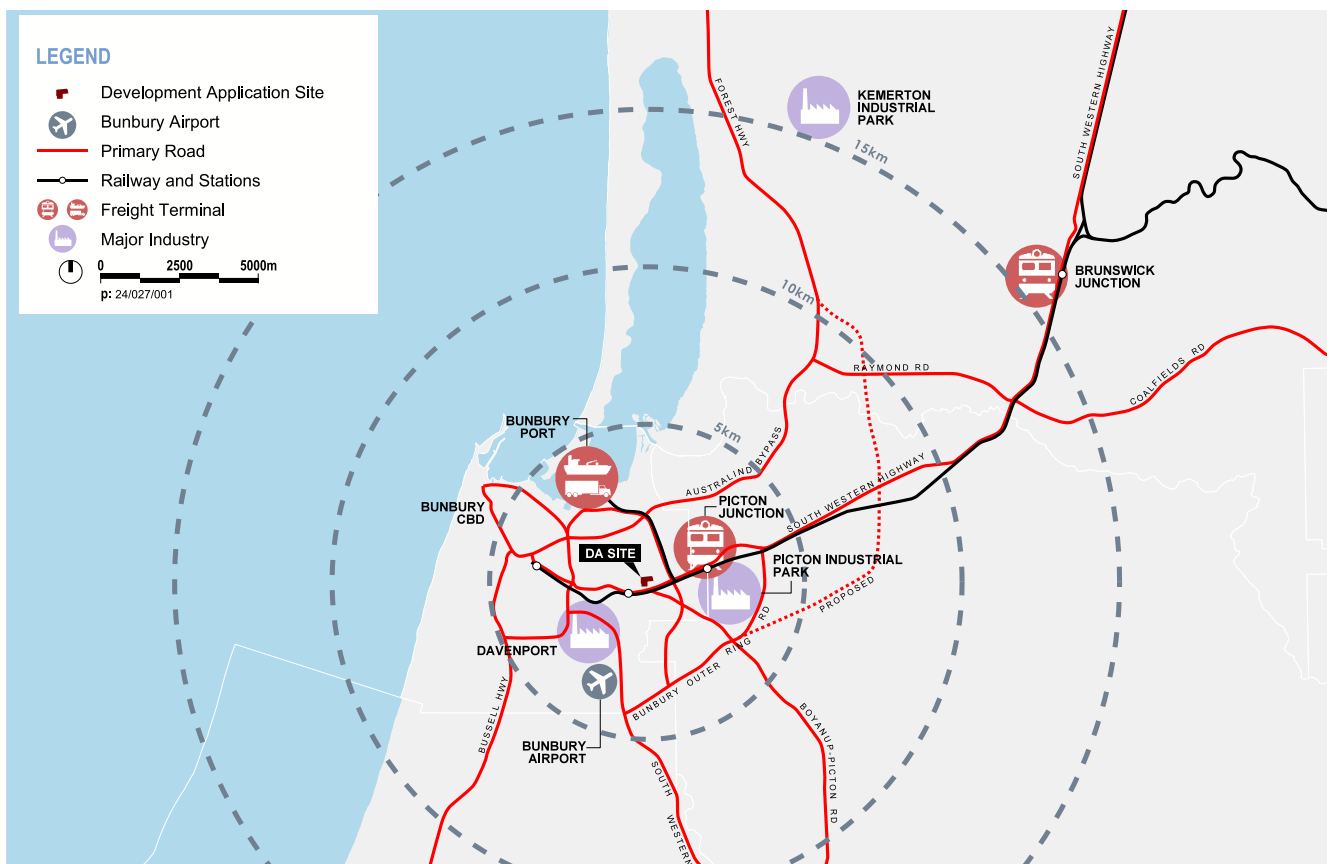


Figure 2: Regional Context Plan

2.3 Subject Site

The subject site is bounded by Willinge Drive to the east, Burnell Street to the west, The Avenue and Rossiter Street to the south, and vacant land with remnant vegetation to the north. The area subject to this application is generally flat and currently undeveloped.

Three bulk storage warehouses are located on the southern portion of the site, with a fourth approved (DA/5/2024/87/1) to be constructed on an area previously used for truck stabling.

Adjacent Lot 101, to the east, is developed with a workshop and hardstand vehicle storage area, also operated by the proponent.

The subject site and Lot 101 operate as a single integrated facility, and the proposed development will form part of this broader operation, as shown in **Figure 2**.



Figure 3: Subject Site

2.4 Land Ownership

Land ownership is outlined in **Table 1** and evidenced by the Certificate of Title included as **Appendix B**. The proposed warehouse will form a natural progression to the existing developments and operations currently being carried out on the site and adjacent Lot 101.

Table 3: Land Ownership Details

Address	Registered Proprietor	Plan No.	Volume/Folio	Total Lot Area
Lot 260 (No.390) Willinge Drive, Glen Iris	Giacci Holdings Pty Ltd	413537	2955/226	9.028ha

3.0 Development Proposal

3.1 Development Overview

The application seeks approval for a total building area of 13,317m² comprising of a warehouse building, external storage bunker area and hardstand area, as listed below and illustrated in **Appendix A**.

- A Single Warehouse Building (7,990m²)
- External Storage Bunker/Cell (5,327m²)
- Stabilised Limestone Hardstand (19,500m²)
- Landscaping, including general landscaping and planted drainage swales (1,310m²).

3.1.1 Land Use Permissibility

The proposed development is considered a 'Warehouse / Storage' land use under the City of Bunbury's LPS8. Warehouse / Storage is defined under LPS8 as:

Warehouse / Storage means premises including indoor or outdoor facilities used for -

- the storage of goods, equipment, plant or materials; or*
- the display or sale by wholesale of goods.*

'Warehouse / Storage' is listed as a 'P' use (Permissible) within the 'Service Commercial' zone, which means that the use is permitted if it complies with all relevant development standards and requirements of LPS8.

3.1.2 Operation

The operations of the proposed development will reflect the existing nature of the development on the site, involving the delivery of mineral sands by truck, with materials to be stored and bagged on-site.

Upon arrival, trucks will unload mineral sands onto designated stockpiles located either within the warehouse, in external storage bunkers, or directly on the hardstand area.

A 12.5-metre-wide circulation space is provided within the warehouse to facilitate heavy vehicle movements east-west. A 16-metre-wide circulation space is provided for circulation servicing the external storage bunkers.

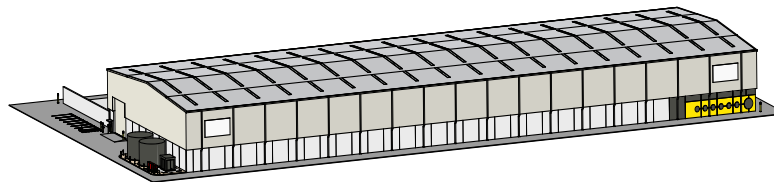


Figure 4: Perspective of Warehouse Building (facing north-east)

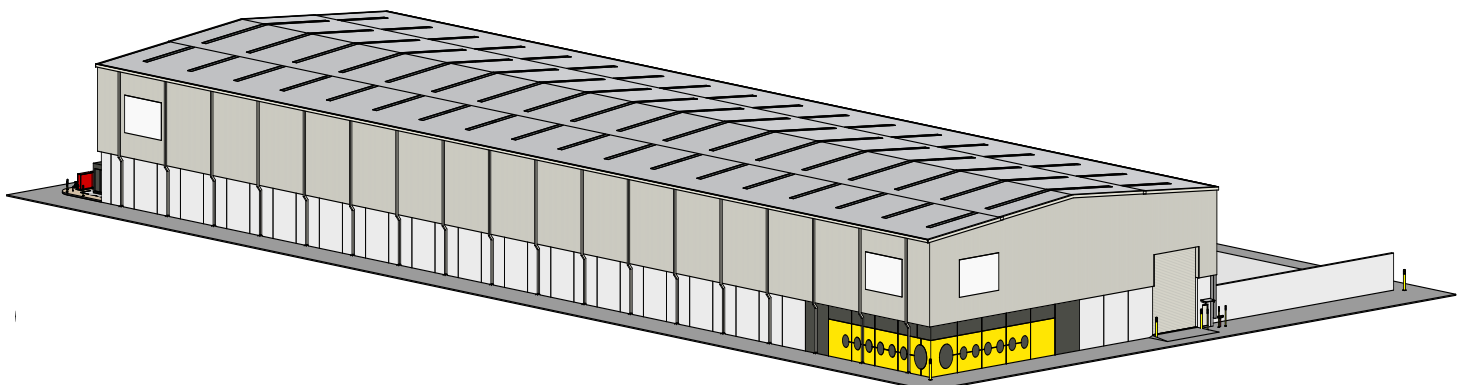


Figure 5: Perspective of Warehouse Building (facing south-west)

3.2 Built Form and Design

The site and elevations of the proposed warehouse are illustrated in **Figure 4** and **Figure 5**, and provided in **Appendix A**.

The built form component is outlined in the following section.

3.2.1 Warehouse

The proposed warehouse will be a rectangular structure measuring approximately 156.1m in length, 49.5m in width, with a ridge height of 15.5m, as illustrated in **Figure 6**.

The warehouse will be accessible from the western end via a roller shutter door, allowing vehicles to traverse through the warehouse and exit via a corresponding roller shutter door at the eastern end. This arrangement facilitates one-way movement of heavy vehicles across the building, supporting safe and efficient circulation.

Each roller shutter door will measure 8m in width and 10.5m in height, ensuring appropriate vertical and horizontal clearance for heavy vehicles. Bollards will be installed adjacent each roller door to ensure legibility and safety.

The warehouse is intended for the storage of mineral sands and represents a coordinated extension of existing operations located on the southern portion of the subject site and adjoining Lot 101.

Internally, the building will comprise three dedicated stockpiling areas, each with a floor area of 1,942m². A detailed summary of the proposed stockpile volumes and capacities is provided in **Table 4** and demonstrated in **Figure 6**.

Table 4: Stockpiling Details

Design Criteria	Internal Stockpiles 1 - 3	External Stockpiles 4 & 5
Height (each stockpile)	11.0m	11.0m
Total Estimated Volume	34,437m ³	30,106m ³
Stockpile Floor Area	5,827m ²	5,192m ²

Please note all volumes and tonnage are an estimate.

The warehouse façade will incorporate a mix of steel and precast concrete panels, consistent with the colour and material palette of the existing development on the subject site. The south-eastern corner of the building will include a yellow and black facade reflecting the Clients branding, and will provide a splash of colour to assist in 'breaking up' the building bulk, as illustrated in **Figure 7**.

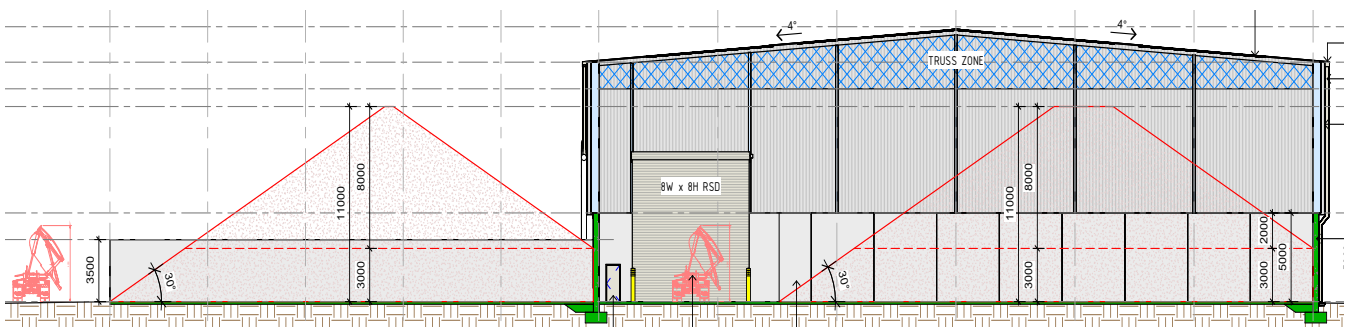


Figure 6: Stockpiling Diagram

3.2.2 External Storage Bunkers

Located along the northern boundary of the warehouse are two external storage bunkers (Stockpiles 4 and 5).

Each storage bunker will be open-air, have a storage area of 2,596m² and will be separated by concrete tilt-up retaining wall, as shown in **Figure 7**. The northern side will remain open to enable access for heavy vehicles for loading and unloading purposes.

The area between the northern site boundary and the bunkers will include a 9,610m² asphalt circulation space. This will accommodate heavy vehicle movements similar to those within the warehouse. Vehicles will enter via a western crossover, circulate around the bunkers, and exit via the egress only crossover connecting to Rossiter Street.

The materials to be stored and bagged, primarily mineral sands, are typically inert and do not generate significant levels of dust, noise, odour, or particulate emissions. As such, the proposed use is not expected to adversely impact nearby sensitive land uses.

3.2.3 Hardstand

The balance of the subject site to the east of the warehouse is proposed to be developed for hardstand storage. The hardstand will consist of stabilised limestone and will support additional storage space for sand and bauxite minerals until such time the client seeks to develop this area.

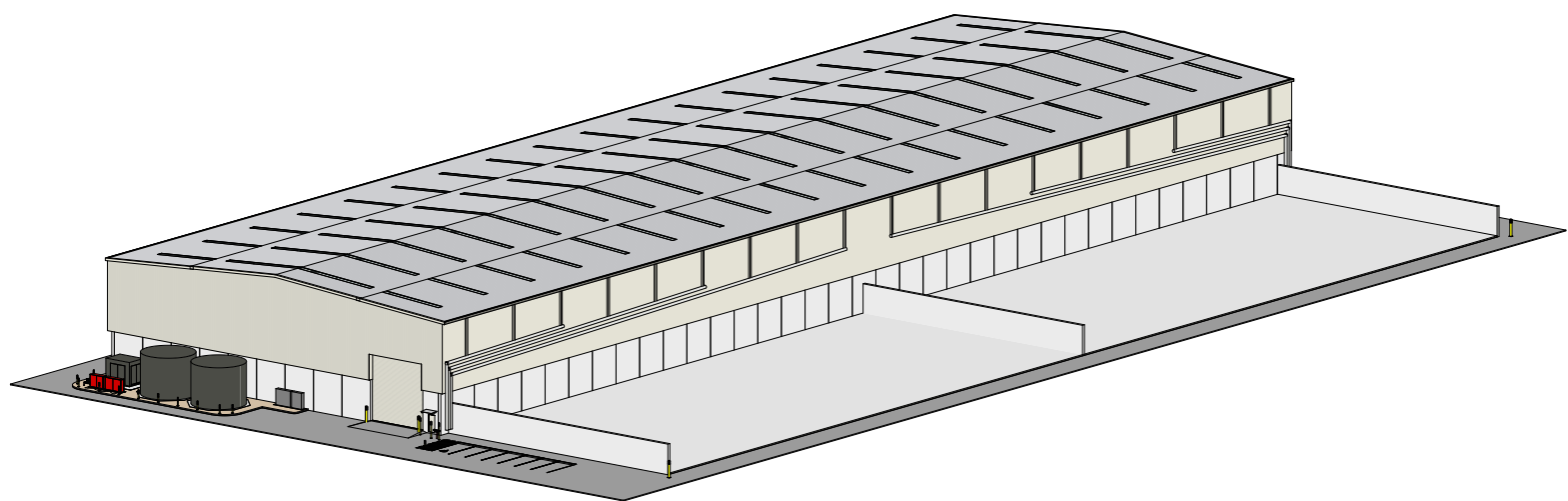


Figure 7: Perspective of warehouse building (facing south-east)

3.3 Access, Circulation and Parking

This proposal is supported by a Transport Impact Statement (TIS) included as **Appendix C**. The report concludes that the anticipated traffic generated by the proposed development will have a negligible impact on the surrounding road network.

3.3.1 Site Access

The proposed development will utilise the existing access route from Willinge Drive at the Rossiter Street intersection to the east. Two new crossovers are proposed along the southern boundary of the site, with 'Access A' functioning as an entry only and 'Access B' as an egress only, as shown in **Figure 8** and **Appendix C**.

Access to the external storage bunkers will follow the same one-way circulation pattern, with vehicles travelling north of the bunkers before looping south to connect with the warehouse egress crossover. The hardstand area is designed to accommodate vehicle movements as required, including for the unloading of materials.

All service and delivery vehicles will follow this one-way circulation and will enter and exit the site in forward gear only.

3.3.2 Coordinated Operations & Circulation

The subject site will operate in an integrated manner with the existing approved operations, functioning as a single facility. Noting the shared nature of the development, the proposal will have a negligible impact on the surrounding road network.

3.3.3 Heavy Vehicle Access / Swept Path

Traffic generated by the proposed development will primarily be heavy vehicles, including Restricted Access Vehicle (RAV) classes 4 - 7. There is dedicated and sufficient hardstand area for heavy vehicles on all sides of the warehouse to facilitate internal circulation.

The site currently accommodates RAV 4 vehicles, and swept path diagrams demonstrating compliant entry and exit manoeuvres are provided in **Figure 9** and **Appendix C**.

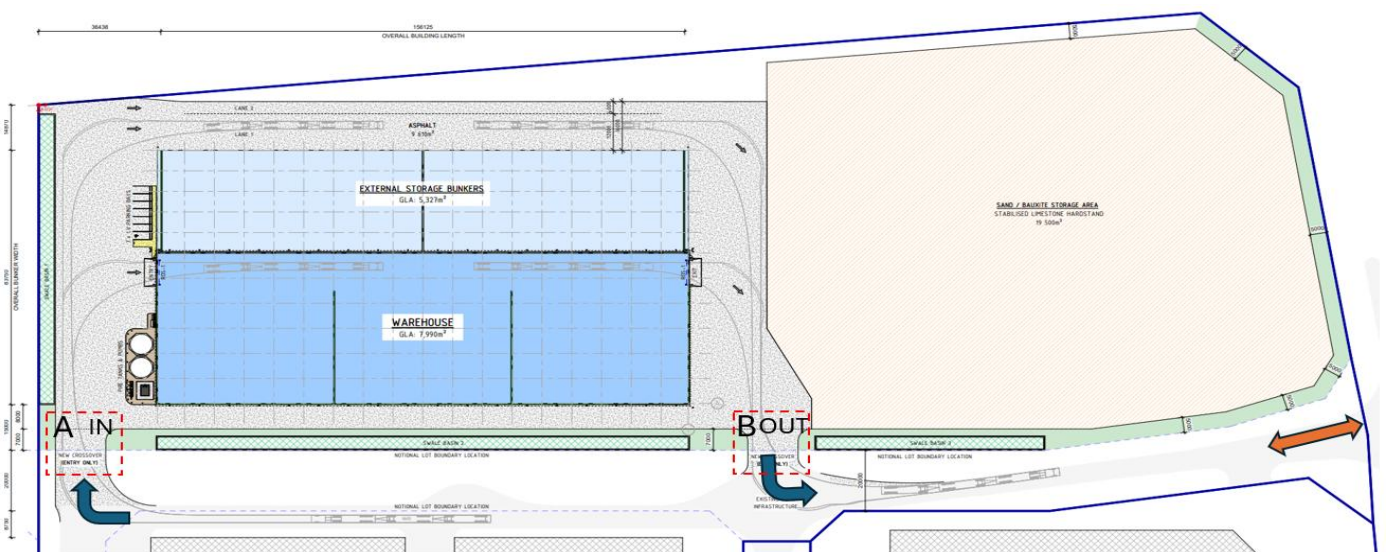


Figure 8: Proposed Site Access Arrangement)

The proposed development is expected to accommodate an average of 18 heavy vehicle (HV) movements per day, with a peak of up to 24 movements on high-volume days. These movements will be spread throughout the day, and it is unlikely that multiple HV arrivals will occur within a short time frame.

Traffic movement times will align with standard industrial hours, primarily between 6:00am – 8:00am and 4:00pm– 6:00pm, with approximately 20% of traffic occurring during these peak periods.

All loading and unloading activities will occur entirely on-site.

3.3.4 Parking

The proposed development is part of larger site operations associated with the existing development. As such, the proposal will generate a total of four additional staff.

A total of seven light vehicle parking bays, including one ACROD-compliant bay, are proposed on the western side of the external storage bunkers, as shown in **Figure 8**.

The proposed parking provision achieves the staff car parking demand, and provides surplus bays to accommodate in-frequent visitor parking.

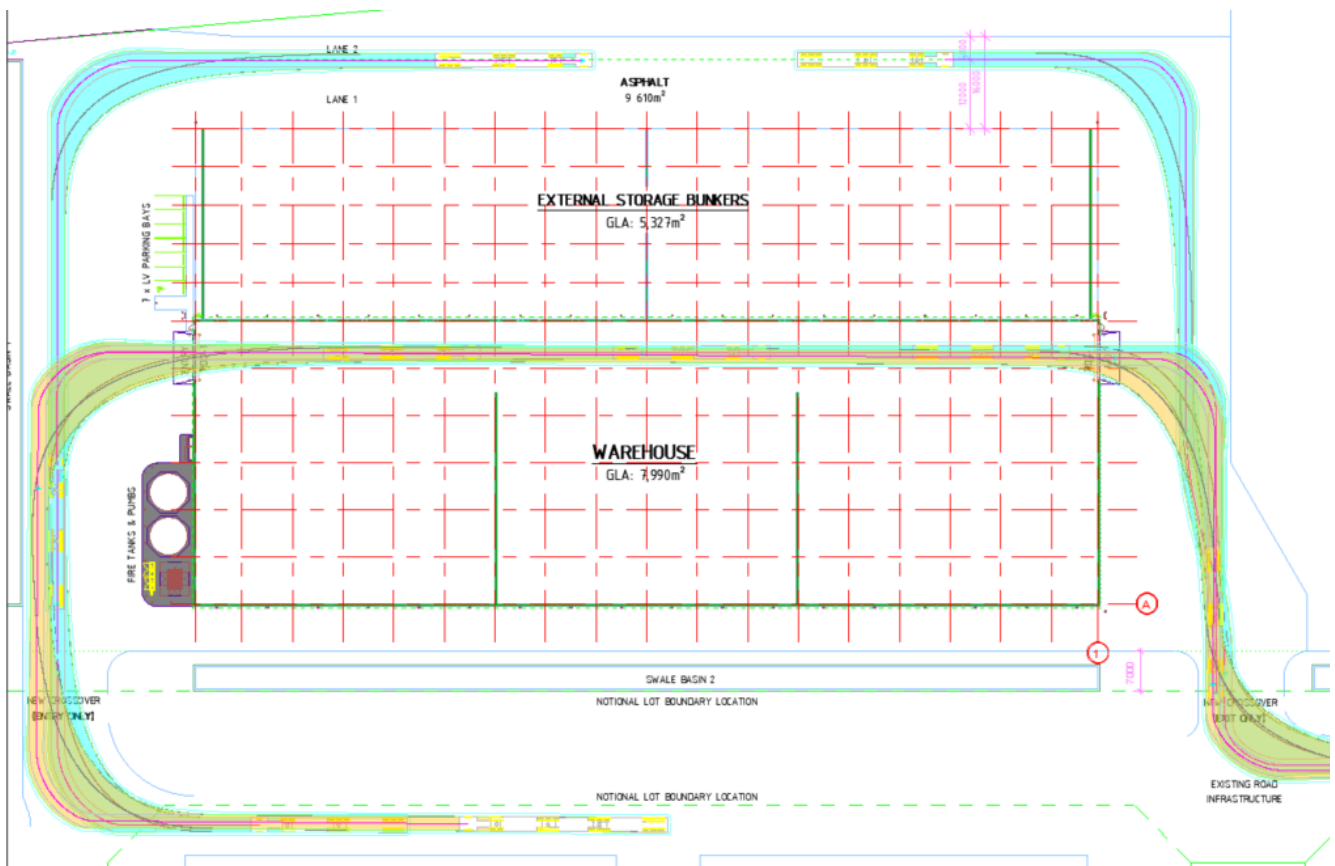


Figure 9: RAV 4 Swept Path Analysis

3.4 Landscaping

A comprehensive Landscape Concept Plan has been prepared to support the proposed development, with the overall plan shown in **Figure 10** and detailed plans included in **Appendix D**. The proposal includes a total of 1,310m² of landscaping along the boundaries of the development site.

Landscaping is predominantly proposed as swale basins running along the boundaries of the development site, which will be planted in accordance with the plant mix outlined.

The landscaping will provide visual relief from any surrounding public points of view, in addition to providing stormwater drainage management, and is consistent with the development of the broader site.

3.5 Stormwater Drainage

A Stormwater Drainage Management Plan has been prepared and included in **Appendix E**. This plan outlines that drainage storage required from the catchment of 49,395m² catchment area is 2,904m³. The total estimated storage available within the network of swales and basins proposed across the site is 3,076m³, providing an excess of capacity on site.

In recognition of the City’s desire for the proposal to ensure consistency with the adopted Local Water Management Strategy (LWMS) for the broader area, a technical note has been prepared by the project hydrologist, Jim Davies and Associates (JDA) (**Appendix F**).

This technical note confirms that the proposal meets the expectations of the LWMS as adopted.

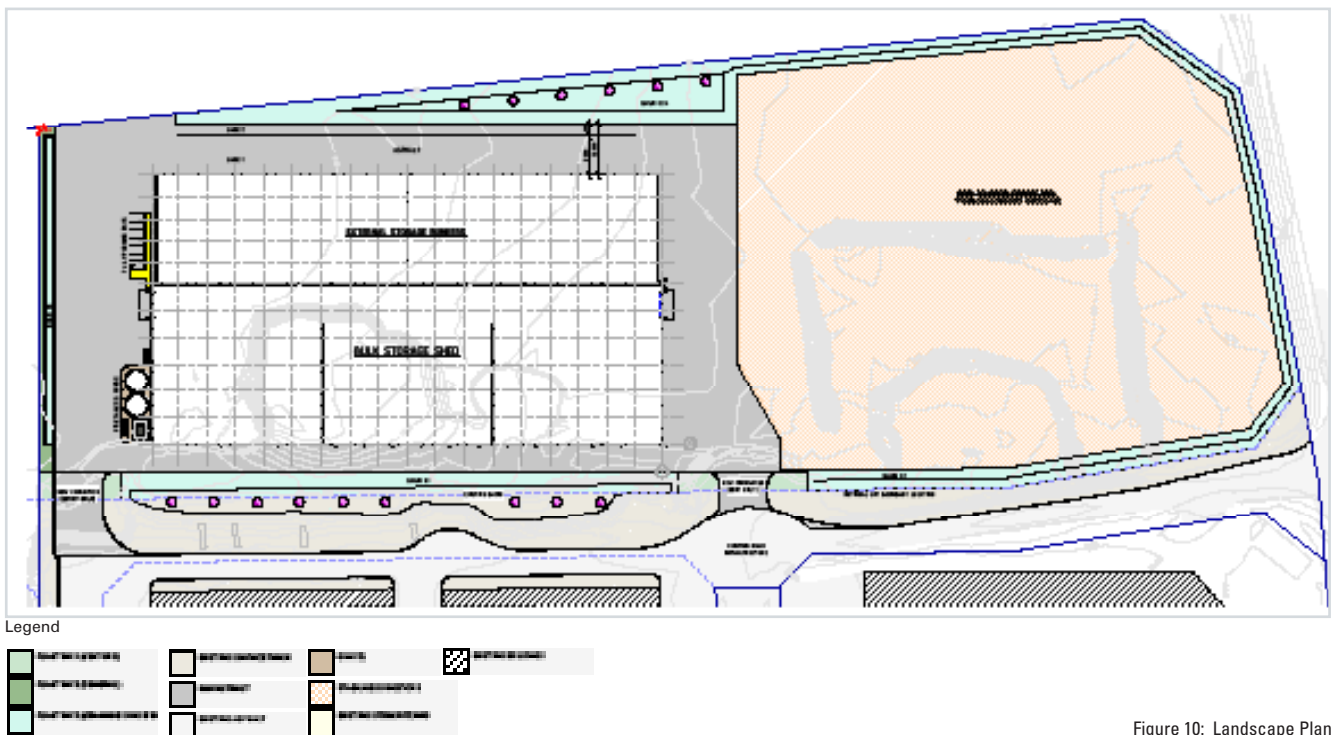


Figure 10: Landscape Plan

3.6 Essential Services

3.6.1 Water

The subject site has an existing water connection which will be extended in accordance with separate approvals from the Water Corporation.

3.6.2 Power

The existing power connection to the subject site will be extended from the existing development, sufficient in providing for general power outlet and lighting purposes to the proposed development.

3.6.3 Wastewater Management

The proposed development does not include any ablution facilities or wash down areas which would necessitate connection to wastewater management infrastructure.

3.6.4 Waste Management

The proposed development does not entail the creation of additional waste, as waste management is sufficiently accommodated with existing development on-site and adjacent Lot 101.

4.0 Planning Framework

4.1 City of Bunbury Local Planning Strategy 2018

The City of Bunbury *Local Planning Strategy (LPS) 2018* was endorsed by the WAPC on the 1 March 2018. The LPS outlines five 'Key Priority Areas', each with land use planning objectives to facilitate the strategic intentions for the area.

Under *Key Priority Area 4 - Regional Economy* the city sets out the key objective for industrial development is;

- *To facilitate the timely development of existing zoned industrial land with particular emphasis on smaller, more employment intensive, clean, light, high-tech and service industries.*

The subject site is identified under a 'Service Commercial Areas,' refer **Figure 11**, defined in the strategy as areas with maximum locational advantages

to activity centres and strategic transport connections that comprise low impact commercial uses such as showrooms, warehouses and light industries which meet the service and employment needs of the city.

The proposed development is deemed to be in alignment with the planning objectives of the area. It serves as a natural extension of the existing low-impact warehouse developments and associated operations in the precinct.

Additionally, the development site capitalises on the strong transport links proximate without posing any detrimental impacts on the surrounding area's amenity.

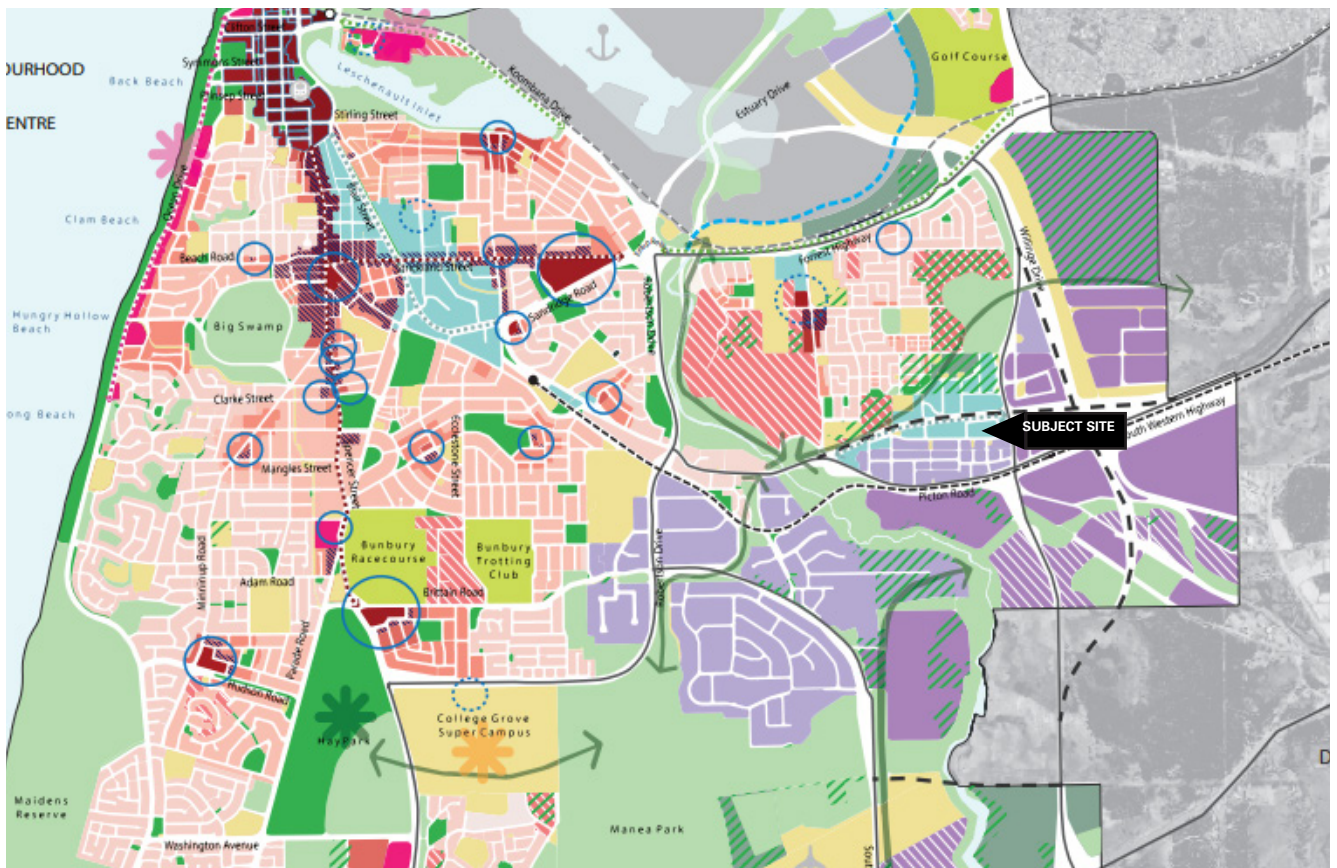


Figure 11: Local Planning Strategy Extract

4.2 Greater Bunbury Region Scheme

The subject site is zoned 'Industrial' under the Greater Bunbury Region Scheme (GBRS) (refer **Figure 12**), as is the entirety of the surrounding area.

The adjacent South Western Highway (south) and Willinge Drive (east) are both reserved Primary Regional Roads under the GBRS.

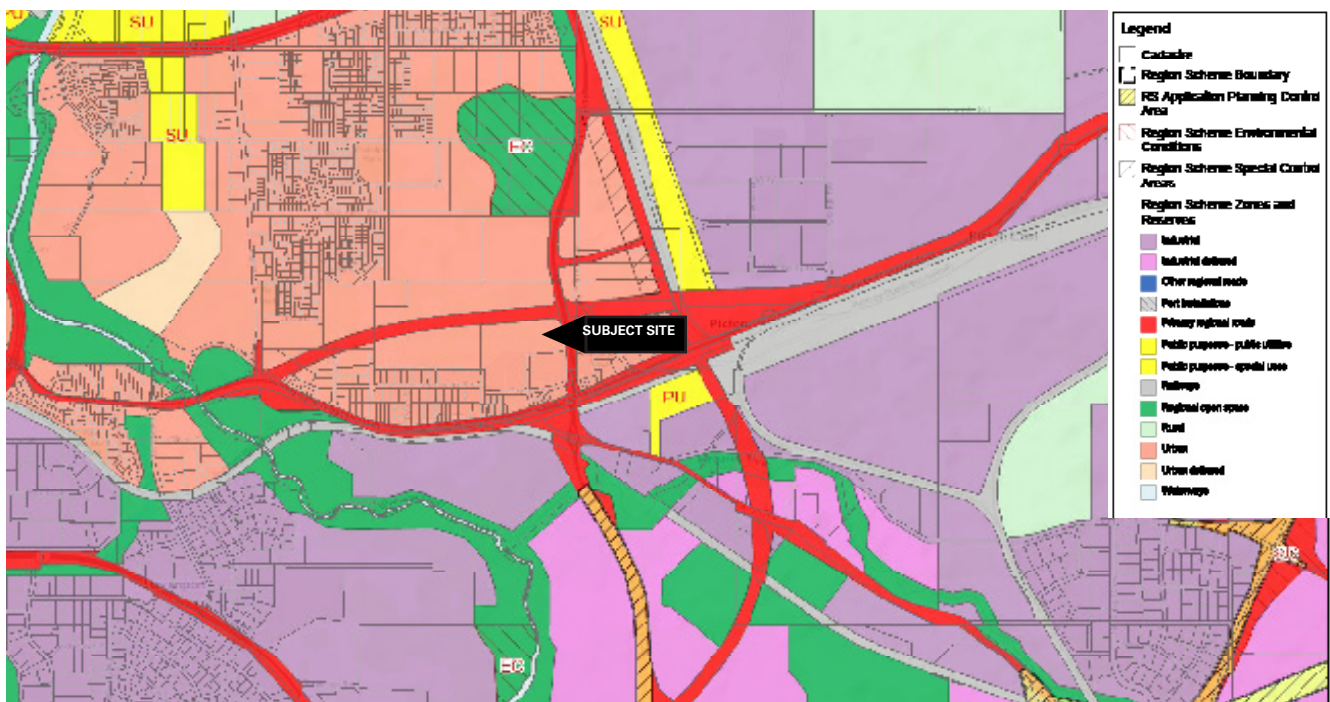


Figure 12: Greater Bunbury Region Scheme Extract

4.3 City of Bunbury Local Planning Scheme No. 8

The subject site is zoned ‘Service Commercial’ under the City Bunbury *Local Planning Scheme No. 8* (LPS8), refer **Figure 13**.

The objectives of the ‘Service Commercial’ zoning are:

- To accommodate commercial activities which, because of the nature of the business, require good vehicular access and/or large sites.
- To provide for a range of wholesale sales, showrooms, trade and services which, by reason of their scale, character, operational or land requirements, are not generally appropriate in, or cannot conveniently or economically be accommodated in the regional centre, other activity centres or industrial zones.

The proposed development is consistent with the objectives of the zone, given it benefits from strong accessibility to key transport and freight routes,

including the South Western Highway, Willinge Drive (connecting to Forrest Highway), the Bunbury Railway Line, and nearby industrial areas such as Picton Industrial Park and Davenport.

Additionally, the subject site forms part of a broader operational facility, with an existing internal road network specifically designed to accommodate the proposed development, noting its current operation across both the subject site and the adjoining Lot 101.

The road reserve identified under LPS8 along the southern boundary of the development site is currently owned and operated by the proponent.

At present, it functions as a private driveway providing access to the site. It is intended that this arrangement will continue until such time as the City initiates formal acquisition of the land for public road purposes.

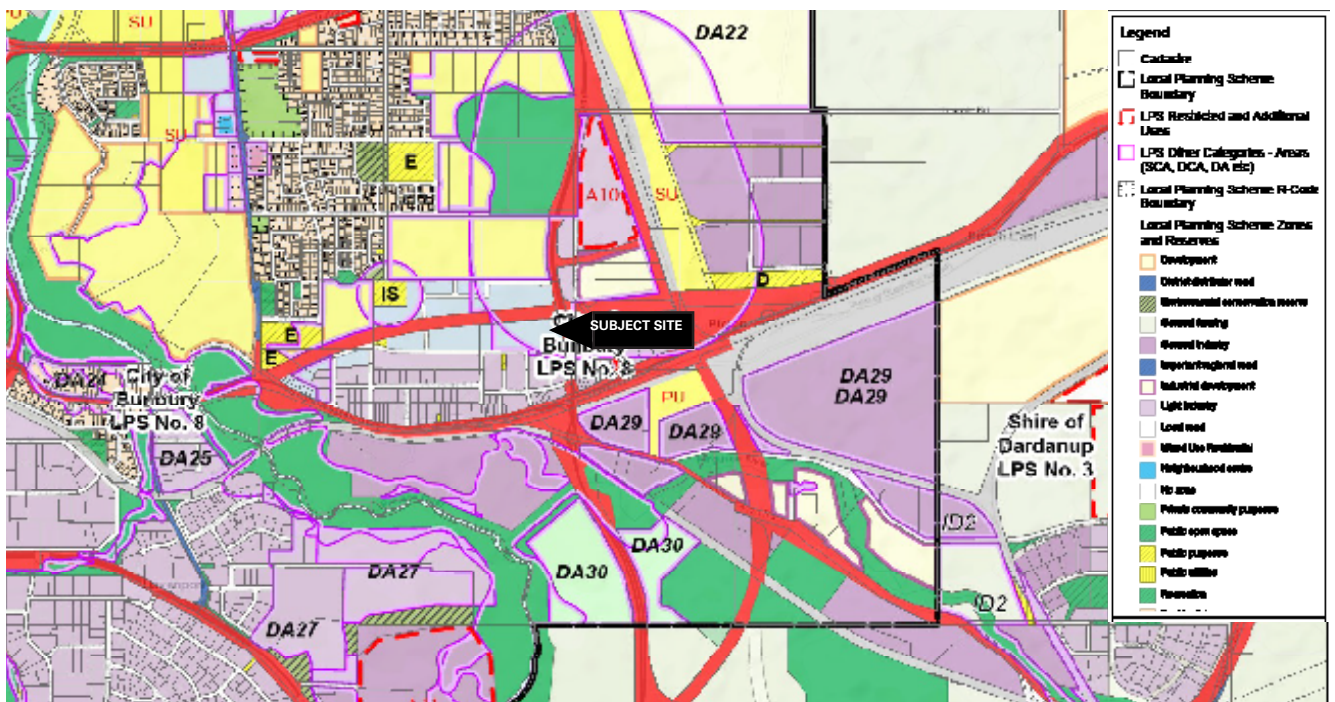


Figure 13: Local Planning Scheme Extract

4.3.1 Special Control Area

The subject site is identified to be contained within the boundaries of two Special Control Areas (SCA), as outlined below:

- SCA Abattoir Notification Area; and
- SCA Abattoir.

The SCA's seek to restrict the designation of sensitive

land uses within the buffer area, with a notification required to be placed on titles to ensure land owners are informed about the potential odour and noise impacts.

The proposed development has been assessed against the objectives of each SCA and is considered compliant (**Table 5**).

Table 5: SCA Objectives and Provisions

Objectives	Proposal
<p>Purpose:</p> <ul style="list-style-type: none"> • To designate land surrounding the abattoir where sensitive land uses will not be permitted <p>Objective</p> <ul style="list-style-type: none"> • To inform land owners of potential odour and noise impacts. <p>Additional Provisions</p> <ul style="list-style-type: none"> • All proposed lots within 500m from Lot 100 on Deposited Plan: 61127 Wimbridge Road, Picton, (the abattoir) shall be subject to a Notification on the Title advising occupiers that the lot is located within 500m of an abattoir and has the potential to be affected by odour and noise. 	<p>Compliant.</p> <p>The proposed development is located approximately 400 metres from the boundary of Lot 100 on Deposited Plan 61127. However, it is noted that the development footprint itself is situated more than 500 metres from the abattoir and the proposal does not involve a sensitive land use.</p> <p>On this basis, it is considered that a notification on title is not warranted.</p>
<p>Purpose</p> <ul style="list-style-type: none"> • To designate land surrounding the abattoir where sensitive land uses will not be permitted. <p>Objectives</p> <ol style="list-style-type: none"> To avoid land use conflict between the abattoir and sensitive land uses. To protect the abattoir from encroachment by incompatible land uses that would adversely impact on the efficient operations of the abattoir. To provide for the expansion of the abattoir in a way that maximises amenity, Minimises environmental and health impacts and takes account of risk to nearby sensitive land uses. To promote the development of compatible land uses in areas affected by off-site impacts of the abattoir. <p>Additional Provisions:</p> <ul style="list-style-type: none"> • No residential or other sensitive uses (as defined by EPA Guidance Statement No. 3 – Separation Distances Between Industrial and Sensitive Land Uses, June 2005) are to be located on land nominated within this Special Control Area. 	<p>Compliant.</p> <p>The development does not propose any residential or other sensitive land uses on the site. The proposal forms a logical progression to the existing land uses on the subject site and surrounding properties.</p>

4.3.2 Development Requirements

The proposed development is compliant with the applicable development requirements included in the Schedule 5 of LPS8 - Additional Zone Development Requirements, and Section 4.7 of *Local Planning Policy No.3 - Zone Development Requirements*, as noted in **Table 6**.

Table 6: LPS8 Development Requirements

LPS8 Provision	LPP 3.0 Provision	Proposal
Floorspace Areas		
Minimum net lettable area of 200m ² per lot (including a strata lot or survey strata lot) applies for bulky goods showroom uses.	N/A	Compliant. No Net Lettable Area (NLA) for bulky goods showroom or office uses is proposed as part of this development.
Maximum net lettable area of 200m ² per lot (including a strata lot or survey strata lot) applies for office uses (exclusive of 'incidental' office uses).		
Setbacks		
Primary Street Front Setback Minimum 4.0m	<ul style="list-style-type: none"> a. Minimum 4m front setback from the primary street, subject to the discretion of the local government. b. Minor encroachments, such as awnings, canopies, porches or the equivalent, may project not more than 1m into the street setback area, subject to the discretion of the local government. 	Compliant. The development achieves a 5m setback from Willinge Drive (primary street). This space is proposed to accommodate landscaping. The setback area will be kept clear with no encroachments.
Secondary Street Front Setback Minimum 3m	Minimum 3m front setback from the secondary street.	Compliant. The development achieves a 15m setback from the secondary street (Rossiter Street).
Where not adjoining Residential Zone premises, minimum nil (0m) building setback to rear and side boundaries, subject to appropriate car parking, access and loading arrangements	Minimum nil (0m) building setback to side and rear boundaries, subject to appropriate car parking, access arrangements.	Compliant. Noted. Setbacks are provided to ensure sufficient landscaping and circulation space is provided to ensure adequate separation.

LPS8 Provision	LPP 3.0 Provision	Proposal
Landscaping		
<p>Primary and secondary front street setback areas and other setback areas not used for access, parking and/or waste management are to be landscaped in accordance with the provisions of the relevant local planning policy and Schedule 5.</p>	<ul style="list-style-type: none"> a. Primary and secondary street front setback areas are to be landscaped for the full frontage of the lot/ development site, excluding any pedestrian and vehicular access point areas, to a minimum width of 1.5m. b. Of the required landscaped area, a maximum of up to 50% may be hard landscaping (hardscape), in accordance with an approved landscape plan prepared to the specifications and satisfaction of the local government. c. Other setback areas not used for access, parking and/or waste management are to be landscaped in accordance with the Scheme and the relevant local planning policy. d. Except for a change of use of an existing development, on-site car parking areas within new developments are to be landscaped with shade trees planted at the rate of no less than 1 tree per 8 car parking bays. Species must have a clean trunk to 2m with non-invasive roots to the specifications and satisfaction of the local government. 	<p>The proposal involves a 5-metre-wide landscaping strip along the primary (east) street frontages.</p> <p>This landscaping strip incorporates a swale basin within the southern section (refer to Figure 10). This landscaping strip is severed by the egress-only crossover, beyond which it continues along the southern boundary as a 7-metre-wide landscaping strip, also incorporating a swale basin.</p> <p>The subject site has been substantially developed in accordance with approvals granted by the City of Bunbury over many years, inclusive of landscaping within the setback area to The Avenue.</p> <p>On this basis it is not considered that further landscaping of the subject site is warranted.</p>
Loading / Vehicle Access and Parking		
<p>N/A</p>	<p>In accordance with the Scheme and the relevant local planning policy.</p> <p>Car parking bays/areas adjoining property boundaries to residential premises are visually and acoustically screened from any adjoining dwelling.</p>	<p>Loading and unloading areas are all located on site, separate to any public access areas.</p>
Building Height		
<p>N/A</p>	<p>Maximum of 15m (4 storeys) above natural ground level, or as prescribed in any relevant structure plan and/or local development plan.</p>	<p>The building exceeds the maximum permitted ridge height by 5.75 metres. In accordance with Section 5.1 of LPP3, this variation is considered acceptable for the following reasons:</p> <p>Site Works: Extensive site works are required to grade the site appropriately, with fill largely located in the northern portion of the site having minimal visual impact on the existing streetscape.</p> <p>Eave Height Compliance: The proposed eave height of 13.5 metres (FFL) complies with the applicable height controls, with minor variation to the NGL required from the streetscape.</p> <p>Limited Visual Impact: Given the scale and bulk of the warehouse, the ridge line will not be visible from the streetscape and will have minimal visual impact.</p> <p>Contextual Consistency: The proposed height is consistent with the character of the surrounding area, including existing warehouse developments of similar or greater height.</p>

Building Design and Appearance

N/A

- a. Development must be designed to address the street and complement or enhance the local streetscape character and amenity.
- b. Building façades and walls that front a street (primary and/or secondary) are to be detailed and articulated with design indentations/reliefs, variation in materials, architectural features, colour schemes and active frontages, that include door and window openings, to reduce the visual impact of large blank wall spaces and provide for visual interest.
- c. All building facades facing a primary street are constructed in brick, painted concrete, masonry or glass, or a combination of those materials for 2/3 of the vertical height of the facade or 2.4 metres above natural ground level whichever is greater.
- d. All external surfaces of concrete tilt panel must be painted.
- e. Building orientation and frontages to a street public and/or open space must be designed for public safety and must incorporate major openings that permit passive surveillance over public and semi-public spaces, including car parking areas.
- f. Buildings should be designed to minimise overshadowing of adjoining premises and/or public open space in order to ensure adequate provision of direct sun and ventilation for buildings and to ameliorate the impacts of building bulk, privacy and overshadowing on adjoining properties. Protection of solar access for neighbouring properties should avoid significant overshadowing of:
 - (i) outdoor living areas;
 - (ii) north facing major openings to habitable rooms;
 - (iii) north and west facing roof areas;
 - and(iv)existing solar collectors.

The proposed warehouse is setback and shielded from the existing surrounding road reserves, but has in any event been designed in accordance with the building design and appearance provisions of the City's policy.

4.4 Wimbridge Structure Plan

The Wimbridge Structure Plan was endorsed by the WAPC on 16 April 2015. The development site is identified within the 'Mixed Business' precinct for the purpose of the Structure Plan (**Figure 14**).

The proposed warehouse development is considered consistent with the land use precinct identified on the Structure Plan, as it is assumed that the 'Mixed Business' precinct is intended to accommodate a range of service commercial type uses, consistent with the LPS8 zoning of the site.

The proposed Development Application has been addressed against the requirements of the Structure Plan in **Table 7**.

Indicative New Loop Road

The proposed development departs from the indicative road layout shown on the approved Structure Plan, specifically by not including the internal loop road depicted through the balance of Lot 260.

This variation is considered justified and appropriate on the following basis:

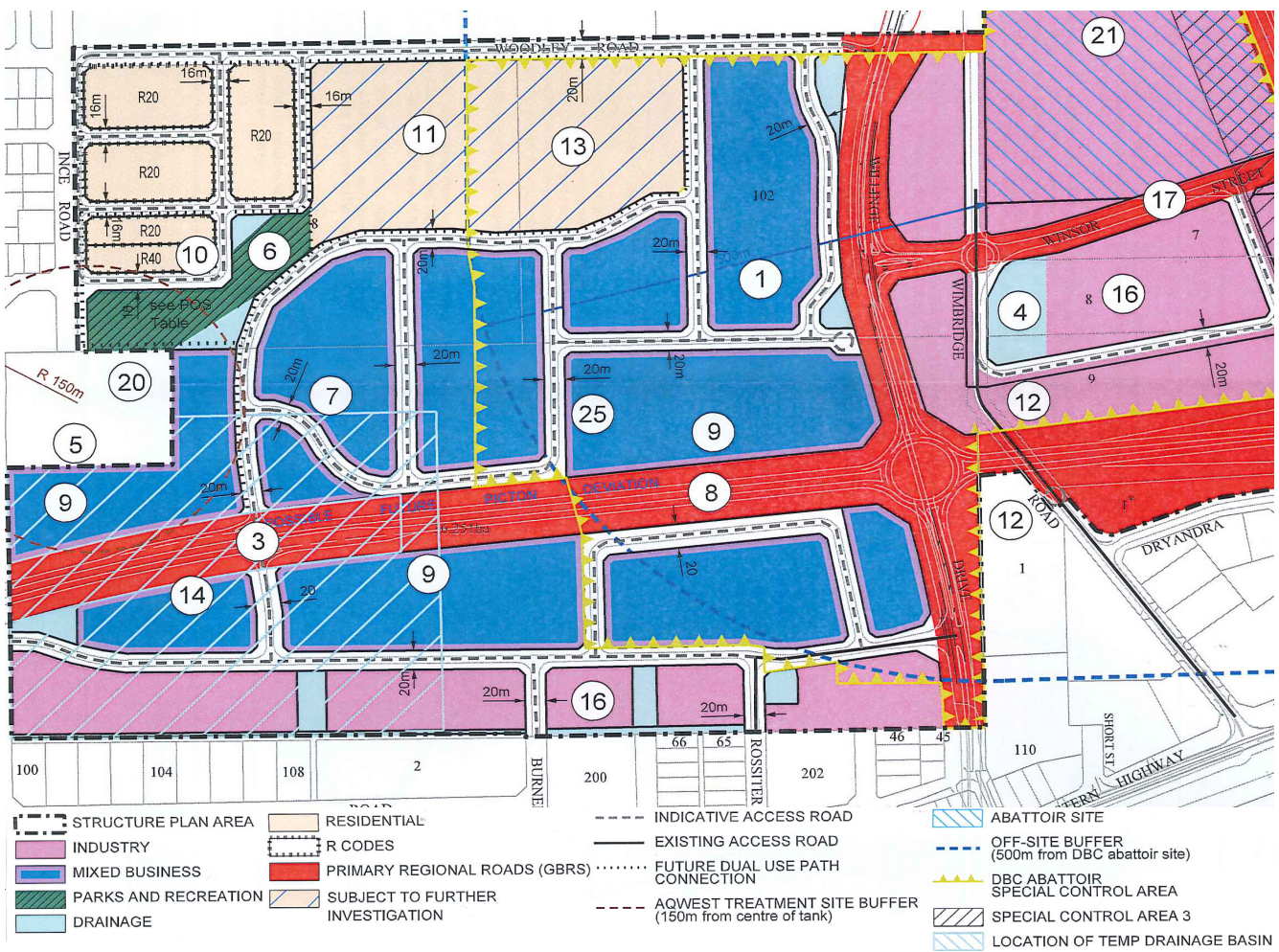


Figure 14: Wimbridge Structure Plan

- **Due Regard Document:** While due regard must be given to the Structure Plan, the internal loop road has not been formally incorporated into the City's Local Planning Scheme as a road reserve. As such, it does not have statutory effect and the land remains available for development.
- **Purpose and Function:** The loop road was originally intended to facilitate small-lot subdivision across the balance of Lot 260. As this subdivision pattern is not proposed, either now or in the short to medium term, the loop road no longer serves a strategic or functional planning purpose. Retaining the land for integrated warehouse and industrial operations better reflects the current development intent and land use efficiency.
- **Pre-Lodgement Consultation:** During pre-lodgement discussions with the City of Bunbury (16 September 2024), the City advised that the exclusion of the loop road from the development application was capable of consideration and support, noting that its purpose is redundant without subdivision.
- **Consistency with Scheme:** This variation relates solely to the loop road. All other Local Road reservations traversing Lot 260, including the east-west link between Willinge Drive and Lot 261, are acknowledged and have been appropriately reflected in the development plans as 'proposed future road connections'. Development within these areas is limited to at-grade driveway connections only, consistent with the intent of the Scheme and capable of future integration as public roads.

In summary, the proposed variation is considered to be a minor and logical departure from the Structure Plan. It aligns with current development intentions and maintains consistency with the Local Planning Scheme.

Table 7: Structure Plan Provisions

Note	Requirements	Comment
1	No residential or other sensitive uses are to be located on land nominated within the abattoir and Water Treatment Special Control Area.	The development does not propose residential or other sensitive land uses within Abattoir and Water Treatment Special Control Area (SCA).
2	Land use proposals within Special Control Area 3 - Glen Iris Service Corridor Buffer Area, as identified in the GBRS, shall be assessed against the following criteria outlined in Division 4 of the Greater Bunbury Region Scheme - the purpose of the area: - planning requirements: and - consultation requirements.	The proposed development is not located within Special Control Area 3.
3	The Staged implementation of the intersection of the proposed north south subdivision road with the future Picton Deviation is to be negotiated by the developer with Main Roads WA. The subdivision road is allowed to be a continuous road until the Deviation occurs at which time arrangement will apply unless agreed otherwise with Main Roads. The southern section of the subdivision road shall be connected to the Picton Deviation via a left in only (from the Highway) arrangement. The northern section of the subdivision road shall be connected to the Picton Deviation via a full tee junction arrangement.	Not applicable to this development.
4	All development shall incorporate stormwater management systems in accordance with the Drainage Management Strategy and associated Local Water Management Strategy prepared by Thompson McRobert Edgeloe Group. No temporary drainage structures will be approved. If any drainage management requirements cross property boundaries, the developer shall negotiate a resolution to the satisfaction of the City of Bunbury prior to any subdivision or development approvals.	Onsite stormwater management has been designed in accordance with the approved Local Water Management Strategy, as outlined in Appendix E and F .
5	The land within the 150m Water Treatment Buffer is excluded from residential and other sensitive as defined by EPA Statement No.3, land uses unless otherwise negotiated with Aqwest following a risk management assessment of the final chlorine storage area, and to the satisfaction of the City.	The proposed development is not located within the 150m Water Treatment Buffer area.
6	Suitable landscape buffers shall be provided to residential land uses to the satisfaction of the City of Bunbury, and all costs shall be the responsibility of the developer.	The proposed development is not located within proximity to residential or other sensitive land uses.
7	Prior to the approval of any subdivision or development application, the developer/s shall prepare a Drainage Staging and Contributions Plan to the satisfaction of the City of Bunbury to address the timing, staging, land acquisitions and subdividers' contributions towards the implementation of drainage infrastructure as identified by the 'Drainage Management Strategy.'	The subject development is self contained and does not require any staging or implementation of drainage infrastructure across other sites. As a result, there is no merit to a requirement for the preparation of a Drainage Staging and Contributions Plan.
8	South Western Highway proposed realignment to be set aside as a separate lot pending acquisition by Main Roads WA.	Not applicable to this development application.

Note	Requirements	Comment
9	Detailed Area Plan/s shall be required as a condition of subdivision for all lots fronting the existing and future primary regional road. Development adjacent to the Bypass must be orientated towards the Bypass in terms of building facades and landscaping as specified in such Detailed Area Plan/s. There shall be no direct access permitted onto the Bypass.	The application does not propose subdivision.
10	A detailed Area Plan is required as a condition of subdivision for any land designated as R40 or higher.	
11	Area subject to further investigation. Subject to separate structure planning.	Not relevant to this application.
12	The future Wimbridge Road shall be severed and a cul-de-sac is to be constructed by Main Roads WA when the South Western Deviation is constructed.	
13	Drainage Investigation Area to be in accordance with the Drainage Management Strategy and associated Local Water Management Strategy.	Drainage management across the development site has been comprehensively addressed in Appendix E and F .
14	The area marked 'Temp Drainage Basin' shall be subject to a legal agreement naming the City of Bunbury as having an interest in the land as an asset with all associated costs borne entirely by developer/s. The legal agreement shall be in place prior to any site works or development commencing within the Structure Plan Area, and shall remain in place until the downstream drainage infrastructure, as identified in the Drainage Management Strategy, is implemented to the satisfaction of the City of Bunbury. The developer is to demonstrate, to the satisfaction of the City of Bunbury, that the 'Temp Drainage Basin' will satisfy all the requirements as identified in the 'Drainage Management Strategy'.	The subject site is not within the area marked for 'Temporary Drainage Basin'.
15	No development or subdivision approvals will be issued for the area marked for 'Temporary Drainage Basin' until such time as the full implementation of the Western Drainage Outlet, as identified in the Drainage Management Strategy, has been achieved.	
16	'Industry Noxious', 'Industry Port', 'Industry - General' uses are not permitted under the 'Industry' classification on the Structure Plan.	Warehouse / Storage land use proposed.
17	The proposed Winsor Street Extension to the Port Access Road is to be set aside as a separate lot pending acquisition by Main Roads WA.	
18	Site contamination investigations are to be undertaken for Lots 58, 100, 201, 202 and 203, as applicable, prior to subdivision application stage.	Not applicable to this development application.
19	A Fire Management Plan is to be prepared and endorsed by Department of Fire and Emergency Services (DFES) prior to subdivision application stage.	
20	The Water Corporation is intending to acquire land to co-locate a sewer pump station adjacent to the existing Aqwest site which will require modification to the structure plan.	

5.0 Conclusion

This application seeks approval to extend the existing warehouse and storage facility at Lot 260 through the provision of an additional warehouse, storage bunkers and hardstand area.

The proposal should be supported by the City of Bunbury and approved by the Development Assessment Panel on the following grounds:

- The proposed development demonstrates compliance with the City of Bunbury *Local Planning Scheme No. 8* and the State and Local Planning Policies applicable to the site and the proposed form of development.

- The site is highly accessible, with key freight routes located within close proximity.
- The proposed development will not have any adverse impacts on adjoining land uses. Specialist reports and assessments have been undertaken to ensure that the development has no adverse impacts on surrounding developments.

We look forward to working with the City of Bunbury to achieve approval for the proposed development.

MEMORANDUM

Subject	City of Bunbury Request for Further Information (RFI) – Applicant Responses
Date	14 th October 2025
Reference	25/025
To	City of Bunbury
From	TBB Planning, Urbanise & Q Design & Construct

Request for Further Information

TBB Planning is pleased to provide the following information in response to the City of Bunbury’s request for further information dated 14th August 2025.

Please find attached the updated development plans and stormwater management plan as per the following:

- Updated set of DA Plans inclusive of landscaping plan (**Attachment A**); and
- Updated Stormwater and Drainage Plans (**Attachment B**).

1. Stormwater Management & Drainage

The updated Stormwater Management Plan (SMP) has been revised to align with the principles of the City’s Wimbridge Precinct Structure Plan and Local Water Management Strategy (LWMS). The design retains all earthworks and infrastructure within the subject lot, maintains natural flow directions, and ensures post-development discharge rates match pre-development conditions.

A response to the requests in the City’s email dated 14th August 2025 is provided in **Table 1** below.

Table 1 Applicant Response Summary

Matter	City Comment	Design / Applicant Response
Battering & Subsoil Discharge (cross-boundary works)	“Battering and infrastructure being constructed outside of the subject lot boundaries including within a MRWA road reserve and Lot 261. No consent has been provided...the plan includes a subsoil discharge into the MRWA road reserve...”	The updated design ensures all stormwater infrastructure is contained within the site. No new works, earthworks, or pipework extend into the MRWA road reserve or adjoining properties. The northern subsoil line will ultimately connect to MRWA’s future drainage but remains temporarily capped, with interim discharge directed internally to the southwest outlet. This allows on-site management until the road network is completed. The approach fully contains infrastructure within the lot while providing a logical future connection consistent with the LWMS.
Controlled Outlet to Lot 261 (Post-development Flows)	“Inclusion of a peak discharge outlet onto Lot 261... would contribute to stormwater on the adjoining lot.”	The SMP includes one controlled outlet to Lot 261, positioned to align with the existing pre-development flow path and the discharge location identified in the LWMS. The system maintains pre- and post-development flow rates consistent with the requirements of the LWMS, and to ensure no additional impact of drainage on the adjacent landholding. To facilitate maintaining existing flows, weep holes in the

		<p>retaining wall allow gradual release of any groundwater or minor seepage, avoiding any concentration of flow or erosion on the adjacent site.</p> <p>This is a consistent and accepted approach that achieves LWMS objectives.</p>
Overland Flows and Willinge Drive Crossing	<p>“Cutting off overland flow possibilities... would cause upstream blocking on the eastern portion of Willinge Drive.” “Lack of consideration for the adequacy of the crossing which may, as part of this development, require upgrading.”</p>	<p>The proposed works do not alter the existing MRWA culvert or external overland flow routes. The system manages internal runoff within the site and discharges via the existing outlet. External catchment flow east of Willinge Drive remains unchanged. Post-development discharge rates are equivalent to pre-development, so the existing crossing capacity remains adequate. The design allows seamless integration with MRWA’s future drainage and does not trigger any upgrade requirements at this stage.</p>
Integration with Wimbridge LWMS and Other Lot 260 Areas	<p>“The SMP does not adequately account for stormwater flows from the rest of Lot 260.”</p>	<p>The updated SMP accounts for the entire Lot 260 catchment by providing sufficient detention volume within the on-site basin to capture all contributing roof and hardstand runoff.</p> <p>The design matches the LWMS detention and discharge criteria and does not rely on the downstream basin within Lot 261. The system will continue to operate effectively once the broader precinct drainage is in place.</p>
Infrastructure within Local Road Reserve	<p>“The SMP includes the use of stormwater infrastructure within an area of land reserved as a Local Road under LPS8.”</p>	<p>The revised plan shows all stormwater basins, swales and pipework located entirely within the freehold development area, clear of the Local Road Reserve. The drainage basin now functions wholly on-lot, ensuring the future road corridor remains unimpeded and consistent with the structure plan intent.</p>

2. Road Reserve Ceding

The City has indicated a potential condition requiring the ceding and construction of the local road reserve to the south of the development, including removal of existing infrastructure. We do not support this condition for the following reasons.

1. There is no need and nexus between the proposed ceding and/or construction of the road and the proposed development. As shown on the submitted plans, works in this area are limited to driveway connections that provide safe, functional access to approved and proposed development on the subject site. The corridor is preserved for future delivery to public standard through normal subdivision and infrastructure processes and the approved structure plan outcome is not prejudiced.
2. The proposed road is solely to the benefit of surrounding landowners, and not to the proponent, who has existing access and egress from the surrounding road network. It should be to the cost of the proponent to provide a public road which only benefits surrounding landowners. The road is identified in the local planning scheme as a road reserve, and remains available for acquisition when and if surrounding subdivision is to occur which relies upon that portion of road reserve.

On this basis, ceding and constructing the local road as part of this approval would be premature and unrelated to the development’s needs.

3. Landscaping

The City has suggested amendments to the landscaping plan to provide screening along Willinge Drive and ensure that landscaping is positioned outside the Local Road reserve. Specifically, the City of Bunbury advised:

- **City of Bunbury:** “Please note, the landscaping plan will require amendment to ensure that landscaping is positioned outside of the Local Road reserve and that landscaping adequately screens the proposed hardstand storage area – in particular, through provision of trees on the Willinge Drive side of the

development. Should you proceed with the application and it be recommended for approval, this could be conditioned.”

In response, the updated Landscaping Plan (refer **Attachment A**) locates all planting and drainage features including swales and basins, wholly outside the internal Local Road reserve, with no works proposed within that reserve.

The plan also introduces ten medium-to-large Weeping Peppermint trees within the eastern drainage basin along the Willinge Drive frontage. This planting provides an effective green buffer that will soften views of hardstand and storage areas from the street, addressing the City’s request for enhanced screening while maintaining clear separation from the future road corridor.

On this basis, the amended Landscaping Plan is considered to satisfactorily address the City’s comments.

DAP Timing / Deferral

The revised design addresses the matters raised by MRWA and DWER in relation to drainage and stormwater discharge, and potential encroachment into adjoining reserves. The revised SWMP and drainage design ensures all drainage infrastructure is contained within the development lot, maintains pre and post development flows, discharge rates, and mitigates potential cross-boundary impacts or back-flooding to the MRWA reserve.

On this basis, TBB respectfully requests that there is no need for re-referral of the amended application to DWER or MRWA acknowledging the design responses have fully addressed the technical matters identified.

To allow for appropriate review, and to ensure the information is properly coordinated prior to referral to the DAP, it is requested that the Responsible Authority Report (RAR) be deferred to 31 October 2025.

TBB considers this extension both reasonable and necessary to enable thorough assessment and agreement on the revised documentation. This timeframe will also support a coordinated approach between all parties and provide confidence that the final recommendation to the DAP accurately reflects a compliant and technically resolved outcome.

Conclusion

We trust the updated information meets the requirements of the City of Bunbury. We look forward to working with the City to progress the application to approval.



Attachment A

UPDATED DA PLANS



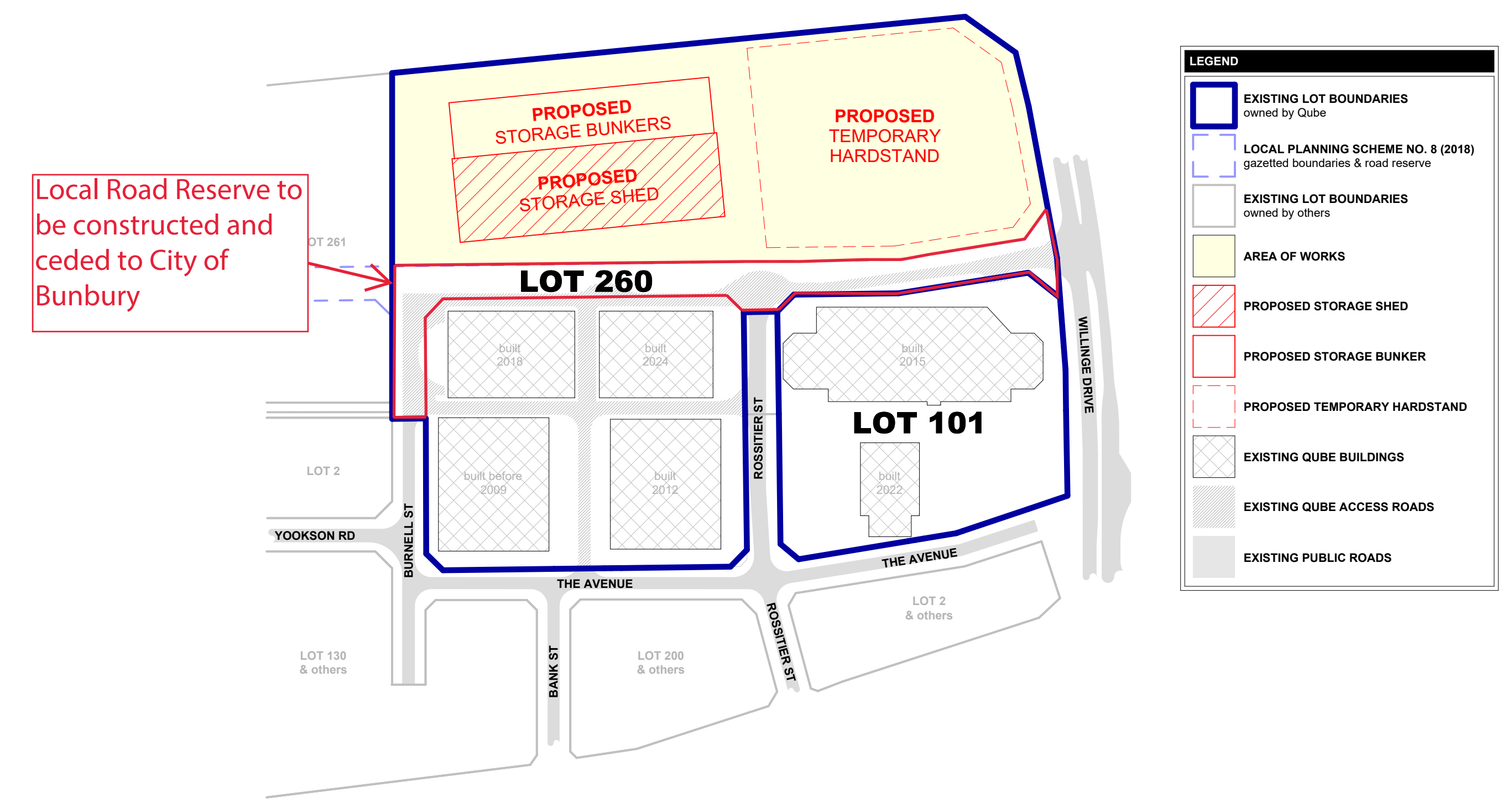


IMAGE SOURCE: NEARMAPS
10 APRIL 2025

QUBE PICTON PLP BULK STORAGE SHED & TEMP HARDSTAND PROPOSAL

LOT 260 (NO.390) WILLINGE DRIVE
GLEN IRIS, 6230

DRAWING REGISTER			
Sheet Number	Sheet Name	REV	Date
DA-0001	COVER PAGE	D	2025.09.19
DA-1000	SITE PLAN	D	2025.09.19
DA-1100	GROUND FLOOR & ROOF PLAN	D	2025.09.19
DA-2000	ELEVATIONS	D	2025.09.19
DA-3000	SECTIONS	D	2025.09.19
DA-L001	LANDSCAPING PROPOSAL	D	2025.09.19



1 Locality Plan
1 : 2500

2 Site Diagram
1 : 2500

0m 50m 100m 150m 200m 250m
Visual Scale 1:2500

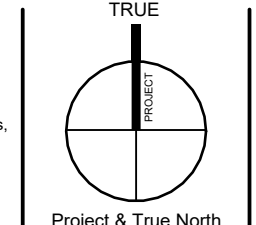
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DEVELOPMENT APPLICATION

PLOTTED 24/09/2025 11:33:59 AM



QUBE PICTON PLP - BULK STORAGE SHED & TEMP HARDSTAND PROPOSAL
LOT 260 (NO.390) WILLINGE DRIVE, GLEN IRIS, 6230
FOR QUBE BULK

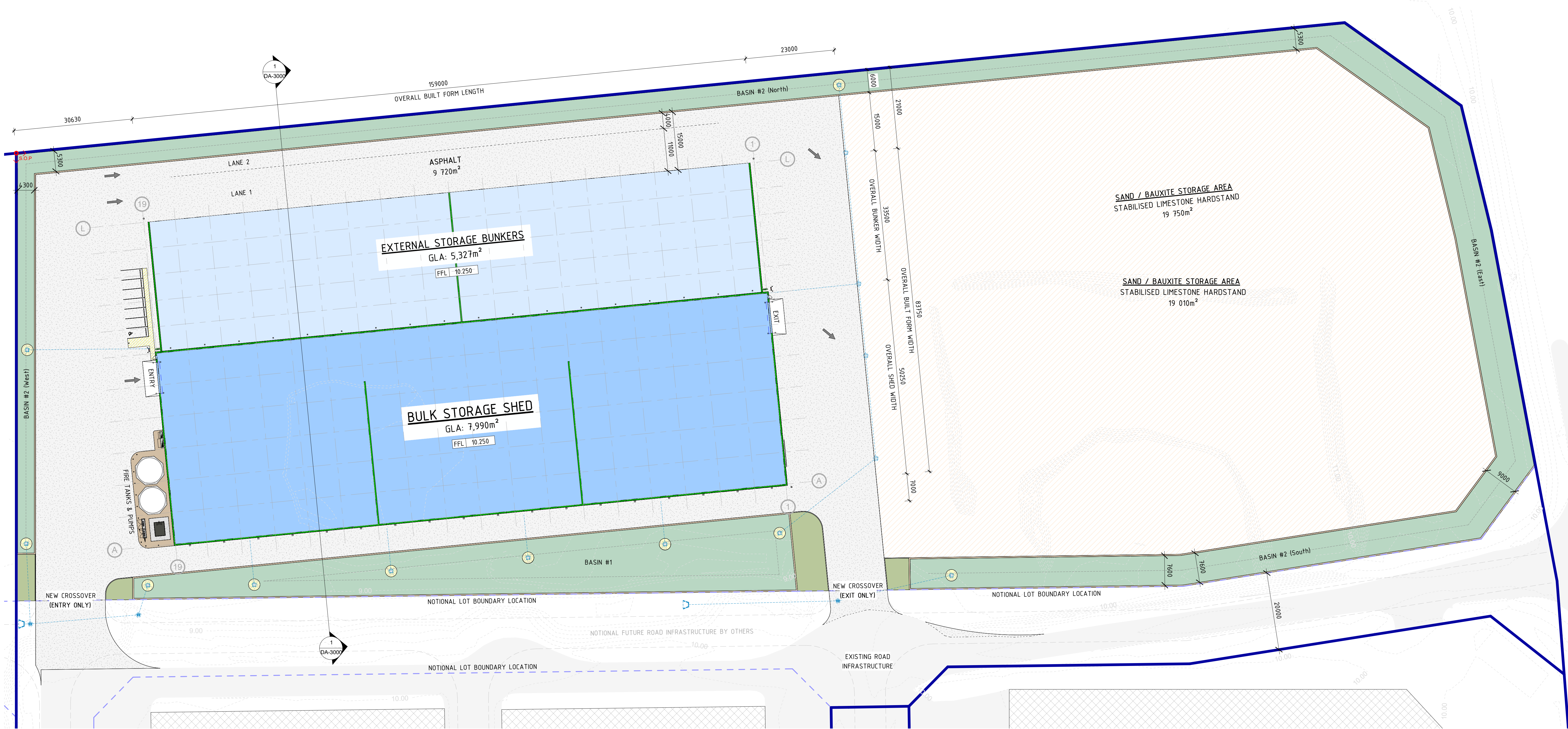
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COVER PAGE

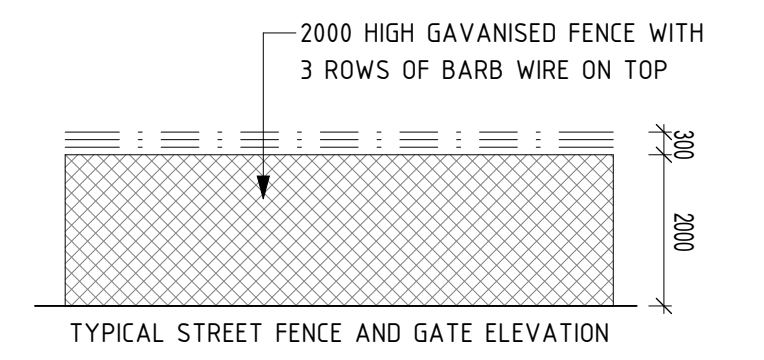
Project Number **T24072**
Revision **D**
Date **2025.09.19**
Scale **1 : 2500 @ A1**

SHEET NO.
DA-0001
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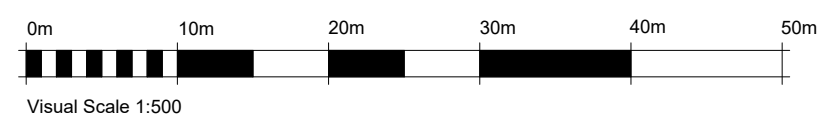


3 Site Plan of Proposed Development

1 : 500



FENCE TYPE FN-01
SCALE 1:100



SITE INFORMATION	
LOT NUMBER	260
PHYSICAL ADDRESS	390 Willinge Drive, High Wycombe Glen Iris
TOTAL LOT AREA	9.028 ha
DEPOSITED PLAN	413537
BUILDING USE	Bulk Storage Shed
BCA CLASS	Class 7b

SITE DEVELOPMENT SUMMARY	
AREA OF WORKS	48 855m ²
BUILDING AREA	Warehouse (GLA) 7 990 m ² External Storage Bunker/Cell 5 327 m ² TOTAL 13 317 m²

PARKING	
BAYS PROVIDED	Standard LV Parking 6 Accessible Bay 1 Truck Parking 0 TOTAL BAYS 7

LANDSCAPING	
AREA OF WORKS	48 855m ²
AREA SCHEDULE	Landscaping (ext) 297 m ² Landscaping (int) 111 m ² Swale 6 757 m ² TOTAL 7 165 m²
COVERAGE	REQUIRED 5% of 48 855.0m ² = 2 442.7m ² ACTUAL 7 165 m² (14.67%)

NOTES

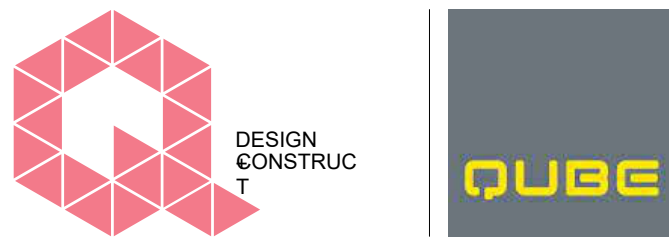
- SWALE CALCULATIONS ARE AN ESTIMATE ONLY, SUBJECT TO GEOTECHNICAL REPORTS & DESIGN BY CIVIL ENGINEER.
- ALL TRUCK SWEEPS ARE INDICATIVE ONLY, SUBJECT TO TRAFFIC ENG. CONSULTANT

42m TRIPLE

LEGEND	
	LANDSCAPED AREA PM1 - FEATURE
	LANDSCAPED AREA PM2 - DRAINAGE BASIN
	GRAVEL
	STABILISED LIMESTONE
	PAINTED LINE MARKING
	WAREHOUSE
	CONCRETE BUNKER
	NEW ASPHALT
	EXISTING ASPHALT

DEVELOPMENT APPLICATION

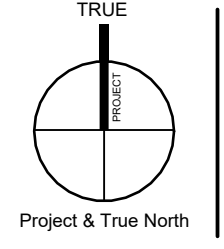
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QUBE PICTON PLP - BULK STORAGE SHED & TEMP HARDSTAND PROPOSAL

LOT 260 (NO.390) WILLINGE DRIVE, GLEN IRIS, 6230
FOR QUBE BULK

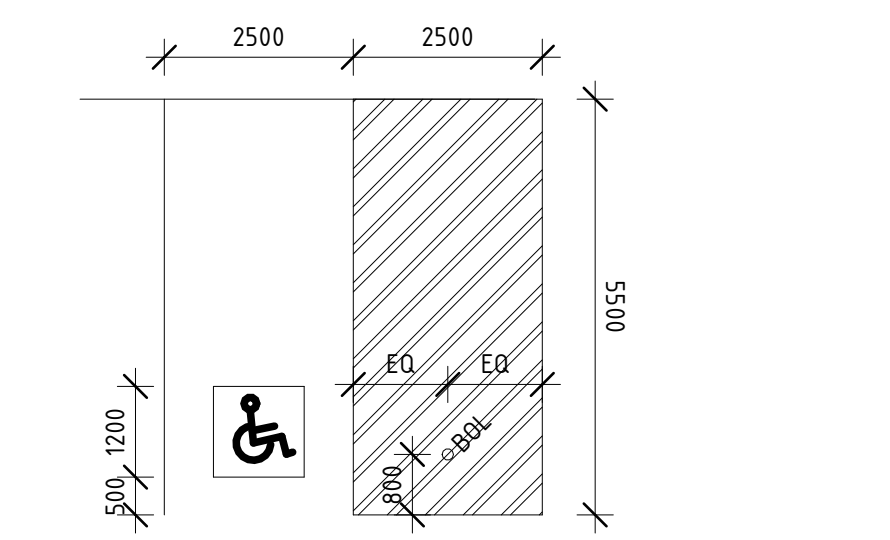
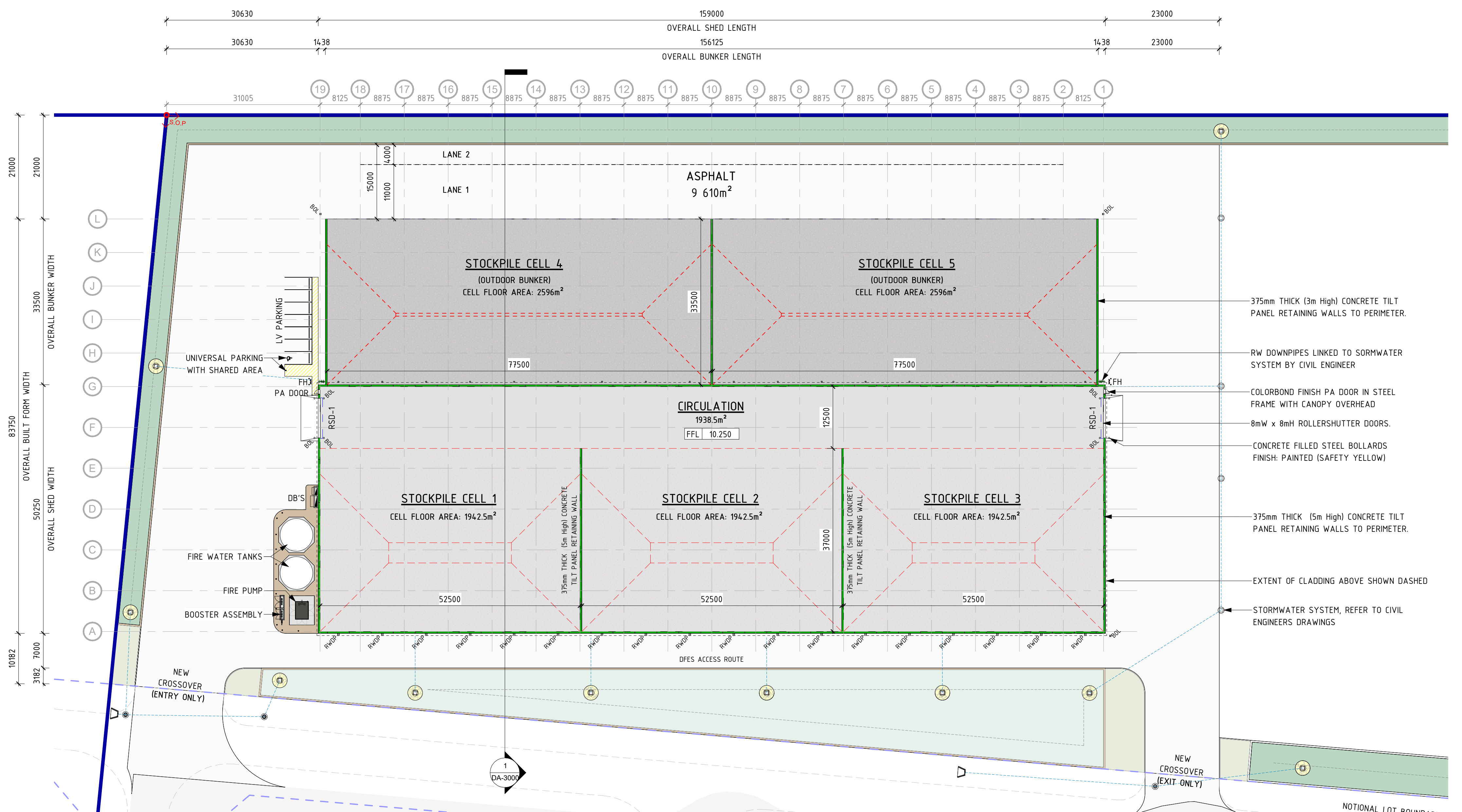
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SITE PLAN

Project Number	T24072
Revision	D
Date	2025.09.19
Scale	As indicated @ A1

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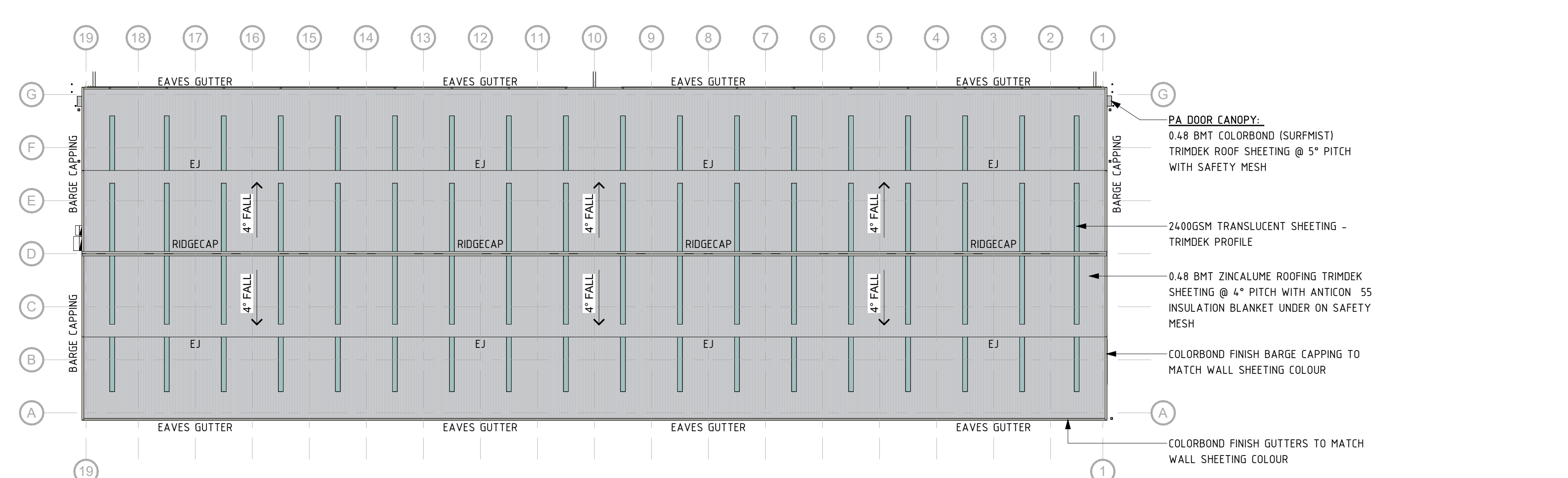
BOLLARD SHALL BE MIN. 1300MM IN HEIGHT & POSSESS A SURFACE 30% LUMINANCE CONTRAST AS PER AS2890.1
 INTERNATIONAL SYMBOL FOR ACCESS AS PER AS1428.1; AS2890.6
 LINEMARKING OF DEDICATED SPACE TO BE ALTERNATIVE COLOUR (BLUE OR YELLOW) TO THE GENERAL PARKING BAYS AS PER AS2890.

UNIVERSAL CAR PARKING BAY WITH SHARED AREA
 SCALE 1:100

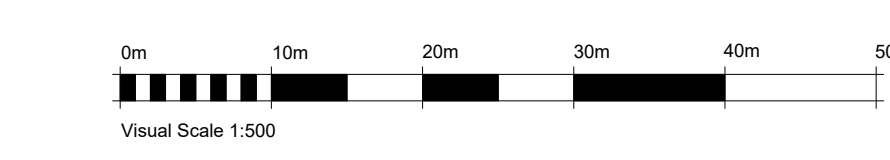
DESIGN CRITERIA	MINERAL SANDS			
	INDOOR CELLS 1, 2 & 3	OUTDOOR CELLS 4 & 5		
		PER CELL	TOTAL FOR 3 CELLS	PER CELL
Dimension of open site (W)	52.5m	77.5m	77.5m	77.5m
Dimension of other side (L)	37.0m	3.0m	33.5m	3.0m
Retained Height (Hr)	4.7m	11.0m	11.0m	11.0m
Total Height (Ht)	11.0m	30°	30°	30°
Angle of Repose (Ar)	30°	2.0mt/m³	2.0mt/m³	2.0mt/m³
Bulk Density (BD)	2.0mt/m³	11,479m³	34,437m³	15,053m³
Total Volume (V)	22,957mt	68,874mt	30,105mt	60,210mt
Total Mass (M)	1,942.5m³	5,827.5m³	2,596m³	5,192m³
Stockpile Floor Area				

PLEASE NOTE: ALL VOLUMES & TONNAGES ARE AN ESTIMATE

1 Floor Plan
 1 : 500

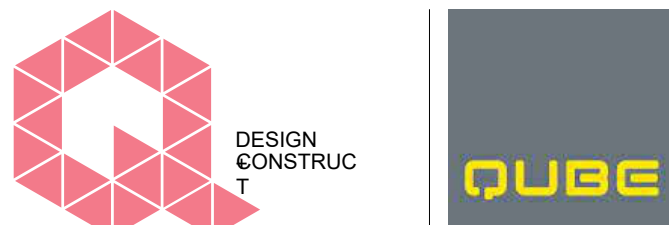


2 Roof Plan
 1 : 500



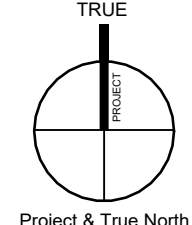
DEVELOPMENT APPLICATION

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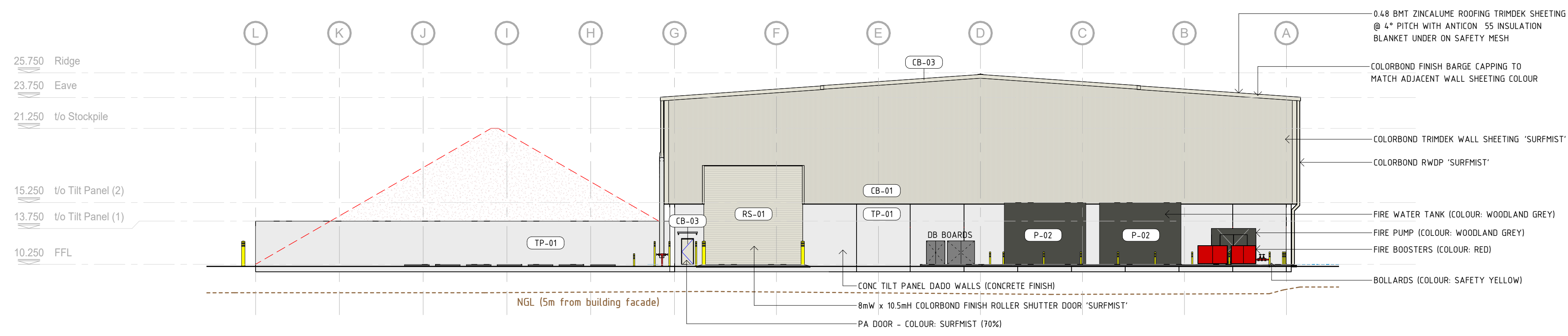
QUBE PICTON PLP - BULK STORAGE SHED & TEMP HARDSTAND PROPOSAL
 LOT 260 (NO.390) WILLINGE DRIVE, GLEN IRIS, 6230
 FOR QUBE BULK

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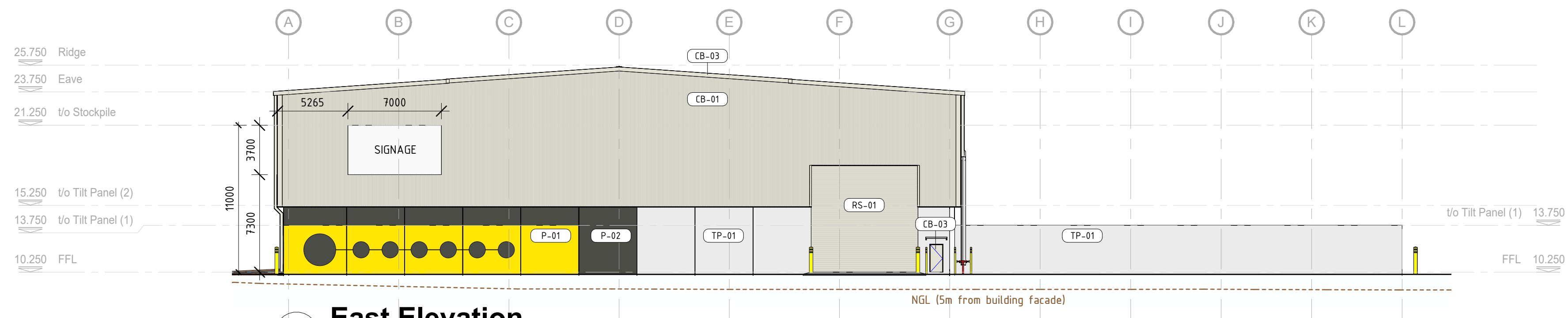


GROUND FLOOR & ROOF PLAN

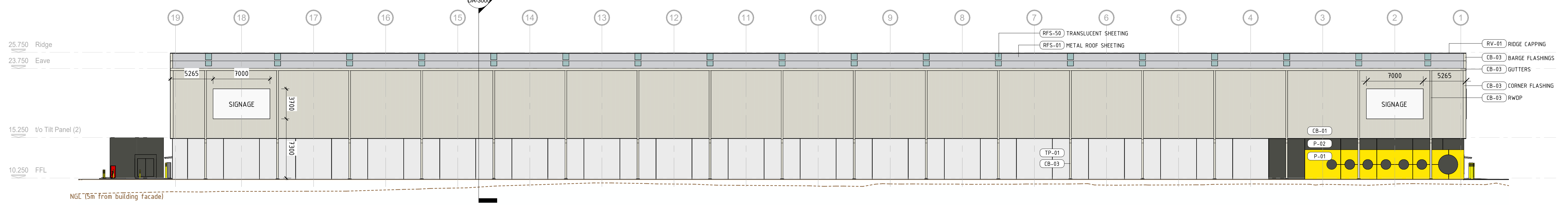
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Revision	D		
Date	2025.09.19		
Scale	As indicated @ A1		



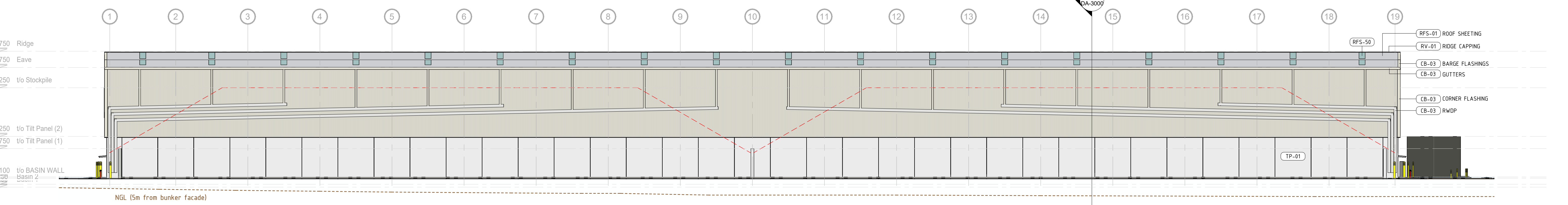
1 West Elevation
1 : 250



2 East Elevation
1 : 250

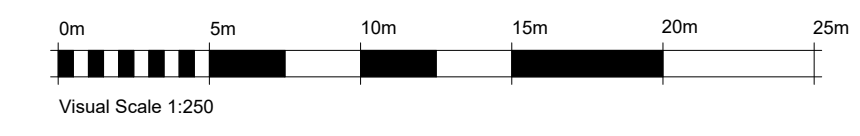


4 South Elevation
1 : 250



3 North Elevation
1 : 250

LEGEND	
TP-01	CONCRETE TILT PANEL WALLS WITH CONCRETE FINISH
P-01	PAINT FINISH COLOUR: CUBE YELLOW RGB: 255, 210, 0
P-02	PAINT FINISH COLOUR: WOODLAND GREY
CB-01	TRIMDEK WALL CLADDING IN COLORBOND SURFMIST
CB-02	TRIMDEK WALL CLADDING IN COLORBOND WOODLAND GREY
CB-03	COLORBOND IN SURFMIST
RS-01	POWDERCOATED ROLLER SHUTTER IN SURFMIST
RFS-01	ZINCALUME ROOF SHEETING
RFS-50	TRANSLUCENT ROOF SHEETING
RV-01	ZINCALUME RIDGE CAP



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QUBE PICTON PLP - BULK STORAGE SHED & TEMP HARDSTAND PROPOSAL
LOT 260 (NO.390) WILLINGE DRIVE, GLEN IRIS, 6230
FOR QUBE BULK

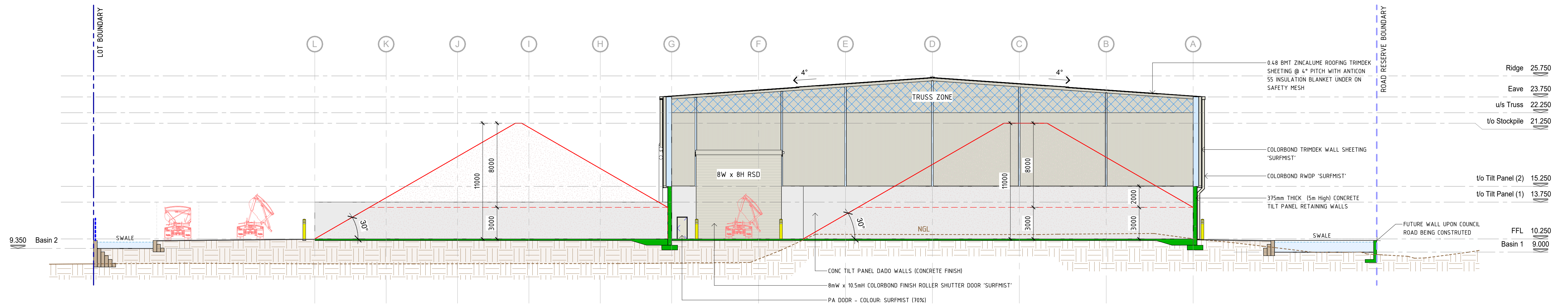
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Project & True North

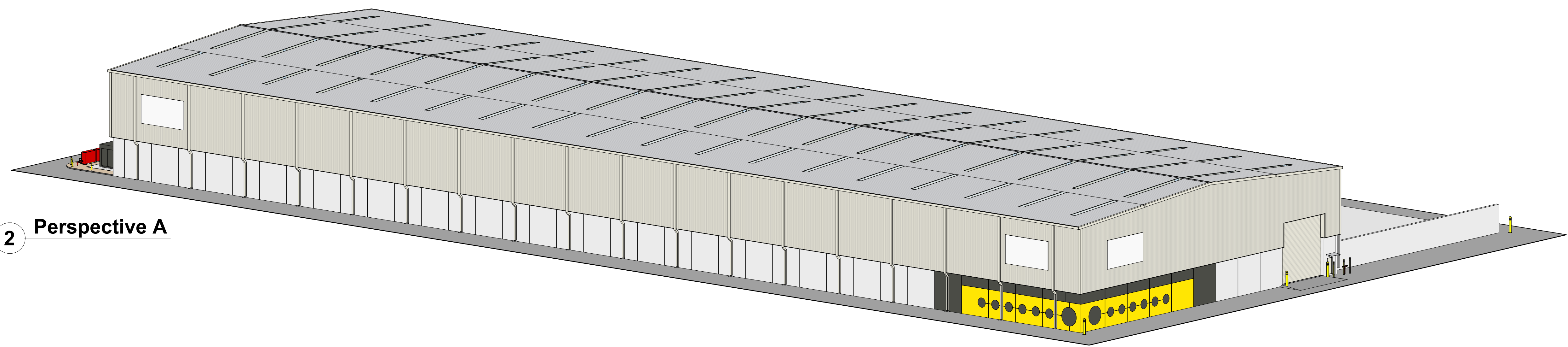
ELEVATIONS

Project Number **T24072**
Revision **D**
Date **2025.09.19**
Scale **As indicated @ A1**

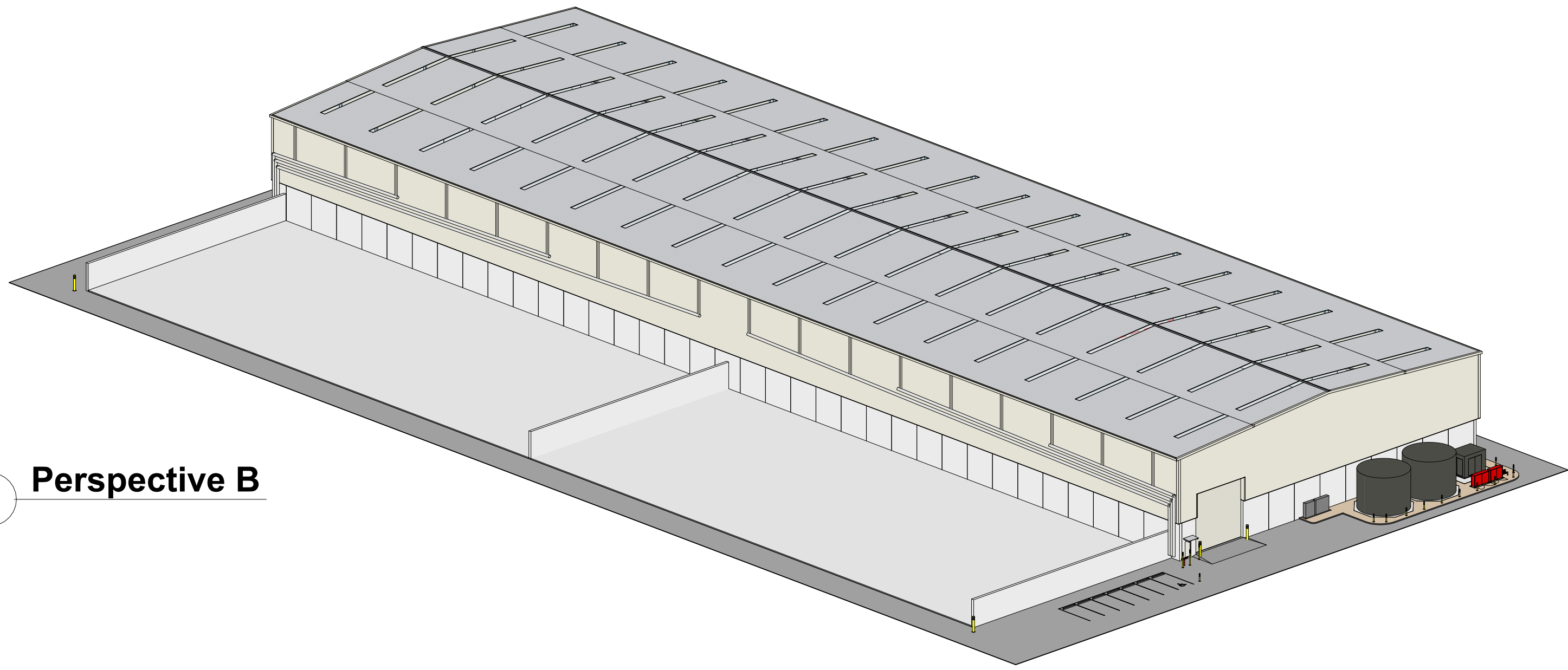
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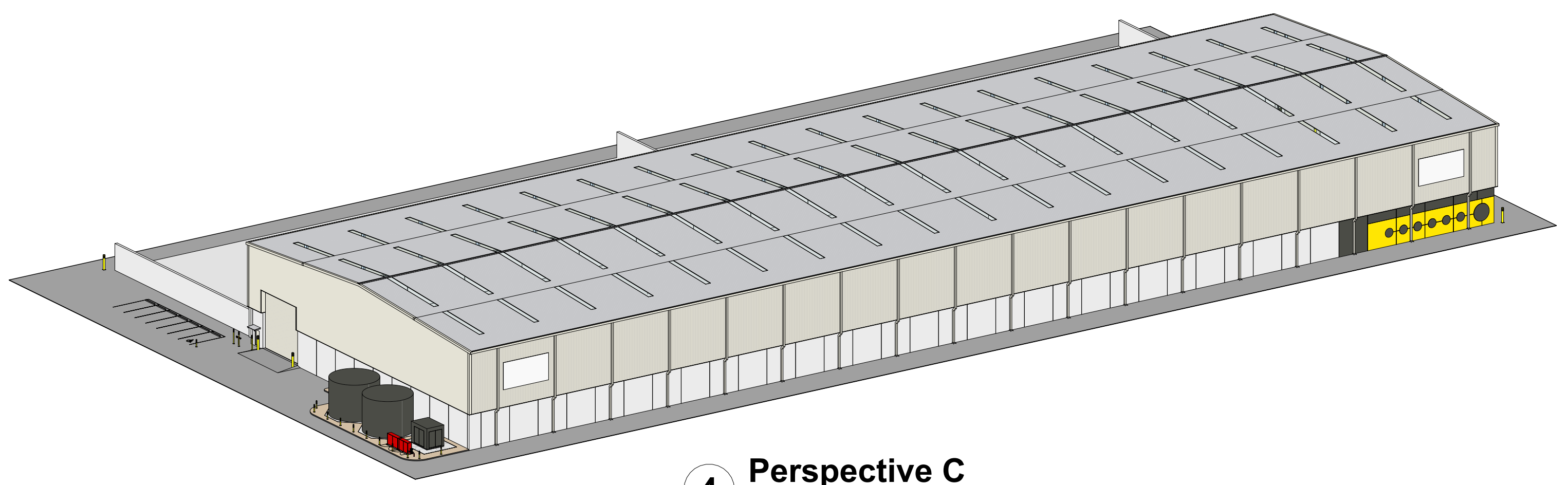
1 Section 1
1 : 200



2 Perspective A



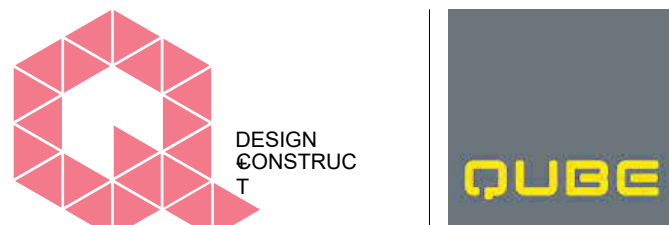
3 Perspective B



4 Perspective C

ISSUED FOR
DEVELOPMENT APPLICATION

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QUBE PICTON PLP - BULK STORAGE SHED & TEMP HARDSTAND PROPOSAL
LOT 260 (NO.390) WILLINGE DRIVE, GLEN IRIS, 6230
FOR QUBE BULK

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Project & True North

SECTIONS

Project Number	T24072
Revision	D
Date	2025.09.19
Scale	1 : 200 @ A1

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1 Landscaping Plan

1 : 500

LEGEND	
	PLANT MIX 1 (GENERAL) 2 plants per m ² 297m ²
	PLANT MIX 2 (DRAINAGE SWALE/ BASIN) 2 plants per m ² 6,692m ²
	GRAVEL 111m ²
	STONE PITCHING 65m ²
	STABILISED LIMESTONE
	ASPHALT
	EXISTING ASPHALT
	EXISTING SURFACE FINISH
	EXISTING STONE PITCHING
	EXISTING BUILDINGS

PLANT MIX 1 - FEATURE PLANTING	
	EREMOPHILA GLABRA Common Name: Kalbarri Carpet Size: 0.2m high x 2m spread
	CONOSTYLIS CANDIDANS Common Name: Grey Cotton Head Size: 0.5m high x 0.5m spread
	ANIGOZANTHOS FLAVIDUS Common Name: Tall Kangaroo Paw (Yellow) Size: 2m high x 1m spread
	LEUCOPHYTA BROWNII Common Name: Cushion Bush Height: 1m high x 1m spread
GENERAL NOTES <ul style="list-style-type: none"> • 2 plants per m² • 140mm pots • Pine Bark Mulch 	

PLANT MIX 2 - DRAINAGE SWALES/BASINS	
	JUNCUS KRAUSSII Common Name: Sea Rush Size: 1m high x 1m spread
	BAUMEA JUNCEA Common Name: Bare Twig Rush Height: 1m high x 1m spread
	FIGINIA NODOSA Common Name: Knobby Club Rush Height: 1m high x 1m spread
	LEPIDOSPERMA GLADIATUM Common Name: Coast Sword Sedge Height: 1m high x 1m spread
GENERAL NOTES <ul style="list-style-type: none"> • 2 plants per m² • Tubestock Planting • Bioretention as per civil eng. requirements 	

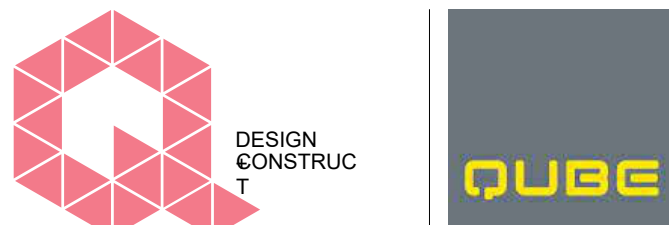
TREE LEGEND	
	AGONIS FLEXUOSA Common Name: Weeping Peppermint Size: 4-10m (h) x 4-10m (w) *medium - large
<p>A fast-growing WA native with weeping branches that provide excellent shade, wind protection, and are known to prevent soil erosion. It tolerates most soils and can live for a long time. It blooms small white flowers in spring.</p>	
GENERAL NOTES <ul style="list-style-type: none"> • 35L size 	

NOTES	
TREE NOTES: <ul style="list-style-type: none"> • TREE LOCATION INDICATIVE PENDING SERVICES LOCATIONS AND SIGHT-LINE ANALYSIS. • TREES SPECIES TO BE MIXED TO CREATE AN INFORMAL NATURAL AESTHETIC • ALL TREES TO BE STAKED AND TIED 	
PLANTING NOTES: <ul style="list-style-type: none"> • PLANTING MIX LOCATIONS SHOWN ON DA-L001 • UNDERSTOREY PLANTING TO BE MASS PLANTED, AS A RANDOM MIX TO FURTHER ENHANCE THE NATURAL AESTHETIC. • ALL MULCH TYPES TO BE A MINIMUM DEPTH OF 100MM WITH 50MM BREATHING SPACE AROUND STEMS AND TRUNKS OF PLANTS. 	
IRRIGATION NOTES: <ul style="list-style-type: none"> • IRRIGATION TO ALL LANDSCAPE AREAS. • POP - UP FLOOD BUBBLERS TO SUIT SIZE OF GARDEN BEDS TO ENSURE ANY OVER-SPRAY AND WASTAGE IS MITIGATED. • ALLOW FOR SLEEVES UNDER ROAD TO IRRIGATION LANDSCAPE. SLEEVES TO BE DN200, INSTALLED 750MM BELOW GROUND AND EXTEND 600MM BEYOND THE KERB - ENDING IN SOFT LANDSCAPE AREA • IRRIGATION MAY BE SWITCHED OFF OR REDUCED BEYOND THE TWO YEAR MAINTENANCE PERIOD 	
GENERAL NOTES: <ul style="list-style-type: none"> • FENCING - REFER ARCHITECTURAL AND CIVIL DRAWING PACKAGES; • WALLING - REFER CIVIL DRAWING PACKAGE FOR WALLING REQUIREMENTS; • WHERE FERTILISER IS REQUIRED - SLOW RELEASE FERTILISER SHOULD BE APPLIED 	
IF APPLICABLE: <ul style="list-style-type: none"> • ATU RETICULATION - AREA TO BE DESIGNED IN ACCORDANCE WITH THE RELEVANT STANDARDS AND GUIDELINES; • BUSHFIRE MANAGEMENT - ALL LANDSCAPE TO BE MAINTAINED TO MEET THE LOW THREAT BUSHFIRE STATE; • OVERHEAD POWERLINES - LOCATED ON STREET VERGE TO BE CONSIDERED • CROSSOVERS - TO BE TO SHIRES SPECIFICATION 	

LANDSCAPING		
AREA OF WORKS		48 855m ²
AREA SCHEDULE	Landscaping _{plant}	297 m ²
	Landscaping _{swale}	111 m ²
	Swale	6 757 m ²
	TOTAL	7 165 m²
COVERAGE	REQUIRED	5% of 48 855.0m ² = 2 442.7m ²
	ACTUAL	7 165 m ² [14.67%]

DEVELOPMENT APPLICATION

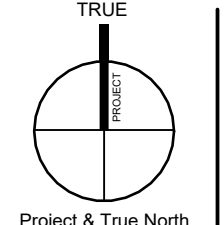
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QUBE PICTON PLP - BULK STORAGE SHED & TEMP HARDSTAND PROPOSAL

LOT 260 (NO.390) WILLINGE DRIVE, GLEN IRIS, 6230 FOR QUBE BULK

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LANDSCAPING PROPOSAL

Project Number	T24072	SHEET NO.	
Revision	D	DA-L001	
Date	2025.09.19		
Scale	As indicated @ A1		

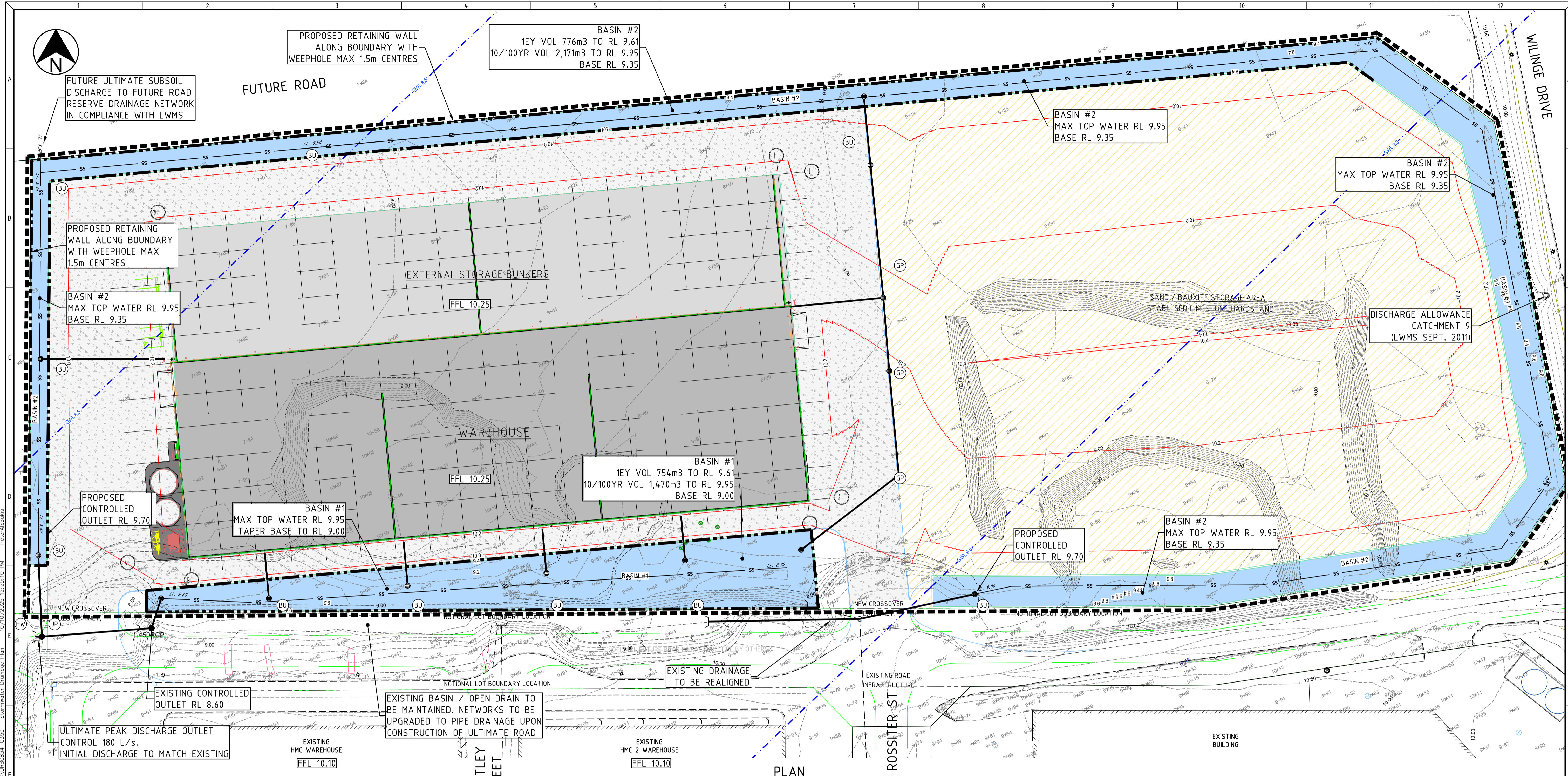
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Attachment B

REVISED STORMWATER DRAINAGE PLAN





10/10/2025 12:29:10 PM PeterAlebakis
 C:\Users\PeterAlebakis\URBANISE WA PTY LTD\URB0834 - T24072 - Qube Picton Kangaroo Pond - Design\Deliverables\DWG\URB0834-C350 - Stormwater Drainage Plan

NOTES

- THIS DRAWING SHALL BE READ IN CONJUNCTION WITH ALL URB0779 DRAWINGS, SPECIFICATION
- THE CONTRACTOR SHALL LOCATE ALL EXISTING SERVICES WITHIN THE CONTRACT AREA PRIOR TO THE COMMENCEMENT OF WORK. WHERE EXISTING AND PROPOSED WORKS INTERSECT, LEVELS ARE TO BE TAKEN AND SUPPLIED TO THE SUPERINTENDENT.
- ALL DIMENSIONS ARE IN METRES UNLESS SHOWN OTHERWISE.
- THE CONTRACTOR SHALL LOCATE ALL LEVELS FROM ESTABLISHED BENCH MARKS.
- ALL BENCH MARKS ARE TO BE PROTECTED AND PRESERVED.
- ALL STORMWATER DRAINAGE PIPES ARE TO BE RC CLASS 2 INSTALLED TO MANUFACTURES SPECIFICATIONS. UNLESS OTHERWISE NOTED.
- ALL STORMWATER DOWNPIPES PIPES SHALL BE PVC WITH MIN STIFFNESS SN8.
- ALL TRENCHING, PIPE BEDDING AND BACKFILLING SHALL BE IN ACCORDANCE WITH AS3725.
- STORMWATER PIPE ALIGNMENT SHALL BE AS INDICATED ON THE PLANS AND LIASION WITH STRUCTURAL ON FOOTING LOCATIONS.
- ALL JUNCTION PITS AND ENTRY PITS SHALL BE LOCATED AS SHOWN IRRESPECTIVE OF PIPE LENGTHS SHOWN ON THE DRAWINGS.
- AT LOW POINTS THE ENTRY PITS SHALL BE LOCATED AT THE LOW POINT OF THE PAVEMENT.
- THE CONTRACTOR SHALL IMMEDIATELY REPORT ANY DISCREPANCY OR CLASH WITH OTHER SERVICES TO THE SUPERINTENDENT.

LEGEND

- PROPOSED DEVELOPMENT BOUNDARY
- PROPOSED JUNCTION PIT
- EXISTING DRAINAGE NETWORK
- PROPOSED GULLY / BUBBLE UP PIT
- (BU) DENOTES PIT REFERENCE NUMBER
- GP - GULLY PIT
BU - BUBBLE UP PIT
PS - PUMP PIT
JP - JUNCTION PIT
HW - HEADWALL
- PROPOSED SUBSOIL AND INVERT LEVEL
- 23.0 PROPOSED FINISHED SURFACE LEVEL
- 13.0 DENOTES EXISTING SURFACE CONTOURS
- DENOTES EXISTING SURFACE SPOT LEVEL
- DENOTES ROADWAY KERB / EDGE
- DENOTES EXISTING BUILDINGS
- DENOTES GROUND WATER CONTOURS
- DENOTES DETENTION STORAGE BASIN

PLAN
SCALE 1:500



ISSUED FOR APPROVAL
 NOT TO BE USED FOR CONSTRUCTION PURPOSES

STORMWATER CALCULATIONS	
DESCRIPTION	ULTIMATE
CATCHMENT	4,395m ²
PEAK DISCHARGE TO MAIN DRAIN	180 L/s
STORAGE REQUIRED (INCLUDING EXISTING BASINS WITHIN CATCHMENT 10 & 11 - LWMS SEPT 2011)	
1EY STORM	1,491m ³
10yr-100yr STORM	1,413m ³
TOTAL NETWORK STORAGE REQUIRED	2,904m ³
PROPOSED BASIN CAPACITIES	1EY 10yr-100yr
BASIN 1	754m ³ 716m ³
BASIN 2	776m ³ 1,395m ³
SUB-TOTAL NETWORK CAPACITY	1,530m ³ 2,111m ³
TOTAL NETWORK CAPACITY	3,641m ³

REV No.	DATE	DRAWN BY	REVIEWED BY	AMENDMENT DESCRIPTION
E	10/10/25	PA	PA	AMENDED AS PER CLIENT COMMENTS AND ISSUED FOR APPROVAL
D	12/09/25	PA	PA	DESIGN AMNDND TO SUIT NEW LAYOUT AND CITY COMMENTS
C	14/05/25	PA	PA	VOLUMES AMENDED, ISSUED FOR APPROVAAL
B	12/05/25	PA	PA	FSL CONTOURS AND SUBSOIL ADDED, BASIN 2 LABEL UPDATED
A	05/05/25	PA	FL	ISSUED FOR APPROVAL

Perth
 Westpoint Centre
 207 / 396 Scarborough Beach Road
 Osborne Park WA 6017
Margaret River
 Wave Court
 7 / 30 Fernside Ave
 Margaret River WA 6285

ENGINEERING SOLUTIONS

CLIENT	QDC						
PROJECT	T24072 - QUBE PICTON KANGAROO POND						
TITLE	STORMWATER DRAINAGE MANAGEMENT PLAN						
DRAFTER	P.ALEBAKIS	DESIGNED	P.ALEBAKIS	APPROVED PROJECT MANAGER		APPROVED PROJECT DIRECTOR	
DATUM	AHD	SCALE	1:500	PROJECT No.	URB0834	DRAWING No.	C350
GRID	NONE	WAPC No.	N/A			REV	E

PTG02151

Transport Impact Statement Lot 260 (No.390) Willinge Drive, Glen Iris

April 2025 | Revision A

Prepared for QDC Pty Ltd

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REPORT DETAILS

Unique Document Identification

	Information
Document Title	TIS - Lot 260 (No.390) Willinge Drive, Glen Iris
Project Number	PTG02151
Document ID	Rev A
Client	QDC Pty Ltd

Revision Details

Revision No.	Date	Comments
Rev A	30/4/25	For Issue

1 INTRODUCTION

1.1 Background

PTG Consulting WA (PTG) has been commissioned by QDC Pty Ltd to prepare a Traffic Impact Statement (TIS) for the proposed development of a Bulk Storage Facility (Shed and Bunkers) at Lot 260 (No.390) Willinge Drive, Glen Iris, WA 6229.

This report has been prepared in accordance with the Western Australian Planning Commission (WAPC) Transport Assessment Guidelines for Developments: Volume 4 – Individual Developments (2016) and the Transport Impact Statement (TIS) Checklist is included at **Appendix A**.

Specifically, this report aims to assess the operations of the proposed development internally and its connections to the adjacent road network, with a focus on traffic volumes, access and accessibility.

This report also outlines the requirements and opportunities associated with traffic and transport within the development, referencing relevant Council and WAPC policies and guidelines as well as best-practice planning within Western Australia.

2 PROPOSED DEVELOPMENT

2.1 Existing Land Use

The Site is located at Lot 260 (No.390) Willinge Drive, Glen Iris, WA 6229. The Site location is shown in Figure 1.

Figure 1 Site Location

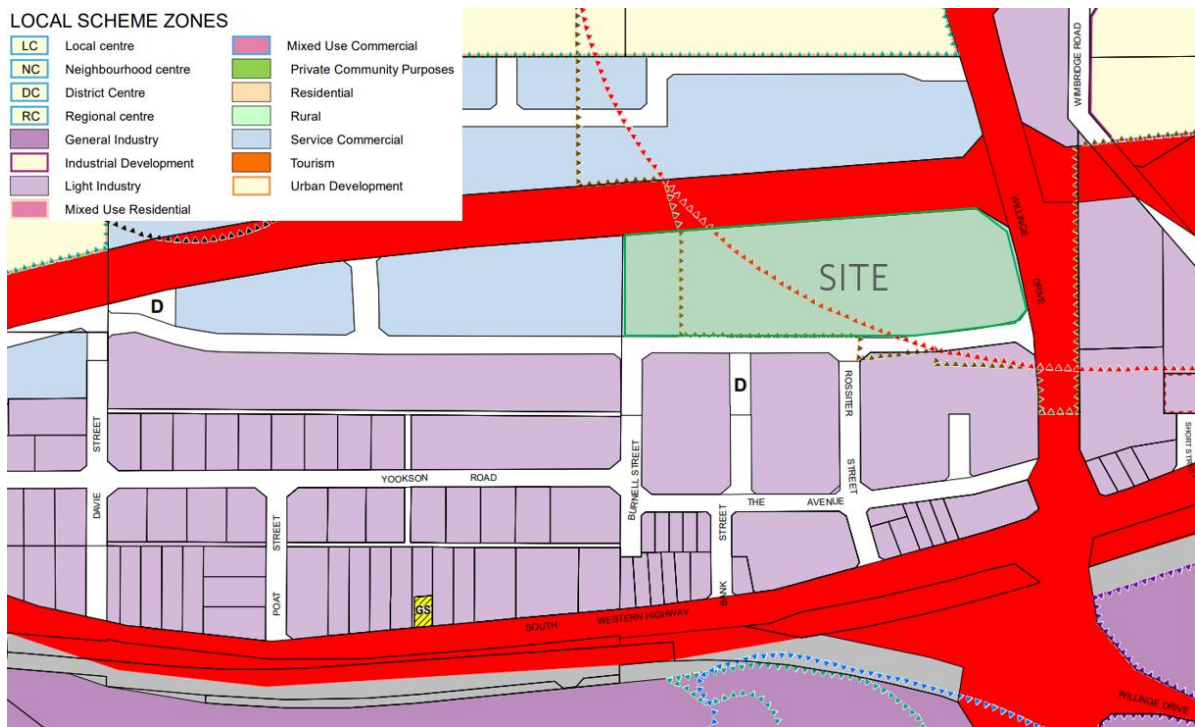


Source: Nearmap

2.2 Context with Surrounds

According to the *City of Bunbury Local Planning Scheme No.8*, the Site is currently zoned as "Service Commercial". There are no major attractors and generators located within 800m of the Site. Surrounding land uses include General Industrial and Service Commercial. Figure 2 shows the zoning within the surrounding area of the Site.

Figure 2 Land Use Zoning within the Surrounding Area of the Site



Source: WAPC - City of Bunbury Local Planning Scheme No.8 (Map 9)

2.3 Existing Site Generation and Land Uses

The existing site is a vacant land currently used for truck parking, see Figure 1.

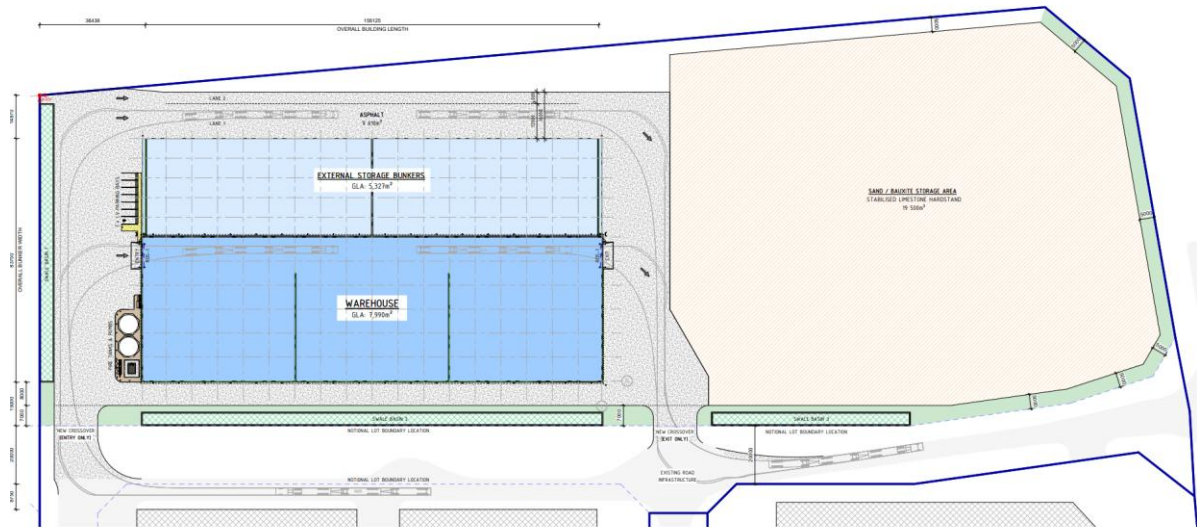
2.4 Proposed Land Use

The proposed development is an expansion to the existing Qube Ports and Bulk site, includes:

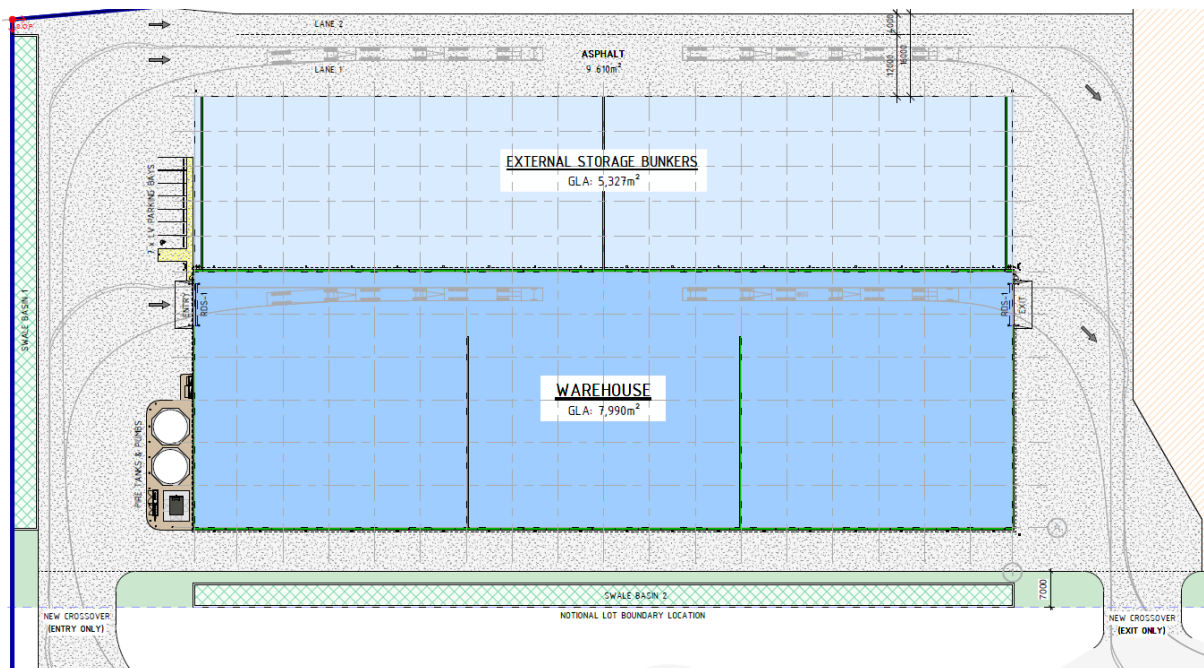
- » 5 new stockpile cells (2 outdoor bunkers and 3 storage sheds); and
- » A storage limestone hardstand (19,500m²)

Details and locations of the proposed uses are shown in Figure 3.

Figure 3 Proposed Development



3 Site Plan of Proposed Development
1:500



Source: QDC

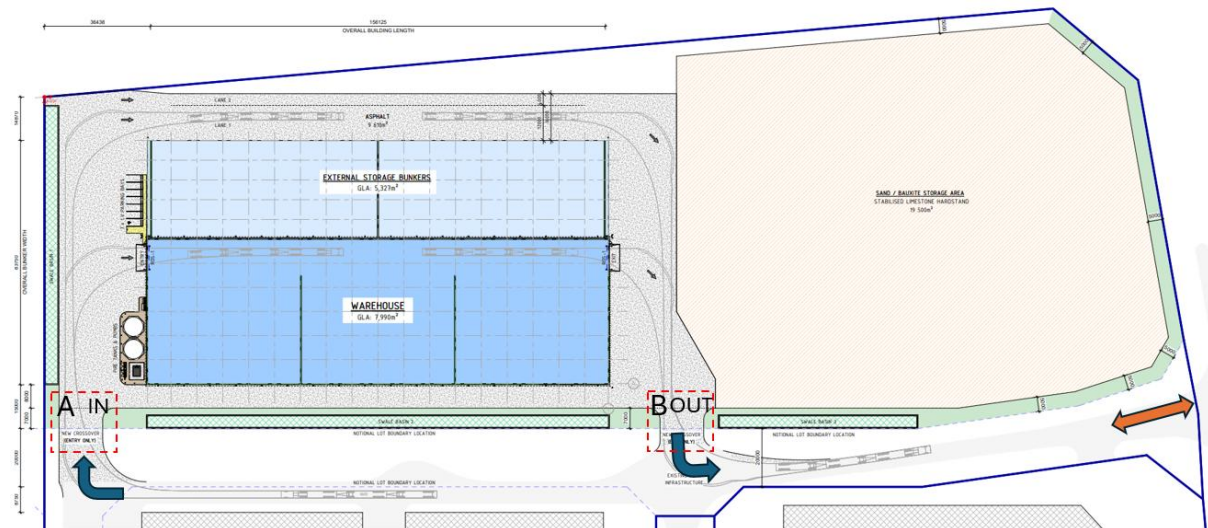
3 VEHICULAR ACCESS AND PARKING

3.1 Access Arrangements

The proposed development will gain access via the existing Rossiter Street and Willinge Drive intersection. Two (2) new crossovers will be constructed onto the existing road infrastructure south of the Site (See Figure 4), as described below:

- » Access A - Inbound only
- » Access B - outbound only

Figure 4 Proposed Site Access



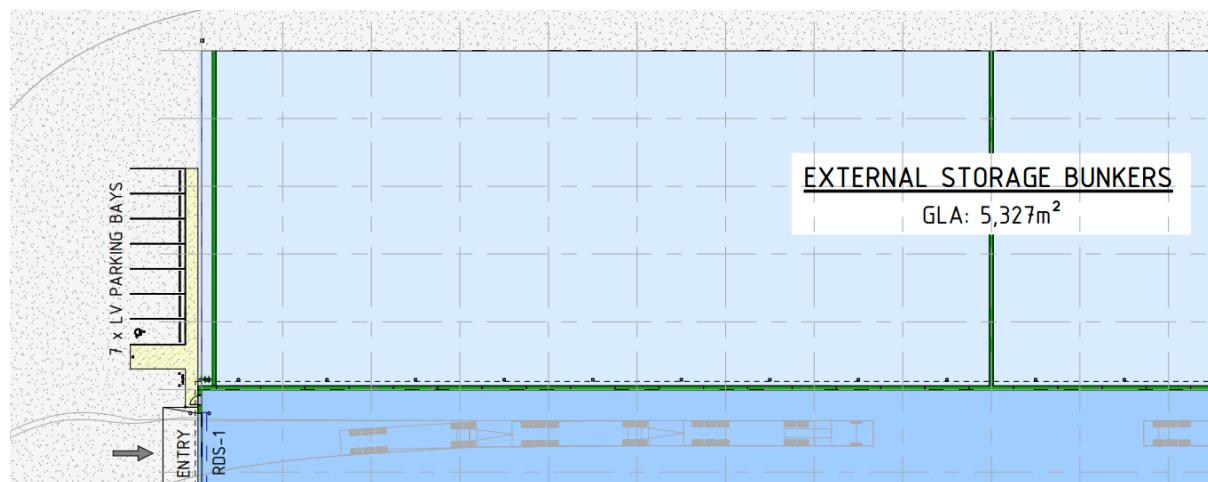
Source: QDC

3.2 Public, Private, Disabled Parking Set-Down & Pick-Up

The proposed development is a part of a larger site and will see 4 additional staff dedicated for this expansion. This is expected to generate a light vehicle parking demand of 4 bays.

The development proposed to install 7 light vehicle parking bays (including 1 ACROD bay) located to the west of the External Storage Bunkers. The proposed parking provision will satisfy the staff car parking demand, with some surplus bays to accommodate for occasional visitor parking demand.

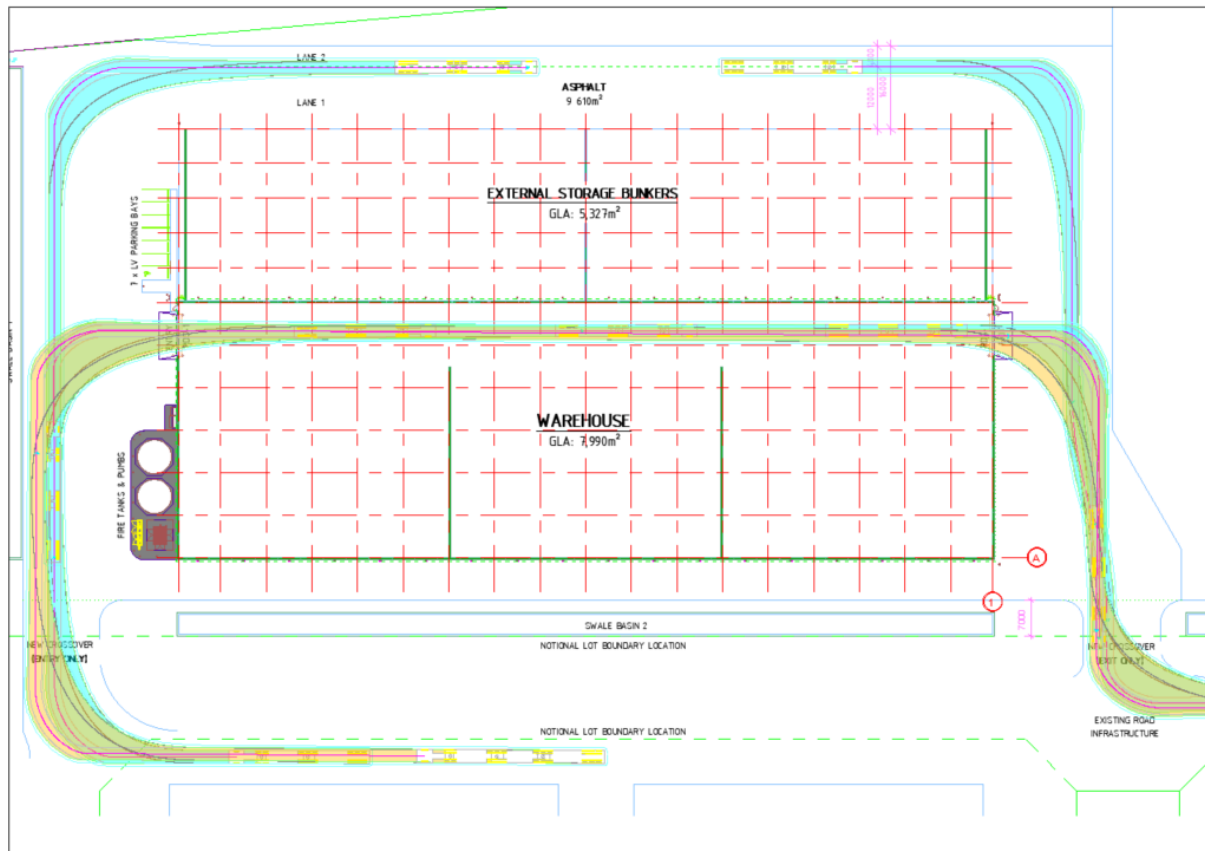
Figure 5 Proposed Car Parking Provision



3.3 Swept Path Assessment

Swept Path Assessment shown in Figure 6 shows that RAV4 and RAV7 could be accommodated on Site.

Figure 6 RAV 4 and RAV 7 Swept Path



4 SERVICE/DELIVERY VEHICLES (NON-RESIDENTIAL)

4.1 Access Arrangements

All service and delivery vehicles will use the same access as described in Section 3.1.

4.2 Loading Facilities

All loading and unloading will occur within the proposed building. The largest truck expected to attend the Site is a 42m B-triple (RAV 7).

5 SERVICE VEHICLES (RESIDENTIAL)

Not applicable as proposal is for General Industrial (see next section).

6 HOURS OF OPERATION (NON-RESIDENTIAL)

The development is anticipated to operate 24 hours per day, 365 days a year.

7 TRAFFIC VOLUMES

7.1 Development – Daily / Peak Traffic Volumes

Operational details are as follows:

No new extra dedicated staff for the proposed development

- » Average number of trucks per day = 9 trucks (18 movements)/day
- » Number of trucks expected during peak periods = up to 12 trucks (24 movements)/day max.

The trip generated by the additional heavy vehicles are expected to be distributed throughout the day and is unlikely to see multiple trucks arriving on Site within a short period of time. A heavy vehicle is expected to

Table 1 presents the likely traffic generation during each of the peak hours, assuming 20% of the daily truck traffic to occur during the peak hour(s).

Table 1 Trip Generation Rates

Land Use	Source	Yield	Unit	Trip Generation Rate (2 way)	
				AM	PM
Warehouse - trucks	Client	2	trucks	2 in, 2 out (total of 4)	2 in, 2 out (total of 4)
Warehouse - light vehicles		4	staff	4 in, 0 out (total of 4)	0 in, 4 out (total of 4)

*20% of daily traffic during peak period

Directional trip distribution rates and estimated trips generated are detailed in Table 2 and Table 3.

Table 2 Directional Trip Distribution Rates

Vehicle Type	AM Peak		PM Peak	
	In	Out	In	Out
General Industrial - trucks	50%	50%	50%	50%
General Industrial - LV	100%	0%	0%	100%

Table 3 Development Trip Generation

Vehicle Type	AM Peak		PM Peak	
Trucks	2	2	2	2
Light Vehicles	4	0	0	4
Total	8		8	

As can be seen from the above, the trip generation associated with the development is anticipated to be lower than 10 during the peak hour, which calls under the “Low Impact” category as per the Transport Impact Assessment Guidelines Vol 1 (WAPC).

7.2 Types of Vehicles

The proposed development will be generating light vehicles and some heavy vehicle movements, including Restricted Access Vehicle (RAV) classes 4 to 7.

8 TRAFFIC MANAGEMENT ON FRONTAGE STREETS

8.1 Existing Intersections

No changes are required or proposed for the existing intersections along South Western Highway or Willinge Drive as a result of this development.

8.2 Daily / Peak Traffic Volumes

Existing weekday traffic volumes were obtained from Main Roads WA Traffic Map for key road sections in the vicinity of the Site and are shown below in Table 4.

Table 4 Existing Traffic Volumes on adjacent Roads

Location	Year	Weekday Traffic Volumes (two-way)		
		Daily	AM Peak Hour	PM Peak Hour
Wilinge Drive (south of Winsor Rd)	2023/24	5,262	380	453
South Western Highway (east of Poat St)	2022/23	10,972	814	975

Source: Main Roads WA Traffic Map

8.3 Future Road Network

According to the Wimbridge Precinct Structure Plan, a road reserve has been set aside to the north of the Site for “Possible Future Picton Deviation”, which will eventually replace the South Western Highway for district level thoroughfare in the future. However, the timeframe of the road upgrade is currently unknown, and is outside of the known Main Roads WA Long-term plan. Figure 7 shows the approved road network in the vicinity of the Site, depicted in the Wimbridge Precinct Structure Plan.

It is noted that the development proposed a deviation to the previously approved road network within the Site. However, this will have minimal impact to the overall transport network as the Site is will not have a crossover onto the “Possible Future Picton Deviation”.

Figure 7 Wimbridge Precinct Structure Plan



Source: Wimbridge Precinct Structure Plan

11 SITE SPECIFIC ISSUES

The larger Site is currently accessed by RAV class 4 trucks. While the type of trucks is not proposed to change as a result of the proposed development, swept path was also undertaken for RAV class 7 trucks to demonstrate that these trucks would also be able to enter and exit the Site in forward-gear.

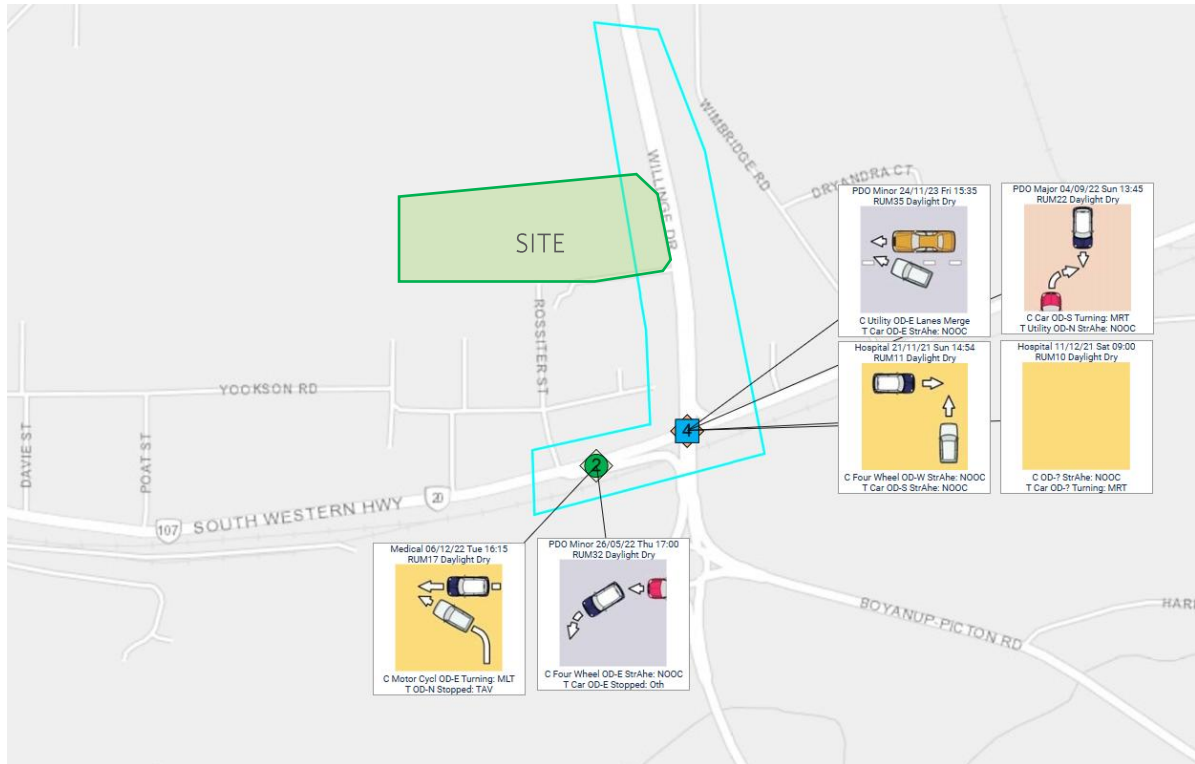
Overall, the Site is self-contained and is likely to operate as usual with minimal impact by the proposed development.

12 SAFETY ISSUES

The crash data obtained from the Main Roads WA Crash Map indicates that no crashes were recorded at the intersection of Willinge Drive / Rossiters Street (where the site access is located) in the previous 5-year period ending in December 2024.

There were 4 crashes recorded at the intersection of Willinge Drive and 2 along South Western Highway, west of Willinge Drive. (see Figure 9).

Figure 9 Recorded Crash Details



Severity	No.	%
Fatal	0	0
Hospital	2	33.33
Medical	1	16.67
PDO Major	1	16.67
PDO Minor	2	33.33

The proposed development has very low traffic generation and forms a part of an existing operation. It is therefore unlikely to have any impact on road safety in the area.

13 SUMMARY AND CONCLUSIONS

This report has been prepared in accordance with the Western Australian Planning Commission (WAPC) Transport Assessment Guidelines for Developments: Volume 4 – Individual Developments (2016); the checklist is included at **Appendix A**.

- » The predicted traffic generation of the development is less than 10 trips during the peak hour, which is considered “low impact” in accordance with the *WAPC TIA Guideline*.
- » Two new access points are proposed for the development, with low expected volume and minimal conflicting movements.
- » The proposed development is expected to generate an additional parking demand of 4 bays, which can be accommodated by the proposed provision of 7 light vehicle bays (including 1 ACROD bay).
- » Public transport and cycling facilities are not available in the surrounding area. However, given the nature of the warehouse development, the absence of these facilities is not considered critical for the Site.
- » Due to the nature of the development and surrounding land uses, any impact on road safety is expected to be negligible.



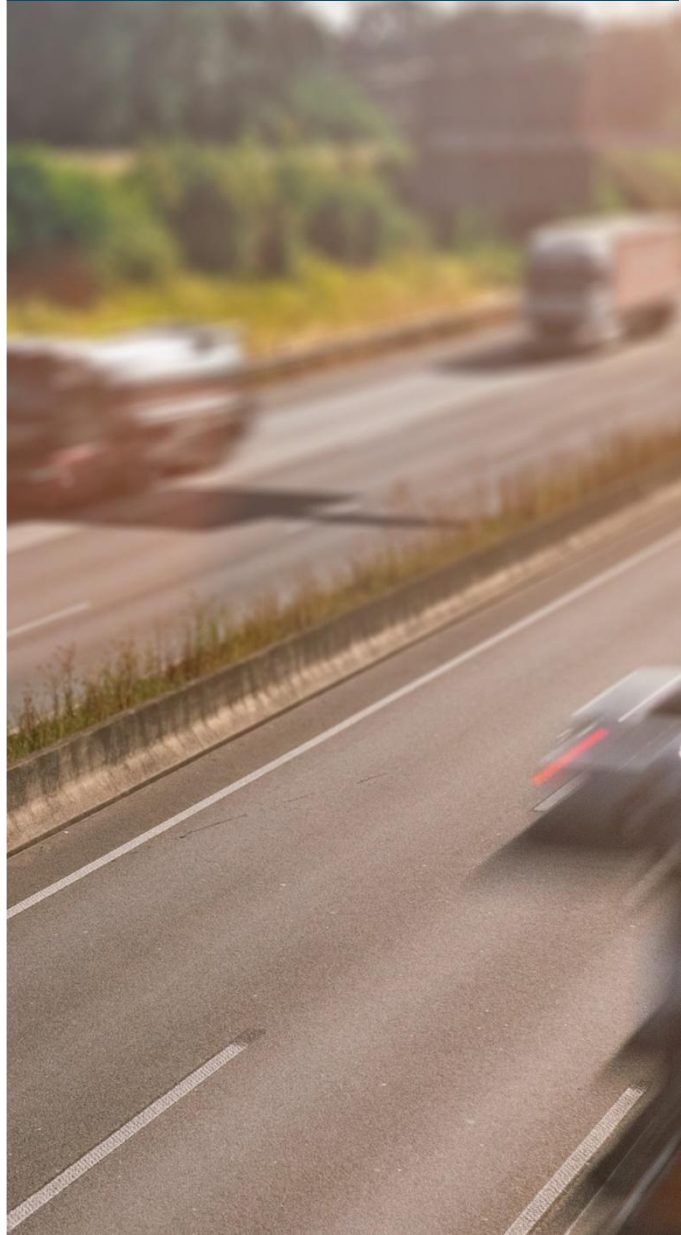
Appendix A

WAPC CHECKLIST FOR INDIVIDUAL
DEVELOPMENT - TRANSPORT
IMPACT STATEMENT



APPENDIX A

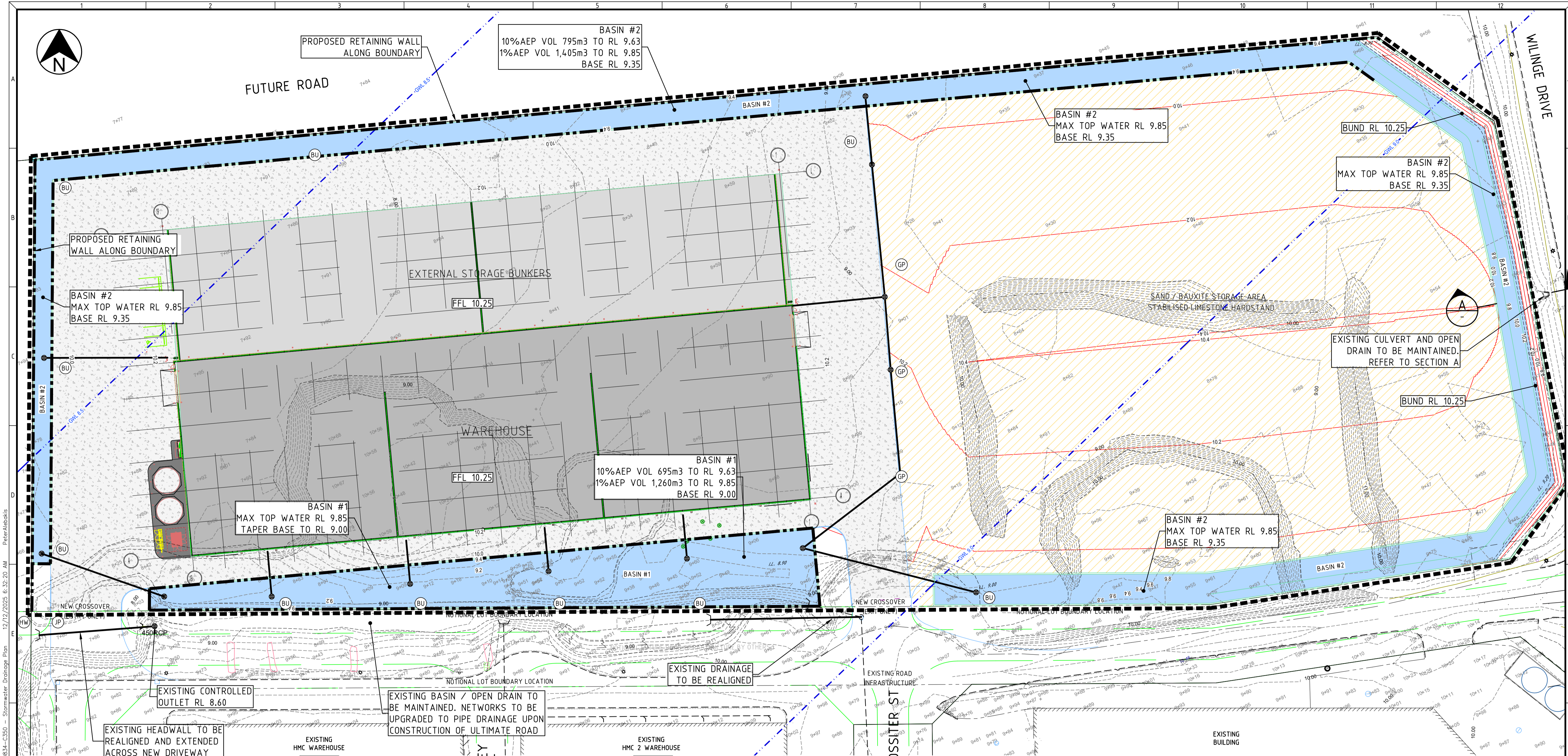
Item	Status	Comments/Proposal
Proposed development	Section 2	
proposed land use	Section 2	
existing land uses	Section 2	
context with surrounds	Section 2	
Vehicular access and parking	Section 3	
access arrangements	Section 3	
public, private, disabled parking set down / pick up	Section 3	
Service vehicles (non-residential)	Section 4	
access arrangements	Section 4	
on/off-site loading facilities	Section 4	
Service vehicles (residential)	Section 5	
Rubbish collection and emergency vehicle access	N/A	
Hours of operation (non-residential only)	N/A	
Traffic volumes	Section 7	
daily or peak traffic volumes	Section 7	
type of vehicles (e.g. cars, trucks)	Section 7	
Traffic management on frontage streets	Section 8	
Public transport access	Section 9	
nearest bus/train routes	Section 9	
nearest bus stops/train stations	Section 9	
pedestrian/cycle links to bus stops/train station	Section 9	
Pedestrian access/facilities	Section 10	
existing pedestrian facilities within the development (if any)	Section 10	
proposed pedestrian facilities within development	Section 10	
existing pedestrian facilities on surrounding roads	Section 10	
proposals to improve pedestrian access	N/A	
Cycle access/facilities	N/A	
existing cycle facilities within the development (if any)	N/A	
proposed cycle facilities within the development	N/A	
existing cycle facilities on surrounding roads	Section 10	
proposals to improve cycle access	Section 10	
Site specific issues	Section 11	
Safety issues	Section 12	
identify issues	N/A	
remedial measures	N/A	



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C:\Users\PeterAlebakis\URBANISE WA PTY LTD\URB0834 - T24072 - Qube Picton Kangaroo Pond - Design\Deliverables\DWG\URB0834-C350 - Stormwater Drainage Plan 12/12/2025 6:32:20 AM PeterAlebakis

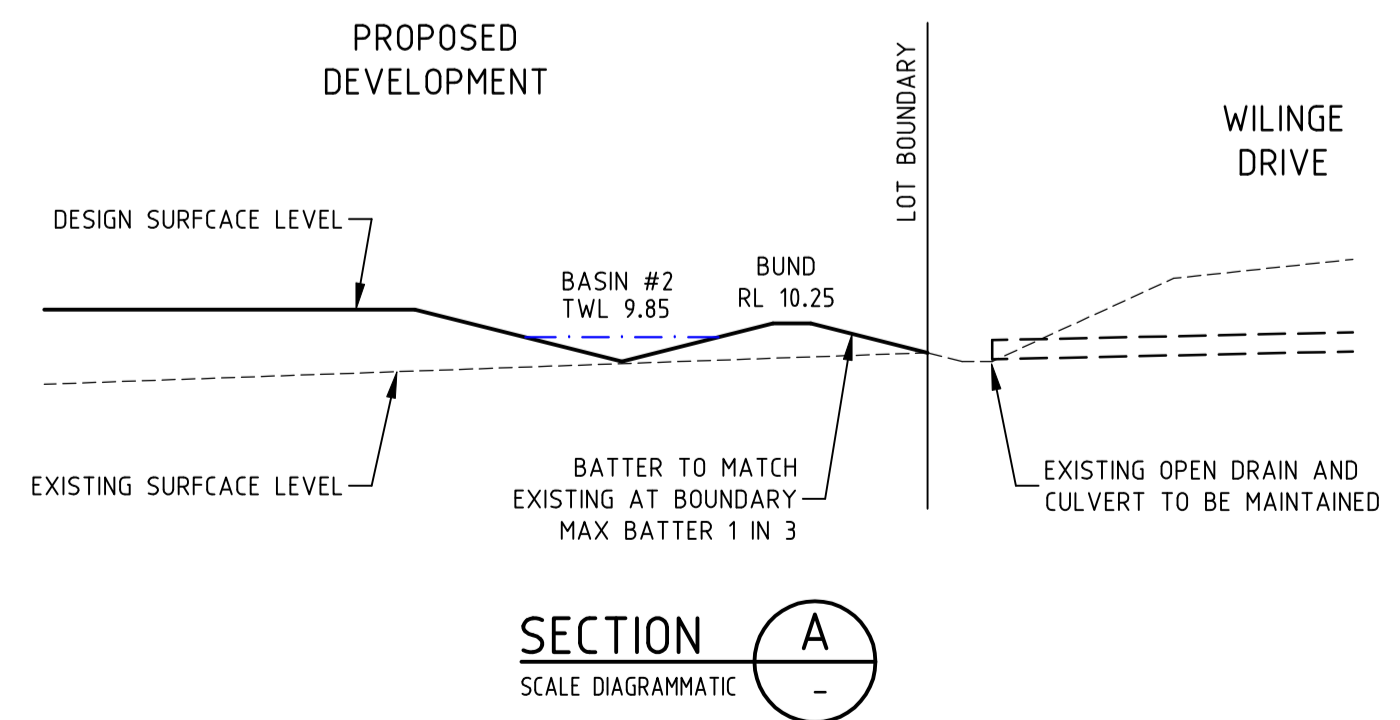
PLAN SCALE 1:500



ISSUED FOR APPROVAL
NOT TO BE USED FOR CONSTRUCTION PURPOSES

- NOTES**
- THIS DRAWING SHALL BE READ IN CONJUNCTION WITH ALL URB0779 DRAWINGS, SPECIFICATION
 - THE CONTRACTOR SHALL LOCATE ALL EXISTING SERVICES WITHIN THE CONTRACT AREA PRIOR TO THE COMMENCEMENT OF WORK. WHERE EXISTING AND PROPOSED WORKS INTERSECT, LEVELS ARE TO BE TAKEN AND SUPPLIED TO THE SUPERINTENDENT.
 - ALL DIMENSIONS ARE IN METRES UNLESS SHOWN OTHERWISE.
 - THE CONTRACTOR SHALL LOCATE ALL LEVELS FROM ESTABLISHED BENCH MARKS.
 - ALL BENCH MARKS ARE TO BE PROTECTED AND PRESERVED.
 - ALL STORMWATER DRAINAGE PIPES ARE TO BE RC CLASS 2 INSTALLED TO MANUFACTURES SPECIFICATIONS, UNLESS OTHERWISE NOTED.
 - ALL STORMWATER DOWNPIPES PIPES SHALL BE PVC WITH MIN STIFFNESS SN8.
 - ALL TRENCHING, PIPE BEDDING AND BACKFILLING SHALL BE IN ACCORDANCE WITH AS3725.
 - STORMWATER PIPE ALIGNMENT SHALL BE AS INDICATED ON THE PLANS AND LIAISON WITH STRUCTURAL ON FOOTING LOCATIONS.
 - ALL JUNCTION PITS AND ENTRY PITS SHALL BE LOCATED AS SHOWN IRRESPECTIVE OF PIPE LENGTHS SHOWN ON THE DRAWINGS.
 - AT LOW POINTS THE ENTRY PITS SHALL BE LOCATED AT THE LOW POINT OF THE PAVEMENT.
 - THE CONTRACTOR SHALL IMMEDIATELY REPORT ANY DISCREPANCY OR CLASH WITH OTHER SERVICES TO THE SUPERINTENDENT.

- LEGEND**
- PROPOSED DEVELOPMENT BOUNDARY
 - PROPOSED JUNCTION PIT
 - EXISTING DRAINAGE NETWORK
 - PROPOSED GULLY / BUBBLE UP PIT
 - DENOTES PIT REFERENCE NUMBER
 - PROPOSED SUBSOIL AND INVERT LEVEL
 - PROPOSED FINISHED SURFACE LEVEL
 - DENOTED EXISTING SURFACE CONTOURS
 - DENOTES EXISTING SURFACE SPOT LEVEL
 - DENOTES ROADWAY KERB / EDGE
 - DENOTES EXISTING BUILDINGS
 - DENOTES GROUND WATER CONTOURS
 - DENOTES DETENTION STORAGE BASIN



STORMWATER CALCULATIONS	
DESCRIPTION	ULTIMATE
CATCHMENT	49,395m ²
PEAK DISCHARGE OFFSITE	0 L/s
INFILTRATION RATE	2m/day
INFILTRATION AREA	6,255m ²
SITE STORAGE REQUIREMENTS	
10% AEP STORM	1,490m ³
1% AEP STORM	2,665m ³
PROPOSED BASIN CAPACITIES	
BASIN 1	1,470m ³
BASIN 2	1,785m ³
TOTAL NETWORK CAPACITY	3255m ³

REV No.	DATE	DRAWN BY	REVIEWED BY	AMENDMENT DESCRIPTION	
F	11/12/25	PA	PA	PA	STORMWATER AMENDED WITH NO OFFSITE DISPOSAL CONNECTION
E	10/10/25	PA	PA	PA	AMENDED AS PER CLIENT COMMENTS AND ISSUED FOR APPROVAL
D	12/09/25	PA	PA	PA	DESIGN AMDNED TO SUIT NEW LAYOUT AND CITY COMMENTS
C	14/05/25	PA	PA	PA	VOLUMES AMENDED, ISSUED FOR APPROVAL
B	12/05/25	PA	PA	PA	FSL CONTOURS AND SUBSOIL ADDED, BASIN 2 LABEL UPDATED

Urbanise wa
ENGINEERING SOLUTIONS

Perth
Westpoint Centre
207 / 396 Scarborough Beach Road
Osborne Park WA 6017


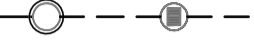



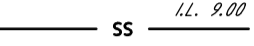
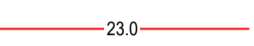


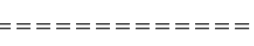
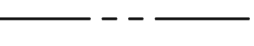

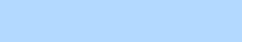
Margaret River
Wave Court
7 / 30 Fernside Ave
Margaret River WA 6285

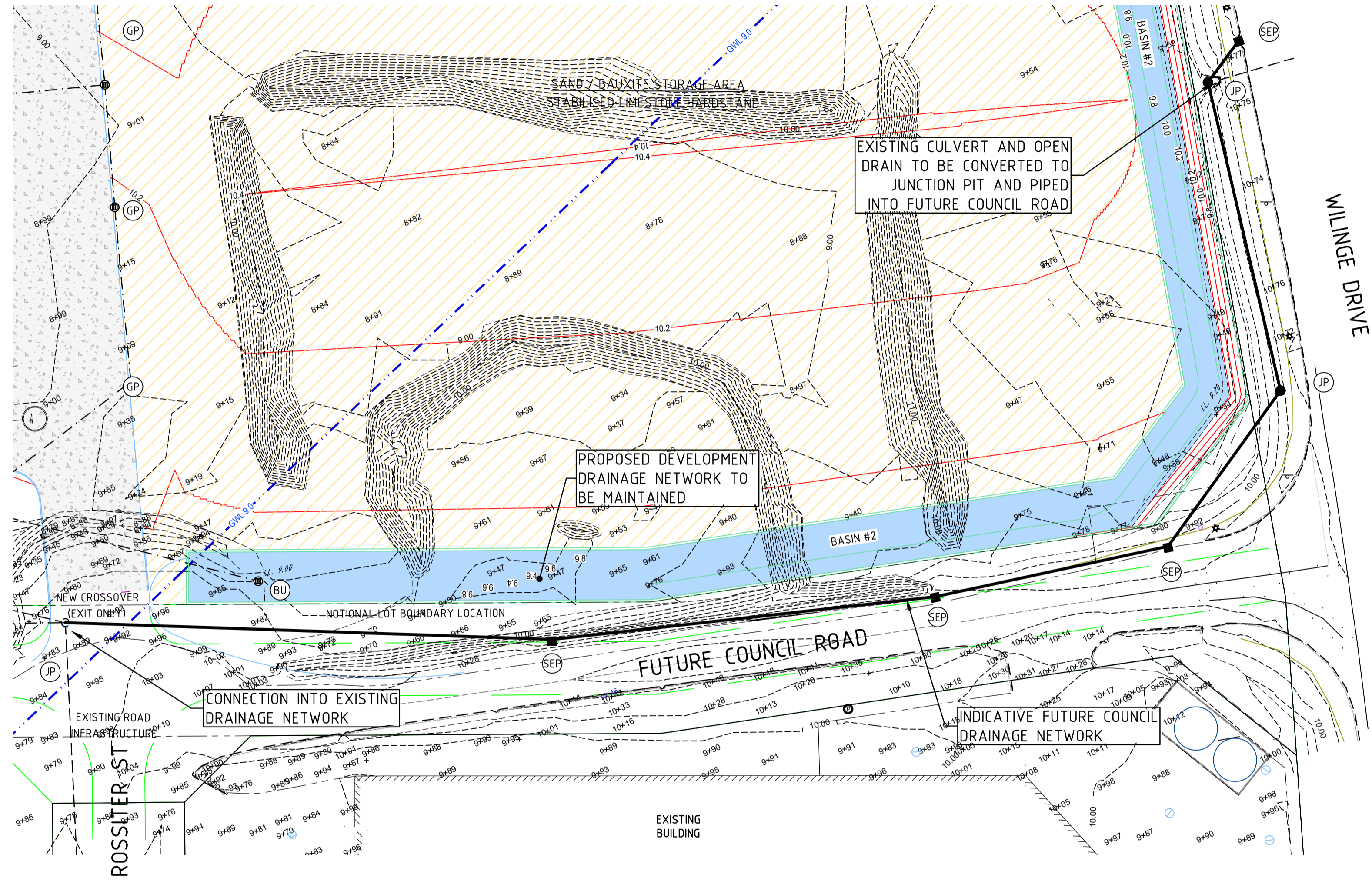
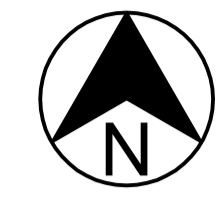
CLIENT QDC	DESIGNED P.ALEBAKIS	APPROVED PROJECT MANAGER	APPROVED PROJECT DIRECTOR
PROJECT T24072 - QUBE PICTON KANGAROO POND	SCALE 1:500	PROJECT No. URB0834	DRAWING No. C350
TITLE STORMWATER DRAINAGE MANAGEMENT PLAN	DRAFTED P.ALEBAKIS	WAPC No. N/A	REV F

NOTES

- THIS DRAWING IS CONCEPTUAL PLAN AND SUBJECT TO FUTURE ROAD DESIGN
- ALL DIMENSIONS ARE IN METRES UNLESS SHOWN OTHERWISE.

LEGEND

-  DENOTES FUTURE JUNCTION PIT
-  EXISTING DRAINAGE NETWORK
-  DENOTES FUTURE SEP / GULLY / BUBBLE UP PIT
-  DENOTES DEVELOPMENT DRAINAGE NETWORK
-  DENOTES PIT REFERENCE NUMBER
-  DENOTES SUBSOIL AND INVERT LEVEL
-  DENOTES DEVELOPMENT FINISHED SURFACE LEVEL
-  DENOTES EXISTING SURFACE CONTOURS
-  DENOTES EXISTING SURFACE SPOT LEVEL
-  DENOTES ROADWAY KERB / EDGE
-  DENOTES EXISTING BUILDINGS
-  DENOTES GROUND WATER CONTOURS
-  DENOTES DEVELOPMENT STORAGE BASIN

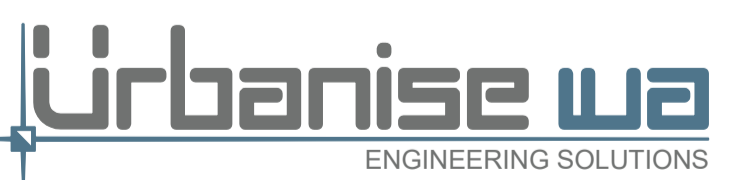


PLAN
SCALE 1:500



CONCEPT ONLY - SUBJECT TO FUTURE ROAD DETAIL DESIGN

FOR INFORMATION ONLY
NOT TO BE USED FOR CONSTRUCTION PURPOSES

REV No.	DATE	DRAWN BY	PA	PA	PA	ISSUED FOR INFORMATION
						AMENDMENT DESCRIPTION
						Perth Westpoint Centre 207 / 396 Scarborough Beach Road Osborne Park WA 6017 Margaret River Wave Court 7 / 30 Fernin Ave Margaret River WA 6285
CLIENT QDC						
PROJECT T24072 - QUBE PICTON KANGAROO POND						
TITLE WILINGE DRIVE LWMS PIPE ALIGNMENT ULTIMATE BASED ON LWMS						
DRAFTER P.ALEBAKIS		DESIGNED P.ALEBAKIS		APPROVED PROJECT MANAGER		APPROVED PROJECT DIRECTOR
DATUM AHD		SCALE 1:500		PROJECT No. URB0834		DRAWING No. SK002
GRID NONE		WAPC No. N/A				REV A

C:\Users\PeterAlebakis\URBANISE WA PTY LTD\URB0834 - T24072 - Qube Picton Kangaroo Pond - Design\Deliverables\DWG\URB0834 - SK002 - Wilinge Drive LWMS Pipe Alignment - 6/05/2026 5:51:14 PM - PeterAlebakis

Schedule of Submissions

Proposed Warehouse Storage and Hardstand

Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
Government / Service Agency Comments			
1.	<p>Main Roads WA</p> <p>Daniel Naude</p> <p>swrplanning@mainroads.wa.gov.au</p>	<p>I refer to the City’s request for advice regarding the above development and our recent discussion.</p> <p>Please be advised that Main Roads has significant concerns with the proposed drainage concept/approach and the encroachment of the development into the planned South Western Highway – Picton Deviation corridor (adjoining Lot 253), and recommends that the proposal be deferred to address these concerns, discussed below in more detail.</p> <p>As discussed, the proposed development is located ‘down-stream’ in the catchment precinct and to the west of Willinge Drive which conveys stormwater from the up-stream catchment via a series of culverts. In this specific location, the applicable RL levels of the culvert comprise the following: U/S9.42 & D/S 9.37.</p> <p>The proposed RL levels (8.9 increasing to 9.4 and maximum levels of 9.7/9.95) of the new detention and storage system are generally higher than the outlet of the culvert system on Willinge Drive and will therefore has the potential to cause a back flooding effect onto the drainage catchment/system east of the development site and Main Roads assets,</p>	<p>Noted.</p> <p>Applicant has revised the Stormwater Management Plan and the remaining technical issues noted by Main Roads WA can be included in a final revised stormwater management plan. This can be conditioned.</p>

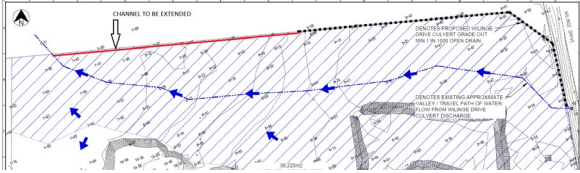
Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>which is not supported in principle. It is recommended that the design be reviewed to have regard to the RL levels of the existing culvert on Willinge Drive to address this issue. It is also recommended that cross-sections/further information be provided to better to facilitate interpretation of the proposal and, in particular, to confirm development setback(s) to the planned South Western Highway – Picton Deviation corridor.</p> <p>It is further noted that stormwater infrastructure encroaches into the Picton Deviation corridor, which is not supported in principle. It appears that the WAPC (current owner of adjoining Lot 253) has not endorsed the application forms to suggest that the Commission agrees for the encroachment to occur into their land which will ultimately be transferred to the Commissioner of Main Roads. It is recommended that the design be modified to remove any batters spilling into Lot 253, to ensure that all development is wholly located within the development site, and for an appropriate development setback to Lot 253 be determined to allow for ongoing future maintenance of the swales etc. by likely small machinery.</p> <p>Main Roads does also not support subsoil discharge into Lot 253 / the Future Picton Deviation in the manner proposed.</p> <p><u>Further Response Received following revised Stormwater Management Plan</u></p> <p>Main Roads has reviewed the further information and provide the following</p>	

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>comments for the City's consideration.</p> <ul style="list-style-type: none"> • In relation to the drainage option proposing to redirect drainage within the development site to the north and along the future SWH deviation, the revised flow path is supported in principle, as it is similar to the original proposal we supported, but it will require further engineering details/clarification: <ul style="list-style-type: none"> ○ The channel along the northern boundary will need to be extended to the location that the flows currently leave the property, to maintain the existing flow path. See image below. ○ A geometry will need to be provided for the channel (more than just a line on a page). Any earthworks or excavation, as well as access paths will need to be kept within the property boundary, to avoid constructing private infrastructure within the future road reserve. ○ Detailed design drawings will need to include typical channel section/s, earthworks extents, and construction specification. As the proposed channel will be quite flat, it will require some silt trapping capacity. <p>Channel extension:</p>	

Schedule of Submissions
 Proposed Warehouse Storage and Hardstand
 Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		 <p> <ul style="list-style-type: none"> In relation to the potential 'ultimate' piped drainage solution, further design detail is required, including whether the developer is proposing to fund and build it or is merely put forward as a potential future configuration by authorities? In this regard, it is noted that the development to date has been considered as a departure from the subdivisional layout on the endorsed structure plan, which originally depicted a public road system that would have been required to be built and serviced to an urban standard, including drainage provision comprising a pit and pipe system which connects to a broader drainage system for the planning precinct. </p> <p>It is recommended that the above information be provided prior to a decision being made.</p> <p><u>Clarification of Condition for Stormwater</u></p> <p>I have discussed the City's request seeking agreement to the deferral of the northern drainage channel's civil design component as a condition of</p>	

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>development approval (clearances stage) with the Acting Director, South West Operations. The Region is agreeable to this approach, given the proposed wording of the condition(s) outlined in your communication below.</p> <p>Main Roads requests that a copy of the determination notice be provided to us for record keeping purposes.</p>	
2.	<p>Department of Planning, Lands and Heritage</p> <p>Scott Penfold Regional South Planning regionalsouthplanning@dplh.wa.gov.au</p>	<p>Regarding the proposed development application, the Department of Planning, Lands and Heritage – Land Use Planning division has the following comments:</p> <p>Greater Bunbury Region Scheme</p> <ul style="list-style-type: none"> The application requires planning approval pursuant to the Greater Bunbury Region Scheme (GBRS), as it meets the criteria listed in point 3, Schedule 1 of the Resolution under Clause 27 of the GBRS - 09/05/2014 (RES 2014/03). Given the proposal abuts a Primary Regional Road reserve under the GBRS, the application is to be forwarded to Main Roads WA for comment. Pursuant to point 2, Schedule 2 of the Delegation Powers of Local Governments (GBRS) - 09/05/2014 (DEL 2014/01), the City has delegation for the application (and the responsible authority report) if it accepts the recommendation and advice to Main Roads Western Australia or refuses/recommends refusal of the application. If the above dot point is not met, delegation of the City to prepare the recommendation for the GBRS application falls away and will be required to be made by the Department of Planning, Lands and Heritage. 	Noted.

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>Development Application Comments</p> <ul style="list-style-type: none"> The development application should be considered against the Wimbridge Structure Plan. It is recommended that the application is referred to the Department of Water and Environmental Regulation for consideration of the overall drainage impact and the drainage matters raised in the Wimbridge Structure Plan. Development should consider the ultimate road layout envisaged by the Wimbridge Structure Plan, including ensuring there is sufficient room provided between existing and proposed development for a future public road, which will assist in addressing the road network limitations in the locality. <p>I trust the above is of assistance. Please do not hesitate to contact me on 9791 0588 should you have any queries.</p>	
3.	<p>Department of Water & Environmental Regulations</p> <p>Daniel Wong Environmental Officer daniel.wong@dw.er.wa.gov.au</p>	<p>Thank you for seeking clarification from the Department of Water and Environmental Regulation (DWER) regarding our previous response to this proposal.</p> <p>This proposal is for proposed storage sheds, bunkers and a temporary external hardstand/laydown area (see Screenshot 1 below).</p> <p>The Department has identified that the proposal has the potential for impact on environmental/water resource management. Key issues and recommendations are provided below, and this should be applied in the development approval (DA):</p> <p>Issue 1: The clearing of native vegetation may be subject to the</p>	<p>Noted.</p> <p>Applicant has revised the Stormwater Management Plan and remaining technical issues noted by Department of Water & Environmental Regulation can be included in a final revised stormwater management plan. This can be conditioned.</p>

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>Environmental Protection Act 1986 (EP Act)</p> <p>Advice 1: The following advice notes be applied to this DA: The applicant is to obtain development approval from the City to ensure the proposed clearing for the sheds, bunkers and associated hardstand is exempt under Regulation 5, Item 1 of the Environmental Protection (Clearing of Native Vegetation) Regulations 2004 (the Clearing Regulations). This exemption only applies if development approval is granted by the City first, before the clearing</p> <p>Issue 2: Outfall for stormwater management</p> <p>Advice 2: The following is advised:</p> <p>a) The City should be aware:</p> <ul style="list-style-type: none"> • 1) DWER is of the understanding that in context of the lengthy history of the site, no further development encroaching into the lower lying land would be accepted until the matter of who will pay for and manage the offsite discharge pathway to the intended outfall at the Preston River is resolved. • 2) that as the above outfall issue continues to remain unresolved, the continued various incremental developments disadvantage the owner of the property that is shown as being restricted in the structure plan raising the issue of equity <p>b) In the event this DA is approved by the City:</p> <ul style="list-style-type: none"> • 1) Conditions EC0016 (Stormwater & Drainage Management Plan) be imposed, where the Stormwater Drainage plan be revised to include: 	

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<ul style="list-style-type: none"> i. subsoil drainage lines beneath the existing basin which is being reshaped ii. subsoil drainage lines to be implemented internally throughout the site to better control groundwater levels within the development area – to mitigate the crowning of groundwater levels throughout the area where the storage sheds are being constructed iii. an updated detailed design plan for the Stormwater Drainage to show the Invert Levels and Pipe Sizes for the stormwater network proposed iv. the necessary storage be contained within on-site basins – so as to negate the need for downstream compensations to be made <ul style="list-style-type: none"> • 2) Condition EC0017 (Implement Stormwater & Drainage Management Plan) be imposed to the satisfaction of the City <p>c) The applicant is to consult with, and prove up to the satisfaction of Main Roads Western Australia (MRWA) that their drainage infrastructure will not be adversely impacted from this development</p> <p>Issue 3: Contaminated sites and Acid sulfate soils</p> <p>Advice 3: The following advice noted be applied: Acid sulfate soils (ASS) risk mapping indicates that the site is located within an area identified as representing a moderate to low risk of ASS occurring within 3 metres of the natural soil surface. Please refer to Department of Water and Environmental Regulation’s acid sulfate soil guidelines for information to assist with the management of deep ground disturbing works if required.</p>	

Schedule of Submissions
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Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>https://www.der.wa.gov.au/your-environment/acid-sulfatesoils/69-acidsulfatesoils-guidelines</p> <p>Where the Department has a statutory role, planning applications should be considered prior to the Department issuing any relevant permits, licenses and/or approvals.</p> <p>More detail pertaining to the above issues are provided in Table 1 below.</p> <p>In the event there are modifications to the proposal that may have implications on aspects of environment and/or water management, the Department should be notified to enable the implications to be assessed.</p> <p>Should you require any further information on the comments please contact the undersigned.</p> <p><u>Further Response Received</u></p> <p>Thank you for requesting a review of the above stormwater drainage plan from by the Department of Water and Environmental Regulation (DWER/Department).</p> <p>As per our correspondence (our ref: PA 073471), detailed in Table 1 below back on 4th August 2025, we did not support this development on the basis of drainage issues.</p>	

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>In response to our position, the previous stormwater drainage plan (as submitted under PA 073471, being Rev C, dated: 14/5/2025) has been revised (Rev: F, dated: 11/12/2025) – to which the City is requesting our comments on.</p> <p>In summary, we understand the intent of Rev F was for the development site to hold stormwater for events up to 1% AEP – to which is acceptable to us.</p> <p>DWER has reviewed the revised version, Rev F, and notes the updates to the previous Rev C version in Table 2 below.</p> <p>Our comments are:</p> <ul style="list-style-type: none"> • Rev F proposes a retaining wall along property boundary (Screenshot 1b) to modify the basin profile to increase stormwater storage capacity, but no details have been given regarding the height of the retaining wall. <ul style="list-style-type: none"> ○ Please note that DWER does not provide advice on engineering integrity of structures • Rev C has the stormwater discharge outlet at the south west corner (Screenshot 1a) has been removed in Rev F (Screenshot 1b) • Rev F has been designed to have no offsite discharge – peak offsite 	

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>discharge at 0 L/S (Screenshot 4b) compared to Rev C at 180 L/S (Screenshot 4a)</p> <ul style="list-style-type: none"> • Rev C proposed Basins 1, 2 & 3 with a total ultimate capacity of 1447 m³ (Screenshot 4a) compared to Rev F that proposes Basins 1 & 2 with a total ultimate capacity of 3255 m³ – designed to contain up to 1 % AEP storm event (Screenshot 4b) • Rev F Basins 1 & 2 are interconnected • An infiltration rate of 2m/day is proposed in Rev F, but no supporting information is provided. <ul style="list-style-type: none"> ○ Evidence to support the 2m/day infiltration is requested, based both on soil type and groundwater levels. • A groundwater contour of 9.0m is noted in the drawing Rev F <ul style="list-style-type: none"> ○ Is that verified to be the highest known groundwater level? ○ This has not been specified if it's in metres AHD or if its as an RL (reduced level) • Rev F: Basin 1 has a base of 9.00 m RL & Basin 2 has a RL of 9.35 m <ul style="list-style-type: none"> ○ How would this relate to the highest known ground level in context of the basins capability to infiltrate this water? • It is noted that the bund top water level (TWL) is at 10.25 m RL • Rev F: Basins 1 & 2 have a TWL of 9.85 RL – corresponding to a 1% AEP level 	

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<ul style="list-style-type: none"> ○ As such, no outflows will occur in a 1 % AEP event (and bigger event) noting the outlet at the eastern side is contained in a bund within the property boundary (Bund height of 10.25 m RL) – as per Section A (Screenshot 3b) ○ MRWA advice is recommended to ensure they are comfortable with the way their culvert is maintained as free flowing (Section A in Screenshots 2b & 3b) ● There are no details of runoff coefficients applied, so this cannot be checked ● Details on subsoil drainage lines being implemented internally throughout the site to better control groundwater levels within the development are ● With regards to the above comments, the City is advised to consult with its engineer/s <p>In the event there are modifications to the proposal that may have implications on aspects of environment and/or water management, the Department should be notified to enable the implications to be assessed.</p>	
5.	Department of Planning, Lands and Heritage – Aboriginal Heritage	Thank you for your enquiry dated 27 June 2025 seeking comment from the Department of Planning, Lands and Heritage (DPLH), Aboriginal Heritage Conservation, regarding the proposed development application at Lot 260 (390) Willinge Drive, Glen Iris.	Noted.

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

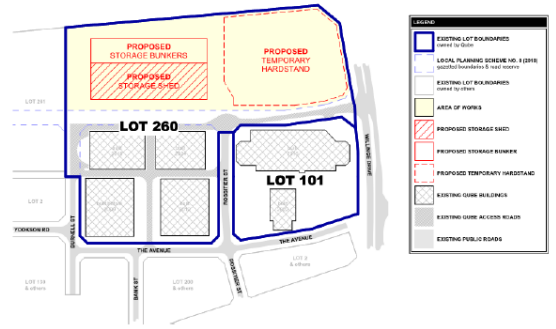
#	Name / Address	Summary of Submission	Officer Comments on Submission
	<p>Crystal Dean crystal.dean@dplh.wa.gov.au</p>	<p>A review of the Register of Places and Objects, as well as the DPLH Aboriginal Heritage Database, concludes that the subject area does not intersect with any known Aboriginal heritage Places or Registered sites.</p> <p>For reported Aboriginal heritage where there is restricted information, the actual location of the site is not publicly available. As such, a larger dithered boundary is represented on the publicly accessible DPLH Aboriginal Cultural Heritage Inquiry System (ACHIS).</p> <p>Therefore, based on the current information held by DPLH, no approvals under the <i>Aboriginal Heritage Act 1972</i> (AHA) are required in this instance.</p> <p>Please note that limited Aboriginal heritage surveys have been completed over the subject land and, as such, it is unknown if there is Aboriginal heritage present. Therefore, the City of Bunbury needs to be aware of its obligations under the AHA.</p> <p>DPLH also advises the City of Bunbury regularly checks ACHIS should new Aboriginal heritage be reported within the subject area. You can search ACHIS by using the following link: Aboriginal Cultural Heritage Inquiry System.</p>	
Public Comments - OBJECTION			
6.	<p>Tecon Australia (Kristy Richardson) Representing Owner of Lot 261 Willinge Drive</p>	<p>OBJECTION</p> <p>Tecon Australia acts on behalf of the owners of Lot 261 Willinge Drive Glen Iris, Bluebone Holdings PTY LTD.</p> <p>While our client is supportive of development generally, and does not have any specific concerns with the maximum wall heights proposed of 15.71 above NGL, on review of the proposed development application we believe</p>	<p>The following comments are provided based on the key issues being raised here of:</p> <ul style="list-style-type: none"> Proposed stormwater management plan is inconsistent with the Local Water Management Strategy in not contributing to a downfall stormwater solution to the area.

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>that the proposed development is not consistent with the Structure Plan and that there are a number of elements which we believe have significant and detrimental impact on our clients land and surrounding road reserves.</p> <p>The subject site, as well as our clients land, is zoned 'Service Commercial' under the City of Bunbury's Local Planning Scheme No 8 and both land parcels are subject to the approved Wimbridge Precinct Structure Plan.</p> <p>In support of the Structure Plan a Drainage Study was prepared by TME In addition to the structure plan and drainage study TME prepared a Drainage Contribution Policy</p> <p>These reports detail the catchments areas across the broader Glen Iris area and were prepared initially to provide an understanding of the catchment is terms of existing drainage conditions.</p> <p>Coordinated drainage across the broader area, as acknowledged in the applicants planning report, is subject to the endorsed Local Water Management Strategy (LWMS) and any development or subdivision of the subject site will need to be demonstrated as being compliant with the planning framework in place including the LWMS.</p> <p>As identified in the development application, the proposed development consists of a bulk storage facility inclusive of significant external storage and hard stand area.</p> <p>The below figure taken from the advertised planning report details the full extent of development on the site. As detailed in the applicants planning report the total building area is 13,317m².</p>	<p>Since public consultation, the applicant has revised the stormwater management plan. However, it is acknowledged that the stormwater management plan is not strictly consistent with the Local Water Management Strategy in that it does not contribute to a final stormwater solution downstream. The applicant seeks to contain all stormwater associated with 1 in 10 and 1 in 100 stormwater event within the lot boundaries of the development. This is further discussed below in the stormwater section.</p> <ul style="list-style-type: none"> • Proposed discharge outlet should not discharge onto neighbouring properties. All stormwater should be maintained within lot boundaries <p>The discharge outlet present within the designated 'Local Road' is an existing asset that partially discharges water from the landowners existing developed area and partially from roads within the structure plan area. The subject development proposal does not propose to use this discharge outlet or propose to contribute additional stormwater being discharged onto surrounding lots with all stormwater proposed to be maintained within lot boundaries.</p> <ul style="list-style-type: none"> • Concern with siteworks outside lot boundaries <p>The plans have been revised to ensure all siteworks are within the development property boundaries and are maintained by retaining walls on the lot boundaries.</p>

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
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The proposal includes a stormwater management plan that outlines that drainage storage required for the development includes 49,395m² catchment with a storage capacity of 2,904m³.

The planning report / submission includes a “technical Note” prepared by JDA Hydraulic Consultants author Mr Jim Davidson. In the introduction of the technical note Mr Davidson makes the following comment:

The review sections are consistent with LWMS chapters. This review of the Plan excludes review of the LWMS

It is noted that Mr Davidsons conclusion is based on calculations utilising storage basins extending over catchments 10 and 11 as detailed in the LWMS. His findings also reference the need for the drainage outlet to the southwest corner over these catchments consistent with the LWMS principals.

The LWMS has as a key principle of ensuring that the Wimbridge Precinct area has been designed to safely store and manage stormwater to ensure impacts on infrastructure, the environment and people’s safety are maintained. It is also noted that full implementation of management measures as identified with the LWMS are essential for orderly and proper

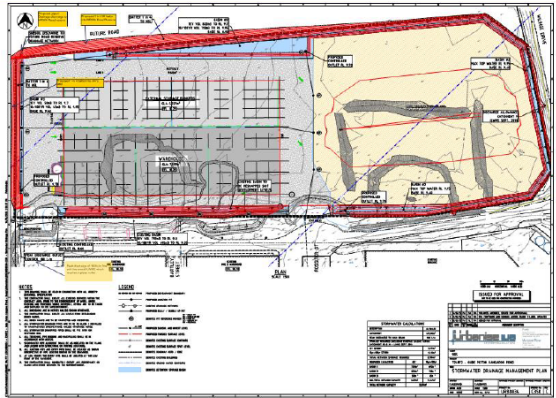
Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>planning.</p> <p>This position has been confirmed by the City in recent discussions and advice provided to the owners of 261 Willinge Drive. The following advice summarises the advice our client has been provided:</p> <p><i>It is acknowledged that incremental development within the structure plan area has been considered over a number of years, however the ultimate drainage solution and the contribution plan has not been secured. It has reached a stage that DWER and the City are unable to support any further development within the area, without the ultimate drainage solution being secured through a conditional development approval.</i></p> <p>The City has noted that in discussions with DPLH and DWER development in the area would only be supported subject to the development of the piped drainage outfall solution at the owner’s cost or preparation of a suitable legal agreement to secure the contributions between the relevant parties/ landowners.</p> <p>The owner of Lot 261 Willinge drive has made representation to relevant landowners to discuss progressing a contributions plan, at this point no response has been provided from any landowner.</p> <p>The basis of the position maintained in the applicant’s proposal is that as quoted in the planning report Stormwater Drainage Management Plan demonstrates that the total estimated storage available within the network of swales and basins proposed across the site is 3076m3 which indicates an excess of capacity on the site.</p> <p>It should be noted that the network of storage capacity referenced does not as stated provide an excess of capacity on site. It is clear from the JDA technical report that stormwater is not managed on the site however relies</p>	

Schedule of Submissions
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Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		<p>on basins and retention off site.</p> <p>It is noted that peak discharge of 180l/s is in line with the overall LWMS requires a piped outlet from Lot 261 Willinge Drive. As the construction of this outlet pipe is not directly in the control of the owners of the subject site then this significantly impacts the viability of the proposed drainage management plan as proposed.</p> <p>The draining of stormwater onto our client’s land would have serious negative implications. It is clear that any proposal to develop upstream of a catchment and drainage outlet should not adversely affect another landowner downstream. The suggestion that because the LWMS allows stormwater to be drained onto our client’s land noting t that the Peak Discharge outlet control is not in place is totally rejected.</p> <p>It is also noted that the Storm Water Drainage Management Plan appears to show a 1 in 4 batter that extends onto lot 261 Willinge which is without consent from the owner.</p> <p>It is our clients understanding from previous comments provided by the City that MRWA has rejected any proposal involving earthworks within the future road reserve that in their opinion could conflict with future requirements for the road. It noted that a batter appears to extend into the MRWA Road Reserve and there is subsoil discharge to the future Road Reserve.</p>	

Schedule of Submissions
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Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		 <p>It is noted that the City has prepared an Information Guide – Stormwater Disposal from Private, Commercial and Industrial Properties, which appears to fall under the City’s Local Planning Policy framework. The purpose of this document is stated to provide guidelines to Developers, the Community and City Development officers of the minimum requirements for the disposal of stormwater from private properties as required by Planning, Building and Local Law, schemes, policies and regulations.</p> <p>While noting that nothing in the guidelines shall prevent an applicant carrying out a detailed drainage study, a key statement within this document is the requirement that ‘Commercial and industrial developments all stormwater runoff is to be managed within the lot.</p> <p>As stated within this guideline all landowners are required to contain stormwater within the lot as outlined under section 9.1 of the City of Bunbury, Private Property Local Law.</p> <p>Conclusion</p> <p>As noted, while our client does not have any concerns with development as</p>	

Schedule of Submissions
Proposed Warehouse Storage and Hardstand
Lot260 (#390) Willinge Drive

#	Name / Address	Summary of Submission	Officer Comments on Submission
		proposed generally the issue of Stormwater Management is of significant concern in that the implications of the stormwater management plan as proposed would result in a significant amount of stormwater being discharged on our client s property.	

ENDORSEMENT PAGE

This structure plan is prepared under the provisions of the City of Bunbury Local
Planning Scheme No. 8

IT IS CERTIFIED THAT THIS STRUCTURE PLAN WAS APPROVED BY RESOLUTION OF THE
WESTERN AUSTRALIAN PLANNING COMMISSION ON:

16 APRIL 2015

In accordance with Schedule 2, Part 4, Clause 28 (2) and refer to Part 1, 2. (b) of the
Planning and Development (Local Planning Schemes) Regulations 2015.

Date of Expiry:

19 OCTOBER 2030

**ENDORSED STRUCTURE PLAN
- CITY OF BUNBURY**
To provide a framework for the future detailed
planning at the subdivision and development stage.

[Signature]
City of Bunbury delegated under section 11.3.1 of the
City of Bunbury Town Planning Scheme No. 7
29/04/15
Date

ENDORSED STRUCTURE PLAN - WAPC
To provide a framework for the future detailed
planning at the subdivision and development stage.

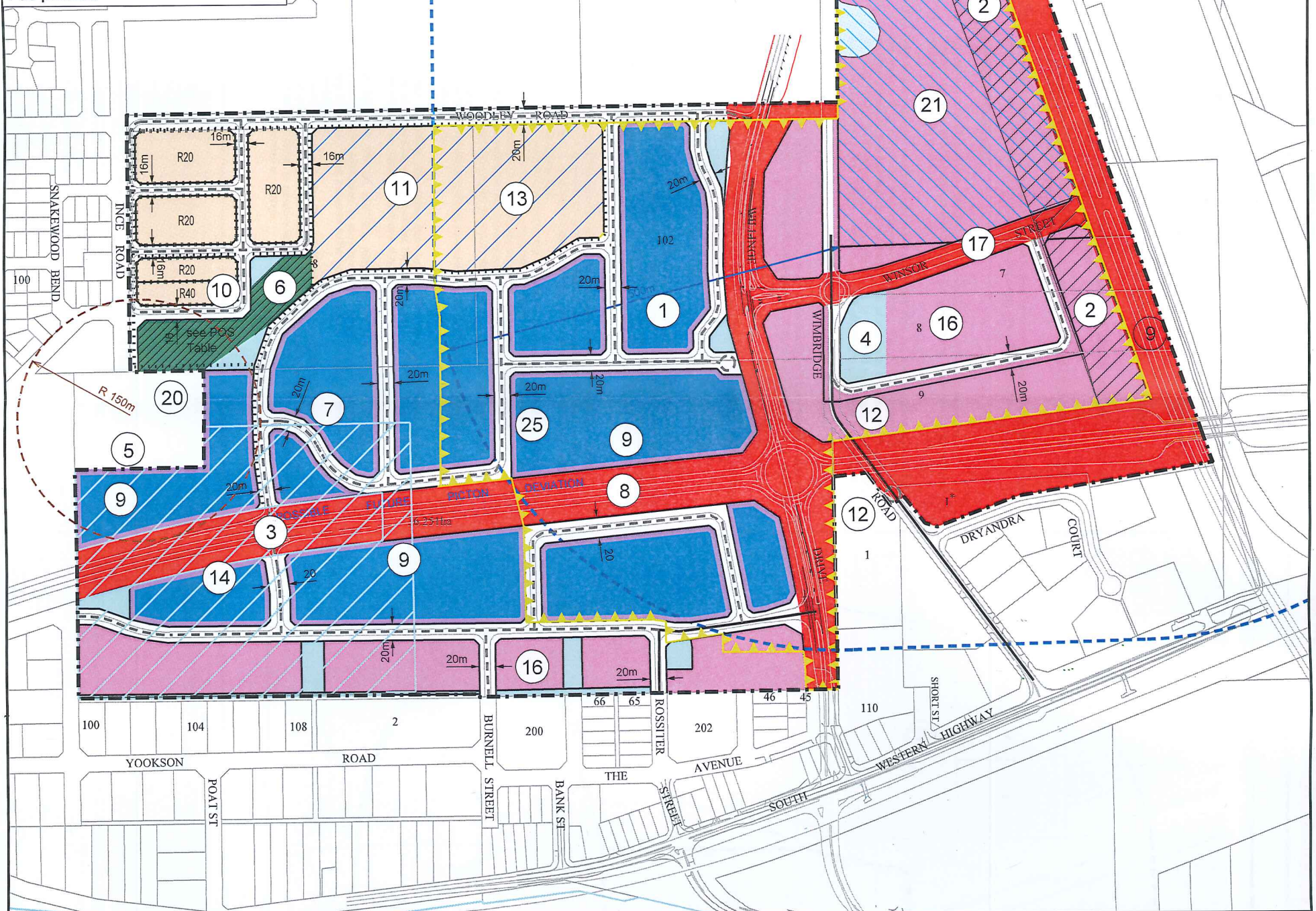
[Signature]
WAPC delegated under section 16 of the
Planning and Development Act 2005
16/4/2015
Date

POS TABLE

Total area of Subdividable Residential Land	6.02 ha
Total area of drainage	0.247 ha
Total area of POS required	0.577 ha
Total area of unconstrained POS provided	1.093 ha

**Refer to Sheet 2
for Structure Plan
Provisions**

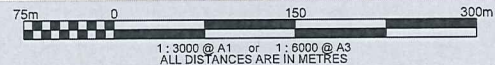
Refer to Sheet 2



STRUCTURE PLAN AREA	RESIDENTIAL	INDICATIVE ACCESS ROAD	ABATTOIR SITE
INDUSTRY	R CODES	EXISTING ACCESS ROAD	OFF-SITE BUFFER (500m from DBC abattoir site)
MIXED BUSINESS	PRIMARY REGIONAL ROADS (GBRS)	FUTURE DUAL USE PATH CONNECTION	DBC ABATTOIR SPECIAL CONTROL AREA
PARKS AND RECREATION	SUBJECT TO FURTHER INVESTIGATION	AQWEST TREATMENT SITE BUFFER (150m from centre of tank)	SPECIAL CONTROL AREA 3
DRAINAGE			LOCATION OF TEMP DRAINAGE BASIN

1* BALANCE OF LOT 1 OWNED BY MRWA TO BE INCLUDED IN FUTURE ROAD RESERVE

**Sheet 1 of 2
WIMBRIDGE PRECINCT
STRUCTURE PLAN**



REVISION	DESCRIPTION	DRAFTER	DATE
Y			
T			
S			
Q			
P			
O			
N			
M	Modification to subject land, provisions & colour match	KS	13.04.2015
L	Modification to subject land, provisions & road link	KS	03.02.2015
K	CUI, DE SAC ADDED AND NOTE 17 ADDED	TDB	15.02.2013

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ORIGINAL PLANNER:	M.L.
ORIGINAL DRAFTER:	K.S.
CREATED DATE:	19.03.2010
AERIAL DATA:	ECW 2008
CADASTRAL DATA:	MGA
TOPOGRAPHIC DATA:	LANDGATE



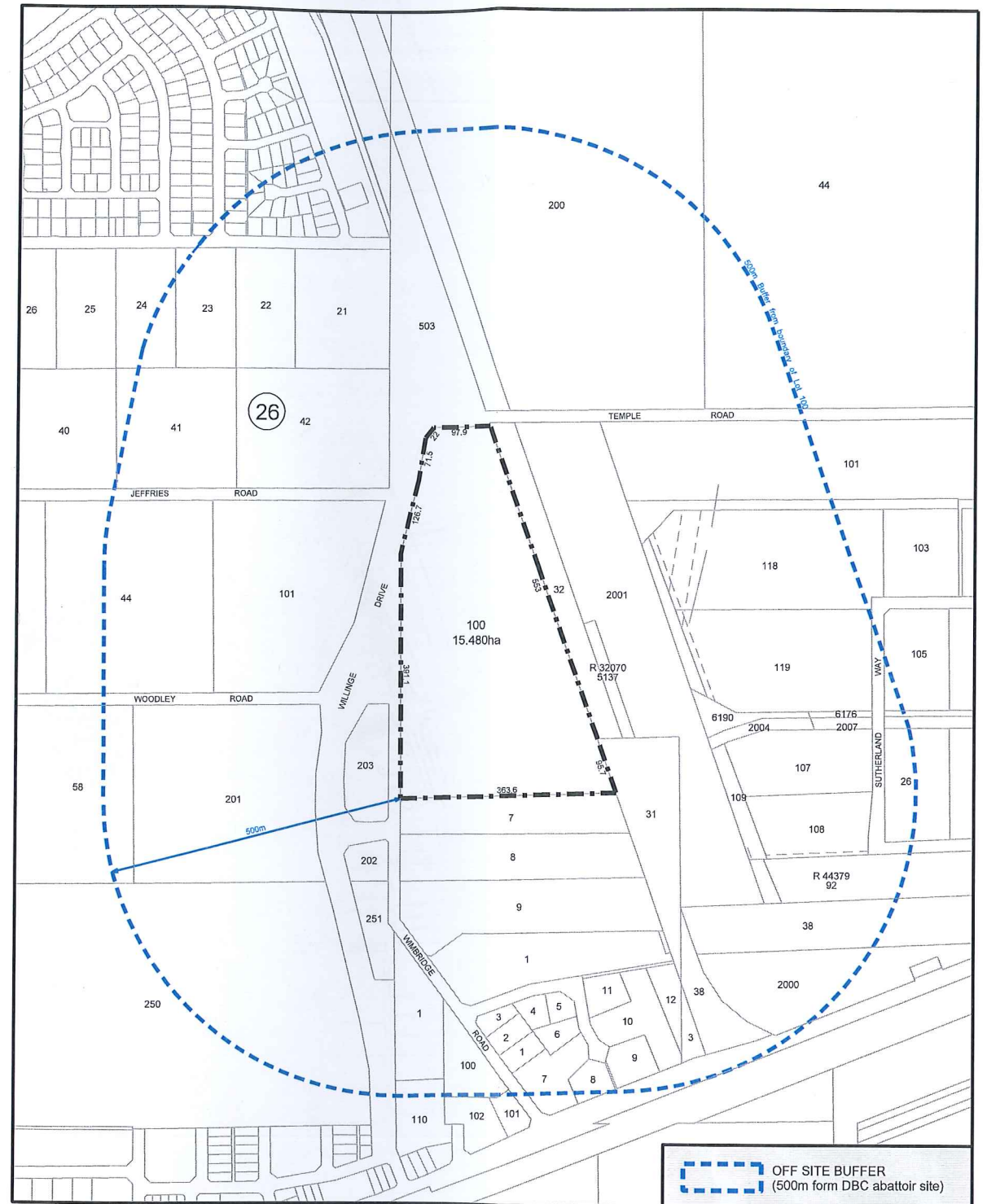
08304P-SP-02M

Structure Plan Provisions

- No residential or other sensitive uses (as defined by EPA Guidance Statement No.3 – Separation Distances Between Industrial And Sensitive Land Uses, June 2005) are to be located on land nominated within the abattoir and Water Treatment Plant Special Control Area.
- Land use proposals within Special Control Area 3 - Glen Iris Service Corridor Buffer Area, as identified in the Greater Bunbury Region Scheme, shall be assessed against the following criteria outlined in Division 4 of the Greater Bunbury Region Scheme:
 - the purpose of the Area;
 - planning requirements; and
 - consultation requirements.
- The Staged implementation of the intersection of the proposed north south subdivision road with the future Picton Deviation (shown as 3 on the map) is to be negotiated by the developer with Main Roads WA. The subdivision road is allowed to be a continuous road until the Deviation occurs at which time the following arrangements will apply unless agreed otherwise with Main Roads.
 - The southern section of the subdivision road shall be connected to the Picton Deviation via a left in only (from the Highway) arrangement.
 - The northern section of the subdivision road shall be connected to the Picton Deviation via a full Tee junction arrangement.
- All development shall incorporate stormwater management systems in accordance with the Drainage Management Strategy and associated Local Water Management Strategy prepared by Thompson McRobert Edgeloe Group. No temporary drainage structures (except for detailed Temp Drainage Basin) will be approved. If any drainage management requirements cross property boundaries, the developer shall negotiate a resolution to the satisfaction of the City of Bunbury prior to any subdivision or development approvals.
- Land within the 150m Water Treatment Buffer is excluded from residential and other sensitive (as defined by EPA Guidance Statement No.3 – Separation Distances Between Industrial and Sensitive Land Uses, June 2005) land uses unless otherwise negotiated with Aqwest following a risk management assessment of the final chlorine storage area, and to the satisfaction of the City.
- Suitable landscape buffers shall be provided to residential uses to the satisfaction of the City of Bunbury, and all costs shall be the responsibility of the developer.
- Prior to the approval of any subdivision or development application, the developer/s shall prepare a Drainage Staging and Contributions Plan to the satisfaction of the City of Bunbury to address the timing, staging, land acquisitions and subdividers' contributions towards the implementation of drainage infrastructure as identified by the 'Drainage Management Strategy'.
- South Western Highway proposed realignment to be set aside as a separate lot pending acquisition by Main Roads WA.
- Detailed Area Plan/s shall be required as a condition of subdivision for all lots fronting the existing and future primary regional road. Development adjacent to the Bypass must be orientated towards the Bypass in terms of building facades and landscaping as specified in such Detailed Area Plan(s). There shall be no direct access permitted onto the Bypass.
- A Detailed Area Plan is required as a condition of subdivision for any land designated as R40 or higher.
- Area subject to further investigation. Subject to separate structure planning.
- The future Wimbridge Road shall be severed and a cul-de-sac is to be constructed by Main Roads WA when the South Western Deviation is constructed.
- Drainage Investigation Area to be in accordance with the Drainage Management Strategy and associated Local Water Management Strategy.
- The area marked 'Temp Drainage Basin' shall be subject to a legal agreement naming the City of Bunbury as having an interest in the land as an asset with all associated costs borne entirely by the developer/s. The legal agreement shall be in place prior to any site works or development commencing within the Structure Plan Area, and shall remain in place until the downstream drainage infrastructure, as identified in the 'Drainage Management Strategy', is implemented to the satisfaction of the City of Bunbury. The developer is to demonstrate, to the satisfaction of the City of Bunbury, that the 'Temp Drainage Basin' will satisfy all the requirements as identified in the 'Drainage Management Strategy'.
- No development or subdivision approvals will be issued for the area marked for "Temporary Drainage Basin" until such time as the full implementation of the Western Drainage Outlet, as identified in the Drainage Management Strategy, has been achieved.
- 'Industry Noxious', 'Industry Port', 'Industry - General' uses are not permitted under the 'Industry' classification on the Structure Plan.
- The proposed Winsor Street extension to the Port Access Road is to be set aside as a separate lot pending acquisition by Main Roads WA.
- Site contamination investigations are to be undertaken for Lots 58, 100, 201, 202 and 203, as applicable, prior to subdivision application stage.
- A Fire Management Plan is to be prepared and endorsed by Department Fire and Emergency Services (DFES) prior to subdivision application stage.
- The Water Corporation is intending to acquire land to co-locate a sewer pump station adjacent to the existing Aqwest site which will require a modification to the Structure Plan.

DEVELOPMENT OF LOT 100

- Lot 100 has an 'Additional Use - Abattoir', with an abattoir use defined as follows: 'abattoir' means premises used commercially for the slaughtering of animals for the purpose of consumption as food products.
- Prior to submitting an application for development approval, the Local Government may require the proponent to submit an Environmental Management Plan (EMP), prepared by a suitably qualified person to the specifications and satisfaction of the Local Government in consultation with Department Environment Regulation. The EMP is to be based upon sustainability principles demonstrating, among other things, containment of all off-site impacts within the off-site buffer from existing and proposed uses.
 - Once approved the EMP shall form part of the conditions of any approval issued for development.
 - The EMP shall address but is not limited to criteria, measures and mechanisms to control:
 - Noise emissions - including assessment of the need for a detailed noise modelling study.
 - Dust emissions.
 - Stock handling and management.
 - Odour emissions - including assessment of the need for detailed odour modelling study.
 - Cleaning and hygiene of animal holding yards.
 - Waste disposal.
- Prior to determining any application for the use of Lot 100 as an abattoir, including any upgrading, alteration or extension of the abattoir use, the Local Government may give notice or require the applicant to give notice for the application to be advertised for public comment for a period of not less than 21 days, and will take into account any submissions lodged during the advertising period when determining the application."
- In determining any application for an abattoir use, the Local Government shall be satisfied that:
 - any alteration or extension of use which cannot reasonably be classified to fall within an abattoir land-use is restricted to uses which are ordinarily classified as being incidental to the abattoir use of the land;
 - the application has been referred to the Department Environment Regulation (DER) and any comments or recommendations received, including but not limited to, odour and noise emissions, discharge of contaminants onto land or into groundwater and surface water plus light spill have been taken into account by the Local Government;
 - the land-use will not present an unacceptable risk to existing sensitive land uses within the off-site buffer.
 - the land-use will not present an unacceptable risk to proposed sensitive land uses outside the off-site buffer.
- All proposed lots within 500 metres from Lot 100 (the DBC Abattoir) shall be subject to a Notification on Title advising occupiers that the lot is located within 500 metres of an abattoir and has the potential to be affected by odour and noise.
- The 'DBC ABATTOIR SPECIAL CONTROL AREA' and the 'OFF-SITE BUFFER' depicted on the structure plan has been established in accordance with the relevant EPA guidelines and Statement of Planning Policy 4.1 - State Industrial Buffer Policy, and has not been determined through a site specific investigation. Any proposed amendment to the 'DBC ABATTOIR SPECIAL CONTROL AREA' and the 'OFF-SITE BUFFER' as a consequence of a site specific study or modelling, to more accurately determine an appropriate land use buffer to sensitive land uses in the locality, will require the structure plan to be reviewed and amended accordingly.



ENDORSED STRUCTURE PLAN - CITY OF BUNBURY

To provide a framework for the future detailed planning at the subdivision and developmental stage.

City of Bunbury delegated under section 11.3.1 of the City of Bunbury Town Planning Scheme No. 7

Date

Sheet 2 of 2 WIMBRIDGE PRECINCT STRUCTURE PLAN

ENDORSED STRUCTURE PLAN To provide a framework for future detailed planning at the subdivision and developmental stage.

Date 16/4/2015
Delegated under s.16 of the Planning & Development Act 2005



100m 0 250 500m
1: 5000 @ A1 or 1: 10000 @ A3
ALL DISTANCES ARE IN METRES

REVISION	DESCRIPTION	DRAFTER	DATE
V			
U			
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M	Modification to provisions	KS	14.04.2015

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ORIGINAL PLANNER:	GL
ORIGINAL DRAFTER:	KS
CREATED DATE:	18.03.2015
AERIAL DATA:	
CADASTRAL DATA:	MGA
TOPOGRAPHIC DATA:	



08304P-SP-02M

PART C – SHIRE OF LEONORA

- 1. Declarations of Due Consideration**
- 2. Disclosure of Interests**
- 3. Form 1 DAP Applications**
 - 3.1 Lot 12 & Lot 13 (No. 70 & 64) Kurrajong Street, Leonora – Workforce Accommodation – DAP/26/03040
- 4. Form 2 DAP Applications**

Nil
- 5. Section 31 SAT Reconsiderations**

Nil

**PART C – Item 3.1 – LOT NO. 12 & 13 (NO. 70 & 64)
KURRAJONG STREET, LEONORA – WORKFORCE
ACCOMMODATION**

**Form 1 – Responsible Authority Report
(Regulation 12)**

DAP Name:	Regional DAP
Local Government Area:	Shire of Leonora
Applicant:	Pinnacle Planning
Owner:	Adam Kelly, Sandra Kelly and Neil Kelly
Value of Development:	\$3.5 million
Responsible Authority:	Shire of Leonora
Authorising Officer:	Ty Matson, Chief Executive Officer
LG Reference:	DA 03/26
DAP File No:	DAP/26/03040
Application Received Date:	6 January 2026
Report Due Date:	20 July 2026
Application Statutory Process Timeframe:	60 days with an additional 117 days agreed
Attachment(s):	1. Development Plans 2. Transport Impact Statement 3. Waste Management Plan 4. Applicant Cover Letter 5. Applicant Response to RFI

Responsible Authority Recommendation

That the Regional DAP resolves to:

1. **Approve** DAP Application reference DAP/26/03040 and accompanying plans in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of the Shire of Leonora Local Planning Scheme No. 2, subject to the following conditions:

Conditions

Administrative Conditions

1. This decision constitutes planning approval only and is valid for a period of 4 years from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect.
2. All development and use of the site must be carried out in accordance with the approved plans and all information submitted as part of this application. These details shall not be altered without prior written consent of the Shire of Leonora.

Amended Plans

3. Prior to applying for a Building Permit application, amended plans to the satisfaction of the Shire of Leonora must be submitted to and approved by the Shire of Leonora. When approved, the plans will be endorsed and will then form part of this approval. The plans must be generally in accordance with the plans submitted with the application/other specified plans, but modified to show the following:
 - a. Screening of air conditioners and any other service infrastructure that is visible from adjoining properties or the nearby public realm to the satisfaction of the Shire of Leonora.
 - b. Dimensions of the water tanks to be provided and also relocated/redesigned so as to not conflict with parking bays or pedestrian walkways, to the satisfaction of the Shire of Leonora.
 - c. The reversing bay, as provided on Lot 12, to be relocated to ensure that sufficient vehicular manoeuvrability is provided.
 - d. A storage area for the storage of alternative waste (including hard, bulk and/or hazardous waste) is to be provided and designed to the requirements of the Shire of Leonora.

Materials and Finishes

4. Prior to applying for a Building Permit, a schedule of materials, finishes and colours shall be submitted to and approved by the Shire of Leonora.

Prior to the occupation of the development, the approved external finishes and colour schemes are to be implemented to the satisfaction of the Shire of Leonora and maintained thereafter.

Landscaping

5. Prior to applying for a Building Permit, a Landscape Plan for the development site and the adjoining road verges is to be submitted to and approved by the Shire of Leonora. The following details are to be included:
 - a. Landscape treatments such as lawn, mulch areas, and bin collection areas.
 - b. Canopy shade trees to be planted adjacent to light vehicle parking areas at a ratio of 1 tree per 4 car bays. The shade trees are to be evenly spread throughout the parking areas.
 - c. The location, species, quantity and pot size of proposed trees and shrubs.
 - d. Areas to be irrigated.
 - e. Pedestrian paths.

Prior to occupation of the development, the landscaping and irrigation of the development site and the adjoining verges is to be installed in accordance with

an approved landscape plan and thereafter maintained to the satisfaction of the Shire of Leonora. Any species that fail to establish within the first two (2) planting seasons following implementation must be replaced in consultation with and to the satisfaction of the Shire of Leonora.

Stormwater Management

6. Prior to works commencing, a Stormwater Management Plan must be submitted to and approved by the Shire of Leonora. Once approved the plan will form part of this approval and must be complied with at all times for the life of this approval.

Fencing

7. Prior to the commencement of any works, a Fencing Plan shall be submitted to and approved by the Shire of Leonora. Once approved, the fencing shall be installed prior to the occupation of the development and maintained in perpetuity, to the satisfaction of the Shire of Leonora.

Access and Parking

8. Prior to occupation or use of the development, all crossovers shall be constructed and sealed to the satisfaction of the Shire of Leonora.
9. The approved parking areas must be fully implemented and constructed to the specification of the Shire of Leonora prior to the occupation or use of the site/development. The parking areas must contain the following:
 - a. All car bays and circulation areas designed to a 'User class 3A' with a B99 vehicle, in accordance with the Australian Standards 2890.1:2004 (or as amended).
 - b. Be sealed (bitumen or concrete), kerbed, drained and line marked.
 - c. Not be obstructed in any way or used for any purpose other than parking.
 - d. Include wheel stops to prevent vehicles interfering with landscaping and pedestrian pathways/movement areas.
 - e. Car bays designed to Australian Standards 2890.6:2004 for bollard & post mounted disabled signage.

The parking spaces shall comply with the above requirements for the duration of the development and shall be maintained to the satisfaction of the Shire of Leonora.

Waste Management

10. The development must be designed, and all works must be carried out in accordance with the approved Waste Management Plan, and the bin storage area being suitably screened at all times from the public realm to the satisfaction of the Shire of Leonora for the duration of the development.

General Conditions

11. Prior to occupation of the development, the landowner shall demonstrate to the Shire of Leonora that the development is suitably connected to reticulated sewer, to the satisfaction of the Shire of Leonora.
12. Prior to occupation of the development, the landowner shall demonstrate to the Shire of Leonora that the development is suitably connected to scheme water, to the satisfaction of the Shire of Leonora.

Advice Notes

1. In respect to the condition requiring for the amendment to the reversing bay on Lot 12, the Shire's preference would be:
 - a. The current reversing bay to replace the disabled bay;
 - b. The current shared bay to replace the reversing bay; and
 - c. The current disabled bay to replace the shared bay.

Details: outline of development application

Region Scheme	N/A
Region Scheme - Zone/Reserve	N/A
Local Planning Scheme	Local Planning Scheme No. 2
Local Planning Scheme - Zone/Reserve	Light Industry
Structure Plan/Precinct Plan	N/A
Structure Plan/Precinct Plan - Land Use Designation	N/A
Use Class and permissibility:	Workforce Accommodation – Discretionary 'D'
Lot Size:	Lot 12 – 2,817m ² Lot 13 – 2,616m ² Aggregate – 5,433m ²
Existing Land Use:	Lot 12 – Vacant Lot 13 – Workforce Accommodation, Warehouse/ Storage, Commercial Vehicle Parking
State Heritage Register	No
Local Heritage	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Heritage List <input type="checkbox"/> Heritage Area
Design Review	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other
Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

The development is a 'Workforce Accommodation' uses intending to accommodate up to 79 occupants. The works and use include the following:

- 20 new dongas (1x 3-Bedroom; 19x 4-Bedroom).
- 1x new kitchen/dining and 1x new Laundry on Lot 12.
- 1x 3-Bedrom Donga and 1x 2-Bedroom Donga on Lot 13 to be retained.
- 2x Kitchen/Laundry/Bath/1-Bedroom Donga on Lot 13 to be retained.
- 1x Gym to be retained on Lot 13.
- 1x Office to be removed from Lot 13.
- A maximum of 84 workers and eight (8) staff – an increase of 79 workers and four (4) staff.
- 34 parking bays, inclusive of 19 bays on Lot 12 (No. 70) Kurrajong Street and 15 bays Lot 13 (No. 64) Kurrajong Street (inclusive of one (1) bay within the carport).

Proposed Land Use	Workforce Accommodation
Proposed Net Lettable Area	N/A
Proposed No. Storeys	1
Proposed No. Dwellings	N/A

Background:

The subject site measures 5,433m² and exists within the industrial area located to the south-west of Leonora Townsite. Specifically, the site is situated within a corridor bound by Kurrajong Street to the east and the freight railway line to the west.

The general surrounding locality is of similar industrial use, with the lots to the immediate north and south of the subject site being vacant. Most lots are characterised by outbuildings, commercial vehicle parking, transportable buildings and sea containers.

A locality plan is provided at Figure 1 and the proposed development plans are provided as **Attachment 1**.



Figure 1 – Location Plan and Aerial Photo

History

Lot 13 (No. 64) Kurrajong Street obtained development approval at the Shire's Council in October 2023. The approval consisted of 'Commercial Vehicle Parking', 'Warehouse/Storage', and 'Workforce Accommodation'. The following is relevant to this approval:

- The 'Workforce Accommodation' is to remain and be amended to contain more dongas and workers across Lot 13 (No. 64) Kurrajong Street.
- The previous 'Commercial Vehicle Parking' and 'Warehouse/Storage' will no longer be required due to the removal of the sea containers and dome shelter, as represented on the development plans contained within **Attachment 1**.

Legislation and Policy:

Legislation

- *Planning and Development Act 2005*
- *Planning and Development (Development Assessment Panels) Regulations 2011*
- *Planning and Development (Local Planning Schemes) Regulations 2015*
- Shire of Leonora Local Planning Scheme No. 2 (**LPS2**)

State Government Policies

- State Planning Policy 5.4 – Road and Rail Noise (**SPP5.4**)
- State Planning Policy 7.0 – Design of the Built Environment (**SPP7.0**)

Position Statements/Other

- Position Statement – Workforce Accommodation

Consultation:

Public Consultation

The application was not required to be advertised to the public for consultation purposes.

Referrals/consultation with Government/Service Agencies

No referrals were required.

Design Review Panel Advice

Not applicable.

Planning Assessment:

The proposal has been assessed against the relevant requirements of the State and Shire's planning framework, as outlined in this report. The minor subjects have been identified in the table below. Detailed discussion on other matters important to the assessment are after the table.

Assessment against Schedule 2 of LPS2			
Provision	Requirement	Proposal	Assessment
3(a)	Development in the Light Industry shall be in conformity with the following: <ul style="list-style-type: none">• Minimum site area of 2,000m².• Minimum street setback of 9m.• Parking calculated at 1 bay per 65m² of Net Lettable Area (NLA).	<ul style="list-style-type: none">• Both lots are larger than 2,000m².• Lot 12 – approximately 7m to the closest water tank and 10.1m to the closest donga. <p>Lot 13 – approximately 4.9m from the street.</p> <ul style="list-style-type: none">• 34 bays provided.	<ul style="list-style-type: none">• Site area complies.• Street setback does not comply for Lot 13 and is discussed further in the report. Lot 12 complies. <p><u>Parking:</u></p> <ul style="list-style-type: none">• Total NLA = approximately 934.8m²• 15 bays required. Complies with a surplus of 19 bays.
3(c)	Development of land for workforce accommodation in the Light Industry zone is subject to development	Workforce Accommodation	Detailed discussion on the design, parking, traffic, noise sensitivity, waste and landscaping is

	approval of the local government and any conditions imposed by the local government relating to standards including buildings, facilities, car parking and landscaping, and may include as a condition a time limit on development.		provided below the table.
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Definitions and Land use Permissibility

Under Division 2 – Land use terms used, Part 6 of LPS2, Part 6, a ‘Workforce Accommodation’ land use is defined as follows:

means premises, which may include modular or relocatable buildings, used –

- (a) *primarily for the accommodation of workers engaged in construction, resource, agricultural or other industries on a temporary basis; and*
- (b) *for any associated catering, sporting and recreation facilities for the occupants and authorised visitors.*

The proposal is intended to be used as a base for the operations of Goldfields Controlled Waste to service mining and related businesses in the Goldfields Region. The proposal is to accommodate fly-in fly-out (**FIFO**) staff. The proposal is therefore consistent with the definition of ‘Workforce Accommodation’ under LPS2.

Under Table 3 – Zoning Table of LPS2, a ‘Workforce Accommodation’ land use is a ‘D’ use within the ‘Light Industry’ zone. Pursuant to clause 18(2) of LPS2, the symbol has the following meaning:

‘D’ – means that the use is not permitted unless the local government has exercised its discretion by granting development approval.

The proposed development is therefore capable of approval.

Traffic and Access

A Transport Impact Statement (**TIS**) was provided by the Applicant (refer Attachment 2). A summary of the findings from the TIS is provided below:

- The proposed development is estimated to generate 29 vehicles per hour in both the AM and PM peak hours.
- Pursuant to the Western Australian Planning Commission’s Transport Impact Assessment Guidelines, the expected increase of traffic of 29 vehicles in the peak hour would have a moderate impact. However, noting that the range of a moderate impact is between 10 to 100 vehicle trips in the peak hour, it is agreed

with the TIS findings that the development will likely create a low-moderate impact.

- The southern crossover of Lot 12 (No. 70) Kurrajong Street is within 6m of the Kurrajong Street and Steel Street intersection. The location however has been deemed acceptable noting that crossover is an entry only, in addition to having relatively low and infrequent use.

In respect to safety internally, it is noted that wheel stops are required on Lot 12 to protect pedestrians, landscaping and damage to infrastructure. This was agreed to be provided by the Applicant as part of the request for further information per Attachment 5. This was however missed when the amended plans were received. As this is a minor design change, it was agreed that this matter could be imposed as a condition of approval.

Additionally, the proposed reversing bay will not adequately work due to the location of the bin store. It is therefore recommended that the reversing bay, shared space and disabled bay should switch positions to have the shared bay being the closest to the bin store to allow for adequate manoeuvrability. As a relatively minor design change, this is recommended to be imposed as a condition of approval.

Both traffic and access are therefore deemed acceptable for the proposal.

Building Design and Landscaping

Further information was requested by the assessing officer to the Applicant to improve the streetscape design, largely due to the utilities/services fronting the street on Lot 13, the sparse landscaping, and the small street setbacks proposed.

In response, the Applicant provided amended plans which only partly addressed the landscaping and not the building design to the street. The street and landscaping would be greatly improved by screening the utilities/services, providing canopy shade trees to parking bays, landscaping within the front setback of Lot 13 and within the road verge. While understanding that this may be an item which the DAP may wish to defer the application for, it is considered that this could be appropriately conditioned without adversely impacting the proposal. Conditions in this respect have therefore been recommended.

Waste Management

A Waste Management Plan (**WMP**) has been provided by the Applicant (Attachment 3). The WMP has calculated that there will be approximately 1,600L of refuse and 800L of recycling per week.

As there is only 2x 1,100L bins proposed, the Applicant has acknowledged that a private contractor will be used and may require to be used twice a week for the refuse.

Alternative waste (such as hard, bulk or hazardous waste) will be removed through occupants coordinating with local municipal verge valet or other private bulk waste removal services. As it is unclear where this alternative waste will be stored, amended plans are required to clearly demonstrate a suitable area for this storage (inclusive of design requirements). Amended plans in this respect has therefore been recommended as a condition of approval.

Rail Noise

The subject site is located within a 'Freight Railway Trigger Distance' as defined by SPP5.4. SPP5.4 is a state policy used to minimise impact of road and rail noise on noise-sensitive land use and development.

Noise sensitive land use is defined within SPP5.4 as:

"Land-uses or development occupied or designed for occupation or use for residential purposes (including dwellings, residential buildings or shortstay accommodation), caravan park, camping ground, educational establishment, child care premises, hospital, nursing home, corrective institution or place of worship."

Whilst it is not clear from the policy whether 'Workforce Accommodation' falls under the definition of a noise-sensitive land use, the wording 'designed for occupation' suggests the Workforce Accommodation may be considered a sensitive land use.

However, it is acknowledged that the following points are relevant:

- There is only one (1) train movement per day and only operates during daylight hours.
- The level of amenity expected for Workforce Accommodation uses, particularly within a 'Light Industry' zone will not be the same as a 'residential' development on residential zoned land.
- Currently, the railway is planned to being decommissioned within the next 1-2 years.

While it is deemed that the proposal is a noise sensitive land use and the provisions of SPP5.4 should apply, the above points have demonstrated that the need for such requirements is not considered necessary.

Amalgamation

Ordinarily a development such as this which occurs over two (2) or more lots would be required to be amalgamated due to the sharing of services, utilities and/or buildings over boundaries.

However, considering that both properties can generally operate independently from one another, with acceptable parking provided, the requirement for the lots to be amalgamated is not recommended. While the only service that would be shared is for waste management, should one of the properties ever cease or be sold, there is adequate room on Lot 13 (No. 64) Kurrajong Street for a bin store to be created and utilised.

Other Matters:

The following matters are considered relatively minor and have been agreed to with the Applicant to be addressed (via their response letter at Attachment 5). It is the assessing officer's position that these matters can be addressed via appropriately worded conditions of approval. The matters are as follows:

1. The water tanks on Lot 12 to be amended to not protrude into parking bays. This was previously requested and agreed to but was not completed.
2. A fencing plan is to be provided, including the provision of permeable fencing for the street boundary. As above, this was previously requested and agreed to but was not completed.

Conclusion:

This report has provided a detailed assessment of the proposed application at Lot 12 and 13 (No. 70 and 64) Kurrajong Street, Leonora.

The development has been considered by the Shire and the development is deemed to be acceptable subject to the conditions recommended. It is considered that the proposed development generally meets the development standards under the planning framework and is a much-needed form of development within the Shire.

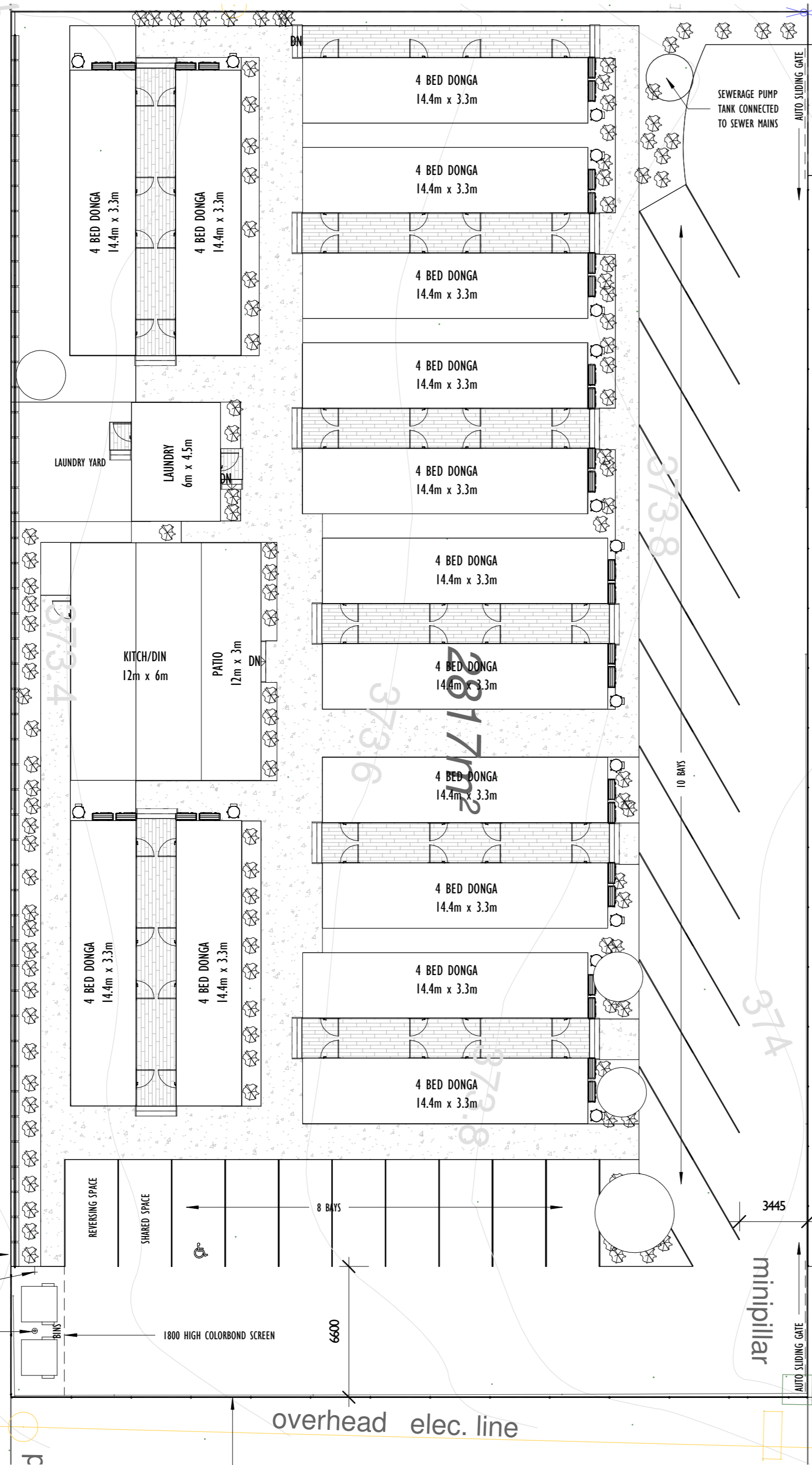
WASTE WATER NOTE:
SEWER MAINS TO BE INSTALLED TO PROPERTY AS PART OF THE PROPOSED WORKS.

1800 HIGH COLORBOND FENCE WITH 2 ROWS OF BARBED WIRE ABOVE

1800 HIGH COLORBOND FENCE WITH 2 ROWS OF BARBED WIRE ABOVE

TAP
DRAIN WITH GROUND AROUND SLOPED TO FALL FOR BIN AREA. DRAIN TO CONNECT TO SEWER MAINS

1800 HIGH COLORBOND FENCE WITH 2 ROWS OF BARBED WIRE ABOVE



water main

STREET

under ground tel. line

underground elec.

KURAJONG

power pole

minipillar

possible private sewer main??

STREET

role

1
A02 Site
1 : 200

SHEET SIZE		A2
REV NO	DESCRIPTION	DATE



M: 0417 957 213

KELLY
70 KURAJONG ST,
LEONORA

CONCEPT LAYOUT

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Drawn by	NL	A02
Checked by	NL	
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COLOURS

WALLS AND ROOF'S OF UNITS TO BE SURFIMIST COLOUR.
FLASHINGS TO BE DEEP OCEAN COLOUR.

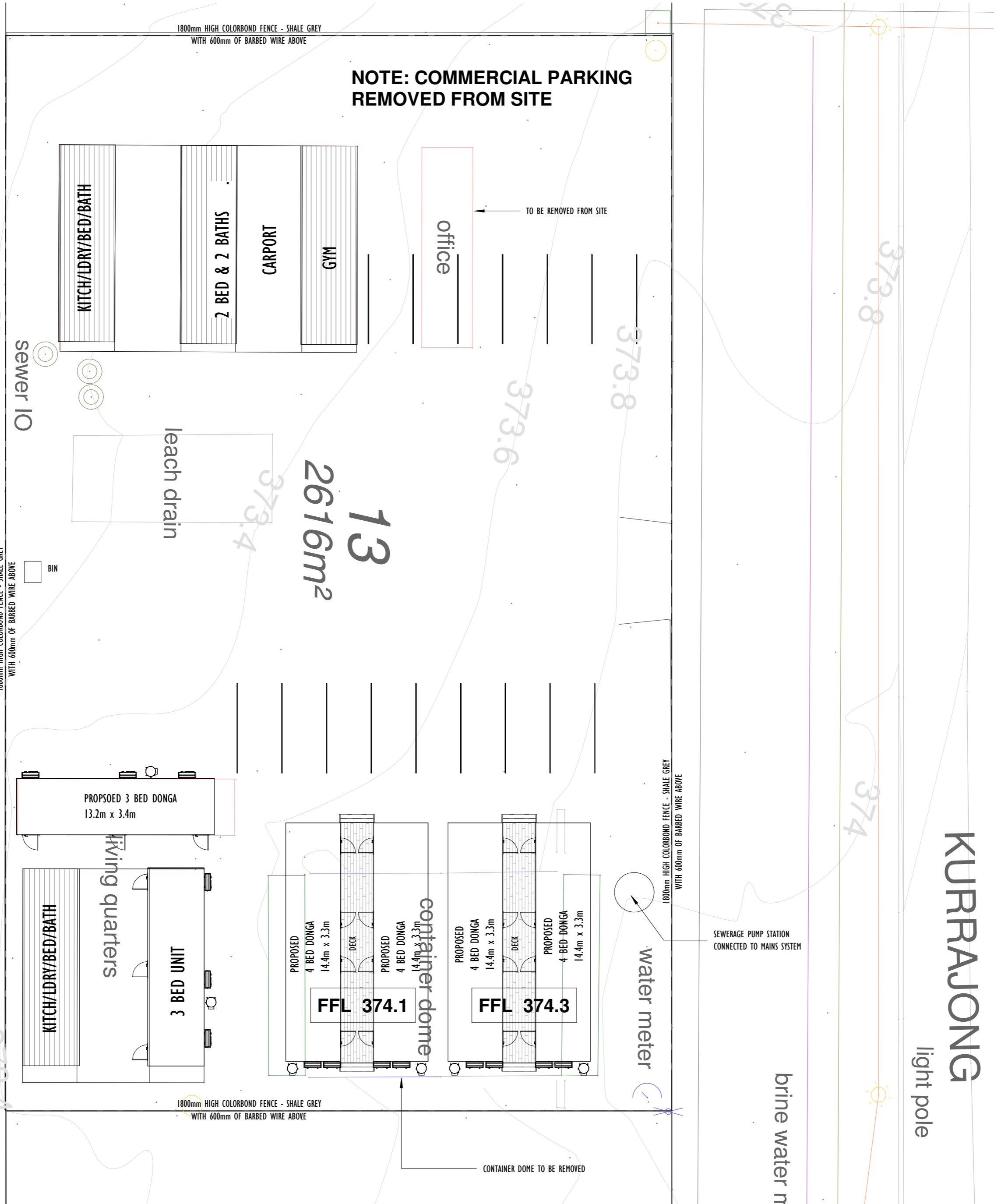
WASTE WATER NOTE-
SEWER MAINS TO BE INSTALLED TO PROPERTY
AS PART OF THE PROPOSED WORKS.

KURRAJONG

SITE

1
A04

1 : 200



**NOTE: COMMERCIAL PARKING
REMOVED FROM SITE**

TO BE REMOVED FROM SITE

office

2616m²
13

PROPOSED 3 BED DONGA
13.2m x 3.4m

KITCH/LDRY/BED/BATH
living quarters
3 BED UNIT

PROPOSED 4 BED DONGA
14.4m x 3.3m
DECK
FFL 374.1
container dome
FFL 374.3
PROPOSED 4 BED DONGA
14.4m x 3.3m
DECK
PROPOSED 4 BED DONGA
14.4m x 3.3m

water meter

SEWERAGE PUMP STATION
CONNECTED TO MAINS SYSTEM

brine water n

light pole

KURRAJONG

SHEET SIZE		A2
REV NO	DESCRIPTION	DATE



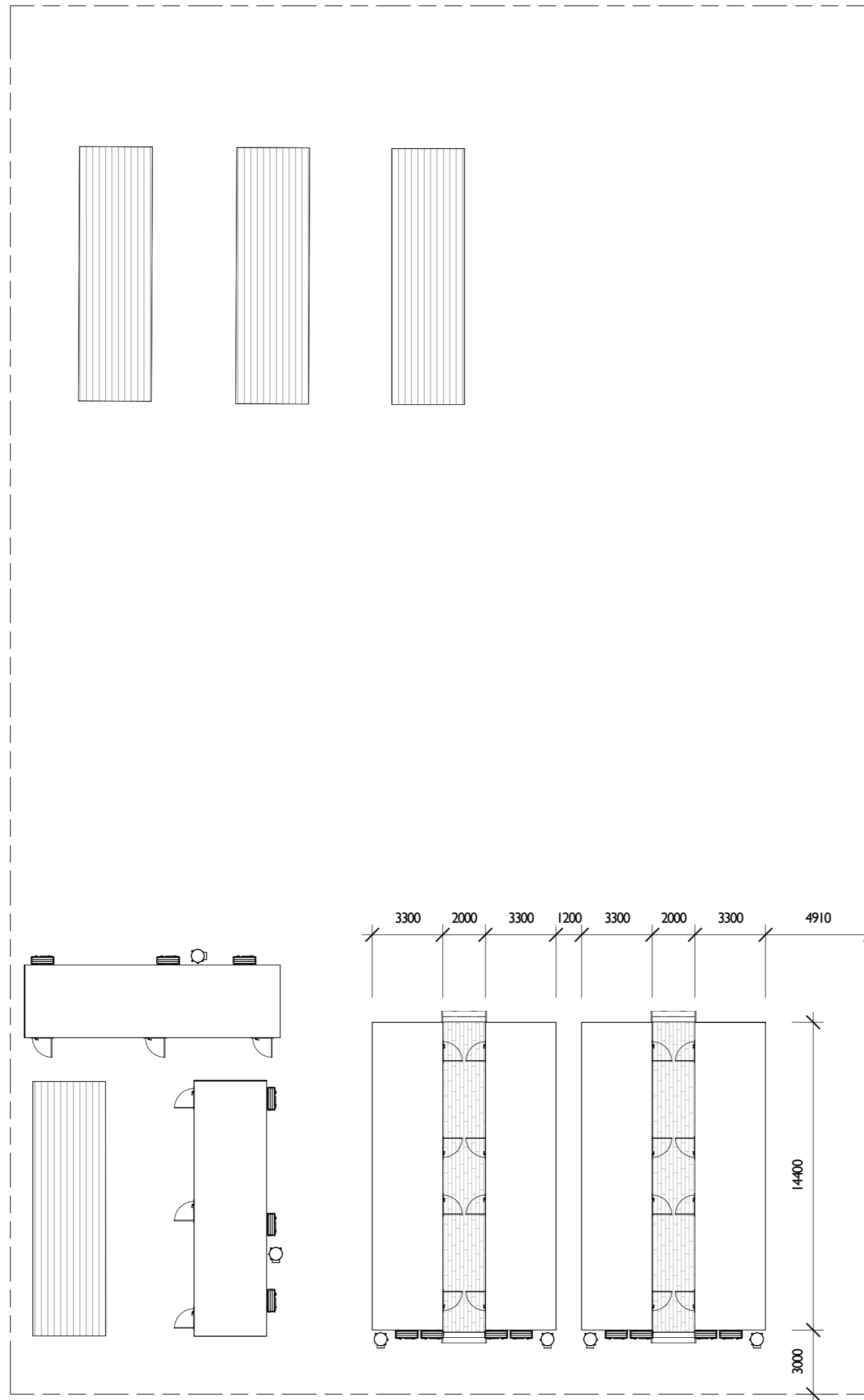
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KELLY
LOT 13 KURRAJONG ST,
LEONORA

CONCEPT LAYOUT

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SETOUT

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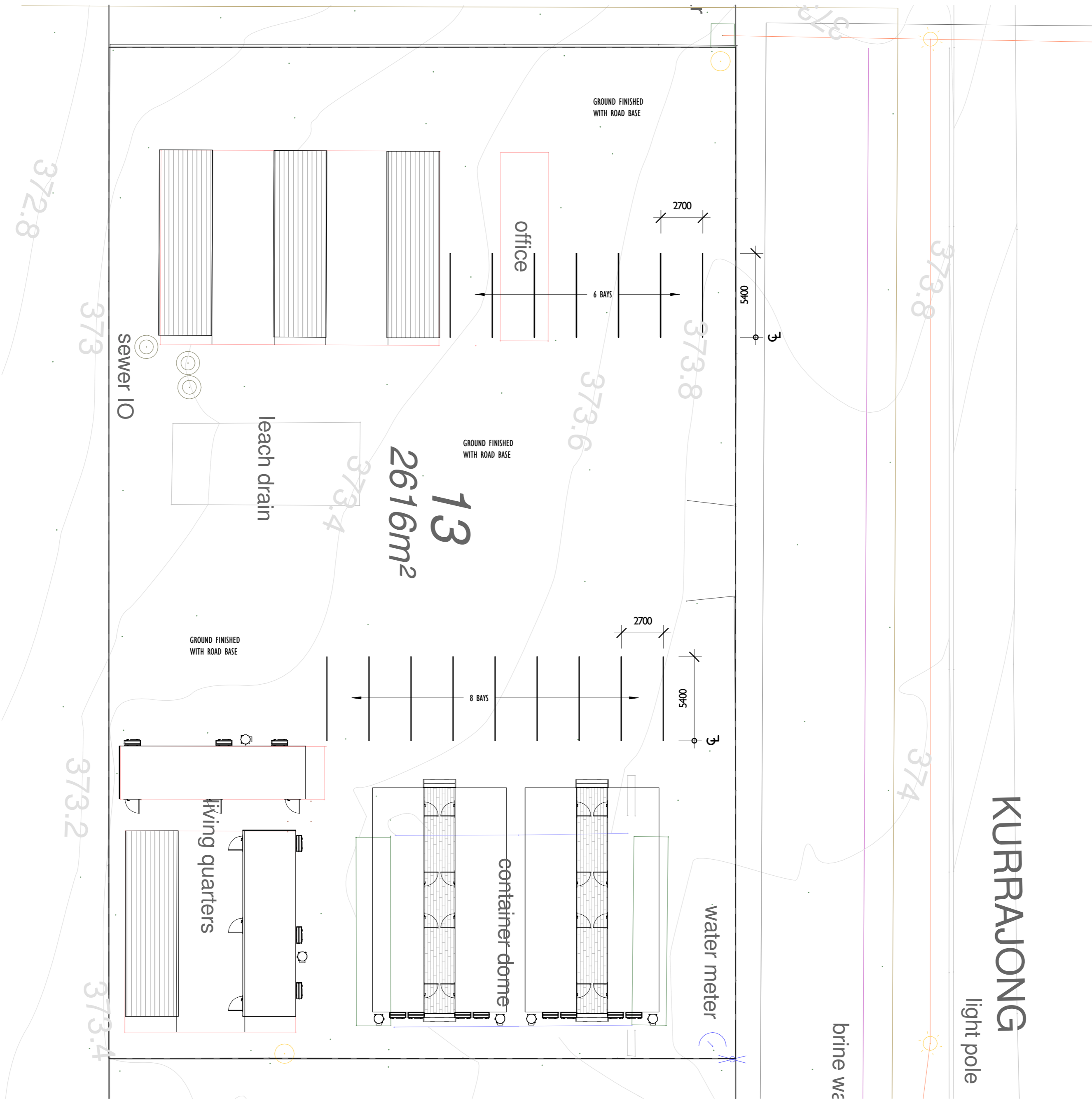
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KELLY
LOT 13 KURRAJONG ST,
LEONORA

SETOUT

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A06
CARPARKING
1 : 200

SHEET SIZE		A2
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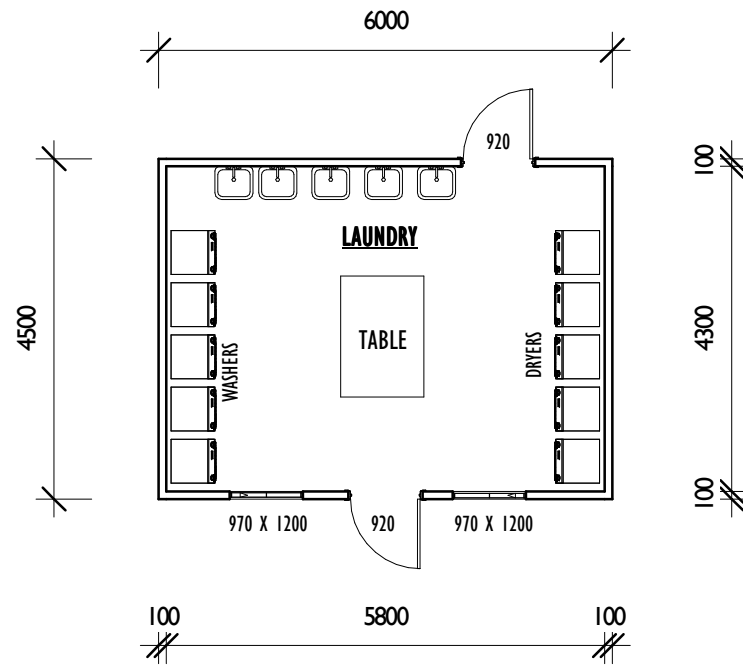
KELLY
LOT 13 KURRAJONG ST,
LEONORA

CARPARKING

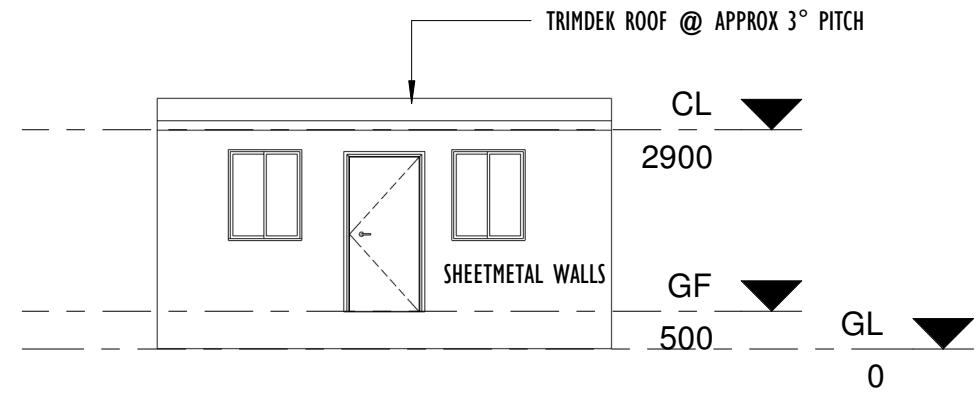
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BUILDING AREA	
LAUNDRY	27.00 m ²
TOTAL AREA	27.00 m ²



1
A07
1.GF
1 : 100



2
A07
FRONT
1 : 100

NOTES

1) THESE DRAWINGS ARE TO BE READ IN CONJUNCTION WITH ALL SUPPLIED DOCUMENTATION, INCLUDING BUT NOT LIMITED TO ENERGY ASSESSMENTS, ENGINEERING, SPECIFICATION AND BUSHFIRE ASSESSMENT WHERE APPLICABLE

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8) ALL FLUED EXHAUSTS TO BE FITTED WITH DAMPIER TO IMPEAD AIR LOSS

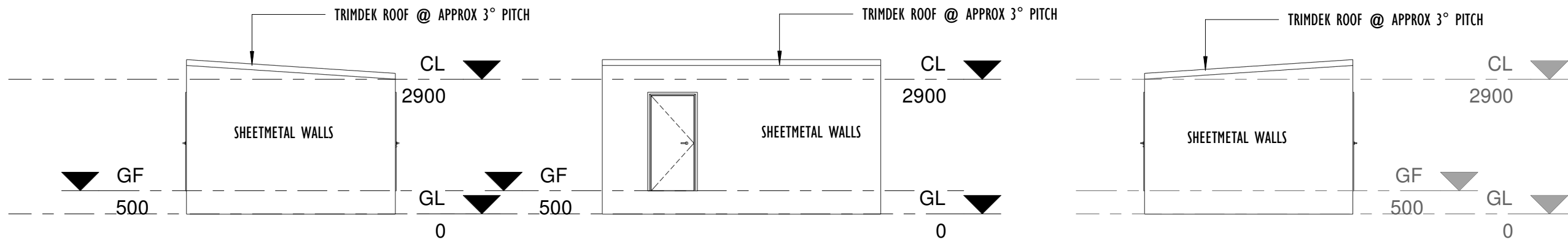
9) MINIMUM TERMITE TREATMENT REQUIREMENTS IS ALL PRIMARY ELEMENTS TO BE TERMITE RESISTANT. FURTHER TREATMENT AT BUILDERS DISCRETION.

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3
A07
LEFT SIDE
1 : 100

4
A07
REAR
1 : 100

5
A07
RIGHT SIDE
1 : 100

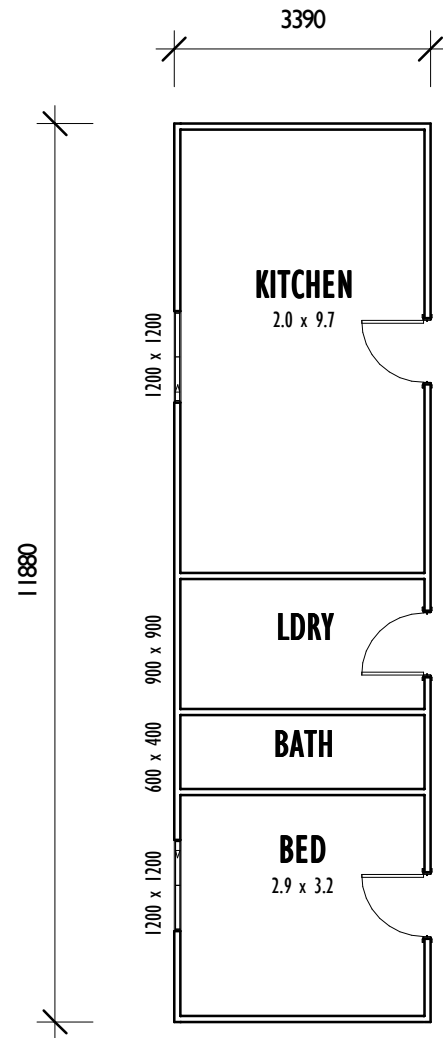
GOLDFIELDS CONTROLLED WASTE	LAUNDRY		SHEET SIZE		A3
			REV NO	DESCRIPTION	DATE
70 KURAJONG ST, LEONORA	A3	Date	OCT 25	A07	
	Drawn by	NL			
	Checked by	NL	Scale		1 : 100



M: 0417 957 213

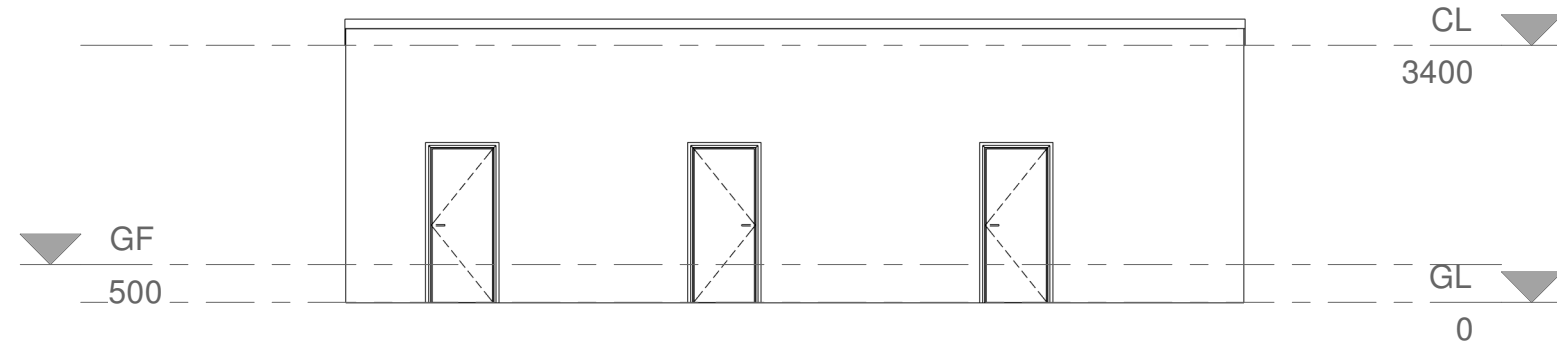
BUILDING AREA

Area	40.27 m ²
TOTAL AREA	40.27 m ²

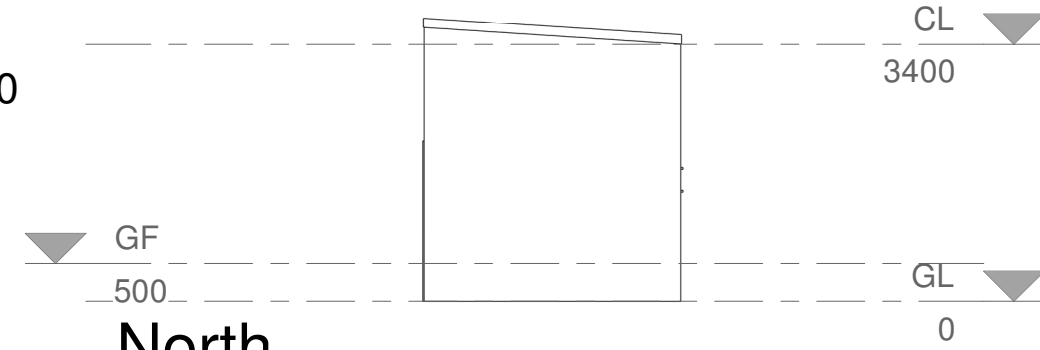


1
A01
1.GF
1 : 100

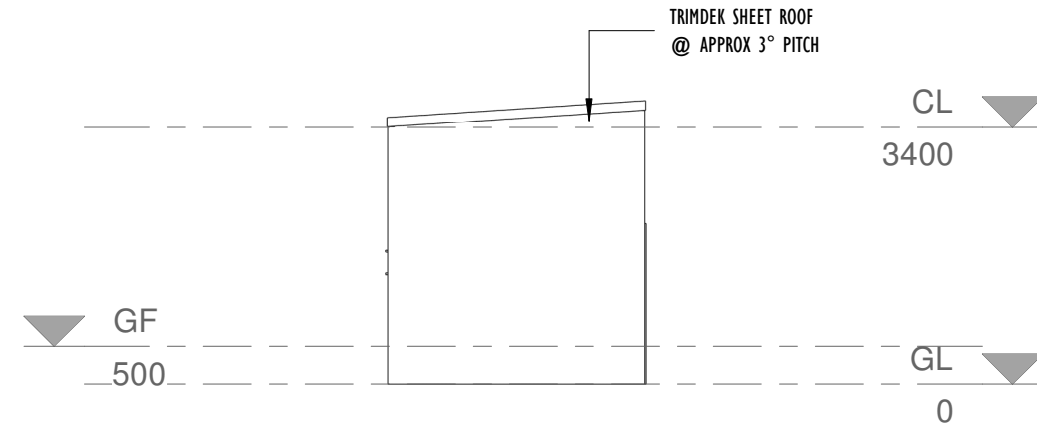
2
A01
East
1 : 100



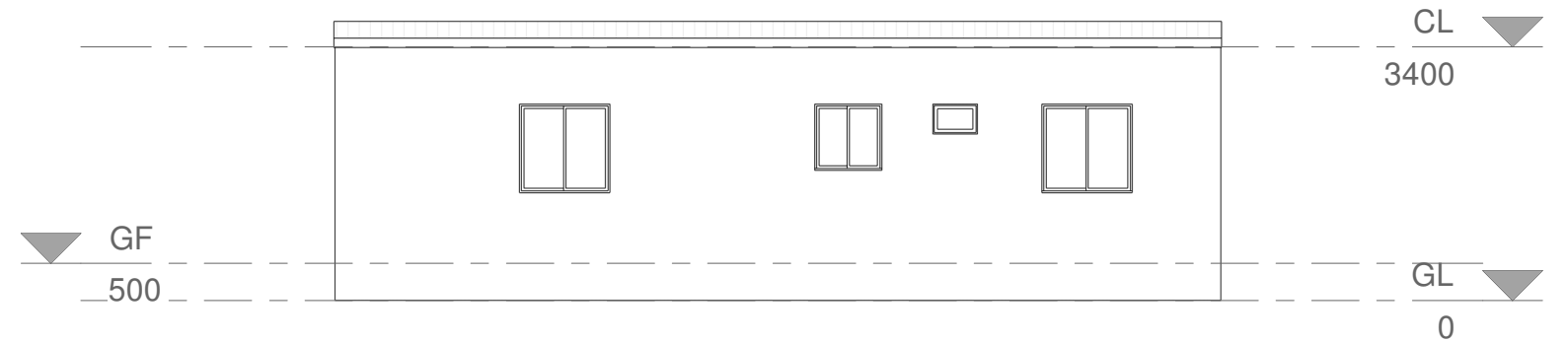
3
A01
North
1 : 100



4
A01
South
1 : 100



5
A01
West
1 : 100



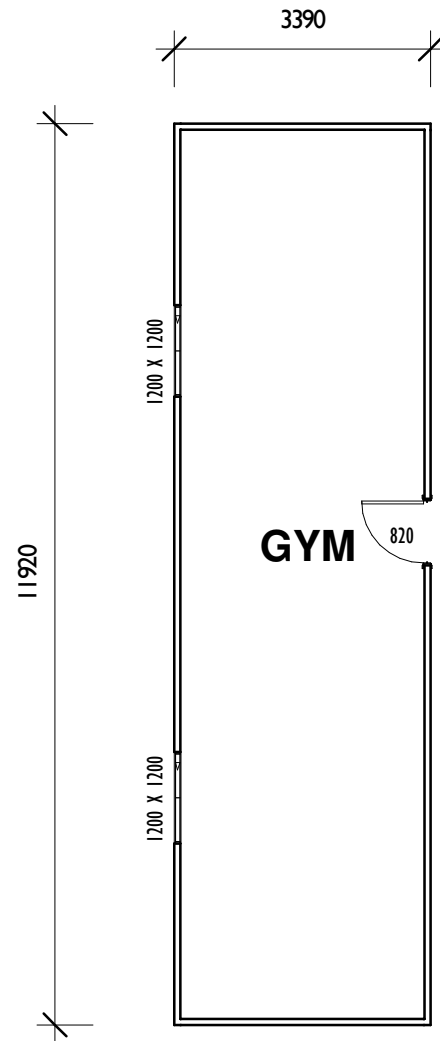
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GOLDFIELDS CONTROLLED WASTE LOT 13 KURAJONG ST, LEONORA	KITCH, BED, LDRY UNIT		SHEET SIZE A3		
	REV NO	DESCRIPTION	DATE		
A3	Date	FEB 26	A01 Scale 1 : 100		
	Drawn by	NL			
	Checked by	NL			

BUILDING AREA

Area	40.41 m ²
TOTAL AREA	40.41 m ²



NOTES

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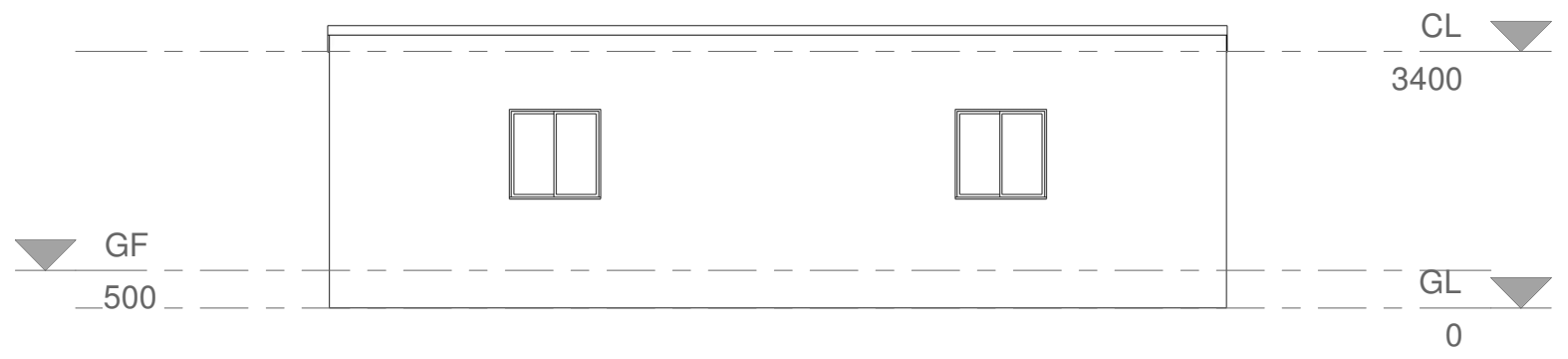
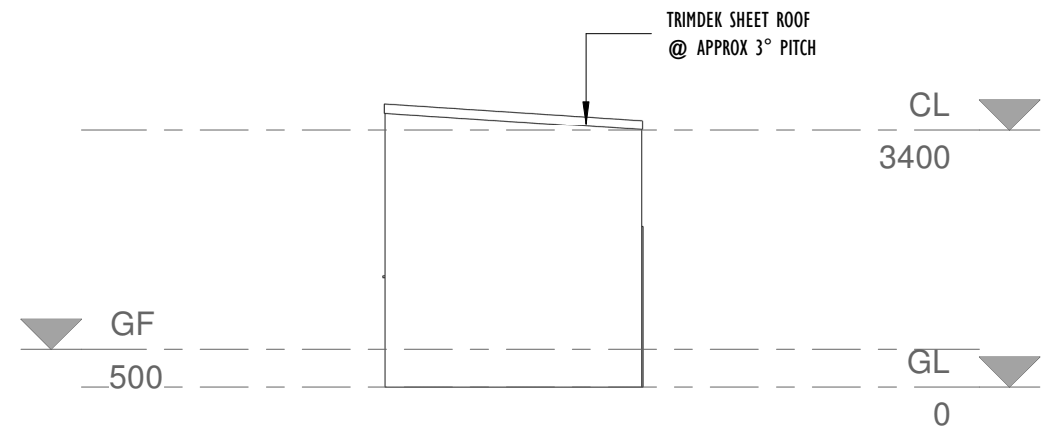
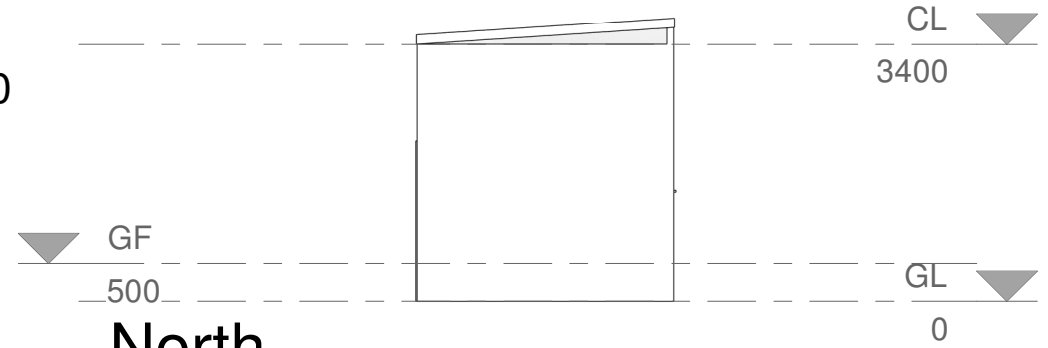
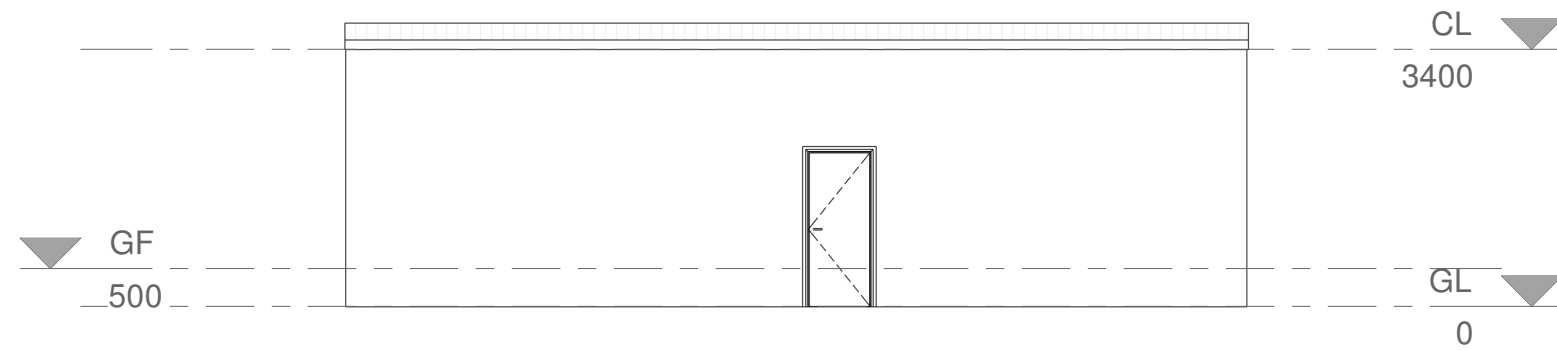
1
A01
1.GF
1 : 100

4
A01
South
1 : 100


5
A01
West
1 : 100

2
A01
East
1 : 100

3
A01
North
1 : 100

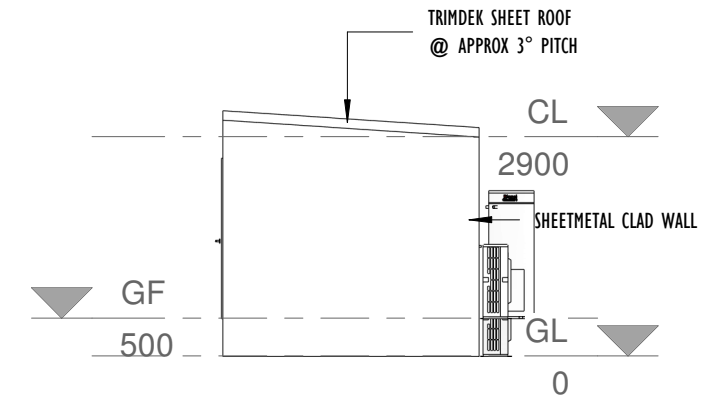
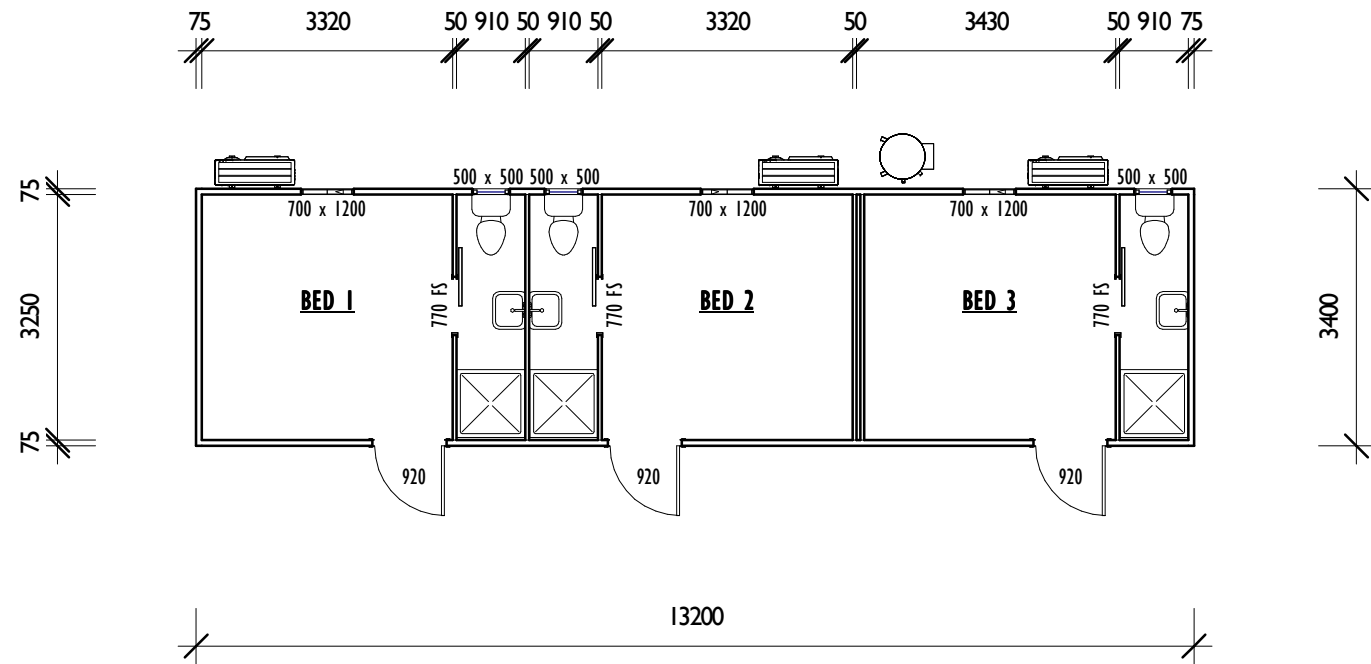


GOLDFIELDS CONTROLLED WASTE LOT 13 KURAJONG ST, LEONORA	GYM		SHEET SIZE		A3
	REV NO	DESCRIPTION	DATE		
A3	Date	FEB 26	A01		
Drawn by		NL			
Checked by		NL	Scale	1 : 100	



M: 0417 957 213

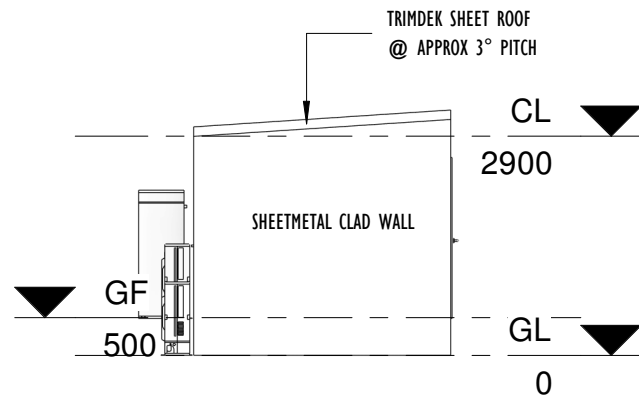
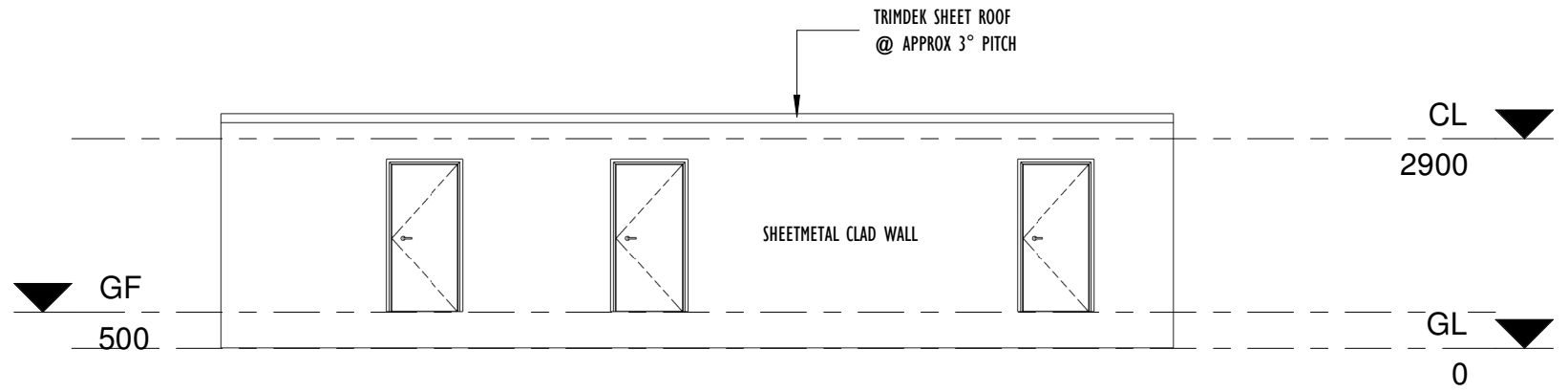
BUILDING AREA	
4 BED UNIT	44.88 m ²
TOTAL AREA	44.88 m ²



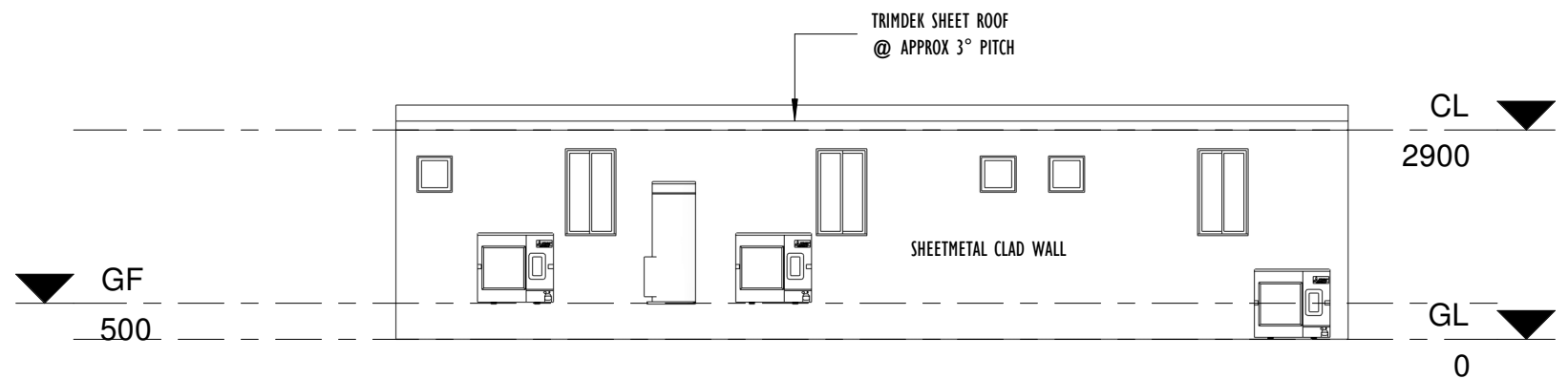
2
A10
RIGHT SIDE
1 : 100

1
A10
1.GF
1 : 100

3
A10
FRONT
1 : 100



4
A10
LEFT SIDE
1 : 100



5
A10
REAR
1 : 100

NOTES

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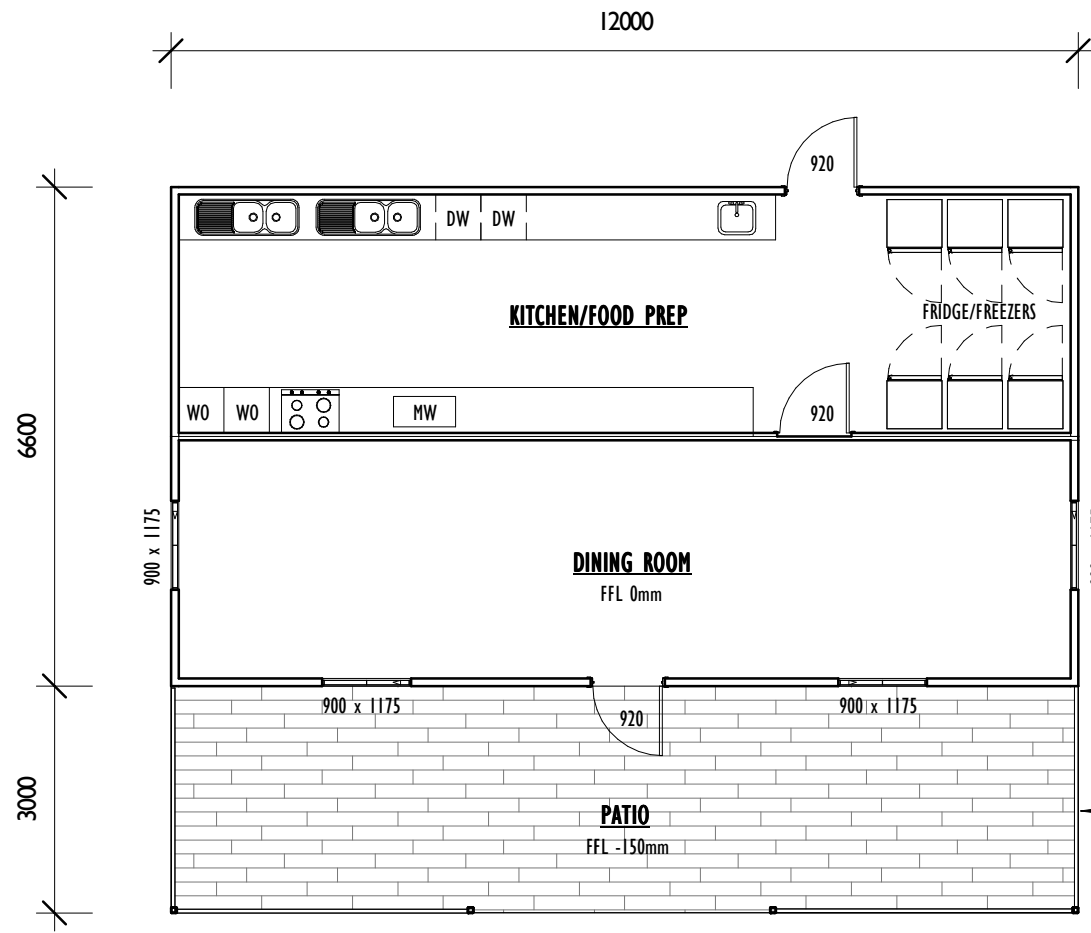
GOLDFIELDS CONTROLLED WASTE	3 BED UNIT		SHEET SIZE		A3
	REV NO	DESCRIPTION	DATE		
70 KURAJONG ST, LEONORA	A3	Date	OCT 25	A10	
	Drawn by	NL			
	Checked by	NL	Scale	1 : 100	



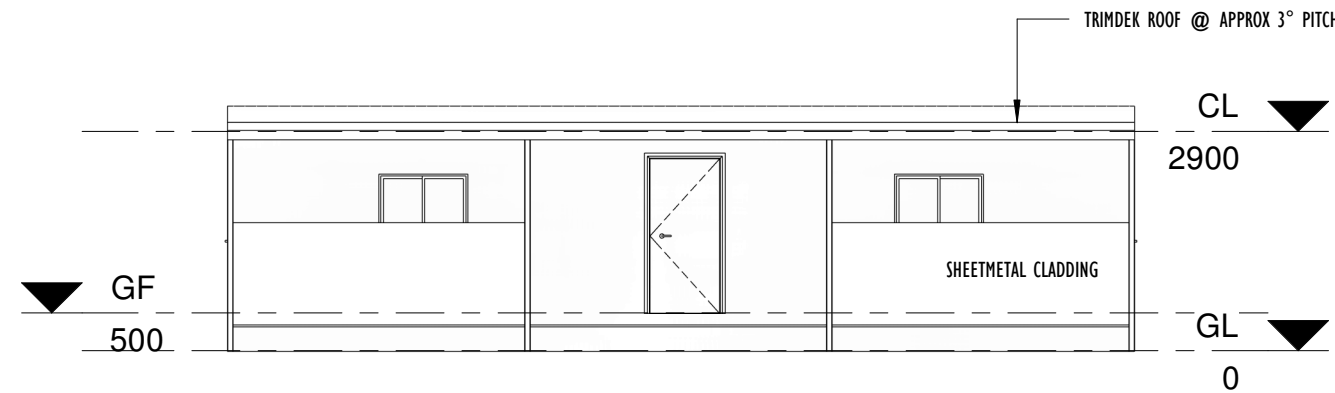
M: 0417 957 213

BUILDING AREA

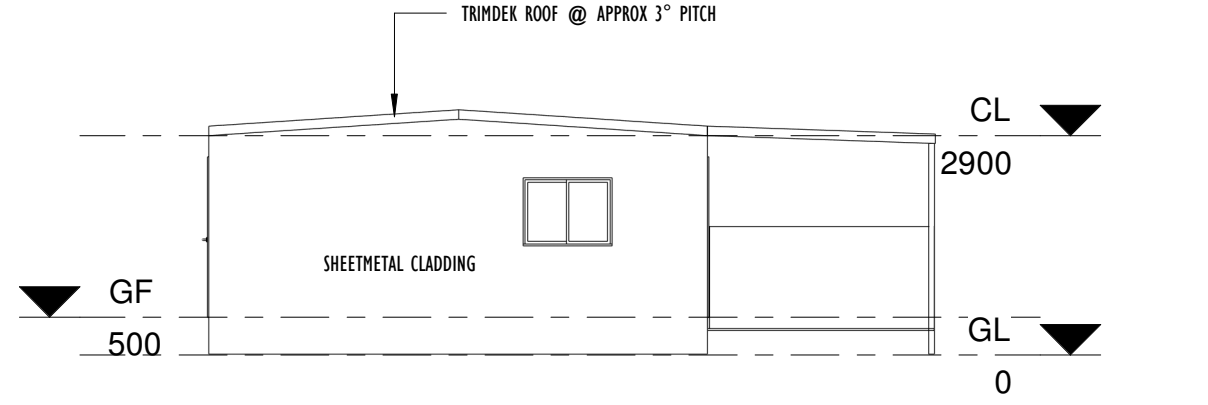
KITCHEN	79.20 m ²
PATIO	35.97 m ²
TOTAL AREA	115.17 m ²



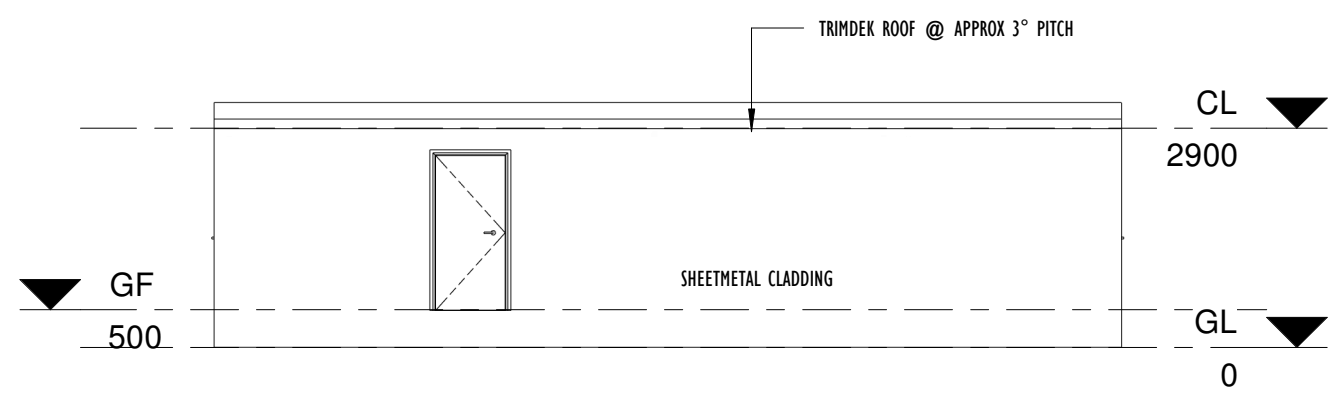
1
A08
1.GF
1 : 100



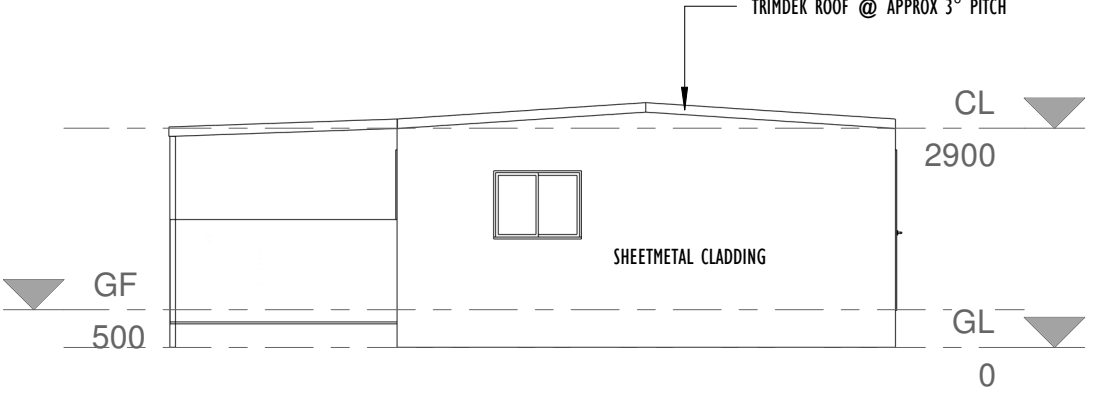
2
A08
FRONT
1 : 100



3
A08
LEFT SIDE
1 : 100



4
A08
REAR
1 : 100




5
A08
RIGHT SIDE
1 : 100

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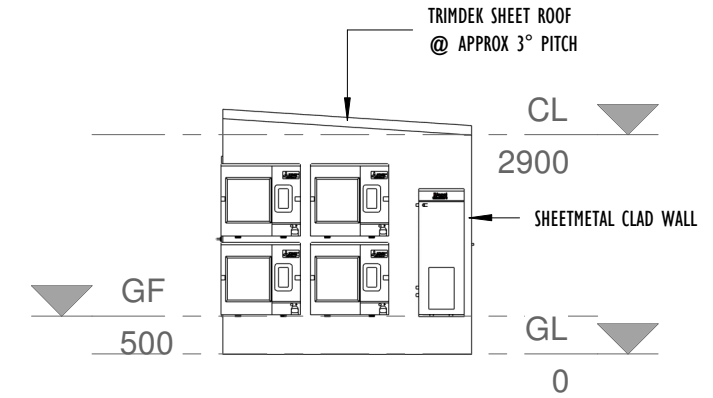
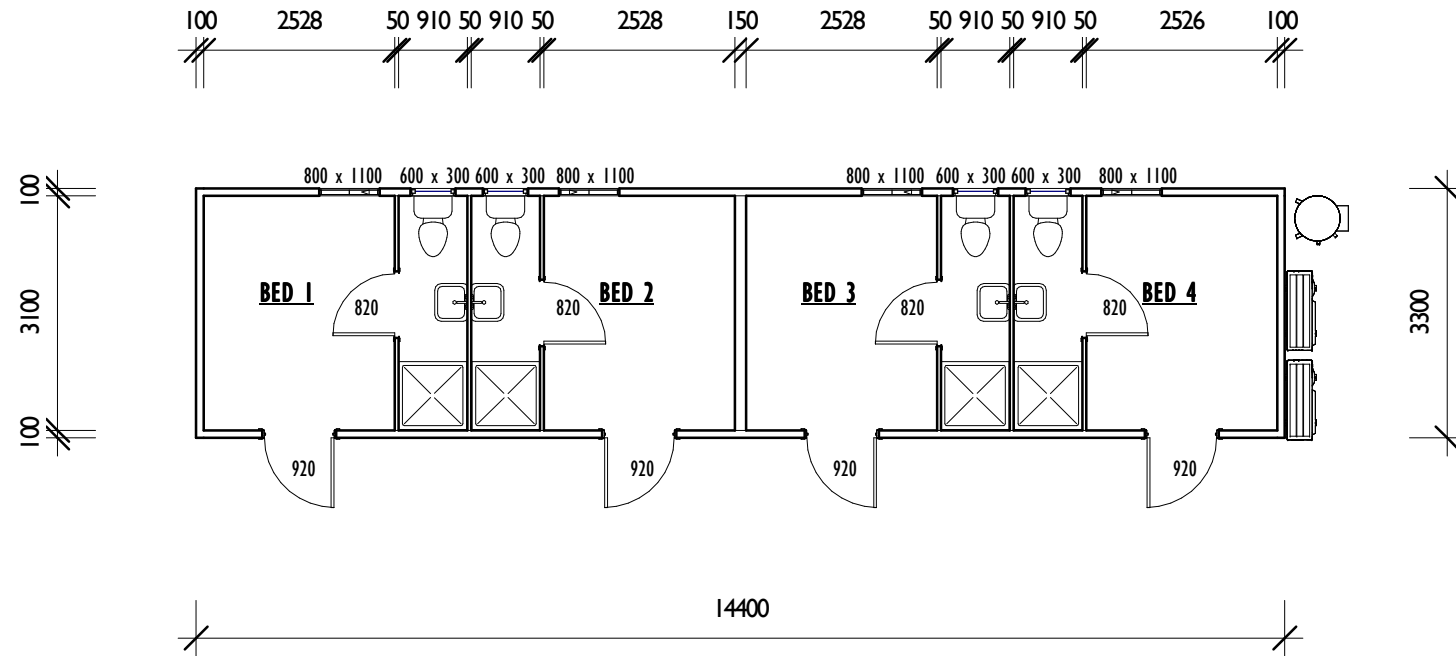
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GOLDFIELDS CONTROLLED WASTE	KITCHEN		SHEET SIZE		A3
	REV NO	DESCRIPTION	DATE		
70 KURAJONG ST, LEONORA	A3	Date	MAY 26	A08	
	Drawn by	NL			
	Checked by	NL	Scale		
			12/05/2026 3:47:16 PM		



M: 0417 957 213

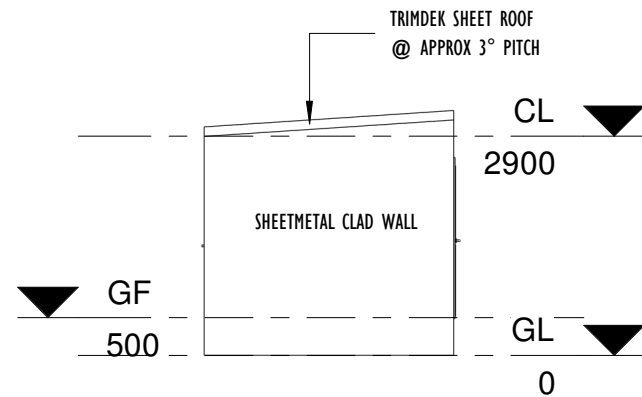
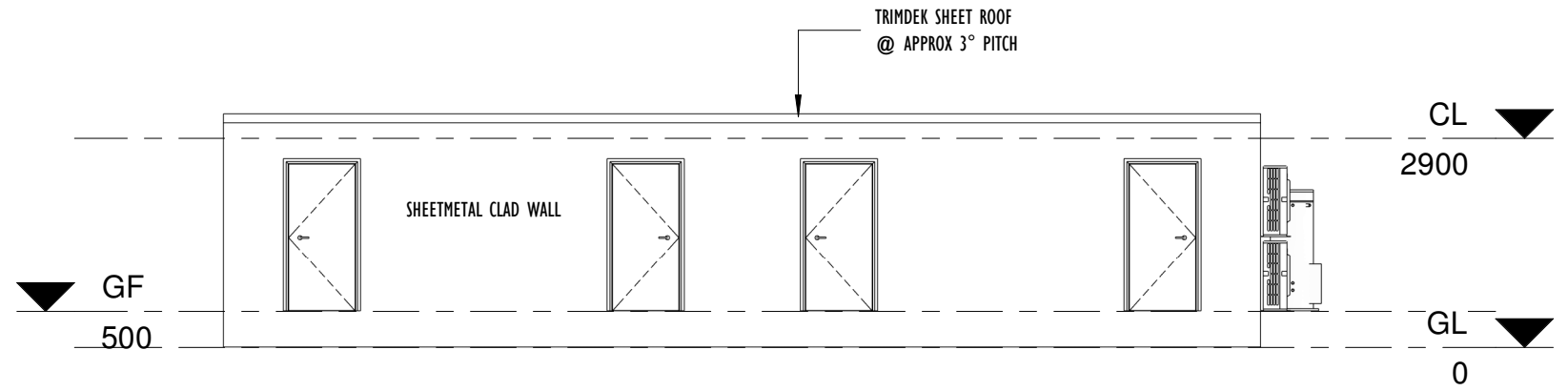
BUILDING AREA	
4 BED UNIT	47.52 m ²
TOTAL AREA	47.52 m ²



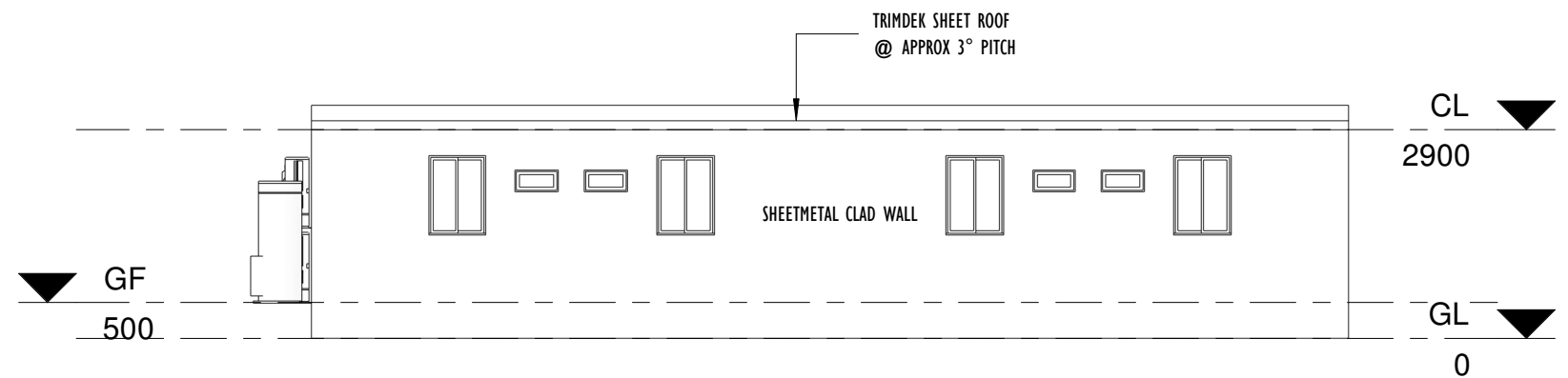
2
A09
RIGHT SIDE
1 : 100

1
A09
1.GF
1 : 100

3
A09
FRONT
1 : 100



4
A09
LEFT SIDE
1 : 100



5
A09
REAR
1 : 100

NOTES

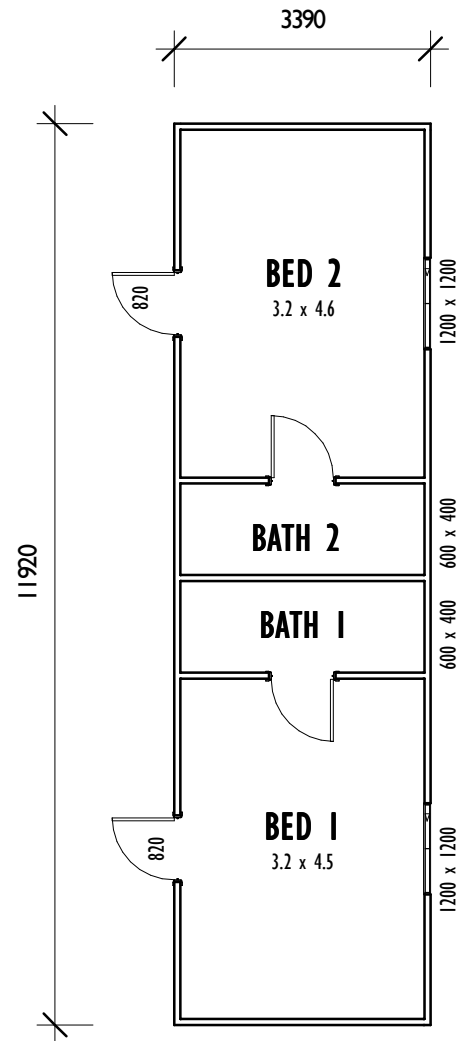
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GOLDFIELDS CONTROLLED WASTE LOT 12 & 13 KURAJONG ST, LEONORA	4 BED UNIT		SHEET SIZE		A3	
	A3	Date	OCT 25	REV NO	DESCRIPTION	DATE
	Drawn by	NL	A09			
	Checked by	NL		Scale	1 : 100	



BUILDING AREA

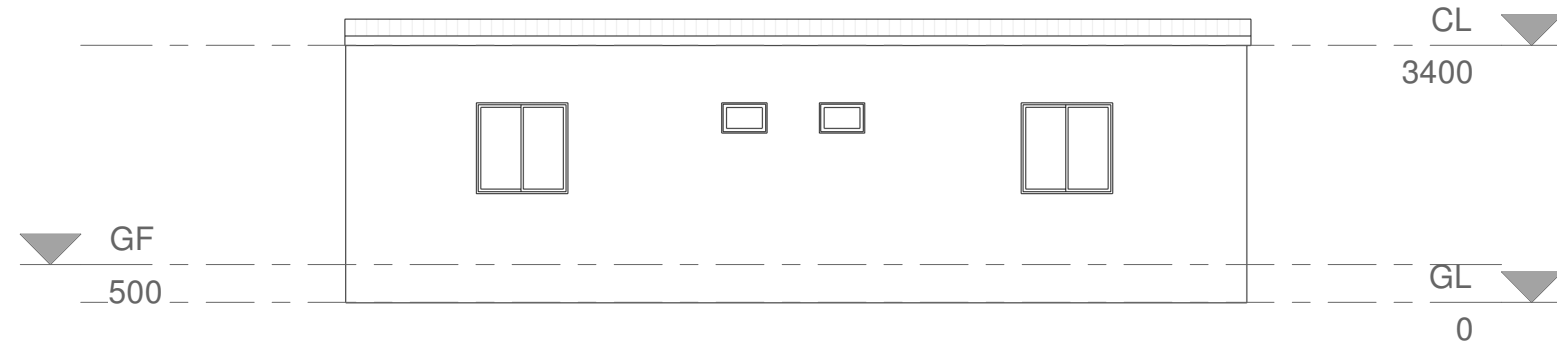
Area	40.41 m ²
TOTAL AREA	40.41 m ²



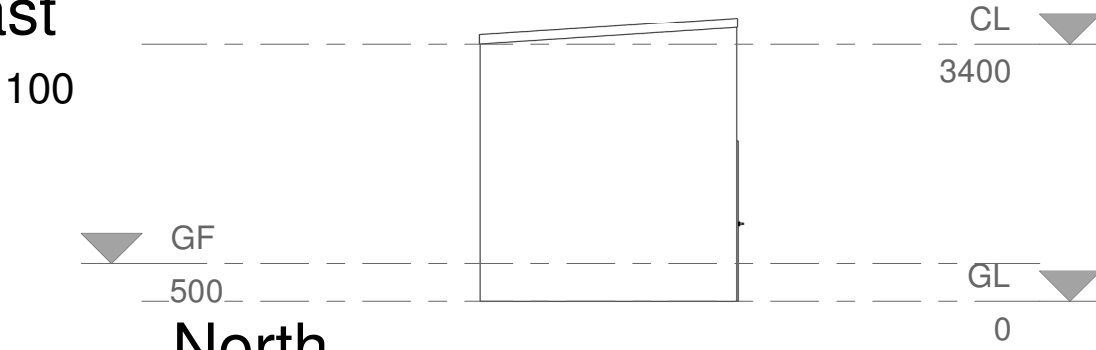
1
A01
1.GF
1 : 100

NOTES

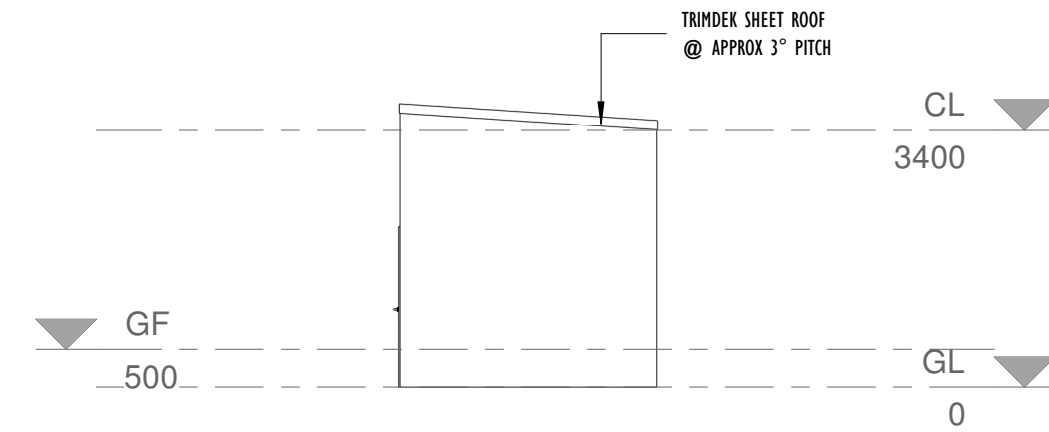
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2
A01
East
1 : 100




3
A01
North
1 : 100



4
A01
South
1 : 100



5
A01
West
1 : 100

<p>GOLDFIELDS CONTROLLED WASTE</p> <p>LOT 13 KURAJONG ST, LEONORA</p>	<p>2 BED UNIT</p>		<p>A01</p>		<p>Scale 1 : 100</p>		<p>12/05/2026 4:05:00 PM</p>		 <p>M: 0417 957 213</p>
	<p>DATE FEB 26</p>		<p>REV NO</p>		<p>DESCRIPTION</p>		<p>DATE</p>		
	<p>Drawn by NL</p>	<p>Checked by NL</p>	<p> </p>	<p> </p>	<p> </p>	<p> </p>	<p> </p>	<p> </p>	



Transport Impact Statement

Project:	Proposed Workers Accommodation 64 - 70 Kurrajong Street, Leonora
Client:	AT, NF & SM Kelly
Author:	P. Nguyen
Date:	25 th May 2026
Shawmac Document #:	2511012-TIS-001

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Document Status: Client Review

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File Reference: Y:\Jobs Active 2025\T&T - Traffic & Parking\Pinnacle_70 Kurrajong St, Leonora_TIS_2511012\3. Documents\3.20 TIS\Pinnacle_70 Kurrajong St, Leonora_TIS_Rev C.docx



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1. Introduction

1.1. Proponent

Shawmac Pty Ltd has been engaged by the landowners of 64-70 Kurrajong Street in Leonora to prepare a Transport Impact Statement (TIS) for a proposed workers accommodation development.

This TIS has been prepared in accordance with the Western Australian Planning Commission (WAPC) *Transport Impact Assessment Guidelines Volume 4 – Individual Developments*. The assessment considers the following key matters:

- Details of the proposed development.
- Vehicle access and parking.
- Provision for service vehicles.
- Daily traffic volumes and vehicle types.
- Traffic management on frontage streets.
- Public transport access.
- Pedestrian access.
- Site specific and safety issues.

1.2. Site Location

The site address is 64 -70 Kurrajong Street in Leonora. The local authority is the Shire of Leonora.

The general site location is shown in **Figure 1**. An aerial view of the existing site is shown in **Figure 2**.

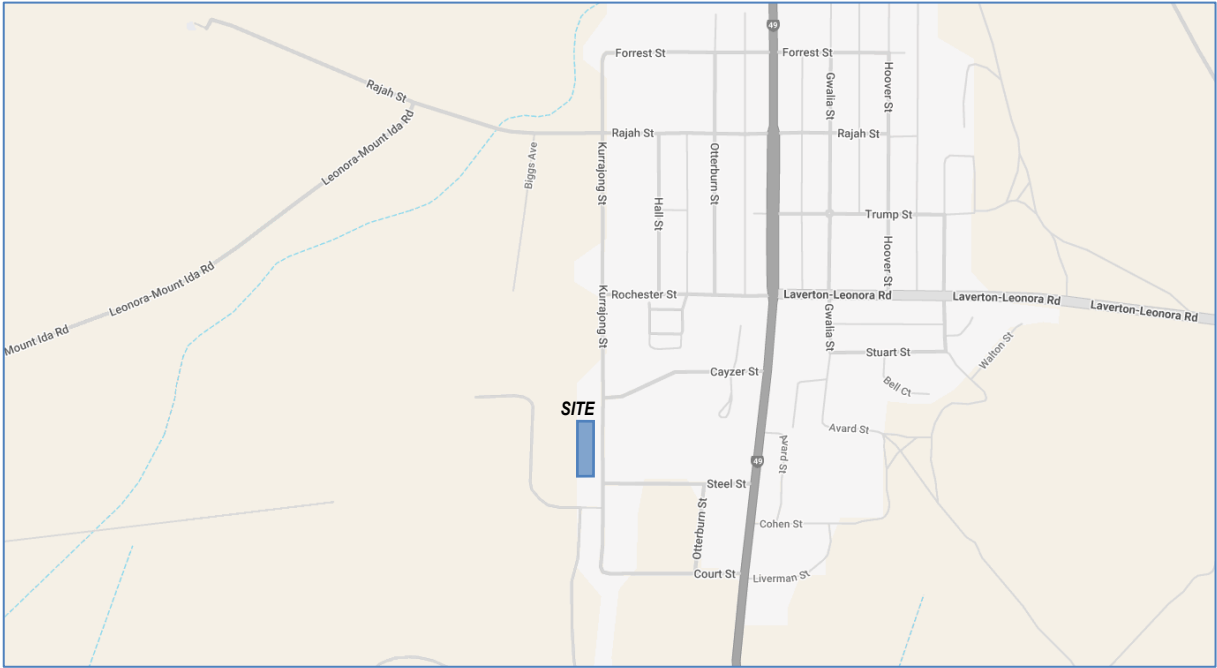


Figure 1: Site Location



Figure 2: Aerial View



1.3. Land Use

There are two existing dongas on the site accommodating up to 5 people.

The proposal is an expanded workers accommodation development to accommodate a maximum of 84 occupants and 8 support staff. The development will comprise 20 dongas, a kitchen, laundry, car parking and other associated amenities.

The site layout is shown in **Figure 3** and **Figure 4**.

2. Traffic Management on Frontage Streets

2.1. Road Network Layout and Hierarchy

The layout and hierarchy of the existing local road network according to the Main Roads WA *Road Information Mapping System* is shown in **Figure 5**.

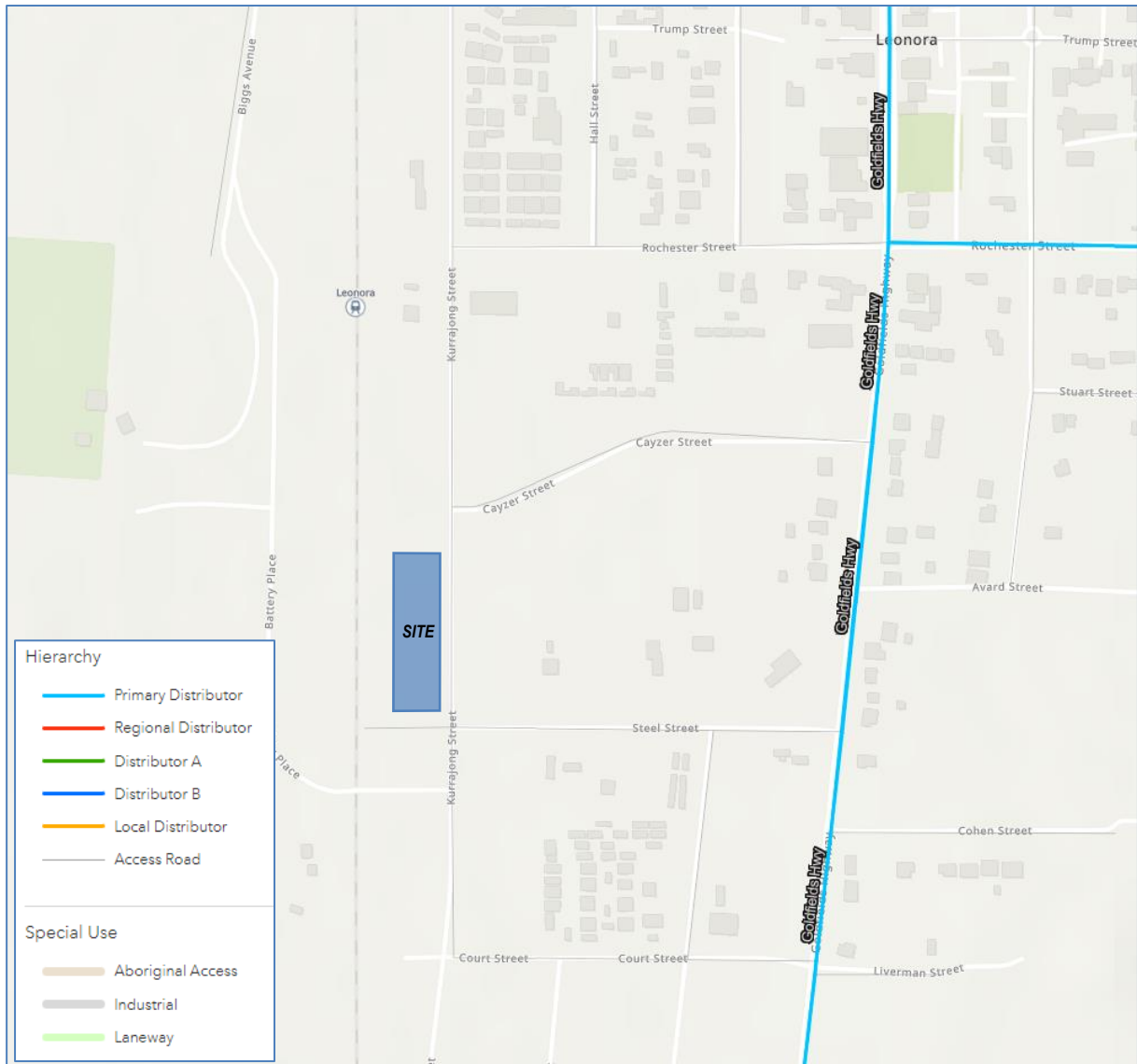


Figure 5: Existing Road Network Hierarchy

2.2. Speed Limit

The existing speed limits are shown in **Figure 6**.

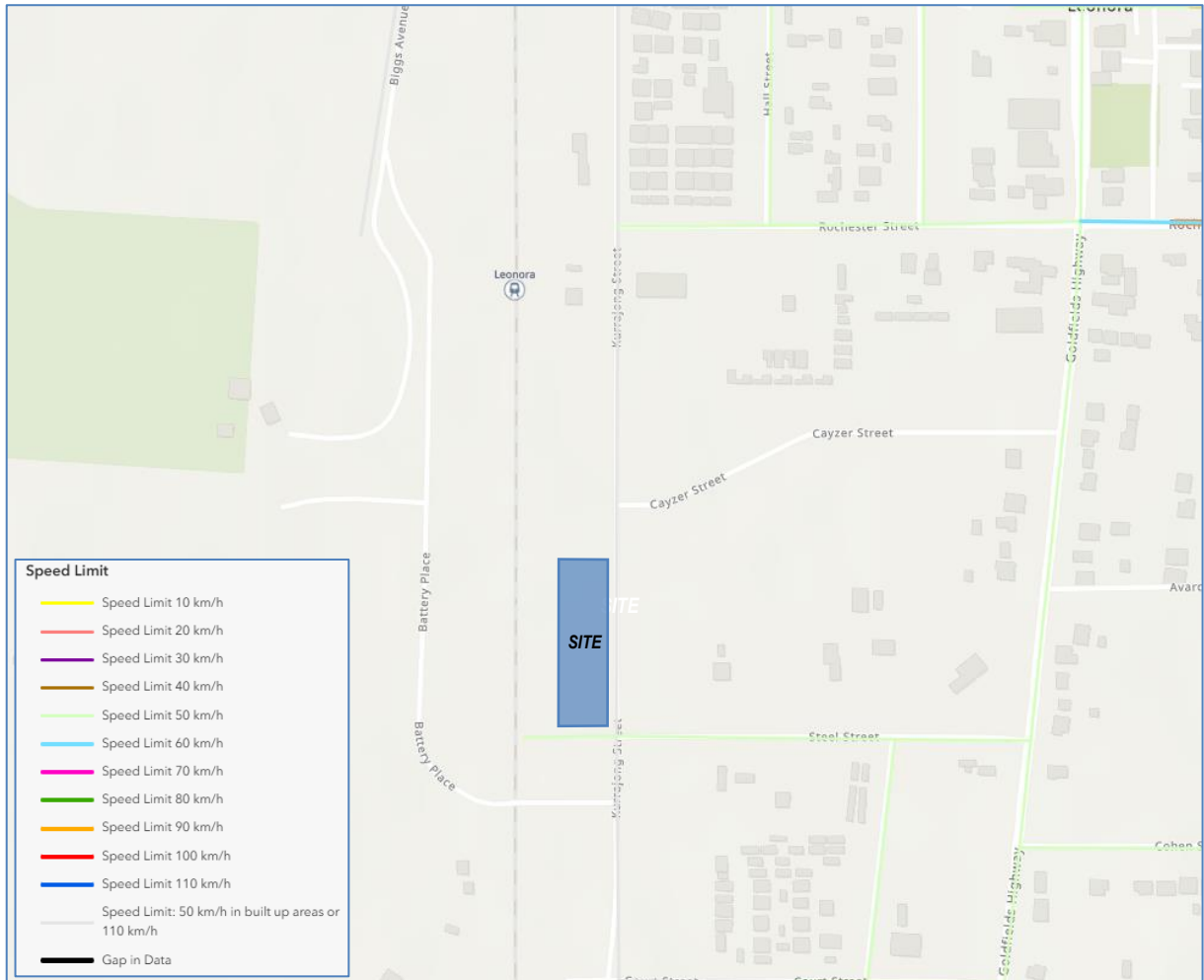


Figure 6: Existing Speed Limits



3. Daily Traffic Volumes

Most workers will not have a private vehicle at the site and will carpool between the site and the nearby work sites. It is therefore assumed that worker transport will generate approximate 1 vehicle movement per 4 workers which equates to approximately 21 outbound vehicle movements in the morning and 21 inbound movements in the evening.

Assuming all support staff drive individually and also travel during the peak periods, support staff will generate an additional 8 vehicle movements during each peak.

Based on the above, the peak hour traffic generation is estimated to be approximately 29 vehicle movements as a worst-case scenario.

In accordance with WAPC TIA guidelines, an increase of between 10 to 100 peak hour vehicles is considered to have a low to moderate impact and is generally deemed acceptable without requiring detailed capacity analysis. The estimated 29 vehicles per hour is at the lower end of this range and therefore the traffic impact is low.

4. Vehicle Access and Parking

4.1. Access

Vehicle access is proposed via three crossovers on Kurrajong Street as shown in **Figure 7**.

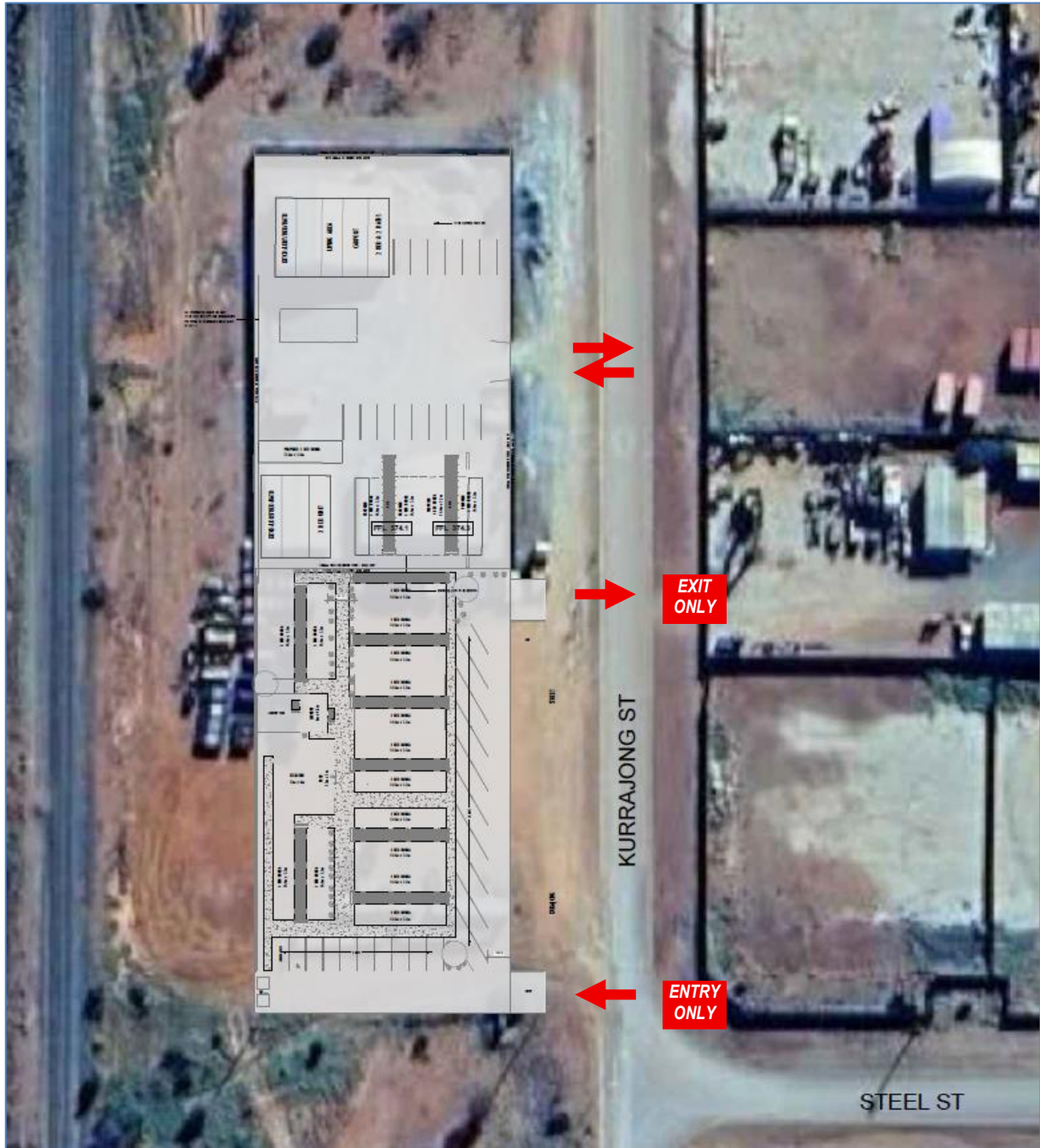


Figure 7: Vehicle Access

Australian Standard AS2890.1-2004 *Parking facilities Part 1: Off street car parking* (AS2890.1) specifies areas within and around intersections that should typically be avoided when locating vehicle accesses. The prohibited zones are shown in **Figure 8**.

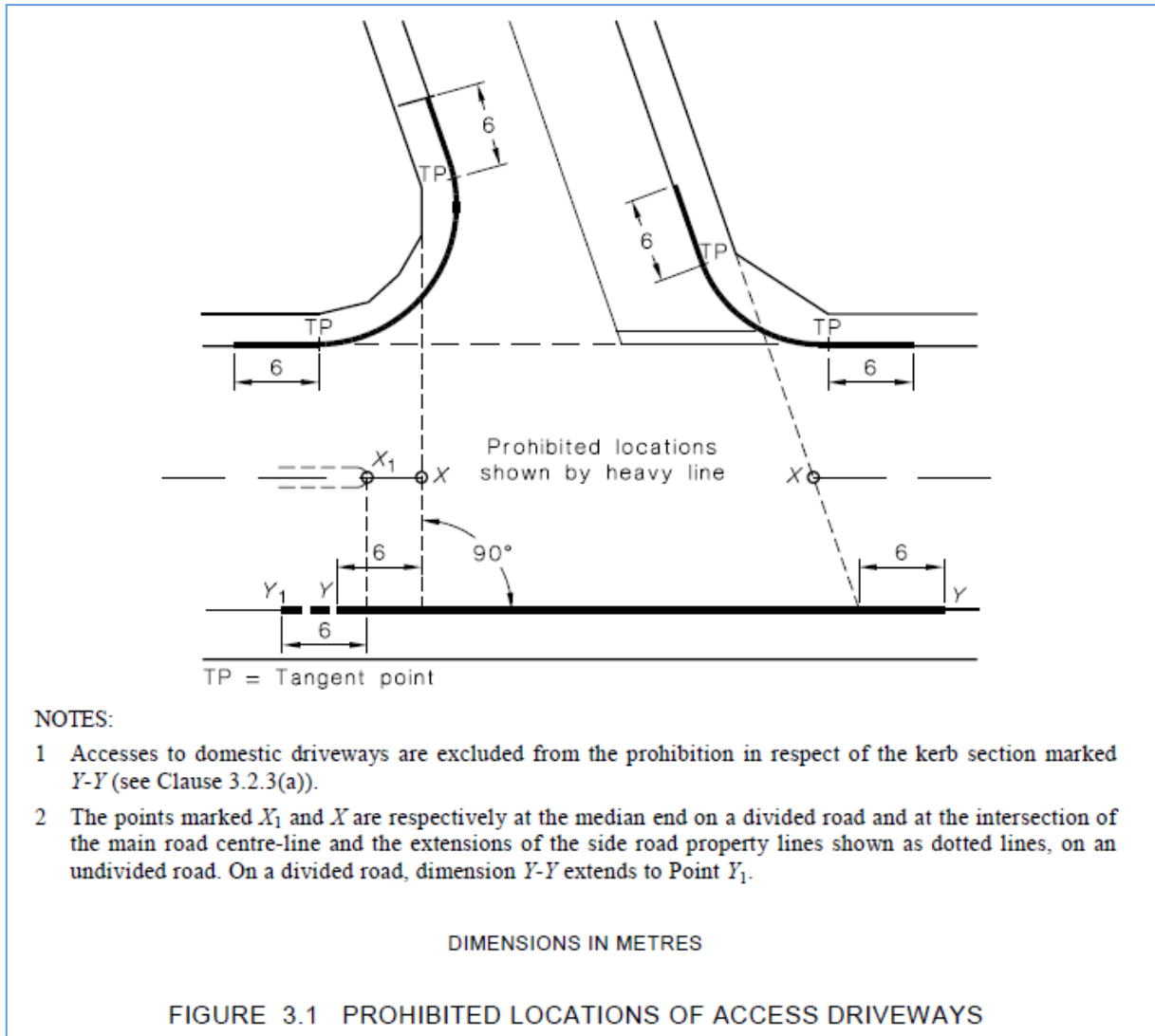


Figure 8: AS2890.1 Prohibited Locations for Access Driveways

The proposed southern crossover is located less than 6m from the tangent point of the Kurrajong Street / Steel Street intersection which is not technically compliant with AS890.1. In this instance, the crossover may be considered acceptable on the basis of the access being entry only which reduces the number of conflicting movements. Further, the site will generate a relatively low volume of additional traffic within short periods of the day and minimal traffic at all other times.

4.2. Access Sight Distance

Sight distance requirements from vehicle exit points are defined in Figure 3.2 of Australian Standard AS2890.1-2004 *Parking facilities Part 1: Off street car parking* (AS2890.1) which are based on the Austroads Stopping Sight Distance shown in **Figure 9**.

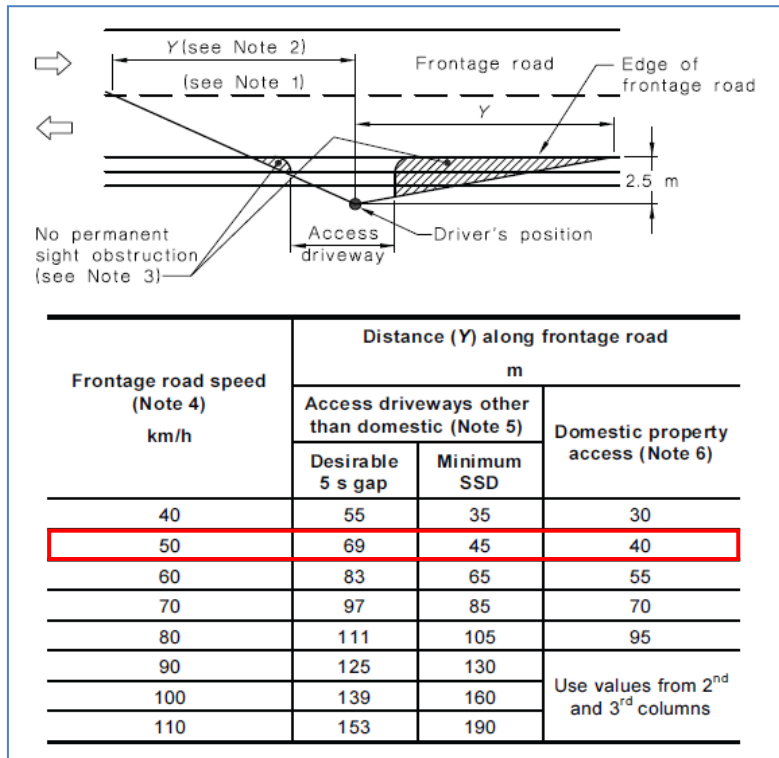


Figure 9: AS2890.1 Sight Distance Requirements

Based on the 50km/h speed limit along Kurrajong Street, the minimum required sight distance is 45m.

As shown in **Figure 10**, the minimum required sight distance is achieved in both directions at both vehicle exit points.

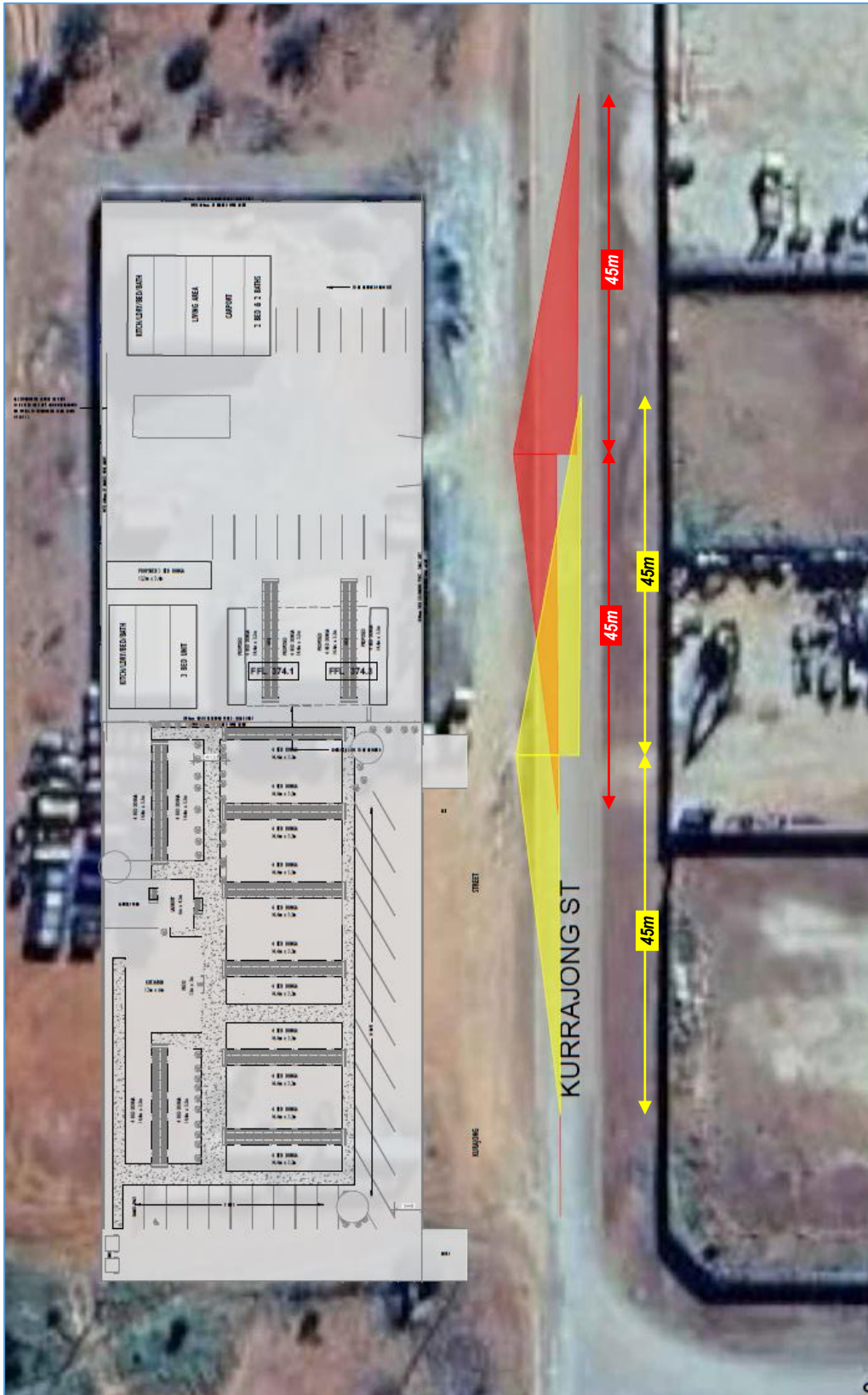


Figure 10: Sight Distance Check



4.3. Parking

The Shire's Local Planning Scheme No. 2 (LPS2) specifies a minimum car parking rate of 1 bay for each 65m² of Net Lettable Area (NLA) in the Light Industry Zone.

Based on the approximately 948m² NLA, the minimum car parking requirement is 15 bays. The total parking provision is 33 bays which exceeds the minimum requirements and is considered to be adequate.

4.4. Provision for Service Vehicles

Waste from the site will be collected on-site from the bin store in the south-west corner of the site. A preliminary vehicle swept path analysis has been undertaken to check waste vehicle manoeuvring. The analysis has been undertaken in AutoTURN vehicle tracking software using a template for a typical 10m waste collection truck. It is assumed that waste trucks will reverse into the southern entry crossover and then leave via the same crossover.

The results are attached as **Appendix A** and these demonstrate adequate manoeuvring, subject to design of the crossovers.



5. Pedestrian Access

There is limited path infrastructure in the vicinity of the site which is common for regional and industrial areas. The demand for walking is expected to be minimal for the proposed use and the provision of additional paths is not warranted.

6. Public Transport Access

There are no known public transport services in the vicinity of the site. There is not expected to be any demand for public transport based on the proposed use.

7. Site Specific Issues and Safety Issues

7.1. Crash History

The crash history of the adjacent road network was obtained from the MRWA Reporting Centre. No crashes have been recorded along the frontage section of Kurrajong Street in the five-year period from 2020 to 2024.

The traffic generation is low and there is no indication that the development will increase the risk of crashes significantly.

8. Conclusion

This Transport Impact Statement for the proposed workers accommodation at 64 to 70 Kurrajong Street in Leonora concluded the following:

- The peak hour traffic generation is estimated to be approximately 29 vehicle movements as a worst-case scenario.
- In accordance with WAPC TIA guidelines, an increase of between 10 to 100 peak hour vehicles is considered to have a low to moderate impact and is generally deemed acceptable without requiring detailed capacity analysis. The estimated 29 vehicles per hour is at the lower end of this range and therefore the traffic impact is low.
- Vehicle access is proposed via three crossovers on Kurrajong Street.
- The proposed southern crossover is located less than 6m from the tangent point of the Kurrajong Street / Steel Street intersection which is not technically compliant with AS890.1. In this instance, the crossover may be considered acceptable on the basis of the access being entry only which reduces the number of conflicting movements. Further, the site will generate a relatively low volume of additional traffic within short periods of the day and minimal traffic at all other times.
- The minimum required sight distance is achieved in both directions at both vehicle exit points.
- The minimum car parking requirement is 15 bays. The total parking provision is 33 bays which exceeds the minimum requirements and is considered to be adequate.
- Waste from the site will be collected on-site from the northern portion of the site. A preliminary vehicle swept path analysis demonstrates adequate manoeuvring, subject to design of the crossovers..
- There is limited path infrastructure in the vicinity of the site which is common for regional and industrial areas. The demand for walking is expected to be minimal for the proposed use and the provision of additional paths is not warranted.
- There are no known public transport services in the vicinity of the site. There is not expected to be any demand for public transport based on the proposed use.
- No crashes have been recorded along the frontage section of Kurrajong Street in the five-year period from 2020 to 2024. The traffic generation is low and there is no indication that the development will increase the risk of crashes significantly.



Appendix A – Vehicle Swept Paths

WASTE MANAGEMENT PLAN

Proposed Workforce Accommodation
Kurrajong Street, Leonora

1.0 Introduction

This waste management plan (WMP) assists and supports Workforce Accommodation to be located at Lot 12 (No. 70), Kurrajong Street, Leonora and Lot 13 (No. 64) Kurrajong Street, Leonora (subject sites) as part of a Development Application currently awaiting determination before the Shire of Leonora.

The WMP has been developed through engagement with the project team and review of the plans around suitable waste storage, collection, and supply of waste facilities and infrastructure.

The proposal contains 20 residential units (Dongas), a Kitchen, Patio, Laundry and Yard Areas. The approach of this WMP is to ensure that a commercial waste management agreement is established for the site, which delivers an acceptable level of service and provides for flexibility in response to future demand.

The WMP is centred around the WALGA Better Practice Guidelines for Commercial and Industrial Waste Management, ensuring the use of appropriate waste generation calculation rates. This will make certain that the waste storage capacity available onsite is sufficient to meet occupant demand and protect the amenity of residents and users of the development.



2.0 SUMMARY OF DEVELOPMENT

The subject sites are located within the Shire of Leonara, having a combined site area of 5433m². The site is to act as a base for operations for Goldfield Waste as they service mining and related businesses within the Goldfields Region.

The proposal for the sites consists of:

- 20 dongas, Sleeping three (3) – four (4) each
- A maximum of 79 occupants
- Kitchen
- Laundry
- Yard area
- Patio
- 33 car parking bays, including 1 disabled bay.

2.1 Location of Development

The subject sites are located on Kurrajong Street, Leonara, within the municipality of the Shire of Leonara.

The proposal is located on sites zoned as “Industry – light” in accordance with Clause 16 of The Shire of Leonara Local Planning Scheme No. 2(LPS 2) with the proposal of Workforce Accommodation classed as “d” discretionary under the zoning table. The proposal is consistent with this zoning.

Lot 13 has an existing approval for workforce accommodation which currently contains; two (2) dongas, one (1) sleeping a maximum of two (2) people and another three (3), along with existing car bays, a kitchen and laundry.

2.2 Number of Floors

The proposal consists of 20 residential dongas, a kitchen, laundry and patio. All dongas and structures are to be developed with a single storey typology.

2.3 Number of Dwelling Units by Size

The proposal contains 20 residential dongas, that will sleep three (3) to four (4) at once, with each to be 47.52 m² in size, each containing an ensuite

For the purposes of this waste management plan the dongas have been classified as “Backpacker accommodation” in order to assign waste generation rates in accordance with WALGA’s Commercial and Industrial Waste Management Plan Guidelines.

The development will be capable of occupying up to 84 workers; however, it is not expected to house more than 40 at one time.

2.4 Size of Non-Residential Buildings

The proposed kitchen/dining/patio areas have a combined size of 108m² and the laundry is 27m² in size.

2.5 Details of the Intended Use of the Development

The development is designed and proposed for the purpose of workplace accommodation, for Goldfields Waste who service mining and related businesses in the Goldfields.

This accommodation will be serviced by cleaners, maintenance staff and site managers.



3.0 WASTE GENERATION RATES

The WALGA Guidelines for Waste Management have been utilised for the purpose of developing waste generation rates for the proposal.

3.1 Waste Calculation Rates

The following table details the gross waste generation rates for the development. Due to the WALGA guidelines not providing specific rates for workplace accommodation, waste generation calculations have been based on the rates provided for backpacker accommodation.

Calculations have been made with a conservative approach, as it is anticipated that occupants will consume a large amount of food and produce waste onsite rather than at the proposed development.

The waste for the residential dongas has been calculated to accommodate 40 workers.

Commercial Waste Rate	Refuse (L/week)	Recycling (L/week)
Residential Units	40L/Occupant per week	20L/Occupant per week
Gross waste generation/week	1,600L per week	800L per week

3.1.1 Refuse

The proposal has a generation of 1,600 litres of general refuse per week.

- As per the sites plans, the development will provide a 1,100L general refuse bin, which is to be stored in its designated area, at the rear of the site's carpark.
- As this does not meet the requirement for the anticipated waste generation waste collection may be necessary at a less than weekly basis, this is feasible due to the site being serviced via private waste collection which is flexible.

3.1.2 Recycling

The proposal has a generation of 800 litres of recyclable waste per week.

- As per the sites plans, the development will provide a 1,100L recycling bin, which is to be stored in its designated area, at the rear of the site's carpark.

4.0 BIN STORAGE AREA

The plans for the proposal indicate that bins will be stored at the bottom of the site’s driveway/carpark area, external to buildings at the site. This will limit the visibility of bins from buildings and street view and allow for easy transportation of bins to Kurrajong Street for collection.

4.1 Waste Disposal

Residents and staff at the site will be required to present waste to the bin storage location.

4.2 Bin Storage Area Size

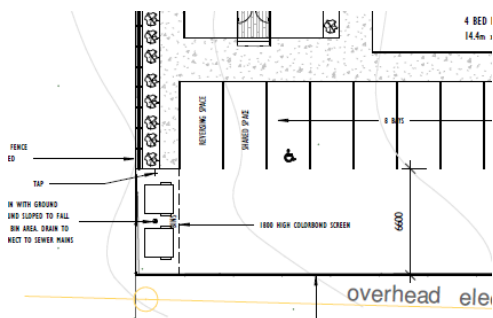
The bin storage requirement for the development is based on the following generation rates and storage capacity, based on weekly collection:

Commercial Waste Rate	Refuse (L/week)	Recycling (L/week)
Units	40L/Occupant per week	20L/Occupant per week
Gross waste generation/week	1,600L per week	800L per week
Bin storage area requirement	1 x 1,100L bin	1 x 1,100L bin

The development is therefore required to be capable of catering for 2 x 1,100L bins, including one (1) 1,100L general waste bin, and one (1) 1,100L recycling bin.

4.3 Bin Storage Area Layout

Bins are to be stored in a location that is external from buildings, at the rear of the carpark/driveway, as per the below drawing:



4.4 Alternative Waste

Any alternative waste such as hard, bulk or hazardous waste is to be coordinated for separate removal through occupants coordinating with local municipal verge valet or other private bulk waste removal services as appropriate.

4.5 Ventilation

As the bin storage area is to be located outside, the bin storage area will receive satisfactory natural ventilation.

4.6 Vermin Protection

The bin store is to be treated with vermin proof solutions, with final detailing to occur at building permit stage of the project.

4.7 Noise Reduction

As the bin stores do not include any mechanical infrastructure by way of compactors, the noise emanating from each bin store is limited to opening and closing of bins, which is a minimal acoustic impact.

4.8 Stormwater Ingress Prevention and Drainage

Stormwater is to be directed away from the buildings and into the adjacent stormwater creek.



5.0 COLLECTION METHOD AND FREQUENCY

5.1 Collection Vehicle to be Utilised

The proposal is to be serviced via private waste collection vehicles.

5.2 Movement of Collection Vehicles/Swept Path Analysis

Due to the location of the bin collection point inside the subject site, swept path analysis has confirmed the site is able to accommodate a waste vehicle reversing into the site.



5.3 Collection Location

All bins will be collected via internal collection at the rear of the parking area.

5.4 Transfer of Waste to the Collection Vehicle

Bins will be presented for collection, kerbside on Kurrajong Street by a designated manager at the site who will ensure that all bins are accessible for the waste collection vehicle at the designated/organised collection time.

The use of 1,100L bins will allow for the transfer of bins to occur manually, without the need for assisting equipment or machinery.

5.5 Frequency of Collection

The provision of a 1,100-litre bin for general refuse for the development and the anticipated generation of 1,600 litres per week of general waste, therefore, collection may be required more than once a week. This is attainable due to the site being serviced via private waste services and will need to be organised by the sites management.

Collection of recycling is to be on a weekly basis.

5.6 Chutes

The proposal does not include any provisions for a bin chute.

5.7 Compactor

The proposal has demonstrated it sufficiently caters to the waste generation rates through the quantity, capacity and collection of bins nominated in this report. Consequently, there is no necessity for a compactor at the site, which also prevents the potential for acoustic impacts on building occupants and neighbouring land uses that comes with this infrastructure.

5.8 Bin Lifter

As the development is serviced by two (2) x 1,100L bins, there is no requirement or demand to provide bin lifters.

Residents and staff will not be expected to transfer waste from smaller bins into larger stores, as the development does not propose any such double-handling methods.

6.0 WASTE PROVIDER

The proposal has been designed to cater for the provision of private contractor-based waste collection services in accordance with the WALGA Guidelines.



7.0 EDUCATION

The education of the sites staff and occupants by the responsible onsite manager will be required, to confirm the obligations of staff with respect to waste management presentation to the central bin store.

Site management will need to ensure that bins are transported and presented on Kurrajong Street for kerbside collection via private waste service vehicles for designated or organised collection times for both streams of waste.

The bins are to remain behind setback lines and be always kept clear from the view of the street. Bins are additionally to be kept free of vehicle and pedestrian access routes, so as not to create impediments to the movement of vehicles and/or pedestrians accessing the site.



8.0 ONGOING MANAGEMENT

The site's management will need to ensure that occupants are educated on the appropriate waste practices and that this WMP is sustained and respected once the site is developed and in function. Site management will also need to ensure the following takes place:

- Education of new occupants/workers on the presentation and different streams of waste
- Transport and presentation of bins for easy and accessible collection for each stream of waste via the site's private waste services.
- Ensuring the site's driveway/car bays are kept clear and that bins are not impacting the site's accessibility for vehicles and pedestrians
- Ensuring that bins are stored so that they are not visible from Kurrajong Street, neighbouring properties and worker accommodation.
- Site management will need to be required to organise the collection of general refuse with the site's private waste services to ensure that collection is at a frequency that prevents the site's general refuse bin meeting capacity, prior to its collection.

27 October 2025

Chief Executive Officer
Shire of Leonora
Via Online Lodgement

Attention: Planning Services

Dear Sir/Madam

**Development Application
Workforce Accommodation
64 and 70 Kurrajong Street**

Pinnacle Planning acts on behalf of the owners of Lot 12 (No. 70), Kurrajong Street, Leonora and Lot 13 (No. 64), Kurrajong Street, Leonora (subject sites), who are seeking approval for the subject proposal.

To assist with progressing the Application, please find the following Application particulars attached:

- Completed and signed Application Forms;
- A copy of the plans and elevations of the proposal; and
- A copy of the certificate of title.

SUMMARY OF PROPOSAL

The subject sites are located in the Shire of Leonora, have a combined site area of 5433m². The property is to assist as a base for operations of Goldfields Waste, as they service mining and related businesses in the Goldfields Region.

This application is not subject to the provision of the Metropolitan Region Schemes and as such, this application will be based solely off the provisions of the Local Planning Scheme No.2.

The proposal for the subject site includes:

- 20 dongas, sleeping three (3)- four (4) each
- A maximum of 79 occupants
- Kitchen
- Laundry
- Yard area

- Patio
- 33 car parking bays, incl one (1) disabled space.

Lot 13 has an existing approval for workforce accommodation at the site and there is currently on the site; two (2) dongas, one (1) which sleeps a maximum of two (2) people and another three (3) bed unit with existing car ports and kitchen and laundry. Totalling a current maximum of five (5) workers on site at any time. With an additional three to five (3-5) members of management and support staff.

TOWN PLANNING CONSIDERATIONS

Metropolitan Region Scheme

The application site is not subject to the provisions of the Metropolitan Region Scheme.

Shire of Leonora Local Planning Scheme No. 2

Under the provisions of the Shire of Leonora Local Planning Scheme No. 2 (LPS2), the subject site is zoned as "Industry- light." In accordance with Clause 16 of LPS2, the objectives of the zone are as follows:

- “• *To provide for a range of industrial uses and service industries generally compatible with urban areas, that cannot be located in commercial zones.*
- *To ensure that where any development adjoins zoned or developed residential properties, the development is suitably set back, screened or otherwise treated so as not to detract from the residential amenity.*
- *To allow for workforce accommodation where it can be demonstrated that it would support an operational light industrial land use.”*

Under clause 17 of LPS2, Workforce Accommodation is classed as a "D" discretionary under the zoning table. This means that the use is not permitted unless the local government has exercised its discretion by granting development approval.

The zoning allows for workforce accommodation that supports operational light industrial land use. The workforce accommodation will accommodate workers that support the local industrial use as per the objectives of the zoning.

The requirements of development within a light industry zone are as follows:

- *Minimum site area- 2000m²*
- *Setback from street alignment- 9m*
- *No. of car parking spaces- 1 for each 65m² of NLA*

The combined site area where this proposal is located is 5433m².

The site is separated into Lot 12 and Lot 13. Lot 12, along the primary street, Kurrajong Street, the site, including the car parking spaces, is setback nine (9) metres from the street. The LPS2 does not state if it requires the entire development to be set back nine (9) metres from the street or just the buildings. However, this proposal, at Lot 12, would comply with both.

Lot 13, however, seeks a setback of 4.9 metres in lieu of nine (9) metres. The dongas within the front setback area replace an existing structure on the site from a 2023 approval. The application on Lot 13 is not seeking to develop within a setback area that is not already built on.

Additionally, the Kurrajong Street road reserve verge is 14 metres wide, which means in conjunction with the setback of the first donga, the effective setback is 18.9 metres from the road pavement.

For these reasons we consider the front setback variation appropriate.

The proposal is also consistent with the car parking space requirements as outlined below.

The proposal is consistent with this zoning.

Car Parking Bays

As per the Light industry zone and General industry zone requirements, one (1) car parking space is required for each 65m² of NLA. The total floor area of the proposed Workforce Accommodation units equates to 947.76m² and one (1) car parking space per 65m² of NLA equates to 14.58 car parking bays for the site. The proposal meets this with a surplus of 19 car parking spaces and is subsequently compliant with the requirements of development within a light industry zone.

Notwithstanding this, the business use and capacity of the site portray a more accurate number of car parking bays that will be required on site at any one time. The existing and proposed workforce accommodation can sleep a maximum of 84 staff members, and it is assumed that there would also be an average of eight (8) additional support staff on site. Furthermore, the nature of the business means that staff will be sharing utility vehicles to travel to and from their site location. It is much more reasonable to assume a maximum of one (1) car per four (4) staff members on site, creating a maximum of 20 cars plus eight (8) for support staff. This brings the total maximum cars on site to 28 which the proposal allows for.

Additionally, due to the structure of business, it is estimated that the site will only ever be at a maximum of 60% capacity. This creates a maximum of 58 staff members and 20 required spaces. One (1) space per four (4) staff plus eight (8) for site staff to each have their own car bay. Therefore, there is a surplus of 13 car parking spaces when taking into consideration the nature of the business and the reasonable number of staff and cars on site at any one time.

The proposal clearly demonstrates a strong compliance with both the car parking space policy outlined with the LPS2 and what is considered to be a reasonable estimate for number of car parking spaces required.

Please see below:

	Additional staff	Existing	Total	Cars bays per additional staff	Car bays per existing staff	Total estimated required car bays
Workers	79	5	84	19	1	20
Site manager	1	1	2	1	1	2
Cook	1	1	2	1	1	2

Cleaner	1	1	2	1	1	2
Maintenance	1	1	2	1	1	2
Total	83	9	92	23	5	28
Adjusted 60% Total	51	7	58	15	5	20

Table A: estimates of staff numbers and car bay requirements

Setbacks and Amenity

The Shire of Leonora’s local Planning Scheme No.2 outlines an objective of development within the light industry zone;

“to ensure that where any development adjoins zoned or developed residential properties, the development is suitably set back, screened or otherwise treated so as not to detract from the residential amenity”.

The application site does not adjoin any developed residential properties. However, as outlined above, the application is suitably set back as so to mitigate impact of the amenity of residential properties in the area.

Additionally, the LPS2 does not indicate any required side or rear setbacks, however, these have been included within the proposal to ensure compliance with this objective and to not impact amenity. Furthermore, all dongas are proposed to be single storey further mitigating impact on the amenity of the locality.

The proposal is consistent with this objective of the light industry zoning.

State Planning Policy 5.4 Road and Rail Noise

The State’s Planning Policy 5.4 (SPP 5.4) seeks to minimise the adverse impact of road and rail noise on noise-sensitive land-use and/ or development within the specified trigger distance of strategic freight and major traffic routes and other significant freight and traffic routes.

The proposal site is located adjacent to a freight railway. Notwithstanding this, the SPP 5.4 applies only to the following:

- “(a) noise-sensitive land-use within the policy’s trigger distance of a transport corridor as specified in Table 1;*
- b) new or major upgrades of roads as specified in Table 1 and maps (Schedule 1, 2 and 3); or*
- c) new railways or major upgrades of railways as specified in maps (Schedule 1, 2 and 3); or any other works that increase capacity for rail vehicle storage or movement and will result in an increased level of noise.”*

B and C are not applicable. Therefore, only A has the potential to be applied to the subject site. The definition of ‘noise- sensitive land-use’ is key in determining whether policy SPP 5.4 applies to this proposal. The SPP 5.4 defines ‘noise- sensitive land- use’ as:

“Noise sensitive land- use and/ or development is determined by LPS or structure plan zoning that is occupied or designed for occupation or use for residential purposes (including dwellings, residential buildings or short-stay accommodation), caravan park, camping ground, educational establishment, child care premises, hospital, nursing home, corrective institution; or place of worship.”

Workforce accommodation is not listed. It is therefore argued that workforce accommodation is not considered a noise sensitive land use under the SPP 5.4. The definition of ‘short- stay accommodation’ is better suited towards tourism and short-term rentals rather than workforce accommodation. The proposal is not subject to the requirements of The State Planning Policy 5.4.

Notwithstanding this, the impact of rail noise on the site is considered to be minimal. We have been advised that the train runs very infrequently at a maximum of once per week and during daylight hours. The minimal impact of such infrequent disruption is considered acceptable.

Additionally, it is recognised that the level of amenity expected for workforce accommodation, particularly with in a ‘Light Industry’ zone is not considered to be as high as a ‘Residential’ zone which the policy definition is more closely aligned with.

The resulting impact after considering the above points is that the railway and resultant noise will likely have a negligible impact on the workforce accommodation.

CONCLUSION

In considering the above, the following conclusions are evident:

- The proposal is for workforce accommodation that demonstrates an appropriate level of consistency with the Shire of Leonora Local Planning Scheme No 2;
- The development is proposed to be located in accordance with locational criteria set out in the prevailing local planning policy framework;
- The proposal will be consistent with the amenity of the locality.

Based on the above, and submitted information, we seek the Shire of Leonora exercise its discretion in approving the proposal as set out in this application.

Should you require any further information or clarification in relation to this matter, please do not hesitate to contact the undersigned.

Yours faithfully

PINNACLE PLANNING



BEN CARTER

Encl.

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25 May 2026

Chief Executive Officer
Shire of Leonora
Via Online Lodgement

Attention: Planning Services

Dear Sir/Madam

**Development Application – Request for Further Information Resubmission
Workforce Accommodation
64 and 70 Kurrajong Street**

Pinnacle Planning acts on behalf of the owners of Lot 12 (No. 70), Kurrajong Street, Leonora and Lot 13 (No. 64), Kurrajong Street, Leonora (subject sites), who are seeking approval for the subject proposal.

To assist with finalising the Application, please find the following Application particulars attached:

- Amended Plan Set;
- Updated Traffic Impact Statement;
- Updated Waste Management Plan; and
- A copy of advice from the Water Corporation.

SUMMARY OF AMENDMENTS

We confirm the following modifications to the proposal:

- Connection to sewer upgrade (approval provided);
- Confirmation of stormwater annotation details on plan set;
- Confirmation of cessation of commercial vehicle parking on the site;
- Individual floor plans supplied to clarify floorspace for all proposed units;
- Internal and external design annotations for all accommodation units;
- Rainwater tank moved out of parking area;
- Landscaping added to all parking areas, and the perimeter areas of the proposed accommodation units;
- Plans confirm on site interconnected soak wells for onsite stormwater retention;

- Wheel stops added to parking areas;
- Reversing space added to parking area;
- Parking area for Lot 12 has full circulation and double crossover added in to enable vehicles to exit if no parking available;
- Updated traffic impact statement confirms full compliance and functionality of parking spaces and suitable reversing space for a waste truck;
- Plans have been amended to add landscaping, provide permeable fencing for passive surveillance;
- Bin store has been screened with a tap and drain added;
- Updated waste management plan; and
- We confirm the two sites are to work in conjunction with one another.

State Planning Policy 7.0

We note that this proposal is not a qualifying proposal for design review, and further note the proposal is a temporary workforce accommodation proposal of modest scale.

Whilst changes have been made to the plans which improve the functional and amenity based matters, the proposal is not required to be assessed against this policy document.

Should you require any further information or clarification in relation to this matter, please do not hesitate to contact the undersigned.

Yours faithfully

PINNACLE PLANNING



BEN CARTER

Encl.

PART D – OTHER BUSINESS

- 1. State Administrative Tribunal Applications and Supreme Court Appeals**
- 2. Meeting Closure**