



Metro Inner Development Assessment Panel Agenda

Meeting Date and Time: Wednesday, 30 July 2025; 9:30am
Meeting Number: MIDAP/89
Meeting Venue: 140 William Street, Perth

A live stream will be available at the time of the meeting, via the following link:
[MIDAP/89 - 30 July 2025 - City of Fremantle](#)

PART A – INTRODUCTION

1. Opening of Meeting, Welcome and Acknowledgement
2. Apologies
3. Members on Leave of Absence
4. Noting of Minutes

PART B – CITY OF FREMANTLE

1. Declarations of Due Consideration
2. Disclosure of Interests
3. Form 1 DAP Applications
 - 3.1 Lots 27, 29, 300, 301 (No. 234) South Terrace, South Fremantle - Motor Vehicle Wash Development – DAP/24/02818
4. Form 2 DAP Applications
5. Section 31 SAT Reconsiderations

PART C – OTHER BUSINESS

1. State Administrative Tribunal Applications and Supreme Court Appeals
2. Meeting Closure

Please note, presentations for each item will be invited prior to the items noted on the agenda and the presentation details will be contained within the related information documentation



ATTENDANCE	
<i>Specialist DAP Members</i>	<i>DAP Secretariat</i>
Dale Page (Presiding Member)	Kristen Parker
Francesca Lefante (Deputy Presiding Member)	Ashlee Kelly
John Syme	
<i>Part B – City of Fremantle</i>	
Cr Andrew Sullivan (Local Government DAP Member, City of Fremantle)	
Cr Ingrid Van Dorssen (Local Government DAP Member, City of Fremantle)	



PART A – INTRODUCTION

- 1. Opening of Meeting, Welcome and Acknowledgement**
- 2. Apologies**
- 3. Members on Leave of Absence**
- 4. Noting of Minutes**



PART B – CITY OF FREMANTLE

1. Declarations of Due Consideration

2. Disclosure of Interests

3. Form 1 DAP Applications

- 3.1 Lots 27, 29, 300, 301 (No. 234) South Terrace, South Fremantle - Motor Vehicle Wash Development – DAP/24/02818

4. Form 2 DAP Applications

Nil.

5. Section 31 SAT Reconsiderations

Nil.

Part B – Item 3.1 - LOT NO. 27, 29, 300 & 301 (NO. 234) SOUTH TERRACE SOUTH FREMANTLE – MOTOR VEHICLE WASH DEVELOPMENT

**Form 1 – Responsible Authority Report
(Regulation 12)**

DAP Name:	Metro Inner	
Local Government Area:	City of Fremantle	
Applicant:	Apex Planning	
Owner:	Glowmark Nominees Pty Ltd	
Value of Development:	\$3.5 million	
Responsible Authority:	City of Fremantle	
Authorising Officer:	Manager Development Approvals	
LG Reference:	DAP004/24	
DAP File No:	DAP/24/02818	
Application Received Date:	5 December 2024	
Report Due Date:	17 July 2025	
Application Statutory Process Timeframe:	90 Days	
Attachment(s):	<ol style="list-style-type: none"> 1. Amended Development Plans 2. Acoustic Report 3. Amended Operational and Noise Management Plan 4. Amended Transport Impact Statement 5. Stormwater and Sewer Management Plan 6. Planning Report 7. Heritage Officer Assessment 8. Response to City of Fremantle Feedback 9. Schedule of Submissions and Applicant Response 10. Original Development Plans 11. Site Photos 12. Council Meeting Minutes 	
Is the Responsible Authority Recommendation the same as the Officer Recommendation?	<input type="checkbox"/> Yes	Complete Responsible Authority Recommendation section
	<input checked="" type="checkbox"/> No	Complete Responsible Authority and Officer Recommendation sections

Responsible Authority Recommendation

That the Metro Inner DAP resolves to:

1. **Not Accept** that the DAP Application for a "Motor Vehicle Wash", reference DAP/24/02818, is appropriate for consideration as land use and form compatible with the objectives of the Mixed Use zone (Clause 3.2.1 e) in accordance with City of Fremantle Local Planning Scheme No. 4 for the following reasons:
 - a. In relation to part (i), the land use configured in the form proposed is incompatible within the immediate locality and the South Fremantle heritage area more generally. The development is inconsistent with and would have a detrimental impact on the viability of nearby mixed use developments including those with small commercial shop fronts with traditional 'high street' frontages. Locating the proposed land use outside of a Neighbourhood Centre, Commercial and/or Industrial zone is detrimental to the viability and function of those zones. The proposed land use and form of development would be more appropriately located accommodated in a Commercial, Industrial or Neighbourhood Centre zone;
 - b. In relation to part (ii), the proposal fails to provide for residential development either at an upper level and/or at the ground level, and fails to establish the active public domain required in a Mixed Use zone. The proposed use in isolation, a use that would be an 'X' use in the surrounding Residential zones, would have a detrimental impact on the amenity and viability of residential and other non-residential uses in the mixed use zone and the locality generally;
 - c. In relation to part (iii), the form of development is inconsistent with the desired future character of the area, negatively impacting the cultural heritage significance of the South Fremantle heritage area generally and the immediate locality specifically;
 - d. In relation to part (iv), the form of development and the operations as proposed would further erode the amenity for residents in the immediate locality due to its undesirable impact on the streetscape, light overspill, noise impacts over long periods of the day and evening, and vehicles exiting the site wanting to head north or east impacting local residential streets; and,
 - e. In relation to part (v), the form of development as proposed blights the capacity to ensure the ongoing conservation of places of heritage significance in the immediate locality.
2. **Refuse** DAP Application reference DAP/24/02818 and accompanying plans (attachment DA01 (Rev. D), DA02 (Rev. D), DA03 (Rev. E), DA04 (Rev. D), LS01 (Rev. D), LS02 (Rev. D)) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of and the provisions of City of Fremantle Local Planning Scheme No. 4, for the following reasons:

- a. The proposed built form is detrimental to the heritage significance of the South Fremantle Heritage Area in accordance with the objectives of LPP 3.6 Heritage Areas policy (current and draft),
- b. The proposed car wash land use is incompatible with the objectives of the Mixed Use zone in Local Planning Scheme No. 4, having regard to the desired future character of the area which seeks a mix of commercial and residential uses, and the relationship of the development with existing surrounding uses.
- c. The proposed car wash will have an impact on the traffic flow and pedestrian safety in the area in accordance with Clause 67, Schedule 2 of the Planning and Development (Local Planning Schemes) No. 4. Specifically, resultant vehicle queuing onto South Street and the potential for an increase in congestion will be to the detriment of the traffic network, and may result in an unacceptable risk to pedestrians.
- d. The proposed car wash will impact on the amenity of surrounding residential land uses with regard to noise and activity outside of acceptable hours.

Reasons for Responsible Authority Recommendation

The proposed land use is deemed inappropriate for discretionary approval having given due regard to the submissions made after giving special notice in accordance with clause 64 of the *Planning and Development (Local Planning Schemes) Regulations 2015*, Schedule 2.

The Motor Vehicle Wash ('car wash') proposal is for a stand-alone use and presents as a conventional car wash form that would be appropriately located within a Neighbourhood Centre, Industrial or Commercial zone where such activity is typically separated from residential zones and where the more commercial/utilitarian form of development can be accommodated. These zones are specifically established to accommodate uses like a car wash and permitting such uses to be established outside of these zones could be detrimental to the function, viability and diversity of these zones.

The Mixed Use zone, especially when presented as a traditional strip within a recognised heritage area, provides for a somewhat blurred interface between compatible small scale non-residential land uses and the residential side streets. Mixed Use development is also intended to deliver sympathetic urban forms that accommodate a diverse mix of uses and activities that can be successfully interweaved within the predominant residential enclave and which support a vibrant local lifestyle within a walkable neighbourhood. That is, both the land use(s) and the form of development need to be compatible within the surrounding residential zone and sympathetic to the heritage area and desired character of the area.

While it is acknowledged that this car wash, as an 'A' use in LPS4, may be considered subject to discretion, the form of this proposal presents as being wholly inconsistent within the South Fremantle Mixed Use zone. The use is presented within a stand-alone building surrounded by open space accommodating industrial-like functions. The typology of development in isolation is incapable of delivering the suitable urban form expected within the Mixed Use zone. Likewise, it is neither

complimentary or compatible in juxtaposition with the abutting residential developments.

As the car wash function is not shrouded by the inclusion of other elements and/or land uses on the development site, it will have a detrimental impact on the amenity and built form of the immediate locality and the South Fremantle Heritage Area, including as follows:

1. The development proposes a wholly inconsistent built form for this gateway site to the South Fremantle Mixed Use zone and heritage area.
2. The excessive setback of the building from the surrounding streets, the signage, and the placement of bin stores and high screens around the site perimeter collectively present a built form that is the antithesis of the form that is reasonably anticipated on a landmark corner.
3. The LPS4 requires that mixed use development “shall provide for residential uses at upper level” and “provide for a mix of compatible land uses”. While it might be argued that these requirements are intended to be met across a whole zone, the strip form and non-contiguous nature of mixed use zones in South Fremantle requires more emphasis on achieving suitable mixed-use outcomes on individual sites. Furthermore, the subject site is large compared to most sites along the South Terrace mixed use area and there is a reasonable expectation that a genuine mix of uses should be required on larger sites to deliver the diversity of uses and suitable forms expected in a mixed use zone.
4. The closely intertwined and blurred nature of Mixed Use zones within Residential zones places consider emphasis on ensuring that land uses that would not be permitted in the Residential Zone are adequately separated or screened from residences when considered for approval in an abutting mixed use zone. The car wash use would not be capable of approval on the adjoining residential lots but it is not contained within a mixed use development so its impact on the abutting residences should be assessed as if the use was being proposed in a residential zone, i.e., not permitted. The proposal does not provide adequate separation and has a detrimental amenity impact resulting from long hours of noise generation, light overspill, and built forms that are alien within a predominantly residential area.
5. The nature of the site on a busy signalised intersection results in all vehicles exiting the site having to turn left onto South Terrace. Exiting vehicles travelling to destinations that are either to the north or east of the car wash will almost certainly seek to turn off South Terrace into the very tight network of side streets seeking ways to return to the distributor roads heading north and east. The resulting ‘rat-runs’ through the surrounding convoluted road network will have a detrimental impact on the amenity of nearby residents.
6. In the instance that DAP seeks to approve the development, the following amendment to condition 18 is supported *“The Motor Vehicle Wash and all associated facilities, including the dog wash and all vacuums, shall be limited to the operating hours of between 6.30am and 7pm”*. The reason for this amendment is to ensure noise impacts and light overspill associated with the land use remain compatible with the surrounding residential zone where such a land use is an 'X' use and not permitted.

Details: outline of development application

Region Scheme	Metropolitan Region Scheme
Region Scheme - Zone/Reserve	Urban Zone
Local Planning Scheme	Local Planning Scheme No. 4
Local Planning Scheme - Zone/Reserve	Mixed Use Zone
Structure Plan/Precinct Plan	N/A
Structure Plan/Precinct Plan - Land Use Designation	N/A
Use Class and permissibility:	Motor Vehicle Wash – A
Lot Size:	1321m ²
Existing Land Use:	Educational Establishment
State Heritage Register	No
Local Heritage	<input type="checkbox"/> N/A <input type="checkbox"/> Heritage List <input checked="" type="checkbox"/> Heritage Area
Design Review	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other
Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

The application seeks approval for the demolition of an existing building and the construction of a Motor vehicle wash development and signage at No. 234 (Lot 27, 29, 300 & 301) South Terrace, South Fremantle (subject site). The proposed development includes the demolition of the existing building and the construction of a Motor vehicle wash and associated facilities. The key components of the proposed development can be seen at Attachment 1 and are summarised as follows:

Proposed Land Use	Motor Vehicle Wash (A)
Proposed Net Lettable Area	N/A
Proposed No. Storeys	Single Storey
Proposed No. Dwellings	N/A

The works component of the development includes:

- Demolition of the existing building on site.
- Construction of a Motor vehicle wash building consisting of three (3) self-closing car wash bays and a plant room.
- Modification of the existing crossovers on South Street and South Terrace (cars to enter from South Street and exit via South Terrace).
- Installation of four (4) vacuum car bays and three (13) car parking bays.

- Installation of a dog wash facility.
- Construction of a bin store on the corner of South Street and South Terrace.
- Construction of a 2.1m high screen fence along the east side boundary.
- Installation of signage.
- Installation of landscaping.

The applicant submitted amended plans/additional material on 12 June 2025 including the following:

- Modification of the Motor vehicle wash building roof form and materials.
- Amendments to the colour palette and external finishes.
- Amendments to the Transport Impact Statement (including a peer review) and Operational Management Plan (to further address noise management).
- A response to City of Fremantle feedback and additional justification on the merits of the proposal.

Amended development plans are included as Attachment 1. The original development plans are included as an attachment.

Background:

Subject Site

The subject site features an existing building being used as an Educational establishment. The site was previously used as a boat sales yard and other related commercial activities. The site is a corner lot fronting South Street to the north and South Terrace to the west and adjoins a residential lot to the east and shops and residential dwellings to the south. The site is zoned Mixed Use under LPS4 and is within the South Fremantle Precinct Heritage Area. No. 234 South Terrace consists of four (4) lots featuring a combined land area of 1321m² as detailed in the table below. A condition is recommended for the lots to amalgamated prior to occupation of the development.

ADDRESS	LANDOWNER	VOL.	FOLIO	AREA (M²)
Lot 27 (No. 234) South Terrace, South Fremantle	Glowmark Nominees Pty Ltd	1965	368	445m ²
Lot 29 (No. 234) South Terrace, South Fremantle	Glowmark Nominees Pty Ltd	1965	369	438m ²
Lot 300 (No. 234) South Terrace, South Fremantle	Glowmark Nominees Pty Ltd	1965	368	48m ²
Lot 301 (No. 234) South Terrace, South Fremantle	Glowmark Nominees Pty Ltd	1965	369	390m ²

Site History

A search of the property file has revealed the following history for the site:

- Temporary shelter addition to an existing Educational establishment – DA0289/24
- Change of use to boat sales yard and office and the construction of a 1.8m high chainmesh fence – DA250/88A – 1990
- Change of use to real estate offices/marine broker/travel agency and the extension of existing office
- Carport addition to south east corner of site – 8624/1975
- Office and workshop construction for a car sales yard – 1974

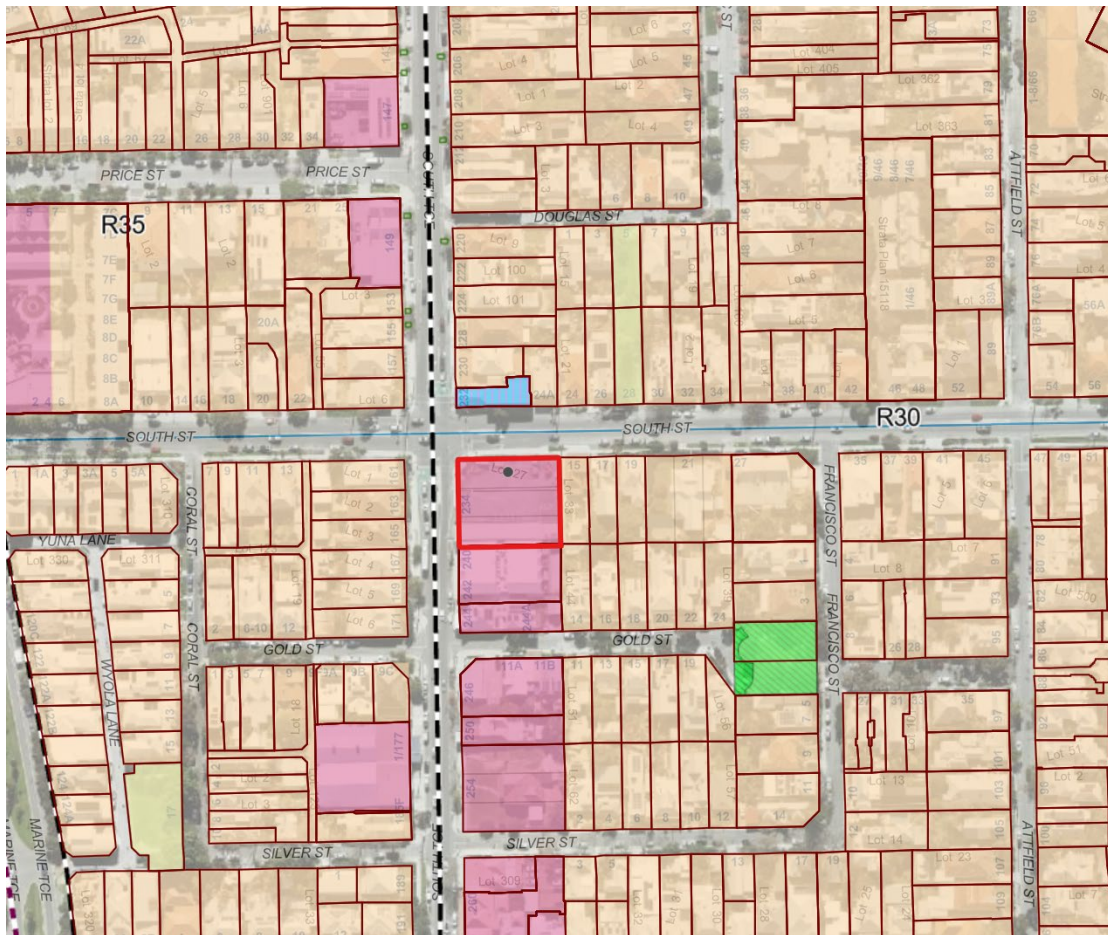


Figure 1 – Planning Context Map

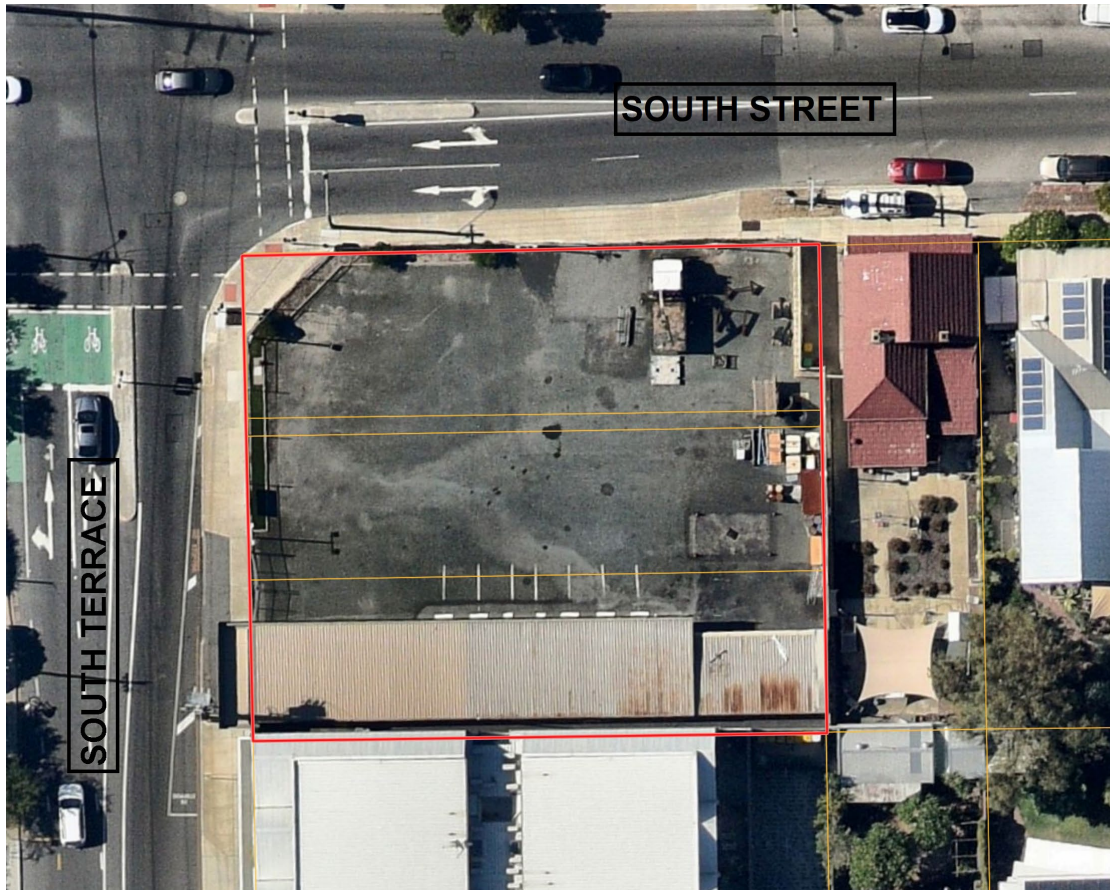


Figure 2 – Site Aerial Image



Figure 3 – Existing Street View

Legislation and Policy:

Legislation

1. *Planning and Development (Local Planning Schemes) Regulations 2015 (WA)*
2. Metropolitan Region Scheme
3. City of Fremantle Local Planning Scheme No. 4

Planning and Development (Local Planning Schemes) Regulation 2015 (WA):

- Schedule 2, Part 8, Clause 64 – Advertising applications
- Schedule 2, Part 9, Clause 67 – Matters to be considered by local government
- Schedule 2, Part 9, Clause 68 – Determination of applications
- Schedule 2, Part 9, Clause 70 – Form and date of determination

Local Planning Scheme No. 4:

- 3.2.1 - (b) Mixed Use zone objectives
- Table 1 - Zoning
- 4.7 - Vehicle Parking

State Government Policies

- State Planning Policy 7.0: Design of the Built Environment

Local Policies

- Local Planning Policy 1.1 – Planning Refunds, Amendments, and Community Consultation
- Local Planning Policy 1.6 – Heritage Assessment and Protection
- Local Planning Policy 1.10 – Construction Sites
- Local Planning Policy 2.8 – Fences
- Local Planning Policy 2.14 – Advertisement Policy
- Local Planning Policy 2.24 – Waste Management Plans for New Development
- Local Planning Policy 3.6 – Heritage Areas

The development assessment has been undertaken in accordance with the abovementioned legislation and policies.

Consultation:

Public Consultation

In accordance with LPP1.1 (Planning Refunds, Amendments, and Community Consultation), the application was advertised to the public from the 15 January 2025 until 14 February 2025 by means of letters to owners/occupiers of properties within a 200m radius of the site, a sign on the street frontage, the City's MySay webpage and notices in the press. A 'Talk to a Planner' session was also held on 25 February 2025 at the City's library.

In response, the City received a total of 91 submissions. 81 submissions were received that objected to the proposal and 10 submissions were received in support of the proposal. A schedule of the public submissions which includes the full details on each

submission as well as the applicant's response to these submissions is included as an attachment.

A summary of the key points raised in the submission in support of the proposal are detailed below:

- The location will provide an opportunity for surrounding residents to wash their vehicles.
- The proposal appears to be an improvement in aesthetic appearance compared to the existing development and is an appropriate use given that many vehicles in the locality are forced to park on the street and will then be capable of being washed without the chemical residue running into the storm water drainage system.
- The proposed dog wash will provide a location for surrounding residents to wash their dogs after visits to the beach.
- The proposed landscaping will be an improvement over the site's current existing paved bitumen surface.
- The proposal is a good utilisation of the site and will provide useful amenities to the residents and ratepayers.

A summary of the key points raised in objection to the proposal are included in the table below:

Table 1 – Summarised Objections and Officer Comments

Issue Raised	Officer comments
The location is not appropriate and will be dangerous for pedestrians. The proposal will generate too much traffic for the area and will result in an increase in traffic congestion. The South Street/South Terrace intersection is one of the most heavily used entry points to the city for both private and public transport. The vehicles exiting onto the street will result in increased traffic through back streets.	The Traffic Impact Statement indicates that the development has the potential to generate up to 18 vehicles per hour. It is expected that many people using the car wash would be residents of the area or people visiting the area for other reasons.
The car wash is not needed, there are already car washes located on Victoria Street, Hampton Road and in North Fremantle.	Figure 4 illustrates the car washes located in Fremantle. They are generally evenly spaced throughout the area. It is not considered that the approval of a small scale car wash would result in an over densification of car washes in the immediate or surrounding area.
The proposal is not in keeping with the South Fremantle heritage area and will detract from the houses, cafes, restaurants and bars that contribute to the area. The proposed carwash, with its utilitarian design featuring painted cement board and a lightweight steel frame, would starkly contrast with the character of the area. No textural, tonal	The applicant submitted an amended design based on advice from the City. The amended design is considered an improvement and will reduce any potential visual impact from the built form.

or formal qualities are present. It appears to be a painted tilt up concrete building.	
The noise pollution from the car wash machinery, vehicles, or other operations will be intolerable and incessant for those of us living in close proximity. The 24/7 operation would subject the surrounding residential area to constant machine noise and activity, particularly at night. It would it be best to restrict operation to daytime hours only.	Appropriate conditions of approval are recommended to ensure the development will operate in accordance with the applicable legislation. The operating hours have been reduced to between 6:30am and 10:30pm as per the Operational and Noise Management Plan included as an attachment.
The site is within a predominantly residential area and is an unacceptable use of the mixed use zone.	The proposal has been assessed against the objectives of the Mixed Use zone in the following sections.
There is little to no mention in the proposal of the potential environmental impact of this facility, particularly regarding chemical runoff and airborne pollutants from sprays. These chemicals could negatively affect the health of residents and the surrounding environment, and these risks need to be seriously addressed.	Water run off from the car wash bays will be directed to a series of silt pumps and convey flows through an oily water separation prior to connection to Water Corporation sewer. A standard condition is recommended for storm water and waste water disposal plans to be approved prior to lodgement of a building permit application.
The car wash will also have a detrimental impact on property values in the area. The noise, traffic, light, and other disturbances will likely make the area, particularly my neighbouring property, less desirable for future buyers, and this will inevitably lower the potential sale price for local residents.	The scale and intensity of the development is not considered to be significant. The design of the building has been amended to reduce any detrimental impact to visual amenity. It is not considered that the proposal will have a significant impact on property values in the area.
A better option would be another boutique build similar to 240 South Terrace that would allow more people to live in and enjoy Fremantle.	Assessment of the application must take into considered the proposal that has been lodged.
The proposed car wash is out of touch with a community who do not prioritise pride in car ownership over more important social issues.	More important social issues are not a consideration in the assessment of the proposal. A car wash will provide a service for residents who may not have a space to wash their vehicle.
The 1.8m high wall along South Terrace creates a potential blind spot for pedestrians. Drivers may have difficulty seeing pedestrians crossing in front of the car wash entrance, increasing the risk of accidents.	The front fence is setback 1m from the lot boundary to allow for acceptable sightlines between the street and subject site.
The proposed landscaping is a positive improvement however, the use on non-natives is not desirable. The renders are misleading and depict plants that	An amended landscaping plan has been submitted which includes a greater variety of native ground covers and shrubs.

don't exist. The use of larger hedging shrubs would soften the predominantly harsh brick and concrete appearance of the site.



Figure 4 – Map of existing Motor vehicle wash facilities in Fremantle.

Other Advice

City of Fremantle Heritage Comment

The demolition of the existing buildings and structures on site is acceptable as it will not adversely affect the values of the heritage area. New infill development needs to positively respond to the character of the heritage area and comply with LPP3.6 Heritage Areas.

The proposed carwash building is not fully consistent with some of the provisions of LPP3.6 Heritage Areas because it is set back behind the established front setback to both South Terrace and South Street. However, the scale, form and articulation of the building has been modified following the receipt of initial heritage advice and it is now largely sympathetic with the general built form character of the heritage area:

- The building has a simple rectangular plan form.
- The building is a relatively modest structure which is largely single storey but with a small two storey section at the South Street end.
- The roof is a simple hip and gable structure that responds to the largely residential character of South Street and with the character of historic commercial buildings in South Terrace.

- The scale of the building is reduced by the articulated verandah element which interprets traditional building form in the area while also providing amenity for building users.
- Building materials are generally sympathetic with the painted and rendered historic commercial buildings in the area and the colour scheme is discrete using creamy limestone colours to blend with the use of natural stone in the area. Signage is also reasonably discrete and limited in size.
- The bin store and car vacuum bay wall elements are 1800mm high fence like elements but there are large gaps between these elements to allow carwash activity to activate the street.

On balance the proposed development will have only a minor impact on the heritage value of the south Fremantle Heritage Area and is therefore supported. The Heritage Officer Assessment is included as an attachment.

City of Fremantle Internal Referral

The application was referred to the relevant internal departments at the City of Fremantle for formal comment as part of the assessment process. All departments were generally satisfied with the proposed development. Any comments received will be addressed via conditions and advice notes. The comments received related to ensuring standard requirements are followed, specifically:

- Building permit required (advice note)
- No unauthorised works within thoroughfare (advice note)
- Construction/demolition management plan (condition)
- Detailed parking plan (condition)
- Vehicle crossover upgrade (advice note)
- Removal of asbestos (advice note)
- Sand drift from property (advice note)
- Noise from construction works (advice note)
- Acoustic noise from development (advice note)

Planning Assessment:

The proposal has been assessed against all the relevant legislative requirements of the Scheme, State and Local Planning Policies, and outlined in the Legislation and Policy section of this report. The following matters have been identified as key considerations for the determination of this application:

- Land use
- Parking
- Demolition of buildings
- Traffic and access
- Noise
- Signage
- Heritage
- Signage

Land Use

Table 2 – Land Use Permissibility

Provision	Proposal	Zone	Permissibility
Local Planning Scheme 4 clause 3.3 - Zoning Table	Motor Vehicle Wash	Mixed Use	A

A Motor Vehicle Wash is an 'A' use in the Mixed Use zone which means that the use is not permitted unless the Council has exercised its discretion and has granted planning approval after giving special notice in accordance with clause 64 of the *Planning and Development (Local Planning Schemes) Regulations 2015*, Schedule 2. In considering an 'A' land use, the Council will have regard to the matters set out in clause 67 of the Regulations, Schedule 2. In this regard the following matters have been considered:

- (a) the aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area;*
- (c) any approved State planning policy*
- (g) any local planning policy for the Scheme area;*
- (k) the built heritage conservation of any place that is of cultural significance;*
- (l) the effect of the proposal on the cultural heritage significance of the area in which the development is located;*
- (m) the compatibility of the development with its setting, including —*
 - (i) the compatibility of the development with the desired future character of its setting; and*
 - (ii) the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development;*
- (n) the amenity of the locality including the following —*
 - (i) environmental impacts of the development;*
 - (ii) the character of the locality;*
 - (iii) social impacts of the development;*
- (s) the adequacy of —*
 - (i) the proposed means of access to and egress from the site; and*
 - (ii) arrangements for the loading, unloading, manoeuvring and parking of vehicles;*
- (t) the amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system in the locality and the probable effect on traffic flow and safety;*
- (w) the history of the site where the development is to be located;*
- (y) any submissions received on the application;*

For the purpose of assessing matter (a) above, the objectives of the Mixed Use zone are as follows:

Development within the mixed use zone shall —

- (i) provide for a mix of compatible land uses including light, services and cottage industry, wholesaling, trade and professional services, entertainment, recreation and retailing of goods and services in small scale premises, including showrooms, where the uses would not be detrimental to the viability of retail activity and other functions of the City Centre, Local Centre and Neighbourhood Centre zones;*

- (ii) provide for residential at upper level, and also at ground level providing the residential component is designed to contribute positively to an active public domain;*
- (iii) ensure future development within each of the mixed used zones is sympathetic with the desired future character of each area;*
- (iv) ensure that development is not detrimental to the amenity of adjoining owners or residential properties in the locality, and*
- (v) conserve places of heritage significance the subject of or affected by the development.*

The proposed development is considered to be consistent with the Regulations and zone objectives for the following reasons:

- The proposal is for a commercial use which includes three (3) wash bays and four (4) vacuums. The scale and intensity of the development is considered appropriate and should not result in any significant impact to the amenity of the locality.
- The approval of this development will not result in an over-densification of motor vehicle wash facilities in the area.
- It is considered that a Motor vehicle wash will provide a convenient service for surrounding residents on a site that has historically been utilised for commercial purposes.
- Any potential impact to the amenity of the area will be minimised by the design of the wash bays which feature self-closing doors. People will not leave their vehicles and the operation of the wash cycle is automatic. The enclosing of vehicles in the wash bay will minimise any spray drift and reduce impact from noise.
- A 2.1m high noise barrier is proposed along the east boundary and a 1.8m high barrier along a portion of the west boundary to mitigate noise from the wash bays and vacuums.
- The Motor vehicle wash and all associated facilities will cease operation at 10:30pm, with reduced operations between the 'night period' (as defined by the Noise Regulations) between 10pm and 10:30pm. The operating hours indicated in the Amended Operational and Noise Management Plan have been included as a recommended condition.
- The design of the proposal has undergone several revisions and is supported by City Heritage Officers. There should be no impact on the amenity of the area resulting from the height, bulk, scale or appearance of the building. The conservation of the heritage significance of the area has been a key focus of this development and is addressed further in the following sections.
- It is considered that the proposal will operate without resulting in any significant impact to the traffic network in the locality, as addressed in the following sections

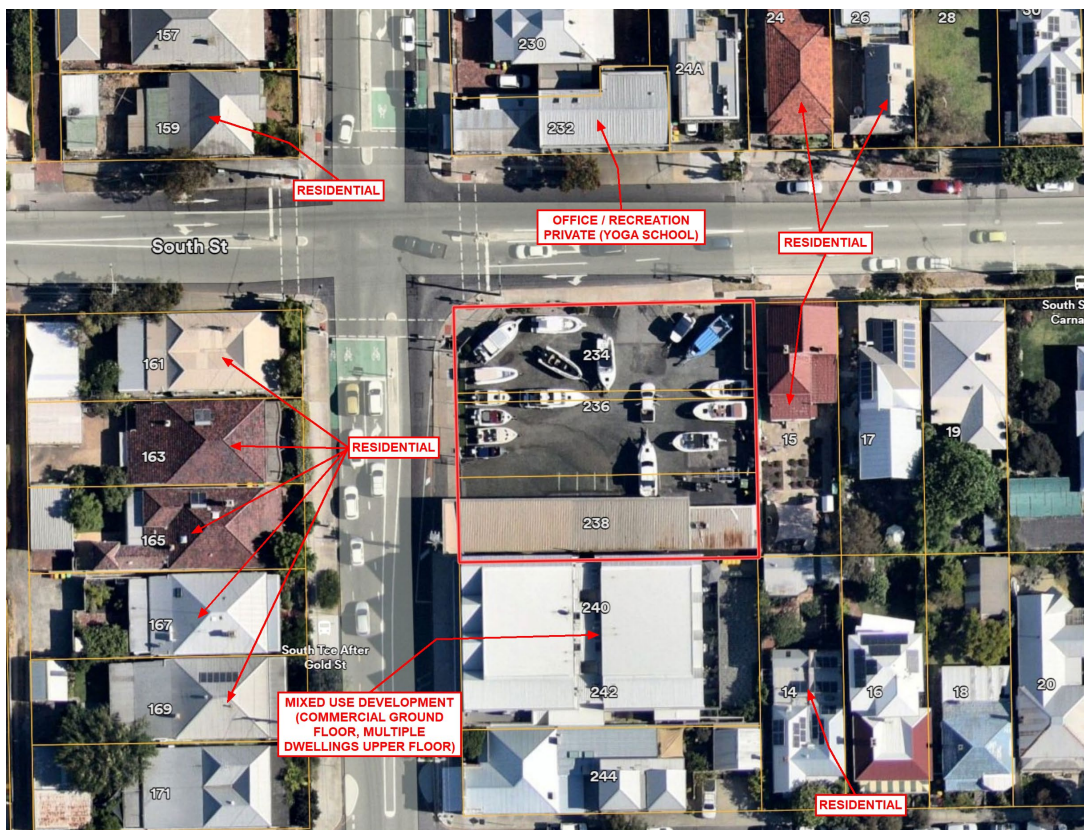


Figure 5 – Illustration of land uses surrounding the subject site.

Parking

The proposed onsite vehicle parking has been assessed against LPS 4 cl. 4.7.2 Vehicle Parking Requirements. Refer Table 3 below.

Table 3 – Required vehicle parking

Element	Requirement	Proposed	Extent of Variation
Motor Vehicle Wash	Car bays required 2: 1 wash bay Required: 6 bays	7 bays (inclusive of 4 vacuum bays)	Complies

The provision of onsite carparking complies with the requirements of LPS4. A standard condition is recommended for a detailed parking plan design which complies with the *Australian Standard AS/NZS 2890* and *AS/NZS 1428*.

Demolition of Buildings

The requirements for the demolition of a building are prescribed under clause 4.14 of LPS4.

4.14.1 Council will only grant planning approval for the demolition of a building or structure where it is satisfied that the building or structure:

- (a) has limited or no cultural heritage significance, and*

(b) does not make a significant contribution to the broader cultural heritage significance and character of the locality in which it is located.

4.14.2 In considering an application under 4.14.1, Council shall have regard to any heritage assessment required under the Planning and Development (Local Planning Schemes) Regulations 2015.

The proposed development is considered to be consistent with clause 4.14 and can be supported for the following reasons:

- The building located on the subject site was constructed between 1961 and 1965 based on historical aerials. The building is considered to be a simple brick and iron warehouse. An assessment was completed by City Heritage Officers and it is considered that the place is of little cultural heritage significance and contributes minimally to the heritage character of the West End Heritage Area.

Traffic and Access

The proposed Motor vehicle wash features crossovers to allow vehicles to enter from South Street and exit onto South Terrace. The crossovers are located adjacent to the existing crossovers as illustrated by Figure 6 below. Appropriate conditions are recommended for the removal of redundant crossovers and the construction of new crossovers prior to occupation. The site will allow for up to 12 vehicles to queue while waiting for a car wash bay to become available.

The applicant has submitted an Amended Transport Impact Statement (TIS) which is included as an attachment. The TIS seeks to demonstrate that the proposed development will operate without resulting in any significant impact to the traffic network in the locality.

The TIS was referred to the City's Engineering Infrastructure team for review with the following comments provided.

- *The amended TIS submitted for the proposed development is generally adequate, with modelling and trip generation rates appropriate. Under typical operating conditions, the development is expected to maintain acceptable traffic flow and internal vehicle circulation.*
- *However, some concerns remain regarding vehicles turning right in to the subject site from South Street and vehicles exiting the site turning right onto South Terrace near a signalised intersection.*
- *These issues present safety/operational risks as well as potential congestion issues which, due to the site's constrained location and surrounding road network, may not be fully resolvable based on the current proposal.*
- *City Officers would consider both accesses being left in / left out with minor civil works such as median island extension, line marking and signage (in-line with City of Fremantle standards and specifications) as being acceptable to resolve traffic congestion and safety concerns.*
- *A design to address the works must be submitted to the City's Engineering Infrastructure Services for approval prior to the lodgement of a Building Permit application. All works are to be completed at the applicant's expense.*

Based on the advice above, a condition is recommended detailing that access to the subject site via the South Street crossover shall be restricted to left turn in only (from

vehicles travelling in a westerly direction along South Street) and vehicle egress from the site via the South Terrace crossover site shall be restricted to left turn out only (vehicles to leave the subject site in a southerly direction along South Terrace). It is considered that this will resolve potential safety/operational risks and avoid any additional congestion in the South Street/South Terrace intersection.

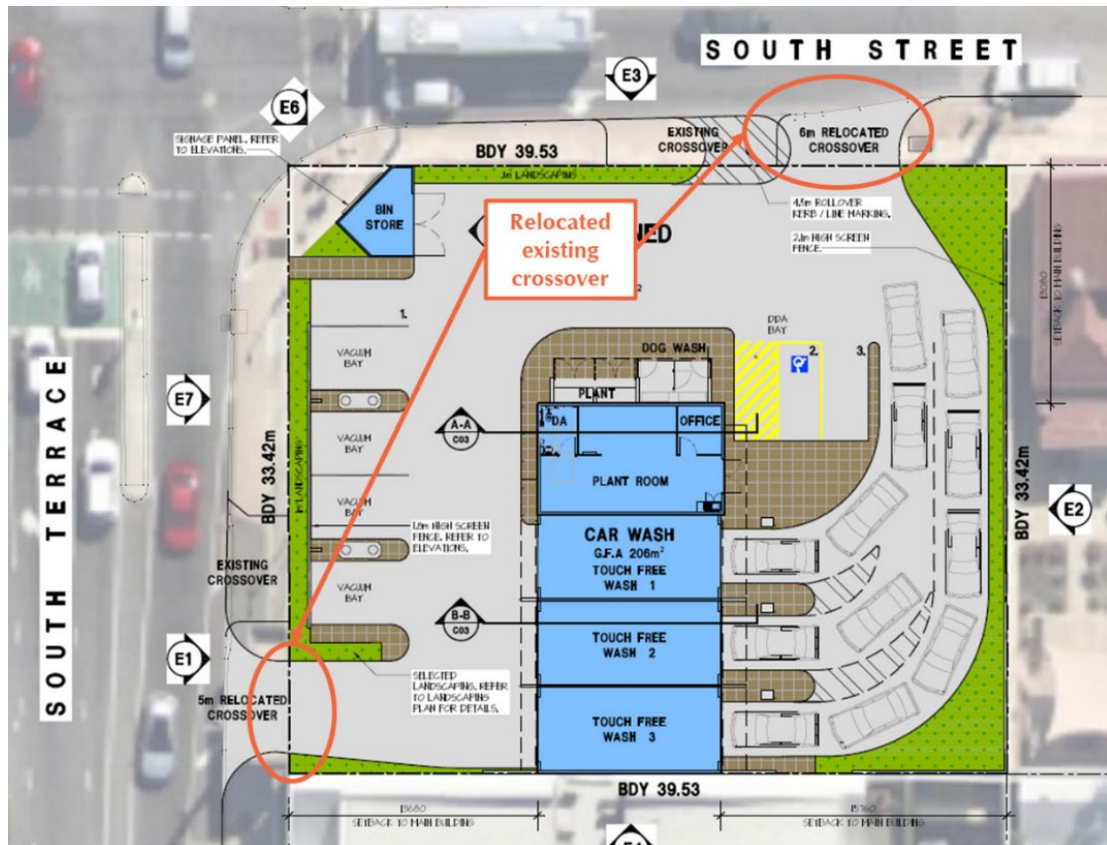


Figure 6 – Relocation of crossovers.

Noise Impact Assessment

The applicant submitted an Acoustic Report, which is included as an attachment, that acknowledges all of the above policies and seeks to demonstrate the proposal is capable of compliance with all requirements stated in the policies and guidelines, providing recommendations where appropriate and noting that acoustic treatments will be further specified when detailed designs and a full mechanical equipment schedule is provided for further assessment.

Recognising the proximity of residential properties, the applicant amended their proposal to have limited operating hours, whereas previously it was proposed to be 24 hours.

A number of appropriate conditions of approval and advice notes have been recommended to ensure the proposal is developed in full accordance with relevant requirements of the above policies and all required acoustic treatments are implemented successfully and for the life of the development.

Heritage

The subject site is located in the South Fremantle Precinct Heritage Area and is subject to assessment against Local Planning Policy 3.6 – Heritage Areas. Refer to the table below.

3.6 Infill development (new buildings)	
3.6.1 Intent	
<p>New buildings within a heritage area should respect and complement the heritage significance of the area. A respectful design approach gives special consideration to the siting, scale, architectural style and form, materials and finishes of the proposed development in relation to its neighbours, without copying historic detailing or decoration. New infill buildings should respond sympathetically to the heritage values of the heritage area as a whole, and also to that part of the heritage area in the vicinity of the proposed development. Imaginative, well designed and harmonious construction is encouraged.</p>	<p>Officer Comments</p> <p>The building is a relatively modest structure which is largely single storey. The roof is a simple hip and gable structure that responds to the largely residential character of South Street and with the character of historic commercial buildings in South Terrace.</p>
3.6.2 Design guidance	
<p>Siting and Scale</p> <p>i. New infill development within a heritage area should:</p> <p>a) Maintain a setting that is consistent with the original streetscape, including front and side setback patterns.</p> <p>b) Have a consistent bulk and scale in relation to the original street pattern. E.g. If the original street pattern is single storey then the new infill development should also be (or present as) single storey (at least to the front section of the lot).</p> <p>c) Have a plate height consistent with the original street pattern. New developments often propose a lower plate height than the earlier and original buildings. To ensure a consistency of scale the plate height is an important element to ensure it is consistent with the original street pattern.</p> <p>ii. New Infill development to secondary streets will be assessed on individual circumstances and merit. Issues to consider include:</p>	<p>Officer Comments</p> <p>As per advice from City Heritage Officers, a proposal that responded to the established street setbacks would have been preferable however, on balance the proposed development will have only a minor impact on the heritage value of the South Fremantle Precinct Heritage Area due to the building being setback into the site.</p>

<p>a) Prevailing streetscape and setbacks of the side street</p> <p>b) Avoiding a continuous wall and providing articulation of walls to a secondary street.</p> <p>c) Avoiding a two-storey height wall to the side street, unless the prevailing streetscape is predominantly two-storey.</p> <p>iii. Street setbacks deemed to comply with the above are specified in Schedule 1 for some areas.</p>	
<p>Building Form</p> <p>The form of the building is its overall shape, size and the general arrangement of its main parts.</p> <p>i. New infill building within a heritage area should respect and harmonise with and be sympathetic to the predominant form of the prevailing streetscape without mimicking heritage detailing.</p> <p>ii. Where a building form is highly repetitive, significant departures in form will appear at variance to the streetscape and should not be introduced.</p> <p>iii. The treatment of new infill buildings in terms of the roof form, proportions, materials, number, size and orientation of openings, ratio of window to wall etc. should relate to that of its neighbours.</p> <p>iv. Symmetry or asymmetry of facades in the prevailing streetscape is an element of form to be kept consistent.</p> <p>v. Contemporary building designs should respond to, and interpret, the scale, articulation and detail of the existing nearby buildings in a modern, innovative and sympathetic way.</p>	<p>Officer Comments</p> <p>The proposal includes a building with a simple rectangular form. It is not attempting to mimic or recreate any heritage detailing in the area and will be sympathetic to the surrounding built form consisting of single storey residential and commercial/ mixed use two storey buildings.</p>
<p>Materials, Colours and Detailing</p> <p>i. Materials and level of detailing should reflect / interpret the predominant materials and detailing of the original prevailing streetscape and not visually dominate the streetscape or adjacent heritage buildings.</p> <p>ii. Whilst the basic form, scale and structure of new development should be consistent with the character of the area, new buildings should not seek to emulate heritage detailing to any great extent: 'Faux' or 'mock' heritage detracts from an understanding and appreciation of the</p>	<p>Officer Comments</p> <p>Building materials are generally sympathetic with the painted and rendered historic commercial buildings in the area and the colour scheme is discrete using creamy limestone colours to blend with the use of natural stone in the area. Signage is also reasonably discrete and limited in size.</p>

<p>original building and will not be supported. New development should blend in with the streetscape but be discernible as new when looked at more closely.</p> <p>iii. Use of original or traditional colours is encouraged. Glossy materials or finishes should be avoided unless a historical precedent for their use can be demonstrated.</p>	
<p>Other Elements</p> <p>Roofs</p> <p>i. Traditionally roof lines are a predominant element of the streetscape. All new infill development shall respond to and reinforce the existing characteristics of the prevailing streetscape regarding plate and wall heights, roof form, ridge lines, parapet lines, roof slopes and eaves overhangs.</p> <p>ii. Roof forms that interpret the predominant roof forms of the prevailing streetscape may be considered.</p> <p>Verandahs / Porches / Awnings</p> <p>i. Verandahs, porches and awnings were often an important element of streetscapes. Inclusion of verandahs, porches and awnings appropriate to the streetscape are encouraged without too precisely mimicking the style of the original character-building elements or heritage detailing.</p>	<p>Officer Comments</p> <p>The roof is a simple hip and gable structure that responds to the largely residential character of South Street and with the character of historic commercial buildings in South Terrace</p> <p>The articulated verandah element interprets traditional building form in the area and will provide amenity for building users.</p>

Signage

The proposal features the following wall signs:

- 3.1m (w) x 1.5m (h) sign located on the front fence on north west corner of the subject site.
- 3.4m (w) x 1m (h) sign located on the north elevation of the building.
- 3.5m (w) x 1.2m (h) sign located on the west elevation of the building.

The proposed signage is assessed against Local Planning Policy 2.14 Advertisement Policy. Refer to the table below.

PROVISION	REQUIREMENT	ACCEPTABLE
1.1 General requirements applicable to all signs:	(a) Advertisements will not be approved on properties primarily used for residential purposes where the advertisement does not pertain to a relevant home business, occupation or store on the site unless	The signage relates specifically to the Motor vehicle wash proposed to operate on the site.

	<p>otherwise provided for in another local planning policy.</p> <p>(b) Advertisements are to be located and designed so as not to cause a hazardous distraction to motorists, pedestrians or other road users.</p> <p>(c) Advertisements will be compatible with the style, scale and character of the surrounding streetscape, and the predominant uses within the locality. Consideration will be given to the number and type of existing signs in the locality so as to avoid visual clutter.</p> <p>(d) Advertisements shall not impede pedestrian or vehicle movements.</p> <p>(e) Illuminated signs are to be maintained to operate as an illuminated sign; and</p> <p>(f) Advertisements are not to emit a flashing or moving light or radio; animation or movement in its design or structure; reflective, retro-reflective or fluorescent materials in its design structure.</p> <p>(g) Advertisements in the form of an Animated signs will not, be supported by Council.</p> <p>(h) Advertisements will not be approved on private land which include,</p> <p>i. the name, logo, or symbol of a company or other organisation that does not own or substantially occupy the site or building on which the advertisement is located, or</p> <p>ii. a product or service not provided on the site on which the advertisement is located;</p> <p>iii. a product or service that does not form part of the signage displaying the name, logo or symbol; of a company or other organisation that owns or substantially occupy the site or building on which the advertisement is located; or</p> <p>iv. signs for an activity or event not occurring on the site on which the advertisement is located.</p>	<p>The signage is not proposed to be illuminated and are not of a scale that would cause any distraction.</p> <p>The scale of the signage is appropriate in relation to the building.</p> <p>There will be no impeding of pedestrian or traffic movements.</p>
<p>Wall Signage</p>	<p>(a) The advertisement does not project above the fascia of the building and does not exceed the frontage of the tenancy; and</p> <p>(b) The advertisement(s) are restricted to three signs per street frontage per tenancy.</p>	<p>The signage will not project above the fascia of the building.</p> <p>The signage proposal includes one sign on the north elevation (facing South Street), one sign on the west</p>

		elevation (facing South Terrace) and a sign on the fence located on the north west corner.
--	--	--

State Planning Policy

State Planning Policy 7.0 - Design of the Built Environment

An assessment has been conducted against the design principles of SPP7.0 which are Context and character, Landscape quality, built form and scale, Functionality and build quality, Sustainability, Amenity, Legibility, Safety, Community, and Aesthetics.

Refer comments below:

- The proposal has been carefully considered against the distinctive characteristics of the area. It is considered that the overall design is a substantial improvement on the original proposal and will not detract from the context, character and heritage value of the area.
- The applicant submitted an amended landscaping plan which included additional shrubs and small trees based on advice provided by City Officers. The proposed landscaping will reduce the visual impact of the built form and contribute to the amenity of the area.
- The built form of the development was subject to several revisions based on advice from City Officers. It is considered that the amended design is a significant improvement and will have no impact on the amenity of the area.
- The proposal includes a legible building design with the street aspects oriented parallel to the boundaries. While the building is setback further than the adjoining buildings fronting South Street and South Terrace, it is considered that this is necessary to allow for vehicle circulation/means of access and egress.
- The traffic safety of the site has been considered. The recommended conditions restricting vehicle access/egress (left turn in and out) will resolve potential traffic safety and congestion concerns. In addition, the proposal features suitable sight lines to ensure safety of pedestrians.
- The proposed building is a simple design with materials and finishes which will contribute to the aesthetic of the South Fremantle area.

Conclusion:

Approval is sought for the demolition of an existing building and the construction of a Motor vehicle wash development and signage at No. 234 (Lot 27, 29, 300 & 301) South Terrace, South Fremantle. The proposed development is consistent with the objectives of the Mixed Use zone and will provide a service for surrounding residents. The proposal will not have a significant impact on the heritage significance of the South Fremantle Precinct Heritage Area. The proposal is supported by the City’s Heritage Officers who have deemed the works acceptable, after several revisions of the design. In accordance with the above, the application is recommended for conditional approval.

Officer Recommendation

It is recommended that the Metro Inner DAP resolves to:

1. **Accept** that the DAP Application reference DAP/24/02818 is appropriate for consideration as a “Motor Vehicle Wash” land use and compatible with the objectives of the zoning table in accordance with City of Fremantle Local Planning Scheme No. 4;
2. **Approve** DAP Application reference DAP/24/02818 and accompanying plans (attachment DA01 (Rev. D), DA02 (Rev. D), DA03 (Rev. E), DA04 (Rev. D), LS01 (Rev. D), LS02 (Rev. D)) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of and the provisions of City of Fremantle Local Planning Scheme No. 4, subject to the following conditions:

Conditions

1. This decision constitutes planning approval only and is valid for a period of 4 years from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect.
2. Prior to lodgement of a Building Permit application for the development hereby approved, storm water and waste water disposal plans, details and calculations must be submitted for approval by the City of Fremantle and thereafter implemented, constructed and maintained to the satisfaction of the City of Fremantle.
3. Prior to lodgement of a Building Permit and/or Demolition Permit application for the development hereby approved, a Construction/Demolition Management Plan shall be submitted and approved, to the satisfaction of the City of Fremantle addressing, but not limited to, the following matters:
 - a) Use of City car parking bays for construction related activities;
 - b) Protection of infrastructure and street trees within the road reserve;
 - c) Security fencing around construction sites;
 - d) Gantries;
 - e) Access to site by construction vehicles;
 - f) Contact details;
 - g) Site offices;
 - h) Noise - Construction work and deliveries;
 - i) Sand drift and dust management;
 - j) Waste management;
 - k) Dewatering management plan;
 - l) Traffic management; and
 - m) Works affecting pedestrian areas.
4. Prior to lodgement of a Building Permit application for the development hereby approved, storm water disposal plans, details and calculations must be submitted for approval by the City of Fremantle and thereafter implemented, constructed and maintained to the satisfaction of the City of Fremantle.

5. Prior to the lodgement of a Building Permit application for the development hereby approved, design plans for the location, materials and construction for screening of any proposed external building plant must be submitted to and approved by the City of Fremantle. All piped, ducted and wired services, air conditioners, hot water systems, water storage tanks, service meters, other plant and bin storage areas must be integrated into the design of the building and located to minimise any visual and/or noise impact on the occupants of nearby properties and screened from view from the street.
6. Prior to lodgement of a Building Permit application for the development hereby approved, an outdoor lighting plan must be submitted and approved by the City of Fremantle. The outdoor lighting is to be designed, baffled and located to prevent any increase in light spill onto the adjoining properties.
7. Prior to lodgement of a Building Permit application for the development hereby approved, the applicant is to submit, and have approved to the satisfaction of the City of Fremantle, a detailed parking and vehicle circulation plan design which complies with the Australian Standard AS/NZS 2890 and AS/NZSC 1428, including parking bay/s, visitor bays, loading bays, disabled bays, aisle widths, circulation areas, driveway/s and points of ingress and egress.
8. Prior to lodgement of a Building Permit application for the development hereby approved, final details of the external materials, colours and finishes of the proposed development, including a physical sample board or materials is to be submitted and approved to the satisfaction of the City of Fremantle, on the advice of the City's Design Advisory Committee.
9. Prior to lodgement of a Building Permit application for the development hereby approved, details of how the recommendations contained in the environmental acoustic report prepared by Herring Storer Associates, dated 18 October 2024 (Ref: 32932-4-24207), are to be implemented into the design/operation of the development are to be submitted and approved to the satisfaction of the City of Fremantle and thereafter maintained for the life of the development.
10. Prior to the lodgement of a Building Permit application for the development hereby approved, details on how vehicle access and egress to the subject site shall be restricted to the following movements:
 - a) Left turn in only via the South Street crossover from vehicles travelling in a westerly direction along South Street; and
 - b) Left turn out only via the South Terrace crossover for vehicles to exit the site and travel in a southerly direction along South Terrace).

The approved traffic access and egress shall be maintained for the life of the development to the satisfaction of the City of Fremantle.

11. Prior to the occupation of the development hereby approved, vehicle crossovers, including any approved works required to restrict vehicle access and egress, shall be constructed to the City's specification and thereafter maintained to the satisfaction of the City of Fremantle.
12. Prior to the occupation of the development hereby approved, any redundant crossovers shall be removed and the verge and kerbing reinstated to the City's

specifications, at the expense of the applicant and to the satisfaction of the City of Fremantle.

13. Prior to occupation of the development hereby approved, the applicant is to submit, and have approved to the satisfaction of the City of Fremantle, an Operational Management Plan addressing the following:
- Operating hours
 - Noise mitigation measures
 - Operation of the motor vehicle wash facilities
 - Deactivation process for wash and vacuum bays during the night period
 - Site and car wash maintenance and servicing procedures
 - Waste management
 - Garden and landscaping maintenance
 - Emergency contact details and complaints management

The Operational Management Plan must be implemented at all times to the satisfaction of the City of Fremantle for the life of the development.

14. Prior to occupation of the development hereby approved, the approved landscaping shall be completed in accordance with the approved plans and maintained for the life of the development to the satisfaction of the City of Fremantle.
15. Prior to the occupation of the development hereby approved, all car parking, and vehicle access and circulation areas shall be maintained and available for car parking/loading, and vehicle access and circulation on an ongoing basis to the satisfaction of the City of Fremantle.
16. Prior to occupation of the development hereby approved, No. 234 (Lots 27, 29, 300 & 301) South Terrace are to be legally amalgamated into one lot on the Certificate of Title. Alternatively, the owner may enter into a legal agreement with the City of Fremantle, drafted by the City's solicitors at the expense of the owner and be executed by all parties concerned. The legal agreement will specify measures to allow the development approval to operate having regard to the subject site consisting of two separate lots, to the satisfaction of the City of Fremantle.
17. Prior to occupation/ use of the development hereby approved, the boundary fence located on the north-west and west boundaries shall be of a clean finish in any of the following materials:
- coloured sand render,
 - face brick,
 - painted surface,
 - other approved finish

and be thereafter maintained to the satisfaction of the City of Fremantle.

18. The Motor Vehicle Wash and all associated facilities, including the dog wash and all vacuums, shall be limited to the operating hours of between 6:30am and 10:30pm.

19. The self-closing doors at the entry and exit of the car wash bays are to remain operational at all times and maintained for the life the development to the satisfaction of the City of Fremantle.
20. All works indicated on the approved plans, including any footings, shall be wholly located within the cadastral boundaries of the subject site.
21. Where any of the preceding conditions has a time limitation for compliance, if any condition is not met by the time requirement within that condition, then the obligation to comply with the requirements of any such condition (other than the time limitation for compliance specified in that condition), continues whilst the approved development continues.

Advice note(s):

1. A Building permit is required for the proposed Building Works. A certified BA1 application form must be submitted and a Certificate of Design Compliance (issued by a Registered Building Surveyor Contractor in the private sector) must be submitted with the BA1.
2. A demolition permit is required to be obtained for the proposed demolition work. The demolition permit must be issued prior to the removal of any structures on site.
3. With regards to condition 10 above, the applicant is encouraged to contact the City of Fremantle's Infrastructure Business Services department via info@fremantle.wa.gov.au or 9432 9999 to consider the works to restrict vehicle movements to left turn in/left turn out, such as median island extensions, line marking and signage. All works are to be completed to City of Fremantle standards and specifications at the applicant's expense.
4. Effective measures shall be taken to stabilize sand and ensure no sand escapes from the property by wind or water in accordance with the City's Prevention and Abatement of Sand Drift Local Law.
5. Any works within the adjacent thoroughfare, i.e. road, kerbs, footpath, verge, crossover or right of way, requires a separate approval from the City of Fremantle's Infrastructure Business Services department who can be contacted via info@fremantle.wa.gov.au or 9432 9999.
6. If construction works involve the emission of noise above the assigned levels in the *Environmental Protection (Noise) Regulations 1997*, they should only occur on Monday to Saturday between 7.00 am and 7.00 pm (excluding public holidays). In instances where such construction work needs to be performed outside these hours, an Application for Approval of a Noise Management Plan must be submitted to the City of Fremantle Environmental Health Services for approval at least 7 days before construction can commence.

Note: Construction work includes, but is not limited to, Hammering, Bricklaying, Roofing, use of Power Tools and radios etc.

7. All noise from the proposed development must comply with the requirements of the *Environmental Protection (Noise) Regulations 1997* (as amended), such as:

- a) mechanical service systems like air-conditioners, exhaust outlets, motors, compressors and pool filters;
- b) vehicles;
- c) amplified acoustic systems; and
- d) patron noise.

It is advised to seek the services of a competent acoustic consultant to assist the applicant to address the potential noise impacts on noise sensitive receivers.

8. Any removal of asbestos is to comply with the following –

Under ten (10) square metres of bonded (non-friable) asbestos can be removed without a license and in accordance with the *Health (Asbestos) Regulations 1992* and the *Environmental Protection (Controlled Waste) Regulations 2001*. Over 10 square metres must be removed by a Class B asbestos removal licence holder for. All asbestos removal is to be carried out in accordance with the *Work Health and Safety Act 2020* and accompanying regulations and the requirements of the Code of Practice for the Safe Removal of Asbestos 2nd Edition [NOHSC: 2002 (2005)];

Note: Removal of any amount of friable asbestos must be done by a Class A asbestos removal licence holder and an application submitted to WorkSafe, Department of Commerce. <https://www.commerce.wa.gov.au/worksafe/>.

9. The applicant is advised that a crossover permit must be obtained from the City's Engineering Department. New/modified crossover(s) shall comply with the City's standard for crossovers, which are available on the City of Fremantle's web site.

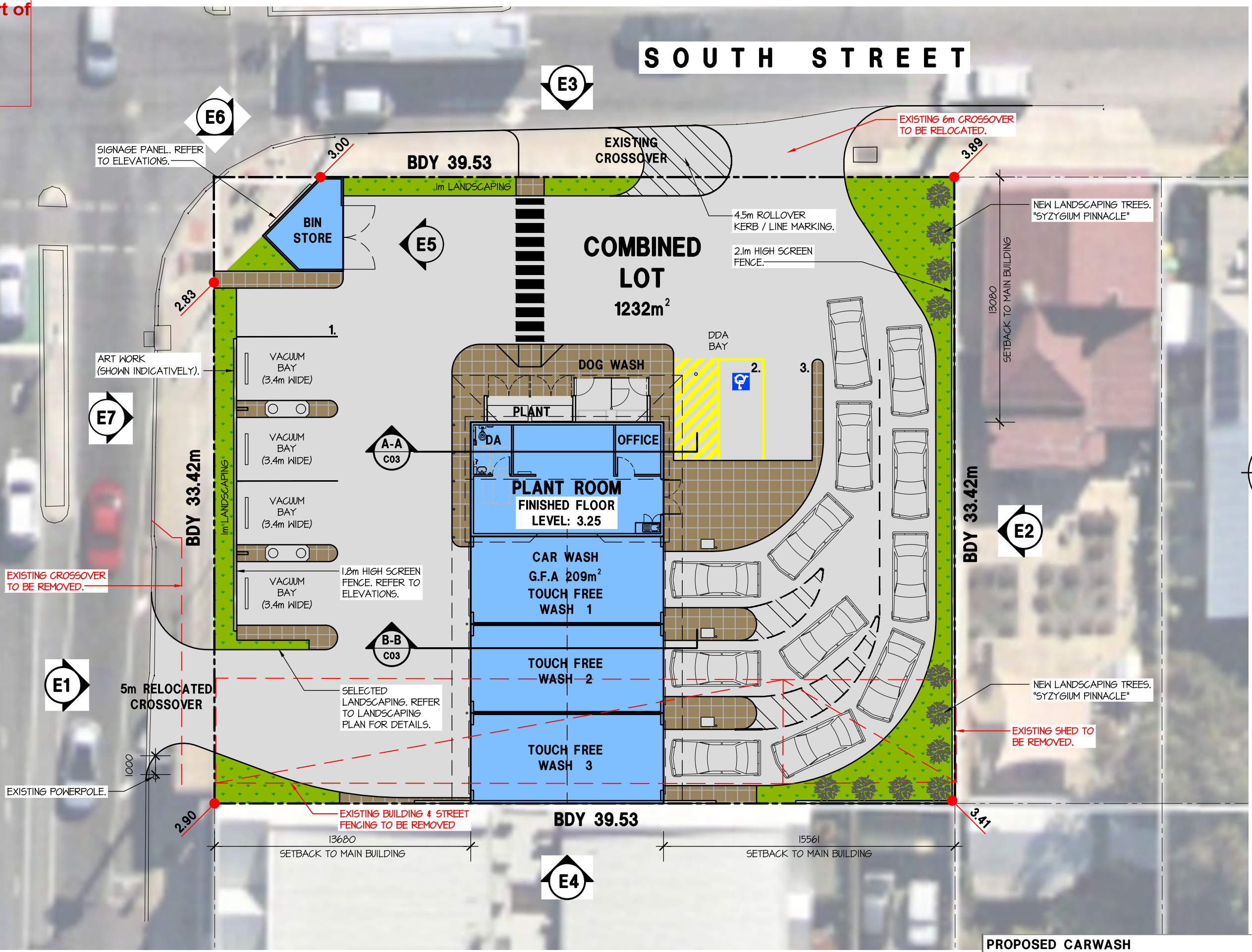
The applicant is advised that the /The new/ modified vehicle crossover shall be separated from any verge infrastructure by:

- a) a minimum of 2.0 metres in the case of verge trees
- b) a minimum of 1.2 metres (in the case of bus shelters, traffic management devices, parking embayment's or street furniture), and
- c) a minimum of 1.0 metre in the case of power poles, road name and directional signs.

10. The applicant is advised that any signage may be subject to a separate application for planning approval.

SOUTH STREET

SOUTH TERRACE



IWA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6009
 PO BOX 199
 NEDLANDS WA 6909
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

PROPOSED SITE PLAN

SCALE 1:200

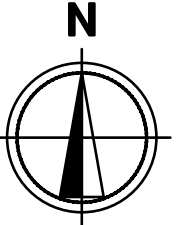
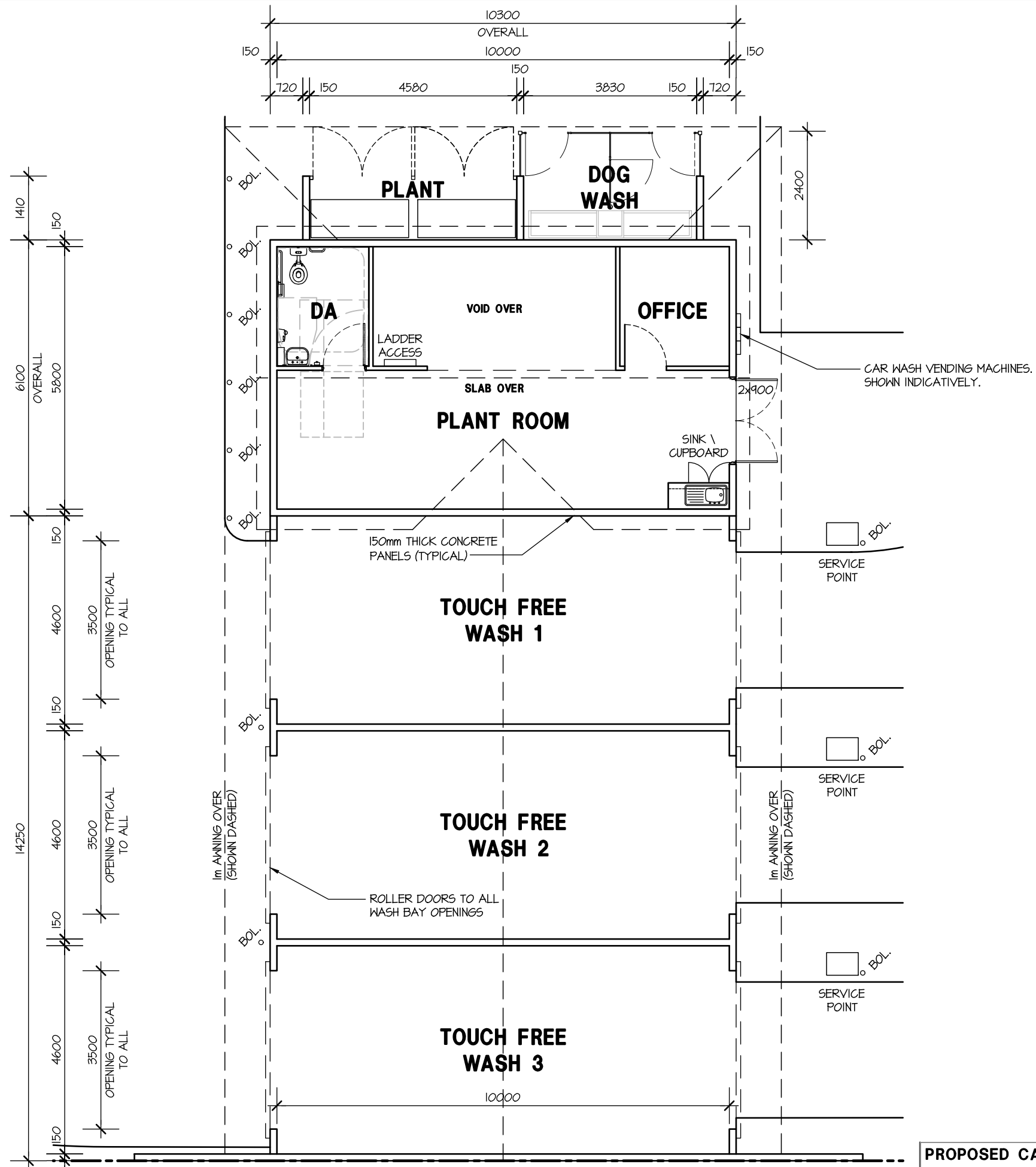
NOTE
 ALL LEVELS TO A.H.D.

PROPOSED CARWASH
 No 234 TO 238
 LOTS 27, 300, 301 & 29
 SOUTH TERRACE CNR SOUTH STREET
 SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD. DO NOT SCALE FROM THIS DRAWING. THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE. THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.	NO: D DATE: 03.06.25 REVISION: DEVELOPMENT APPLICATION ISSUE DRAWN: MDB CHECK: SJH	A3 SHEET
---	--	-----------------

Date	03.06.25
Design	MDB
Drawn	MDB
Checked	SJH
Scale	1:200
Job No.	0857
Dwg	DA01
Rev	D

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
12 June 2025



HA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6009
 PO BOX 199
 NEDLANDS WA 6909
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

FLOOR PLAN
SCALE 1:100

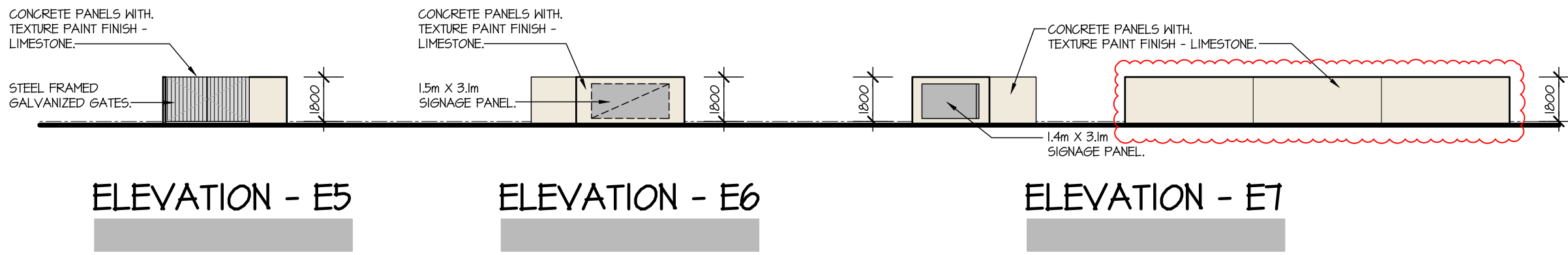
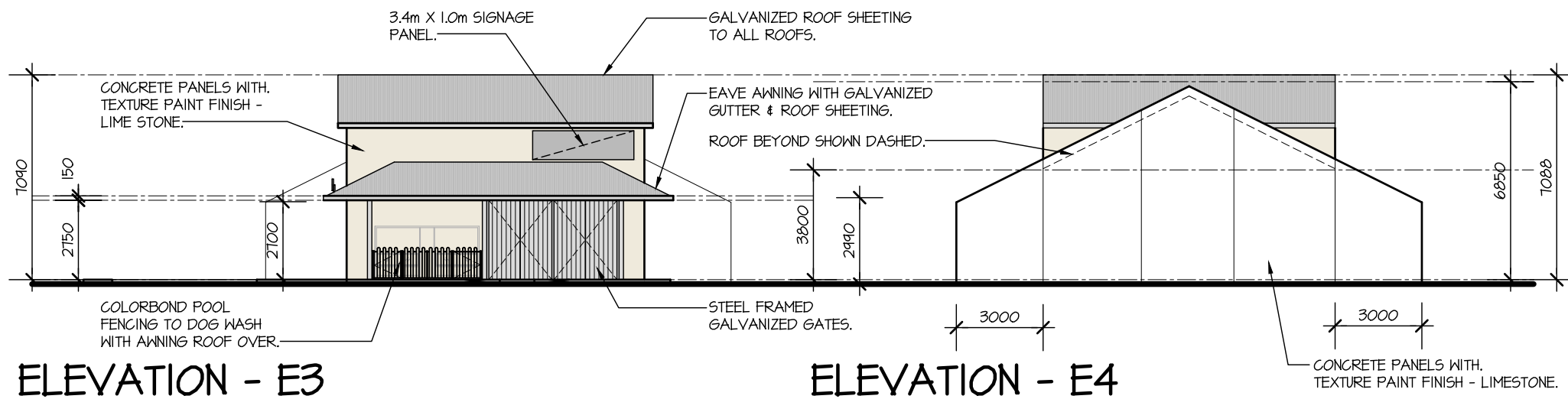
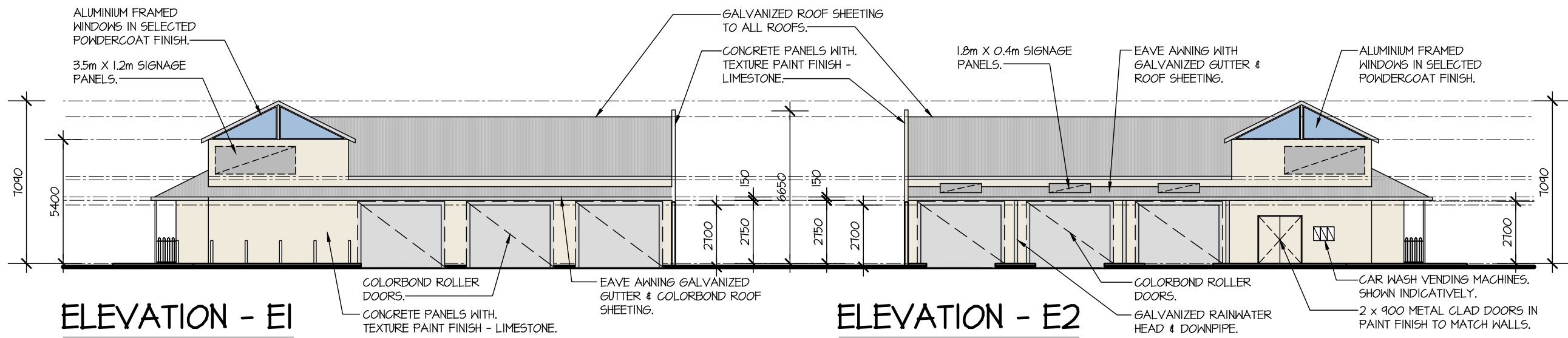
PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

THIS DRAWING IS COPYRIGHT AND MUST NOT BE
 RETAINED, COPIED OR USED WITHOUT THE
 AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
 DO NOT SCALE FROM THIS DRAWING.
 THE CONTRACTOR AND HIS SUBCONTRACTORS ARE
 TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING
 SHOP DRAWINGS OR COMMENCING MANUFACTURE.
 THIS IS A COMPUTER GENERATED DRAWING AND
 SHALL NOT BE ALTERED BY HAND.

NO:	DATE:	REVISION:	DRAWN:	CHECK:
D	03.06.25	DEVELOPMENT APPLICATION ISSUE	MDB	SJH

A3 SHEET

Date - 03.06.25
 Design - MDB
 Drawn - MSB
 Checked - SJH
 Scale - 1:100
 Job No. - 0837
 Dwg - **DA02**
 Rev - **D**



CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
12 June 2025

ELEVATIONS
 SCALE 1:200

PROPOSED CARWASH
 No 234 TO 238
 LOTS 27, 300, 301 & 29
 SOUTH TERRACE CNR SOUTH STREET
 SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

NO.	DATE	REVISION	DRAWN	CHECK
D	03.06.25	DEVELOPMENT APPLICATION ISSUE	MDB	SJH
E	13.06.25	ART WORK REMOVED	MDB	SDS

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
 DO NOT SCALE FROM THIS DRAWING.
 THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE.
 THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.

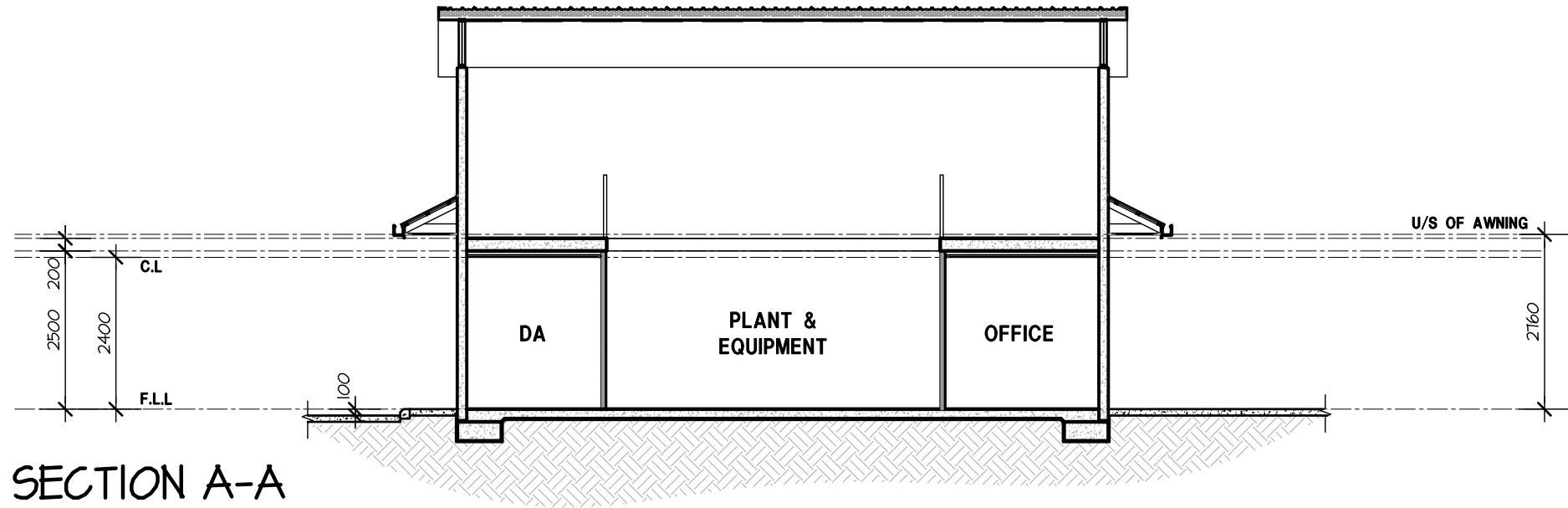
HA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6109
 PO BOX 199
 NEDLANDS WA 6109
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

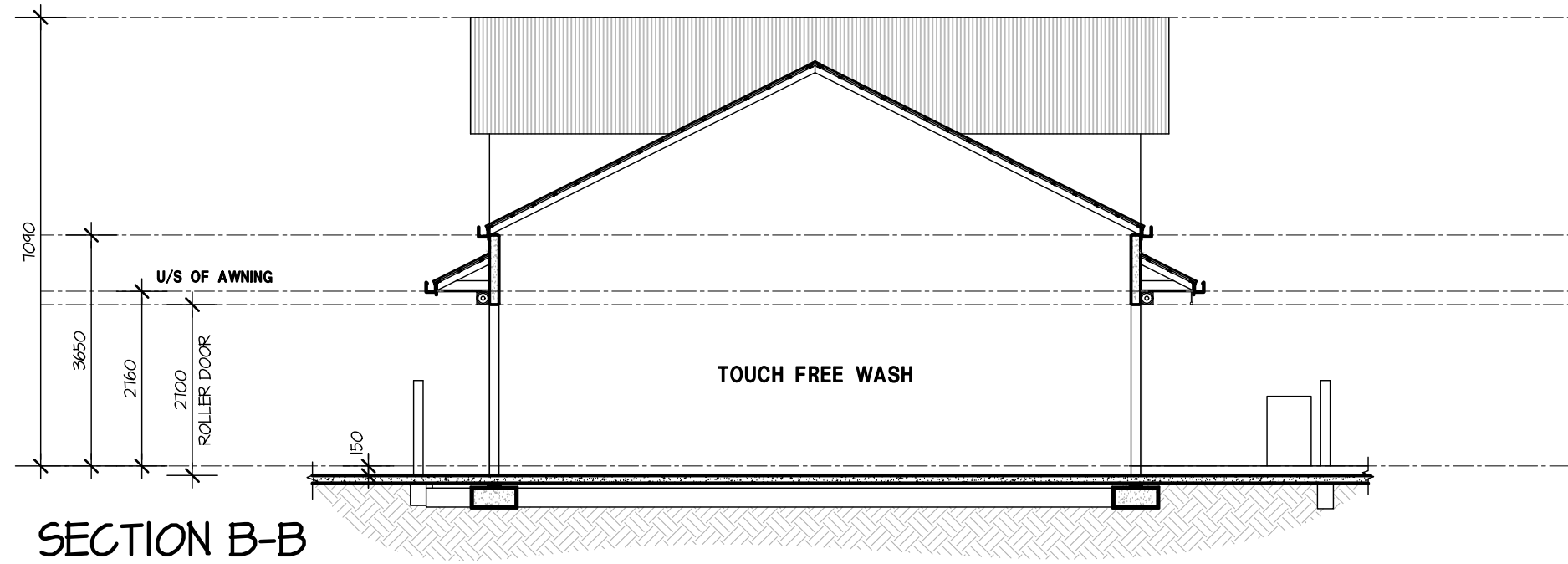
Date - 13.06.25
 Design - MDB
 Drawn - MDB
 Checked - SJH
 Scale - 1:200
 Job No. - 0987
 Dwg - **DA03**
 Rev - **E**

A3 SHEET

CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
12 June 2025



SECTION A-A
 SCALE 1:100



SECTION B-B
 SCALE 1:100

SECTIONS
 SCALE 1:100

PROPOSED CARWASH
 No 234 TO 238
 LOTS 27, 300, 301 & 29
 SOUTH TERRACE CNR SOUTH STREET
 SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD. DO NOT SCALE FROM THIS DRAWING. THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE. THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.	NO:	DATE:	REVISION:	DRAWN:	CHECK:
	D	03.06.25	DEVELOPMENT APPLICATION ISSUE	MDB	SJH

A3 SHEET

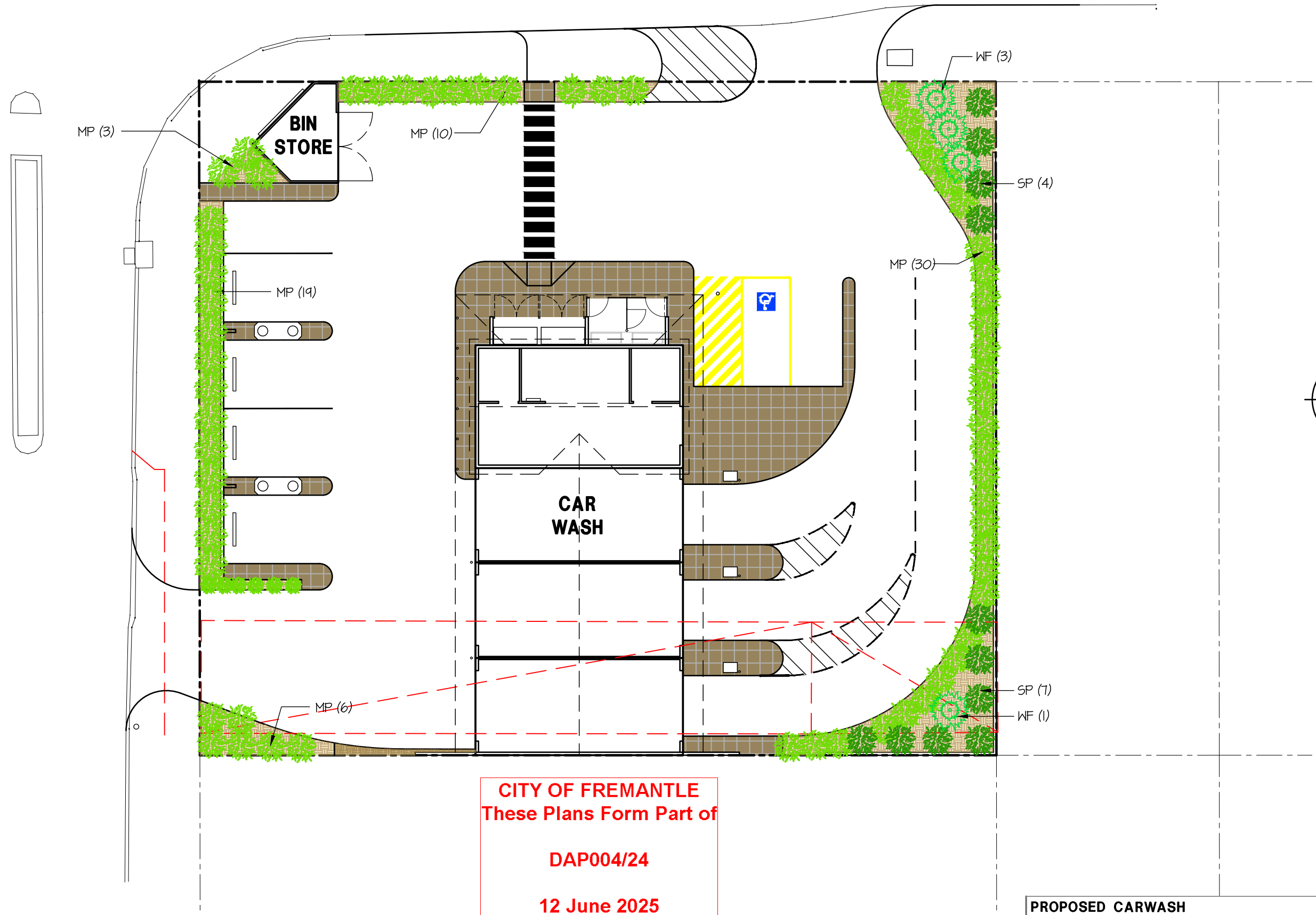
HA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6909
 PO BOX 199
 NEDLANDS WA 6909
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

Date	-	03.06.25
Design	-	MDB
Drawn	-	MDB
Checked	-	SJH
Scale	-	1:100
Job No.	-	0987
Dwg	-	DA04
Rev	-	D

SOUTH STREET

SOUTH TERRACE

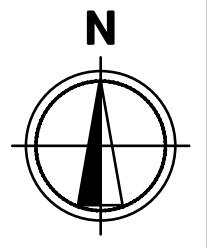


CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025

PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
for GLOWMARK NOMINEES Pty Ltd



IWA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6009
 PO BOX 199
 NEDLANDS WA 6909
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

Date - 03.06.25
 Design - MDB
 Drawn - SJH
 Checked - SJH
 Scale - 1:250
 Job No. - 0887
 Dwg - **LS01**
 Rev - **D**




THIS DRAWING IS COPYRIGHT AND MUST NOT BE
 RETAINED, COPIED OR USED WITHOUT THE
 AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
 DO NOT SCALE FROM THIS DRAWING.
 THE CONTRACTOR AND HIS SUBCONTRACTORS ARE
 TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING
 SHOP DRAWINGS OR COMMENCING MANUFACTURE.
 THIS IS A COMPUTER GENERATED DRAWING AND
 SHALL NOT BE ALTERED BY HAND.

NO.	DATE	REVISION	DRAWN	CHECK
D	03.06.25	DEVELOPMENT APPLICATION ISSUE	MDB	SJH

A3 SHEET

LANDSCAPING PLAN
SCALE 1:200

PLANT SCHEDULE

SYMBOL	BOTANIC NAME	MATURE HEIGHT X WIDTH	MINIMUM INSTALLATION POT SIZE	NUMBER
	MYOPORIUM PARVIFOLIUM	10cm X 1m	13cm	69
	WESTRINGIA FRUITCOSA	1.5m X 1.5m	13cm	4
	SYZYGIUM PINNACLE	1.5m X 6m	40cm	11

SHRUB / GROUND COVER VARIETIES



WESTRINGIA FRUTICOSA



MYOPORIUM PARVIFOLIUM

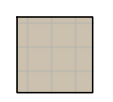


SYZYGIUM PINNACLE

PAVING LEGEND



SELECTED MULCH UNDER PLANTING AS PER NOTES.



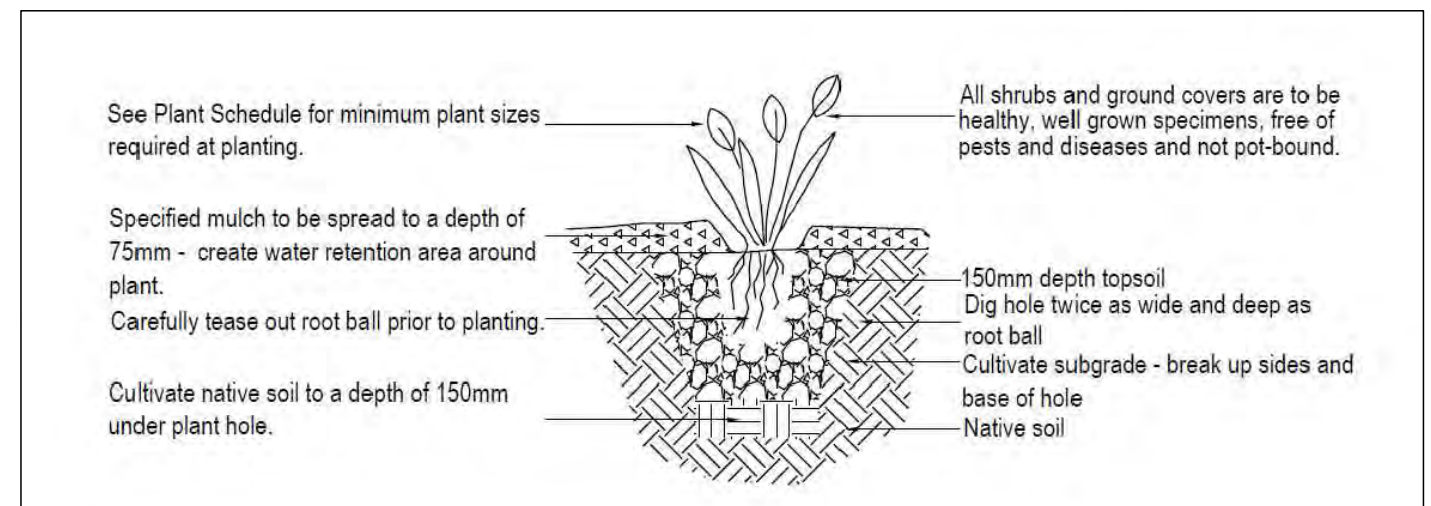
SELECTED PAVED FOOTPATH.

PLANTING NOTES / SPECIFICATIONS

1. SPECIFIED PLANT SPECIES HAVE BEEN SOURCED FROM BENARA NURSERY & DOMUS NURSERY. SPECIES & POT SIZES INDICATED ARE SUBJECT TO AVAILABILITY AT TIME OF PLANTING.
2. ALL PLANTING AREA ARE TO BE PREPARED AND PLANTED IN ACCORDANCE WITH INDUSTRY BEST PRACTICE AND THE INSTRUCTIONS BELOW.
 - A. PREPARATION OF SOIL IN GARDEN BED AREAS
 - I. REMOVE ALL TRACES OF BUILDERS MATERIAL FROM PLANTING AREAS INCLUDING RUBBLE, SAND, MORTAR AND ALL OTHER EXTRANEIOUS MATERIAL.
 - II. REMOVE ALL WEEDS IN GARDEN BEDS AREAS BY SPRAYING WEEDS WITH A STANDARD INDUSTRY HERBICIDE FOLLOWING MANUFACTURERS SPECIFICATIONS AND LEAVE FOR RECOMMENDED TIME.
 - III. REMOVE DEAD PLANT MATTER AFTER TIME SPECIFIED ON HERBICIDE PRODUCT.
 - IV. UNDERTAKE SOIL IMPROVEMENT SUITABLE FOR NATIVE PLANTS.
 - B. MULCH
 - I. AFTER PLANTING, APPLY CHUNKY (20-25mm) PINE BARK WOOD CHIPS TO A MINIMUM DEPTH OF 75mm (MAXIMUM 100mm) TO ALL PLANTING BEDS KEEPING MULCH CLEAR OF PLANT STEMS.

IRRIGATION SPECIFICATIONS

1. ALL GARDEN BEDS TO BE IRRIGATED. WATER SUPPLY TO SCHEME WATER
2. IRRIGATION LAYOUT BY OTHERS.



SHRUB PLANTING DETAILS

NOT TO SCALE

CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
12 June 2025

LANDSCAPING SCHEDULE / NOTES

SCALE 1:200

PROPOSED CARWASH
 No 234 TO 238
 LOTS 27, 300, 301 & 29
 SOUTH TERRACE CNR SOUTH STREET
 SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD. DO NOT SCALE FROM THIS DRAWING. THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE. THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.	NO: _____ DATE: _____ REVISION: _____ DRAWN: _____ CHECK: _____ D 03.06.25 DEVELOPMENT APPLICATION ISSUE MDB SJH
---	---

A3 SHEET

HA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6109
 PO BOX 199
 NEDLANDS WA 6909
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

Date - 03.06.25
 Design - MDB
 Drawn - SJH
 Checked - SJH
 Scale - 1:250
 Job No. - 0857
 Dwg - **LS02**
 Rev - **D**



CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025







**PROPOSED CARWASH
234 – 238 SOUTH TERRACE, CNR SOUTH STREET
SOUTH FREMANTLE**

ENVIRONMENTAL ACOUSTIC ASSESSMENT

JUNE 2024

OUR REFERENCE: 32932-4-24207

DOCUMENT CONTROL PAGE

CITY OF FREMANTLE
These Plans Form Part of

ENVIRONMENTAL ACOUSTIC ASSESSMENT
PROPOSED CARWASH
SOUH FREMANTLE

DAP004/24

5 Dec 2024

Job No: 24207

Document Reference: 32932-4-24207

FOR

HINDLEY & ASSOCIATES

DOCUMENT INFORMATION

Author:	Tim Reynolds	Checked By:	George Watts
Date of Issue:	25 June 2024		

REVISION HISTORY

Revision	Description	Date	Author	Checked
1	Update vacuum unit data	24/09/2024	TR	N/A
2	Updated Drawings	30/09/2024	TR	N/A
3	Planners Comments / Corrections	18/10/2024	TR	N/A

DOCUMENT DISTRIBUTION

Copy No.	Version No.	Destination	Hard Copy	Electronic Copy
1	1	Hindley & Associates Attn: Steven Hindley Email: Steve@hindley.com.au		✓
1	2	Hindley & Associates Attn: Steven Hindley Email: Steve@hindley.com.au		✓
1	3	Hindley & Associates Attn: Steven Hindley Email: Steve@hindley.com.au		✓
1	4	Hindley & Associates Attn: Steven Hindley Email: Steve@hindley.com.au		✓

This report has been prepared in accordance with the scope of services and on the basis of information and documents provided to Herring Storer Acoustics by the client. To the extent that this report relies on data and measurements taken at or under the times and conditions specified within the report and any findings, conclusions or recommendations only apply to those circumstances and no greater reliance should be assumed. The client acknowledges and agrees that the reports or presentations are provided by Herring Storer Acoustics to assist the client to conduct its own independent assessment.

CONTENTS

1.	INTRODUCTION	1
2.	SUMMARY	1
3.	CRITERIA	1
3.1	Environmental Protection (Noise) Regulations 1997	1
4.	MODELLING	3
5.	PREDICTED NOISE EMISSIONS	5
6.	ASSESSMENT	6
6.1	L _{A10} Noise Emissions	6
6.2	L _{max} Noise Emissions	7
7.	CONCLUSION	8

APPENDICIES

A SITE PLAN

1. INTRODUCTION

Herring Storer Acoustics were commissioned to undertake an acoustic assessment of noise emissions associated with the proposed carwash to be located at 234 to 238 (Lots 27,300,301 and 29), South Terrace, corner of South Street, South Fremantle.

This report assesses noise emissions from the premises with regards to compliance with the requirements of the *Environmental Protection (Noise) Regulations 1997*.

For reference, the site plan for the proposed development is attached in Appendix A.

2. SUMMARY

From the analysis undertaken, noise emissions from the proposed 24 hour / 7 days per week car wash has been assessed to comply with the requirements of the *Environmental Protection (Noise) Regulations 1997* at all times with the inclusion of the following noise mitigation :

- The auto wash be installed with doors to limit the noise to that as listed in Table 4.1;
- The number of auto carwashes to be limited to 2 during the night period;
- The number of vacuum units to be limited to 2 during the night period;
- The eastern and part of the southern boundary fence to be 2.1 metres high, as shown on Figure 4.1 in Section 4 - Modelling; and
- A 1.8 metre high barrier to be installed along the western boundary, as shown in Figure 4.1 in Section 4 - Modelling.

3. CRITERIA

3.1 ENVIRONMENTAL PROTECTION (NOISE) REGULATIONS 1997

The allowable noise level for noise sensitive premises in the vicinity of the proposed development is prescribed by the *Environmental Protection (Noise) Regulations 1997*. Regulations 7 and 8 stipulate maximum allowable external noise levels or assigned noise levels that can be received at a premise from another premises. For residential premises, this noise level is determined by the calculation of an influencing factor, which is then added to the base levels shown below. The influencing factor is calculated for the usage of land within two circles, having radii of 100m and 450m from the premises of concern. The base noise levels for residential premises are listed in Table 3.1.

TABLE 3.1 - BASELINE ASSIGNED OUTDOOR NOISE LEVEL

Premises Receiving Noise	Time of Day	Assigned Level (dB)		
		L _{A10}	L _{A1}	L _{Amax}
Noise sensitive premises: highly sensitive area	0700 - 1900 hours Monday to Saturday (Day)	45 + IF	55 + IF	65 + IF
	0900 - 1900 hours Sunday and Public Holidays (Sunday / Public Holiday Day)	40 + IF	50 + IF	65 + IF
	1900 - 2200 hours all days (Evening)	40 + IF	50 + IF	55 + IF
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and Public Holidays (Night)	35 + IF	45 + IF	55 + IF

Note:
 L_{A10} is the noise level exceeded for 10% of the time.
 L_{A1} is the noise level exceeded for 1% of the time.
 L_{Amax} is the maximum noise level.
 IF is the influencing factor.

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
5 Dec 2024

It is a requirement that received noise be free of annoying characteristics (tonality, modulation and impulsiveness), defined below as per Regulation 9.

“impulsiveness” means a variation in the emission of a noise where the difference between L_{Apeak} and $L_{Amax(Slow)}$ is more than 15 dB when determined for a single representative event;

“modulation” means a variation in the emission of noise that –

- (a) is more than 3 dB L_{AFast} or is more than 3 dB L_{AFast} in any one-third octave band;
- (b) is present for more at least 10% of the representative assessment period; and
- (c) is regular, cyclic and audible;

“tonality” means the presence in the noise emission of tonal characteristics where the difference between –

- (a) the A-weighted sound pressure level in any one-third octave band; and
- (b) the arithmetic average of the A-weighted sound pressure levels in the 2 adjacent one-third octave bands,

is greater than 3 dB when the sound pressure levels are determined as $L_{Aeq,T}$ levels where the time period T is greater than 10% of the representative assessment period, or greater than 8 dB at any time when the sound pressure levels are determined as L_{ASlow} levels.

Where the noise emission is not music, if the above characteristics exist and cannot be practicably removed, then any measured level is adjusted according to Table 3.2 below.

TABLE 3.2 - ADJUSTMENTS TO MEASURED LEVELS

Where tonality is present	Where modulation is present	Where impulsiveness is present
+5 dB(A)	+5 dB(A)	+10 dB(A)

Note: These adjustments are cumulative to a maximum of 15 dB.

For this development, the closest residential premises of concern are shown on Figure 3.1 below. Each individual premises has been considered in our assessment, however, to simplify reporting, only the highest noise level for each scenario considered has been reported.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



FIGURE 3.1 – AREA AROUND PROPOSED DEVELOPMENT

Given the commercial premises in the area and that both South Terrace and South Street are secondary roads within the inner circle, the influencing factor at the nearest residential locations have been determined to be +6 dB. Thus, the above, the assigned noise levels are as listed in Table 3.3.

TABLE 3.3 - ASSIGNED OUTDOOR NOISE LEVEL

Premises Receiving Noise	Time of Day	Assigned Level (dB)		
		L _{A 10}	L _{A 1}	L _{A max}
Noise sensitive premises: Highly sensitive area	0700 - 1900 hours Monday to Saturday	51	61	71
	0900 - 1900 hours Sunday and Public Holidays	46	56	71
	1900 - 2200 hours all days	46	56	61
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and Public Holidays	41	51	61

Note: L_{A10} is the noise level exceeded for 10% of the time.
 L_{A1} is the noise level exceeded for 1% of the time.
 L_{Amax} is the maximum noise level.

4. MODELLING

Modelling of the noise propagation from the proposed development was carried out using an environmental noise modelling computer program, "SoundPlan". Calculations were carried out using the EPA worst case weather conditions as stated in the Environmental Protection Authority's "Draft Guideline on Environmental Noise for Prescribed Premises".

Noise modelling was undertaken for the car wash and vacuum bays, assuming the following activities are taking place:

DAY, EVENING and SUNDAY / PUBLIC HOLIDAY DAY PERIOD

- 4 vacuum units in operations and
- 3 carwashes in use.

NIGHT PERIOD

- 2 vacuum units in operations; and
- 2 carwashes in use.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

Additionally, noise modelling was also undertaken for car door closing within the premises.

We understand that the carwash would be open 24 hours per day, 7 days per week. Given the possible usage, noise emissions from the car wash and vacuum bays need to comply with the assigned L_{A10} noise level, with car doors closing complying with the assigned L_{Amax} noise level.

Noise emissions associated with the development, as used in the noise model are listed in Table 4.1.

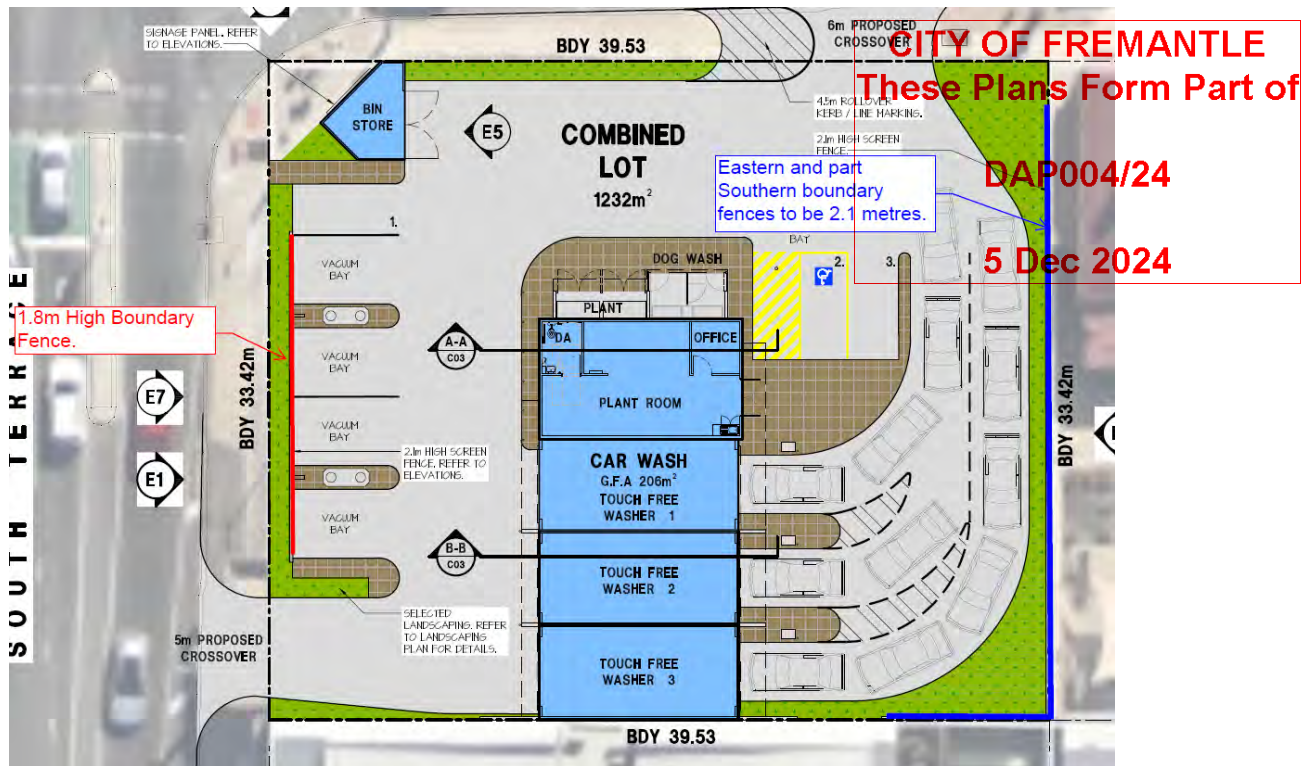
TABLE 4.1 – SUMMARY OF NOISE EMISSIONS FROM OPERATIONS

Item of Equipment	Sound Power Level, (dB(A))
Vacuum Unit	80
Auto carwash (With Doors)	75
Car Door closing	84

It is noted that the residences to the south are divided into western and east premises, with a window to bedrooms, as shown in Figure 4.2. Additionally, noise modelling was undertaken to the balconies of these residences. Noise modelling was undertaken to multiple locations for each of the neighbouring residences, however, to simplify the assessment, on the resultant highest noise levels have been listed.

With regards to noise emissions from the car wash, the critical period for compliance would be during the day on a Sunday or Public holiday, when all equipment could be in use. To comply during these periods, the following noise mitigation is required :

- The auto wash be installed with doors to limit the noise to that as listed in Table 4.1;
- The eastern and part of the southern boundary fence to be 2.1 metre high, as shown on Figure 4.1; and
- A 1.8 metre high barrier to be installed along the western boundary, as shown in Figure 4.1 in Section 4 – Modelling.



5. PREDICTED NOISE EMISSIONS

Calculations were undertaken to all the residences noted on Figure 3.1. The resultant noise levels are listed in Table 5.1.

NOTE: Noise levels were calculated at all adjacent noise sensitive premises, however, for simplicity of reporting, only the highest noise levels have been included below.

TABLE 5.1 – CALCULATED NOISE LEVELS

Item	Residences (dB(A))			
	North	East	South	West
Carwash				
Day Period	40	33	34	40
Night Period	35	31	33	36
Car Door	47	44	44	48

6. ASSESSMENT

The following provided the acoustic assessment for the noise sources requiring compliance, as listed in Table 5.1.

6.1 L_{A10} NOISE EMISSIONS

Noise emissions from the car wash equipment would be steady state and would operate for more than 10% of the time. Hence noise received from the car wash equipment needs to comply with the assigned L_{A10} noise level. Thus, noise received at the neighbouring residences would be assessed under the assigned L_{A10} noise Level:

Given the resultant noise level at the neighbouring residences and likely background noise level in the area, we believe that noise emissions from the car wash equipment are unlikely to be tonal, hence, to be conservative a +5 dB(A) penalty has been applied to the calculated noise level associated with these noise sources. Tables 6.1 and 6.2 list the characteristics that should be included in the assessable noise level for the various times of day.

**TABLE 6.1 – APPLICABLE ADJUSTMENTS AND ASSESSABLE L_{A10} NOISE LEVELS, dB(A)
 CAR WASH – DAY, EVENING and SUNDAY / PUBLIC HOLIDAY DAY PERIODS**

Location	Calculated Noise Level, dB(A)	Applicable Adjustments to Measured Noise Levels, dB(A)			Assessable Noise Level, dB(A)
		Where Noise Emission is NOT music			
		Tonality	Modulation	Impulsiveness	
North	40	+5	-	-	45
East	33	+5	-	-	38
South	34	+5	-	-	39
West	40	+5	-	-	45

**TABLE 6.2 – APPLICABLE ADJUSTMENTS AND ASSESSABLE L_{A10} NOISE LEVELS, dB(A)
 CAR WASH - NIGHT PERIOD**

Location	Calculated Noise Level, dB(A)	Applicable Adjustments to Measured Noise Levels, dB(A)			Assessable Noise Level, dB(A)
		Where Noise Emission is NOT music			
		Tonality	Modulation	Impulsiveness	
North	35	+5	-	-	40
East	31	+5	-	-	36
South	33	+5	-	-	38
West	36	+5	-	-	41

Table 6.3 shows the applicable Assigned Noise Levels, and assessable noise level emissions associated for the scenarios associated with the car wash equipment.

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
5 Dec 2024

**TABLE 6.3 – ASSESSMENT OF L_{A10} NOISE LEVEL EMISSIONS
CAR WASH**

Location	Assessable Noise Level, dB(A)	Applicable Times of Day	Applicable Assigned L _{A10} Noise Level (dB)	Exceedance to Assigned Noise Level (dB)
North	45	Day Period	51	Complies
		Sunday Day Period	46	Complies
		Evenings	46	Complies
	40	Night	41	Complies
East	38	Day Period	51	Complies
		Sunday Day Period	46	Complies
		Evenings	46	Complies
	36	Night	41	Complies
South	39	Day Period	51	Complies
		Sunday Day Period	46	Complies
		Evenings	46	Complies
	38	Night	41	Complies
West	45	Day Period	51	Complies
		Sunday Day Period	46	Complies
		Evenings	46	Complies
	41	Night	41	Complies

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
5 Dec 2024

6.2 L_{MAX} NOISE EMISSIONS

Noise emissions from car doors closing would be assessed under the assigned L_{AMax} noise level.

Based on the definitions of tonality, noise emissions from car doors closing, being an L_{AMax} respectively, being present for less than 10% of the time, would not be considered tonal. However, noise emissions from car doors closing could be impulsive, hence the +10dB penalty has been included in the assessment. Table 6.4 lists the characteristics that should be included in the assessable noise level for car doors closing.

**TABLE 6.4 – APPLICABLE ADJUSTMENTS AND ASSESSABLE L_{A10} NOISE LEVELS, dB(A)
CAR DOOR**

Location	Calculated Noise Level, dB(A)	Applicable Adjustments to Measured Noise Levels, dB(A)			Assessable Noise Level, dB(A)
		Where Noise Emission is NOT music			
		Tonality	Modulation	Impulsiveness	
North	47	-	-	+10	57
East	44	-	-	+10	54
South	44	-	-	+10	54
West	48	-	-	+10	58

Table 6.5 shows the applicable Assigned Noise Levels, and assessable noise level emissions associated for the scenarios associated with the mechanical services.

**TABLE 6.5 – ASSESSMENT OF L_{AMAX} NOISE LEVEL EMISSIONS
 CAR DOOR**

Location	Assessable Noise Level, dB(A)	Applicable Times of Day	Applicable Assigned L _{A10} Noise Level (dB)	Exceedance to Assigned Noise Level (dB)
North	57	Day Period	71	Complies
		Sunday Day Period	71	Complies
		Evenings	61	Complies
		Night	61	Complies
East	54	Day Period	71	Complies
		Sunday Day Period	71	Complies
		Evenings	61	Complies
		Night	61	Complies
South	54	Day Period	71	Complies
		Sunday Day Period	71	Complies
		Evenings	61	Complies
		Night	61	Complies
West	58	Day Period	71	Complies
		Sunday Day Period	71	Complies
		Evenings	61	Complies
		Night	61	Complies

CITY OF FREMANTLE
 These Plans Form Part of
 DAP004/24
 5 Dec 2024

7. CONCLUSION

From the above assessments, the noise received at the neighbouring residences complies with the requirements of the *Environmental Protection (Noise) Regulations 1997* at all times with the noise mitigation incorporated into the design of the development, as outlined below :

- The auto wash be installed with doors to limit the noise to that as listed in Table 4.1;
- The number of auto carwashes to be limited to 2 during the night period;
- The number of vacuum units to be limited to 2 during the night period;
- The eastern and part of the southern boundary fence to be 2.1 metres high, as shown on Figure 4.1 in Section 4 - Modelling; and
- A 1.8 metre high barrier to be installed along the western boundary, as shown in Figure 4.1 in Section 4 - Modelling.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

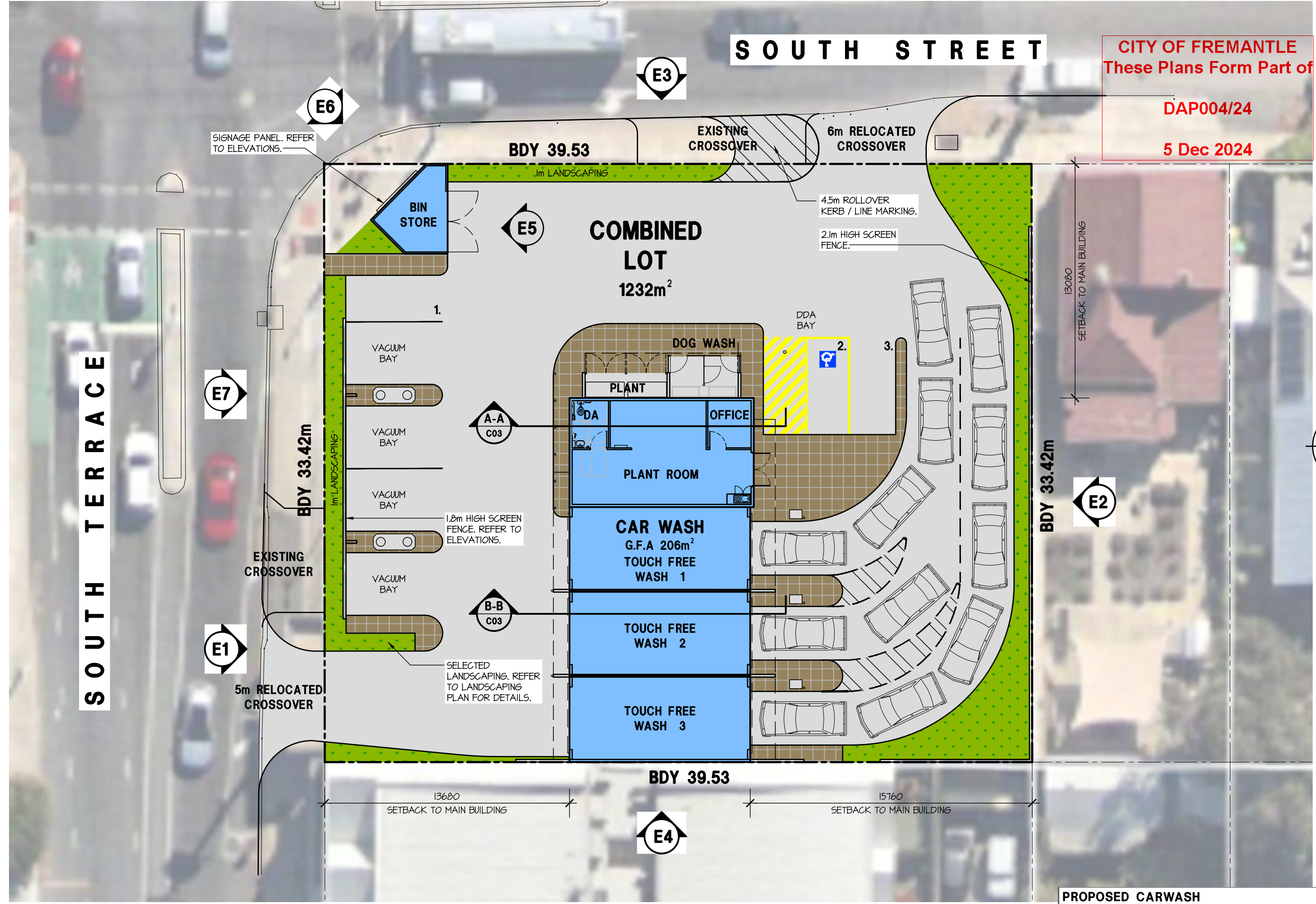
5 Dec 2024

APPENDIX A

SITE PLAN

SOUTH STREET

CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
5 Dec 2024



SOUTH TERRACE



HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6109
 PO BOX 199
 NEDLANDS WA 6109
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

PROPOSED SITE PLAN

SCALE 1:200

PROPOSED CARWASH
 No 234 TO 238
 LOTS 27, 300, 301 & 29
 SOUTH TERRACE CNR SOUTH STREET
 SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

THIS DRAWING IS COPYRIGHT AND MUST NOT BE
 RETAINED, COPIED OR USED WITHOUT THE
 AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
 DO NOT SCALE FROM THIS DRAWING.
 THE CONTRACTOR AND HIS SUBCONTRACTORS ARE
 TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING
 SHOP DRAWINGS OR COMMENCING MANUFACTURE.
 THIS IS A COMPUTER GENERATED DRAWING AND
 SHALL NOT BE ALTERED BY HAND.

NO: DATE: REVISION: DRAWN: CHECK:

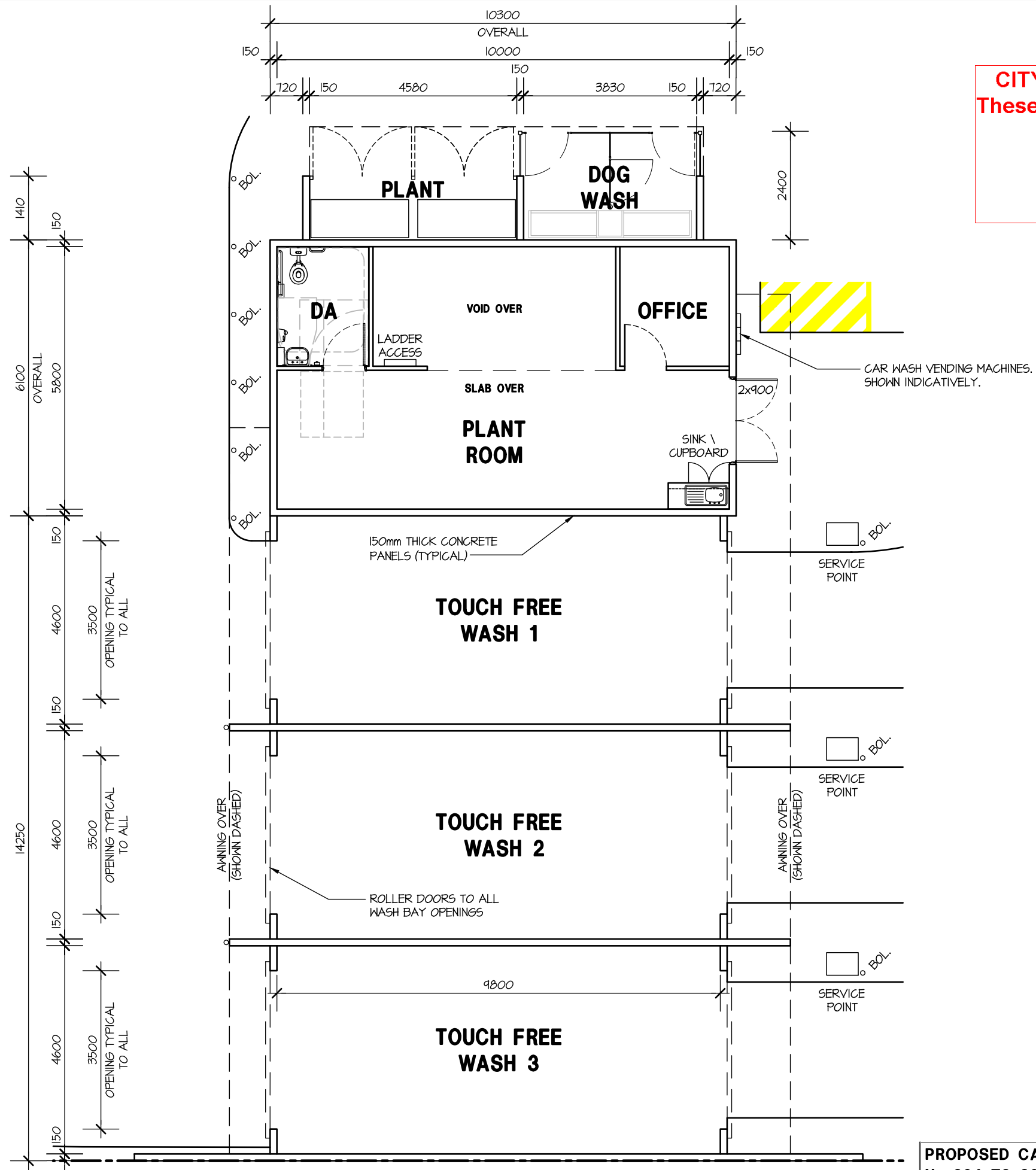
A3 SHEET

Date - 26.09.24
 Design - MDB
 Drawn - MDB
 Checked - SJH
 Scale - 1:250
 Job No. - 0887
 Dwg - **DA01**
 Rev -

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



HA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6109
 PO BOX 199
 NEDLANDS WA 6909
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

FLOOR PLAN
SCALE 1:100

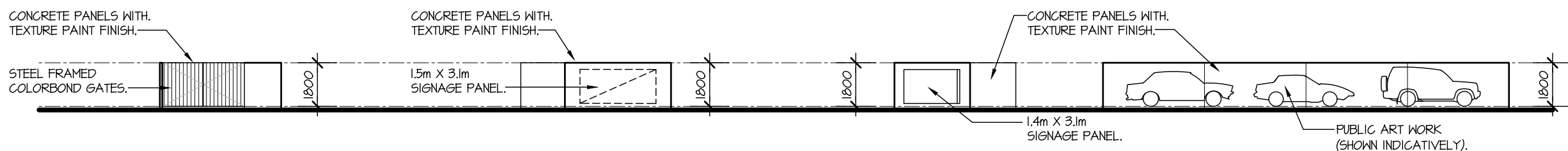
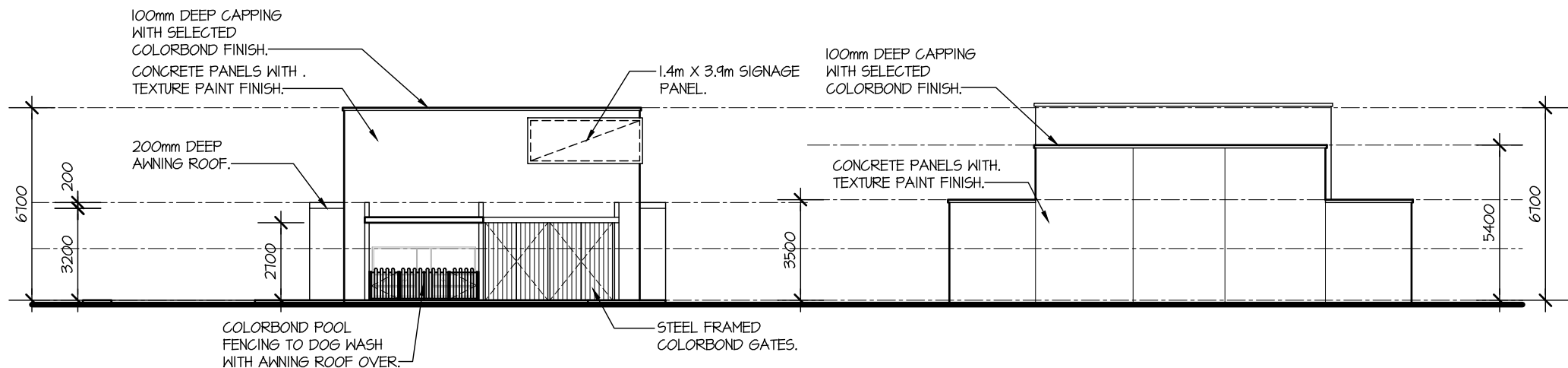
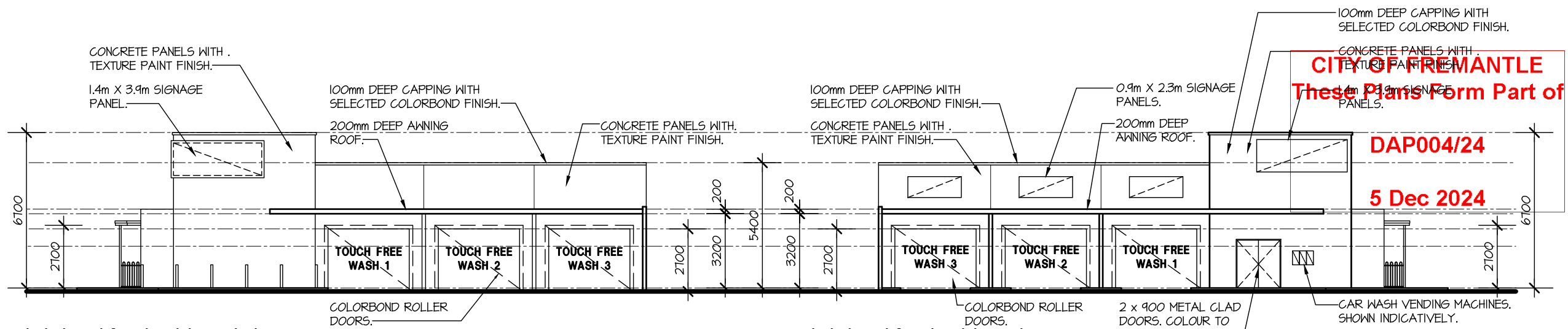
PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
 DO NOT SCALE FROM THIS DRAWING.
 THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE.
 THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.

NO: DATE: REVISION: DRAWN: CHECK:

A3 SHEET

Date - 31.07.24
 Design - MDB
 Drawn - MMB
 Checked - SJH
 Scale - 1:100
 Job No. - 0887
 Dwg - **DA02**
 Rev -



CITY OF FREMANTLE
 These Plans Form Part of

HA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6109
 PO BOX 199
 NEDLANDS WA 6109
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
 DO NOT SCALE FROM THIS DRAWING.
 THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE.
 THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.

NO: DATE: REVISION: DRAWN: CHECK:

A3 SHEET

ELEVATIONS
 SCALE 1:200

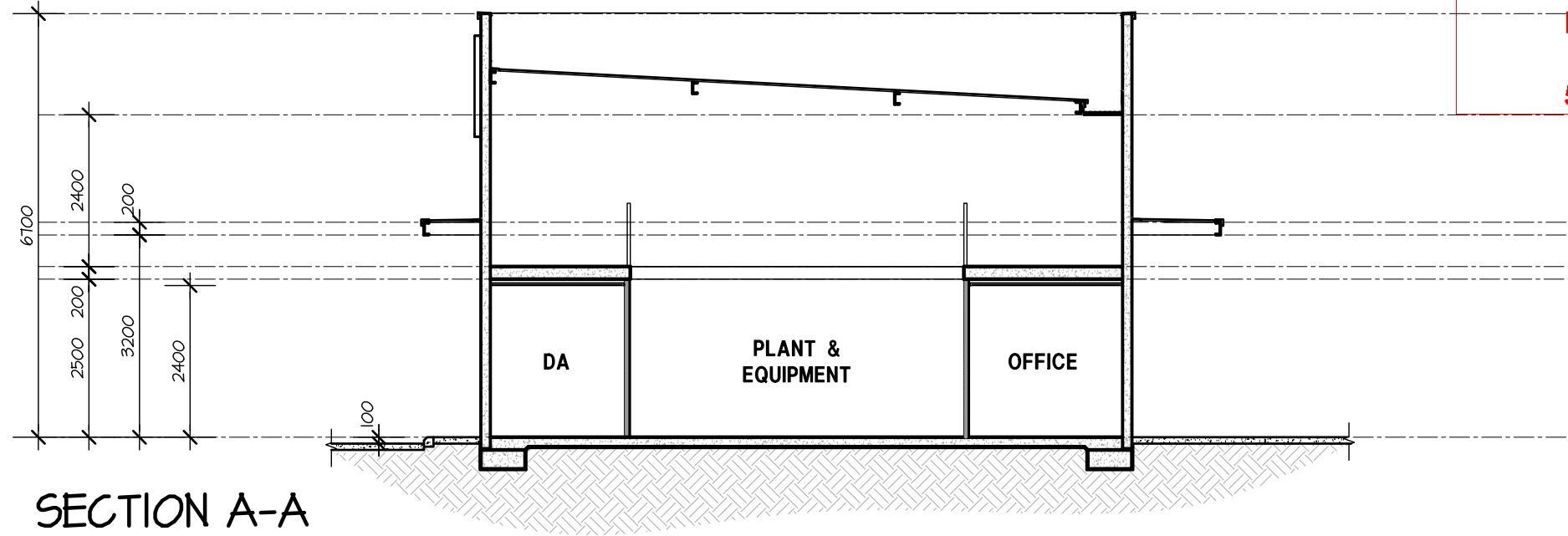
PROPOSED CARWASH
 No 234 TO 238
 LOTS 27, 300, 301 & 29
 SOUTH TERRACE CNR SOUTH STREET
 SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

Date - 31.07.24
 Design - MdB
 Drawn - MdB
 Checked - SJH
 Scale - 1:200
 Job No. - 0987
 Dwg - **DA03**
 Rev -

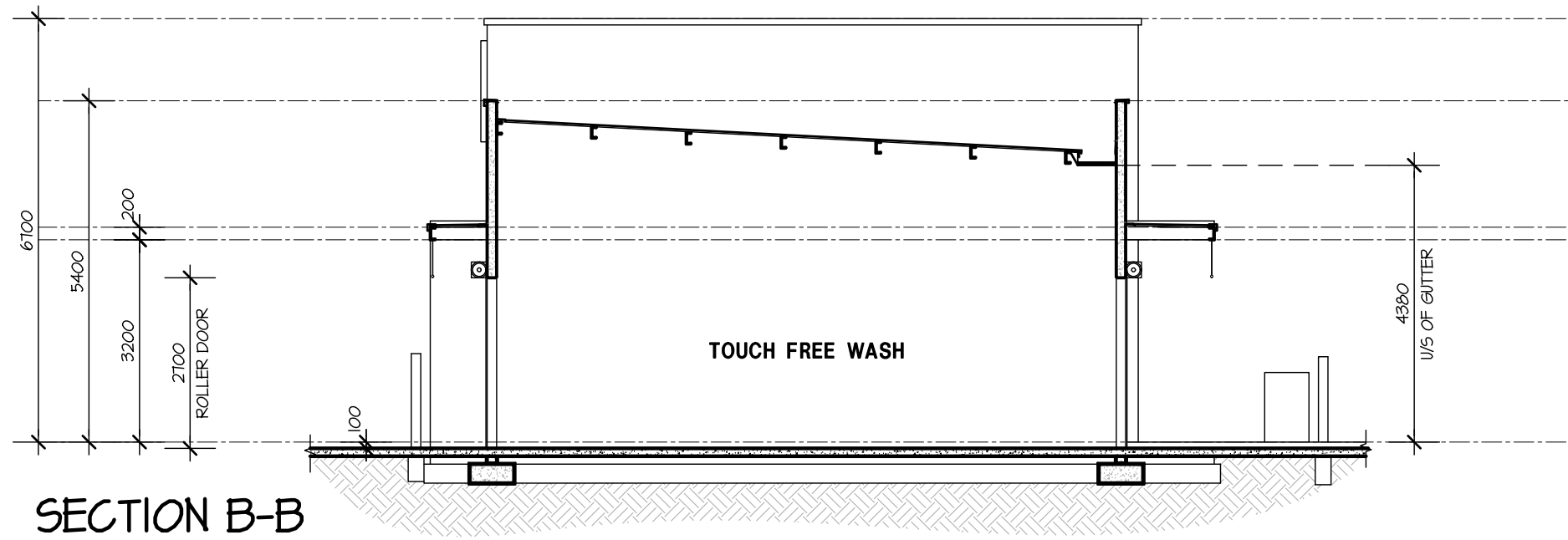
CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



SECTION A-A
SCALE 1:100



SECTION B-B
SCALE 1:100



HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6909
 PO BOX 199
 NEDLANDS WA 6909
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

Date - 31.07.24
 Design - MDB
 Drawn - MDB
 Checked - SJH
 Scale - 1:100
 Job No. 0987
 Dwg **DA04**
 Rev -

THIS DRAWING IS COPYRIGHT AND MUST NOT BE
 RETAINED, COPIED OR USED WITHOUT THE
 AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
 DO NOT SCALE FROM THIS DRAWING.
 THE CONTRACTOR AND HIS SUBCONTRACTORS ARE
 TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING
 SHOP DRAWINGS OR COMMENCING MANUFACTURE.
 THIS IS A COMPUTER GENERATED DRAWING AND
 SHALL NOT BE ALTERED BY HAND.

NO.	DATE	REVISION	DRAWN	CHECK

A3 SHEET
SECTIONS
SCALE 1:100

OPERATIONAL AND NOISE MANAGEMENT PLAN – PROPOSED CAR WASH

LOTS 27, 29, 300, 301 (234, 236, 238) SOUTH TERRACE, SOUTH FREMANTLE

1 INTRODUCTION

This operational and noise management plan (**ONMP**) has been prepared to outline the key operational and noise management practices of the South Fremantle Super Wash to demonstrate how the facility will comply with applicable noise requirements and reduce impact to adjoining properties.

This ONMP captures key details regarding operations, noise management, and waste management in a single, consolidated document.

2 LOCATION

The car wash facility will be located at Lots 27, 29, 300, 301 (234, 236, 238) South Terrace, South Fremantle. Refer to **Figure 1** below.



Figure 1: aerial photograph of subject site

CITY OF FREMANTLE
These Plans Form Part of

OPERATIONAL AND NOISE MANAGEMENT PLAN – PROPOSED CAR WASH

LOTS 27, 29, 300, 301 (234, 236, 238) SOUTH TERRACE, SOUTH FREMANTLE

3 DETAILS OF CAR WASH FACILITY

The car wash facility is comprised of:

- Three automatic wash bays and four vacuum bays
- A fully enclosed plant room with small office and toilet area.
- A dog wash unit with two separate dog wash bays
- A 14.3sqm bin storage enclosure
- Three car parking spaces, including one ACROD space

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
12 June 2025

The car wash facility is partially manned and will operate 7 days per week, opening at 6:30am and closing at 10:30pm.

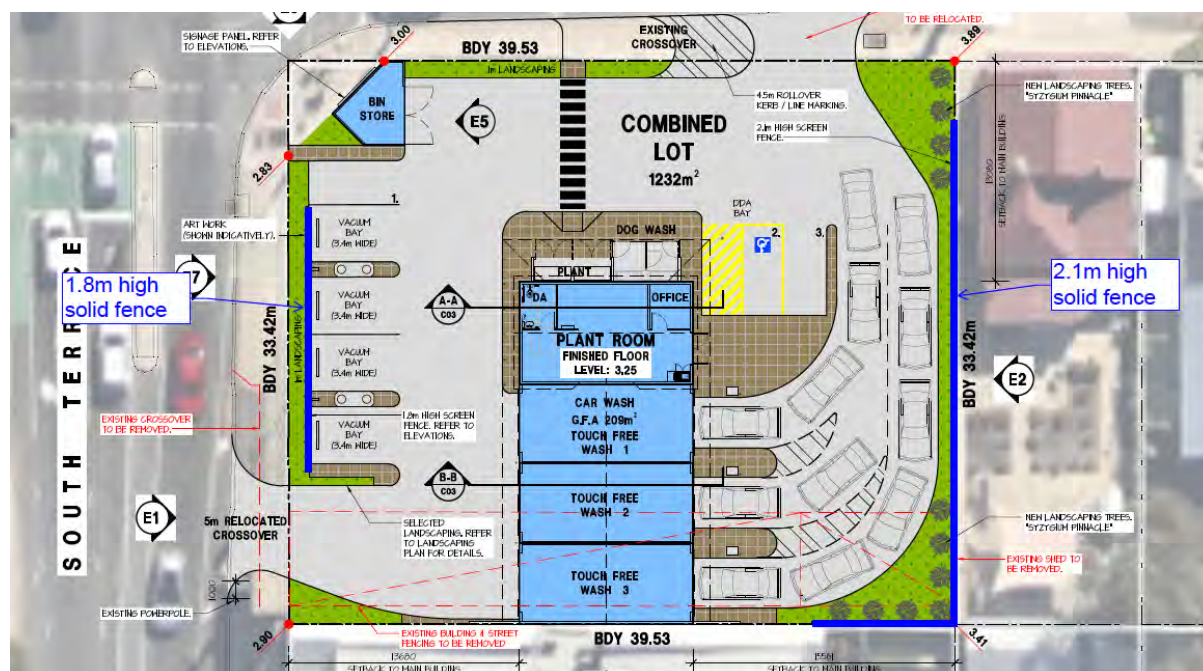
The facility has been designed to incorporate noise attenuation measures ensuring noise is kept to compliant and acceptable levels during the hours of operation.

3 OPERATIONAL AND NOISE MANAGEMENT STRATEGIES

The following sub-sections outline the operational and noise management strategies to ensure noise is kept to compliant and acceptable levels.

3.1 NOISE BARRIERS

In accordance with the environmental acoustic assessment produced by Herring Storer, noise barriers will be installed as per the below image:



It is relevant to note that the above noise barriers formed part of the mitigation strategy for 24 hour operation, which was previously proposed and no longer the case. The barriers therefore afford a high standard of attenuation during all other times.

OPERATIONAL AND NOISE MANAGEMENT PLAN – PROPOSED CAR WASH

LOTS 27, 29, 300, 301 (234, 236, 238) SOUTH TERRACE, SOUTH FREMANTLE

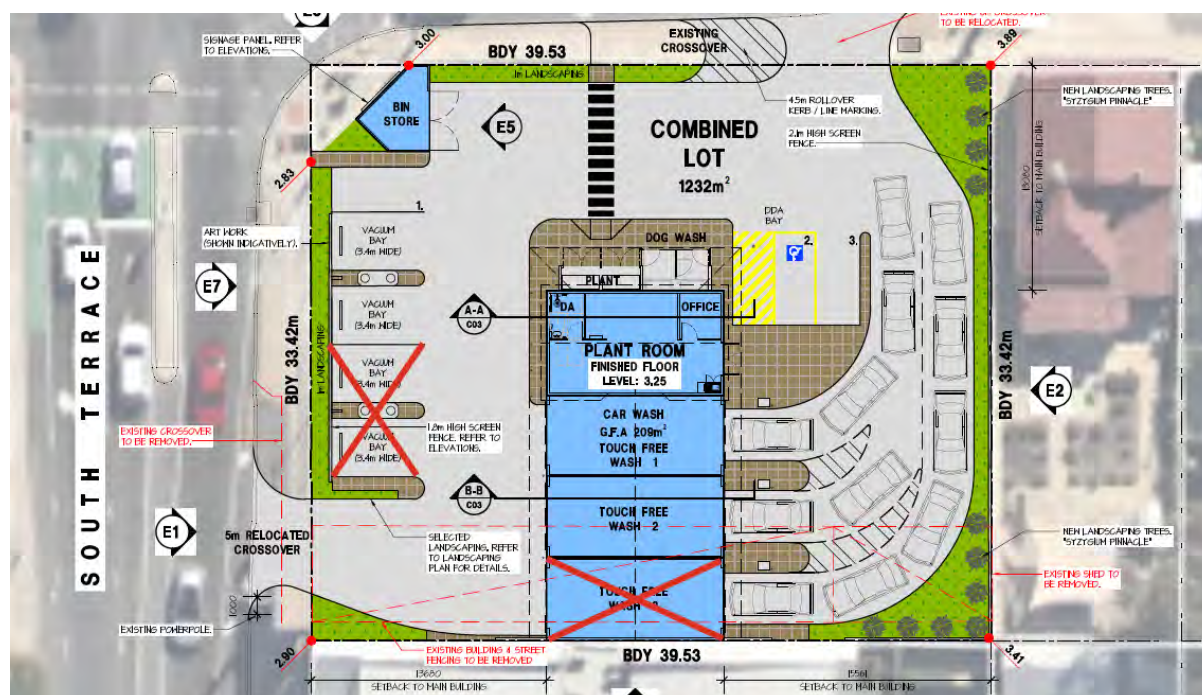
3.2 CAR WASH NOISE MANAGEMENT

In order to limit noise transfer during car wash operation, all auto wash bays will be fitted with self-closing doors which automatically close after a vehicle has entered the wash bay. The doors will remain closed while washing is underway, and re-open automatically once washing is completed. This ensures noise associated with the washing process is effectively contained within the car wash building.

For operations that fall within the hours of 10pm-7am (ie 10pm-10:30pm and 6:30am-7am), being the 'night period' under the Noise Regulations, the following will occur:

- Only two of the three auto wash bays will be activated for use in accordance with the mitigation measures required by the environmental noise assessment. The wash bay which is not operational will be appropriately sign-posted.
- Only two of the four vacuum bays will be activated for use in accordance with the mitigation measures required by the environmental noise assessment. The vacuum bays which are not operational will be appropriately sign-posted.

The autowash and vacuum bays to be deactivated are marked with a red cross in below image, affording the greatest reduction in noise transfer to nearest neighbours:



The deactivation of wash bays and vacuum bays is automated (ie does not require a staff member to manually execute) and the units will be not be capable of operation during the hours prescribed above.

As part of good practice, signage will be posted onsite advising patrons to be courteous during the dark hours and keep noise to a minimum while onsite.

A comprehensive CCTV system will be installed and maintained to ensure full surveillance of the site and operational assets at all times.

CITY OF FREMANTLE
These Plans Form Part
of

DAP004/24

12 June 2025

OPERATIONAL AND NOISE MANAGEMENT PLAN – PROPOSED CAR WASH

LOTS 27, 29, 300, 301 (234, 236, 238) SOUTH TERRACE, SOUTH FREMANTLE

3.3 CAR WASH SERVICING

The car wash facility will be partially manned to ensure efficient operation, and for routine checks and maintenance to be carried out. An attendant will generally be onsite during the following periods:

Monday-Friday: 9am-3pm

Weekends: 9am-5pm

Responsibilities of the car wash attendant whilst onsite include:

- Ensuring adherence to the acoustic mitigation requirements, and assisting customers to enable efficient use of the facility.
- Cleaning the wash bays and vacuum bays.
- Garden and landscape maintenance.
- Emptying coin machines.
- Collection of any rubbish onsite and transferring waste from bins at the vacuum bays to the bin store.
- Checking all plant and equipment.
- General administrative tasks within the internal office space.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025

3.4 WASTE MANAGEMENT

The car wash facility would generate a small amount of waste, mainly via the bins at the vacuum bays. The bins are emptied by the car wash operator on a regular basis, who will transfer the waste to the receptacles within the bin enclosure.

A private contractor would collect the waste onsite generally once per week, during normal business hours. Due to the relatively small amount of waste volume, waste collection is a quick process.

Wastewater is addressed as part of the facility's best-practice infrastructure system to achieve strong environmental standards, which involves an oil/water separator and discharge of all wastewater to sewer in accordance with regulatory requirements. The facility will be required to obtain a trade waste permit from the Water Corporation prior to operation. A summary of the waste water treatment process is described below:

1. Each automatic wash bay contains an underslab 1,000 litre concrete collection pit which is the primary collection point for all the water and wash residues from the automatic wash process. Each pit is covered with a metal grate under which sits a steel mesh basket which collects any soil residue from the wash process. These mesh baskets are emptied daily by the car wash staff.
2. South Fremantle Super Wash will have 3 automatic wash bays (3 x 1,000 litre underground collection pits). These pits are joined in series and the combined wash residue falls by gravity via a 100 mm HDPE pipe into a 5,000 litre dirty water collection tank.

OPERATIONAL AND NOISE MANAGEMENT PLAN – PROPOSED CAR WASH

LOTS 27, 29, 300, 301 (234, 236, 238) SOUTH TERRACE, SOUTH FREMANTLE

3. A pump installed in the dirty water collection tank pumps the wash residue into an oil and water separator (**OWS**) for treatment. The OWS separates oil from the wash residue and the recovered oil is collected in a small tank which is periodically collected by a licenced contractor for disposal. The treated wash residue (which is now free of oil) is then discharged to the Water Corporation sewer.

4 PROCEDURE TO MANAGE SITE OPERATIONS

The facility will adhere to the following procedures to manage site operations in accordance with the matters detailed in this ONMP:

- A copy of the ONMP provided to the operations manager of the car wash facility at commencement of employment.
- Ongoing monitoring of the operation of the facility and adherence to the ONMP at all times by the car wash manager whilst onsite.
- The contact details of the operations manager to be available onsite, should patrons identify any matters requiring resolution.

5 CONTACT DETAILS AND COMPLAINTS PROCEDURE

The business can be contacted as follows:

Telephone: TBA
Email: TBA

The operator's contact details (phone and email) will also be available on their website (TBA).

Complaints relating to the operation of the facility will be addressed in a prompt manner.

Any complaints should be directed in the first instance to the operations manager. The operations manager will evaluate and respond to complaints in a swift and effective manner.

A voicemail service and/or email will facilitate the receipt of any messages or communications if the operator is contacted outside of business hours.

The operations manager will respond to any complaints received via voicemail or email within 24 hours.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025



Engineering a better future for over 20 years!

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

Proposed Automated Car Wash

Cnr South Street & South Terrace,
South Fremantle

Transport Impact Statement

PREPARED FOR:
Glowmark Nominees Pty Ltd

October 2024

Document history and status

Author	Revision	Approved by	Date approved	Revision type
A Navarro	r01	B Bordbar	31/05/2024	Final
A Navarro	r01a	B Bordbar	05/06/2024	Revised Final
R Bajwa	r01b	B Bordbar	08/10/2024	2 nd Revision

File name: t24.139.rb.r01b

Author: Roger Bajwa

Project manager: Behnam Bordbar

Client: Glowmark Nominees Pty Ltd

Project: Cnr South Street & South Terrace, South Fremantle

Document revision: r01b

Project number: t24.139

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
5 Dec 2024

2024 Copyright in all drawings, reports, specifications, calculations and other documents provided by the Consultant in connection with the Project shall remain the property of the Consultant.

The Client alone shall have a license to use the documents referred to above for the purpose of completing the Project, but the Client shall not use, or make copies of, such documents in connection with any work not included in the Project, unless written approval is obtained from the Consultant or otherwise agreed through a separate contract.

TABLE OF CONTENTS

1	INTRODUCTION.....	1
2	DEVELOPMENT PROPOSAL.....	3
3	VEHICLE ACCESS AND PARKING.....	4
3.1	ACCESS.....	4
3.2	PARKING	4
4	PROVISION FOR SERVICE VEHICLES	5
5	HOURS OF OPERATION	6
6	DAILY TRAFFIC VOLUMES AND VEHICLE TYPES	7
6.1	EXISTING DEVELOPMENT TRIP GENERATION.....	7
6.2	PROPOSED DEVELOPMENT TRIP GENERATION.....	7
6.3	TRAFFIC FLOW	8
6.4	IMPACT ON SURROUNDING ROADS.....	9
7	TRAFFIC MANAGEMENT ON THE FRONTAGE STREETS.....	10
8	PUBLIC TRANSPORT ACCESS.....	13
9	PEDESTRIAN ACCESS.....	17
10	CYCLE ACCESS	18
11	SITE SPECIFIC ISSUES	19
11.1	QUEUE ANALYSIS	19
11.1.1	PROPOSED AUTOMATED CAR WASH ROAD NETWORK PEAK HOUR TRIP GENERATION.....	19
11.1.2	CAR WASH QUEUE ANALYSIS.....	19
12	SAFETY ISSUES.....	22
13	CONCLUSIONS.....	23

APPENDIX A: PROPOSED DEVELOPMENT PLAN

APPENDIX B: TURN PATH ANALYSIS

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



REPORT FIGURES

Figure 1: Location of the subject site	1
Figure 2: Crossover arrangement for the proposed development.....	4
Figure 3: Estimated traffic movements for the proposed development.....	8
Figure 4: Eastbound view along South Street	10
Figure 5: Westbound view along South Street.....	11
Figure 6: Southbound view of South Terrace.....	12
Figure 7: Northbound view of South Terrace	12
Figure 8: Bus Services 511, 512 & 513 (Transperth Maps)	14
Figure 9: Bus Service 520 (Transperth Maps)	14
Figure 10: Bus Services 530 & 531 (Transperth Maps).....	15
Figure 11: Bus Service 532 (Transperth Maps)	15
Figure 12: Bus Service 548 (Transperth Maps)	16
Figure 13: Bus Service 549 (Transperth Maps)	16
Figure 14: Extract from Perth Bicycle Network (Department of Transport).....	18
Figure 15: Peak hour queueing analysis	20

REPORT TABLES

Table 1: Bus services available (Transperth).....	13
---	----

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



1 Introduction

This Transport Impact Statement (TIS) has been prepared by Transcore on behalf of Glowmark Nominees Pty Ltd with regards to a proposed automated car wash to be located at Lots 27, 300, 301 & 29 (234 – 238) South Terrace, South Fremantle in the City of Fremantle.

The subject site is located at the southeastern corner of the signalised intersection of South Street/South Terrace. As shown in **Figure 1**, the subject site is bound by South Street to the north, existing residential area to the east, existing residential/commercial development to the south, and South Terrace to the west.



Figure 1: Location of the subject site

The Transport Impact Assessment Guidelines (WAPC, Vol 4 – Individual Developments, August 2016) states: “A Transport Impact Statement is required for those developments that would be likely to generate moderate volumes of traffic¹ and

¹ Between 10 and 100 vehicular trips per hour

therefore would have a moderate overall impact on the surrounding land uses and transport networks". **Section 6.2** of Transcore's report provides details of the estimated trip generation for the proposed development. Accordingly, as the total peak hour vehicular trips are estimated to be less than 100 trips, a Transport Impact Statement is deemed appropriate for this development.

Key issues that will be addressed in this report include the traffic generation and distribution of the proposed development, access and egress movement patterns and parking supply.

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
5 Dec 2024

2 Development Proposal

An automated car wash is proposed to be located at Lot 27, 300, 301301 & 29 (234 – 238) South Terrace, South Fremantle in the City of Fremantle.

The proposed automated car wash comprises 3 car wash tunnels with 4 vacuum bays. The proposed site plan shows the automated car wash will entail a one-way clockwise circulation system with entry to be located adjacent to the relocated existing crossover on South Street. As part of the proposed car wash development, the refuse area is proposed to be located at the northwestern corner of the site.

The proposed car wash development plan is included for reference in **Appendix A**.

Turn path analysis undertaken for a B99 vehicle for the automated car wash facility confirms satisfactory circulation.

Turn path analysis is presented in **Appendix B**.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

3 Vehicle Access and Parking

DAP004/24

5 Dec 2024

3.1 Access

The proposed development will be served by the relocated existing crossovers on South Street and South Terrace. The crossover arrangement is shown in **Figure 2**.

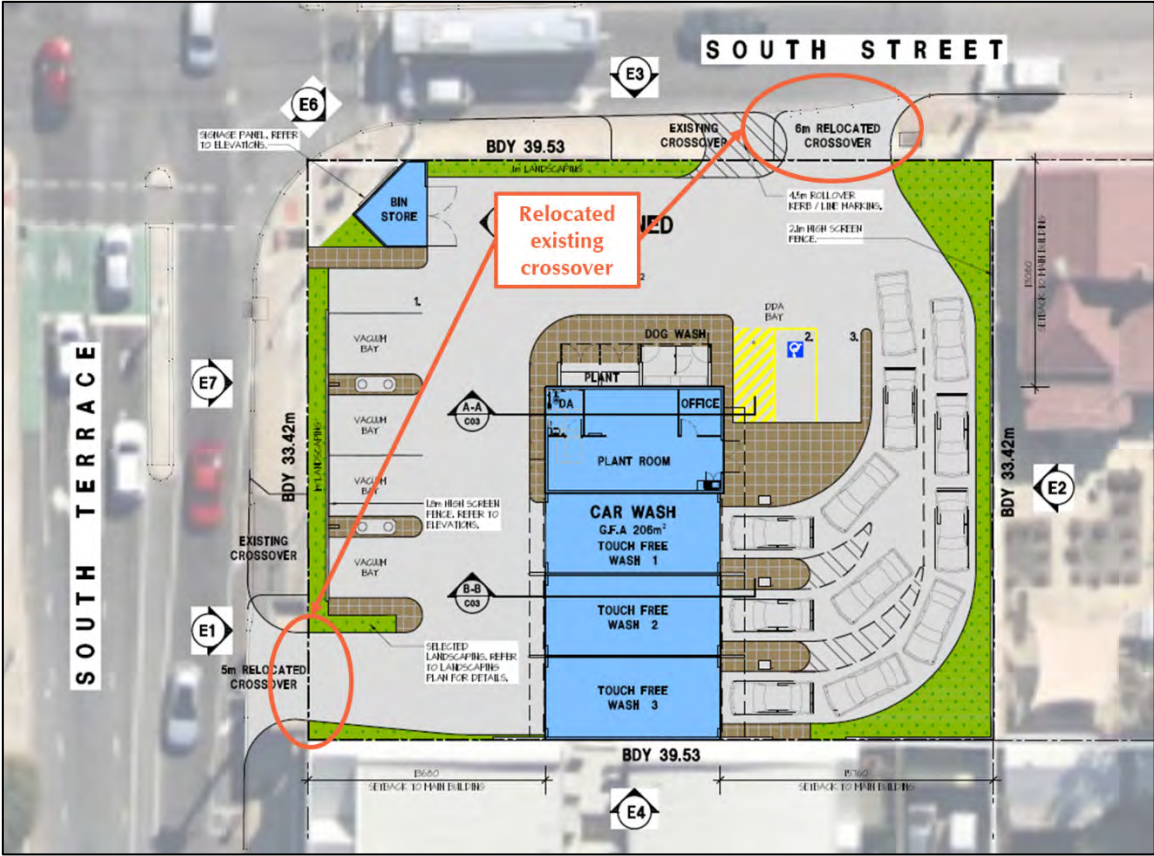


Figure 2: Crossover arrangement for the proposed development

3.2 Parking

The proposed car wash development provides four vacuum bays on the western side of the site and three 90-degree parking bays, where one is next to the vacuum bays and two are located north of the car wash tunnels and next to the plant room.



4 Provision for Service Vehicles

Based on the advice provided to Transcore, the waste collection for the proposed development will be undertaken by a private contractor. The anticipated waste collection truck to service the site is an 11m service vehicle. The bin storage area is to be located at the northwestern corner of the site as shown in the development plan in **Appendix A**.

The waste collection truck will enter the site via South Street crossover in forward gear, park in front of the bin storage area (front loader), reverse onto the car wash lanes, and exit the site via the same crossover in forward gear.

It is proposed that servicing will be conducted outside of peak operation of the proposed development to allow the service vehicle use of car wash lanes for manoeuvring.

For a robust assessment, a 12.5m HRV template is used for the service vehicle turn path analysis. Turn path analysis confirms satisfactory access/egress and circulation to/from the site.

Turn path analysis is presented in **Appendix B**.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

5 Hours of Operation

Based on the advice provided to Transcore, the automated car wash is anticipated to operate 24 hours a day, 7 days a week.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



6 Daily Traffic Volumes and Vehicle Types

6.1 Existing Development Trip Generation

For a robust assessment, it is assumed that the subject site presently does not generate any traffic.

CITY OF FREMANTLE
These Plans Form Part
of
DAP004/24
5 Dec 2024

6.2 Proposed Development Trip Generation

The traffic volumes likely to be generated by the automated car wash have been estimated based on Transcore's experience, information available to Transcore and observations of similar businesses within the Perth Metro area.

Based on the type of machine for the car wash advised by the operator, the adopted trip rate is based on similar businesses that uses the same equipment. The peak patronage to carwashes occurs during the weekends which doesn't coincide with the peak hour of the road network. However, for a robust assessment, it is assumed that the road network peak trip generation for the proposed car wash would be 50% of the peak hour trip generation during a weekend. Similarly, the typical weekday visitation to the car wash is assumed to be about 50% of the weekends. However, as the proposed car wash will be open 24/7, the weekday patronage is assumed to be 12 times the peak hour patronage.

Automated Car Wash – Per Wash Bay:

- Weekday, daily: 72vpd per wash bay;
- Weekday, AM peak hour: 6vph per wash bay; and,
- Weekday, PM peak hour: 6vph per wash bay.

Accordingly, it is estimated that the traffic generations for the automated car wash facility are:

- Weekday, daily: $72 \times 3 = 216$ vpd;
- Weekday, AM peak hour: $6 \times 3 = 18$ vph; and,
- Weekday, PM peak hour: $6 \times 3 = 18$ vph.

Therefore, the proposed development would generate approximately **216** vehicular trips per day with approximately **18** trips each during the weekday AM and PM peak hours. These include the inbound and outbound trips.

The directional split of inbound and outbound trips for the proposed development is assumed to be about 50/50 for inbound/outbound trips during peak hours. The directional split of the peak hour trips are as follows:

- Weekday, AM peak hour: 18vph (9 in / 9 out); and,
- Weekday, PM peak hour: 18vph (9 in / 9 out).

6.3 Traffic Flow

Considering all access/egress to/from the site will be available via the crossovers on South Street and South Terrace, it is concluded that all of the estimated development generated traffic would arrive to the site via South Street and depart from the site via South Terrace and then disperse throughout the local road network.

Based on the approved traffic distribution for the approved development, the traffic distribution adopted for the proposed car wash is as follows:

- 25% from/to north of South Street;
- 25% from/to south of South Street;
- 25% from/to west of South Terrace; and,
- 25% from/to east of South Terrace.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

Figure 3 illustrates trip generation and distribution over the local road network for the proposed development.

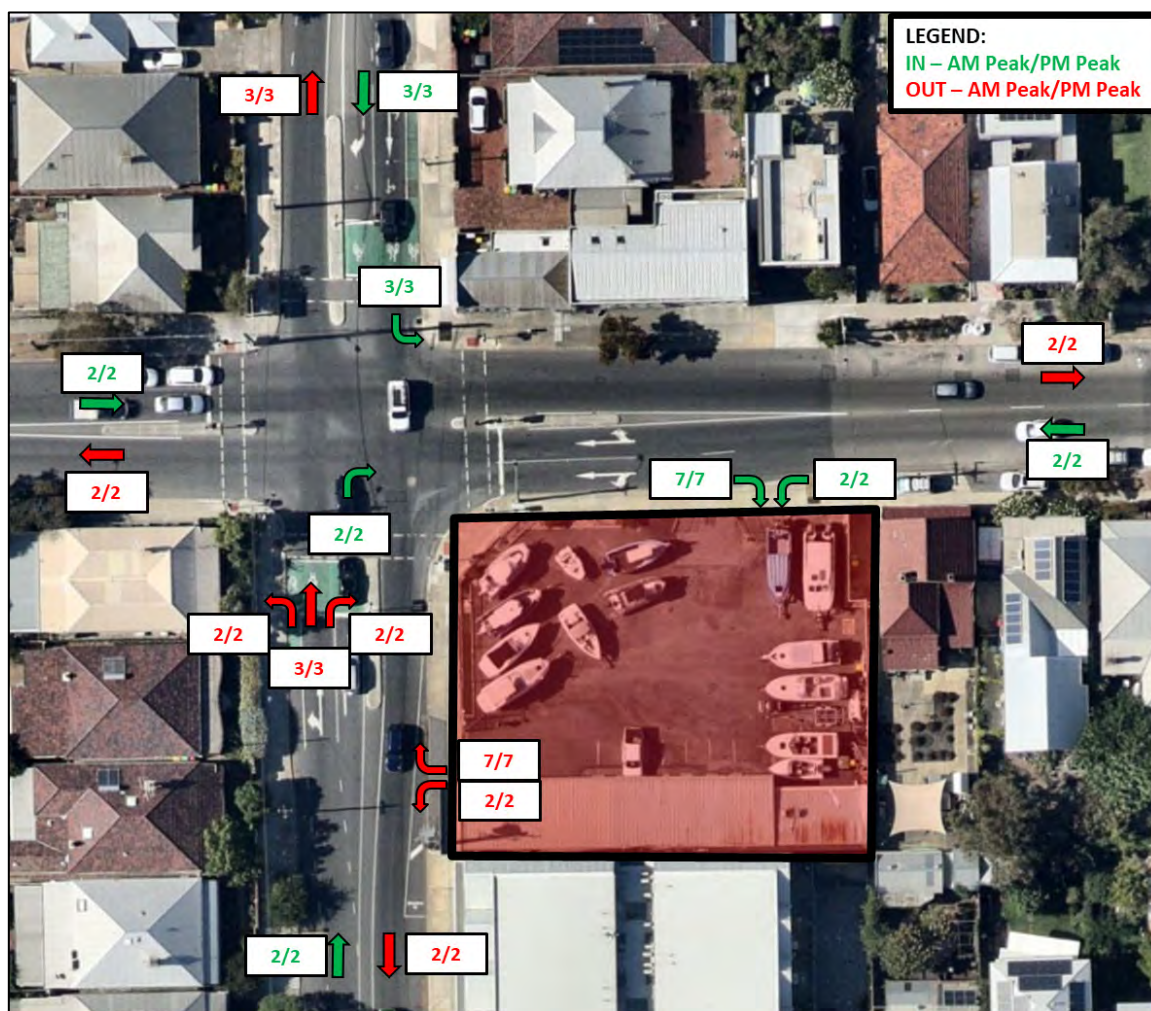


Figure 3: Estimated traffic movements for the proposed development

6.4 Impact on Surrounding Roads

The WAPC *Transport Impact Assessment Guidelines* (2016) provides the following guidance on the assessment of traffic impacts:

“As a general guide, an increase in traffic of less than 10 percent of capacity would not normally be likely to have a material impact on any particular section of road, but increases over 10 percent may. All sections of road with an increase greater than 10 percent of capacity should therefore be included in the analysis. For ease of assessment, an increase of 100 vehicles per hour for any lane can be considered as equating to around 10 percent of capacity. Therefore, any section of road where development traffic would increase flows by more than 100 vehicles per hour for any lane should be included in the analysis.”

The proposed development will not increase traffic flows on any roads adjacent to the site by the quoted WAPC threshold of +100vph to warrant further analysis. Therefore, the impact of development traffic on the surrounding road network will not be significant.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

7 Traffic Management on the Frontage Streets

South Street, north of the subject site, is constructed as a 12.3m wide single carriageway two-lane road. On-street parking spaces and pedestrian paths are provided on both sides of the road. Pedestrian facilities, such as drop kerbs and median refuge, is provided at the signalised intersection.

South Street is classified as a *Distributor B* road in the *Main Roads WA Metropolitan Functional Road Hierarchy*. It operates under the default built-up area speed limit of 50km/h. South Street forms a four-way signalised intersection with South Terrace. Refer to **Figure 4** and **Figure 5** for more details.

According to the traffic counts obtained from *Main Roads WA trafficmap*, South Street, west of South Terrace, carried 5,339 vehicles per day (vpd) in a typical weekday on 2020/21.



Figure 4: Eastbound view along South Street

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



Figure 5: Westbound view along South Street

South Terrace, west of the subject site, is constructed as a 12.3m wide single carriageway two-lane road. On-street parking spaces and pedestrian paths are provided on both sides of the road. Pedestrian facilities, such as drop kerbs and median refuge, are provided at the signalised intersection. Bike lane is provided on the west side of the road, south of the intersection and east side of the road, north of the intersection.

South Terrace is classified as a *Local Distributor* road in the *Main Roads WA Metropolitan Functional Road Hierarchy*. It operates under the posted speed limit of 40km/h. South Terrace forms a four-way signalised intersection with South Street. Refer to **Figure 6** and Figure 7 for more details.

According to the traffic counts obtained from *Main Roads WA trafficmap*, South Terrace, south of South Street, carried 8,931 vehicles per day (vpd) in a typical weekday on 2021/22.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



Figure 6: Southbound view of South Terrace



Figure 7: Northbound view of South Terrace

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
5 Dec 2024

8 Public Transport Access

Nearby public transport services are shown in **Figure 8** and **Figure 13**. The subject site has access to the bus services tabulated in **Table 1**. The nearest bus stops are located on both sides of South Street within close proximity of the site.

Table 1: Bus services available (Transperth)

Bus Services	Days of Service	Service
511	Monday to Sunday (inc Public Holidays)	Murdoch Stn – Fremantle Stn via Somerville Bvd & McCombe Av
512	Monday to Sunday (inc Public Holidays)	Murdoch Stn – Fremantle Stn via Coolbellup Shop Ctr, Phoenix Park Shop Ctr & Cockburn Rd
513	Monday to Sunday (inc Public Holidays)	Murdoch Stn – Fremantle Stn via South St & Coolbellup Shop Ctr
520	Monday to Sunday (inc Public Holidays)	Cockburn Central Stn – Fremantle via South Lake
530	Monday to Sunday (inc Public Holidays)	Cockburn Central Stn – Fremantle Stn via Yangebup Rd
531	Monday to Sunday (inc Public Holidays)	Cockburn Central Stn – Fremantle Stn via Marvell Av & The Grange
532	Monday to Sunday (inc Public Holidays)	Cockburn Central Stn – Fremantle Stn via Beeliar Dr
548	Monday to Sunday (inc Public Holidays)	Fremantle Stn – Rockingham Stn via Cockburn Rd & Patterson Rd
549	Monday to Sunday (inc Public Holidays)	Fremantle Stn – Rockingham Stn via Rockingham Rd & Kwinana Town Centre

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
5 Dec 2024

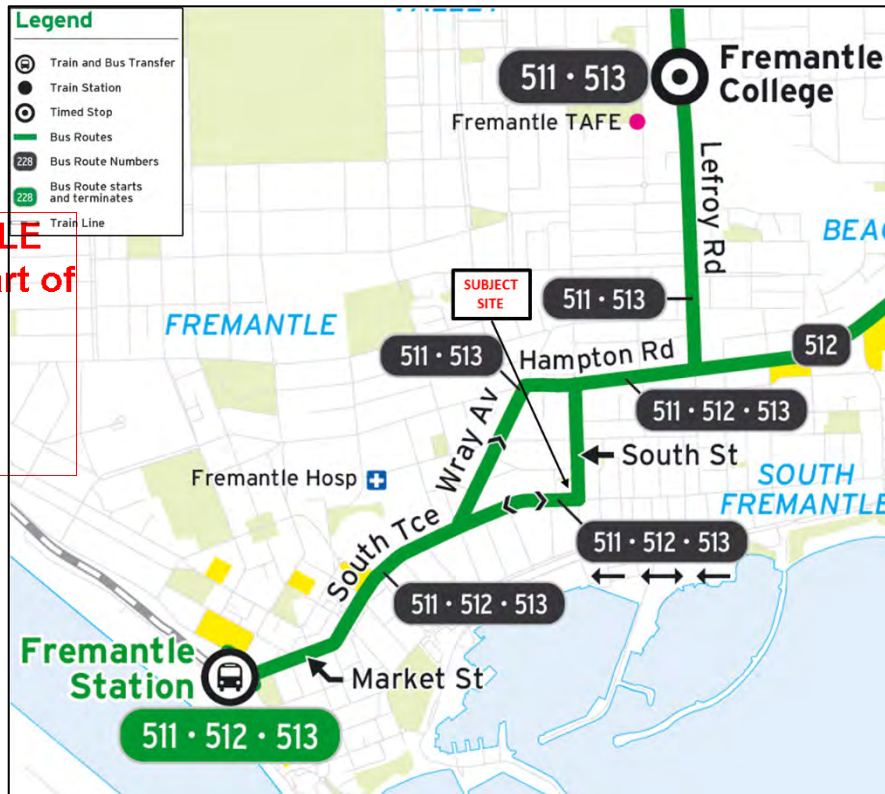


Figure 8: Bus Services 511, 512 & 513 (Transperth Maps)

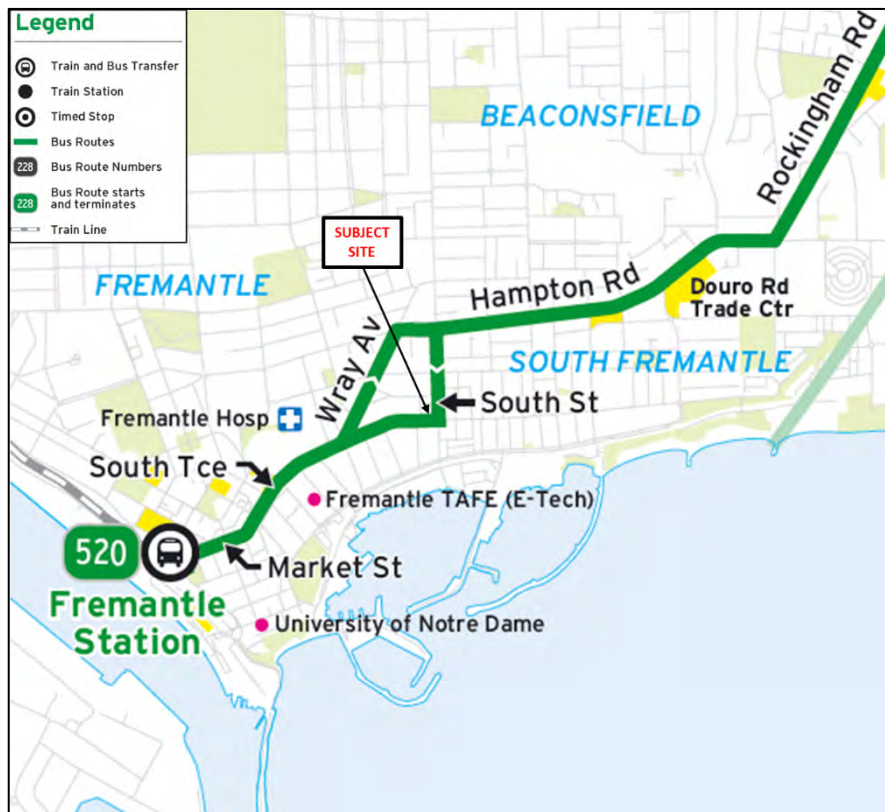


Figure 9: Bus Service 520 (Transperth Maps)



CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
5 Dec 2024

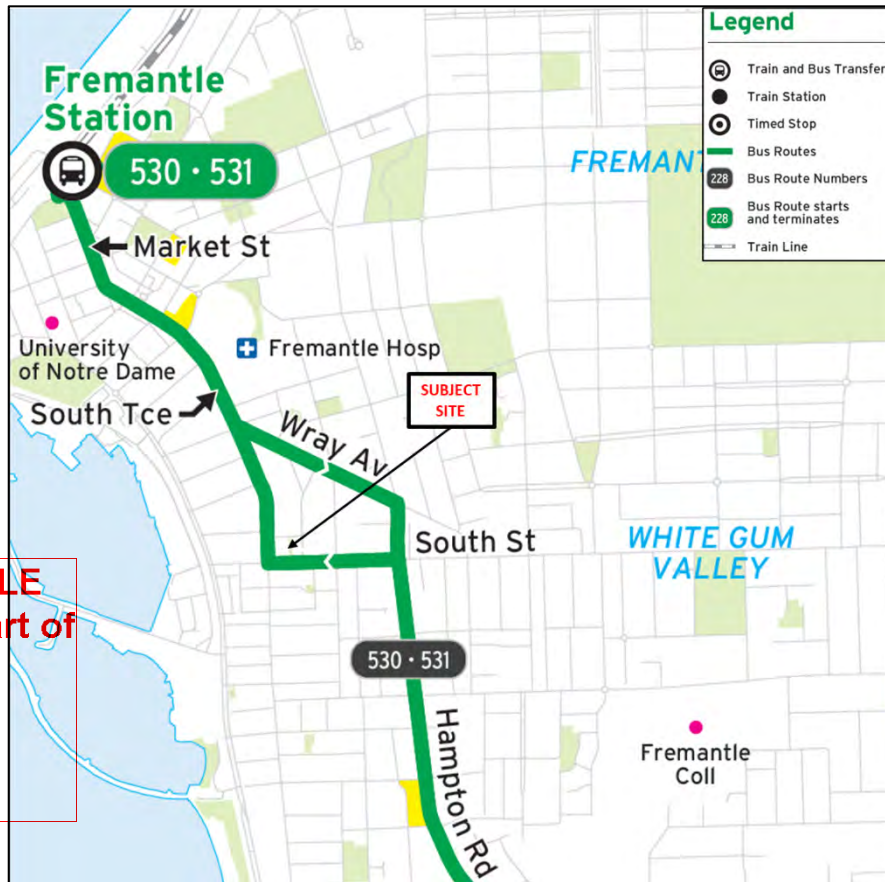


Figure 10: Bus Services 530 & 531 (Transperth Maps)

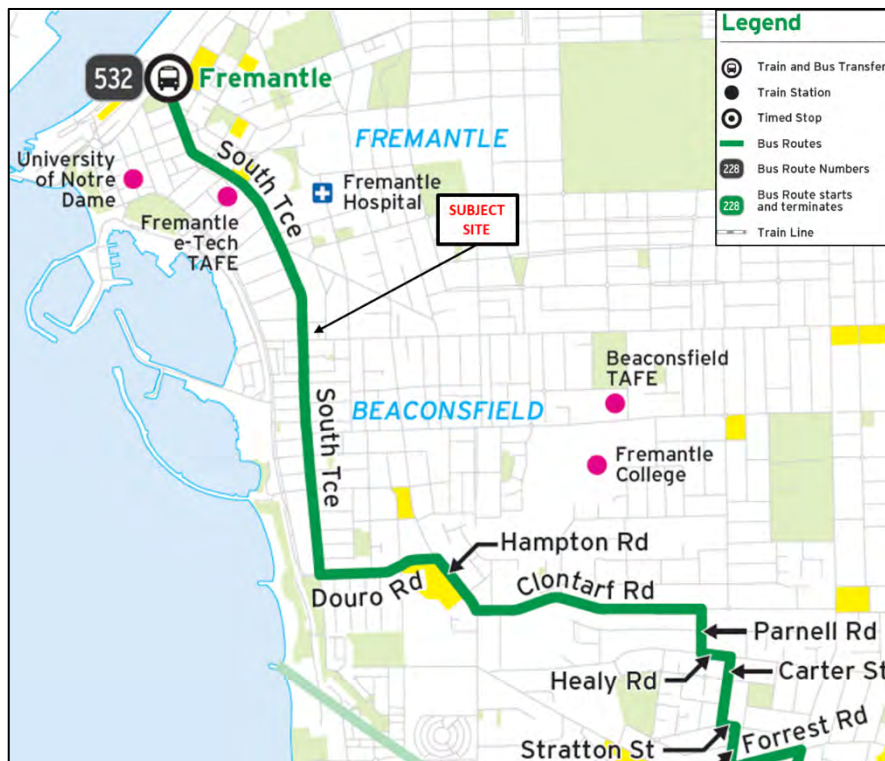
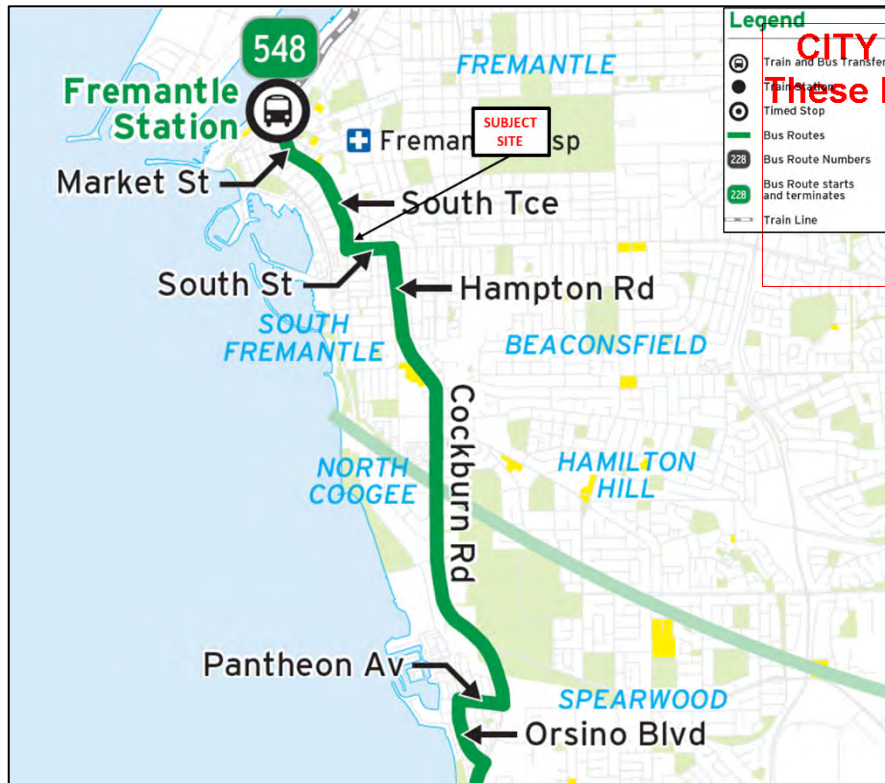


Figure 11: Bus Service 532 (Transperth Maps)





CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
5 Dec 2024

Figure 12: Bus Service 548 (Transperth Maps)



Figure 13: Bus Service 549 (Transperth Maps)



9 Pedestrian Access

Pedestrian paths in the vicinity of the subject site are directly available on both sides of South Street and South Terrace. Pedestrian crossing opportunities are available at the signalised intersection of South Street/South Terrace located in the immediate vicinity of the subject site.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

10 Cycle Access

The Perth Bicycle Network Map illustrated in **Figure 14** shows the existing cyclist connectivity to the subject site. The subject site has direct cycle access to the bicycle lanes or sealed shoulders which are in place along South Terrace and South Street in the vicinity of the subject site.



Figure 14: Extract from Perth Bicycle Network (Department of Transport)

11 Site Specific Issues

In order to ensure the car wash lanes operate satisfactorily and check for any potential queue backs to South Street a queue analysis for the car wash lanes was undertaken.

11.1 Queue Analysis

11.1.1 Proposed Automated Car Wash Road Network Peak Hour Trip Generation

Based on the calculated road network peak hour trip generation of the proposed automated car wash as outlined in **Section 6.2**, the anticipated patronage during road network peak hour is 9 vehicles, where the anticipated trip generation is 18 vehicles per hour which includes both inbound and outbound movements.

11.1.2 Car Wash Queue Analysis

The proposed automated car wash is designed to include three car wash lanes.

The proposed automated car wash provides 12 car stacking capacity upstream the car wash lanes. Based on the information available to Transcore, it is estimated that the typical servicing (washing) time for each lane is 5 minutes. This translates to a service rate of 12 vehicles per hour per car wash lane.

A queue length analysis was undertaken to assess the queuing of vehicles within the car wash lanes. For this purpose, an M/M/1 queuing model was adopted for each lane. The M/M/1 is a single-server queue model that can be used to approximate simple systems.

The queuing model adopted the following assumptions:

- Vehicles arrive randomly following Poisson's probability distribution;
- Service time is exponentially distributed;
- The capacity of the queue in which arriving users wait before being served is infinite (for the purposes of identifying queue space requirements);
- The population of users (i.e., the pool of users) available to join the system is infinite; and,
- The queue is serviced on a first-come, first-served basis.

The results of the queuing analysis are detailed in **Figure 15**.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

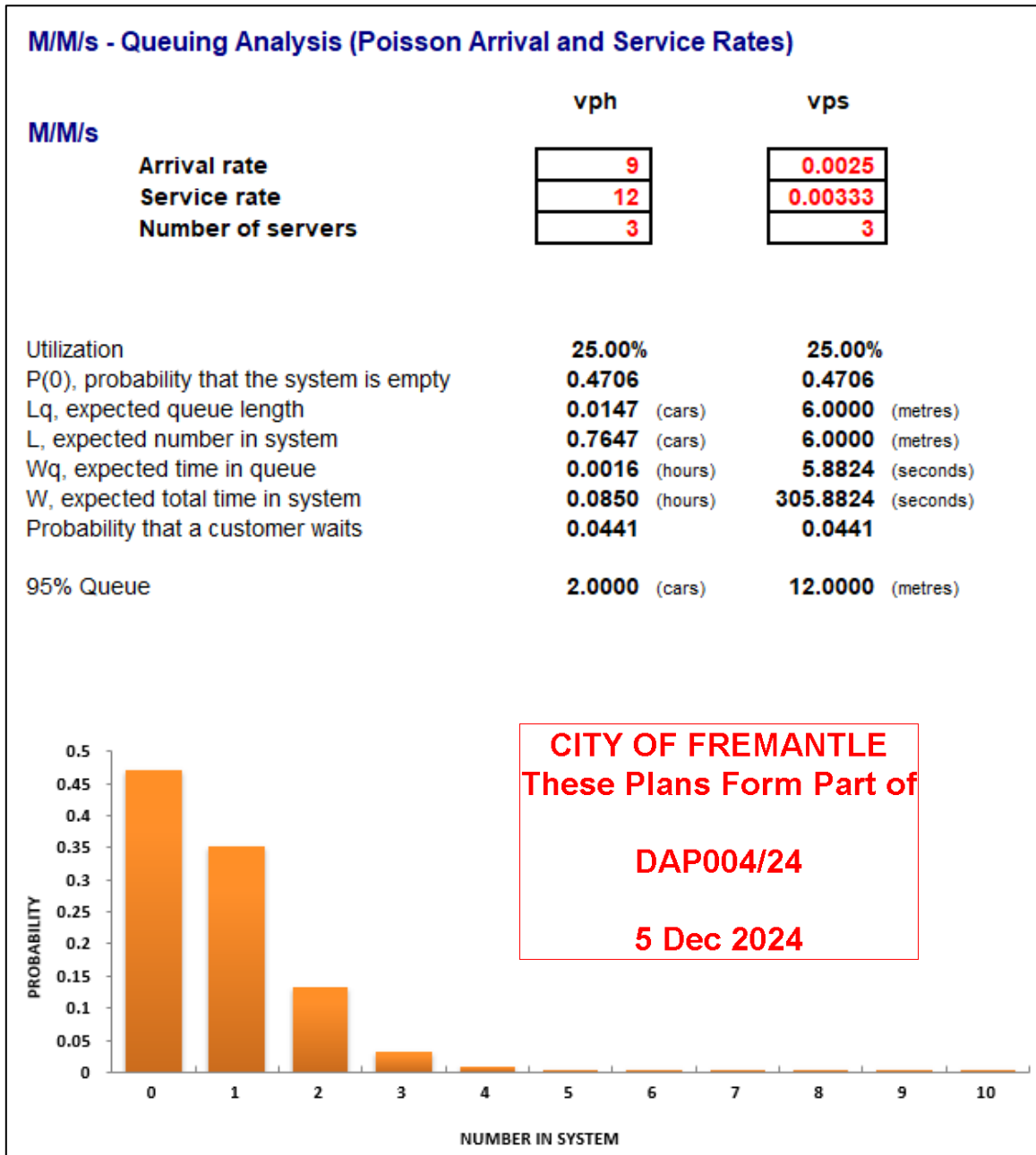


Figure 15: Peak hour queueing analysis

In summary, peak hour queueing analysis of the car wash lanes established the following:

- The system utilisation rate is 25.00% during the peak hour;
- The expected number of vehicles in the system is three;
- The probability of the system (car wash bay) being empty is 47.06%;
- The probability that a customer waits is 4.41%;
- The expected total time in the system is 305.88 seconds; and,
- The 95th percentile queue within the whole system is two vehicles.

The queue length usually adopted for robust analysis is the 95th percentile queue. This queue length will not exceed 95% of the time during the peak hour.

Based on the queue analysis model, it is estimated that under typical peak conditions, the queue back from each car wash lane will be accommodated within the three car wash lanes, with no impact on the internal circulation system and the site's crossovers

No other site-specific issues were identified within the scope of this assessment.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



12 Safety Issues

No safety issues were identified within the scope of this assessment.

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
5 Dec 2024

13 Conclusions

This Transport Impact Statement (TIS) has been prepared by Transcore on behalf of Glowmark Nominees Pty Ltd with regards to a proposed automated car wash to be located at Lots 27, 300, 301 & 29 (234 - 238) South Terrace, South Fremantle in the City of Fremantle.

The site is located at the southeast corner of South Street/South Terrace signalised intersection.

As part of the proposed development, it is proposed to relocate the existing crossovers on South Street and South Terrace furthest away from the signalised intersection of South Street and South Terrace. Also, as part of the proposed car wash development the bin storage area is proposed at the northwestern corner of the site.

Turn path analysis undertaken for a 12.5m waste service truck and B99 vehicle for the automated car wash facility confirms satisfactory entry, egress and circulation to/from and within the site.

The traffic analysis undertaken in this report shows the anticipated increase in trips generated by the proposed automated car wash is relatively low and as such would have an insignificant impact on the surrounding road network.

No site-specific and safety issues have been identified for the proposed development.

In conclusion, the findings of this Transport Impact Statement are supportive of the proposed development.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

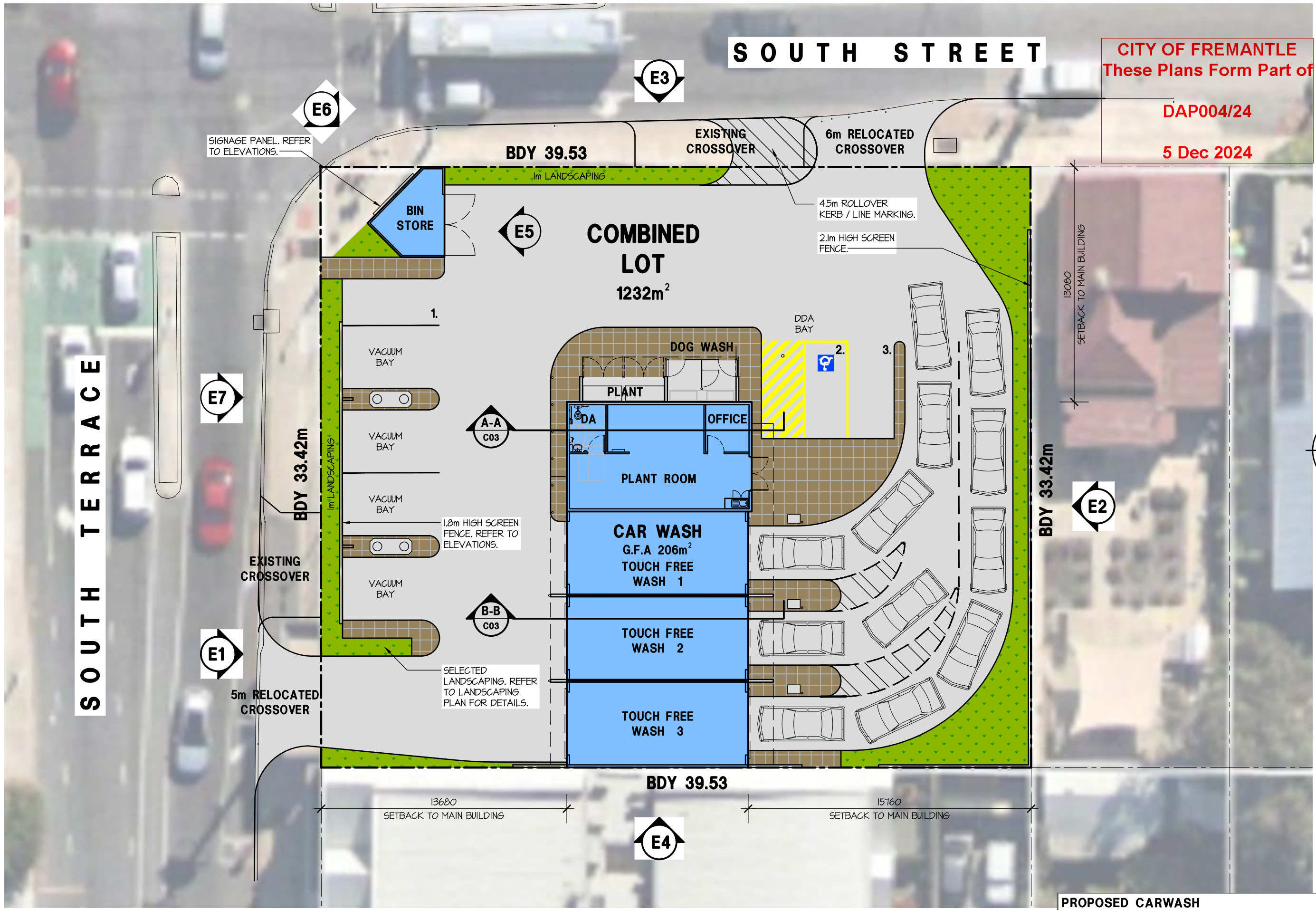
5 Dec 2024

Appendix A

PROPOSED DEVELOPMENT PLAN



Engineering a better future for over 20 years!



CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
5 Dec 2024



IWA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6109
 PO BOX 199
 NEDLANDS WA 6109
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

SOUTH TERRACE

SOUTH STREET

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
 DO NOT SCALE FROM THIS DRAWING.
 THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE.
 THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.

NO: DATE: REVISION: DRAWN: CHECK:

A3 SHEET

PROPOSED SITE PLAN
 SCALE 1:200

PROPOSED CARWASH
 No 234 TO 238
 LOTS 27, 300, 301 & 29
 SOUTH TERRACE CNR SOUTH STREET
 SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

Date - 26.09.24
 Design - MDB
 Drawn - MDB
 Checked - SJH
 Scale - 1:250
 Job No. - 0857
 Dwg - **DA01**
 Rev -

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

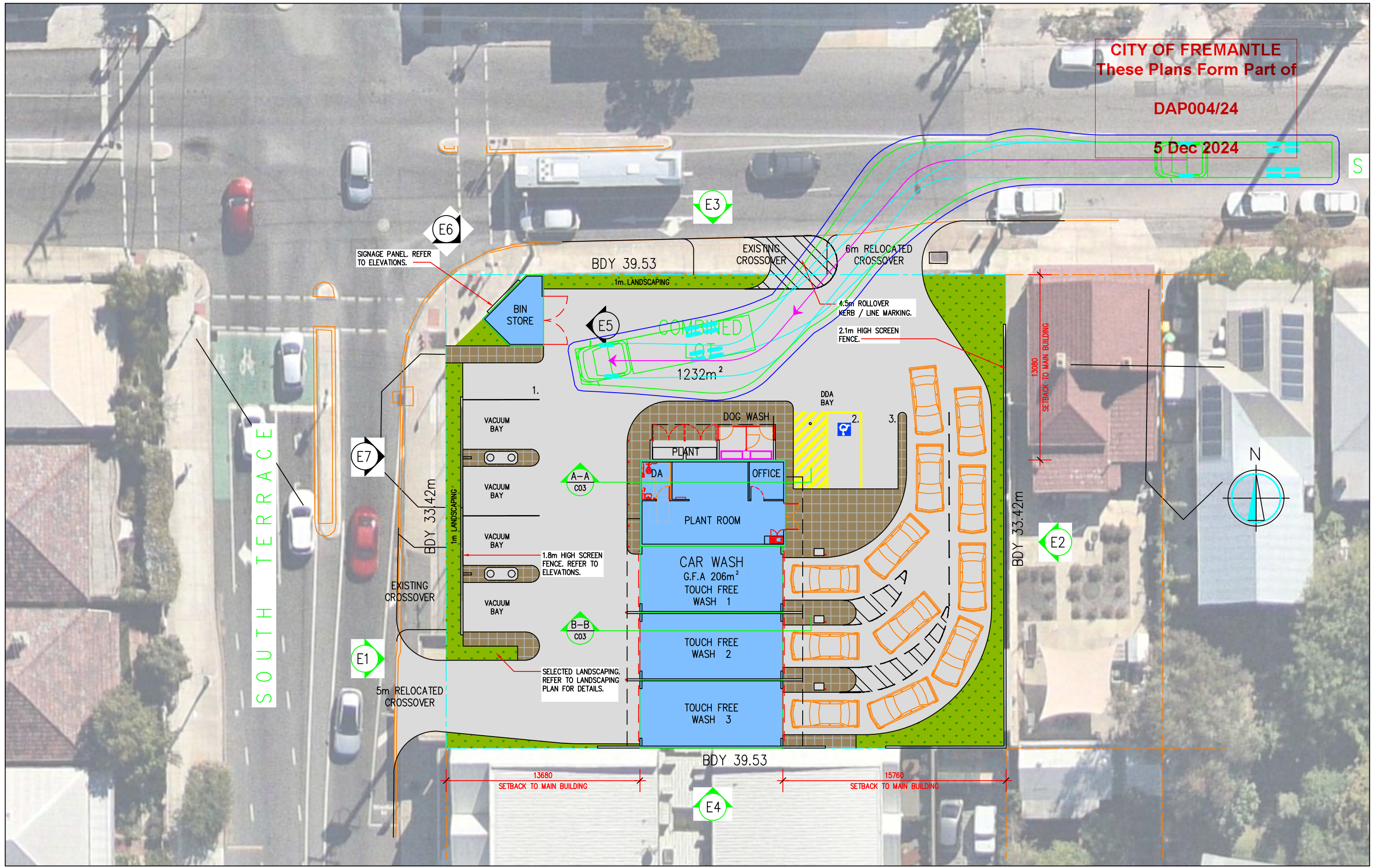
5 Dec 2024

Appendix B

TURN PATH ANALYSIS

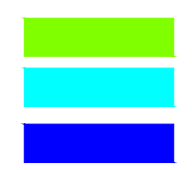


Engineering a better future for over 20 years!



Cnr South Street and South Terrace, South Fremantle
Austroads 2023: 12.5m HRV
Service vehicle entry

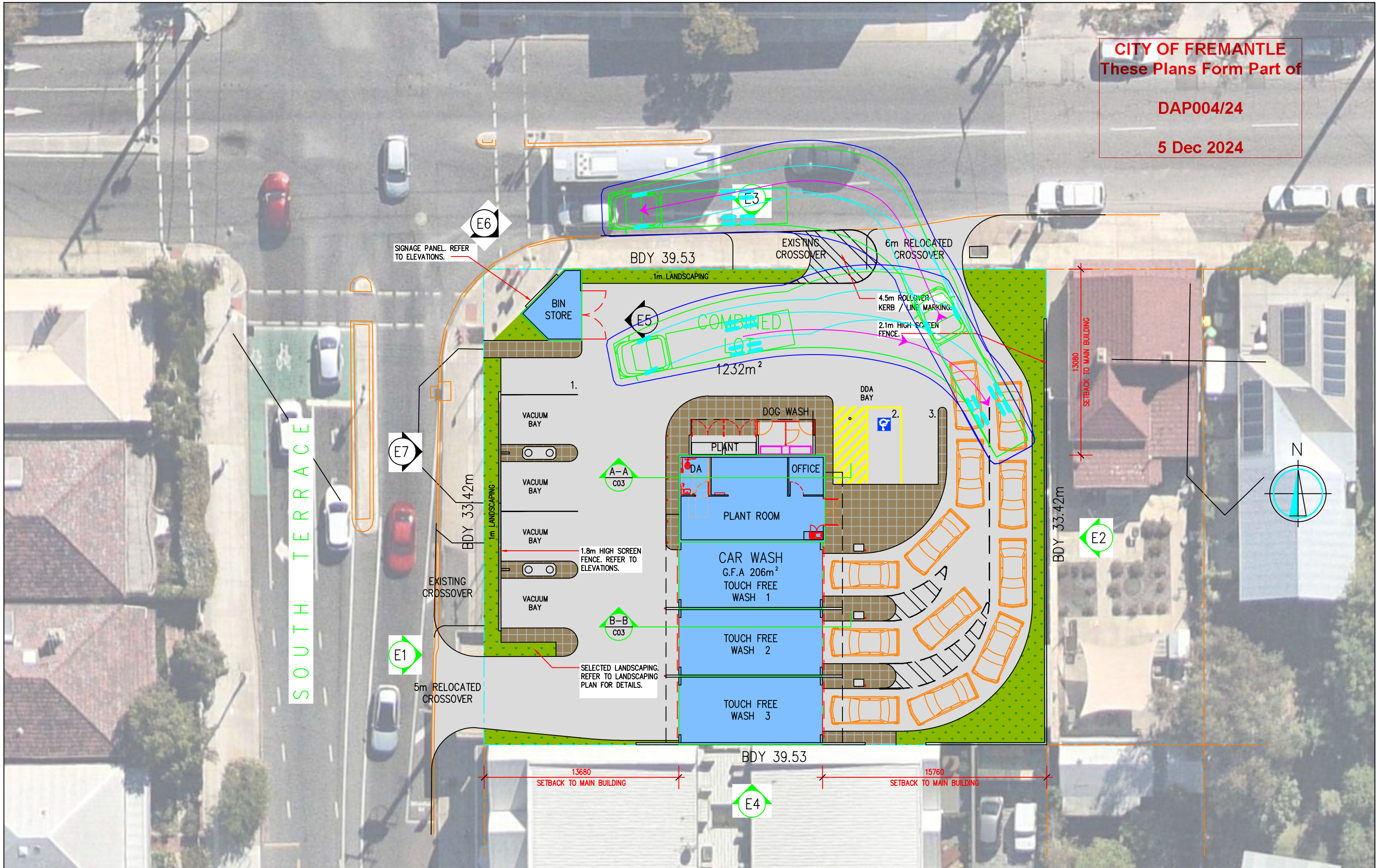
LEGEND
Vehicle Body
Wheel Path
500mm Clearance



t24.139.sk01a
08/10/2024
Scale: 1:250 @ A3

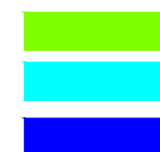


CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
5 Dec 2024



Cnr South Street and South Terrace, South Fremantle
 Austroads 2023: 12.5m HRV
 Service vehicle exit

LEGEND
 Vehicle Body
 Wheel Path
 500mm Clearance



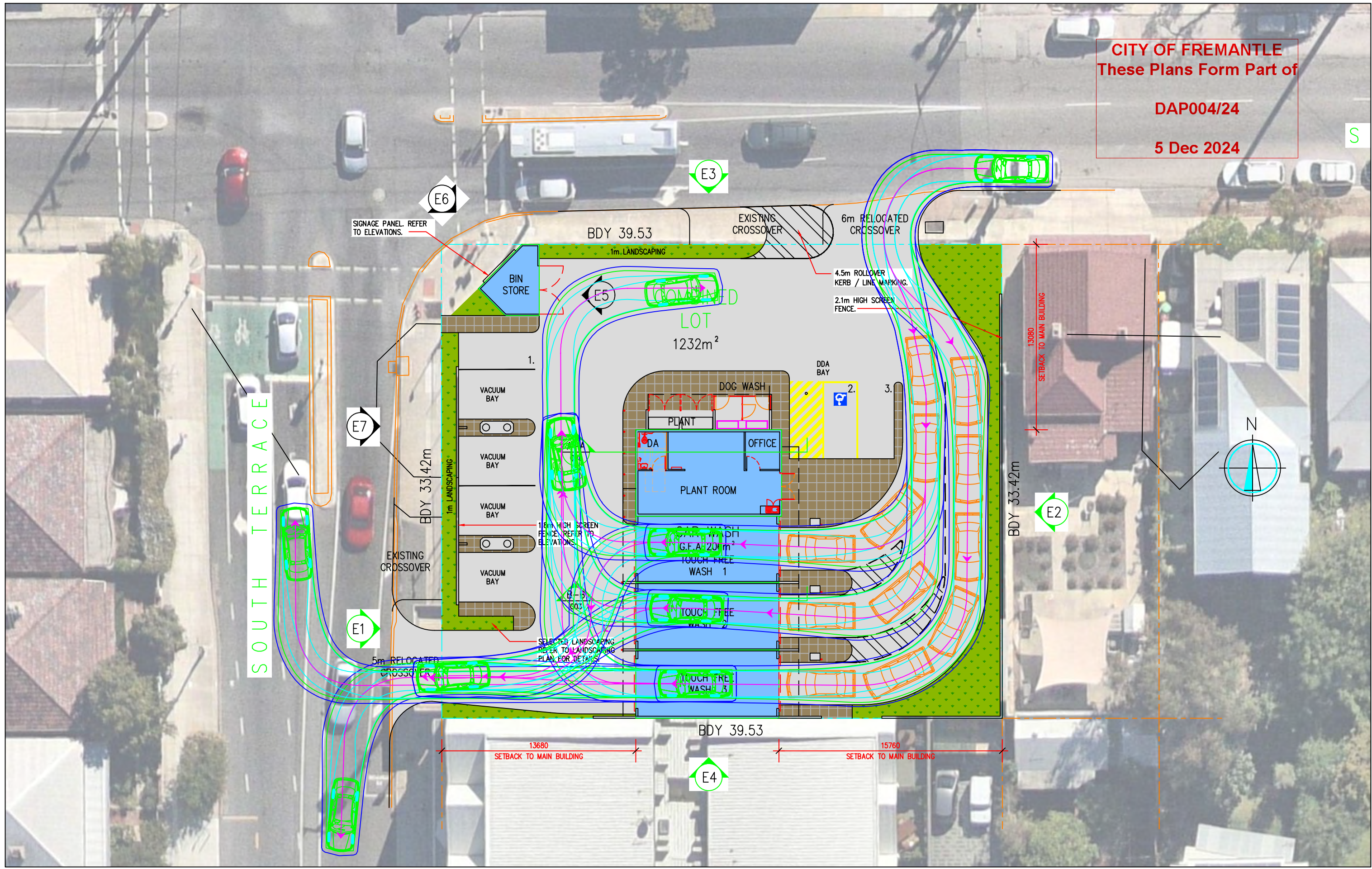
t24.139.sk02a

08/10/2024

Scale: 1:250 @ A3



CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
5 Dec 2024



Cnr South Street and South Terrace, South Fremantle
 Austroads 2023: B99 Passenger Car
 Car wash lanes

LEGEND

- Vehicle Body
- Wheel Path
- 500mm Clearance

t24.139.sk03a
 08/10/2024
 Scale: 1:250 @ A3



Technical Note: No 1a	Date: 05/06/2025
Project No: t24.139	
Project: Proposed Carwash - Corner South Street & South Terrace, South Fremantle	
Subject: Assessment of the development crossovers	

INTRODUCTION

Transcore prepared a Transport Impact Statement (TIS) on behalf of Glowmark Nominees Pty Ltd with regards to a proposed automated car wash to be located at Lots 27, 300, 301 & 29 (234 - 238) South Terrace, South Fremantle in the City of Fremantle. The development entails one crossover on South Street and one crossover on South Terrace as per the subject site existing situation.

This technical note is prepared to investigate the relevant right turn movements by reviewing the traffic volumes on the relevant roads and conducting a desktop assessment of the right turn movements.

The subject site is located at the southeastern corner of the signalised intersection of South Street/South Terrace as shown in **Figure 1**.

PROPOSED DEVELOPMENT

The proposed automated car wash comprises 3 car wash tunnels with 4 vacuum bays. The proposed site plan in **Appendix A** shows the automated car wash will effectively entail a one-way clockwise circulation system with entry from the crossover on South Street and exit via the crossover on South Terrace. Therefore the relevant right turn movements are right turn ins from South Street and right turn outs onto South Terrace.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025



Figure 1: Site location

TRAFFIC VOLUMES

Transport modelling and analysis undertaken in the TIS indicated that approximately 7 vehicles per hour would make a right turn from South Street into the proposed car wash and approximately 7 vehicles per hour would make a right turn out of the development onto South Terrace during the peak operating times.

It's important to note that the peak operating times for a car wash do not coincide with the peak operating times of the road network. The highest patronage for car washes generally happens on weekends, which does not align with the peak hours of the road network. Accordingly, the peak operating times for a car wash typically occur on Saturday midday, depending on weather conditions.

As a result, existing traffic volumes on South Street and South Terrace were established by reviewing the latest SCATS data at the signalised intersection of South Terrace and South Street, as well as the available traffic counts from the Main Roads WA traffic map website. **Figure 2** illustrates the traffic volumes on the surrounding roads and the number of right turn movements at the crossovers.

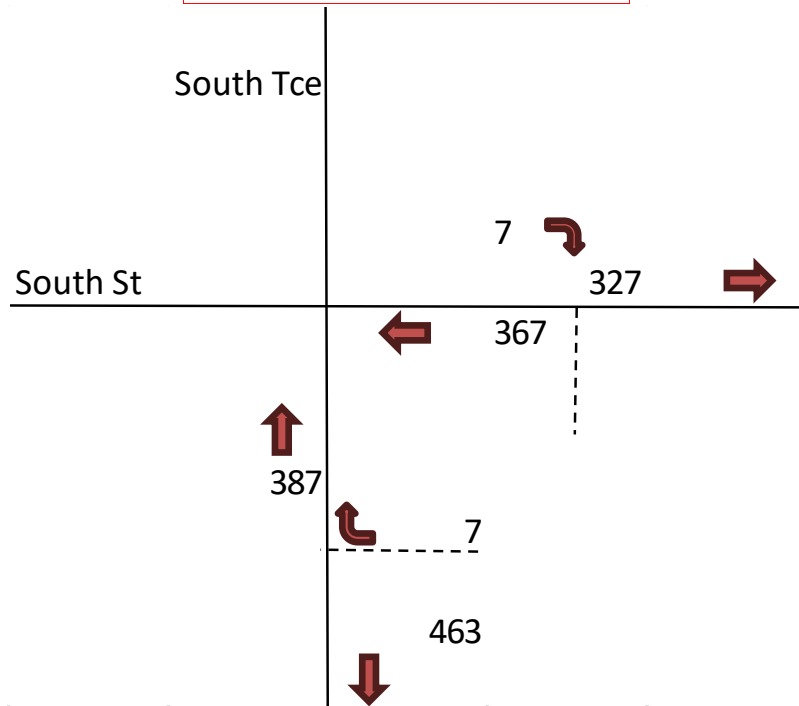


Figure 2: Existing traffic volumes and projected right turn movements at the development crossovers

ASSESSMENT

Austroads Guide to Traffic Management Part 2: Traffic theory Concepts have been sourced to establish the practical capacity of the right turn movements in and out of the proposed development. The practical capacity was determined using the principal gap acceptance formula outlined in Section 5.2 of the *Austroads Guide to Traffic Management*, as follows:

$$C = \frac{qe^{-qT}}{1 - e^{-qT_0}}$$

C: the theoretical absorption capacity in veh/s (multiply by 3600 for veh/h)

q: the volume of the conflicting major traffic stream, in veh/s

T: the size of the critical gap (or critical lag), in s/veh

T0: the follow-up headway, in s/veh

The practical capacity is approximately 0.8 to 0.85 of the theoretical capacity.

The critical gap and follow-up headway for the right turn movements were based on the default values provided by SIDRA computer platform for these parameters.

Additionally, it was assumed that during an average 60-second green phase at the signalised intersection, one-third of the green time would be allocated to South Terrace, while the remainder would be assigned to South Street, as South Street experiences higher traffic volumes and requires a longer green time. The practical capacity was then calculated using the aforementioned formula, taking into account the available green time at the traffic lights (refer [Figure 3](#)).

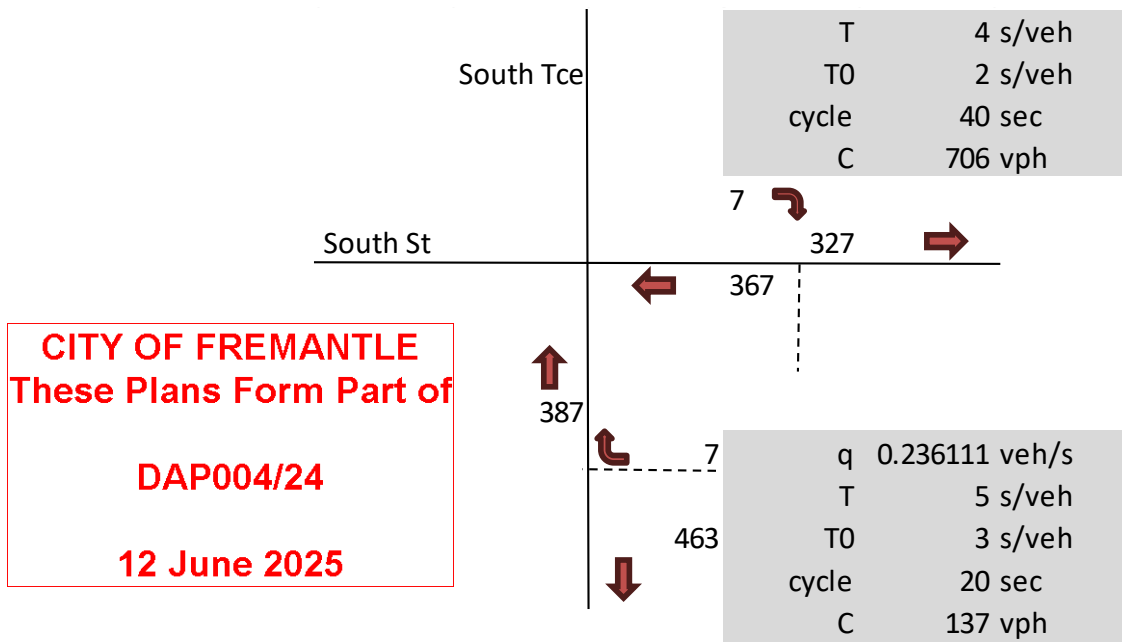


Figure 3: Practical capacity of the right turn movements

The calculated practical capacity for the right turn movement from South Terrace is approximately 706 vph, while the practical capacity for the right turn movement out of the South Terrace crossover is calculated as about 137 vph. This is significantly higher than the projected 7 vph right turn movements at both crossovers.

As a result, the proposed right turn movements at the crossovers are expected to operate satisfactorily and within capacity. Further the operations of the traffic lights will create gaps in the traffic flow which allows the relatively low right turning movements associated with the development traffic to occur satisfactory.

It should be noted that any potential queue from the right turn movement out of the South Terrace crossover would remain internal to the site, thus not disrupting traffic flow on South Terrace. Additionally, the section of South Street near the development entry crossover features a 6.5-meter-wide carriageway, allowing through traffic to bypass any stopped vehicles intending to turn right at the crossover (refer plan in [Appendix B](#)).

CONCLUSIONS

The desktop modelling and analysis presented in this technical note indicate that, the projected right turn movements at the development crossovers during the peak periods are relatively low and approximately 7 vehicles per hour. Also, the analysis established that the calculated practical capacities for these movements are significantly higher than the projected right turns: 706 vehicles per hour turning right into South Street and 137 vehicles per hour turning right out onto South Terrace. This implies that the proposed right turn movements will operate satisfactorily and within capacity.

Any potential queue from the right turn movement out of the South Terrace crossover will be retained internal to the site, ensuring that traffic flow on South Terrace is not disrupted. Moreover, the section of South Street near the development entry crossover features a 6.5-meter-wide carriageway, allowing through traffic to easily bypass any stopped vehicles intending to make a right turn at the crossover.

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
12 June 2025

Appendix A

DEVELOPMENT PLAN

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025

Appendix B

TURN PATH PLAN

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025



Cnr South Street and South Terrace, South Fremantle
Austroads 2023: B99 Passenger Car
Passenger car overtaking a passenger car waiting to turn right

LEGEND
Vehicle Body
Wheel Path
300mm Clearance



t24.139.sk06

05/06/2025

Scale: 1:250 @ A3





Technical Note

Subject: Proposed Car Wash Development – Transport Impact Statement Review

Date: 07/03/2025

Author: Yuyang Ke

Reviewer: Richard Jois

Client: City of Fremantle

1. Introduction

Glowmark Nominees Pty Ltd has commissioned Shawmac Traffic & Safety to review the Transport Impact Statement (TIS) produced by Transcore for the proposed Car Wash Development. The development, located at the corner of South Street and South Terrace in South Fremantle, includes:

- 3 car wash tunnels
- 4 vacuum bays
- A plant room
- 2 dog washrooms
- 2 parking bays (including 1 disabled bay)

The review was conducted in accordance with the WAPC Transport Impact Assessment Guidelines, Australian Standards, and the City of Fremantle Local Planning Policy. The purpose is to assess the validity of assumptions, address road safety concerns, and identify areas requiring clarification before the City of Fremantle makes a decision. The latest amended TIS (Revision 2, dated 8 October 2024) was provided by Glowmark Nominees Pty Ltd.

2. Review of the Transport Impact Statement (TIS)

2.1. Section 1 – Introduction

The information in this section is supported.

2.2. Section 2 – Development Proposal

The information in this section is supported.

CITY OF FREMANTLE
These Plans Form Part of
DAP004/24
12 June 2025



2.3. Section 3 – Vehicle Access and Parking

- It is unclear whether the width of the proposed access points and parking bays complies with the City of Fremantle's Town Planning Scheme No. 4. Confirm that the access width and parking bay dimensions align with the requirements of the Scheme (e.g., minimum widths, layout standards).

2.4. Section 4 – Provision for Service Vehicles

The information in this section is supported.

2.5. Section 5 – Hours of Operation

The information in this section is supported.

2.6. Section 6 – Daily traffic Volumes and Vehicle Types

2.6.1. Section 6.1 – Existing Development Trip Generation

The information in this section is supported.

2.6.2. Section 6.2 – Proposed Development Trip Generation

ITE Land Use Code 939 Parameters

From the ITE Trip Generation Manual (latest edition):

- Vehicle Trip Rate: Typically, car washes under Code 939 generate 88 vehicle trips per day per bay (average rate; confirm with ITE appendices).
- Peak Hour Distribution:
 - Weekday PM Peak: ~12% of daily trips (e.g., 4–6 PM).
 - Weekend Midday Peak: ~18% of daily trips (e.g., 10 AM–2 PM).
- Pass-By Trip Adjustment: ~25% of trips are pass-by (already on the roadway).

Calculations

Daily Trip Generation

For 3 bays:

Daily Trips=88trips/day/bay×3bays=264trips/day

Weekday PM Peak

- Peak Hour %: 12% (from ITE time-of-day distributions).
- Peak Hour Trips: 264 daily trips×0.12=32 trips/hour
- Adjusted for Pass-By Trips: $32 \times (1 - 0.25) = 24$ new trips/hour (net)

Weekend Midday Peak

- Peak Hour %: 18% (from ITE time-of-day distributions).
- Peak Hour Trips: 264 daily trips×0.18=48 trips/hour
- Adjusted for Pass-By Trips: $48 \times (1 - 0.25) = 36$ new trips/hour (net)

Trip Generation Summary

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025



Scenario	Daily Trips	Peak Hour %	Gross Peak Trips	Net Peak Trips (Pass-By Adjusted)
Weekday PM	264	12%	32	24
Weekend Midday	264	18%	48	36

2.6.3. Section 6.3 – Traffic Flow

The information in this section is supported.

2.7. Section 7 - Traffic Management on Frontage Streets

The information in this section is supported.

2.8. Section 8 – Public transport Access

The information in this section is supported.

2.9. Section 9 – Pedestrian Access

The information in this section is supported.

2.10. Section 10 – Cycle Access

The information in this section is supported.

2.11. Section 11 – Site Specific Issues

2.11.1. Section 11.1

Potential queue at the new South St access

- We acknowledge the City's concerns that the estimated queue length may be underestimated. However, our assessment which applied trip generation rates per ITE Land Use Code 939 aligns with the findings of the TIS. In a worst-case scenario where queues form on South Street at the new access point, road users are anticipated to avoid blocking the roadway by proceeding onward if existing car wash queues are observed. Furthermore, the existing low speed environment through the intersection is expected to mitigate associated risks.

Consideration of Surrounding Traffic Dynamics

- To analyse congestion, intersection performance, and spillover effects elements typical of a TIA which conflicts with the Transport Impact Assessment Guidelines. Car wash facilities are classified as moderate-impact developments under these guidelines, requiring a TIS level assessment, not a TIA.

2.11.2. Section 11.1.1 - Proposed Automated Car Wash Road Network Peak Hour Trip Generation

- The information in this section is supported.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025



2.11.3. Section 11.1.2 – Car Wash Queue Analysis

- The information in this section is supported.

2.12. Section 12 – Safety Issues

- **Traffic Safety for Egress onto South Terrace northbound:**

We acknowledge the City's concerns regarding potential safety risks associated with vehicles exiting the proposed development near the signalised intersection. While such movement could increase collision risks, the following factors suggest minimal impact:

- The existing low speed limit through the intersection reduces the severity of potential conflicts.
- The development's traffic generation is estimated at fewer than 30 vehicle trips during peak hours, which is unlikely to materially degrade intersection performance.

Based on this analysis, the project's traffic impact on the intersection is expected to be negligible.

- Recommend adding this specific safety issue to Section 12 of the TIS.

2.13. Section 13 – Conclusions

- Update this section to reflect revisions made to preceding sections.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025

Our Ref: RT/L038B.25
Job No: 25-01-012

21 February 2025

Glowmark Nominees Pty Ltd
PO Box 268, Nedlands
WA 6909

Level 2 Kishorn Court
58 Kishorn Road
Mount Pleasant WA 6153

PO Box 1036
Canning Bridge WA 6153

Tel: (08) 9315 9955
Email: office@portereng.com.au
www.portereng.com.au

Attention: Brian McCubbing

Dear Brian,

**LOTS 27, 300, 301 & 29 (#234, 236 & 238)
PROPOSED CAR WASH SOUTH TERRACE, FREMANTLE
STORMWATER AND SEWER MANAGEMENT PLAN REV A**

The Site Stormwater Management Plan has been prepared in support of an application for development approval for a carwash facility at Lots 27, 300, 301 and 29 South Terrace, Fremantle. The proposed development involves construction of a touchless carwash facility and associated parking.

A copy of the proposed development is presented in *Attachment A*. The site location plan is shown in *figure 1* below, site boundary shown in red. The site is bound by road reserve to the north and west, commercial lot to the south and residential lot to the east.



Figure 1 – Site location plan

The lots will be amalgamated as part of the development application.

LANDFORM

The site is situated in the Coastal Hydrographic Catchment of Fremantle with soils expected to be Tamala Limestone and Safety Bay Sands¹. High infiltration rates are expected on site. A geotechnical investigation will be carried out prior to detail design to finalise the stormwater drainage solution.

Topographical survey² shows the existing site shows crossover levels at RL 4.7m at the north and south boundary. An existing retaining wall is present along the boundary in the north eastern corner of the site.

Groundwater mapping³ suggests existing levels are at around 0.5m AHD or 2.5m below existing ground levels. Groundwater is not expected to be encountered as part of the project works.

STORMWATER DESIGN PRINCIPALS

Quantity

In accordance with City of Fremantle requirements⁴, the design will provide onsite storage for the 1% AEP critical event. An overland flow path will be available to the crossover in the event that the on-site infiltration system fails.

Quality

The proposed touchless carwash will be separated from the stormwater system on site by pavement grading and construction of a roof over the wash bays. The water collected from the carwash will be directed through silt pits and an oily water treatment system that will reduce the level of contaminants to the compliant levels accepted by Water Corporation as indicated in Table 1 below before being discharged to the sewerage system.

%TW Prop	BOD	SS	O&G	TKN	TP	SO4	Cu	Zn
82	21	23	6	1.5	0.5	30	0.15	0.20

Table 1 – Minimum requirements for discharge to Water Corporation Sewer

Pollutants generated from the carpark hardstand areas and roof are expected to be similar to typical pollutants found on roads and carparks - Total Suspended Solids (TSS) and hydrocarbons. The highest concentration of these pollutants is found in the first flush of rainfall, typically calculated at 15mm across the pavement and roof hardstand area. This volume shall be contained and treated on site by infiltration.

Infiltration Rates

Based on the existing ground conditions, an estimated infiltration rate of 8m/day has been adopted for the concept design. This infiltration rate will be confirmed by permeability testing prior to detailed design and the volume of storage adjusted accordingly to suit the actual recorded capacity on site. To account for blockages and silts, the permeability rates obtained during testing should be reduced by 50% to obtain the design infiltration rates.

¹ Gozzard J.R. 1983 Fremantle Sheet 2033 I & 2033 IV, Perth Metropolitan Region, Environmental Geology Series, Geological Survey of Western Australia.

² Survey provided by Hindley and Associates Pty Ltd on 12 February 2025

³ Water and Rivers Commission 1997, *Perth Groundwater Atlas, Map 137*

⁴ City of Fremantle Engineering Technical Guidance Notes Rev4 April 2018

STORMWATER QUANTITY CALCULATION

The stormwater storage system will be designed to contain the 1% AEP critical event on site. Soakwells will be used to provide storage and infiltration to ground.

The Site area hardstand is 1,315m² and a coefficient of 0.9 has been adopted.

The rainfall intensity for the critical 1% AEP event is 22.70mm/hr. Using the Rational Method, the total volume of storage required on site is: **80.6m³**

A copy of the calculation table for the concept design is presented in *Attachment B*.

PROPOSED CARWASH BAY TREATMENT SYSTEM AND SEWER CONNECTION

The car wash bays will direct all water runoff to a series of silt sumps and convey flows through an oily water separator prior to connection to the existing Water Corporation sewer located within the lot along the eastern boundary. The existing sewer junction is expected to be suitable for the anticipated flow rates generated from works on site. All pipework conveying untreated water will be HDPE material.

The treatment system will be selected from the list of pre-approved by Water Corporation systems (below) at detailed design stage.

<https://www.watercorporation.com.au/Help-and-advice/Trade-waste/Approved-pre-treatment-products/Oil-water-separators>

The oily water separator will be sized in accordance with the water meter requirements on site. This calculation will be carried out by the Water Corporation at the time of application of the upgraded water meter.

PROPOSED STORMWATER LAYOUT

The roof and pavement areas will be graded to a number of standalone soakwells with a grated inlet. The base of the soakwells will permit infiltration below ground. All roof areas will be connected to the soakwells. Allowance has been made for full storage to be contained in soakwells below ground level, noting that some capacity could be provided above ground if required for extreme events.

The treatment available on site will be located at the base of the soakwell structures. The TSSs and heavy metals will flow into the soakwells infiltrating base area. The silts will cause a reduction in infiltration at the base over time. Therefore, a maintenance plan outlining the clear-out of the soakwells every 12 months should be put in place. The material cleared out of soakwells should be removed from site.

A copy of the concept stormwater and sewer layout is presented in *Attachment C*.

CONCLUSION

The proposed site can be designed to contain the required stormwater volumes to comply with the City of Fremantle drainage requirements for commercial lots.

The storage volume to be contained on site is the critical 1% AEP event. Treatment of carwash bays will be managed by a proprietary approved system. Treatment of carparking areas will be through on-site infiltration.

A maintenance plan for the carwash site will be developed at detailed design stage to ensure future ongoing maintenance is carried out to maintain the efficiency of the installed network.

The following works are recommended to progress to detailed design stage:

1. Geotechnical investigation to confirm infiltration rates
2. Detailed review of the proposed pavement levels and storage solution
3. Selection of an approved oily water treatment system

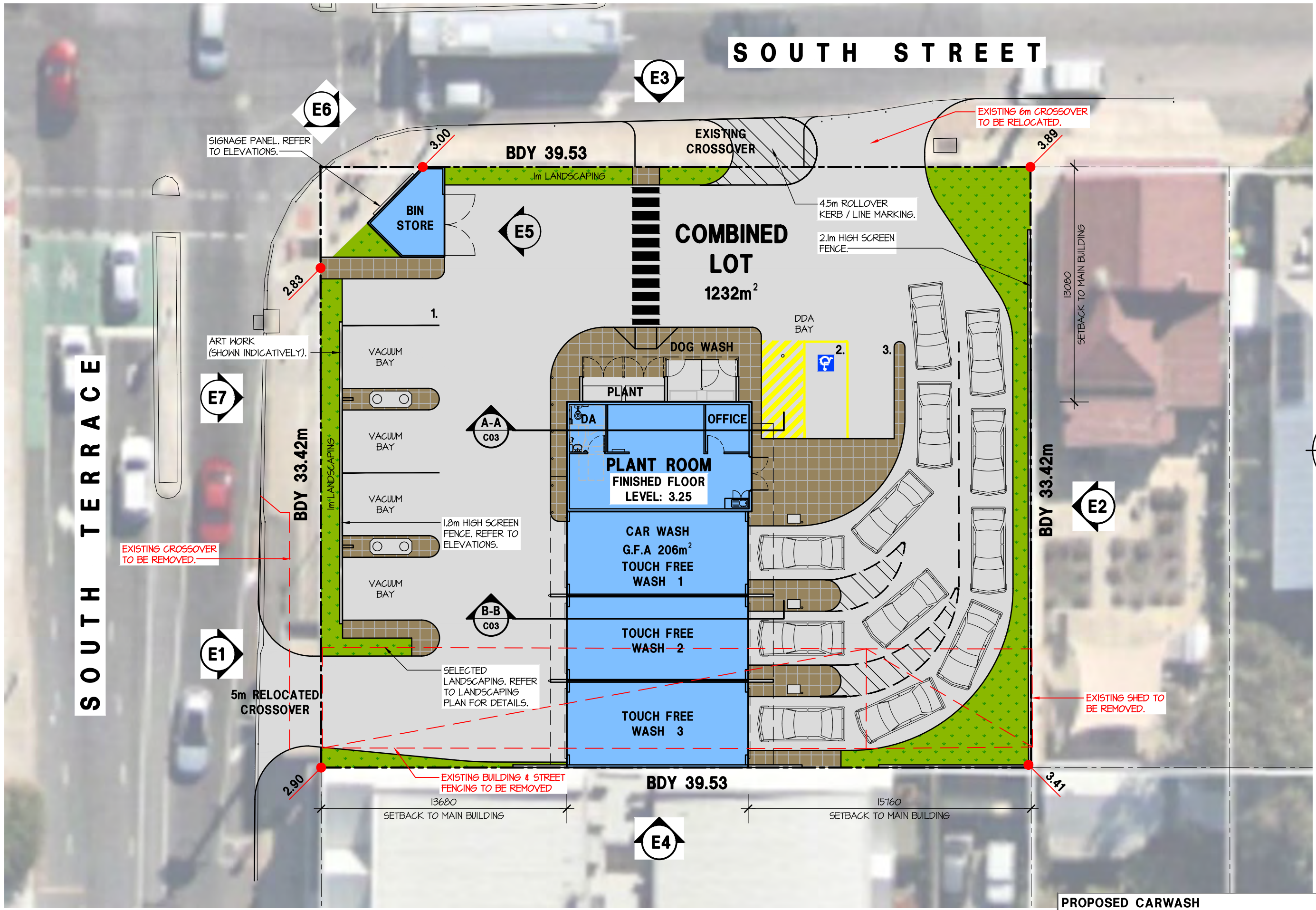
Yours faithfully,

A handwritten signature in black ink, appearing to read 'B. Harris', with a stylized flourish at the end.

BRAD HARRIS
MANAGING DIRECTOR

Enc.

ATTACHMENT A – CONCEPT DEVELOPMENT LAYOUT



SOUTH TERRACE

SOUTH STREET

COMBINED LOT
1232m²

BIN STORE

VACUUM BAY

VACUUM BAY

VACUUM BAY

VACUUM BAY

DOG WASH

PLANT

OFFICE

PLANT ROOM
FINISHED FLOOR
LEVEL: 3.25

CAR WASH
G.F.A 206m²
TOUCH FREE
WASH 1

TOUCH FREE
WASH 2

TOUCH FREE
WASH 3

13080
SETBACK TO MAIN BUILDING

5m RELOCATED CROSSOVER

EXISTING SHED TO BE REMOVED.

EXISTING BUILDING & STREET FENCING TO BE REMOVED

13680
SETBACK TO MAIN BUILDING

15760
SETBACK TO MAIN BUILDING

PROPOSED SITE PLAN

SCALE 1:200

NOTE

ALL LEVELS TO A.H.D

PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
for GLOWMARK NOMINEES Pty Ltd



HINDLEY & ASSOCIATES
BUILDING DESIGNERS
166 STIRLING HIGHWAY
NEDLANDS WA 6109
PO BOX 199
NEDLANDS WA 6109
PHONE - 9386 6699
admin@hindley.com.au
www.hindley.com.au

DEVELOPMENT APPLICATION

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.		DO NOT SCALE FROM THIS DRAWING.	
THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE.			
THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.			
NO:	DATE:	REVISION:	DRAWN: CHECK:
A	08.10.24	DEVELOPMENT APPLICATION ISSUE	MDB SJH

A3 SHEET

Date	-	08.10.24
Design	-	MDB
Drawn	-	MDB
Checked	-	SJH
Scale	-	1:250
Job No.	-	0887
Dwg	-	DA01
Rev	-	A

ATTACHMENT B – CONCEPT STORMWATER CALCULATIONS

Project 234-238 South Terrace, South Fremantle
 Job Number 25-02-012

Date 19-Feb-25
 Engineer Rachel Thomson
 Summary Contain 1% AEP critical event on site
 Scenario Use of soakwells

File Name T016.25
 Revision A
 Reference Document L038.25

IFD Used Fremantle IFD used generated from BoM website
 Ground Conditions High permeability due to existing soils
 Groundwater 1m AHD, 2.5m below ground levels, not expected to be encountered



Intensities (mm/hr)		6 minute		30 minute		1 hour		3 hour		6 hour		12 hour				
		Intensity	Volume	Intensity	Volume	Intensity	Volume	Intensity	Volume	Intensity	Volume	Intensity	Volume			
1 EY (1 year)		68.3	68.3	28.6	28.6	18.6	18.6	9.21	9.21	5.83	5.83	3.62	3.62			
	0.2 EY (5 year)		96.80	96.80	40.50	40.50	25.90	25.90	12.80	12.80	8.18	8.18	5.17	5.17		
		10% AEP (10 Year)		112.00	112.00	46.90	46.90	29.90	29.90	14.80	14.80	9.53	9.53	6.08	6.08	
			1% AEP 100 Year		161.00	161.00	67.30	67.30	43.80	43.80	22.70	22.70	15.10	15.10	9.64	9.64
					2.25	2.25	5.76	5.76	72.00	72.00	2.25	2.25	2.25	2.25	2.25	2.25

Soakwell details
 Soakwell Diameter (m) = 1.8
 Liner Depth (m) = 1.8
 Effective Depth (m) = 1.8
 Soakwell base area (m²) = 2.54
 Soakwell Volume (m³) = 4.58
 No of Soakwells = 12

Drainage Network Details
 Drainage Pit Type 1
 Diameter (m) = 0.8
 Effective Depth (m) = 0.8
 Number of Type 1 pits = 0
 Soakage Base Diameter (m) = 0.8
 Drainage Pit Type 2
 Diameter (m) = 0.8
 Effective Depth (m) = 0.8
 Number of Type 1 pits = 0
 Soakage Base Diameter (m) = 0.8
 Drainage Pipes
 Pipe 1 Diameter (m) = 0.8
 Pipe 1 Length (m) = 0.8
 Pipe 2 Diameter (m) = 0.8
 Pipe 2 Length (m) = 0.8
 Pipe 3 Diameter (m) = 0.8
 Pipe 3 Length (m) = 0.8

Swale Details
 Length (m) = 0.000
 Width (m) = 0.000
 Height (m) = 0.000
 Side Slopes = 1: 3.000
 Freeboard (m) = 0.000 to pavement
 TWL = 1.80 m
 Base RL = 1.7 m
 Base Area (m²) = 0.00
 Swale cross section (m²) = 0.03
 Volume (m³) = 0.00

Drainage Basin
 Length (m) = 0.000
 Width (m) = 0.000
 Height (m) = 0.000
 Side Slopes = 1: 3.000
 Freeboard (m) = 0.000
 TWL = 15.00
 Base RL = 15.00
 Base Area (m²) = 0.00
 Volume (m³) = 0.00

Outflow
 Pipe 1 Diameter (m) = 0
 Pipe 1 Grade (1:?) = 0
 Pipe 1 Capacity (l/s) = 0
 Pipe 2 Diameter (m) = 0
 Pipe 2 Grade (1:?) = 0
 Pipe 2 Capacity (l/s) = 0
 Orifice 1 Diameter (m) = 0
 Orifice 1 Capacity (m) = 0
 Orifice 2 Diameter (m) = 0
 Orifice 2 Capacity (m) = 0
 Weir 1 Capacity (m) = 0
 Weir 2 Capacity (m) = 0

Roca Pipe Chart
 ** chart volume should be increased by 20% if system is operating under head (based on DRAINS Calc)**

Drainage Cells

Product Details	No of Cells
Rigofill ST 800 x 800 x 660(H)	= 0
Rigofill ST 800 x 800 x 350(H)	= 0

Location 1

Cell Dimensions	Height	Length	Width
0.35 x 0	0.35	0	0
0.66 x 0	0.66	0	0
0.80 x 0	0.80	0	0

Location 2

Cell Dimensions	Height	Length	Width
0.35 x 0	0.35	0	0
0.66 x 0	0.66	0	0
0.80 x 0	0.80	0	0

Location 3

Cell Dimensions	Height	Length	Width
0.35 x 0	0.35	0	0
0.66 x 0	0.66	0	0
0.80 x 0	0.80	0	0

Location 4

Cell Dimensions	Height	Length	Width
0.35 x 0	0.35	0	0
0.66 x 0	0.66	0	0
0.80 x 0	0.80	0	0

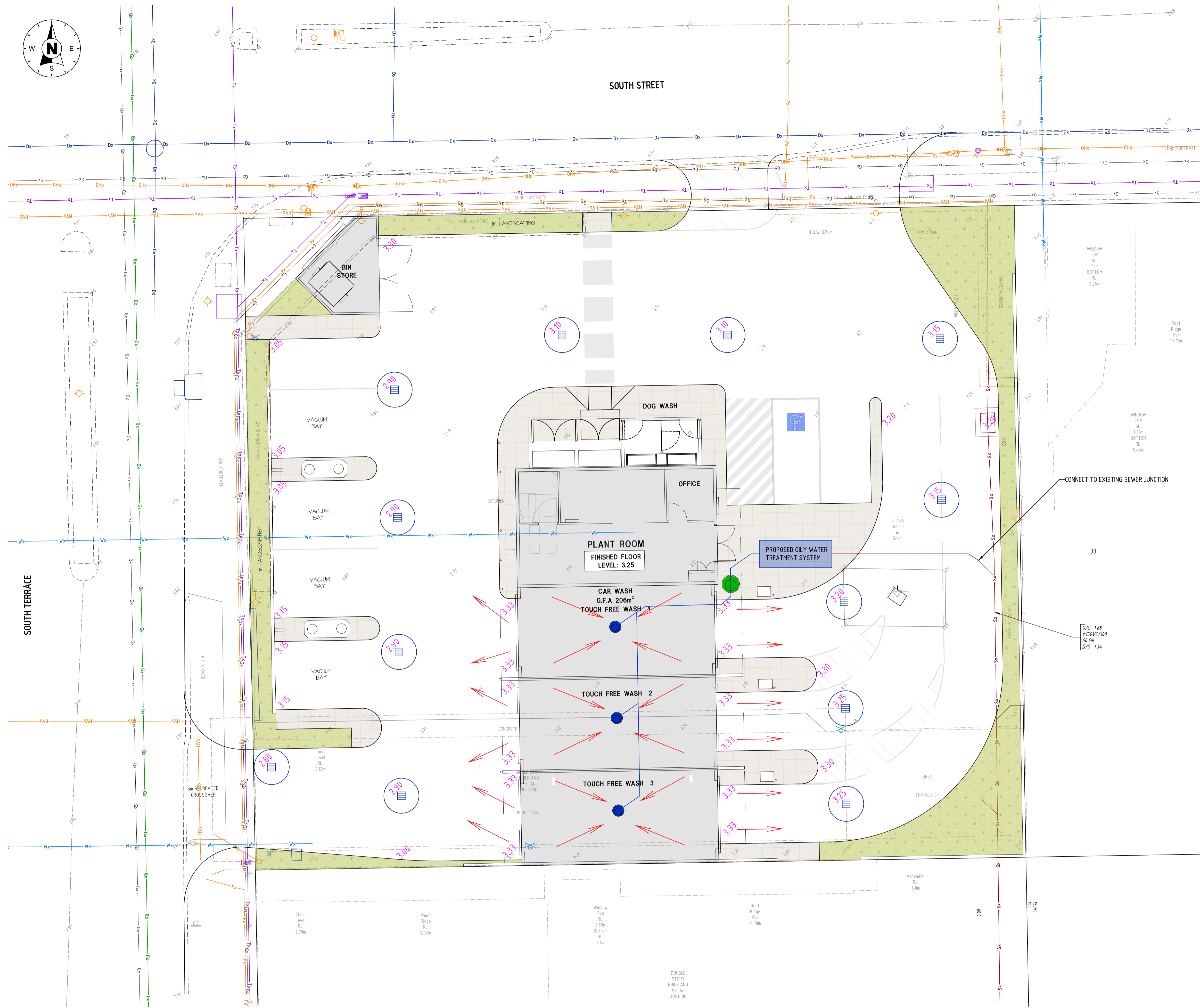
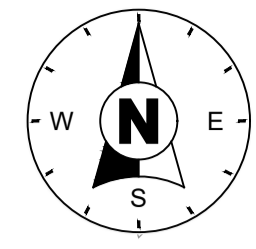
Total Base Area (m²) = 0
 Total Storage Volume (m³) = 0

Catchment Details

Area (ha) = 0.131
 Co-efficient of Runoff = 0.90
 Infiltration
 Rate of Soakage (m/day) = 8.0

Event	1EY (1 Year)						0.2 EY (5 Year)						10% AEP (10 Year)						1% AEP (100 Year)								
	6 min	30 min	1 hour	3 hour	6 hour	12 hour	6 min	30 min	1 hour	3 hour	6 hour	12 hour	6 min	30 min	1 hour	3 hour	6 hour	12 hour	6 min	30 min	1 hour	3 hour	6 hour	12 hour	24 hour	72 hour	
Intensities	68.3	28.6	18.6	9.21	5.83	3.62	96.80	40.5	25.9	12.80	8.18	5.17	112	46.9	29.9	14.80	9.53	6.08	161	67.30	43.80	22.70	15.10	9.64	5.76	2.25	
Q (m ³ /s)	0.0225	0.0094	0.0061	0.0030	0.0019	0.0012	0.0318	0.0133	0.0085	0.0042	0.0027	0.0017	0.0368	0.0154	0.0098	0.0049	0.0031	0.0020	0.0529	0.0221	0.0144	0.0075	0.0050	0.0032	0.0019	0.0007	
Volume	8.1	16.9	22.0	32.7	41.4	51.4	11.5	24.0	30.7	45.4	58.1	73.4	13.3	27.8	35.4	52.5	67.7	86.4	19.1	39.8	51.8	80.6	107.2	136.9	163.6	191.7	
SW Vol.	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97	54.97
Soak Vol	1.02	5.09	10.18	30.54	61.07	122.15	1.02	5.09	10.18	30.54	61.07	122.15	1.02	5.09	10.18	30.54	61.07	122.15	1.02	5.09	10.18	30.54	61.07	122.15	244.29	732.87	
SW Total	55.98	60.05	65.14	85.50	116.04	177.11	55.98	60.05	65.14	85.50	116.04	177.11	55.98	60.05	65.14	85.50	116.04	177.11	55.98	60.05	65.14	85.50	116.04	177.11	299.26	787.84	
Drn Net Vol	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Drn Net Soak Vol	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Drn Net Total	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Outflow Total	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Outflow Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Drn Cell Volume	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Drn Cell Soak Vol	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Drn Cell Total Vol	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Basin Volume	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Basin Soak Vol	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Basin Total Vol	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Swale Vol	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Swale Soak	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Swale Total	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
Total Vol	56.0	60.1	65.1	85.5	116.0	177.1	56.0	60.1	65.1	85.5	116.0	177.1	56.0	60.1	65.1	85.5	116.0	177.1	56.0	60.1	65.1	85.5	116.0	177.1	299.3	787.8	

ATTACHMENT C – CONCEPT STORMWATER AND SEWER MANAGEMENT PLAN



LEGEND

- 3.15 CONCEPT PAVEMENT LEVEL
- PAVEMENT FALL DIRECTION
- Ø18 x 1.8m DEEP GRATED SOAKWELLS
- VENTED COLLECTION PIT (OPTIONAL)
- SILT SUMP - MINIMUM CAPACITY 100 LITRES
- PROPOSED HDPE DN100 DRAINAGE PIPE - UNTREATED WATER
- uPVC DN100 PROPOSED INTERNAL SEWER - TREATED WATER
- OILY WATER TREATMENT SYSTEM
- PROPOSED ROAD/KERB
- EXISTING ROAD/KERB
- EXISTING PATH
- EXISTING SEWER MAIN
- EXISTING DRAINAGE PIPE
- EXISTING WATER MAIN
- EXISTING POWER LINE
- EXISTING HIGH VOLTAGE POWER LINE
- EXISTING OVERHEAD POWER LINE AND POLE
- EXISTING TELECOMMUNICATIONS
- EXISTING GAS LINE
- EXISTING ABANDONED GAS LINE
- PROPOSED LANDSCAPE AREA
- PROPOSED PAVING AREA

DRAINAGE SUMMARY TABLE

THE ON SITE STORMWATER SYSTEM HAS BEEN DESIGNED TO CONTAIN THE 1% AEP CRITICAL EVENTS, IN ACCORDANCE WITH CITY OF FREMANTLE REQUIREMENTS

DESIGN SUMMARY

POST DEVELOPMENT AEP	1%
DESIGN STORM DURATION	3 hour
RAINFALL INTENSITY	22.70 mm/hr
AREA	1310 m ²
COEFFICIENT - POST DEVELOPMENT	0.9

STORAGE VOLUMES

STORAGE VOLUME REQUIRED	80.60 m ³
STORAGE VOLUME AVAILABLE	85.50 m ³
INFILTRATION RATE (ASSUMED)	8 m/DAY
SOIL CONDITIONS	SANDY OVER LIMESTONE (GEOLOGY MAPPING)

STORAGE STRUCTURES

SOAKWELLS	12 No.	1.8m DIA X 1.8m DEEP	54.97 m ³
SOAKWELL INFILTRATION	12 No.	1.5m DIA AT BASE	30.54 m ³
TOTAL STORAGE PROVIDED			85.5 m ³

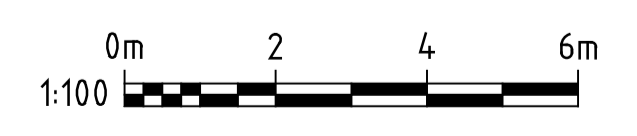
OVERFLOW

OVERFLOW LEVEL	2.80 m AHD (TBC)
LOCATION	SOAKWELL AT SOUTHERN CROSSOVER
FREEBOARD TO FFL	450mm

CAUTION: WORKS ARE NEAR EXISTING ASSETS
 THE PROPOSED WORKS ARE WITHIN CLOSE PROXIMITY TO AN EXISTING WATER CORPORATION ASSET AND PRESENTS A HIGHER RISK TO THIS EXISTING INFRASTRUCTURE.

ASSET PROTECTION CONTROLS MAY APPLY WHEN UNDERTAKING THESE CONSTRUCTION WORKS

THE CONTRACTOR SHALL COMPLETE A WATER CORPORATION ASSET PROTECTION RISK ASSESSMENT AND OBTAIN A CLEARANCE TO WORKS PERMIT AS REQUIRED. THE CONTRACTOR SHALL FOLLOW ALL PROTOCOLS AS NOMINATED BY THE WATER CORPORATION WHEN UNDERTAKING THE WORKS.



PROJECT: **234-238 SOUTH TCE SOUTH FREMANTLE**

19-2-2025	ISSUED FOR APPROVAL	MEG
	REVISION	BY

COPYRIGHT
 COPYRIGHT © PORTER CONSULTING ENGINEERS. ALL RIGHTS RESERVED.
 CAD DRAWING DO NOT MANUALLY ALTER. THE USER SHALL BE RESPONSIBLE FOR "SITE CHECKING" ALL DIMENSIONS BEFORE COMMENCEMENT OF WORK. THE CLIENT HAS LICENSE TO USE THIS DRAWING FOR THE PROJECT ONLY.
 ONLY PLANS WITH NUMERICAL REVISION REV '1' OR HIGHER AND PRINTED IN FULL COLOUR SHALL BE USED FOR CONSTRUCTION. IF THE PLANS PRINTED IN BLACK AND WHITE OR GREY SCALE IT IS NOT TO BE USED FOR CONSTRUCTION.

Porter Consulting Engineers
 Level 2 Kishorn Court
 58 Kishorn Road
 Mt Pleasant 6153 WA
 PO Box 9336
 Canning Bridge 6153 WA
 Tel (08) 9315 9955
 Email office@portereng.com.au
 www.portereng.com.au

CLIENT: **GLOWMARK NOMINEES PTY LTD**

DRAWING: **STORMWATER AND SEWER MANAGEMENT PLAN**
 STATUS: **FOR APPROVAL**

SCALE	Custom	DRAWING No.		REV No.		ORIGINAL DRAWING SIZE	
DATE	Feb-25		25-2-12/500	A	A1		
DESIGN	BIH						
DRAWN	MEG						
CHECK	APPD						

Proposed Motor Vehicle Wash

Application for Planning Approval



234-238 South Terrace, South Fremantle

November 2024

Application for planning approval
Lots 27, 29, 300, 301 (234-238) South Terrace, South Fremantle

apex
planning

CITY OF FREMANTLE
These Plans Form Part of

Application for planning approval

Lots 27, 29, 300, 301 (234-238) South Terrace, South Fremantle

DAP004/24

Prepared for Glowmark Nominees Pty Ltd

5 Dec 2024

DOCUMENT CONTROL

DESCRIPTION	DATE
241129 24-274 DA report - South Fremantle Car Wash.docx	29 November 2024

Apex Planning

Phone: 0416 672 501

Email: admin@apexplanning.com.au

Address: 3/128 Main Street, Osborne Park 6017

© 2024 Apex Planning Pty Ltd

All rights reserved. The information contained in this document and its appendices is confidential and intended solely for the use of the client for the purpose it was prepared. This document remains the intellectual property of Apex Planning Pty Ltd. This document may not be used, sold, reproduced or copied by any person without the prior written consent of Apex Planning Pty Ltd. The use or copying of this document in whole or in part without the written permission of Apex Planning Pty Ltd constitutes an infringement of copyright. No liability is accepted whatsoever for any unintended third party use or reliance on this report.

CONTENTS

1	INTRODUCTION	1
1.1	PRE-LODGEMENT ENGAGEMENT	1
2	LAND DESCRIPTION	2
2.1	LOT DETAILS AND ENCUMBRANCES	2
3	CONTEXTUAL CONSIDERATIONS	3
3.1	EXISTING SITE CONDITIONS	3
3.2	APPROVAL HISTORY	3
3.3	DESCRIPTION OF CONTEXT	4
4	PROPOSED DEVELOPMENT	6
4.1	SIGNAGE	7
4.2	TRAFFIC AND ACCESS	8
4.2.1	ACCESS ARRANGEMENTS AND SITE CIRCULATION	8
4.3	ACOUSTIC COMPLIANCE	9
5	STATUTORY PLANNING ASSESSMENT	10
5.1	METROPOLITAN REGION SCHEME (MRS)	10
5.2	CITY OF FREMANTLE LOCAL PLANNING SCHEME NO.4 (LPS4)	10
5.2.1	GENERAL DEVELOPMENT REQUIREMENTS	11
5.3	LPP 2.8 – FENCES	11
5.4	LPP 2.10 – LANDSCAPING OF DEVELOPMENT AND EXISTING VEGETATION ON DEVELOPMENT SITES	13
5.5	LPP 2.14 – ADVERTISING	14
6	CONCLUSION	16

APPENDICES

APPENDIX 1:	CERTIFICATES OF TITLE
APPENDIX 2:	DEVELOPMENT PLANS
APPENDIX 3:	TRANSPORT IMPACT STATEMENT
APPENDIX 4:	ACOUSTIC ASSESSMENT

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

1 INTRODUCTION

Apex Planning has prepared this application for planning approval on behalf of Glowmark Nominees Pty Ltd, the landowner of Lots 27, 29, 300, 301 (234-238) South Terrace, South Fremantle (hereafter referred to as the **development site**).

The proposal involves the establishment of a motor vehicle wash (with incidental dog wash) which would activate and improve a currently disused site in a key corner location, and provide convenient services to the surrounding community and travelling public.

The design of the facility adopts distinct design features to achieve a suitable built form response for its corner location, including an accentuated roof line with a variation of colours and treatments and canopy articulations in a coastal colour palette to reflect the character of the South Fremantle area.

The design and layout of the facility has been informed through expert traffic and acoustic input and the application is supported by expert assessments to demonstrate its suitability for establishment on the development site.

The application warrants the support of the City of Fremantle and the approval of the Metro Inner DAP.

1.1 PRE-LODGEMENT ENGAGEMENT

On 7th October 2024, Apex Planning and representatives of the landowner attended a pre-lodgement meeting with the City of Fremantle.

A number of matters were discussed relating to land use suitability, design, traffic/access, acoustic considerations, and the key elements of the statutory planning framework.

The feedback received at this meeting has informed the refinement and finalisation of the application materials.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

2 LAND DESCRIPTION

2.1 LOT DETAILS AND ENCUMBRANCES

The land subject of this application for planning approval is described in **Table 1** below.

Table 1: Lot details				
Lot	Plan / Deposited Plan	Volume	Folio	Ownership
27	1789	1965	368	Glowmark Nominees Pty Ltd
29	1789	1965	369	Glowmark Nominees Pty Ltd
300	302437	1965	368	Glowmark Nominees Pty Ltd
301	302437	1965	369	Glowmark Nominees Pty Ltd

The Certificates of Title are provided at **Appendix 1**. There are no encumbrances listed on the CTs which relate to the proposed development.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

3 CONTEXTUAL CONSIDERATIONS

The following sub-sections describe the contextual characteristics of the site and local area. Refer to **Figure 1: Aerial Photo** on the subsequent page, which illustrates the development site and surrounds.

3.1 EXISTING SITE CONDITIONS

The development site is comprised of four separate lots totalling 1,321sqm which collectively form a square shape. The site has frontage to South Street (north) and South Terrace (west).

The site was previously occupied by a boat sales premises and contains an existing single storey building along its southern boundary with a nil setback to South Terrace and to the southern boundary. The rest of the site contains hardstand previously used for the external display of boats.

The site is currently provided with existing crossovers to each street frontage which were utilised as part of the boat sales premises.

The site has historically been secured by a permeable cyclonic fence with gates to both crossovers to prevent unauthorised access during after-hours periods.

With regard to services and utilities, the site has access to power and reticulated water and reticulated sewer, with survey data indicating a sewer line currently traverses within the property along its eastern boundary from the south.

3.2 APPROVAL HISTORY

As noted in section 3.1 above, the development site was previously occupied by a boat sales premises which included:

- An existing single storey building along the southern boundary.
- Hardstand in other areas of the site used for the display of boats.
- A crossover to each street frontage.
- A permeable cyclonic fence along the site perimeter.

The applicable land use of Motor Vehicle, Boat or Caravan Sales remains an 'A' discretionary use in the Mixed Use zone of the City's Local Planning Scheme No.4 (**LPS4**) and is taken to be a currently approved and conforming use on the site in the configuration described above.

A historical image of the site when it was operational as a boat sales premises is provided as **Figure 2**.



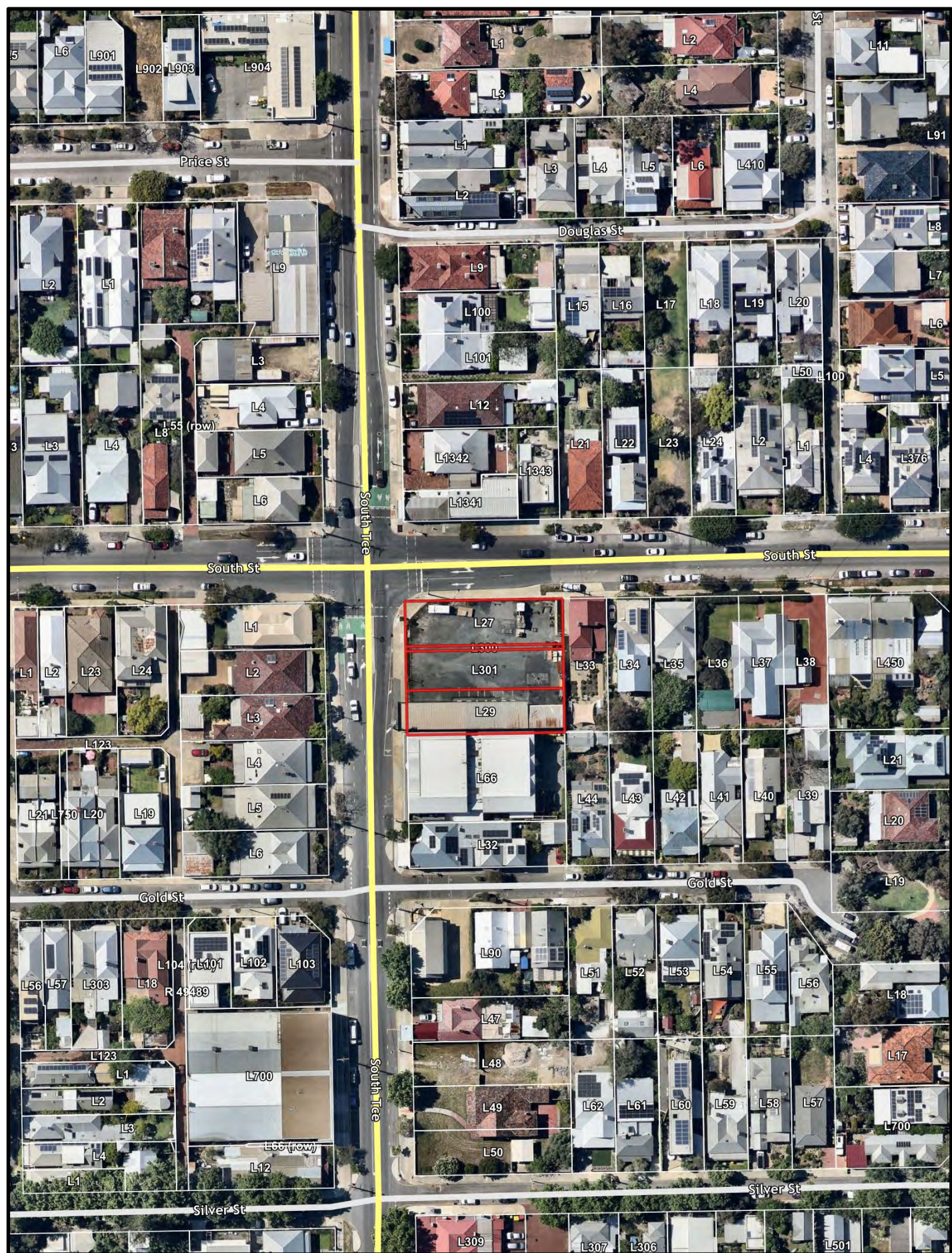


Figure 1: Aerial Photo

CITY OF
FREMANTLE
 These Plans Form
 Part of



Drawn: Alessandro Stagno
 Rev: 0

Lots 27, 29, 300, 301 (234-238) South Terrace,
South Fremantle

DAP004/24
 5 Dec 2024

Source: MNG Access
 Date: 25 November 2024

apex
 planning



Figure 2: historical aerial image of site as operational boat sales premises.

3.3 DESCRIPTION OF CONTEXT

In terms of regional context, the development site is within the locality of South Fremantle which is approximately:

- 16km south-west of the Perth CBD
- 1.5km south-east of the Fremantle strategic centre
- 12km north-west of the Cockburn secondary centre
- 250m east of the Fremantle boat harbour / marina

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

The development site is located at the south-eastern corner of the signalised intersection of South Street and South Terrace.

South Street is a key east-west transport route which connects South Fremantle (west) to Canning Vale (east). South Street in the vicinity of the site is classified as a Distributor B under the Main Roads state road hierarchy and operates under the default speed limit of 50km/h as a single carriageway two-lane road. In 2020/21, South Street west of South Terrace carried approximately 5,339 vehicle per day on a typical weekday.

South Terrace is a more localised transport route which runs between Fremantle (north) and South Beach (south). South Terrace is classified as a Local Distributor under the Main Roads state road hierarchy and locally operates under a posted speed limit of 40km/h as a single carriageway two-lane road. In 2021/22, South Terrace south of South Street carried approximately 8,931 vehicles per day on a typical weekday.

Application for planning approval

Lots 27, 29, 300, 301 (234-238) South Terrace, South Fremantle

apex
planning

In terms of its immediate surroundings, the subject site is:

- Adjacent to land zoned Local Centre containing commercial uses at the northern side of South Street, with residential uses at the eastern side of the local centre land.
- Adjoins residential property along its eastern boundary.
- Adjoins a two storey mixed use development along its southern boundary, comprised of commercial uses on the ground floor and residential uses at the upper level.
- Adjacent to residential uses at the western side of South Terrace.

The local area features a relatively mixed character of residential and non-residential uses, noting South Terrace includes precincts of Mixed Use zoned land containing shops, restaurants/cafes, and mixed use developments.

South Street also features a mixed character noting the existence of commercial zoned land comprising a Neighbourhood Centre approximately 300m east of the site, a local centre directly adjacent to the site on the northern side of South Street. South Street terminates at Marine Terrace to the west, which provides access to the marina containing a range of maritime uses/activities.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

4 PROPOSED DEVELOPMENT

The proposal involves the demolition/removal of what exists on the site and its replacement with a brand new motor vehicle wash comprising of three automatic wash bays, four vacuum bays, and an incidental dog wash. The development plans (including 3D images) are provided at **Appendix 2** for reference.

The facility is proposed to operate 24 hours, 7 days per week. The motor vehicle wash will generally be unmanned but will have a staff attending periodically for routine maintenance.

The proposal would activate and improve a currently disused site at a key corner location, and provide convenient services to the surrounding community and travelling public.

The development will retain and improve the site's existing crossovers to South Street and South Terrace, by shifting both crossovers further away from the signalised intersection for optimised separation.

The layout of the development is such that a one-way flow of traffic is encouraged, with most patrons anticipated to enter the facility via South Street. The three automatic wash bays have a stacking capacity of 12 vehicles which is anticipated to amply cater for periods of peak demand. In terms of car parking, the facility provides a total of three bays including an ACROD bay.

Automatic wash bays are typically considered superior to manual wash bays because of a very quick through-put (usually a matter of minutes) and because potential impacts can be attenuated by enclosing the units as has been done with this proposal.

The motor vehicle wash is designed such that it presents as a commercial building and addresses both street frontages with a structural built form response constructed of fibre cement panels using a feature canopy element to articulate the building in an oceanic colour to reinforce the coastal character of South Fremantle. The roof accentuates at the eastern side of the building to invoke a sense of presence. The building is positioned centrally within the site, providing a nil setback to the southern boundary (adjoining an existing boundary wall) and is set well back from other boundaries.

The benefit of a structural design approach is not only improved built form, but also improved acoustic attenuation as the automatic wash bays are fully enclosed and will utilise an automated rollerdoor system to reduce noise transfer while in operation.

The facility provides four vacuum bays along the South Terrace frontage screened with a 1.8m high rendered masonry feature wall and will adopt a form of automotive styled artwork to improve its presentation and celebrate the nature of the land use.

At the northern side of the wash building, a dog wash is provided. The dog wash is beneath a feature canopy and directly adjacent to a demarcated pedestrian connection to the South Street footpath which enables members of the community to comfortably walk their dogs to the site.

Application for planning approval

Lots 27, 29, 300, 301 (234-238) South Terrace, South Fremantle

An enclosed 1.8m high waste structure is provided at the north-western corner of the site to be constructed of concrete panels in a texture paint finish, with simple signage to address the intersection.

The periphery of the site contains landscaping buffers to improve the site's contribution to local amenity, as shown on the landscape plan which forms part of the drawing set. This includes 1m wide landscaping strips along both road frontages and a wider landscaping strip along the southern side of the wash entry.

3D images depicting the street views of the development are extracted below to visually demonstrate the design quality of the proposal.



View from the intersection of South Street / South Terrace.



View from South Terrace.

4.1 SIGNAGE

The development includes the following signage:

- On the elevation facing South Terrace, a 1.4m x 3.9m signage panel integrated at the top of the wall to contain simple lettering.
- On the elevation facing South Street, a 1.4m x 3.9m signage panel integrated at the top of the wall to contain simple lettering.
- On the rear elevation, three 0.9m x 2.3m signage panels to indicate the entry points of the automatic wash bays.
- On the waste enclosure, a 1.4m x 3.1m signage panel facing South Terrace and a 1.5m x 3.1m signage panel facing the intersection.

All signage is to contain simple lettering in a design style and colour palette complementing the wider development. An assessment against the City's signage policy is provided later in this report.

4.2 TRAFFIC AND ACCESS

The proposed development is supported by a Transport Impact Statement (**TIS**) produced by Transcore, provided at **Appendix 3**. The TIS is prepared in accordance with the requirements of the WA Planning Commission's traffic impact assessment guidelines.

The TIS demonstrates:

- The development would generate approximately 216 trips per day, comprised of 18 trips each during the weekday AM and PM peak hours. As is evident, the motor vehicle wash is not a use which inherently generates significant traffic during peak periods which means a more spread out and less impactful usage of the facility in relation to the surrounding road network.
- A queuing and stacking analysis which shows that under typical peak conditions, the queue back from each car wash lane will not spill out into the site's internal circulation system or the site's crossovers.
- The facility is designed to accommodate the satisfactory movements of B99 vehicles and a maximum 12.5m long service vehicle for waste collection as depicted on the swept paths contained in the TIS.

The TIS demonstrates that the proposed development is acceptable from a traffic and access point of view.

4.2.1 ACCESS ARRANGEMENTS AND SITE CIRCULATION

In terms of access arrangements, the development proposes:

- The repositioning and redesign of the site's existing crossover to South Street, shifting the crossover eastward further away from the intersection with a width of 6m and a 4.5m rollover kerb / line marking for larger service vehicles.
- The repositioning and redesign of the site's existing crossover to South Terrace, shifting the crossover southward further away from the intersection with a width of 5m.

The development substantially improves the existing situation by shifting both existing/approved crossovers further away from the intersection and establishing a use which does not generate significant traffic during the peak periods of the road network.

The layout of the development is such that a one-way flow is encouraged. An effective circulation system is easily achieved with wide driveways and intuitive crossover positioning.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

Servicing is anticipated to occur relatively infrequently given the nature of the land use. Waste collections would be undertaken during off-peak periods to afford optimal site area for manoeuvrability, as shown on the swept paths.

4.3 ACOUSTIC COMPLIANCE

Herring Storer Acoustics have undertaken an environmental acoustic assessment for the facility, noting the proposed 24/7 operation. The acoustic assessment is provided at **Appendix 4**.

Compliance with the *Environmental Protection (Noise) Regulations 1997* is readily achieved at all times with the incorporation of physical mitigation and operational management as outlined below:

- A 2.1m high solid fence to be provided along the entire eastern and a portion of the southern boundary as shown on Figure 4.1 of the acoustic assessment and depicted on the proposed development plans.
- A solid 1.8m high fence along the South Terrace frontage adjoining the vacuum bays.
- The automatic wash bays to be installed with rollerdoors to limit noise transfer while in operation.
- The number of automatic wash bays to be limited to 2 during the night period.
- The number of vacuum bays to be limited to 2 during the night period.

With inclusion of the above mitigation measures, the noise levels emitted by the facility are comfortably within the compliance range and achieve levels which are lower than the permitted maximum.

The acoustic compliance of the facility is therefore to a high standard and ensures the amenity of the local area is not adversely impacted.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

5 STATUTORY PLANNING ASSESSMENT

5.1 METROPOLITAN REGION SCHEME (MRS)

The development site and adjoining roads are zoned Urban under the MRS.

The development proposes a land use consistent with the Urban zone, and warrants approval.

5.2 CITY OF FREMANTLE LOCAL PLANNING SCHEME NO.4 (LPS4)

Under the City's LPS4, the development site is zoned Mixed Use R30. Refer to **Figure 3: Zoning Map**.

The objectives for the Mixed Use zone as set out by Clause 3.2.1(e) are extracted below:

Development within the mixed use zone shall:

(i) provide for a mix of compatible land uses including light, service and cottage industry, wholesaling, trade and professional services, entertainment, recreation and retailing of goods and services in small scale premises, including showrooms, where the uses would not be detrimental to the viability of retail activity and other functions of the City Centre, Local Centre and Neighbourhood Centre zones;

(ii) provide for residential at upper level, and also at ground level providing the residential component is designed to contribute positively to an active public domain;

(iii) ensure future development within each of the mixed used zones is sympathetic with the desired future character of each area,

(iv) ensure that development is not detrimental to the amenity of adjoining owners or residential properties in the locality, and

(v) conserve places of heritage significance the subject of or affected by the development.

The land use of Motor Vehicle Wash is identified as an 'A' discretionary use in the Mixed Use zone, meaning the use is capable of approval at the discretion of the decision-maker. The dog wash component is a minor and incidental element to the predominant use.

The proposed development is considered to meet the objectives of the Mixed Use zone and warrant discretion for the following reasons:

- The nature of the use is similar to the uses referenced under limb i of the objectives, noting there is reference to light / service industry and showrooms which are inherently car based uses. In addition to this, it is relevant to note by way of example that the Warehouse/Storage use is a 'P' use in the Mixed Use zone which is inherently a car based use.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

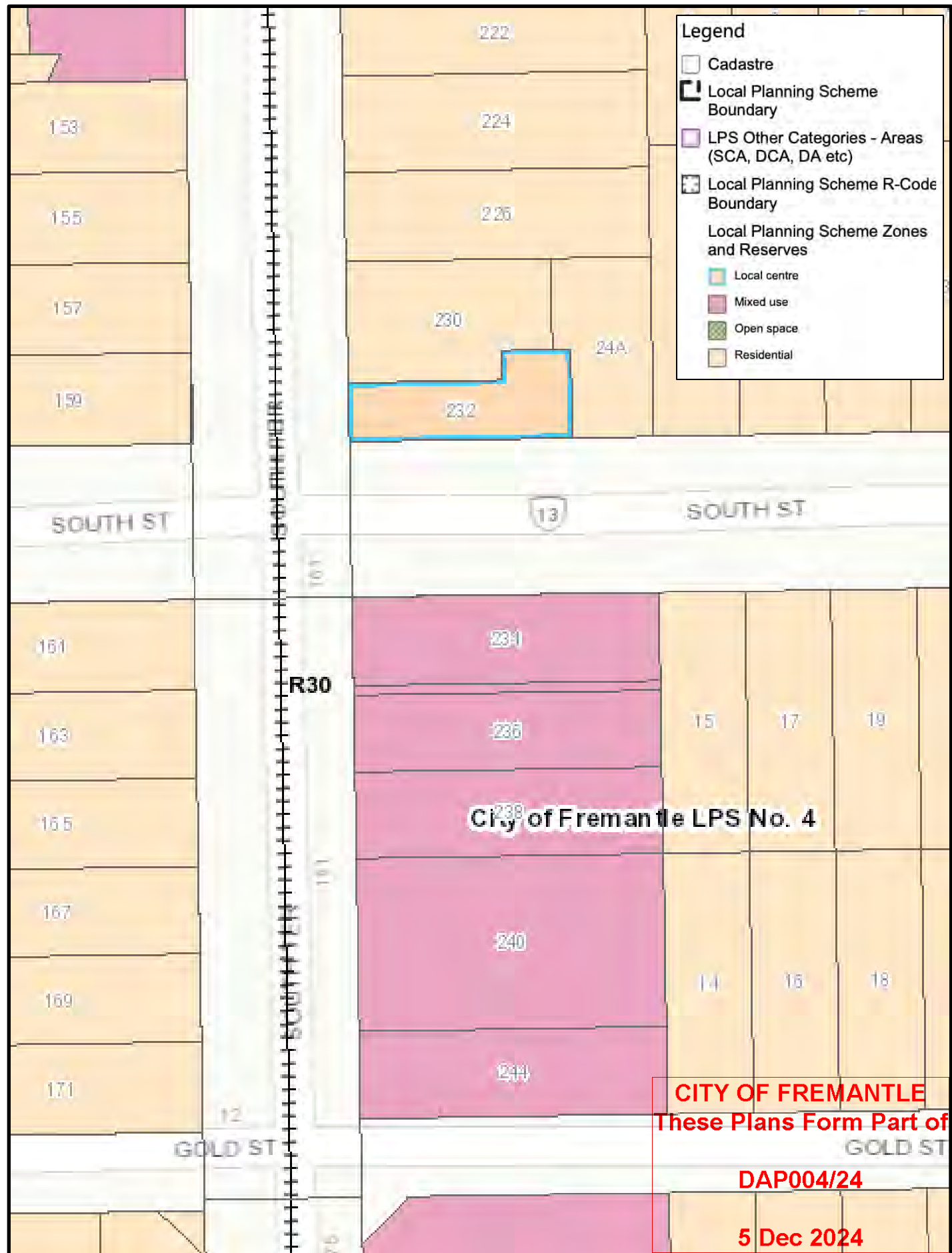


Figure 3: Zoning Map

Lots 27, 29, 300, 301 (234-238) South Terrace, South Fremantle



NORTH

Drawn: Alessandro Stagno

Rev: 0

Source: PlanWA

Date: 25 November 2024

apex
planning

- The site has been historically used as, and is currently approved for, a boat sales premises (Motor Vehicle, Boat or Caravan Sales). The nature and configuration of this use arguably provides for a less desirable planning outcome compared to the proposed motor vehicle wash which offers improved built form presence, more landscaping, a higher level of site activation, and improved traffic/access outcomes.
- The motor vehicle wash use and incidental dog wash will provide a valuable and convenient service to the community and travelling public, on roads which carry relatively high traffic volumes.
- The traffic assessment produced in support of the proposal demonstrates that the peak hour traffic generation of the proposed use is very low, and with the improved crossover positioning, is expected to result in acceptable outcomes in the context of the surrounding road network.
- The acoustic assessment demonstrates the facility will achieve acceptable noise levels at all times with physical and operational mitigation measures which are to be incorporated into the development.

The proposed Motor Vehicle Wash use is therefore appropriate for establishment on the site and warrants the City’s support.

5.2.1 GENERAL DEVELOPMENT REQUIREMENTS

Part 4 of LPS4 sets out general development requirements that apply to development on land in scheme area. **Table 2** below provides an assessment against the relevant requirements.

Table 2: general development requirements	
Scheme requirement	Response
Commercial and Industrial Development	
<p>4.6.1 Building Requirements All development shall comply with the building requirements as outlined in schedule 7 (local planning areas).</p>	<p>The development site is within Local Planning Area 4 – South Fremantle. The only applicable requirement under LPA4 is a maximum external wall height of 7 metres for the Mixed Use zone. The proposed development has a maximum external wall height of 6.7 metres which complies with the LPA4 requirements.</p>
<p>4.7.2 Vehicle Parking Requirements The following vehicle parking requirements shall apply to the following use classes: Motor vehicle wash: 2 bays / 1 wash bay No delivery bays or bicycle racks required</p>	<p>With three wash bays, six spaces are required. The development provides four vacuum bays plus three parking spaces (including an ACROD bay). The parking provision exceeds the minimum requirements of the scheme.</p>

5.3 LPP 2.8 – FENCES

The City’s LPP2.8 provides development standards for fencing within the scheme area.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

Table 3 below provides a response to the relevant policy criteria under LPP2.8.

Table 3: Response to LPP2.8 policy criteria	
Policy criteria	Response
1. Requirements applicable to all fencing except for properties on the heritage list or properties subject to a local area planning policy that includes provisions for fencing	
1.1 Fences within the primary street setback area as viewed from the street, and side fences abutting public open space reserves shall be visually permeable above 1.0m within heritage areas and 1.2m in other areas to a maximum height of 1.8m, with piers not higher than 2.0m.	No fencing is proposed to South Street, the primary street of the development (noting this would be the main entrance to the site).
1.2 Fences within a secondary street setback area and outside of a primary street setback area may be solid to a maximum height of 1.8m.	A maximum 1.8m high solid fence is proposed along South Terrace (the secondary street). The fence is set back from the street boundary by 1m (with a landscape strip in between the fence and the street boundary) and the exterior of the fence will contain artwork to improve its visual appearance.
1.3 Council may exercise discretion to vary the height of fences in the primary and/or secondary street setback area(s) where any of the following apply: a) the proposed fence height is consistent with the established pattern of fences within the streetscape b) minor variations are made necessary by virtue of the sloping topography of the site c) chain link, mesh, or garrison fences on a lot with non-residential land use and that are greater than 1.8m in height shall be permitted where in the opinion of Council, it is necessary to provide security to a commercial or industrial property, and are consistent with the established pattern of fences within the streetscape.	N/A
1.4 Council may permit solid fencing for a portion of the total length of the boundary within the primary street setback area, where surveillance between a habitable room window of the dwelling and the street and approach to the dwelling is available, and either of the following criteria is satisfied: a) where it is necessary to provide privacy screening where there is no alternative outdoor living area to the front setback b) where it is consistent with the prevailing streetscape.	N/A
• 3. Sightlines at Vehicle Access Points on Non-residential Land	
3.1 Where a fence is proposed on a lot containing wholly non-residential land use, abuts a vehicle access point, and is not of visually permeable construction, sight lines are to be provided in accordance with the relevant Australian Standard AS2890.2 (as amended).	The proposed solid fence along South Terrace is set back 1.8m from the crossover and therefore provides compliant vehicle sightlines as shown on the drawings.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

5. Side and Rear Boundary Fences and Screening Materials	
<p>5.1 Council will not approve side and/or rear boundary fences greater than 1.8m in height, or screening material that projects more than 500mm above the top of an approved fence unless the proposed fence/screening will not have any significant impact on adjoining properties by way of overshadowing, solar access, or loss of views.</p>	<p>A 2.1m high solid fence is proposed along the eastern and a portion of the southern boundary, as per acoustic recommendations.</p> <p>The fence to the eastern boundary:</p> <ul style="list-style-type: none"> • Does not create unreasonable overshadowing or diminishment of solar access, as the fence is along an east/west boundary. • Does not create loss of views, as the adjoining property is single storey and the location of the site does not have access to any notable views. <p>The fence to the southern boundary abuts the bin storage area and car park of the adjoining site, hence no adverse impacts would be created.</p>
<p>5.2 Council will have particular regard to comments made by neighbouring owners / occupiers of adjoining properties where consultation is carried out, and will only consider the criteria in 5.1 to be met where it is satisfied that no significant adverse impacts on the amenity of the neighbouring property will occur.</p>	<p>Noted.</p>

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

5.4 LPP 2.10 – LANDSCAPING OF DEVELOPMENT AND EXISTING VEGETATION ON DEVELOPMENT SITES

The City’s LPP2.10 provides guidance on the requirement and assessment of landscape plans and associated criteria.

Whilst it is noted that a landscape plan is not strictly required for the subject development under LPP2.10, a landscape plan forms part of the application materials as part of good practice.

Table 4 below provides a response to the relevant policy criteria under LPP2.10.

Table 4: Response to LPP2.10 policy criteria	
Policy criteria	Response
2. Landscaping	
<p>2.1 In the event a Landscape Plan is required the Landscape Plan must show existing trees or vegetation to be retained. The Landscape Plan shall be included as part of the approved plans and is required to be completed as part of the development. In this situation, the following condition shall be placed on the approval: “Prior to occupation of the development, all landscaping as shown on the approved plan shall be installed and thereafter maintained to the satisfaction of the Chief Executive Officer.”</p>	<p>The development site contains no existing trees.</p>
<p>2.2 In cases where landscaping is required, but the detail of that landscaping is not required to be submitted as part of a planning application, the</p>	<p>A landscape plan forms part of the drawing set which depicts planting treatments within the proposed landscape buffer strips along the street</p>

following condition shall be placed on the approval;

"Areas shown on the approved plan as landscaping shall be landscaped to a minimum of 80% coverage of the designated landscaping area at the canopy height using any combination of the following:

- Landscaped areas are to be fully reticulated or native water wise plants are to be used
- Trees of suitable species for urban locations
- Hedged shrubs not taller than 2 metres at maturity
- Lawn/ground cover/grasses
- Exposed ground treated with organic or inorganic mulch
- Landscaping is to be no higher than 0.75 metres within 1.5 metres of vehicular access points where a driveway meets the verge/public street

The landscaping shall be installed within 60 days of the occupation of the development, and thereafter maintained to the satisfaction of the Chief Executive Officer."

frontages and side/rear boundaries. The planting arrangements can be addressed to the specification and satisfaction of the local authority.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

5.5 LPP 2.14 – ADVERTISING

The development includes signage which require assessment against the requirements of the City’s LPP2.14.

The development includes the following signage:

- On the elevation facing South Terrace, a 1.4m x 3.9m signage panel integrated at the top of the wall to contain simple lettering.
- On the elevation facing South Street, a 1.4m x 3.9m signage panel integrated at the top of the wall to contain simple lettering.
- On the rear elevation, three 0.9m x 2.3m signage panels to indicate the entry points of the automatic wash bays.
- On the waste enclosure, a 1.4m x 3.1m signage panel facing South Terrace and a 1.5m x 3.1m signage panel facing the intersection.

The above signs are classified as Wall Signs under the policy.

Table 5 below provides an assessment against the standards set out by the City’s Signs LPP for wall signs.

Table 5: assessment against performance standards for signage	
Policy standard	Response
Wall, Fascia or Projecting Signs	

Application for planning approval

Lots 27, 29, 300, 301 (234-238) South Terrace, South Fremantle

apex
planning

2.4.1 Wall, Fascia or Projecting Signs are deemed acceptable where:

- (a) The advertisement does not project above the fascia of the building and does not exceed the frontage of the tenancy; and
- (b) The advertisement(s) are restricted to three signs per street frontage per tenancy.

None of the proposed signs project above the fascia of the building or exceed the frontage of the building.

The proposal does not exceed the limit of three signs per street frontage.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

6 CONCLUSION

This application for planning approval involves the redevelopment of Lots 27, 29, 300, 301 (234-238) South Terrace, South Fremantle with a brand new motor vehicle wash comprising three automatic wash units, four vacuum bays, and an incidental dog wash.

The information presented in this application for planning approval demonstrates the proposed development is acceptable and warrants the City's support for the following reasons:

- The proposed motor vehicle wash would replace an existing/approved boat sales premises on the site, and arguably results in an improved planning outcome in comparison to the site's currently approved development typology.
- The proposal will activate a currently disused corner site with a 24/7 use which will provide convenient and valuable services to the relatively high volumes of traffic using both frontage roads.
- The proposed motor vehicle wash is designed to a standard which is much better than the typical facility, using an enclosed structural built form typology presenting as a commercial building to address the site's corner location.
- The proposal is consistent with the applicable requirements of the planning framework, including the City's LPS4 and associated local planning policies.
- The development will improve the site's current access configuration, shifting the access points further away from the intersection of South Street and South Terrace.
- A traffic assessment has been prepared which demonstrates acceptable outcomes from a traffic generation, internal queuing, and internal circulation point of view.
- An acoustic assessment demonstrates acceptable and compliant noise levels received at the adjacent and adjoining properties with the incorporation of reasonable and achievable mitigation measures.

The application warrants the support of the City of Fremantle and the approval of the Metro Inner DAP.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

Heritage Comment – REV 1

Address: 234 South Terrace, South Fremantle
Application number: DAP0004/24
Proposal: Car Wash
Requesting officer: Jonathan Dornan
Date: 16/06/2025

The purpose of this heritage comment is to assess the changes to the place that are proposed in revised drawings received 13/6/25 for DAP0004/24 and the effect that they will have upon the heritage values of 234 South Terrace and the South Fremantle Heritage Area. The proposed changes include:

- Demolition of existing single storey office
- New single storey carwash building and associated service areas
- 1800mm high solid concrete fence for part of South Terrace boundary

HERITAGE LISTINGS

Heritage Place Name	-
State Register of Heritage Places	No
City of Fremantle Heritage List	No
City of Fremantle Heritage Area	South Fremantle Precinct Heritage Area
Local Heritage Survey	No
Management Category	N/A
Inherit database place record	South Fremantle Heritage Area - 22386
Further comment	This is NOT a Contributory Place

Heritage Comments:

Historic aerial photographs and sewerage plans show that in the 1940s – 1960s this site was largely open in character with a small brick building on the corner of the South Terrace and South Street and a tennis court. These structures were removed in the early 1970s when the site was redeveloped into its current configuration with a narrow, rectangular commercial building running along the south side boundary of the site and the remainder bitumen paving. This existing building and paving have little heritage value and do not contribute to the character of the heritage area.

The demolition of the existing buildings and structures on site is acceptable as it will not adversely affect the values of the heritage area. New infill development needs to positively respond to the character of the heritage area and comply with LPP3.6 Heritage Areas.

South Fremantle is a historic, largely residential but mixed-use suburb which was developed in the late Eighteenth and early Twentieth Centuries. Commercial buildings such as shops and service businesses were established in nodes along South Terrace and light industrial and warehouse development associated with the railway, port and maritime industries was scattered through the area. Development is low in scale but relatively dense and finely grained with a high proportion of heritage buildings forming significant heritage streetscapes.

234 South Terrace is a corner block which addresses two important local streets which have a distinctive but slightly different built form character:

- South Terrace has a mixed-use character of heritage and modern infill buildings with commercial, residential and former Industrial building typologies. Building scale is generally low with one and two storey buildings and some recently constructed three storey developments, including the adjacent property at 240-242 South Terrace. There is some variety in the front street setbacks with commercial buildings built up to the front boundary and houses with small front gardens but generally front setbacks are modest and side setbacks minimal. Large open gardens and forecourts are rare.
- South Street is more uniform and is characterized by single-storey heritage houses dating from the late Nineteenth to early Twentieth Century. Houses are set behind modest front gardens and have minimal side setbacks.

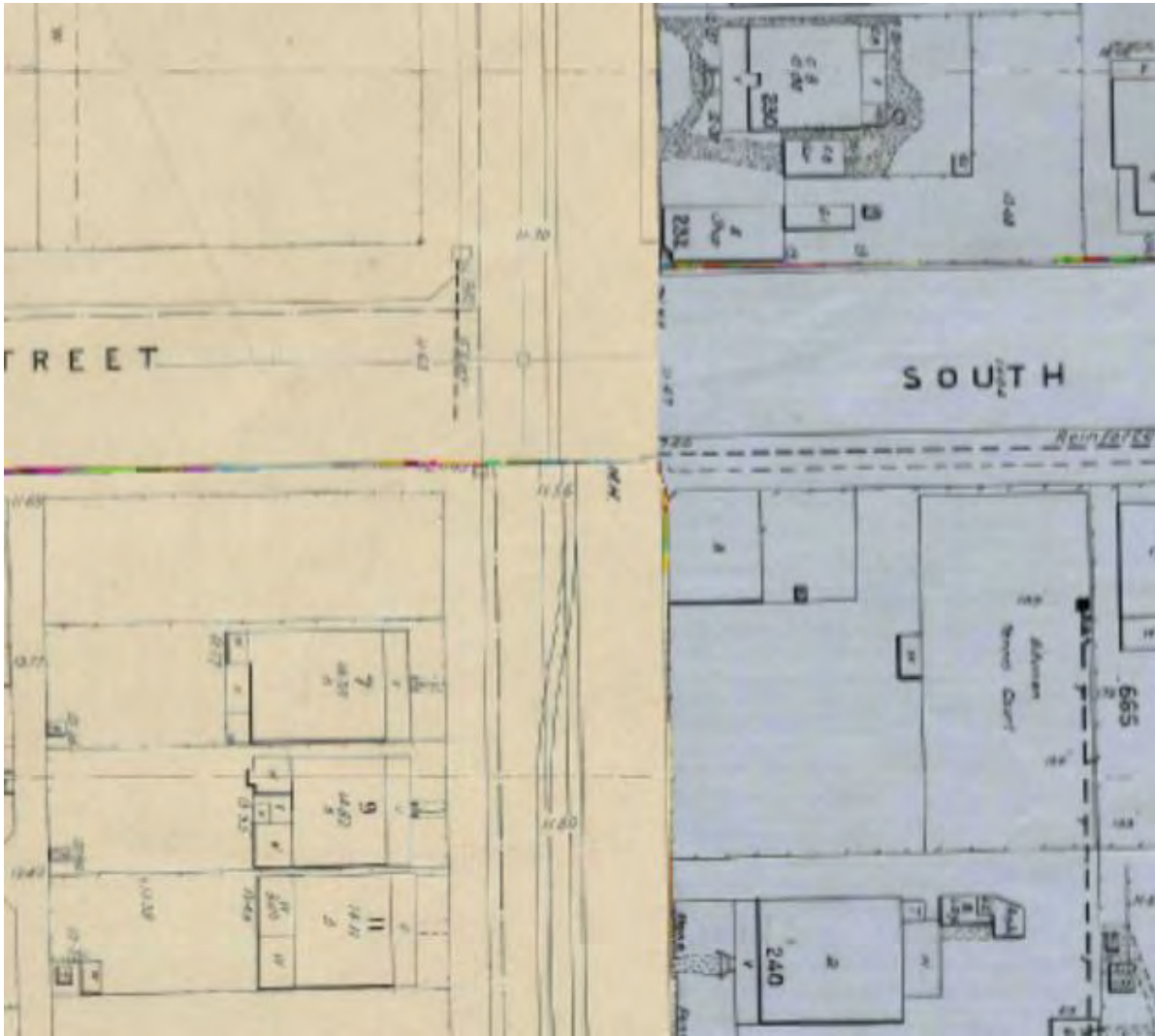
The proposed carwash building does not fully comply with LPP3.6 Heritage Areas because it is set back behind the established front setback to both South Terrace and South Street. However, the scale, form and articulation of the building has been modified following the receipt of initial heritage advice and it is now largely sympathetic with the general built form character of the heritage area:

- The building has a simple rectangular plan form
- The building is a relatively modest structure which is largely single storey but with a small two storey section at the South Street end
- The roof is a simple hip and gable structure that responds to the largely residential character of South Street and with the character of historic commercial buildings in South Terrace
- The scale of the building is reduced by the articulated verandah element which interprets traditional building form in the area while also providing amenity for building users
- Building materials are generally sympathetic with the painted and rendered historic commercial buildings in the area and the colour scheme is discrete using creamy limestone colours to blend with the use of natural stone in the area. Signage is also reasonably discrete and limited in size.
- The bin store and car vacuum bay wall elements are 1800mm high fence like elements but there are large gaps between these elements to allow carwash activity to activate the street

While a proposal that responded to the established street setbacks would have been preferable, on balance the proposed development will have only a minor impact on the heritage value of the south Fremantle Heritage Area.

RECOMMENDATIONS:

The works proposed in this application are acceptable as they will have only a minor impact on the heritage values of the South Fremantle Heritage Area.



234 South Terrace was a largely vacant site in 1950 with a small corner building and tennis court. Metropolitan Sewerage, Fremantle District Sheet 1522, 1954



234 South Terrace was redeveloped in the 1970s but remained largely vacant, Landgate Aerial 1981

Assessment comments dated 18 February 2025	
CoF comment	Applicant response
Zone objectives	
<p>The proposal would be inconsistent with the objectives of the Mixed Use zone detailed below.</p> <p><i>Development within the mixed use zone shall —</i></p> <p><i>(i) provide for a mix of compatible land uses including light, services and cottage industry, wholesaling, trade and professional services, entertainment, recreation and retailing of goods and services in small scale premises, including showrooms, where the uses would not be detrimental to the viability of retail activity and other functions of the City Centre, Local Centre and Neighbourhood Centre zones;</i></p> <p><i>(ii) provide for residential at upper level, and also at ground level providing the residential component is designed to contribute positively to an active public domain;</i></p> <p><i>(iii) ensure future development within each of the mixed used zones is sympathetic with the desired future character of each area;</i></p> <p><i>(iv) ensure that development is not detrimental to the amenity of adjoining owners or residential properties in the locality, and</i></p> <p><i>(v) conserve places of heritage significance the subject of or affected by the development.</i></p> <p>It is considered as being inconsistent for following reasons:</p> <ul style="list-style-type: none"> • The use would not be compatible with the existing surrounding uses consisting of shops, residential dwellings and offices/professional services. • The proposal is inconsistent with the desired future character of the area. • The proposed use has the potential to be detrimental to adjoining residential premises by way of noise and traffic impacts. 	<p>It is firstly noted the originally lodged proposal has been amended in a number of ways following meetings and discussions with the City. In support of this response package, the following information is provided:</p> <ul style="list-style-type: none"> • Amended plans (Appendix 1) • Amended Operation and Noise Management Plan (Appendix 2) • Peer review of TIS (Appendix 3) • Traffic technical note addressing right turn movements (Appendix 4) <p>The revised development is considered to represent a positive and realistic step toward the Mixed Use zone objectives compared to the historical and current use/condition of the subject site, taking into account the following:</p> <ul style="list-style-type: none"> • Objective (i) of the Mixed Use zone specifically references a number of non-residential uses which are identified as “compatible” uses, including light/service industry, entertainment uses, and small scale retail uses. The proposed Motor Vehicle Wash is a use which is compatible in this context, and will provide a needed service to the community noting most recent traffic data for the frontage roads shows a high level of vehicular patronage in the local area (8,931vpd along South Terrace and 5,339vpd along South Street). • From a contextual point of view, the subject site is part of a mixed use zoned precinct along South Terrace which contains an eclectic range of non-residential uses offering various services to the surrounding community (including co-located residential uses) at differing times of the day and night, creating a distinct level of activity in the area. The subject development bears similarities in this regard. There are numerous examples of cafes which open from early morning (many from 5:30am), bars/restaurants which operate into the evening and night period (many up to 10pm or midnight), and other shops and personal services with their own operating hours. These uses attract a level of vehicular patronage, evidenced by traffic volumes in the locality.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025

- The proposal would be detrimental to the heritage significance of the South Fremantle Heritage Area (considered to be a place of heritage significance).

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025

- The reduced operating hours and modifications to the Operational and Noise Management Plan (**ONMP**) demonstrate that compliant and acceptable noise levels would be received at nearby properties. Additionally, screen landscaping has been provided along the eastern and south-eastern boundary interfaces to provide a visual buffer between the facility and its neighbours, further improving the relationship of the development with its surroundings.
- The amended design of the facility brings the proposal in line with the City’s heritage policy and provides a more meaningful built form response to both frontage roads compared to existing development on the site (explained in further detail in the next section of this table).
- Traffic impacts have been comprehensively assessed through a peer review and additional technical note, demonstrating the traffic generation and general traffic flow characteristics of the use will not have any material impact to either of the frontage roads.

Heritage impact

The proposed development does not compliment the heritage significance of the heritage area or respond sympathetically to the heritage character of the surrounding established streetscapes of South Terrace and South Street.

In addition to the above, the development does not comply with Local Planning Policy 3.6 Heritage Areas as –

- The proposal does not respond sympathetically to the setting, form, massing, architectural style, detailing or material palette of the surrounding heritage streetscapes.
- it is set well back from the average street setbacks of the adjacent properties and does not address the street corner.
- it does not respond in a contemporary way to the form of the surrounding heritage buildings or their architectural style or massing
- it does not respond to the proportioning, fenestration patterns, detailing, materials or colours of the surrounding heritage buildings.

After consideration of various sketch options and consultation with the City’s heritage expert, the design has been amended to better align with LPP3.6 and to be more sympathetic to the intended heritage character of the local area.

Comparison images between the original proposal and the revised proposal are provided below, which show a significant improvement through reconfigured roof format, incorporation of feature gables, contiguous ‘wrap around’ feature verandah, and a more complementary colour palette.



CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025



The design approach adopted aligns with feedback from the City's heritage officer, which is partially extracted below:

While the best outcome would have been for the development to be built up to the front boundaries on South Terrace and South Street to reference traditional commercial development in the area as this will not suit the proposed use of the place then the new building should respond more to the surrounding residential development.

A couple of local examples along South Terrace which have guided the design changes to this proposal are provided below:



CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025



It is evident that the proposal is sensitively designed and will not detract from the intended character of the local area.

Traffic impact

There were a number of concerns regarding the potential impact from the proposed development on traffic movement in the area.

Vehicle Queuing and Rear-End Collision Potential

- a. Ingress from South Street (Eastbound Traffic):
 Queuing Analysis:
 - The TIS estimates a 95th percentile queue length of 2 vehicles per lane during peak hours, suggesting that queues will be contained within the site.
 - However, this analysis may underestimate real-world conditions, especially during weekends when car wash demand typically surges.
 Potential Impacts:
 - If on-site capacity is exceeded, vehicles may queue onto South Street, obstructing the eastbound lane and increasing the risk of rear-end collisions.
 - Vehicles slowing down to enter the car wash may disrupt the flow of traffic, leading to sudden braking and potential accidents.

Following discussions with the City’s engineer and in giving the feedback provided by the City genuine consideration, an independent peer review of the TIS prepared by Transcore was arranged.

The peer review has closely considered the trip generation rates, traffic assessment methodology, potential for queuing, surrounding traffic dynamics, queue analysis, and safety considerations.

Additionally, a technical note has been prepared which looks closely at the relevant right turn movements associated with the proposed development to demonstrate that the traffic flows in and out of the subject site are safe and acceptable.

Traffic and access considerations have therefore been assessed comprehensively to satisfy the concerns raised by the City’s officers.

Response to City of Fremantle assessment comments

Proposed motor vehicle wash – 234-238 South Terrace, South Fremantle

- b. Egress onto South Terrace (Northbound Traffic):
Exit Challenges:
- Vehicles exiting onto South Terrace intending to head north must navigate a potentially complex manoeuvre, especially if traffic volumes are high.
 - The proximity to the signalized intersection may result in insufficient gaps for safe entry, causing delays and potential traffic conflicts.
 - Vehicles exiting the car wash may slow down or hesitate at South Terrace, affecting through-traffic flow.
- Safety Concerns:
- Vehicles waiting for a safe gap to merge may cause unexpected stops, leading to rear-end collisions.
 - Extended wait times can lead to risky manoeuvres, such as darting into traffic, increasing the likelihood of accidents.

Shortcomings in the Transport Impact Statement

- a. Underestimation of Peak Demand:
- The TIS bases its analysis on average weekday peak hours, estimating 18 vehicle movements per hour.
 - Car wash facilities often experience higher demand on weekends/holidays and peak times. The TIS lacks a comprehensive analysis of these peak periods, potentially underrepresenting the true traffic impact.
- b. Inadequate Consideration of Surrounding Traffic Dynamics:
- The intersection of South Street and South Terrace is a critical junction with existing traffic complexities.
 - The TIS does not sufficiently account for:
 - Existing congestion levels during various times of the day.
 - The impact of additional ingress and egress movements on the overall intersection performance.
 - Potential spill-over effects on nearby residential streets due to diverted or overflow traffic.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025

Noise impact

Response to City of Fremantle assessment comments

Proposed motor vehicle wash – 234-238 South Terrace, South Fremantle

It is noted that additional information will be provided regarding noise and operational management. However, given the close proximity of residential dwellings, it is considered that there is potential for the amenity of the occupants to be impacted.

Refer to the commentary and assessment provided within the ‘zone objectives’ section of this table. Whilst there is “potential” for amenity impacts to nearby residential dwellings, it is essential to recognise that the subject site forms part of an established mixed use precinct which already contains a range of uses operating throughout the early morning, day and night to provide services to the surrounding community. This creates a distinct local amenity for those residential properties which are co-located with the mixed use precinct.

In consideration of the potential for impact, the operating hours of the proposal have been reduced from 24 hours to 6:30am-10:30pm which generally aligns with the times that activity occurs along South Terrace from existing uses. The ONMP has been modified to reflect the change in operating hours and provide further detail on how the facility will be managed to ensure no undue noise impact would be created to the neighbouring properties.

An acoustic assessment was prepared as part of the original application package which demonstrated compliant noise levels at the adjoining properties on a 24 hour basis. The operating hours have been reduced, but the amount of noise mitigation has remained the same. This is with the intention of ensuring noise transfer is mitigated beyond the “minimum” during the revised hours of operation.

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

12 June 2025

Summary of Submissions – Motor Vehicle Wash Development and Signage – DAP004/24

Date Commenced: 15 January 2025

Date Ended: 14 February 2025

Total Submissions Received: 91 Submissions

Consultation Method: Letters to owners/occupiers within a 200m radius, sign on site, MySay page, talk to a planner session, notice in the press.

No.	Submission	Applicant Response
1.	Total support. In no sense a conflict or a significant change from the boat yard or the current "training" yard. Need somewhere to wash my car where my run off doesn't go to the street / storm drains.	Noted.
2.	I think this is a bad location for a car wash. Dangerous for pedestrians, unsightly, brings too much traffic to the area, smelly and noisy. There are a lot of reasons why it would be detrimental to the suburb and would spoil the amenity of the area.	<ul style="list-style-type: none"> The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.
3.	Absolutely against this proposal & feel that it shows just how out of touch the planning approvals team at Freo Council can be. It's just not Freo. I'm very tired & am going to bed, so is all I can say now. A big NO WAY from me	<ul style="list-style-type: none"> The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.
4.	South Terrace represents a core area of Fremantle with residential housing, cafes, restaurants, bars, and more recently wellness studios like Pilates. A car wash located centrally amongst these businesses and residents detracts from the strip they've collectively contribute to. A better	<ul style="list-style-type: none"> The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and

	<p>location which is still central would be attached to the Shell petrol station.</p>	<p>is supported by expert traffic, acoustic, and stormwater assessments.</p> <ul style="list-style-type: none"> • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
<p>5.</p>	<p>I oppose the proposed use of this site as a car wash. It is an extremely busy corner with pedestrians, cars, bicycles, buses, traffic banks up along South Street and along South Terrace as many vehicles turn there. It is difficult as a pedestrian crossing the road there and the addition of cars trying to drive in and out of a car park on that site is going to add significantly to the traffic congestion. There is a bus stop on South Street 20 metres from that site. Residents who live near that intersection have to park on South Street as they have no off street parking. I oppose the use as completely out of character with the areas amenity and ambience, which is residential, small businesses like cafes, bakeries, bars, restaurants and retail. A car wash like this is usually situated on a main road, which would not have the access issues this site has. The car wash is not needed. There are already car washes on Victoria Street, Hampton Road (2) and in North Fremantle.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • The perceived oversupply of a land use is not a relevant town planning consideration.
<p>6.</p>	<p>I'm just writing as I'm concerned for this development. I've lived at my address for over 2 yrs in close proximity to the proposed site. In this timeframe I have witnessed and or helped many minor car accidents that occur on the corner of Sth terrace and Sth st - it is a busy area. I feel the proposed site will add further significant traffic pressure and add to safety concerns for pedestrians as well as contribute to the initial issue of accidents on that specific corner. As a seasoned educator, and Fremantle community member I hope my thoughts can be taken into consideration and highlight the importance of avoiding further OHS risk factors - particularly for the yearly summer season when the area is very busy - again with foot traffic and vehicle traffic. I trust the senior planner is well equipped and understands there are also establishments</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal

	<p>such as the local, Percy flints, kerfuffle etc that the local community and visitors love to embrace and travel to daily and walk to from public transport sites- ie freeo train station. The city of Fremantle is a popular destination and sustainable options for ppl to visit sth freeo via walking, bike riding etc should be encouraged by implementing site proposals that do not encourage further traffic congestion. Thank you for your time and careful consideration with my feedback.</p>	<p>impact to the road network and acceptable access arrangements.</p> <ul style="list-style-type: none"> • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
7.	<p>I am disappointed to find that this site would even be considered for this carwash proposal. It is not in keeping with the heritage status of South Fremantle or the cities green ideas of reducing vehicle movements within the City area.</p> <p>The South Street x South Terrace intersection is one of the most heavily used entry points to the city for both private and public transport. This intersection see's very heavy traffic use which not be complimented by attracting more vehicles to the area. The entry off of South Street one bus length back from the lights on South Street may cause confusion to drivers turning left and if the car wash is full there appears to be no space to deal with the backlog of vehicles that might be waiting pushing them back onto South Street.</p> <p>The exit onto South Terrace, from looking at the plans, would only facilitate a left hand turn putting more vehicles through South Fremantle with no option but to use side streets as rat runs to return North and East unless they travel the full length and exit via Douro Road which already see's very high volumes of traffic.</p> <p>The area is already well serviced by carwashes from this address being Rhino car wash Hampton Rd1.8km's, BP Queen Victoria St 2.8km, Magic Hand carwash 5km Stirling Highway, Super Soakers South Street 4.9km. There is a plentiful supply of car wash facilities already within the area.</p> <p>Fremantle is trying to be an entertainment, arts and social hub by offering something different from other parts of the metropolitan area. A place where you can go explore not just drive in wash your car then leave. This proposal adds no value to the local economy as people will come wash their car then leave that is assuming they will come at all as these car wash facilities can easily be found in any other number of suburbs.</p> <p>It is also not in keeping with the cities own South Fremantle heritage project being completely out of step architecturally to the surrounding buildings. This is a poorly placed being on a heavily congested intersection and out of character for its surrounds. The car wash project</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.

	<p>is a strange addition to a city centre and would be better located in an industrial area.</p>	
<p>8.</p>	<p>Not at all in tune with the area, will cause traffic jam, unnecessary noise pollution as well as being an absolute an eye sore! Please, no 🙅🙅🙅</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.
<p>9.</p>	<p>The plans for this site would activate an unsightly and under-utilised space in semi-industrial spot, that would not make great residential housing. A car wash with dog wash would provide facility for locals that do not have much off-site parking and reduce the incidence of washing cars on roads that enable pollutants to progress to the stormwater drainage. Available facilities are limited locally to the Hampton Rd site, where the auto-wash is substandard. The evaluation of this proposal seems to consider the traffic and noise issue. It is important that environmental issues are also considered. Consideration of energy use (I don't see any solar panels in the plans), water re-use, biodegradable detergents, pressurised vs heated water application, waste water disposal. The facility has a big "plant" section to it, and it should be</p>	<p>Noted.</p>

	<p>considered whether that is large enough to house a 5 star-sustainable facility. None of this detail appears in these plans and the council should expect to see what is intended in this respect even if the approvals for that are required to be issued by other agencies.</p>	
<p>10.</p>	<p>This is a terrible terrible idea for such a spot. As always... Do better free council, you've ruined central, don't tarnish past south st</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
<p>11.</p>	<p>Definitely not! Too busy an intersection, two car washes very close by and thus location is for foot traffic shops, bars, cafes and this would greatly detract from the feel of the "strip"</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
<p>12.</p>	<p>I am writing to express my strong objection to the proposed 24/7 car wash facility planned for the site located at 234 South Terrace, South Fremantle. I have serious concerns regarding both the immediate and long-term impact this development will have on me personally, my neighbours, the community and the surrounding area. Firstly, I am deeply dismayed that this proposal appears to be in stark contrast to the values and wishes of the South Fremantle community. This location, which is within a predominantly residential area, with some</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.

small surrounding local businesses that are contained and encompass a 'wholesome neighbourhood feel', is simply not suited for such a commercial venture. It brings nothing that invokes 'Fremantle' nor does it bring a necessary or niche retailer to the area.

I believe this car wash is a ludicrous use of this 'mixed use block' and will have significant negative consequences for the following reasons and many more I'm sure.

1. 24/7 Operations: The proposal to operate the car wash 24 hours a day, 7 days a week is a major concern. This will cause ongoing disruption for nearby residents, including noise, light pollution, and traffic. The impact of these issues on the quality of life for those living nearby cannot be overstated.
2. Traffic and Access: The additional traffic generated by the car wash will likely cause severe congestion on South Terrace and South Street. It is highly probable that traffic flow will be disrupted, resulting in blocked driveways and difficulty accessing garages for residents in the area, including myself. This is a clear concern that has not been adequately addressed in the proposal. It will also affect the walkability of the boundary paths with so many cars turning in and out of the entry and exit. I have already witnessed multiple crashes and accidents on this intersection and do not think that introducing a steady stream of extra cars is smart in any way.
3. Noise: The noise generated by such a facility—whether from the car wash machinery, vehicles, or other operations—will be intolerable and incessant for those of us living in close proximity. I do not believe that the suggested fencing and roller doors will mitigate this issue in any meaningful way.
4. Aesthetic and Community Impact: The car wash proposal adds no value to the neighbourhood's aesthetic or character. It is out of touch with what residents in South Fremantle want for their community I believe. There is no alignment with the area's unique cultural identity, and the proposed development does not contribute to the sense of community spirit, or vibrancy that South Fremantle is known and loved for.
5. Environmental Concerns: There is little to no mention in the proposal of the potential environmental impact of this facility, particularly regarding chemical runoff and airborne pollutants from sprays. These chemicals could negatively affect the health of residents and the surrounding environment, and these risks need to be seriously

- Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
- A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
- Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.
- Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.
- With regard to environmental concerns, it is essential to recognise that motor vehicle wash facilities are purpose-designed facilities with specialised water treatment equipment and require a trade waste permit from Water Corp prior to operation. Conversely, vehicle washing at a private premises occurs without any of these controls, with runoff containing chemicals entering the drainage system. As the autowash bays have self-closing doors on both sides, no spray drift will occur.

	<p>addressed.</p> <p>6. Long-Term Consequences: The car wash will also have a detrimental impact on property values in the area. The noise, traffic, light, and other disturbances will likely make the area, particularly my neighbouring property, less desirable for future buyers, and this will inevitably lower the potential sale price for local residents.</p> <p>7. I would invite some personal reflection also, would you want to live next door to this? My guess is NO, so why inflict it on rate paying members of this community. A much better option for this site would be another boutique build similar to 240 South Terrace that would allow more people to live in and enjoy a much loved part of Perth let alone Fremantle. There is a housing crisis that this could in time help with. Having shop sites on ground level also encourages small business and community interaction which is much appreciated.</p> <p>As a resident and member of the community, I am gobsmacked that the council thinks this is an appropriate use of this site.</p> <p>This development will erode the character, enjoyment and livability of South Fremantle. I strongly believe that this type of business is more suited for a more industrial area, such as the southern end of Hampton Road, rather than in a quiet, residential part of our neighbourhood. Not to mention that there is ALREADY a car wash on Hampton Road, plus one in North Fremantle, Mosman Park and Melville to name a few within a 3 to 15 minute radius!</p> <p>I urge the council to reconsider this proposal and take into account the overwhelming concerns raised by local residents, myself included. The long-term implications of this development are too great to overlook, and I will be actively advocating for this proposal to be stopped.</p> <p>Thank you for your attention to this matter. I look forward to hearing from you soon and hope that the council will take the necessary steps to preserve the integrity of our community.</p>	
13.	<p>The consultants for the proposed development on the neighbouring site to the immediate west of our property have briefed us as to what is being proposed. In view of the fact that the site was already operating as a commercial business, has not been overly well maintained over recent years, involves a boat yard and now a training centre which has a fully covered aged bitumen surface and a contemporary showroom style building on the southern boundary what is being proposed is a brand new development which will be subject to modern day standards in terms of noise protection measures and other requirements. This would</p>	Noted.

	<p>appear to us to represent an improvement in aesthetic appearance and an appropriate use given that many vehicles in the locality are forced to park on the street and will then be capable of being washed without the chemical residue running into the storm water drainage system which is surely an improvement from an environmental perspective.</p>	
14.	<p>Our company is the registered proprietor of the subject site to which the Application to the DAP applies.</p> <p>If the Application is successful, the car wash operator which would construct the development and operate the business is well known to us and already operates five other car wash sites in the Perth metropolitan area which use the latest technology.</p> <p>We were very pleased with the nature of the proposed business introduced to us by the operator which only involves three automatic car wash bays and the latest vacuum technology which generates substantially less noise than older technology facilities.</p> <p>The automatic bays would generate less active interface than a traditional car wash business and the design for those bays ensures containment of noise emissions.</p> <p>Apart from these observations the general presentation of the design for the proposed development represents a vast improvement on the current appearance of the site and will bring at least some landscaping to the corner site whereas the current appearance is somewhat lacking.</p> <p>A number of people in the locality, including apartment dwellers, have dogs and a number of those dogs go for a swim along the local beaches. The inclusion of a dog wash in the design will provide local residents with the ability to readily wash their dogs on the way home.</p> <p>Looking at a before and after scenario the proposed development is clearly an improvement on what exists at present.</p>	Noted.
15.	<p>I object to this type of use. It is not in keeping with the environment ethos of South Fremantle. This section of town is becoming quite vibrant and such a development will destroy that by giving it a light industry feel. It will certainly make us strongly consider moving the shop away from the area.</p> <p>Additionally, It's already quite a difficult intersection - it's hard to turn right from south terrace into our driveway depending on how the traffic backs up, this will make it harder a more dangerous.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert

		<p>traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.</p> <ul style="list-style-type: none"> • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
16.	<p>Hi. I do not think this type of business is appropriate for the local area. We have mostly restaurants, coffee shops and small boutique shops with such things as clothing, bakeries and jewellery etc. The added parking traffic will also strain an already difficult area. We love our local businesses. Surely there is a better location for a car wash. Definitely not appropriate</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • A Motor Vehicle Wash is not a use which inherently generates parking demand, as patrons have no reason to remain onsite after washing or vacuuming their vehicle.
17.	<p>The first and most obvious question is "Does the South Fremantle community need an additional Carwash/Dogwash"? The undersigned believe that the proposed Carwash will have an immediate and long term negative impact on the area and is neither needed nor wanted. CoF should refuse the development application based on the following grounds:</p> <p>1. The area is well serviced by three existing Carwash facilities:</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and

- Rhino Car Wash facility near the Hampton/Douro Road traffic lights. (25,500 per week*)
- Magic Hand Car Wash on Stirling Hwy, North Fremantle (21,400 per week*)
- SuperSoakers Car & Dog Wash on South St O'Connor (28,300 per week*)

2. The proposed facility is very ugly and out of place on South Terrace. The appearance is "low cost industrial" at best and totally out of character with the existing street-scape. The painted cement board and lightweight steel frame will not age well and are not suited to an area where heritage is taken very seriously by the community.

3. The proposed facility is very water and power intensive and it will create noise 24/7. The proposal notes that the peak use of car washes is on weekends (and holidays) when residents spend quality time at home. So the maximum carwash noise disturbance will have the maximum impact on residents. The proposed CarWash is not environmentally friendly and out of touch with a community who do not prioritise pride in car ownership over more important social issues.

4. At the lower end of projected number of cars washed per day, the facility will use approximately 150kWh of electrical power every 24 hours. This could double during peak use periods. The Carwash proposal does not mention any installation of Solar panels to reduce the impact on Fremantle's aging power grid.

5. At the lower end of projected number of cars to be washed, the facility will use approximately 7.5 tons of water per day. The Carwash proposal does not indicate any waste water treatment or recycling of water. Untreated toxic water will be dumped directly into the sewer lines placing the burden for effective treatment on ratepayers. Waste water from car washing contains lead and other toxic pollutants.

6. The proposed facility is fully automatic and never staffed so it will make zero contribution to existing businesses in the vicinity.... I.e lunches, coffee etc

7. The large plot of land needed for the CarWash could alternatively be developed into up to 10-15 apartments and additional shops, a land use that is fully aligned with CoF's stated policy of environmentally responsible inner city mixed development.

8. South Terrace has great potential for creative mixed development with an emphasis on higher density affordable housing, foot traffic, public transport and cycleways. We assume the CoF recent investment in

is supported by expert traffic, acoustic, and stormwater assessments.

- Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
- A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
- The perceived oversupply of a land use is not a relevant town planning consideration.
- Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.
- With regard to environmental concerns, it is essential to recognise that motor vehicle wash facilities are purpose-designed facilities with specialised water treatment equipment and require a trade waste permit from Water Corp prior to operation. Conversely, vehicle washing at a private premises occurs without any of these controls, with runoff containing chemicals entering the drainage system. As the autowash bays have self-closing doors on both sides, no spray drift will occur.

	<p>traffic calming and traffic reduction will continue and the result should see more through traffic using Hampton Road or Marine Parade and less traffic using South Terrace. (Currently only 8,500 cars per week*) Approving a CarWash on a potentially valuable mixed-use location on South Terrace makes no sense and DAP 004/24 should be refused.</p>	
18.	<p>As a concerned resident of South Fremantle, I would like to formally lodge my objection to the proposed development of a new carwash at 234-238 South Terrace (DAP 004/24). While the applicant may suggest that the project brings some benefits, I believe the long-term consequences for the local community and environment would be overwhelmingly negative.</p> <p>There are several key reasons why this proposed carwash should not be approved:</p> <p>1. Existing Market Saturation South Fremantle is already well-served by a number of carwash facilities within close proximity. There are three established businesses in the immediate area that cater to local demand: Rhino Car Wash near the Hampton/Douro Road intersection Magic Hand Car Wash on Stirling Hwy, North Fremantle SuperSoakers Car & Dog Wash on South St, O'Connor These existing businesses already meet the demand for vehicle washing services in the region, meaning there is little to no need for another facility. An additional carwash would only create unnecessary competition and likely reduce the viability of these already established services, many of which are locally owned and integral to the community.</p> <p>2. Visual and Aesthetic Concerns South Terrace is a significant street in Fremantle, with a vibrant, heritage-conscious community. The proposed carwash, with its utilitarian design featuring painted cement board and a lightweight steel frame, would starkly contrast with the character of the area. It would undermine the visual and historical integrity of the streetscape, which is highly valued by local residents. Such a facility does not fit within the aesthetic of South Terrace, nor does it respect the community's commitment to preserving the heritage of Fremantle.</p> <p>3. Environmental Impact and Resource Consumption One of my major concerns with the proposal is the significant environmental impact the carwash would have. According to the development plans, the facility will consume around 150 kWh of</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • The perceived oversupply of a land use is not a relevant town planning consideration. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area. • With regard to environmental concerns, it is essential to recognise that motor vehicle wash facilities are purpose-designed facilities with specialised water treatment equipment and require a trade waste permit from Water Corp prior to operation. Conversely, vehicle washing at a private premises occurs without any of these controls, with runoff containing chemicals entering the drainage system. As the autowash bays have self-closing doors on both sides, no spray drift will occur. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic

electricity per day, which could double during peak usage periods. This excessive energy consumption would put additional strain on Fremantle's aging power grid and is incompatible with the city's sustainability goals. Furthermore, the carwash is expected to use tons of water each day, with no provisions for water recycling or wastewater treatment. Wastewater from car washing can contain hazardous chemicals such as oils, lead, and other pollutants. The discharge of untreated wastewater directly into the sewer system would place an unfair burden on local ratepayers and the local government to manage these contaminants effectively.

4. Noise Pollution

The carwash would be operating 24 hours a day, seven days a week, which raises significant concerns about noise pollution. Noise levels, especially during weekends and public holidays when residents are likely to be home, would be intrusive and disruptive. As the facility is fully automatic and self-service, there will be no on-site staff to manage noise, making it even more difficult to control.

5. Lack of Economic Benefit for the Community

The proposed carwash is fully automated, meaning it will not employ local staff or create opportunities for local businesses, such as cafes or retail shops, to benefit from increased foot traffic. South Fremantle thrives on a sense of community and small local businesses that cater to residents and visitors alike. A self-service, automated carwash, operating without staff and offering no interaction with the community, will do little to enhance the local economy.

6. Alternative, More Sustainable Development

The site currently earmarked for the carwash offers a significant opportunity for a more beneficial development, such as mixed-use housing or retail. The proposed development could include 10-15 apartments and shops that would promote greater urban density in line with the City of Fremantle's sustainable growth strategy. This would support the local economy, provide much-needed housing, and encourage the type of vibrant, walkable environment that the City is working towards.

7. Alignment with City of Fremantle's Vision

South Fremantle is an area undergoing significant transformation, with efforts being made to reduce traffic, promote sustainable transport options, and encourage more community-oriented development. A carwash at this location does not align with these objectives. The traffic

assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.

- A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.

	<p>data shows that only 8,500 vehicles use South Terrace weekly, and the City's traffic calming initiatives will likely reduce this further in the coming years. Encouraging car-dependent businesses like a carwash is counterproductive to the broader vision of creating a more sustainable, pedestrian-friendly community.</p> <p>8. Public Sentiment and Community Feedback</p> <p>It is clear from discussions with my neighbors and fellow community members that there is little support for this proposed development. The negative impacts on local residents, the environment, and the future of the neighborhood are significant concerns for many in the community. I believe that approving this development would not only go against the wishes of the residents but could also have a detrimental effect on Fremantle's reputation as a forward-thinking and sustainable city.</p> <p>Conclusion</p> <p>For all of the above reasons, I strongly urge the Development Assessment Panel to refuse the application for the proposed carwash at 234-238 South Terrace. This development is incompatible with the character of the area, offers minimal community benefits, and poses significant environmental and social risks. I believe the land could be put to far better use, such as for mixed-use residential development, in line with the City of Fremantle's commitment to sustainable and community-oriented growth.</p> <p>Thank you for considering my concerns.</p>	
19.	<p>The submission, while advising that increased traffic in the area will be minimal, doesn't address the current traffic situation at the corner of South St & Tce. Both streets are single lane, barring the provision of short turn left lanes. Both streets consistently everyday, at all hours, have a large line of cars waiting to turn when signals change, that back up past both entrances to the proposed carwash. Any car entering/leaving will cause traffic backing up even further, and if trying to enter from the far side of the road, will stop traffic in both east/west and north/south, due to traffic not being able to navigate around the turning vehicle, in part due also to parking available on these roads, preventing cars being able to negotiate around turning vehicles. Traffic in/out of the previous boatyard was minimal each day, with only one or two vehicles arriving/leaving each day.</p>	<ul style="list-style-type: none"> • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
20.	<p>Hi there - I don't think the proposal to use the space here as a carwash should be approved. The space in question is on the corner of two busy</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of

	<p>roads and one of my concerns is drive in drive out will add to already busy traffic. it will add danger to pedestrians who cross nearby to the entrances of this. I am concerned about the environmental impact of having waste water with chemicals/detergents in it draining away on site add spray for passers by. I have a local retail shop very close to this and am concerned at the impact this will have on my footfall and the experience my customers will have- i feel many will choose to stay away with a business use like this adjacent. Typically car washes are located away from residential and retail high streets and approving this will be very detrimental to the currently charming local area and community here.</p>	<p>LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.</p> <ul style="list-style-type: none"> • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
21.	<p>We see this as a great utilisation of this site which will give useful amenities to the residents and ratepayers.</p>	<p>Noted.</p>
22.	<p>This is an inappropriate use for a key site at the interface between Fremantle and South Fremantle. These types of businesses are best suited to the town periphery and co-located with petrol stations, shopping centres and the like - not at the gateway to one of Fremantle's most beautiful and dynamic areas.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.
23.	<p>I am against this proposal as it does not fit in with the community minded regeneration of South terrace. I would prefer a series of shops and cafes in my neighbourhood particularly on a high profile site such as this. I am happy to travel to a light industrial area to wash my car</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.

<p>24.</p>	<p>I am NOT supportive of this application as I feel that this use is not appropriate for this site.</p> <p>This is a busy intersection generally, and the development of a carwash at this site will draw additional traffic to the area and will compromise the safety and compound the challenges associated with this intersection for pedestrian, cycle and vehicular traffic.</p> <p>A business of this nature makes no contribution to the South Fremantle / Fremantle micro economy (ie attracting clients and providing opportunity for a broader experience) with minimal employment opportunities for our community providing a negligible if not negative social license to operate.</p> <p>Similarly I don't believe such a development to be the best - or even an appropriate - use of this prime block, particularly given it's "Corner Influence" and giving consideration to surrounding developments. It will rather have a negative impact on the streetscape and surrounding businesses. A more appropriate business would be professional services, specialist retail, or an office/residential development which makes a contribution economically and socially to our community.</p> <p>I believe such a development is better placed on the 'fringes' of the city, rather than the midst of the principle Fremantle-South Fremantle connecting route.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
<p>25.</p>	<p>I would oppose this development. This type of commercial development is out of place in a residential and developing hospitality/retail precinct. There is no shortage of better suited vacant commercial sites in the area closer to Rockingham Road, as well as an existing car wash not far away on Hampton Road. Not sure what the traffic management plan is or how the council will deal with it. What car numbers does the business plan estimate? Cars will be fed into the car wash from South Street and exit onto South Terrace, either wanting to turn right to get back onto South Street or turning left on South Terrace (a light traffic zone) and then turning left up Lefroy or continuing along South Terrace. This is completely the wrong type of commercial development for South Terrace. Thank you.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the

		<p>relatively high local vehicular patronage with a valuable and needed service.</p>
26.	<p>Very strange choice of use for prime site on important Fremantle location. This area links south Freo and Freo together and putting something that could literally be built anywhere else is very odd and risks bringing the vibe whole area down. Build this in industrial area along Hampton instead. Do not support at all in any way shape or form.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
27.	<p>This is a terrible idea and use of land that will cause traffic congestion and safety issues for cars and pedestrians. This precinct has - including through Fremantle investment - been designated as pedestrian friendly. This development will increase car traffic and the risk of that traffic congesting cars entering and exiting one of the busiest intersections in the vicinity.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.

28.	<p>I have read the submission carefully and do not believe this is in the interest of the surrounding businesses and residences. A car washing business as shown in the proposal does not fit in with the look and feel of the South Terrace area, and will add car traffic to what is largely a bike riding environment. There are already ample car wash businesses around Fremantle, a drive through option at the BP station on Queen Victoria Street, a hand car wash service on Hampton Road as well as the Magic Hand Car wash in North Fremantle. In addition there are ample other car wash locations in Bicton, Palmyra and O'Connor.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.
29.	<p>Totally wrong business for this site and area. Dangerous intersection already, this will exacerbate the current situation.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.

		<ul style="list-style-type: none"> • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
30.	<p>The proposal to establish an unmanned car wash on the corner of a busy area in historic South Fremantle raises significant concerns. First, South Fremantle is renowned for its heritage character, with unique architecture, vibrant community spaces, and a pedestrian-friendly atmosphere. Introducing a facility such as an unmanned car wash risks detracting from the aesthetic and cultural significance of the area. The visual impact, combined with the potential for increased noise and activity, is likely to disrupt the historic charm and appeal of the neighbourhood.</p> <p>Second, the chosen location on a busy corner exacerbates traffic concerns. An unmanned car wash would generate increased vehicle movements as drivers queue, enter, and exit the facility. This would likely cause congestion (which is currently already a problem), pose safety risks for pedestrians and cyclists, and reduce the overall functionality of the intersection. Given South Fremantle's focus on sustainable urban planning, this seems counterproductive.</p> <p>Additionally, unmanned facilities often raise concerns about littering, water run-off, and improper use. Without on-site management, these issues could escalate, compromising the cleanliness and safety of the surrounding area. This is especially problematic in a community that values environmental sustainability and careful stewardship of shared spaces.</p> <p>Overall, the introduction of an unmanned car wash in such a location would conflict with the identity and priorities of historic South Fremantle. A thoughtful reconsideration of its impact and compatibility with the area is strongly advised.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.

		<ul style="list-style-type: none"> An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility.
31.	This looks like a good project for the site	Noted.
32.	<p>It seems like a noisy activity to have so close to residences and the acoustic report doesn't do much to reassure.</p> <p>As it's unstaffed, it could attract anti-social behaviour during evenings and overnight.</p> <p>South Fremantle needs more businesses that employ people rather than unstaffed.</p>	<ul style="list-style-type: none"> Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility.
33.	I live a few blocks from this site and fully support this proposal as proposed.	Noted.
34.	Build the car wash! We need a good drive through car wash in the area so I don't have to go to another suburb to wash my car this way!!!!	Noted.
35.	<p>This doesn't contribute to the community in any way. There is already a car wash 2 minutes down the road on Hampton.</p> <p>This prime street corner deserves something beautiful that will serve the neighbourhood, not bring more traffic to an already congested area that has limited driver visibility due to the proximity of buildings to the kerb.</p>	<ul style="list-style-type: none"> The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.

<p>36.</p>	<p>I don't believe this to be a desirable development in this location for the following reasons:</p> <ol style="list-style-type: none"> 1) Traffic congestion queueing, entering and leaving the site. 2) as this is planned to be an unstaffed business, no local jobs are being created or maintained. 3) Carwashes already exist within the near vicinity South along Hampton Road. 4) The local area has been focussed for some years now on minimising and slowing vehicular traffic and focussing on providing safe access for pedestrian and bicycle use. Promoting increased motor vehicle traffic in the area is counter to this. 	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
<p>37.</p>	<p>Not consistent with the surrounding area, not the kind of use for the area as it's residential and coastal. Belongs in an industrial area not in prime location cafe strip.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.
<p>38.</p>	<p>Would like to see something else there doesn't fit with South Terrace. Already a few car washes within 5 minutes. Also very busy road would create even more congestion with buses and vehicles. South St and South Terrace battle to cope now let alone with more vehicles trying to enter and leave proposed washing area.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and

		is supported by expert traffic, acoustic, and stormwater assessments.
39.	<p>I have significant reservations about the proposed car wash development. I believe the site is unsuitable for this type of project, as it would add unnecessary pressure to traffic flow at an already congested and busy corner, creating potential safety and accessibility concerns for vehicles entering and exiting the site.</p> <p>Additionally, I feel this type of business is not appropriate for the area, which is evolving into a vibrant and growing food district. A car wash does not align with the urban character or the vision for this precinct and may detract from its appeal as a destination for dining and social activities.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
40.	<p>Fully approve the application, we have a large population in Fremantle living in large multi floor units and apartments with ever more new apartments being built.</p> <p>These residential properties do not have car washing facilities so the residents will need this project</p>	Noted.
41.	<p>I do not support the development application on the grounds that it is inappropriate use of land for that location, will result in further congestion and risk safe passage of traffic-pedestrian bikes and cars.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.

		<ul style="list-style-type: none"> • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
42.	<p>It is a bizarre proposal considering the location. This is part of Fremantle is one of, if not the only part of the city that is undergoing a revival with younger families with kids moving into the neighbourhood, buying and renovating heritage-listed houses that had been neglected for decades. The businesses and the family-friendly urban life along South terrace are Fremantle's most positive story in recent years, attracting investment and bustling with life. The council should be supporting this through its urban planning and strategically building the connection between the city centre and South Fremantle, to create a continuous, alive, safe, inhabited urban area and counteract the dire state and reputation of Fremantle's centre. The blocks around the intersection of South Street and South Terrace are key to this revival, and the council should encourage investment and development into infill residential and urban misses-use business premises. A carwash is exactly the opposite of what is needed - it would break that urban link to the city centre for decades to come and discourage walking, cycling and street level living in the area. A carwash belongs in a suburban, industrialised area, not in the predominantly heritage-listed core of the city.</p> <p>Further, the traffic study, besides referencing traffic data from 2020/2021, making it obsolete to the point of being useless, predicts a 'mild' increase in traffic. This is absurd, considering the proposal would add two entry/exit points to an already busy and dangerous intersection. The council should review the number of traffic accidents around the intersection only in the last year - we have ourselves witnessed three potentially fatal crashes just in front of our house last year with cars hitting the posts and parked cars on the side of the road - to realise that the traffic should be slowed down and reduced and not 'mildly' increased.</p> <p>Again, this proposal is located in the residential core of the city centre where people should be able to walk out of their houses without fearing for their lives and the lives of their children. If the council is interested in revitalising Fremantle and attracting families to live, it should build on and support the organic revival this area has been undergoing in the last</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.

	few years. The traffic should be reduced and slowed down and not increased and residential infil should be encouraged. There are other parts of the wider city area that are suitable for a carwash.	
43.	a facility for washing vehicles should be in an industrial area not in a residential /cafe precinct this commercial operation will not enhance the area in my opinion	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
44.	Whilst I wonder whether we need another car wash in the area there being car wash facilities on Hampton road opposite the shopping centre. I have no objection to the proposal.	Noted.
45.	There is a car wash in North Fremantle. There is a car was on Hampton Road. This is a prime site and I can not believe this is the only development that can be envisioned for a South Tce site	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • The perceived oversupply of a land use is not a relevant town planning consideration.

46.	<p>I am totally opposed to this car wash I live on South Terrace and can think of many reasons this is a very bad idea for our location. Traffic lights plus car wash I've seen when I lived in Brisbane is bad for traffic congestion and dangerous for all especially cyclists and pedestrians. People turning immediately into a car wash after turning onto South Terrace will be a hazard. We have a vibrant mix here of residential and business like cafes and retail this will be out of character for this area and ugly as an entry point south Freo. Please do not allow this to go ahead we need to carefully nurture our local flavour and this does not! There is a car wash close by in a more suitable location we do NOT need another one</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
47.	<p>I do not want a car wash and signage on the cnr South Terrace and South St. I feel that it is commercially inappropriate to position in the small ex fishing village vibe and community hub of South Fremantle. The build up of cars and queues on the corner of two busy streets will be impractical. It holds no visual/real commercial value for the community being placed in what is essentially an historic hub. Let us maintain the small, historic interesting high street feel of South Fremantle, a carwash does not fit this criteria. Please do not approve this application.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
48.	<p>The proposed automated car wash represents unnecessary and inappropriate development in that location and does not align with what I understand to be the principles of the Fremantle Planning strategy (Section 3, Fremantle Planning Strategy, 2001). Further, I do not believe that it meets the objectives for mixed use zones as set out in the City of Fremantle Local Planning Scheme No 4 (LPS4). Unnecessary development South Fremantle does not need a drive in/drive out car wash facility in this location. There are two large carwash facilities less than 2 km away</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.

(or 4 minutes drive) on Hampton Road, South Fremantle and a BP automated car wash on Queen Victoria Street about 2.6 km away (6 minutes drive). These carwashes are in light industrial (nonresidential) precincts with businesses and services that people already drive to (supermarket, bottle shop, automotive repairs, petrol station, etc), and which represent more appropriate locations for this type of commercial activity.

Inappropriate development

The proposed development will not enhance a sense of community or encourage participation in community life (Goal A4, Fremantle Planning Strategy, p.34). South Fremantle is a medium density residential area with shops, cafes and service businesses along South Terrace. It is also a heritage zone. The proposed development will not contribute to this local commercial centre or align with its identifiable character. At the moment the site is a former boat sales yard, a facility from an earlier time when this area contained more light industrial premises, often connected with the port and marina. Many of these sites have been converted into vibrant residential and commercial spaces. The proposal states that the previous use of the site as a boat sales premises "arguably provides for a less desirable planning outcome compared to the proposed motor vehicle wash which offers improved built form presence, more landscaping, a higher level of site activation and improved traffic/access outcomes." I disagree:

- Claiming that a vehicle wash offers improved built form to the rundown facilities that are there currently is a very low bar. Rather, Council should be looking to the future and considering what type of development is best suited to the character of this local area. Once built, this facility will be there for a long time. The proponent's argument that the proposed development will look better than the previous boat yard is an aesthetic consideration and, if the proposed development is to be assessed on its aesthetic merits, I would suggest that a bitumen drive through and rectangular buildings clad in white is not in keeping with the character of the local area.

- The claim of more landscaping is also a very low bar. An edging of groundcover (that may or may not survive) does little to reduce the thermal mass of a site which is predominantly black bitumen.

- The site activation will come only from cars driving in and driving out. Is that the kind of activation we want? People will not be parking their

- A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
- The perceived oversupply of a land use is not a relevant town planning consideration.
- Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.
- Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.

cars, visiting businesses in the local area and engaging with the community. They will be simply driving through.

- As for improved traffic outcomes, when it was a boat sales yard, there was very little traffic driving in and out of the site. The carwash will increase the number of cars exiting onto South Terrace. I note that the artist's view of the proposed development shows a person on a bicycle at the point on South Terrace where the bike lane ends and cyclists have to merge with the traffic. We should be prioritizing the needs of pedestrians and cyclists rather than cars.

City of Fremantle LPS4

Section 5.2 of the Planning Report for the proposed carwash presents the objectives for Mixed Use zones as set out in Clause 3.2.1e (City of Fremantle LPS4). The proposed development meets none of the five objectives. Rather the proponent notes that the proposed development can only be approved at the discretion of the decision-maker. They reference objective

(i) that allows for "light, service and cottage industry" and "services in small scale premises including showrooms". Their argument that these types of businesses are inherently car-based uses is a stretch. I also refer to the points I've raised above, that the Council should be considering the future of this area and the type of community it wants to build. Regarding the other objectives:

(ii) provide for residential – does not meet this objective.

(iii) ensure future development....is sympathetic with the desired future character of each area – a drive in/drive out facility is not enhancing the vibrancy of South Fremantle, making the area more appealing to pedestrians or encouraging people to engage with the local businesses.

(iv) ensure that development is not detrimental to the amenity of adjoining owners or residential properties – the proponent has proposed setbacks as well as traffic and acoustic controls to meet this requirement. However, a 24 hour/7 day a week drive through facility is not appropriate to a predominantly residential area. Other businesses in the vicinity operate during business and restaurant hours.

(v) conserve places of heritage significance, the subject of or affected by the development – the proposed development is in a heritage area and while the subject site is not heritage listed, the proposed carwash is not in keeping with this heritage precinct.

<p>49.</p>	<p>Kaya, My family and I live (for the last 19 years) about 60 metres in a straight line due West from the proposed car wash. Most of the breeze (and therefore sounds) come from a Westerly direction. I think it's great that a use has been found for this prominent and significant site and note the Acoustics study and the incorporation of walls and roller doors to reduce noise. My concerns are as follows:</p> <p>*I doubt the roller doors will actually be utilised properly once the car wash is up and running. I use car washes regularly and the ones that have roller doors installed do not use them except when the facility is closed. You will not have the ability to police this use on a daily basis;</p> <p>* The noise will travel. I don't think the study has incorporated how much the noise will travel with the breeze. This will be especially so at night, when sound travels far easier. Currently we hear all activity on South Tce at night, and even the attendants on the speaker at the service station on the corner of Hampton Rd, which is further away;</p> <p>*I oppose the 24 hour operation and believe that this will significantly impact the residents in the vicinity.</p> <p>*This is my main issue and that of other neighbours in the street.</p> <p>*I believe a fair and reasonable allowance for trading hours is 7am - 7pm.</p> <p>* Trading hours could be extended after 6-12 months of operation if a neighbourhood survey was in favour. I doubt that this would be supported by the neighbourhood though.</p> <p>* Living in an urban environment comes with some noise and we understand this. It is vital though to support amenity and neighbourhood cohesion. We spend a lot of time on our back deck and many neighbours do the same. The sound of industrial vacuums and high pressure nozzles will severely impact that amenity and ability to enjoy our outdoor spaces, especially at night.</p>	<ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility. • The self-closing roller doors are automated via the equipment in the plant room and will be utilised while the facility is operational.
<p>50.</p>	<p>My husband and I live (for the last 26 years) about 100 metres South West from the proposed car wash. Most of the breeze (and therefore sounds) come from a Westerly direction. We think it is good that a use has finally been found for this prominent site and note the acoustics study and the incorporation of walls and roller doors to reduce noise. Our concerns are as follows:</p> <p>*We doubt the roller doors will actually be utilised properly once the car wash is up and running. You will not have the ability to police this use on a daily basis;</p> <p>* The noise will travel. We don't think the study has incorporated how</p>	<ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.

	<p>much the noise will travel with the breeze. This will be especially so at night, when sound travels far easier. Currently we already hear activity on South Tce at night, and some from South Street;</p> <p>*We strongly oppose the 24 hour operation as we believe that this will significantly impact the residents in the vicinity.</p> <p>*This is our main issue and, we are aware, that of other neighbours in the street.</p> <p>*We believe a fair and reasonable allowance for trading hours is 7am - 7pm, and would not oppose this.</p> <p>* Trading hours could be extended after 6-12 months of operation if a neighbourhood survey was in favour. We doubt that this would be supported by the neighbourhood though.</p> <p>* Living in an urban environment comes with some noise and we all understand this. It is vital though to support amenity and neighbourhood cohesion. We spend a lot of time in our back yard on the deck, and many neighbours do the same. The sound of industrial vacuums and high pressure nozzles will severely impact that amenity and ability to enjoy our outdoor spaces, especially at night.</p>	<ul style="list-style-type: none"> • An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility. • The self-closing rollerdoors are automated via the equipment in the plant room and will be utilised while the facility is operational.
51.	<p>1. Please no 24 hours operation! We have already seen this part of South Terrace change from calm street to a bustling precinct with tenfold the traffic, noise, theft and vandalism in a little over a decade (while also enjoying the conveniences this change has brought about). I ask that operating hours are restricting to daytime only. There seems to be an incredible increase in Fremantle of 7/11 and 24 hr businesses recently. Please don't sandwich our neighbourhood with another one of these.</p> <p>2. Also, please enforce a requirement of green canopy into the submission. South Freo's rapid change and infill has seen many trees and green vegetation disappear and all the extra cars and concrete appear to have made the place noticeably hotter in summer. Due to all the rapid development our streets have also become more prone to flooding with significantly less soil for rain to run off into.</p> <p>Thanks for taking our input into account.</p>	<ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility.
52.	<p>This South St and South Terrace intersection presents a real gateway opportunity to the gorgeous South Freo precinct, and I couldn't think of a more wholly inappropriate use there than a car wash. It's a tight site, on a busy corner, and it will add nothing good to the</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is

	<p>area. But that's just my opinion.</p>	<p>suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.</p> <ul style="list-style-type: none"> • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
<p>53.</p>	<p>I live to this lot and this seems like a terrible idea. We live in a South Tce facing apartment and have serious concerns about the light, noise, chemicals and traffic impacts of having a car wash next door to our apartment building. This is all onto top of potential traffic impacts on a corner that frequently sees accidents, noise complaints and near misses. There are already a huge amount of issues with people parking across our driveway or in the no stopping zone making exiting our car park extremely dangerous. This issue is exacerbated by the power pole and speed sign preventing line of sight to the traffic light. The addition of frequent exiting cars is a recipe for disaster here for resident safety. Finally, this really does not seem to match the Fremantle identity. This is the kind of business frequently found in a commercial area not a strip famous for boutique stores and coffee shops.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal

		<p>impact to the road network and acceptable access arrangements.</p>
<p>54.</p>	<p>In simple terms, the site is one the best locations for infill residential development in the souther area of Fremantle. The proposal is a substantial underutilisation of a prominent site that has some of the best accessibility in Fremantle - despite which, by definition, everyone will be coming by car - except a few dog-owners walking their pets to the dog wash.</p> <p>See attached file for more detail.</p> <p>The property is zoned 'mixed use' so 'vehicle wash' is an 'A' use, meaning: "that the use is not permitted unless the Council has exercised its discretion and has granted planning approval after giving special notice (advertising) in accordance with clause 64 of the Planning and Development (Local Planning Schemes) Regulations 2015, Schedule 2." However, it is almost entirely surrounded by 'residential', for which a vehicle wash is an 'X' (or not permitted) use. Even the property to the immediate south has a substantial residential component in the actual development. It has been suggested that the site is a former petrol station (prior to the 1970s, at which time there is photographic evidence of its being a used-car yard) and therefore could not be used for residential development. However, even if it is an old petrol station site, it can be remediated to be suitable for residential development. Sure, there is a cost and it takes time, but the returns can still be substantial. According to Engineers Australia "The greatest challenges in repurposing service stations are the cost and time required for full remediation, which can stretch to more than \$1 million per site and take between two and five years. Converting valuable sites to high or medium-density housing or commercial office buildings can be a profitable exercise for both the landowner and developer — even with the cost of remediation" (https://createdigital.org.au/challenges-remediating-petrol-stations/).</p> <p>The current proposal is a substantial underutilisation of a prominent site that has some of the best accessibility in Fremantle - despite which, by definition, everyone will be coming by car - except a few dog-owners walking their pets to the dog wash. There are a lot of operational issues, including vehicle access/egress at both South Street and South Terrace so close to the busy signalised intersection and, on approaches, where queues already form which would make right turn entry (south Street)</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • No part of the application materials produced by the applicant contends that the subject site was used as a petrol station. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.

	<p>and exit (South Terrace) difficult. The site has almost 400 buses a day into Fremantle (and through Fremantle by train to the rest of the Metropolitan Area) and over 200 outbound to Murdoch, Cockburn Central, Kwinana and Rockingham. The difference is outbound 511, 513, 520, 530 and 531, which travel via South Street inbound to Fremantle but via Wray Avenue in the outbound direction. Wray Avenue shops (with Galati and Franks for food needs) are a little over 400m walk (5 minutes). Fremantle Primary School is 600m (8 minutes). Fremantle Hospital is just 650m (8 min walk) and Fremantle Markets are 900m (12 minutes). In the southerly direction, there are any number of coffee shops, clothes shops, cafes, restaurants, bars/pub within 900m (12 minutes walk) - many within 400m. There's even a (very good) fish and chip shop just 450m away on Hampton Road. Given all this, if you were to ask me to identify the best place for innovative residential development (with minimal or even no car parking) south of Fremantle Centre, it would be this site. There are many examples of this overseas, sometimes with the addition of a car-share scheme for when a resident does need a car. Amongst other things, this type of development would substantially reduce concerns about vehicle access and egress so close to a busy signalised intersection. It would also lend itself to affordable or social housing, with those who have no need of a car not having to pay for car parking space(s). The space no longer required for car parking could be devoted to community amenities of one sort or another – offsetting possible limitations of affordable apartments. In addition to the matters outlined above, if means cannot be found to ensure (eg through incentives, encouragement or mandation) residential development (especially social or affordable housing) on sites such as this then it substantially increases the difficulty of moving towards the planning objective of increasing residential density in existing developed areas</p>	
55.	<p>Although I support redevelopment of the site, I feel a 24hr car wash is perhaps not the best option. It has to be anticipated that there will be an increase in noise pollution for local residents. It would also be anticipated that there will also be an increase in traffic at this already busy local suburban intersection so a detailed traffic management plan should be circulated for consideration on how the impact to local streets will be mitigated. Perhaps a curfew could be considered for a 12hr daily operation - perhaps 7am to 7pm.</p>	<ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.

		<ul style="list-style-type: none"> • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
56.	<p>A 24 hour car wash in a residential area is a very bad idea. This area is very quiet most nights and a 24 hour operation would have a significant impact on the amenity of the area for residents. Further, the location is at an intersection with traffic lights, and the entry and exit of cars into the car wash would potentially impede traffic flow. South St is particularly busy during both weekdays and on weekends. I think the proposed location is inappropriate and would have little or no benefit to residents of the area.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
57.	<p>I have a further comment. The development is special purpose. This means if the business case fails (i.e., another carwash when there are many others around), it will be difficult to repurpose the site for an alternate use. While this is arguably owner's risk/problem, residents/council may have to put up with a derelict site for a long time. This is the wrong type of development for this area.</p>	Noted.
58.	<p>I strongly oppose this idea. Putting a 24 hour carwash on the corner of this busy intersection is a stupid idea. South st and South Terrace are both very busy roads already, building</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to

	<p>this would naturally increase car traffic to these already bus roads. More cars on the roads would unnecessarily increase congestion for existing residents, visitors as well as the multiple bus routes that utilise these roads. There is a bus stop very near to where this would be located, which if there is increased traffic in and out of the proposed yard, could inhibit the bus stop getting close enough to the curb to allow people in wheelchairs and/or people with prams access to the bus. Furthermore, increased car traffic would bring more cars to area where there is already scarce parking, worsening the parking situation. More cars would also increase the existing traffic noise and pollution.</p> <p>Both roads and footpaths are highly utilised by people on bicycles and pedestrians. Increased car traffic entering and exiting the proposed lot would pose a safety risk for cyclists and pedestrians if motorists are not careful. Also, the cars entering and exiting the lot would have to give way to pedestrians and cyclists, holding up the traffic behind them, holding up buses and other motorists.</p> <p>I feel this type of business does not fit the aesthetic of the neighbourhood and the city as a whole. I love living in fremantle and feel that the proposed car washing business is a waste of this precious space. I look forward to the city of Fremantle to proposing ideas that improve our community in a meaningful way.</p>	<p>discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.</p> <ul style="list-style-type: none"> • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.
59.	<p>Fully supportive of the proposal . These sites are popular in so far as people want them, provides employment, activates a dormant (eyesore) site whilst not (like another cafe) demand more street parking in the area. Proceed</p>	<p>Noted.</p>
60.	<p>I oppose the 24 hour operation. We have enough local noise without a 24 hour car wash. Personally I would not wash or vacuum my car before 7am or after 7pm in respect of my neighbourhood.</p>	<ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6: 30am-10: 30pm 7 days were week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.

<p>61.</p>	<p>I am not opposed to the development of the site however I do not support a car wash in this location. I don't believe this facility reflects the needs of a medium density suburb with a unique character and doesn't add to an ever increasing focus on pedestrians over cars in the area or to the vibrant street life.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
<p>62.</p>	<p>I do not support this development application for the following reasons:</p> <p>1) The nature of the business is out of synch with the village nature of the surrounding street scape. Localised retail and hospitality venues provide incentives for people to visit the area, spend money in local businesses, wander and soak up the interesting and funky environment. It's a stretch to conclude that a car wash contributes to that vibe. In fact, quite the opposite - a car wash is a soulless commercial business that should be located outside of the centre of the village.</p> <p>2) That corner really represents the anchor property of the beginning of the Sth Fremantle precinct which attracts many residents, non-residents and tourists alike. Just ask your "This is Fremantle" marketing consultants if a car wash adds to the vibe being created to attract people. In fact, ask your self this question....would you include the launch of that car wash in your "This is Fremantle" newsletter? I'm guessing not!! So that tells you how the addition of that business is not a net positive to what you're trying to build in Fremantle.</p> <p>3) That corner is already a very busy intersection with large volumes of traffic. In rush hour it can be quite difficult to turn right into South Street due to the traffic flowing out of Fremantle township towards Sth Fremantle. I regularly see cars that have pulled into the intersection to turn right, cross on red because they have been unable to turn due to the traffic coming the other way. This is dangerous and will only be exacerbated with the addition of significant vehicular traffic to the location.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.

<p>63. I am opposed to an automated 24 hour Car Wash on the corner of South Street and South Terrace.</p> <p>I am opposed on the grounds of noise, traffic and safety considerations. The zone in which it is to be established is mixed residential and commercial use.</p> <p>The zoning of this area has been the same in the 30 years I have lived on Gold Street.</p> <p>During this time there has been rapid housing development and density due to the increase value of properties in the area.</p> <p>Consequently, there has been an increase in apartments built in the last few years such as the apartment block adjacent to the proposed wall of the car wash. This has outdoor high up balconies. This did not exist 30 years ago when the commercial zoning appeared appropriate.</p> <p>Also there has been building infill in many properties. Consequently, there is a much higher density of people living in this area who will be impacted by noise and traffic.</p> <p>The proposal states that the car wash would “provide convenient services to the surrounding community and travelling public”</p> <p>That is a very presumptive statement. Were the community asked if they wanted a convenient car wash ?</p> <p>I would dispute that any resident in South Fremantle does not prefer to wash their car at the automated car wash on Hampton Road in South Fremantle than have the noise and traffic of a car wash in their immediate area.</p> <p>There are 2 car washes on Hampton road : Rhino Wash which is 7am to 8 pm is a 6 minute drive or 1.4 Km away from the proposed car wash and Freo Car wash 8am to 5pm is a 1.3km or 5 minute drive from the proposed site. These are in a more appropriate sites and even these car washes do not operate 24 hours in a less dense residential area.</p> <p>Hampton Road has good access for the travelling public</p> <p>We have no need for a third car wash in the close vicinity. So I would dispute the proposed car wash offers any benefit to the community or travelling public.</p> <p>A more appropriate statement would be that “the car wash is a commercial venture for the profit of a few people and disadvantaging the community by imposing more noise and traffic and compromising safety of pedestrians and cyclists”.</p> <p>In other words greed not altruism is the true nature of the proposal</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • The perceived oversupply of a land use is not a relevant town planning consideration. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days were week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
---	--

Any 24 hour commercial operation that generates noise is more suited to an area solely for industrial use. Nobody wants to live next door to a 24 hour operation.

I understand the land is zoned commercial but it has been used in the past 30 years without impact to the surrounding community.

Noise

The acoustic report does not mention the type of sound proofing materials which need to be used. The report is useless without this information.

A case study (attached) in Melbourne had a car wash closed down due to noise complaints.

It was found the drying section with the 6 powerful air fans generated extremely annoying high levels of noise. Significant sound pressure levels of 108DB(A).

To reduce the fan noise it was proposed

1. replace the type of fans with a quieter system (So the system used is important)
2. Line the internal fan housing with sound absorption material to reduce airborne noise
3. Reduce the fan speed to reduce the noise output
4. Apply damping treatment to the fan housing to reduce resonating noise
5. Fan mounts had to be replaced with more sound absorbing material

Measures suggested to be put in place were :

Lining the inside of the fan housing with an acoustic sound-absorbing material combined with damping material

Installing sound absorbing materials in the drying section

Encapsulation of the air fans in a semi enclosure using heavy plasterboard and sound absorbing material.

None of these measures have been mentioned in the acoustic report for this proposal only roller doors which are unlikely to be used appropriately and have not stated what materials these are made of.

I hope these measures will be applied to this proposal.

The Car Wash has been proposed to include roller doors. Our neighbour says the car washes he uses have roller doors installed but they are only used when the facility is closed.

Again if no one is there to police this, the measure is useless. Will the roller doors be guaranteed to be used and are they made of a shock absorbing material ?

The Car Wash proposal mentions a 2.1metre high fence on the Eastern and part of Southern Boundary Fence. There is no mention of sound proofing materials the fence is made of.

Also, noise can travel easily over the top of a fence.

Can assurance be given that the lining of the air fan housing will be an acoustic sound absorbing material which is combined with vibration damping material.

TRAFFIC and Safety

I live on Gold Street turning right onto South Terrace is already a difficult, dangerous manoeuvre with pedestrians outside the café, parked vehicles and a tree obscuring vision of traffic travelling South along South Terrace.

The proposal states that there will be 216 vehicles in 24 hours. That is 432 vehicles per 24 hours crossing pavements on South Street and South Terrace each vehicle has to enter and exit. That equates to a vehicle crossing the pavement at a busy junction every 3.5 minutes . As the traffic will be quieter at night you could say more traffic during the day so during the day approx. every 1.5 minutes a car crossing pavement either at South Street or South Terrace.

The number of pedestrians around that junction is high. Parents with prams going to near by cafes. I often see primary school children on small bicycles with parents behind going from South Fremantle who have to cross the site of the proposed exit along South Terrace to travel Northwards to Fremantle Primary School.

Cyclists travelling along South Terrace will be vulnerable to more traffic exiting from South Terrace.

To turn Right on to South Terrace from the proposed exit is impossible often when the lights are red, due to the queue of traffic waiting at the red light. Then when the traffic lights turn green, the cars exiting from the car wash will have very little time to turn Right before the traffic travelling South along South Terrace is upon them.

Cars travelling South on South Street turning left onto South Terrace will be faced very quickly with cars exiting the car wash.

The exit of the car wash is not in the 40km/hr zone so cars can be travelling along South terrace at the exit of the car wash at 50km/hr.

Long Term Vision

With increasing population density in the South Fremantle area it is depressing and disappointing that even a proposal of a 24 hour

	<p>automated Car Wash would be deemed appropriate in a high dense residential area.</p> <p>What people want in better pedestrian access enjoyment of the amenities of the local café and yoga centre opposite to the proposed car wash on South Street.</p> <p>The last thing needed is a commercial adventure which is 24 hours disturbing people at night and increasing traffic congestion.</p>	
64.	<p>I think this is not appropriate use case for this site. South Terrace is transforming into a dense residential area and expanding the dining precinct down that strip. A carwash clashes with the upmarket, vibrant, pedestrianised precinct that the council is developing.</p> <p>Whilst it is true that the lot has previously been industrial, it ought to move towards retail or residential. Consider the fuel station that used to be on the corner of Wray and South st. That transformation into a dining, retail and residential space has been fantastic for that area. I do not approve of this use case and I urge the council to reject it.</p>	<ul style="list-style-type: none"> The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.
65.	<p>The location of the car wash is not adequate. The area is mainly residential and the report from apex planning distorts this picture: 'South Street also features a mixed character noting the existence of commercial zoned land comprising a Neighbourhood Centre approximately 300m east of the site,...' This 'Neighbourhood Center' is a petrol station and a car rental, which are a safety concern and known drug spot. A car wash is a development, which is not in line with any of the mixed use land around - even the former boat shop or the current training facility are more adequate. This is a 24/7 automated car wash, which creates noise, light and smells 24/7 and not interaction with public space. The area has a vibrant community with art and cafe culture. A 24 hour automated car wash adjoining residential / mixed residential properties on all sides is a bad idea in the wrong place and should not go ahead. In addition, the entrance and exit would make a known traffic hotspot (South Tce / South St intersection) even more dangerous.</p>	<ul style="list-style-type: none"> The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
66.	<p>I am against the proposal of a car wash on the corner of South st and South Terrace. The area is always full of people walking to bars and restaurants and I don't believe this fits with community needs. It would be better further up South St. I believe this type of business will also add too much congestion to the corner.</p>	<ul style="list-style-type: none"> The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and

		<p>is supported by expert traffic, acoustic, and stormwater assessments.</p> <ul style="list-style-type: none"> • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
67.	<p>Dear Planning Committee I object to this proposed application for a car wash on the corner of South St/South Tce. I believe it would not be an appropriate location for this type of business due to:-</p> <ol style="list-style-type: none"> 1. Increase in the associated noise; 2. Increase in traffic going in and out on a busy corner possibly with cars towing boats etc; 3. Over spray of sudsy water; 4. The overall diminution of residents amenity. <p>A better location could be sourced in a less densely housed area. Thank you for considering my submission.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
68.	<p>The acoustic assessment report provided is lacking robustness in particular for reference and justification to key inputs parameters in particular for table 4-1: -Vacuum unit (80 Dba) -Auto carwash (75 Dba)</p> <p>These values have no justification and appear to be on the lower range from industry standard. Common publication would indicate for example noise level of vacum cleaner to be between 70-100DbA. By considering only 2-5 Db above the noise level considered by these items, most of the results presented in report would be above acceptable level.</p> <p>Table 6.3 results in particular are at the limit (41 Dba result for a limit for 41 DBa and still marked as compliant) Conclusion of acoustic report therefore should be revised to state that even with the mitigation proposed (2 bays at night and 2 vacuum in operation) the total noise level are at the acceptable noise limit (and not</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Since consultation, the operating hours have been reduced to 6: 30am-10: 30pm 7 days were week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-

	<p>below for all cases). Further justification is to be provided to ensure that the vacuum cleaner, doors operations and cleaning bay will not exceed the value considered as input in this report (table 4-1). Even 2Dba more would then increase the acceptable level results of the report for most of the cases considered.</p> <p>Was benchmark done on similar car washing business and noise recorded for comparison?</p> <p>More comments on the noise assessment report:</p> <ul style="list-style-type: none"> -no reference to wind and noise propagation along South Terrace - impact on local business and resident further away? -no reference to echo assessment with such high level noise propagation and diffraction along the propose fences and existing high wall of adjacent building. -no reference to aircon units equipment and additional noise. <p>Personal conclusion is that development proposal is significantly underestimating noise level and propagation to the local community (business and residents). Car washing business belongs to commercial / industrial area where these nuisance are expected and away from built-in residential areas.</p>	<p>considered and compliant noise amenity outcome.</p> <ul style="list-style-type: none"> • An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility.
69.	<p>My main concern regarding this proposal is unwanted and intrusive noise because I and many other residents live either directly adjacent to the proposal or close by.</p> <p>I note that the proposal only details two physical noise mitigation features. These are a boundary wall and roller shutter doors which presumably will be closed when vehicles are washed (not explicitly stated).</p> <p>I note that an environmental acoustic assessment has been carried out. Apex Planning have submitted a detailed report to Council. 241129 24-274 DA report- South Fremantle Car wash.docx Paragraph 4.3 discusses acoustic compliance.</p> <p>This states "With the inclusion of above mitigation measures, the noise levels emitted by the facility are comfortably within the compliance range and achieve levels which are lower than the permitted maximum. The acoustic compliance of the facility is therefore to a high standard and ensures the amenity of the local area is not adversely impacted"</p> <p>I challenge this statement and consider it to be very misleading.</p> <p>Close examination of many of the tables included in the acoustic report, for example Table 6.3 show many of the predicted noise levels are at or</p>	<ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility.

	<p>very close to the maximum allowed noise limit. At least one result is exactly on the redline at 41DB.</p> <p>The acoustic report does not give any margin of error, it is based on modelling and can only be an estimate at best.</p> <p>Therefore I believe that the acoustic report is at best borderline in relation to noise level compliance.</p> <p>I wish to discuss an interesting case study of a car wash that was built in Melbourne in a residential area recently.</p> <p>The facility was closed down because of excessive noise. Noise mitigation measures were introduced resulting in an impressive reduction in noise such that the facility could be reopened.</p> <p>Car Wash Noise and EPA Regulation- A Case Study Dr. Marek Kierzkowski and Dr. Harvey Law https://www.acoustics.asn.au/conference_proceedings/AAS2017/papers/p117.pdf</p> <p>This report discusses in detail additional physical acoustic mitigation measures that were retrofitted such as choice of fans, reduced fan speed lining of ducts and other surfaces with acoustic absorbing material as well as sound absorbing mounting brackets for the fans.</p> <p>These measures resulted in an impressive 20DB noise reduction.</p> <p>Therefore, if the proposal is accepted by Council, I request Council to include these additional physical acoustic mitigation measures in the plans.</p> <p>These additional measures would not be particularly expensive or onerous if included in the original plans rather than as a retrofit.</p> <p>It would be in the interest of the proponents, as well as residents, to avoid an acrimonious and potentially expensive dispute with local residents by including as many acoustic mitigation measures in the original design as practicable.</p>	
70.	<p>Objections</p> <p>4.2 Traffic and Access</p> <p>Traffic Impact Study</p> <p>I submit the following points to substantiate my objections to this Study.</p> <p>1. The submitted Vehicle Counts are invalid due to their being dated, hence irrelevant.</p> <p>South Street West 20/21 South Terrace South 21/22</p>	<ul style="list-style-type: none"> Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.

Please refer to West Australian Newspapers, 31 January 2025 Page 11 "Congestion getting worse in last 12 months" according to recent RAC Survey.

2. What is the value of location of nearest bus stops? Are car wash customers potentially parking their cars and catching buses elsewhere?
3. Failure to identify ALL local bus routes – failure to identify bus routes 998 and 999 that frequent the corner of South Street and South Terrace – 7 Days per week, every 15 minutes (or less) Monday to Friday
4. Failure to identify traffic entering car wash from South Street West, and South Terrace, North & South. These cars could queue prior to entry, awaiting changing of traffic lights causing traffic blockage for traffic heading east on South Street.
5. Failure to identify issues created by customers exiting car wash onto South Terrace, heading South. They could be restricted exiting by traffic heading south on South Terrace. Thus, causing blockage for cars exiting car wash bays.
6. Failure to identify customers exiting car wash onto South Terrace to travel north. Could be restricted by traffic heading north on South Terrace and or traffic turning east onto South Street (right turn lane only). Thus, causing blockage for cars exiting car wash bays.
7. Failure to identify vehicles who have ability to exit car wash onto South Street.
8. Failure to identify vehicles who have ability to enter car wash from South Terrace. Refer to Legend Map +24.139.sk03a

SUMMARY

This Traffic Impact Study fails to identify (existing and highly probable) scenarios in traffic flow.

Quote "the layout of the development is such that a one-way flow is encouraged". BUT NOT RESTRICTED

One just has to visit local car wash outlets (example - South Street, O'Connor) to observe the queuing especially on the weekends. On weekends the traffic entering and exiting Fremantle via intersection of South Street and South Terrace would have to be considered as "heavy". A factor the Traffic Impact Study does not reflect in it's assessments.

6. TIS Page 16 CONCLUSION

The proposed development potential "to improve the development typology" and "activate a currently disused corner site". This does not provide adequate proof to substantiate the granting of approval for this development.

<p>71.</p>	<p>A 24 hour car wash will create unnecessary noise after hours. Multiple crossovers to the site, although appear to exist will create safety issues. The design resolution of the car wash building does not reflect the character of south Fremantle. No textural, tonal or formal qualities are present. Appears to be a painted tilt up concrete. Solid fence creates an unfriendly pedestrian interface.</p>	<ul style="list-style-type: none"> • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.
<p>72.</p>	<p>We formally object to the proposed development. As owners and residents of 14 Gold Street, South Fremantle, we are directly impacted. Our property borders the southeastern corner of 234 South Fremantle, as shown below. This application seeks approval to construct a Car Wash on the development site that would operate 24 hours a day, 7 days a week. We raise concerns about this application based on several points as detailed below: 2 Noise Generation This site is zoned for Mixed Use and has hosted commercial businesses with low traffic during normal hours. The current application proposes a 24/7 operation attracting 216 vehicular trips per day (see Transport Impact Statement). This will increase traffic noise at all hours, on top of facility noise. Vehicles will pass within a car length of our boundary, idling and generating noise from engines (and associated fumes),</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise

automatic doors, car washes, and vacuum cleaners, potentially even at 3am.

Road Safety

The Transport Impact Assessment claims, "No safety issues were identified within the scope of this assessment." We trust the council is aware of the actual traffic problems in the area and won't accept this statement. South Street is a major artery feeding Fremantle. According to the Main Roads Traffic map (refer trafficmap - Main Roads WA), nearly 10,000 cars use this road daily. This data was collected in 2020/21, so it's reasonable to assume traffic has increased since then as a result of ongoing development. 3 The junction at South Street and Hampton Road has the most recorded traffic accidents in Fremantle, with 60 crashes in the last five years (refer: Main Roads Data) The South Street to South Terrace junction reported 13 crashes during the same period. This road is already a high-risk route to and from Fremantle. The Traffic Impact Assessment estimates 216 vehicle trips per day. Some vehicles will head west along South Terrace, while others will cross the busy (and already congested) South Street near the traffic lights, causing hold ups and congestion. This level of traffic inevitably increases the road safety risk at this junction.

Application for Planning Approval (document: 241129 24-274 DA report – South Fremantle Car Wash)

We draw the Council's attention to the misleading statements in the Planning Application. Specifically, section 5.2 of the CITY OF FREMANTLE LOCAL PLANNING SCHEME NO.4 (LPS4) claims that the application meets the objectives of the Mixed Use zone.

- The reference that the proposal use meets the reference to light / service industry and showrooms which are inherently car based uses is misleading at best. This application is for a 24 hour a day, seven days a week business that will attract 216 vehicular trips per day. This is entirely different operation to a showroom or warehouse.
- There is a reference that the proposed application provides a more desirable application than the previous Boat Yard business, and presumably than the current Safety Training business. How can a 24 hour a day, high traffic business ever be considered more desirable than a showroom / training business that operates in standard working hours?
- The claim that 216 vehicular trips will not have any effect on the Road Safety of the area is demonstrably untrue. ANY increase in entry and exit to this site MUST introduce additional safety risks.

Management Plan which demonstrate a well-considered and compliant noise amenity outcome.

- Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.

	<ul style="list-style-type: none"> The statement that noise will be acceptable, 24 hours a day in a primarily residential area has already been noted above. The creative writing used in this application does not consider any of the negative effects that this proposal will bring to the local area. We trust the Council will be cognisant of this when evaluating the application. <p>Summary We believe that this application should be rejected due to the issues raised above. However, should the Council still believe that approval should be granted we would request that the following provisions be included in the approval:</p> <ol style="list-style-type: none"> Hours of use MUST be restricted to Normal working hours of 6am to 6pm, preferable 6 days per week. Noise mitigation restrictions need to be applied. As a minimum automatic shutter doors must be closed when car wash is operating. The design of fans, reduced fan speeds and installation of noise absorbing materials should be employed. External Vacuums need to be enclosed to prevent noise generation. Regulating operating hours will help reduce vehicle numbers, however the added risk will remain and can only be resolved but rejecting this application in favour of a business more appropriate for the location. <p>Thank you for your consideration and please do not hesitate to contact either of us if you need any additional information.</p>	
73.	<p>This is not an appropriate site for a carwash. It is already an extremely busy corner and this will further exacerbate traffic at this intersection. Also it does not fit with the streetscape & the neighbourhood in general.</p>	<ul style="list-style-type: none"> The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
74.	<p>I am OPPOSED to it being a car wash. I say this as we constantly have unruly behaviour at certain times which we put up with as they are usually walking past.</p>	<ul style="list-style-type: none"> The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to

	<p>By having a 24hr operation next door we would then have to endure excessive noise whether that is from customers cleaning, noise from the machines to vehicles coming and going at all hours of the night. The proposed site will also create another place for unruly people to hang around during the night being rowdy, also I feel we already deal with enough graffiti on our apartment complex and by being 24hr operation I feel this would have people hanging around and potentially increasing this type of behaviour. Because of this I strongly oppose a 24hr car wash operation at this location. This type of business needs to be in a commercial industrial area, not next to a residential complex.</p>	<p>discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.</p> <ul style="list-style-type: none"> • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility.
<p>75.</p>	<p>I oppose to the proposal of developing the site into a car wash for several reasons:</p> <ol style="list-style-type: none"> 1) an increase of traffic- South Street is increasingly traffic jammed and the existence of a car wash will contribute to greater number of cars coming in and out of car wash 24 hours a day 2) noise - there are numerous studies confirming the detrimental effect of the noises coming from car wash facilities in densely populated urban areas 3) a car wash is highly inappropriate and inadequate facility for the area which is populated with older couples and families with young children- most of them walking and cycling in the area. Placing an industrial facility in the middle of the cafe and shop district and amidst the family houses and apartments just does not make any sense 	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.

	<p>4) air pollution ; a car wash will not only attract more cars which will more pollute the residential area but also the particles of chemicals sprayed off the cars by water will definitely be inhaled by residents and pedestrians</p> <p>5) Completely illogical choice of a facility on a premium location; there is no need for a car wash (plenty of those on Hampton road, and in NorthFremantle, Melville and Myaree) why not instead build an apartment building, a daycare centre, a vet hospital or medical centre, shops or a cultural centre perhaps with a library)</p> <p>6) dangerous location for the car wash - there are numerous and frequent car accidents along South Street- cars speeding from Marine Terrace and merging into one line on the corner of South Terrace and South Street) where the car wash is planned). Having cars coming in and out of a car wash will contribute to a greater risk of more car accidents.</p>	<ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6: 30am-10: 30pm 7 days were week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility. • With regard to environmental concerns, it is essential to recognise that motor vehicle wash facilities are purpose-designed facilities with specialised water treatment equipment and require a trade waste permit from Water Corp prior to operation. Conversely, vehicle washing at a private premises occurs without any of these controls, with runoff containing chemicals entering the drainage system. As the autowash bays have self-closing doors on both sides, no spray drift will occur. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
76.	<p>I would like to raise my concerns with the land use (and in particular the hours of operation) of the proposed car wash development at 234 South Tce. I note the following:</p> <p>* I understand that the most recent use of the land has been as a boat dealership. Importantly this type of business operates during daylight</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is

	<p>hours, which was more reasonable for a location that has multiple residential dwellings on either side of it.</p> <p>* A more suitable use (than a car wash) would appear to be a mixed use development of similar type to the neighbouring property at 240 South Tce. This would provide more housing and social benefit to the neighbourhood and should be more profitable for developers.</p> <p>* The intersection of South Tce and South Street is problematic at the best of times with a CAT service and cars from both South Tce and South Street creating a lot of congestion at this point. Adding a high throughput car-wash with cars lining up (likely onto the street) will create havoc at this location... the location itself does not have adequate space to allow for waiting cars, which will create even more traffic issues.</p> <p>* The proposed 24 hour operation is both out of character with the area and will create a nuisance for neighbours with regards to both noise and smell of the car wash. Whilst it seems to be an unsuitable land use for the reasons listed above, if the car wash was approved, it should observe more reasonable opening hours (eg - 7am to 7pm) to allow neighbours to enjoy their evening and get some sleep without the constant noise and smell of the car wash.</p> <p>In conclusion, I urge the rejection of the car wash proposal in its current form due to the issue listed above.</p>	<p>suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.</p> <ul style="list-style-type: none"> • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.
77.	<p>Hello. Normally I am pro development for Fremantle and South Fremantle as I think it is needed. Unfortunately I can not support the DA for a car wash at 234 South Terrace. South Terrace has a special character which embraces cafes, bars, boutique shop and homes. The car wash sticks out like a sore thumb and in my opinion will definitely negatively impacts the amenity and unique charm of the area. While it is acknowledged that South Terrace is largely zoned mixed use there are many neighbouring homes and a 24 hour cash wash is not in keeping with a residential area. Furthermore the 24 hour operation - even at a reduced capacity - will be disruptive and could be the first on a slippery slope of other businesses seeking to operate throughout the night.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.

<p>78.</p>	<p>I invite the planning committee to consider the following reasons why this application should NOT be approved:</p> <ol style="list-style-type: none"> 1. No need for such a facility at this location has been demonstrated. There are at least 2 automated car wash sites closeby in the area, as well as at least 2 good hand wash and detailing operations. 2. No reasons to support a large 24 hours facility have been suggested. 3. The area is established as mixed residential and commercial use. The proposal is for a large 24 hour carwash which is out of keeping with the current usage of the adjacent area. 4. South Fremantle has seen a considerable increase in residential density in recent years including apartments close to the proposed site. Many people living close to the proposed site will be impacted. 5. The anticipated impacts from the operation of such a business are obvious: <ol style="list-style-type: none"> (1) Noise from the operation of the machines; (2) Noise from additional traffic entering from South Street and exiting onto South Terrace; (3) Congestion from the traffic movement, whether or not this is considered to be additional to the current traffic passing through this intersection. Because: A. It will be on the corner of the intersection of South Street and South Terrace. This is already a busy junction. Traffic travelling along South Terrace south of the intersection has increased markedly in recent years, with not only cars, but trucks and delivery vehicles. The 532 bus crosses this junction. Thus cars exiting onto South Terrace will be likely to be obstructed by, and cause obstruction to, cars stationary at the lights or traffic proceeding along South Terrace in a southerly direction. B. As a resident of Gold Street I am familiar with the real difficulty of exiting onto South Terrace in either direction. Further, cars attempting to exit and turn right will cause potential obstruction to any traffic turning left onto South Terrace from South Street, and traffic queuing to turn right at the lights. It is not difficult to see that some cars may get stuck and block the southerly carriageway. (4) Danger to pedestrians and cyclists. The area around this junction has considerable foot traffic. I have coffee at the corner of Gold Street most mornings and have observed how busy the footpath is. There are children going to school on foot, and bicycle, there are people walking dogs, pushing prams and riding bikes too. Pedestrians will have to negotiate the traffic associated with the proposed carwash - both moving and stationary: a clear potential danger, particularly to children. The 	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • The perceived oversupply of a land use is not a relevant town planning consideration. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
------------	--	--

	<p>drivers are likely to be focused more on the traffic movement than kids on scooters</p>	
<p>79.</p>	<p>As South Fremantle local, a psychologist in the Fremantle area, and as someone who adores the community values in this area - I am writing to express my concerns regarding the proposed development of a 24/7 car wash at 234 South Terrace, South Fremantle, While I appreciate the need for development, I believe this proposal is not in line with the character of the neighbourhood and will create significant issues for local residents,</p> <p>Key Concerns:</p> <p>1, Unsuitable Land Use The most recent use of this land was as a boat dealership, a business that operated during standard daylight hours, This was far more compatible with the surrounding residential area, In contrast, a high-throughput car wash operating 24/7 introduces significant noise, traffic congestion, and environmental disturbances, making it an inappropriate fit for this location,</p> <p>2, Traffic and Safety Risks The intersection of South Terrace and South Street is already a heavily congested area, particularly with the existing bus service and regular vehicle traffic, A car wash will likely cause further congestion, particularly as waiting cars line up onto the street, creating additional hazards for both motorists and pedestrians, The site itself does not have adequate space to accommodate queued vehicles, further exacerbating traffic flow issues,</p> <p>3, Impact on Residential Amenity A 24-hour business in this location is out of character with the surrounding area, which consists of multiple residential dwellings, The noise and chemical odours associated with continuous car wash operations will create a significant nuisance for nearby residents, affecting their ability to enjoy their homes and disrupting sleep during evening hours,</p> <p>4. Health Concerns from Chemical Exposure Car wash operations involve the regular use of strong detergents and chemicals, which will become airborne and affect the surrounding environment. For residents living directly next door, this poses a serious</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • With regard to environmental concerns, it is essential to recognise that motor vehicle wash facilities are purpose-designed facilities with specialised water treatment equipment and require a trade waste permit from Water Corp prior to operation. Conversely, vehicle washing at a private premises occurs without any of

	<p>health hazard, particularly for those using their balconies or outdoor spaces. Continuous exposure to these chemicals in the air could have adverse effects on respiratory health and overall well-being.</p> <p>5. Alternative Development Options</p> <p>A more suitable use for this land would be a mixed-use development, similar to the neighbouring property at 240 South Terrace. This would provide additional housing and social benefits to the community while being more aligned with the existing character of the area. It may also offer a more profitable long-term investment for developers.</p> <p>Request for Restriction of Trading Hours:</p> <p>If this development is to proceed, I strongly urge the council to impose operating restrictions, such as limiting business hours to 7:00 AM - 7:00 PM, to mitigate its impact on residents. This would allow neighbours to enjoy their evenings without excessive noise, odour pollution, and chemical exposure.</p> <p>Conclusion:</p> <p>Given the significant concerns outlined above, I urge the council to reject this car wash proposal in its current form. At the very least, strict operating hour restrictions should be enforced to protect the well-being of local residents. I appreciate your time in considering this matter and would welcome any updates on the proposal's progress.</p>	<p>these controls, with runoff containing chemicals entering the drainage system. As the autowash bays have self-closing doors on both sides, no spray drift will occur.</p>
80.	<p>I am writing to raise my concerns regarding the proposed 24-hour car wash development at 234 South Terrace. This location is surrounded by residential dwellings, and a business of this nature operating around the clock is entirely unsuitable for the area.</p> <p>Main Concerns:</p> <p>1. Disruption to Residential Amenity</p> <p>The noise, traffic, and odours associated with a 24/7 car wash would significantly affect the quality of life for residents. Many of us have balconies and outdoor spaces that would be impacted by the constant presence of chemical fumes, vehicle noise, and general disruption. Unlike the previous boat dealership on this site, which operated only during daylight hours, this proposed business would introduce activity and disturbances at all hours.</p> <p>2. Traffic and Safety Hazards</p> <p>The intersection of South Terrace and South Street is already a high-traffic area, including bus services and regular congestion. A car wash</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Traffic and access considerations have been comprehensively considered through an expert

	<p>would introduce more vehicles stopping, waiting, and queuing, potentially blocking traffic and creating further hazards, The site itself does not have sufficient space for cars to queue off the street, meaning overflow would worsen congestion and pose risks to pedestrians and cyclists.</p> <p>3. Health Risks from Chemical Exposure Car washes involve the use of strong detergents and cleaning agents that will inevitably become airborne. Given the proximity of this development to residential homes, residents-especially those using balconies or keeping windows open-will be continuously exposed to these fumes. Prolonged exposure raises concerns about potential health risks, particularly for those with respiratory conditions.</p> <p>4. More Suitable Alternatives A more appropriate use of this land would be a mixed-use development, in line with neighbouring properties. This would enhance the area by providing additional housing and community-focused spaces rather than introducing a business that primarily serves passing traffic with no real benefit to local residents.</p> <p>Request for Reasonable Trading Hours Should this development proceed, I strongly urge the council to impose trading restrictions, limiting operating hours to 7:00 AM -7:00 PM to prevent unnecessary disruption to those living nearby. Late-night and overnight operations are completely inappropriate in a residential setting.</p> <p>Conclusion Considering the concerns outlined above, I ask the council to reject this proposal in its current form. If the project moves forward, operating restrictions must be enforced to reduce the negative impact on residents. I appreciate your attention to this matter and look forward to updates regarding this proposal.</p>	<p>traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.</p> <ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome. • With regard to environmental concerns, it is essential to recognise that motor vehicle wash facilities are purpose-designed facilities with specialised water treatment equipment and require a trade waste permit from Water Corp prior to operation. Conversely, vehicle washing at a private premises occurs without any of these controls, with runoff containing chemicals entering the drainage system. As the autowash bays have self-closing doors on both sides, no spray drift will occur.
81.	<p>Subject: Opposition to Proposed 24/7 Car Wash at 234 South Terrace – Concerns About Suitability and Impact</p> <p>I am writing to formally object to the proposed development of a 24-hour car wash at 234 South Terrace. As someone who spends a considerable amount of time in South Fremantle, I am deeply concerned about the impact this business will have on traffic, noise levels, air quality, and overall neighbourhood amenity.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and

While I understand that development is necessary, a 24/7 car wash is simply not appropriate for this location. The previous business on this site, a boat dealership, only operated during standard business hours, which was much more compatible with the surrounding residential properties.

Key Concerns:

1. Excessive Noise and Disturbance

A car wash operating at all hours will generate ongoing noise from vehicles, machinery, and customer activity. This will significantly disrupt the ability of local residents to enjoy their homes, particularly in the evenings when peace and quiet should be expected in a primarily residential area.

2. Traffic Congestion and Safety Risks

This corner already struggles with congestion due to its location on a busy bus route. A high-volume car wash will add to the problem, with vehicles queuing up and potentially blocking the flow of traffic. The site is not designed to accommodate waiting cars, making it highly likely that spillover onto the street will create further hazards for drivers, cyclists, and pedestrians.

3. Health Hazards from Chemical Fumes

One of the biggest concerns is the constant exposure to airborne car wash chemicals. Residents living adjacent to the site will have no way to avoid these fumes when using their balconies or opening windows. This raises significant health concerns, particularly for those with asthma or other respiratory conditions. The long-term effects of exposure to these chemicals should not be overlooked.

4. A More Appropriate Use for the Site

Rather than a 24/7 car wash, a more suitable use for this land would be a mixed-use development, similar to what exists at 240 South Terrace. This would contribute positively to the neighbourhood by providing additional housing and commercial spaces that align with the existing community character.

Request for Restricted Hours if Approved

If the council chooses to approve this development despite its negative impacts, I urge you to restrict the hours of operation to 7:00 AM – 7:00 PM to reduce the effect on residents. This is the only way to ensure that those living nearby are not subject to excessive noise, traffic disruptions, and exposure to air pollutants at all hours.

Conclusion

is supported by expert traffic, acoustic, and stormwater assessments.

- A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
- Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements.
- Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.
- With regard to environmental concerns, it is essential to recognise that motor vehicle wash facilities are purpose-designed facilities with specialised water treatment equipment and require a trade waste permit from Water Corp prior to operation. Conversely, vehicle washing at a private premises occurs without any of these controls, with runoff containing chemicals entering the drainage system. As the autowash bays have self-closing doors on both sides, no spray drift will occur.

	<p>For the reasons outlined above, I request that the council reject this proposal in its current form. If approval is granted, operating restrictions must be put in place to protect the well-being of residents. I appreciate your time in considering this matter and look forward to hearing the council's response.</p>	
82.	<p>I am a long-term resident of South Fremantle. I am deeply concerned about the proposal to build a 24/7 car wash at 234 South Terrace, South Fremantle. The character of our community, which has long been one of the defining features of this area, is at risk of being undermined by this development.</p> <p>One of the reasons I, along with many other residents, love Fremantle is its unique community spirit and sense of safety. It is a place where families and friends gather, and the peaceful, family-oriented atmosphere is something we all cherish. A 24/7 car wash facility in this area would not only disrupt this harmony, but it would also be out of context with the surrounding precinct. This development is more suited to a semi-industrial area, far removed from the homes, schools, and businesses that make up the vibrant heart of Fremantle. The airborne chemicals emitted by the car wash could pose a risk to public health, particularly to children and vulnerable individuals living nearby.</p> <p>Furthermore, noise pollution is another major concern. The operation of hoses, vacuum cleaners, and other machinery throughout the night would create an unacceptable level of noise, severely disturbing the peace of the neighborhood. The fact that the facility would be unmanned during the night only adds to the potential for disruption. This area, already frequented by patrons of nearby eating and drinking establishments, could easily become a congregation point for late-night gatherings, creating additional safety concerns and a nuisance to residents.</p> <p>If the development must go ahead, I strongly urge that it be restricted to operating hours of 7:00 am to 7:00 pm, and that tall noise-cancelling barriers be installed along all boundaries of the property to mitigate the impact on nearby residents.</p> <p>Additionally, in the context of the current housing crisis, would it not make more sense to develop a residential complex similar to the one located at 240 South Terrace? This would be far more in keeping with the character of the area, benefiting the local community and providing much-needed housing close to existing amenities, public transport, and schools.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.

	<p>Traffic congestion at the intersection of South Terrace and surrounding roads is already a serious concern. Drivers often find themselves waiting through multiple light rotations to turn right, and there is no room to widen the road or create a turning lane, as evidenced by the traffic challenges at the nearby intersection of Hampton Road and South Street. The introduction of a 24/7 car wash would exacerbate these issues, increasing traffic and creating additional hazards for drivers and pedestrians alike.</p> <p>I urge you to reconsider the proposal for this development and instead focus on projects that are more appropriate for the area and in line with the values of the Fremantle community.</p>	
83.	<p>The proposed 24 hour car wash clearly does not meet the objectives of the 'mixed use' zone and should not be approved.</p> <p>Impact on amenity - noise</p> <p>It is clear that a car wash and industrial vacuum running at any time of the day or night would significantly impact the amenity of the nearby homes. Councillors should ask themselves, "would I mind living next door to a 24/7 car wash?" and if the answer is ambiguous or a no - then they can't be satisfied that the proposal is not detrimental to the amenity of the surrounding properties.</p> <p>The acoustic report concludes that the car wash would meet noise guidelines - however, this is not the question that should be asked. The question is whether or not the noise from the car wash would impact amenity of the neighbours. That report also takes at face value the manufacturers stated noise emissions and does not take into account wear and tear of machinery which impacts noise. It also fails to take into account the amplifying impact of the build environment including the southern wall. Common sense would tell you that an automated car wash with outside industrial vacuums would significantly impact amenity. A 24/7 car wash would clearly have a significant negative impact on the amenity of residential properties in the vicinity. The City's website discussed acceptable noise by saying "if you can clearly hear noise from a neighbour after 10pm within your home it's likely to be above the permitted levels." The proposed car wash cannot be an acceptable noise for what is ostensibly a residential area.</p> <p>I note that while the intersection of South and South can be a noisy place during the day. It is usually dead quiet after 10pm on weeknights and after 12am on weekends (once the traffic leaving Freo stops). The planning report makes a wild claim when stating "the acoustic</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-

	<p>compliance of the facility is therefore to a high standard and ensures the amenity of the local area is not adversely impacted". The noise of a 24/7 car wash would clearly impact the amenity of the surrounding residences. Any other conclusion would be unreasonable.</p> <p>Impact on community - the future of Fremantle</p> <p>Outside of the issue of noise, my biggest concern is just how incredibly boring a car wash is. Council should not use their discretion to approve this land use because it's just too boring. This site is the start of the South Freo strip. There is an opportunity for something that contributes to the street scape and the community. Housing, retail or hospitality would all be a great addition to this corner.</p> <p>Landscaping</p> <p>The suggested landscaping misses an opportunity to create a nice green space on the corner. This could be improved by widening the 1m strip on the outside of the block and by planing a heap of large trees.</p> <p>Conclusion</p> <p>A car wash is not a suitable use of this site. There is a clear impact on the amenity and noise will definitely be an issue. If the application does progress it should only be on the basis of reduced hours and an increase in green space with a bunch of trees planted.</p>	<p>considered and compliant noise amenity outcome.</p>
84.	<p>Surely there is a more sophisticated offering for this site given it's mixed use zoning, maximum build height potential and central location to Fremantle's most up and coming precinct?</p> <p>This is a low rise light industrial proposal on a much more advanced multi-storey mixed-use site.</p> <p>The application doesn't match the zoning and is more suited to O'Connor, small parts of Beaconsfield or Hilton/Hamilton Hill. Where in fact there are already these facilities.</p> <p>We live very close to this site, traffic congestion and noise is already an issue, the current tenant has constantly getting vehicles into the site with traffic backed up through traffic light cycles.</p> <p>As neighbours we really want to see this site developed to it's full potential and would support an application inline with a mixed-use development. Give us something more dynamic please!</p> <p>This is an opportunity for the City of Fremantle Planners to look to the future of what South terrace can become and shape it in that manner. South terrace is a commercial retail and food & beverage strip, this kind of development is not suited at all.</p> <p>I frequent carwashes and they are noisy, dirty establishments, check</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.

	<p>North Fremantle and Hampton Rd. This isn't what residents and the City should approve in such a key area for South Fremantle. I work in this field and there are developers crying out for a site of this potential, let's not approve something so unsuited to the area.</p>	
85.	<p>Whilst the proposal has merit as a use of an underutilised site that would benefit local residents, I am not satisfied that traffic issues have been fully addressed. This is an extremely busy intersection, especially at certain times of day. The evidence for this is, slightly ironically, provided by the applicant in the data on public transport. As long as a large number of bus routes involve a turn from South Street into South Terrace congestion will be inevitable. As regards the services provided, I note that similar services are already provided in the vicinity. Given that the site is in private ownership it would be wonderful if medium density housing could be considered, as a more profitable as well as more appropriate use of an iconic site. Having seen similar slightly challenging sites developed for housing elsewhere I am confident that this is feasible.</p>	<ul style="list-style-type: none"> • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service.
86.	<p>Traffic congestion & Parking The area leading up to the traffic light intersection of South St and South Tce is currently often an area of significant traffic congestion particularly the westerly moving traffic on South St coming from the Hampton Road end and this is the approach which cars will be mainly using to enter the Proposed Car Wash site. Those vehicles approaching the site from any other direction will be forced to turn across two lanes of opposing traffic almost immediately upon traversing the intersection and are therefore likely to present a regular obstacle to the smooth flow of traffic through the intersection. The Transport Impact Statement confirms the current high traffic use on South Street in quoting Main Roads WA figures of 5,339 vehicles per day (vpd) in a typical weekday in 2020/21. As a local resident I have noticed that high volume traffic has made car access, in entering and exiting my property, increasingly difficult over the last couple of years which suggests that the above 2020 figures may now be outdated. Cited estimates of 216 car trips per day on week ends, based on car wash use alone, do not appear to include allowance of dog wash and vacuum only usage. There are only 3 parking bays on the site, and one assumes that at least one or two of these will be regularly utilised by</p>	<ul style="list-style-type: none"> • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is not a use which inherently generates parking demand, as patrons have no reason to remain onsite after washing or vacuuming their vehicle. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-

operational staff leaving little provision for customers arriving with an animal to wash or awaiting to access the vacuuming area or dog washing facility. This could put additional strain on the limited roadside parking available for local residents.

Potential Noise Disturbance

I am particularly concerned about the extent of additional noise disturbance that is likely to occur from the washing process with its use of high pressure water, the regular operation of electrical vacuuming, increased car engine noise, frequent car door banging, potential regular loud music from vehicles, loud voices of customers and the regular frequency of increased dog barking. The acoustic report does not seem to take all of these factors into consideration. It is reported in decibel (dB) comparison reports that a car wash at 20ft is 89 dB and a smaller residential vacuum cleaner in excess of 70 dB (IAC Acoustics).

Furthermore it is cited that continued exposure to noise above 70 dBA (adjusted decibels) over time will cause hearing loss (Center for Hearing and Communication). The acoustic report cites baseline figures for acceptable background noise which range from 45dB during Week days to 35 dB overnight figures and argues that because of its commercial location an influencing factor of +6 Db be added to all time periods. This assumption is flawed as the immediate vicinity is predominantly residential and those few nearby commercial businesses only operate during normal business hours so the influencing factor is not applicable during evenings and overnight. Table 4.1 of the acoustic report lists the expected noise from only 3 different sources at the car wash (wash machines, vacuuming and car doors) with estimates ranging from 75 to 84 dB. This is possibly an underestimate when considered in comparison with other available estimates. Table 6.5 displays assessable noise levels based on modelling ranging from 54 to 58 dB which correspond to an adjusted A10 range of 61 to 71 dB from night to day time. These figures are claimed to be compliant! How is it acceptable to local residents who are accustomed to a current noise level of 35 dB at night having to tolerate a regular nocturnal noise level of 61 dB which, in all likelihood may well be regularly exceeded at least 10% of the time by even louder noises. I fail to understand or believe that even the proposed slight reduction in usage during nocturnal hours will mean that the noise levels will be within acceptable limits for local residents. Reducing Wash machines from 3 to 2 and active vacuum bays from 4 to 2 at nights still means a lot of wash and vacuum noise for those trying to sleep with all

considered and compliant noise amenity outcome.

- Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.

	<p>windows open on a still hot summer's night or when a strong westerly gale carries the incessant din across the whole of South Fremantle. Whilst acknowledging that a boat sales business and more recently a mechanical training business have respectively been the recent previous operators on the proposed planning site, neither of these business produced persistent protracted noise with loud sounds being only in short bursts and confined to very brief intervals during the normal working week and never on weekends. The operating times of this proposed Car Wash business are 24/7 which means this will have a major impact on local residents and be a very intrusive disturbance to the relative current tranquillity of this local community.</p> <p>Incompatible with the Character of the Area</p> <p>I do not consider that the current proposed Car Wash business is an appropriate fit with the character of the local area which is residential and mixed commercial. The site is surrounded on three sides with residential properties and the other nearby commercial businesses, being predominantly cafes and small vendors, which sit much more compatibly alongside the existing residential properties. A 24/7 car wash enterprise seems totally at odds with the rest of the community and would be better located in a more semi industrial zoning where it would be less likely to negatively impact upon residential occupants. There is not even a need for this facility as the rea is already well serviced with currently 5 available car wash sites within a 5 km radius of the proposed location. One seemingly positive aspects of the proposal is the inclusion of landscaping and Australian native plants however even this is overstated and does not go far enough. The major planting is Myoporum Pavifolium, a carpet like ground cover which is not even a WA native. The contrived pictorial image is misleading in often depicting larger plants than are intended and seeking to convey a green ambience by the inclusion of two leafy trees in a directly adjacent property where no such plants exist. I would have suggested the inclusion of a native tree be positioned amongst the Westingias in the triangular garden near the South St entrance together with some taller hedging natives (such as Grevillea Olivacea) along the eastern boundary rather than mainly very low ground covers. The use of larger hedging shrubs would soften the predominantly harsh brick and concrete appearance of the site.</p>	
87.	<p>I would prefer it wasn't a 24/7 car wash. The Noise, the extra traffic - non of that is appealing.</p>	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of

		<p>LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments.</p>
<p>88.</p>	<p>I oppose the construction of a car wash at the corner of South Tce. and South St. in South Fremantle, for the below reasons:</p> <ol style="list-style-type: none"> 1. Contamination of the site and surrounding area: Automatic touchless carwashes use multiple very aggressive chemicals in their effort to remove dirt/grime/insects from cars (because there are no brushes or friction doing this). These aggressive chemicals not only find their way onto the surrounding roads (via leakage and also transfer by exiting cars) and therefore stormwater drains, but they also travel as a mist outside of the site, and are unsafe to breath, especially in such busy residential area with pedestrians and cafes in the immediate vicinity. 2. Noise: Automatic car washes have two very loud components - first is the high pressure pump, which resides in the plant room, and creates high water pressure for the car wash. The second are the blow-driers, which are in the car washing bay, and used to dry the car. Both of these components are extremely loud, normally producing in excess of 90dB(a) measured adjacent to the sound source. Even if the car wash bay and the plant room are enclosed, the sounds emanating from these process will be detrimental to the residents, visitors to the area, and the local businesses. I have lived in Fremantle for many years, and we can often hear the trains going by, concerts at different venues around Freo - imagine hearing the awful noise of pumps and blowers all day and night! 3. Traffic and hazards: A car wash with 3 automatic machines can easily wash in excess of 350 cars per day. Having the entry and exit adjacent to the busy intersection are sure to create traffic issues and further congestion in an already busy area. 4. A car wash doesn't fit the local environment: I would understand if this car wash was being proposed on Hampton Rd in Beaconsfield, as this is a busy industrial site, however, the natural "Freo" feel will be totally lost if a busy (and ugly) concrete car wash is allowed to be built in 	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • With regard to environmental concerns, it is essential to recognise that motor vehicle wash facilities are purpose-designed facilities with specialised water treatment equipment and require a trade waste permit from Water Corp prior to operation. Conversely, vehicle washing at a private premises occurs without any of these controls, with runoff containing chemicals entering the drainage system. As the autowash bays have self-closing doors on both sides, no spray drift will occur. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised

	<p>the unique crossroads of Fremantle and South Fremantle. It totally does not suit the area, and will detrimentally detract from the natural beauty of the area.</p> <p>5. 24hr operation: If you walk south down South Tce on any night past 8pm, it's fairly quiet... sometimes you hear the chatter of people sitting on their front porches, a group of people may be walking to a restaurant near by - but it's peaceful and pleasant, as residential areas should be. In summary, having a 24hr car wash operate in the middle of this residential area is simply crazy!!! The noise will travel far and wide, residents will be forced to leave because of the traffic and the noise, and visitors will wonder what this big ugly concrete place is when entering through Fremantle's southern gateway. Please - keep Freo unique, natural, and beautiful.</p>	<p>design of the facility is sensitive to the local heritage features in the South Fremantle area.</p> <ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.
89.	<p>I am writing to express my concerns regarding the proposed development of a car wash facility at 234 South Terrace, South Fremantle, which is located directly beside the property that I have been a long term tenant of. While I understand the potential benefits of this business, I believe there are several issues that need to be addressed in order to minimize its impact on the local community and surrounding properties, including mine. Key Concerns:</p> <ul style="list-style-type: none"> • Pedestrian Safety: The open design of the site, while looking better than the current fence, could create a potential hazard for pedestrians who may choose to cut across the courtyard. We as humans will always look for the shortest distance to walk or ride. I hear a lot of people riding bikes, skateboards and scooters down the footpath outside my room and can only imagine the risk this could pose to drivers entering or leaving the car wash. • 24-Hour Operation: I am particularly concerned about the proposed 24-7 operation of this business. This would subject the surrounding residential area to constant machine noise and activity, particularly at night. This is a high residential area and having a business that can operate 24/7 seems absurd. • Increased Noise Pollution: The previous businesses on this site, while noisy, did not operate outside of standard business hours. The proposed 24-hour operation of this car wash would significantly increase noise pollution, especially during late-night and early-morning hours. The sound of machines operating at inappropriate times would negatively affect the peace and quiet of the surrounding residential area. 	<ul style="list-style-type: none"> • The proposed Motor Vehicle Wash is a discretionary use in the Mixed Use zone of LPS4 and is capable of approval subject to discretion. The proposed Motor Vehicle Wash is suitably located at an accessible corner location, is designed in a sensitive manner, and is supported by expert traffic, acoustic, and stormwater assessments. • Traffic and access considerations have been comprehensively considered through an expert traffic assessment which has also been subject to a peer review, demonstrating minimal impact to the road network and acceptable access arrangements. • A Motor Vehicle Wash is an appropriate use for the site and will provide local residents and the relatively high local vehicular patronage with a valuable and needed service. • Since consultation, the operating hours have been reduced to 6:30am-10:30pm 7 days a week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic

- Increased Light Pollution: The proposal to reduce the height of the existing 2.7m high section of fence along the driveway to 2.1m will result in more sunlight entering the main bedroom of this house. This is generally a negative during the summer months but may be acceptable during winter. More concerning, however, is the potential increased light pollution due to spotlights being used during evening and morning hours, which will disrupt my ability to sleep and negatively impact the general ambiance of the area.

- Contamination from Product Runoff: There is a potential for contamination from runoff associated with the car wash's operations. I am concerned about the impact this could have on local water systems, soil, and the environment in general.

- Blind Spot for Pedestrians: The 1.8m high wall along South Terrace creates a potential blind spot for pedestrians. Drivers may have difficulty seeing pedestrians crossing in front of the car wash entrance, increasing the risk of accidents.

- Lack of Manual Car Wash Facility: The proposal to not include a manual car wash option may exclude 4WD vehicles that have bolt-on accessories that cannot easily use automated car wash systems.

Recommendations:

- Reduced Trading Hours: To address the concerns regarding noise and disturbance, I recommend limiting the car wash's operating hours to avoid late-night and early-morning operations. This would help to reduce the impact on surrounding residents.

- Manual Wash Bay: To accommodate equipped 4wd vehicles, I recommend that at least one manual wash bay be included in the facility. This would ensure that 4WD owners and others with vehicle accessories can still access the service.

- Transparent Fencing: I recommend that the 1.8m fence facing South Terrace be made partially transparent or replaced with a lighter, less obstructive material. This would help with the line of sight between both driver and pedestrian in this area that has heavy foot traffic.

- Taller Bushes or Trees: I suggest planting taller bushes or trees along the eastern wall of the property to help reduce noise from the car wash operations. These could serve as a natural sound barrier and improve the overall aesthetic of the area.

Conclusion:

While I understand that a car wash facility may bring economic benefits to the area, the issues mentioned above need to be carefully considered

assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.

- Since consultation, the architectural design of the facility has been revised to better align with the City's heritage policy. The revised design of the facility is sensitive to the local heritage features in the South Fremantle area.
- With regard to environmental concerns, it is essential to recognise that motor vehicle wash facilities are purpose-designed facilities with specialised water treatment equipment and require a trade waste permit from Water Corp prior to operation. Conversely, vehicle washing at a private premises occurs without any of these controls, with runoff containing chemicals entering the drainage system. As the autowash bays have self-closing doors on both sides, no spray drift will occur.
- An Operational and Noise Management Plan has been prepared in support of the proposal which demonstrates well-considered operational measures for the facility.

	<p>to ensure that the development does not have an adverse impact on the surrounding community. I respectfully request that these concerns be taken into account, and that modifications be made to the proposal to address the potential negative effects.</p>	
90.	<p>The conclusion is that the proposal is well within acceptable acoustic limits. However I think the applicants concede some uncertainty regarding acoustic impacts because they propose to only use two of the three bays at night time. Given this uncertainty, would it be an option for planning to restrict operation to daytime hours only, with an option to review hours of operation at a later date and then extending them if acoustic impacts are limited. Given some uncertainty re acoustic impact, much easier to start with limited hours initially, which could potentially be increased than to have 24 hr operation at start and then to try to reduce hours,</p>	<ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6: 30am-10: 30pm 7 days were week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.
91.	<p>I am concerned by the noise potential of an automated 24 hour Car Wash on the corner of South Street and South Terrace to disturb local residential properties. Acoustic Report The acoustic report falls short on various details so that it is impossible to gauge the true impact of the venture.</p> <ul style="list-style-type: none"> • The acoustic report does not specify the type of sound proofing materials which will be used. • A fence on the Eastern side would have to be much higher to protect double story residence to the East and South East from the noise. No evidence that 2.1m would protect local housing. • The report does not specify the types of equipment namely air fans which will be used (Q1 below). • The council states that the noise levels sit comfortably within the guidelines. In several incidences the noise levels were stated as being on or near the guidelines at 37, 38 and 41, 41 being the limit. • There is no information as to the roller doors, ie materials they are made from and when they will be used as a sound barrier. • There is no detail as to the structure, materials and use of the fan damping housing. <p>Melbourne Case Study (attached)</p>	<ul style="list-style-type: none"> • Since consultation, the operating hours have been reduced to 6: 30am-10: 30pm 7 days were week which generally aligns with the operating hours of existing uses along South Terrace. The proposal is supported by an acoustic assessment and Operational and Noise Management Plan which demonstrate a well-considered and compliant noise amenity outcome.

A case study (attached) in Melbourne had a car wash closed down due to noise complaints.

It was found once operations started, that the drying section with the 6 powerful air fans generated extremely high levels of noise (significant sound pressure levels of 108DB(A)).

To reduce the fan noise in order to re-open it was proposed to:

1. Replace the type of fans with a quieter system
2. Line the internal fan housing with sound absorption material to reduce airborne noise
3. Reduce the fan speed to reduce the noise output
4. Apply damping treatment to the fan housing to reduce resonating noise
5. Replace fan mounts with more sound absorbing material

Questions to be answered:

1. Could the company be asked to provide the missing details cited above so that the true impact of sound may be re-assessed?
2. If not, why not? Explain.
3. Could the company to be asked to implement the above measures cited in the Melbourne case study before building the car-wash, hence ensuring sound protection in this (now mostly) residential area? This could save the developers money.
4. If not, why not? Explain

APPENDIX 2

DEVELOPMENT PLANS

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

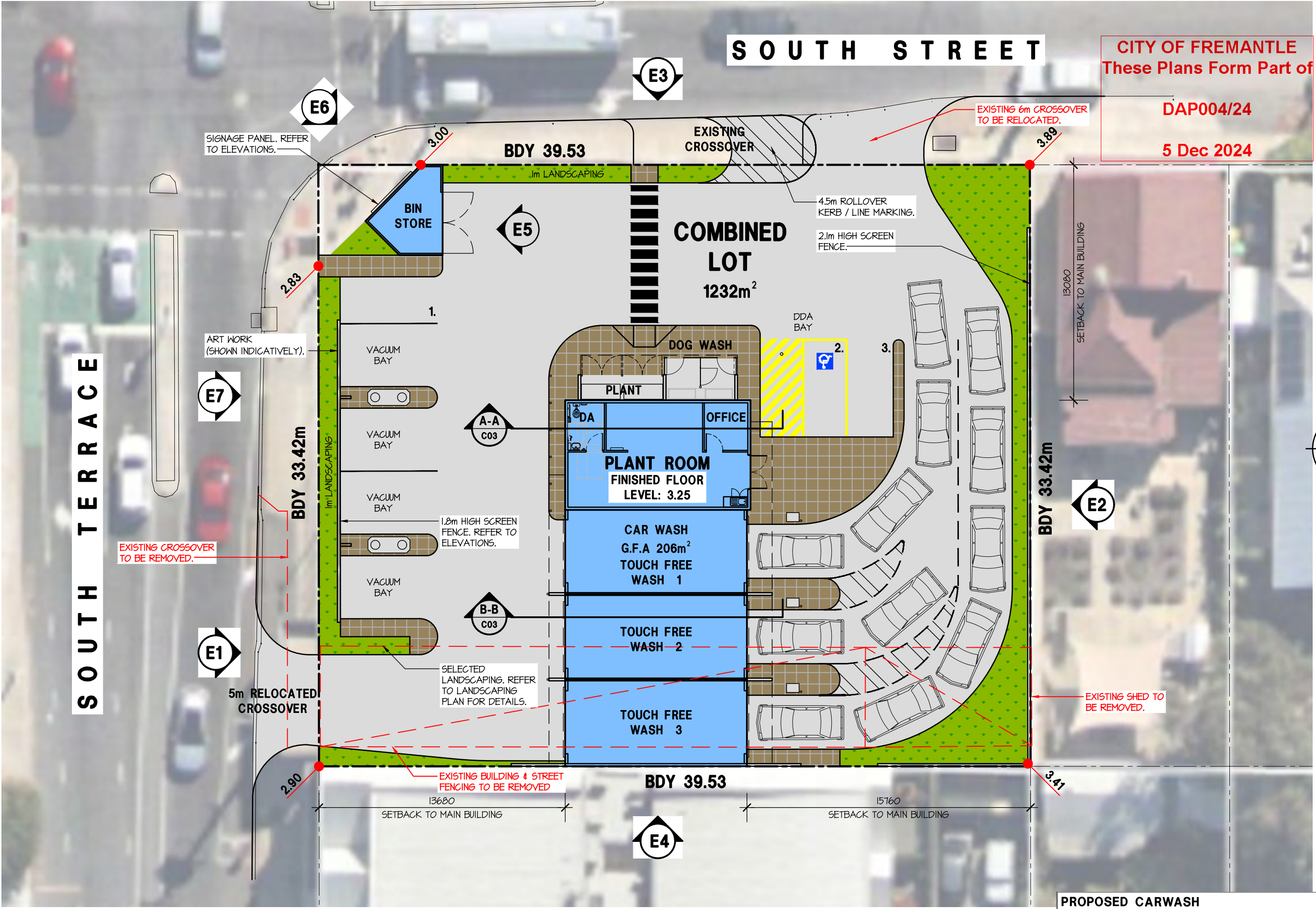
5 Dec 2024

SOUTH STREET

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



SOUTH TERRACE



HINDLEY & ASSOCIATES
BUILDING DESIGNERS
166 STIRLING HIGHWAY
NEDLANDS WA 6109
PO BOX 199
NEDLANDS WA 6109
PHONE - 9386 6699
admin@hindley.com.au
www.hindley.com.au

DEVELOPMENT APPLICATION

PROPOSED SITE PLAN

SCALE 1:200

NOTE

ALL LEVELS TO A.H.D

PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
for GLOWMARK NOMINEES Pty Ltd

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD. DO NOT SCALE FROM THIS DRAWING. THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE. THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.

NO.	DATE	REVISION	DRAWN	CHECK
A	08.10.24	DEVELOPMENT APPLICATION ISSUE	MDB	SJH

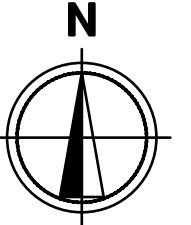
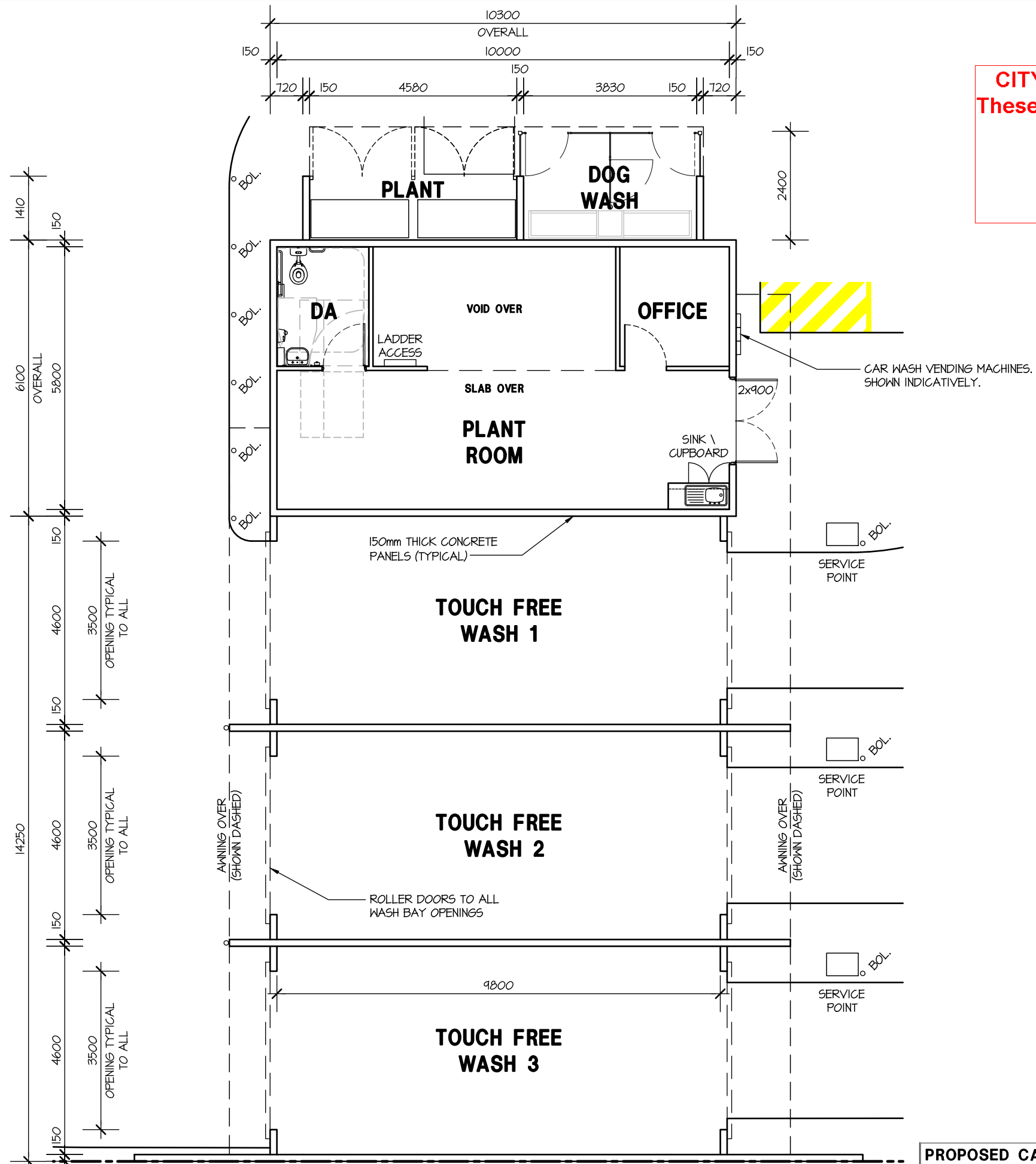
A3 SHEET

Date	-	08.10.24
Design	-	MDB
Drawn	-	MDB
Checked	-	SJH
Scale	-	1:250
Job No.	-	0887
Dwg	-	DA01
Rev	-	A

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



HA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6109
 PO BOX 199
 NEDLANDS WA 6909
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

FLOOR PLAN
SCALE 1:100

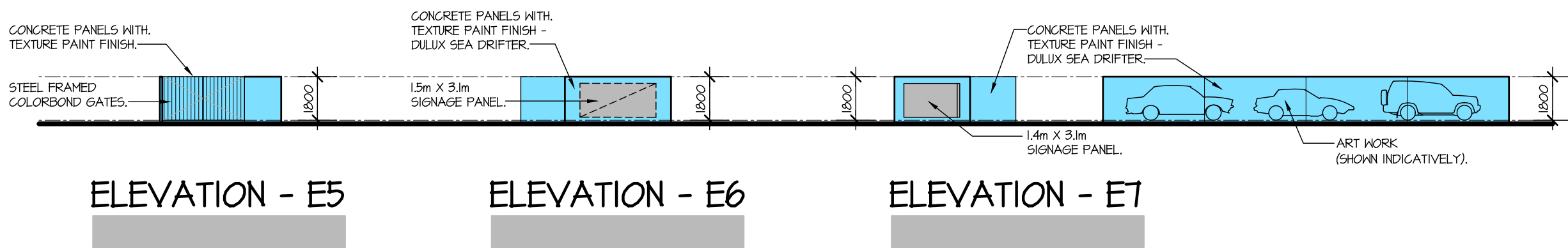
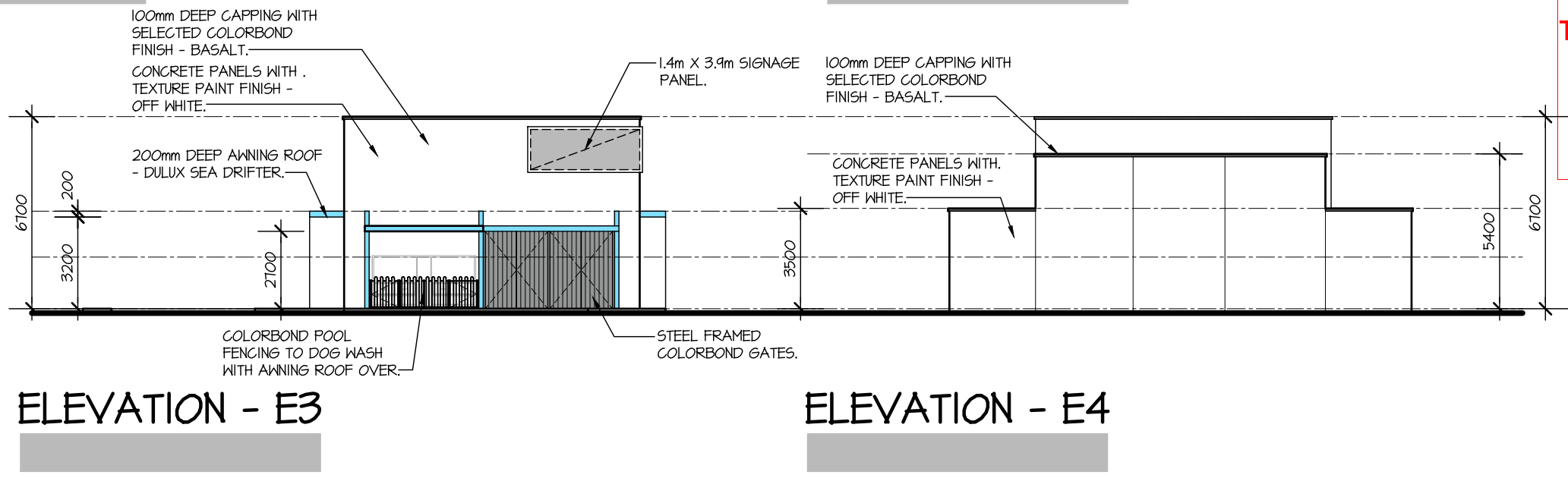
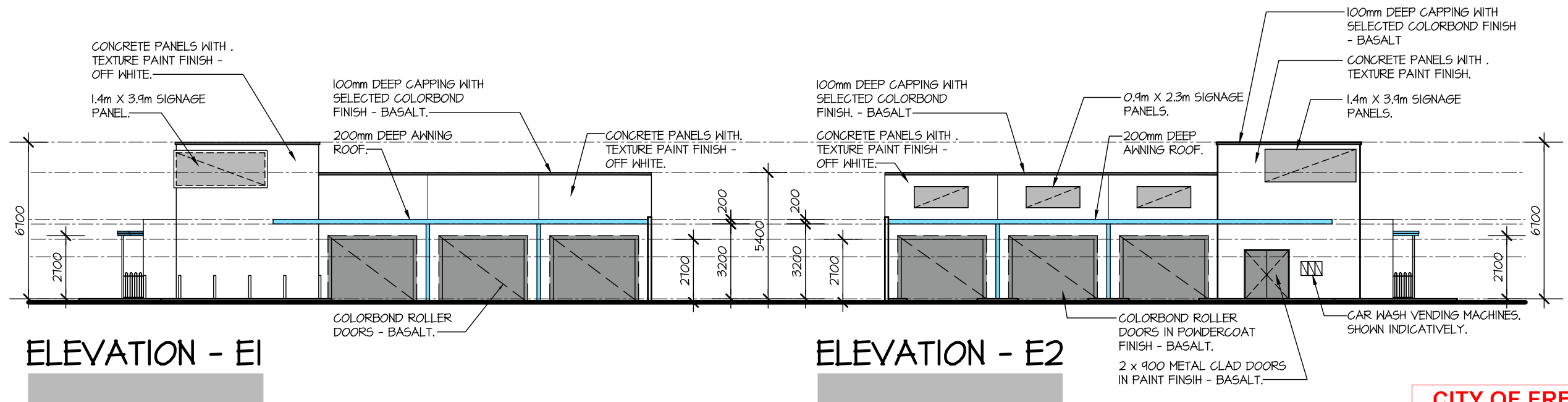
PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD. DO NOT SCALE FROM THIS DRAWING. THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE. THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.

NO:	DATE:	REVISION:	DRAWN:	CHECK:
A	08.10.24	DEVELOPMENT APPLICATION ISSUE	MDB	SJH

A3 SHEET

Date	-	08.10.24
Design	-	MDB
Drawn	-	MDB
Checked	-	SJH
Scale	-	1:100
Job No.	-	0887
Dwg.	-	DA02
Rev.	-	A



CITY OF FREMANTLE
 These Plans Form Part of
DAP004/24
5 Dec 2024

HA
 HINDLEY & ASSOCIATES
 BUILDING DESIGNERS
 166 STIRLING HIGHWAY
 NEDLANDS WA 6909
 PO BOX 199
 NEDLANDS WA 6909
 PHONE - 9386 6699
 admin@hindley.com.au
 www.hindley.com.au

DEVELOPMENT APPLICATION

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
 DO NOT SCALE FROM THIS DRAWING.
 THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE.
 THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.

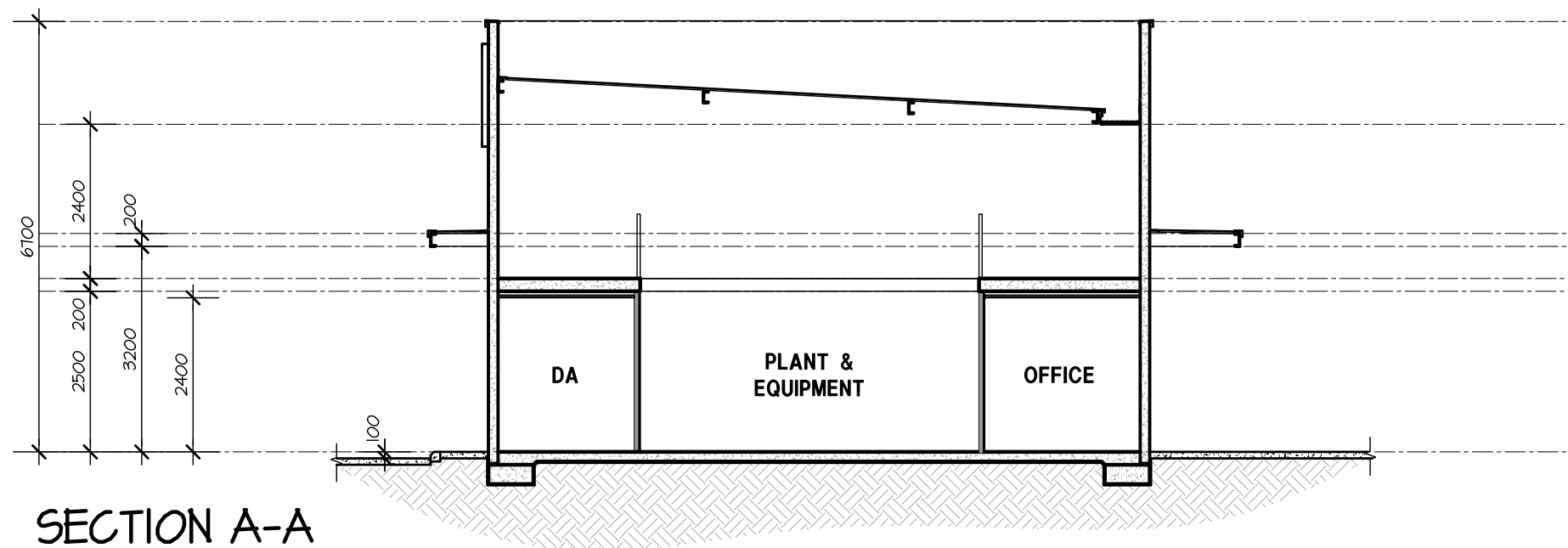
NO.	DATE	REVISION	DRAWN	CHECK
A	08.10.24	DEVELOPMENT APPLICATION ISSUE	MDB	SJH

A3 SHEET

ELEVATIONS
 SCALE 1:200

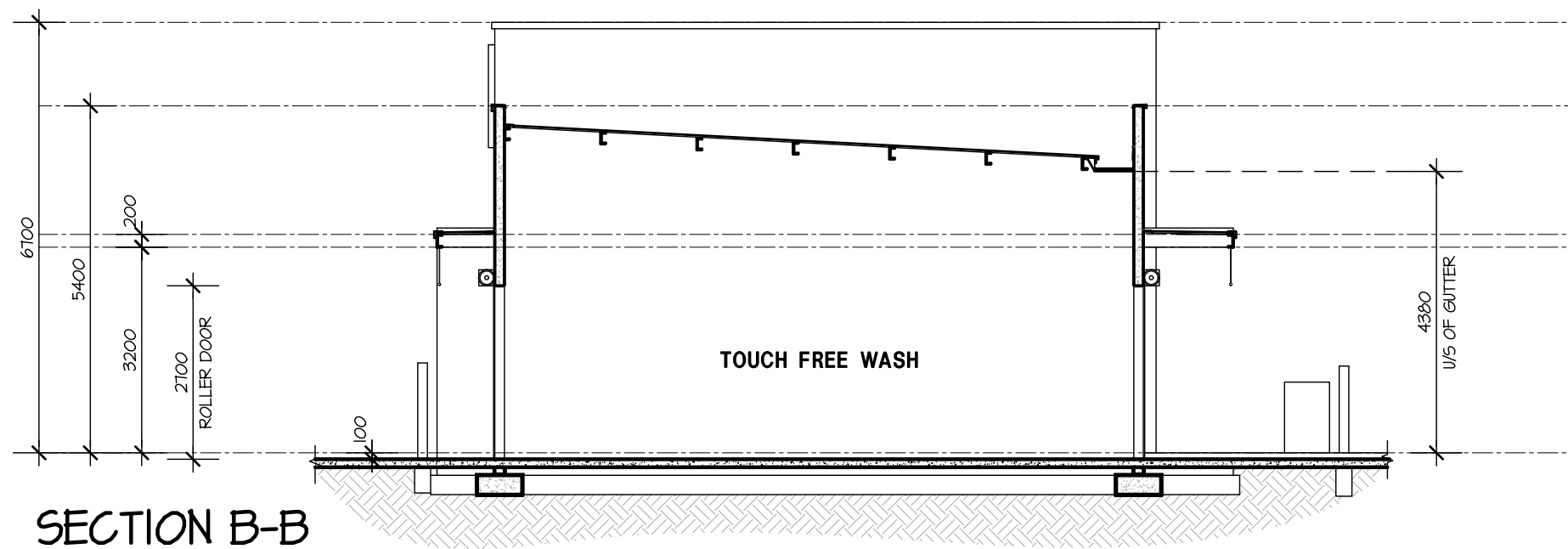
PROPOSED CARWASH
 No 234 TO 238
 LOTS 27, 300, 301 & 29
 SOUTH TERRACE CNR SOUTH STREET
 SOUTH FREMANTLE
 for GLOWMARK NOMINEES Pty Ltd

Date	08.10.24
Design	MDB
Drawn	MDB
Checked	SJH
Scale	1:200
Job No.	0987
Dwg No.	DA03
Rev	A



SECTION A-A

SCALE 1:100



SECTION B-B

SCALE 1:100



HINDLEY & ASSOCIATES
BUILDING DESIGNERS

166 STIRLING HIGHWAY
NEDLANDS WA 6909

PO BOX 199
NEDLANDS WA 6909

PHONE - 9386 6699
admin@hindley.com.au
www.hindley.com.au

DEVELOPMENT APPLICATION

SECTIONS
SCALE 1:100

A3 SHEET

PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
for GLOWMARK NOMINEES Pty Ltd

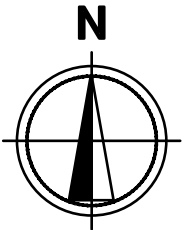
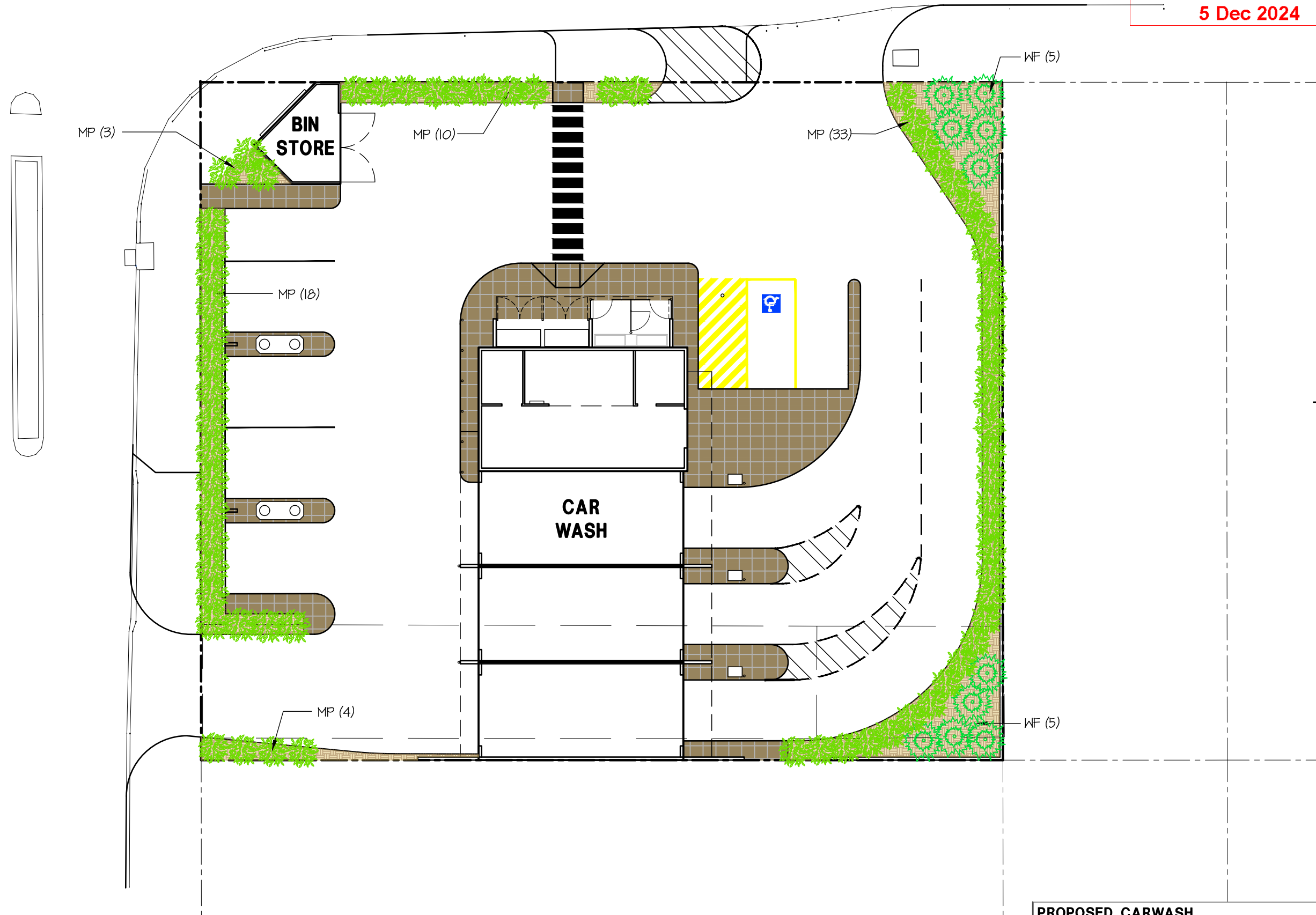
THIS DRAWING IS COPYRIGHT AND MUST NOT BE
RETAINED, COPIED OR USED WITHOUT THE
AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
DO NOT SCALE FROM THIS DRAWING.
THE CONTRACTOR AND HIS SUBCONTRACTORS ARE
TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING
SHOP DRAWINGS OR COMMENCING MANUFACTURE.
THIS IS A COMPUTER GENERATED DRAWING AND
SHALL NOT BE ALTERED BY HAND.

NO.	DATE	REVISION	DRAWN	CHECK
A	08.10.24	DEVELOPMENT APPLICATION ISSUE	MDB	SJH

Date	-	08.10.24
Design	-	MDB
Drawn	-	MDB
Checked	-	SJH
Scale	-	1:100
Job No.	-	0987
Dwg	-	DA04
Rev	-	A

SOUTH STREET

SOUTH TERRACE



HINDLEY & ASSOCIATES
BUILDING DESIGNERS

166 STIRLING HIGHWAY
NEDLANDS WA 6909

PO BOX 199
NEDLANDS WA 6909

PHONE - 9386 6699
admin@hindley.com.au
www.hindley.com.au

DEVELOPMENT APPLICATION

LANDSCAPING PLAN

SCALE 1:200

PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
for GLOWMARK NOMINEES Pty Ltd



THIS DRAWING IS COPYRIGHT AND MUST NOT BE
RETAINED, COPIED OR USED WITHOUT THE
AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
DO NOT SCALE FROM THIS DRAWING.
THE CONTRACTOR AND HIS SUBCONTRACTORS ARE
TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING
SHOP DRAWINGS OR COMMENCING MANUFACTURE.
THIS IS A COMPUTER GENERATED DRAWING AND
SHALL NOT BE ALTERED BY HAND.

NO.	DATE:	REVISION:	DRAWN:	CHECK:
A	08.10.24	DEVELOPMENT APPLICATION ISSUE	MDB	SJH

A3 SHEET

Date	-	08.10.24
Design	-	MDB
Drawn	-	MDB
Checked	-	SJH
Scale	-	1:250
Job No.	-	0887
Dwg	-	LS01
Rev	-	A

PLANT SCHEDULE

SYMBOL	BOTANIC NAME	MATURE HEIGHT X WIDTH	MINIMUM INSTALLATION POT SIZE	NUMBER
	MYOPORIUM PARVIFOLIUM	10cm X 1m	13cm	68
	WESTRINGIA FRUITCOSA	1.5m X 1.5m	13cm	10

SHRUB / GROUND COVER VARIETIES



WESTRINGIA FRUTICOSA



MYOPORIUM PARVIFOLIUM

FLOORING LEGEND



SELECTED MULCH UNDER PLANTING AS PER NOTES.



SELECTED PAVED FOOTPATH.

PLANTING NOTES / SPECIFICATIONS

- SPECIFIED PLANT SPECIES HAVE BEEN SOURCED FROM BENARA NURSERY & DOMUS NURSERY. SPECIES & POT SIZES INDICATED ARE SUBJECT TO AVAILABILITY AT TIME OF PLANTING.
- ALL PLANTING AREA ARE TO BE PREPARED AND PLANTED IN ACCORDANCE WITH INDUSTRY BEST PRACTICE AND THE INSTRUCTIONS BELOW.
 - PREPARATION OF SOIL IN GARDEN BED AREAS
 - REMOVE ALL TRACES OF BUILDERS MATERIAL FROM PLANTING AREAS INCLUDING RUBBLE, SAND, MORTAR AND ALL OTHER EXTRANEIOUS MATERIAL.
 - REMOVE ALL WEEDS IN GARDEN BEDS AREAS BY SPRAYING WEEDS WITH A STANDARD INDUSTRY HERBICIDE FOLLOWING MANUFACTURERS SPECIFICATIONS AND LEAVE FOR RECOMMENDED TIME.
 - REMOVE DEAD PLANT MATTER AFTER TIME SPECIFIED ON HERBICIDE PRODUCT.
 - UNDERTAKE SOIL IMPROVEMENT SUITABLE FOR NATIVE PLANTS.
 - MULCH
 - AFTER PLANTING, APPLY CHUNKY (20-25mm) PINE BARK WOOD CHIPS TO A MINIMUM DEPTH OF 75mm (MAXIMUM 100mm) TO ALL PLANTING BEDS KEEPING MULCH CLEAR OF PLANT STEMS.

IRRIGATION SPECIFICATIONS

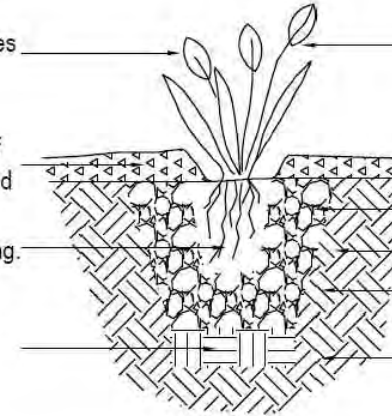
- ALL GARDEN BEDS TO BE IRRIGATED. WATER SUPPLY TO SCHEME WATER
- IRRIGATION LAYOUT BY OTHERS.

See Plant Schedule for minimum plant sizes required at planting.

Specified mulch to be spread to a depth of 75mm - create water retention area around plant.

Carefully tease out root ball prior to planting.

Cultivate native soil to a depth of 150mm under plant hole.



All shrubs and ground covers are to be healthy, well grown specimens, free of pests and diseases and not pot-bound.

150mm depth topsoil
Dig hole twice as wide and deep as root ball
Cultivate subgrade - break up sides and base of hole
Native soil

SHRUB PLANTING DETAILS

NOT TO SCALE

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024

THIS DRAWING IS COPYRIGHT AND MUST NOT BE RETAINED, COPIED OR USED WITHOUT THE AUTHORITY OF HINDLEY & ASSOCIATES PTY LTD.
DO NOT SCALE FROM THIS DRAWING.
THE CONTRACTOR AND HIS SUBCONTRACTORS ARE TO VERIFY ALL DIMENSIONS ON SITE BEFORE MAKING SHOP DRAWINGS OR COMMENCING MANUFACTURE.
THIS IS A COMPUTER GENERATED DRAWING AND SHALL NOT BE ALTERED BY HAND.

NO.	DATE	REVISION	DRAWN	CHECK
A	08.10.24	DEVELOPMENT APPLICATION ISSUE	MDB	SJH

A3 SHEET

LANDSCAPING SCHEDULE / NOTES

SCALE 1:200

PROPOSED CARWASH
No 234 TO 238
LOTS 27, 300, 301 & 29
SOUTH TERRACE CNR SOUTH STREET
SOUTH FREMANTLE
for GLOWMARK NOMINEES Pty Ltd

HA
HINDLEY & ASSOCIATES
BUILDING DESIGNERS
166 STIRLING HIGHWAY
NEDLANDS WA 6109
PO BOX 199
NEDLANDS WA 6909
PHONE - 9386 6699
admin@hindley.com.au
www.hindley.com.au

DEVELOPMENT APPLICATION

Date - 08.10.24
Design - MDB
Drawn - SJH
Checked - SJH
Scale - 1:250
Job No. - 0887
Dwg - **LS02**
Rev - **A**

CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



CITY OF FREMANTLE
These Plans Form Part of

DAP004/24

5 Dec 2024



SOUTH TERRACE, NO. 234 (LOT 27, 29, 300, 301), SOUTH FREMANTLE
- MOTOR VEHICLE WASH DEVELOPMENT AND SIGNAGE - (JD
DAPO04/24)



Photo 1: Front of subject site as viewed from the corner of South Terrace and South Street.



Photo 2: Subject site viewed from South Terrace.



Photo 3: Subject site as viewed from South Street.



Photo 4: Subject site as viewed from South Street.



Minutes

Ordinary Meeting of Council

Wednesday 9 July 2025 6:00 pm



C2507-3 SOUTH TERRACE, NO. 234 (LOT 27, 29, 300, 301), SOUTH
FREMANTLE - MOTOR VEHICLE WASH DEVELOPMENT AND
SIGNAGE - (JD DAP004/24)

Meeting date:	9 July 2025
Responsible officer:	Manager Development Approvals
Voting requirements:	Simple Majority Required
Attachments:	1. Amended Development Plans 2. Acoustic Report
Additional Information: <i>(viewed electronically)</i>	3. Amended Operational and Noise Management Plan 4. Amended Transport Impact Statement and Peer Review 5. Stormwater and Sewer Management Plan 6. Planning Report 7. Heritage Officer Assessment 8. Response to City of Fremantle Feedback 9. Schedule of Submissions and Applicant Response 10. Original Development Plans (Obsolete) 11. Site Photos

SUMMARY

Approval is sought for the demolition of an existing building and the construction of a motor vehicle wash development and signage at No. 234 (Lot 27, 29, 300 & 301) South Terrace, South Fremantle.

As the development value is \$3.5 million, the applicant has opted to have the application determined by the Metro Inner Development Assessment **Panel (DAP)**. **The City's Responsible Authority Report (RAR) is referred to Council for comment.**

The application is recommended for approval.

PROPOSAL

Detail

Approval is sought for the demolition of an existing building and the construction of a Motor vehicle wash development and signage at No. 234 (Lot 27, 29, 300 & 301) South Terrace, South Fremantle. The proposed works include:

- Demolition of the existing building on site.



- Construction of a Motor vehicle wash building consisting of three (3) self-closing car wash bays and a plant room.
- Modification of the existing crossovers on South Street and South Terrace (cars to enter from South Street and exit via South Terrace).
- Installation of four (4) vacuum car bays and three (13) car parking bays.
- Installation of a dog wash facility.
- Construction of a bin store on the corner of South Street and South Terrace.
- Construction of a 2.1m high screen fence along the east side boundary.
- Installation of signage.
- Installation of landscaping.

The applicant submitted amended plans/additional information on 12 June 2025 including the following:

- Modification of the Motor vehicle wash building roof form.
- Amendments to the colour palette and external finishes.
- Amendments to the Transport Impact Statement (including a peer review) and Operational and Noise Management Plan.
- A response to City of Fremantle feedback and additional justification on the merits of the proposal.

Amended development plans are included as Attachment 1.

Site/application information

Date received:	5 December 2024
Owner name:	Glowmark Nominees Pty Ltd
Submitted by:	Apex Planning
Scheme:	Mixed Use R30
Heritage listing:	South Fremantle Precinct Heritage Area
Existing land use:	Educational Establishment
Use class:	Motor Vehicle Wash
Use permissibility:	A



STRATEGIC IMPLICATIONS

This item is in keeping with the City of Fremantle's Strategic Community Plan 2024 – 2034:

Liveable City - Sustainable growth in city centre population

- Fremantle is recognised as a development-friendly city as a result of flexible and adaptable approaches to planning.

VOTING AND OTHER REQUIREMENTS

Simple Majority Required



OFFICER'S RECOMMENDATION

Moved: Cr Frank Mofflin

Seconded: Cr Jenny Archibald

Council:

SUPPORT the Officer's Recommendation to APPROVE, under the Metropolitan Region Scheme and Local Planning Scheme No. 4, the demolition of an existing building and the construction of a Motor vehicle wash development and signage at No. 234 (Lot 27, 29, 300 & 301) South Terrace, South Fremantle, subject to the conditions outlined in the responsible authority report.

In accordance with clause 9.7 of the [Meeting Procedures Policy](#), Cr Ben Lawver moved the following amendment, as provided in the Additional Documents:

AMENDMENT

Moved: Cr Ben Lawver

Seconded: Cr Adin Lang

Amend proposed Condition 18 in the Responsible Authority Report as follows:

18. *The Motor Vehicle Wash and all associated facilities, including the dog wash and all vacuums, shall be limited to the operating hours of between 6.30am and ~~10.30pm~~ 7pm.*

Amendment Carried: 9/0

For:

Mayor Hannah Fitzhardinge, Cr Jenny Archibald, Cr Geoff Graham,
Cr Adin Lang, Cr Andrew Sullivan, Cr Jemima Williamson-Wong
Cr Fedele Camarda, Cr Ben Lawver, and Cr Frank Mofflin

Against:

Nil

Reasons for amendment:

To ensure noise impacts and light overspill associated with the land use remain compatible with the surrounding residential zone where such a land use is an 'X' use and not permitted.



COUNCIL DECISION ITEM C2507-3
(Amended officer's recommendation)

Moved: Cr Frank Mofflin

Seconded: Cr Jenny Archibald

Council:

SUPPORT the Officer's Recommendation to **APPROVE**, under the Metropolitan Region Scheme and Local Planning Scheme No. 4, the demolition of an existing building and the construction of a Motor vehicle wash development and signage at No. 234 (Lot 27, 29, 300 & 301) South Terrace, South Fremantle, subject to the conditions outlined in the responsible authority report.

Lost: 0/9

For:

Nil

Against:

Mayor Hannah Fitzhardinge, Cr Jenny Archibald, Cr Geoff Graham,
Cr Adin Lang, Cr Andrew Sullivan, Cr Jemima Williamson-Wong
Cr Fedele Camarda, Cr Ben Lawver, and Cr Frank Mofflin

In accordance with clause 9.7 of the [Meeting Procedures Policy](#), Cr Andrew Sullivan moved the following alternative motion, as provided in the Additional Documents, with minor amendments accepted by the Presiding Member:

COUNCIL DECISION ITEM C2507-3
(Alternative motion)

Moved: Cr Andrew Sullivan

Seconded: Cr Fedele Camarda

Council:

DO NOT SUPPORT the Officer's Recommendation to **APPROVE**, under the Metropolitan Region Scheme and Local Planning Scheme No. 4, the demolition of an existing building and the construction of a Motor vehicle wash development and signage at No. 234 (Lot 27, 29, 300 & 301) South Terrace, South Fremantle, and offer the following recommendation:

That the Metro Inner DAP resolves to:

1. Not Accept that the DAP Application for a "Motor Vehicle Wash", reference DAP/24/02818, is appropriate for consideration as land use and form compatible with the objectives of the Mixed Use zone



(Clause 3.2.1 e) in accordance with City of Fremantle Local Planning Scheme No. 4 for the following reasons:

- a. **In relation to part (i), the land use configured in the form proposed is incompatible within the immediate locality and the South Fremantle heritage area more generally. The development is inconsistent with and would have a detrimental impact on the viability of nearby mixed use developments including those with small commercial shop fronts with traditional 'high street' frontages. Locating the proposed land use outside of a Neighbourhood Centre, Commercial and/or Industrial zone is detrimental to the viability and function of those zones. The proposed land use and form of development would be more appropriately located accommodated in a Commercial, Industrial or Neighbourhood Centre zone;**
 - b. **In relation to part (ii), the proposal fails to provide for residential development either at an upper level and/or at the ground level, and fails to establish the active public domain required in a Mixed Use zone. The proposed use in isolation, a use that would be an 'X' use in the surrounding Residential zones, would have a detrimental impact on the amenity and viability of residential and other non-residential uses in the mixed use zone and the locality generally;**
 - c. **In relation to part (iii), the form of development is inconsistent with the desired future character of the area, negatively impacting the cultural heritage significance of the South Fremantle heritage area generally and the immediate locality specifically;**
 - d. **In relation to part (iv), the form of development and the operations as proposed would further erode the amenity for residents in the immediate locality due to its undesirable impact on the streetscape, light overspill, noise impacts over long periods of the day and evening, and vehicles exiting the site wanting to head north or east impacting local residential streets; and,**
 - e. **In relation to part (v), the form of development as proposed blights the capacity to ensure the ongoing conservation of places of heritage significance in the immediate locality.**
2. Refuse DAP Application reference DAP/24/02818 and accompanying plans (attachment DA01 (Rev. D), DA02 (Rev. D), DA03 (Rev. E), DA04 (Rev. D), LS01 (Rev. D), LS02 (Rev. D)) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the Planning and Development (Local Planning Schemes) Regulations 2015, and the



provisions of and the provisions of City of Fremantle Local Planning Scheme No. 4, for the following reasons:

- a. The proposed built form is detrimental to the heritage significance of the South Fremantle Heritage Area in accordance with the objectives of LPP 3.6 Heritage Areas policy (current and draft),
- b. The proposed car wash land use is incompatible with the objectives of the Mixed Use zone in Local Planning Scheme No. 4, having regard to the desired future character of the area which seeks a mix of commercial and residential uses, and the relationship of the development with existing surrounding uses.
- c. The proposed car wash will have an impact on the traffic flow and pedestrian safety in the area in accordance with Clause 67, Schedule 2 of the Planning and Development (Local Planning Schemes) No. 4. Specifically, resultant vehicle queuing onto South Street and the potential for an increase in congestion will be to the detriment of the traffic network, and may result in an unacceptable risk to pedestrians.
- d. The proposed car wash will impact on the amenity of surrounding residential land uses with regard to noise and activity outside of acceptable hours.

Carried: 9/0

For:

Mayor Hannah Fitzhardinge, Cr Jenny Archibald, Cr Geoff Graham,
Cr Adin Lang, Cr Andrew Sullivan, Cr Jemima Williamson-Wong
Cr Fedele Camarda, Cr Ben Lawver, and Cr Frank Mofflin

Against:

Nil

Reasons for alternative motion:

The proposed land use is deemed inappropriate for discretionary approval having given due regard to the submissions made after giving special notice in accordance with clause 64 of the Planning and Development (Local Planning Schemes) Regulations 2015, Schedule 2.

The Motor Vehicle Wash ('car wash') proposal is for a stand-alone use and presents as a conventional car wash form that would be appropriately located within a Neighbourhood Centre, Industrial or Commercial zone where such activity is typically separated from residential zones and where the more



commercial/utilitarian form of development can be accommodated. These zones are specifically established to accommodate uses like a car wash and permitting such uses to be established outside of these zones could be detrimental to the function, viability and diversity of these zones.

The Mixed Use zone, especially when presented as a traditional strip within a recognised heritage area, provides for a somewhat blurred interface between compatible small scale non-residential land uses and the residential side streets. Mixed Use development is also intended to deliver sympathetic urban forms that accommodate a diverse mix of uses and activities that can be successfully interweaved within the predominant residential enclave and which support a vibrant local lifestyle within a walkable neighbourhood. That is, both the land use(s) and the form of development need to be compatible within the surrounding residential zone and sympathetic to the heritage area and desired character of the area.

While it is acknowledged that this car wash, as an 'A' use in LPS4, may be considered subject to discretion, the form of this proposal presents as being wholly inconsistent within the South Fremantle Mixed Use zone. The use is presented within a stand-alone building surrounded by open space accommodating industrial-like functions. The typology of development in isolation is incapable of delivering the suitable urban form expected within the Mixed Use zone. Likewise, it is neither complimentary or compatible in juxtaposition with the abutting residential developments.

As the car wash function is not shrouded by the inclusion of other elements and/or land uses on the development site, it will have a detrimental impact on the amenity and built form of the immediate locality and the South Fremantle Heritage Area, including as follows:

1. The development proposes a wholly inconsistent built form for this gateway site to the South Fremantle Mixed Use zone and heritage area.
2. The excessive setback of the building from the surrounding streets, the signage, and the placement of bin stores and high screens around the site perimeter collectively present a built form that is the antithesis of the form that is reasonably anticipated on a landmark corner.
3. The LPS4 requires that mixed use development "shall provide for residential uses at upper level" and "provide for a mix of compatible land uses". While it might be argued that these requirements are intended to be met across a whole zone, the strip form and non-contiguous nature of mixed use zones in South Fremantle requires more emphasis on achieving suitable mixed-use outcomes on individual sites. Furthermore, the subject site is large compared



to most sites along the South Terrace mixed use area and there is a reasonable expectation that a genuine mix of uses should be required on larger sites to deliver the diversity of uses and suitable forms expected in a mixed use zone.

4. The closely intertwined and blurred nature of Mixed Use zones within Residential zones places consider emphasis on ensuring that land uses that would not be permitted in the Residential Zone are adequately separated or screened from residences when considered for approval in an abutting mixed use zone. The car wash use would not be capable of approval on the adjoining residential lots but it is not contained within a mixed use development so its impact on the abutting residences should be assessed as if the use was being proposed in a residential zone, i.e., not permitted. The proposal does not provide adequate separation and has a detrimental amenity impact resulting from long hours of noise generation, light overspill, and built forms that are alien within a predominantly residential area.
5. The nature of the site on a busy signalised intersection results in all vehicles exiting the site having to turn left onto South Terrace. Exiting vehicles travelling to destinations that are either to the north or east of the car wash will almost certainly seek to turn off South Terrace into the very tight network of side streets seeking ways to return to the distributor roads heading north and east. The resulting 'rat-runs' through the surrounding convoluted road network will have a detrimental impact on the amenity of nearby residents.



LOT NO. 27, 29, 300 & 301 (NO. 234) SOUTH TERRACE SOUTH
 FREMANTLE – MOTOR VEHICLE WASH DEVELOPMENT

Form 1 – Responsible Authority Report
 (Regulation 12)

DAP Name:	Metro Inner	
Local Government Area:	City of Fremantle	
Applicant:	Apex Planning	
Owner:	Glowmark Nominees Pty Ltd	
Value of Development:	\$3.5 million	
Responsible Authority:	City of Fremantle	
Authorising Officer:	Manager Development Approvals	
LG Reference:	DAP004/24	
DAP File No:	DAP/24/02818	
Application Received Date:	5 December 2024	
Report Due Date:	10 July 2025	
Application Statutory Process Timeframe:	90 Days	
Attachment(s):	<ol style="list-style-type: none"> 1. Amended Development Plans 2. Acoustic Report 3. Amended Operational and Noise Management Plan 4. Amended Transport Impact Statement 5. Stormwater and Sewer Management Plan 6. Planning Report 7. Heritage Officer Assessment 8. Response to City of Fremantle Feedback 9. Schedule of Submissions and Applicant Response 10. Original Development Plans 11. Site Photos 	
Is the Responsible Authority Recommendation the same as the Officer Recommendation?	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	Complete Responsible Authority Recommendation section
	<input type="checkbox"/> No	Complete Responsible Authority and Officer Recommendation sections



Officer Recommendation

That the Metro Inner DAP resolves to:

1. Accept that the DAP Application reference DAP/24/02818 is appropriate for consideration as a "Motor Vehicle Wash" land use and compatible with the objectives of the zoning table in accordance with City of Fremantle Local Planning Scheme No. 4;
2. Approve DAP Application reference DAP/24/02818 and accompanying plans (attachment DA01 (Rev. D), DA02 (Rev. D), DA03 (Rev. E), DA04 (Rev. D), LS01 (Rev. D), LS02 (Rev. D)) in accordance with Clause 68 of Schedule 2 (Deemed Provisions) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, and the provisions of and the provisions of City of Fremantle Local Planning Scheme No. 4, subject to the following conditions:

Conditions

1. This decision constitutes planning approval only and is valid for a period of 4 years from the date of approval. If the subject development is not substantially commenced within the specified period, the approval shall lapse and be of no further effect.
2. Prior to lodgement of a Building Permit application for the development hereby approved, storm water and waste water disposal plans, details and calculations must be submitted for approval by the City of Fremantle and thereafter implemented, constructed and maintained to the satisfaction of the City of Fremantle.
3. Prior to lodgement of a Building Permit and/or Demolition Permit application for the development hereby approved, a Construction/Demolition Management Plan shall be submitted and approved, to the satisfaction of the City of Fremantle addressing, but not limited to, the following matters:
 - a) Use of City car parking bays for construction related activities;
 - b) Protection of infrastructure and street trees within the road reserve;
 - c) Security fencing around construction sites;
 - d) Gantries;
 - e) Access to site by construction vehicles;
 - f) Contact details;
 - g) Site offices;



- h) Noise - Construction work and deliveries;
 - i) Sand drift and dust management;
 - j) Waste management;
 - k) Dewatering management plan;
 - l) Traffic management; and
 - m) Works affecting pedestrian areas.
4. Prior to lodgement of a Building Permit application for the development hereby approved, storm water disposal plans, details and calculations must be submitted for approval by the City of Fremantle and thereafter implemented, constructed and maintained to the satisfaction of the City of Fremantle.
 5. Prior to the lodgement of a Building Permit application for the development hereby approved, design plans for the location, materials and construction for screening of any proposed external building plant must be submitted to and approved by the City of Fremantle. All piped, ducted and wired services, air conditioners, hot water systems, water storage tanks, service meters, other plant and bin storage areas must be integrated into the design of the building and located to minimise any visual and/or noise impact on the occupants of nearby properties and screened from view from the street.
 6. Prior to lodgement of a Building Permit application for the development hereby approved, an outdoor lighting plan must be submitted and approved by the City of Fremantle. The outdoor lighting is to be designed, baffled and located to prevent any increase in light spill onto the adjoining properties.
 7. Prior to lodgement of a Building Permit application for the development hereby approved, the applicant is to submit, and have approved to the satisfaction of the City of Fremantle, a detailed parking and vehicle circulation plan design which complies with the Australian Standard AS/NZS 2890 and AS/NZSC 1428, including parking bay/s, visitor bays, loading bays, disabled bays, aisle widths, circulation areas, driveway/s and points of ingress and egress.
 8. Prior to lodgement of a Building Permit application for the development hereby approved, final details of the external materials, colours and finishes of the proposed development, including a physical sample board or materials is to be submitted and approved to the satisfaction of the City of Fremantle, on the advice of the City's Design Advisory Committee.



9. Prior to lodgement of a Building Permit application for the development hereby approved, details of how the recommendations contained in the environmental acoustic report prepared by Herring Storer Associates, dated 18 October 2024 (Ref: 32932-4-24207), are to be implemented into the design/operation of the development are to be submitted and approved to the satisfaction of the City of Fremantle and thereafter maintained for the life of the development.
10. Prior to the lodgement of a Building Permit application for the development hereby approved, details on how vehicle access and egress to the subject site shall be restricted to the following movements:
 - a) Left turn in only via the South Street crossover from vehicles travelling in a westerly direction along South Street; and
 - b) Left turn out only via the South Terrace crossover for vehicles to exit the site and travel in a southerly direction along South Terrace).

The approved traffic access and egress shall be maintained for the life of the development to the satisfaction of the City of Fremantle.

11. Prior to the occupation of the development hereby approved, vehicle crossovers, including any approved works required to restrict vehicle access and egress, shall be constructed to the City's specification and thereafter maintained to the satisfaction of the City of Fremantle.
12. Prior to the occupation of the development hereby approved, any redundant crossovers shall be removed and the verge and kerbing reinstated to the City's specifications, at the expense of the applicant and to the satisfaction of the City of Fremantle.
13. Prior to occupation of the development hereby approved, the applicant is to submit, and have approved to the satisfaction of the City of Fremantle, an Operational Management Plan addressing the following:
 - Operating hours
 - Noise mitigation measures
 - Operation of the motor vehicle wash facilities
 - Deactivation process for wash and vacuum bays during the night period
 - Site and car wash maintenance and servicing procedures



- Waste management
- Garden and landscaping maintenance
- Emergency contact details and complaints management

The Operational Management Plan must be implemented at all times to the satisfaction of the City of Fremantle for the life of the development.

14. Prior to occupation of the development hereby approved, the approved landscaping shall be completed in accordance with the approved plans and maintained for the life of the development to the satisfaction of the City of Fremantle.
15. Prior to the occupation of the development hereby approved, all car parking, and vehicle access and circulation areas shall be maintained and available for car parking/loading, and vehicle access and circulation on an ongoing basis to the satisfaction of the City of Fremantle.
16. Prior to occupation of the development hereby approved, No. 234 (Lots 27, 29, 300 & 301) South Terrace are to be legally amalgamated into one lot on the Certificate of Title. Alternatively, the owner may enter into a legal agreement with the City of Fremantle, drafted by the City's solicitors at the expense of the owner and be executed by all parties concerned. The legal agreement will specify measures to allow the development approval to operate having regard to the subject site consisting of two separate lots, to the satisfaction of the City of Fremantle.
17. Prior to occupation/ use of the development hereby approved, the boundary fence located on the north-west and west boundaries shall be of a clean finish in any of the following materials:
 - coloured sand render,
 - face brick,
 - painted surface,
 - other approved finish

and be thereafter maintained to the satisfaction of the City of Fremantle.

18. The Motor Vehicle Wash and all associated facilities, including the dog wash and all vacuums, shall be limited to the operating hours of between 6:30am and 7:00pm.



19. The self-closing doors at the entry and exit of the car wash bays are to remain operational at all times and maintained for the life the development to the satisfaction of the City of Fremantle.
20. All works indicated on the approved plans, including any footings, shall be wholly located within the cadastral boundaries of the subject site.
21. Where any of the preceding conditions has a time limitation for compliance, if any condition is not met by the time requirement within that condition, then the obligation to comply with the requirements of any such condition (other than the time limitation for compliance specified in that condition), continues whilst the approved development continues.

Advice note(s):

1. A Building permit is required for the proposed Building Works. A certified BA1 application form must be submitted and a Certificate of Design Compliance (issued by a Registered Building Surveyor Contractor in the private sector) must be submitted with the BA1.
2. A demolition permit is required to be obtained for the proposed demolition work. The demolition permit must be issued prior to the removal of any structures on site.
3. With regards to condition 10 above, the applicant is encouraged to contact the **City of Fremantle's Infrastructure Business Services** department via info@fremantle.wa.gov.au or 9432 9999 to consider the works to restrict vehicle movements to left turn in/left turn out, such as median island extensions, line marking and signage. All works are to be completed to **City of Fremantle standards and specifications at the applicant's expense.**
4. Effective measures shall be taken to stabilize sand and ensure no sand escapes from the property by wind or water in accordance with the City's Prevention and Abatement of Sand Drift Local Law.
5. Any works within the adjacent thoroughfare, i.e. road, kerbs, footpath, verge, crossover or right of way, requires a separate approval from the **City of Fremantle's Infrastructure Business Services** department who can be contacted via info@fremantle.wa.gov.au or 9432 9999.
6. If construction works involve the emission of noise above the assigned levels in the Environmental Protection (Noise) Regulations 1997, they should only occur on Monday to Saturday between 7.00 am and 7.00 pm



(excluding public holidays). In instances where such construction work needs to be performed outside these hours, an Application for Approval of a Noise Management Plan must be submitted to the City of Fremantle Environmental Health Services for approval at least 7 days before construction can commence.

Note: Construction work includes, but is not limited to, Hammering, Bricklaying, Roofing, use of Power Tools and radios etc.

7. All noise from the proposed development must comply with the requirements of the Environmental Protection (Noise) Regulations 1997 (as amended), such as:
 - a) mechanical service systems like air-conditioners, exhaust outlets, motors, compressors and pool filters;
 - b) vehicles;
 - c) amplified acoustic systems; and
 - d) patron noise.

It is advised to seek the services of a competent acoustic consultant to assist the applicant to address the potential noise impacts on noise sensitive receivers.

8. Any removal of asbestos is to comply with the following –

Under ten (10) square metres of bonded (non-friable) asbestos can be removed without a license and in accordance with the Health (Asbestos) Regulations 1992 and the Environmental Protection (Controlled Waste) Regulations 2001. Over 10 square metres must be removed by a Class B asbestos removal licence holder for. All asbestos removal is to be carried out in accordance with the Work Health and Safety Act 2020 and accompanying regulations and the requirements of the Code of Practice for the Safe Removal of Asbestos 2nd Edition [NOHSC: 2002 (2005)];

Note: Removal of any amount of friable asbestos must be done by a Class A asbestos removal licence holder and an application submitted to WorkSafe, Department of Commerce.

<https://www.commerce.wa.gov.au/worksafe/>.

9. The applicant is advised that a crossover permit must be obtained from the City's Engineering Department. New/modified crossover(s) shall



comply with the City’s standard for crossovers, which are available on the City of Fremantle’s web site.

The applicant is advised that the /The new/ modified vehicle crossover shall be separated from any verge infrastructure by:

- a) a minimum of 2.0 metres in the case of verge trees
- b) a minimum of 1.2 metres (in the case of bus shelters, traffic management devices, parking embayment’s or street furniture), and
- c) a minimum of 1.0 metre in the case of power poles, road name and directional signs.

10. The applicant is advised that any signage may be subject to a separate application for planning approval.

Details: outline of development application

Region Scheme	Metropolitan Region Scheme
Region Scheme - Zone/Reserve	Urban Zone
Local Planning Scheme	Local Planning Scheme No. 4
Local Planning Scheme - Zone/Reserve	Mixed Use Zone
Structure Plan/Precinct Plan	N/A
Structure Plan/Precinct Plan - Land Use Designation	N/A
Use Class and permissibility:	Motor Vehicle Wash – A
Lot Size:	1321m ²
Existing Land Use:	Educational Establishment
State Heritage Register	No
Local Heritage	<input type="checkbox"/> N/A <input type="checkbox"/> Heritage List <input checked="" type="checkbox"/> Heritage Area
Design Review	<input checked="" type="checkbox"/> N/A <input type="checkbox"/> Local Design Review Panel <input type="checkbox"/> State Design Review Panel <input type="checkbox"/> Other



Bushfire Prone Area	No
Swan River Trust Area	No

Proposal:

The application seeks approval for the demolition of an existing building and the construction of a Motor vehicle wash development and signage at No. 234 (Lot 27, 29, 300 & 301) South Terrace, South Fremantle (subject site). The proposed development includes the demolition of the existing building and the construction of a Motor vehicle wash and associated facilities. The key components of the proposed development can be seen at Attachment 1 and are summarised as follows:

Proposed Land Use	Motor Vehicle Wash (A)
Proposed Net Lettable Area	N/A
Proposed No. Storeys	Single Storey
Proposed No. Dwellings	N/A

The works component of the development includes:

- Demolition of the existing building on site.
- Construction of a Motor vehicle wash building consisting of three (3) self-closing car wash bays and a plant room.
- Modification of the existing crossovers on South Street and South Terrace (cars to enter from South Street and exit via South Terrace).
- Installation of four (4) vacuum car bays and three (13) car parking bays.
- Installation of a dog wash facility.
- Construction of a bin store on the corner of South Street and South Terrace.
- Construction of a 2.1m high screen fence along the east side boundary.
- Installation of signage.
- Installation of landscaping.

The applicant submitted amended plans/additional material on 12 June 2025 including the following:

- Modification of the Motor vehicle wash building roof form and materials.
- Amendments to the colour palette and external finishes.



- Amendments to the Transport Impact Statement (including a peer review) and Operational Management Plan (to further address noise management).
- A response to City of Fremantle feedback and additional justification on the merits of the proposal.

Amended development plans are included as Attachment 1. The original development plans are included as additional information.

Background:

Subject Site

The subject site features an existing building being used as an Educational establishment. The site was previously used as a boat sales yard and other related commercial activities. The site is a corner lot fronting South Street to the north and South Terrace to the west and adjoins a residential lot to the east and shops and residential dwellings to the south. The site is zoned Mixed Use under LPS4 and is within the South Fremantle Precinct Heritage Area. No. 234 South Terrace consists of four (4) lots featuring a combined land area of 1321m² as detailed in the table below. A condition is recommended for the lots to amalgamated prior to occupation of the development.

ADDRESS	LANDOWNER	VOL.	FOLIO	AREA (M ²)
Lot 27 (No. 234) South Terrace, South Fremantle	Glowmark Nominees Pty Ltd	1965	368	445m ²
Lot 29 (No. 234) South Terrace, South Fremantle	Glowmark Nominees Pty Ltd	1965	369	438m ²
Lot 300 (No. 234) South Terrace, South Fremantle	Glowmark Nominees Pty Ltd	1965	368	48m ²
Lot 301 (No. 234) South Terrace, South Fremantle	Glowmark Nominees Pty Ltd	1965	369	390m ²

Site History



A search of the property file has revealed the following history for the site:

- Temporary shelter addition to an existing Educational establishment – DA0289/24
- Change of use to boat sales yard and office and the construction of a 1.8m high chainmesh fence – DA250/88A – 1990
- Change of use to real estate offices/marine broker/travel agency and the extension of existing office
- Carport addition to south east corner of site – 8624/1975
- Office and workshop construction for a car sales yard – 1974

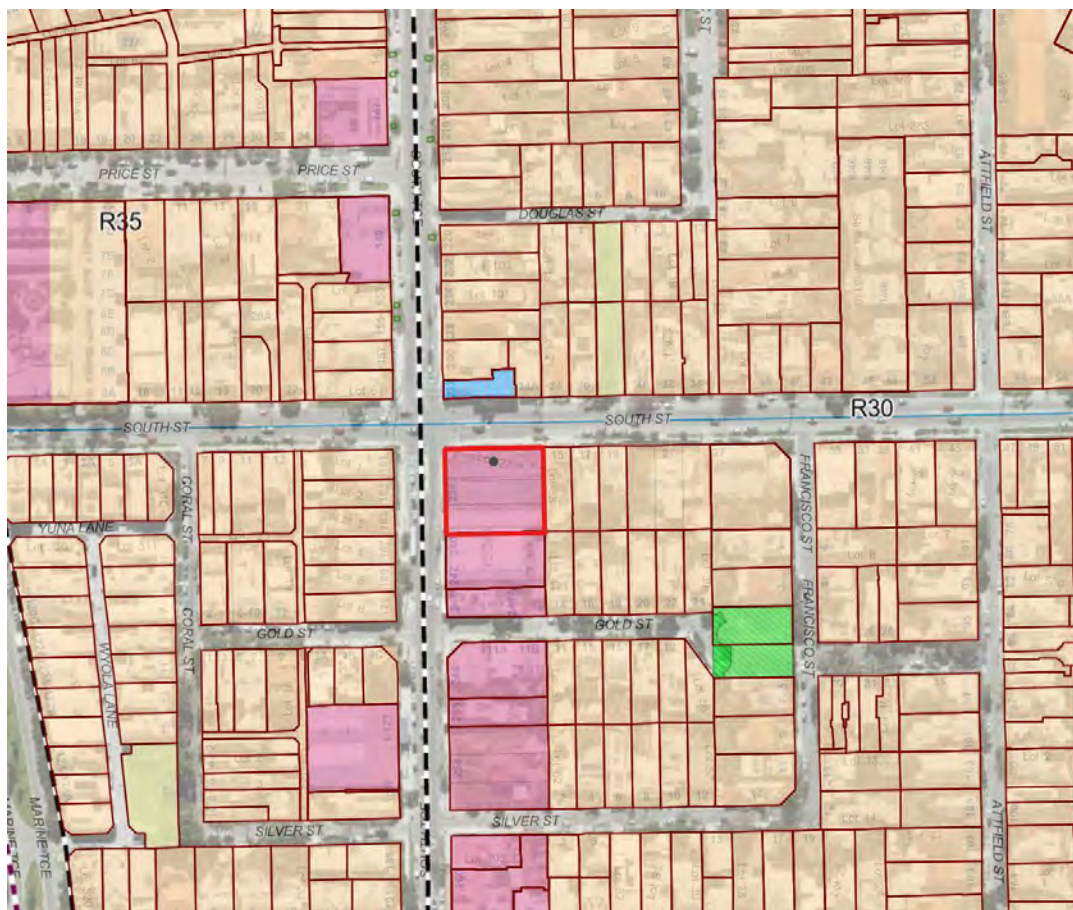


Figure 1 – Planning Context Map



Figure 2 – Site Aerial Image



Figure 3 – Existing Street View



Legislation and Policy:

Legislation

1. *Planning and Development (Local Planning Schemes) Regulations 2015 (WA)*
2. Metropolitan Region Scheme
3. City of Fremantle Local Planning Scheme No. 4

Planning and Development (Local Planning Schemes) Regulation 2015 (WA):

- Schedule 2, Part 8, Clause 64 – Advertising applications
- Schedule 2, Part 9, Clause 67 – Matters to be considered by local government
- Schedule 2, Part 9, Clause 68 – Determination of applications
- Schedule 2, Part 9, Clause 70 – Form and date of determination

Local Planning Scheme No. 4:

- 3.2.1 - (b) Mixed Use zone objectives
- Table 1 - Zoning
- 4.7 - Vehicle Parking

State Government Policies

- State Planning Policy 7.0: Design of the Built Environment

Local Policies

- Local Planning Policy 1.1 – Planning Refunds, Amendments, and Community Consultation
- Local Planning Policy 1.6 – Heritage Assessment and Protection
- Local Planning Policy 1.10 – Construction Sites
- Local Planning Policy 2.8 – Fences
- Local Planning Policy 2.14 – Advertisement Policy
- Local Planning Policy 2.24 – Waste Management Plans for New Development
- Local Planning Policy 3.6 – Heritage Areas

The development assessment has been undertaken in accordance with the abovementioned legislation and policies.



Consultation:

Public Consultation

In accordance with LPP1.1 (Planning Refunds, Amendments, and Community Consultation), the application was advertised to the public from the 15 January 2025 until 14 February 2025 by means of letters to owners/occupiers of properties within a 200m radius of the site, a sign on the street frontage, the City’s MySay webpage and notices in the press. A ‘Talk to a Planner’ session was also held on 25 February 2025 at the City’s library.

In response, the City received a total of 91 submissions. 81 submissions were received that objected to the proposal and 10 submissions were received in support of the proposal. A schedule of the public submissions which includes the full details on each submission as well as the applicant’s response to these submissions is included as additional information.

A summary of the key points raised in the submission in support of the proposal are detailed below:

- The location will provide an opportunity for surrounding residents to wash their vehicles.
- The proposal appears to be an improvement in aesthetic appearance compared to the existing development and is an appropriate use given that many vehicles in the locality are forced to park on the street and will then be capable of being washed without the chemical residue running into the storm water drainage system.
- The proposed dog wash will provide a location for surrounding residents to wash their dogs after visits to the beach.
- The proposed landscaping will be an improvement over the site’s current existing paved bitumen surface.
- The proposal is a good utilisation of the site and will provide useful amenities to the residents and ratepayers.

A summary of the key points raised in objection to the proposal are included in the table below:

Table 1 – Summarised Objections and Officer Comments

Issue Raised	Officer comments
The location is not appropriate and will be dangerous for pedestrians. The proposal will generate too much traffic for the area and will result in	The Traffic Impact Statement indicates that the development has the potential to generate up to 18 vehicles per hour. It is expected that many people using



<p>an increase in traffic congestion. The South Street/South Terrace intersection is one of the most heavily used entry points to the city for both private and public transport. The vehicles exiting onto the street will result in increased traffic through back streets.</p>	<p>the car wash would be residents of the area or people visiting the area for other reasons.</p>
<p>The car wash is not needed, there are already car washes located on Victoria Street, Hampton Road and in North Fremantle.</p>	<p>Figure 4 illustrates the car washes located in Fremantle. They are generally evenly spaced throughout the area. It is not considered that the approval of a small scale car wash would result in an over densification of car washes in the immediate or surrounding area.</p>
<p>The proposal is not in keeping with the South Fremantle heritage area and will detract from the houses, cafes, restaurants and bars that contribute to the area. The proposed carwash, with its utilitarian design featuring painted cement board and a lightweight steel frame, would starkly contrast with the character of the area. No textural, tonal or formal qualities are present. It appears to be a painted tilt up concrete building.</p>	<p>The applicant submitted an amended design based on advice from the City. The amended design is considered an improvement and will reduce any potential visual impact from the built form.</p>
<p>The noise pollution from the car wash machinery, vehicles, or other operations will be intolerable and incessant for those of us living in close proximity. The 24/7 operation would subject the surrounding residential area to constant machine noise and activity, particularly at night. It would it be best to restrict operation to daytime hours only.</p>	<p>Appropriate conditions of approval are recommended to ensure the development will operate in accordance with the applicable legislation. The operating hours have been reduced to between 6:30am and 10:30pm as per the Operational and Noise Management Plan included as an attachment.</p>
<p>The site is within a predominantly residential area and is an unacceptable use of the mixed use zone.</p>	<p>The proposal has been assessed against the objectives of the Mixed Use zone in the following sections.</p>



<p>There is little to no mention in the proposal of the potential environmental impact of this facility, particularly regarding chemical runoff and airborne pollutants from sprays. These chemicals could negatively affect the health of residents and the surrounding environment, and these risks need to be seriously addressed.</p>	<p>Water run off from the car wash bays will be directed to a series of silt pumps and convey flows through an oily water separation prior to connection to Water Corporation sewer. A standard condition is recommended for storm water and waste water disposal plans to be approved prior to lodgement of a building permit application.</p>
<p>The car wash will also have a detrimental impact on property values in the area. The noise, traffic, light, and other disturbances will likely make the area, particularly my neighbouring property, less desirable for future buyers, and this will inevitably lower the potential sale price for local residents.</p>	<p>The scale and intensity of the development is not considered to be significant. The design of the building has been amended to reduce any detrimental impact to visual amenity. It is not considered that the proposal will have a significant impact on property values in the area.</p>
<p>A better option would be another boutique build similar to 240 South Terrace that would allow more people to live in and enjoy Fremantle.</p>	<p>Assessment of the application must take into considered the proposal that has been lodged.</p>
<p>The proposed car wash is out of touch with a community who do not prioritise pride in car ownership over more important social issues.</p>	<p>More important social issues are not a consideration in the assessment of the proposal. A car wash will provide a service for residents who may not have a space to wash their vehicle.</p>
<p>The 1.8m high wall along South Terrace creates a potential blind spot for pedestrians. Drivers may have difficulty seeing pedestrians crossing in front of the car wash entrance, increasing the risk of accidents.</p>	<p>The front fence is setback 1m from the lot boundary to allow for acceptable sightlines between the street and subject site.</p>
<p>The proposed landscaping is a positive improvement however, the use on non-natives is not desirable. The renders are misleading and depict plants that don't exist. The use of larger hedging shrubs would soften the predominantly harsh brick and concrete appearance of the site.</p>	<p>An amended landscaping plan has been submitted which includes a greater variety of native ground covers and shrubs.</p>



Figure 4 – Map of existing Motor vehicle wash facilities in Fremantle.

Other Advice

City of Fremantle Heritage Comment

The demolition of the existing buildings and structures on site is acceptable as it will not adversely affect the values of the heritage area. New infill development needs to positively respond to the character of the heritage area and comply with LPP3.6 Heritage Areas.

The proposed carwash building is not fully consistent with some of the provisions of LPP3.6 Heritage Areas because it is set back behind the established front setback to both South Terrace and South Street. However, the scale, form and articulation of the building has been modified following the receipt of initial heritage advice and it is now largely sympathetic with the general built form character of the heritage area:



- The building has a simple rectangular plan form.
- The building is a relatively modest structure which is largely single storey but with a small two storey section at the South Street end.
- The roof is a simple hip and gable structure that responds to the largely residential character of South Street and with the character of historic commercial buildings in South Terrace.
- The scale of the building is reduced by the articulated verandah element which interprets traditional building form in the area while also providing amenity for building users.
- Building materials are generally sympathetic with the painted and rendered historic commercial buildings in the area and the colour scheme is discrete using creamy limestone colours to blend with the use of natural stone in the area. Signage is also reasonably discrete and limited in size.
- The bin store and car vacuum bay wall elements are 1800mm high fence like elements but there are large gaps between these elements to allow carwash activity to activate the street.

On balance the proposed development will have only a minor impact on the heritage value of the south Fremantle Heritage Area and is therefore supported. The Heritage Officer Assessment is included as additional information.

City of Fremantle Internal Referral

The application was referred to the relevant internal departments at the City of Fremantle for formal comment as part of the assessment process. All departments were generally satisfied with the proposed development. Any comments received will be addressed via conditions and advice notes. The comments received related to ensuring standard requirements are followed, specifically:

- Building permit required (advice note)
- No unauthorised works within thoroughfare (advice note)
- Construction/demolition management plan (condition)
- Detailed parking plan (condition)
- Vehicle crossover upgrade (advice note)
- Removal of asbestos (advice note)
- Sand drift from property (advice note)
- Noise from construction works (advice note)
- Acoustic noise from development (advice note)

Planning Assessment:

The proposal has been assessed against all the relevant legislative requirements of the Scheme, State and Local Planning Policies, and outlined in the Legislation



and Policy section of this report. The following matters have been identified as key considerations for the determination of this application:

- Land use
- Parking
- Demolition of buildings
- Traffic and access
- Noise
- Signage
- Heritage
- Signage

Land Use

Table 2 – Land Use Permissibility

Provision	Proposal	Zone	Permissibility
Local Planning Scheme 4 clause 3.3 - Zoning Table	Motor Vehicle Wash	Mixed Use	A

A Motor Vehicle Wash is an 'A' use in the Mixed Use zone which means that the use is not permitted unless the Council has exercised its discretion and has granted planning approval after giving special notice in accordance with clause 64 of the *Planning and Development (Local Planning Schemes) Regulations 2015, Schedule 2*. In considering an 'A' land use, the Council will have regard to the matters set out in clause 67 of the Regulations, Schedule 2. In this regard the following matters have been considered:

- (a) the aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area;*
- (c) any approved State planning policy*
- (g) any local planning policy for the Scheme area;*
- (k) the built heritage conservation of any place that is of cultural significance;*
- (l) the effect of the proposal on the cultural heritage significance of the area in which the development is located;*
- (m) the compatibility of the development with its setting, including —*
 - (i) the compatibility of the development with the desired future character of its setting; and*
 - (ii) the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the*



- likely effect of the height, bulk, scale, orientation and appearance of the development;*
- (n) the amenity of the locality including the following —*
- (i) environmental impacts of the development;*
 - (ii) the character of the locality;*
 - (iii) social impacts of the development;***
- (s) the adequacy of —*
- (i) the proposed means of access to and egress from the site; and*
 - (ii) arrangements for the loading, unloading, manoeuvring and parking of vehicles;*
- (t) the amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system in the locality and the probable effect on traffic flow and safety;*
- (w) the history of the site where the development is to be located;*
- (y) any submissions received on the application;*

For the purpose of assessing matter (a) above, the objectives of the Mixed Use zone are as follows:

Development within the mixed use zone shall —

- (i) provide for a mix of compatible land uses including light, services and cottage industry, wholesaling, trade and professional services, entertainment, recreation and retailing of goods and services in small scale premises, including showrooms, where the uses would not be detrimental to the viability of retail activity and other functions of the City Centre, Local Centre and Neighbourhood Centre zones;*
- (ii) provide for residential at upper level, and also at ground level providing the residential component is designed to contribute positively to an active public domain;*
- (iii) ensure future development within each of the mixed used zones is sympathetic with the desired future character of each area;*
- (iv) ensure that development is not detrimental to the amenity of adjoining owners or residential properties in the locality, and*
- (v) conserve places of heritage significance the subject of or affected by the development.*

The proposed development is considered to be consistent with the Regulations and zone objectives for the following reasons:

- The proposal is for a commercial use which includes three (3) wash bays and four (4) vacuums. The scale and intensity of the development is



considered appropriate and should not result in any significant impact to the amenity of the locality.

- The approval of this development will not result in an over-densification of motor vehicle wash facilities in the area.
- It is considered that a Motor vehicle wash will provide a convenient service for surrounding residents on a site that has historically been utilised for commercial purposes.
- Any potential impact to the amenity of the area will be minimised by the design of the wash bays which feature self-closing doors. People will not leave their vehicles and the operation of the wash cycle is automatic. The enclosing of vehicles in the wash bay will minimise any spray drift and reduce impact from noise.
- A 2.1m high noise barrier is proposed along the east boundary and a 1.8m high barrier along a portion of the west boundary to mitigate noise from the wash bays and vacuums.
- The Motor vehicle wash and all associated facilities will cease operation at 10:30pm, with reduced operations between the 'night period' (as defined by the Noise Regulations) between 10pm and 10:30pm. The operating hours indicated in the Amended Operational and Noise Management Plan have been included as a recommended condition.
- The design of the proposal has undergone several revisions and is supported by City Heritage Officers. There should be no impact on the amenity of the area resulting from the height, bulk, scale or appearance of the building. The conservation of the heritage significance of the area has been a key focus of this development and is addressed further in the following sections.
- It is considered that the proposal will operate without resulting in any significant impact to the traffic network in the locality, as addressed in the following sections



Figure 5 – Illustration of land uses surrounding the subject site.

Parking

The proposed onsite vehicle parking has been assessed against LPS 4 cl. 4.7.2 Vehicle Parking Requirements. Refer Table 3 below.

Table 3 – Required vehicle parking

Element	Requirement	Proposed	Extent of Variation
Motor Vehicle Wash	Car bays required 2: 1 wash bay Required: 6 bays	7 bays (inclusive of 4 vacuum bays)	Complies

The provision of onsite carparking complies with the requirements of LPS4. A standard condition is recommended for a detailed parking plan design which complies with the *Australian Standard AS/NZS 2890* and *AS/NZS 1428*.



Demolition of Buildings

The requirements for the demolition of a building are prescribed under clause 4.14 of LPS4.

4.14.1 Council will only grant planning approval for the demolition of a building or structure where it is satisfied that the building or structure:

- (a) has limited or no cultural heritage significance, and*
- (b) does not make a significant contribution to the broader cultural heritage significance and character of the locality in which it is located.*

4.14.2 In considering an application under 4.14.1, Council shall have regard to any heritage assessment required under the Planning and Development (Local Planning Schemes) Regulations 2015.

The proposed development is considered to be consistent with clause 4.14 and can be supported for the following reasons:

- The building located on the subject site was constructed between 1961 and 1965 based on historical aerials. The building is considered to be a simple brick and iron warehouse. An assessment was completed by City Heritage Officers and it is considered that the place is of little cultural heritage significance and contributes minimally to the heritage character of the West End Heritage Area.

Traffic and Access

The proposed Motor vehicle wash features crossovers to allow vehicles to enter from South Street and exit onto South Terrace. The crossovers are located adjacent to the existing crossovers as illustrated by Figure 6 below. Appropriate conditions are recommended for the removal of redundant crossovers and the construction of new crossovers prior to occupation. The site will allow for up to 12 vehicles to queue while waiting for a car wash bay to become available.

The applicant has submitted an Amended Transport Impact Statement (TIS) which is included as an attachment. The TIS seeks to demonstrate that the proposed development will operate without resulting in any significant impact to the traffic network in the locality.

The TIS was referred to the City's Engineering Infrastructure team for review with the following comments provided.



- *The amended TIS submitted for the proposed development is generally adequate, with modelling and trip generation rates appropriate. Under typical operating conditions, the development is expected to maintain acceptable traffic flow and internal vehicle circulation.*
- *However, some concerns remain regarding vehicles turning right in to the subject site from South Street and vehicles exiting the site turning right onto South Terrace near a signalised intersection.*
- *These issues present safety/operational risks as well as potential congestion issues **which, due to the site's constrained location and surrounding road network, may not be fully resolvable based on the current proposal.***
- *City Officers would consider both accesses being left in / left out with minor civil works such as median island extension, line marking and signage (in-line with City of Fremantle standards and specifications) as being acceptable to resolve traffic congestion and safety concerns.*
- *A design to address the works must **be submitted to the City's Engineering Infrastructure Services** for approval prior to the lodgement of a Building Permit application. All works are to be completed at the **applicant's** expense.*

Based on the advice above, a condition is recommended detailing that access to the subject site via the South Street crossover shall be restricted to left turn in only (from vehicles travelling in a westerly direction along South Street) and vehicle egress from the site via the South Terrace crossover site shall be restricted to left turn out only (vehicles to leave the subject site in a southerly direction along South Terrace). It is considered that this will resolve potential safety/operational risks and avoid any additional congestion in the South Street/South Terrace intersection.



Heritage

The subject site is located in the South Fremantle Precinct Heritage Area and is subject to assessment against Local Planning Policy 3.6 – Heritage Areas. Refer to the table below.

<i>3.6 Infill development (new buildings)</i>	
3.6.1 Intent	
<p>New buildings within a heritage area should respect and complement the heritage significance of the area. A respectful design approach gives special consideration to the siting, scale, architectural style and form, materials and finishes of the proposed development in relation to its neighbours, without copying historic detailing or decoration. New infill buildings should respond sympathetically to the heritage values of the heritage area as a whole, and also to that part of the heritage area in the vicinity of the proposed development. Imaginative, well designed and harmonious construction is encouraged.</p>	<p><i>Officer Comments</i> The building is a relatively modest structure which is largely single storey. The roof is a simple hip and gable structure that responds to the largely residential character of South Street and with the character of historic commercial buildings in South Terrace.</p>
3.6.2 Design guidance	
<p><i>Siting and Scale</i></p> <p>i. New infill development within a heritage area should:</p> <p>a) Maintain a setting that is consistent with the original streetscape, including front and side setback patterns.</p> <p>b) Have a consistent bulk and scale in relation to the original street pattern. E.g. If the original street pattern is single storey then the new infill development should also be (or present as) single storey (at least to the front section of the lot).</p> <p>c) Have a plate height consistent with the original street pattern. New developments often propose a lower plate height than the earlier and original buildings. To ensure a consistency of scale the plate height is an important element to ensure it is consistent with the original street pattern.</p> <p>ii. New Infill development to secondary streets will be assessed on individual circumstances and merit. Issues to consider include:</p> <p>a) Prevailing streetscape and setbacks of the side street</p>	<p><i>Officer Comments</i> As per advice from City Heritage Officers, a proposal that responded to the established street setbacks would have been preferable however, on balance the proposed development will have only a minor impact on the heritage value of the South Fremantle Precinct Heritage Area due to the building being setback into the site.</p>



<p>b) Avoiding a continuous wall and providing articulation of walls to a secondary street.</p> <p>c) Avoiding a two-storey height wall to the side street, unless the prevailing streetscape is predominantly two-storey.</p> <p>iii. Street setbacks deemed to comply with the above are specified in Schedule 1 for some areas.</p>	
<p><i>Building Form</i></p> <p>The form of the building is its overall shape, size and the general arrangement of its main parts.</p> <p>i. New infill building within a heritage area should respect and harmonise with and be sympathetic to the predominant form of the prevailing streetscape without mimicking heritage detailing.</p> <p>ii. Where a building form is highly repetitive, significant departures in form will appear at variance to the streetscape and should not be introduced.</p> <p>iii. The treatment of new infill buildings in terms of the roof form, proportions, materials, number, size and orientation of openings, ratio of window to wall etc. should relate to that of its neighbours.</p> <p>iv. Symmetry or asymmetry of facades in the prevailing streetscape is an element of form to be kept consistent.</p> <p>v. Contemporary building designs should respond to, and interpret, the scale, articulation and detail of the existing nearby buildings in a modern, innovative and sympathetic way.</p>	<p><i>Officer Comments</i></p> <p>The proposal includes a building with a simple rectangular form. It is not attempting to mimic or recreate any heritage detailing in the area and will be sympathetic to the surrounding built form consisting of single storey residential and commercial/ mixed use two storey buildings.</p>
<p><i>Materials, Colours and Detailing</i></p> <p>i. Materials and level of detailing should reflect / interpret the predominant materials and detailing of the original prevailing streetscape and not visually dominate the streetscape or adjacent heritage buildings.</p> <p>ii. Whilst the basic form, scale and structure of new development should be consistent with the character of the area, new buildings should not seek to emulate heritage detailing to any great extent: 'Faux' or 'mock' heritage detracts from an understanding and appreciation of the original building and will not be supported. New development should blend in with the streetscape but be discernible as new when looked at more closely.</p>	<p><i>Officer Comments</i></p> <p>Building materials are generally sympathetic with the painted and rendered historic commercial buildings in the area and the colour scheme is discrete using creamy limestone colours to blend with the use of natural stone in the area. Signage is also reasonably discrete and limited in size.</p>



<p>iii. Use of original or traditional colours is encouraged. Glossy materials or finishes should be avoided unless a historical precedent for their use can be demonstrated.</p>	
<p><i>Other Elements</i> <i>Roofs</i> i. Traditionally roof lines are a predominant element of the streetscape. All new infill development shall respond to and reinforce the existing characteristics of the prevailing streetscape regarding plate and wall heights, roof form, ridge lines, parapet lines, roof slopes and eaves overhangs. ii. Roof forms that interpret the predominant roof forms of the prevailing streetscape may be considered. <i>Verandahs / Porches / Awnings</i> i. Verandahs, porches and awnings were often an important element of streetscapes. Inclusion of verandahs, porches and awnings appropriate to the streetscape are encouraged without too precisely mimicking the style of the original character-building elements or heritage detailing.</p>	<p><i>Officer Comments</i> The roof is a simple hip and gable structure that responds to the largely residential character of South Street and with the character of historic commercial buildings in South Terrace The articulated verandah element interprets traditional building form in the area and will provide amenity for building users.</p>

Signage

The proposal features the following wall signs:

- 3.1m (w) x 1.5m (h) sign located on the front fence on north west corner of the subject site.
- 3.4m (w) x 1m (h) sign located on the north elevation of the building.
- 3.5m (w) x 1.2m (h) sign located on the west elevation of the building.

The proposed signage is assessed against Local Planning Policy 2.14 Advertisement Policy. Refer to the table below.

PROVISION	REQUIREMENT	ACCEPTABLE
<p>1.1 General requirements applicable to all signs:</p>	<p>(a) Advertisements will not be approved on properties primarily used for residential purposes where the advertisement does not pertain to a relevant home business, occupation or store on the site unless otherwise</p>	<p>The signage relates specifically to the Motor vehicle wash proposed to operate on the site. The signage is not proposed to be illuminated and are not</p>



	<p>provided for in another local planning policy.</p> <p>(b) Advertisements are to be located and designed so as not to cause a hazardous distraction to motorists, pedestrians or other road users.</p> <p>(c) Advertisements will be compatible with the style, scale and character of the surrounding streetscape, and the predominant uses within the locality. Consideration will be given to the number and type of existing signs in the locality so as to avoid visual clutter.</p> <p>(d) Advertisements shall not impede pedestrian or vehicle movements.</p> <p>(e) Illuminated signs are to be maintained to operate as an illuminated sign; and</p> <p>(f) Advertisements are not to emit a flashing or moving light or radio; animation or movement in its design or structure; reflective, retro-reflective or fluorescent materials in its design structure.</p> <p>(g) Advertisements in the form of an Animated signs will not, be supported by Council.</p> <p>(h) Advertisements will not be approved on private land which include,</p> <ul style="list-style-type: none"> i. the name, logo, or symbol of a company or other organisation that does not own or substantially occupy the site or building on which the advertisement is located, or ii. a product or service not provided on the site on which the advertisement is located; iii. a product or service that does not form part of the signage displaying the name, logo or symbol; of a company or other organisation that owns or substantially occupy the site or building on which the advertisement is located; or 	<p>of a scale that would cause any distraction.</p> <p>The scale of the signage is appropriate in relation to the building.</p> <p>There will be no impeding of pedestrian or traffic movements.</p>
--	--	--



	iv. signs for an activity or event not occurring on the site on which the advertisement is located.	
Wall Signage	(a) The advertisement does not project above the fascia of the building and does not exceed the frontage of the tenancy; and (b) The advertisement(s) are restricted to three signs per street frontage per tenancy.	The signage will not project above the fascia of the building. The signage proposal includes one sign on the north elevation (facing South Street), one sign on the west elevation (facing South Terrace) and a sign on the fence located on the north west corner.

State Planning Policy

State Planning Policy 7.0 - Design of the Built Environment

An assessment has been conducted against the design principles of SPP7.0 which are Context and character, Landscape quality, built form and scale, Functionality and build quality, Sustainability, Amenity, Legibility, Safety, Community, and Aesthetics.

Refer comments below:

- The proposal has been carefully considered against the distinctive characteristics of the area. It is considered that the overall design is a substantial improvement on the original proposal and will not detract from the context, character and heritage value of the area.
- The applicant submitted an amended landscaping plan which included additional shrubs and small trees based on advice provided by City Officers. The proposed landscaping will reduce the visual impact of the built form and contribute to the amenity of the area.
- The built form of the development was subject to several revisions based on advice from City Officers. It is considered that the amended design is a significant improvement and will have no impact on the amenity of the area.
- The proposal includes a legible building design with the street aspects oriented parallel to the boundaries. While the building is setback further than the adjoining buildings fronting South Street and South Terrace, it is considered that this is necessary to allow for vehicle circulation/means of access and egress.
- The traffic safety of the site has been considered. The recommended conditions restricting vehicle access/egress (left turn in and out) will resolve potential traffic safety and congestion concerns. In addition, the proposal features suitable sight lines to ensure safety of pedestrians.



- The proposed building is a simple design with materials and finishes which will contribute to the aesthetic of the South Fremantle area.

Conclusion:

Approval is sought for the demolition of an existing building and the construction of a Motor vehicle wash development and signage at No. 234 (Lot 27, 29, 300 & 301) South Terrace, South Fremantle. The proposed development is consistent with the objectives of the Mixed Use zone and will provide a service for surrounding residents. The proposal will not have a significant impact on the heritage significance of the South Fremantle Precinct Heritage Area. The proposal is supported by the City's Heritage Officers who have deemed the works acceptable, after several revisions of the design. In accordance with the above, the application is recommended for conditional approval.



PART C – OTHER BUSINESS

- 1. State Administrative Tribunal Applications and Supreme Court Appeals**
- 2. Meeting Closure**